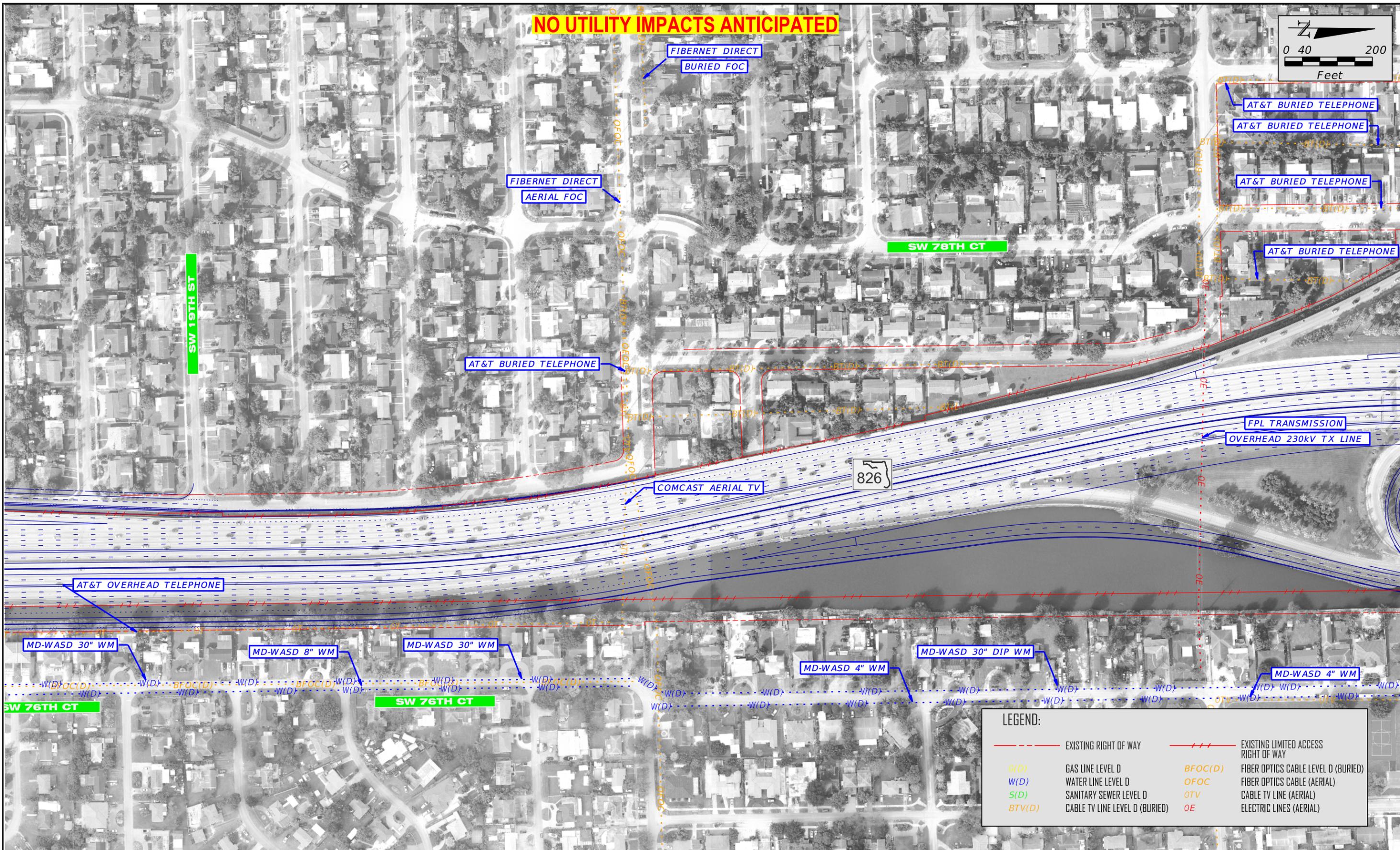
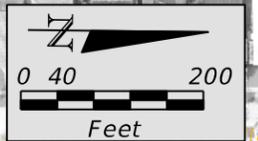
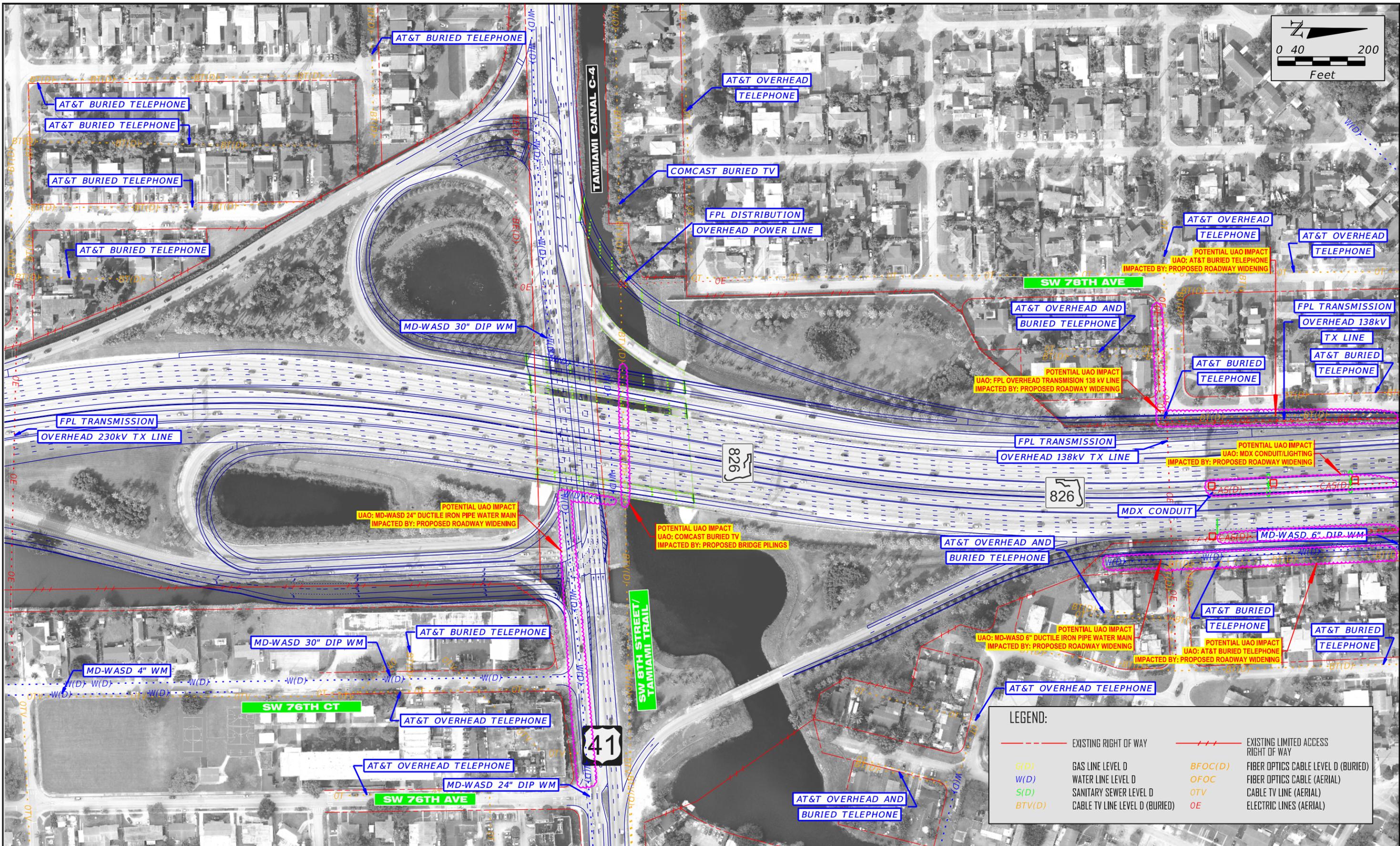
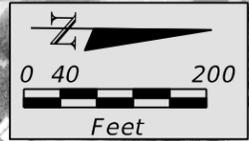


**NO UTILITY IMPACTS ANTICIPATED**



**LEGEND:**

	EXISTING RIGHT OF WAY		EXISTING LIMITED ACCESS RIGHT OF WAY
	G(D) GAS LINE LEVEL D		BFOC(D) FIBER OPTICS CABLE LEVEL D (BURIED)
	W(D) WATER LINE LEVEL D		OFOC FIBER OPTICS CABLE (AERIAL)
	S(D) SANITARY SEWER LEVEL D		OTV CABLE TV LINE (AERIAL)
	BTV(D) CABLE TV LINE LEVEL D (BURIED)		OE ELECTRIC LINES (AERIAL)

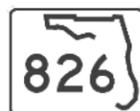


**LEGEND:**

---	EXISTING RIGHT OF WAY	---	EXISTING LIMITED ACCESS RIGHT OF WAY
G(D)	GAS LINE LEVEL D	BFOC(D)	FIBER OPTICS CABLE LEVEL D (BURIED)
W(D)	WATER LINE LEVEL D	OFOC	FIBER OPTICS CABLE (AERIAL)
S(D)	SANITARY SEWER LEVEL D	OTV	CABLE TV LINE (AERIAL)
BTV(D)	CABLE TV LINE LEVEL D (BURIED)	OE	ELECTRIC LINES (AERIAL)



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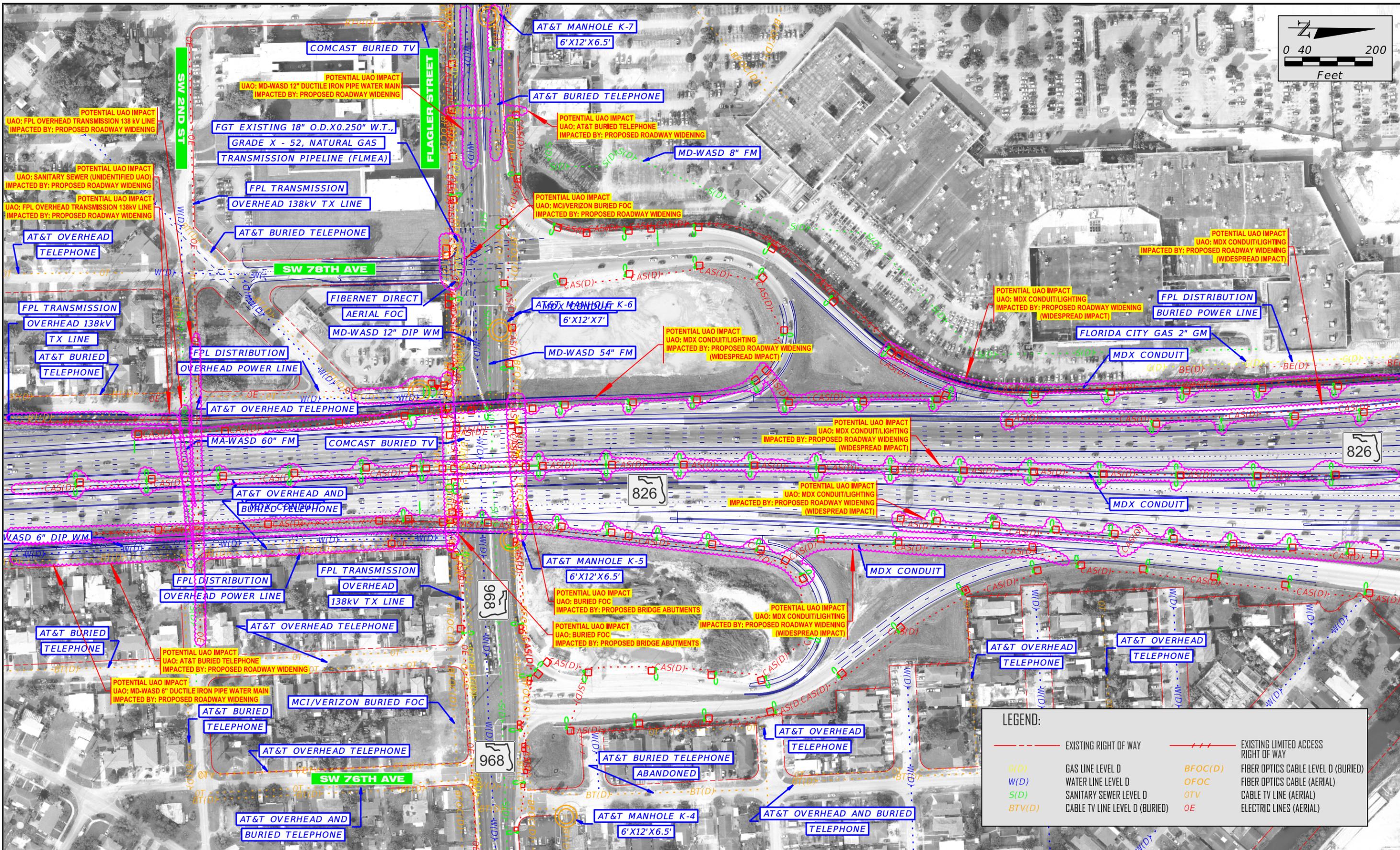
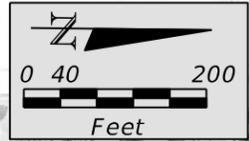


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 From US 1/SR 5/Dixie Highway to SR 836/Dolphin Expressway  
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**SR 826/PALMETTO EXPRESSWAY  
 UTILITY IMPACT ASSESSMENT  
 ALTERNATIVE 2**

SHEET NO.  
 14



**LEGEND:**

	EXISTING RIGHT OF WAY		EXISTING LIMITED ACCESS RIGHT OF WAY
	GAS LINE LEVEL D		FIBER OPTICS CABLE LEVEL D (BURIED)
	WATER LINE LEVEL D		FIBER OPTICS CABLE (AERIAL)
	SANITARY SEWER LEVEL D		CABLE TV LINE (AERIAL)
	CABLE TV LINE LEVEL D (BURIED)		ELECTRIC LINES (AERIAL)



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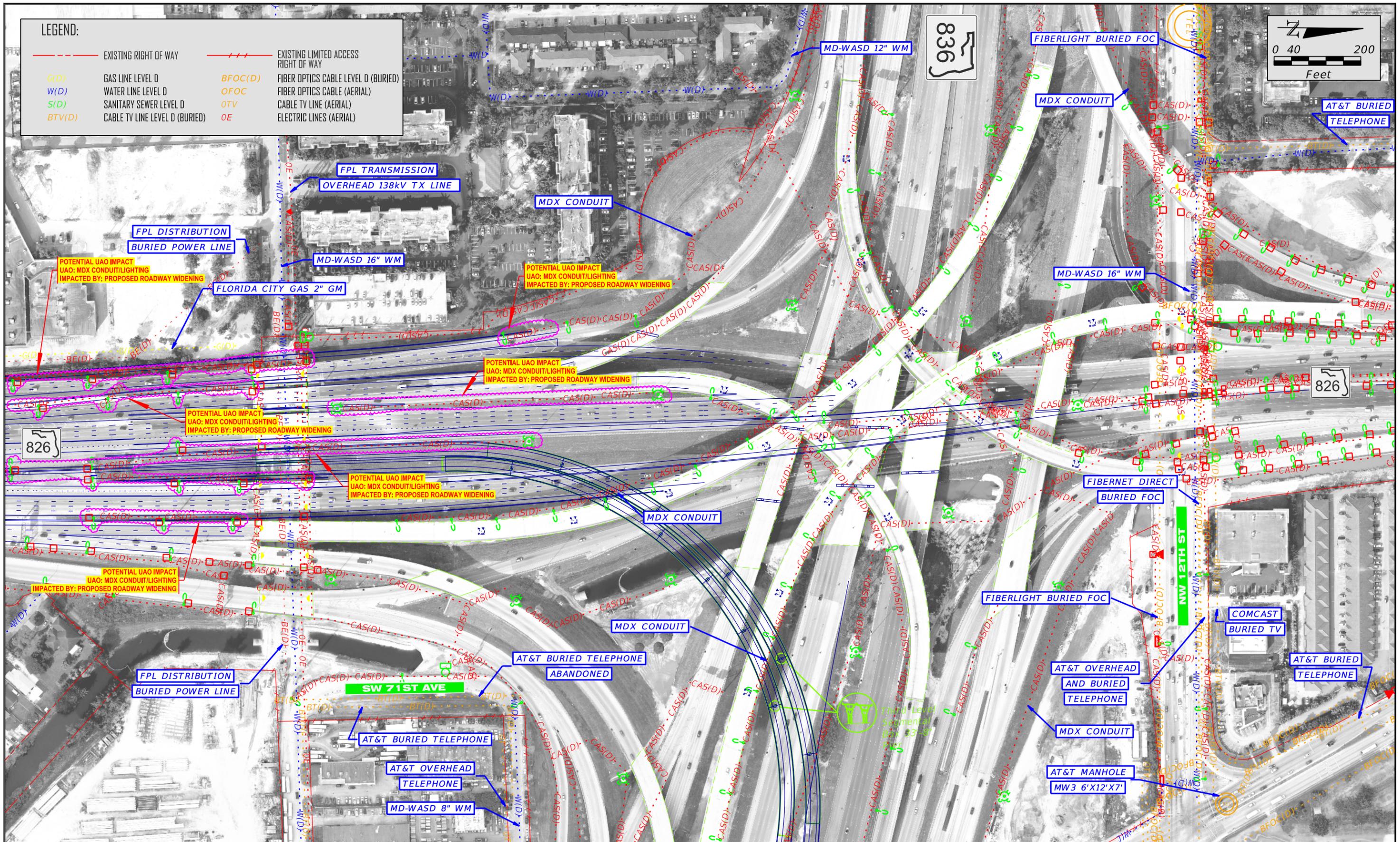
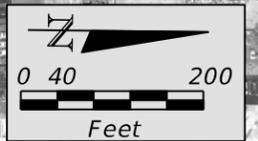
**SR 826/PALMETTO EXPRESSWAY  
 UTILITY IMPACT ASSESSMENT  
 ALTERNATIVE 2**

SHEET NO.  
**15**

JULY 2019

**LEGEND:**

- |         |                                |         |                                      |
|---------|--------------------------------|---------|--------------------------------------|
| ---     | EXISTING RIGHT OF WAY          | ---     | EXISTING LIMITED ACCESS RIGHT OF WAY |
| G(D)    | GAS LINE LEVEL D               | BFOC(D) | FIBER OPTICS CABLE LEVEL D (BURIED)  |
| W(D)    | WATER LINE LEVEL D             | OFOC    | FIBER OPTICS CABLE (AERIAL)          |
| S(D)    | SANITARY SEWER LEVEL D         | OTV     | CABLE TV LINE (AERIAL)               |
| BT(V/D) | CABLE TV LINE LEVEL D (BURIED) | OE      | ELECTRIC LINES (AERIAL)              |



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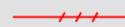
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ETDM No.: 14308

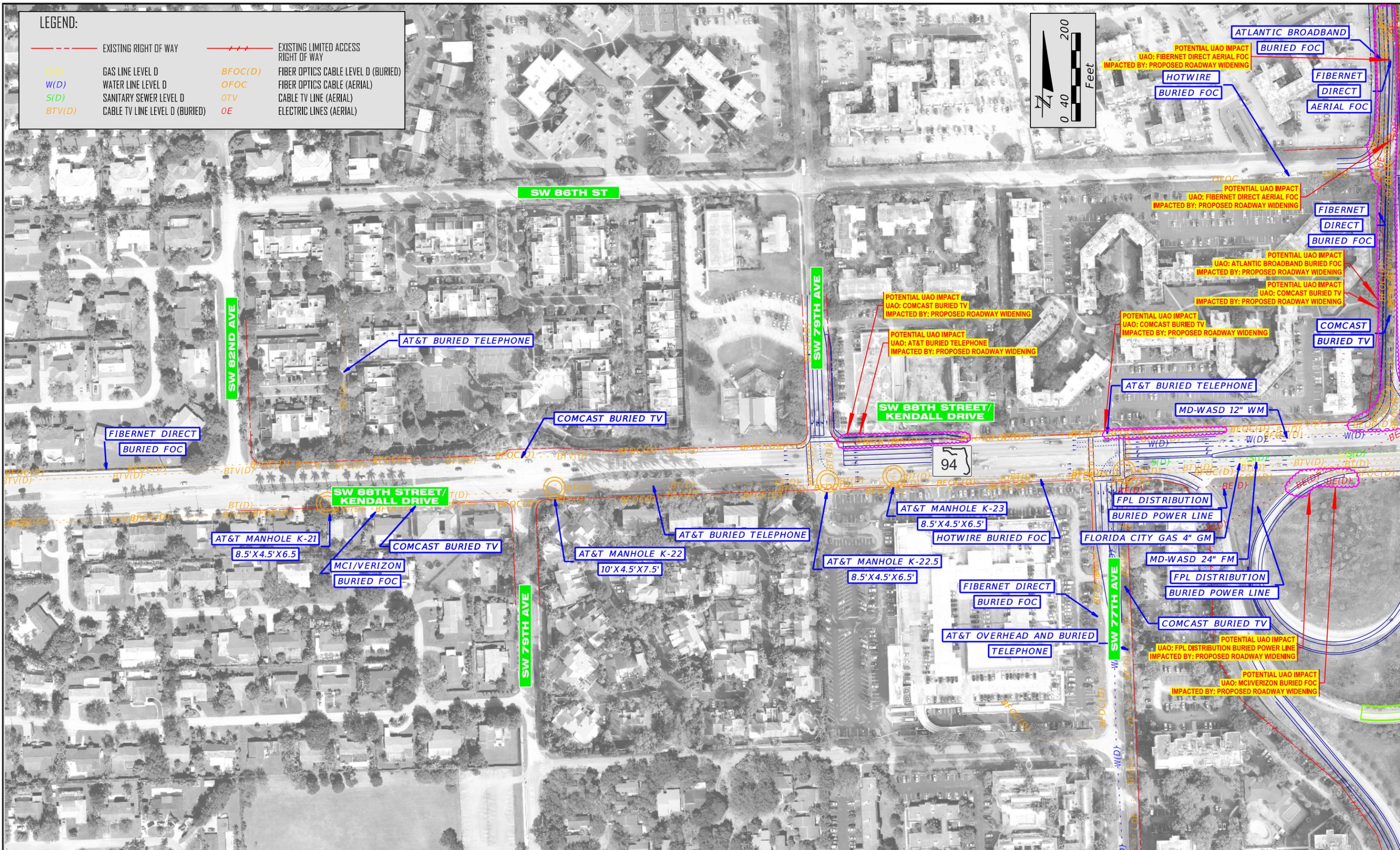
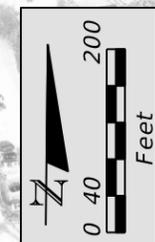
**SR 826/PALMETTO EXPRESSWAY  
UTILITY IMPACT ASSESSMENT  
ALTERNATIVE 2**

SHEET NO.

16

**LEGEND:**

- |                                                                                   |                                         |                                                                                   |                                             |
|-----------------------------------------------------------------------------------|-----------------------------------------|-----------------------------------------------------------------------------------|---------------------------------------------|
|  | EXISTING RIGHT OF WAY                   |  | EXISTING LIMITED ACCESS RIGHT OF WAY        |
|  | G(D) GAS LINE LEVEL D                   |  | BFOC(D) FIBER OPTICS CABLE LEVEL D (BURIED) |
|  | W(D) WATER LINE LEVEL D                 |  | OFOC FIBER OPTICS CABLE (AERIAL)            |
|  | S(D) SANITARY SEWER LEVEL D             |  | OTV CABLE TV LINE (AERIAL)                  |
|  | BT(V(D)) CABLE TV LINE LEVEL D (BURIED) |  | OE ELECTRIC LINES (AERIAL)                  |



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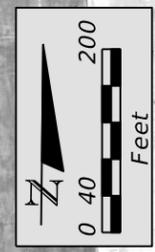
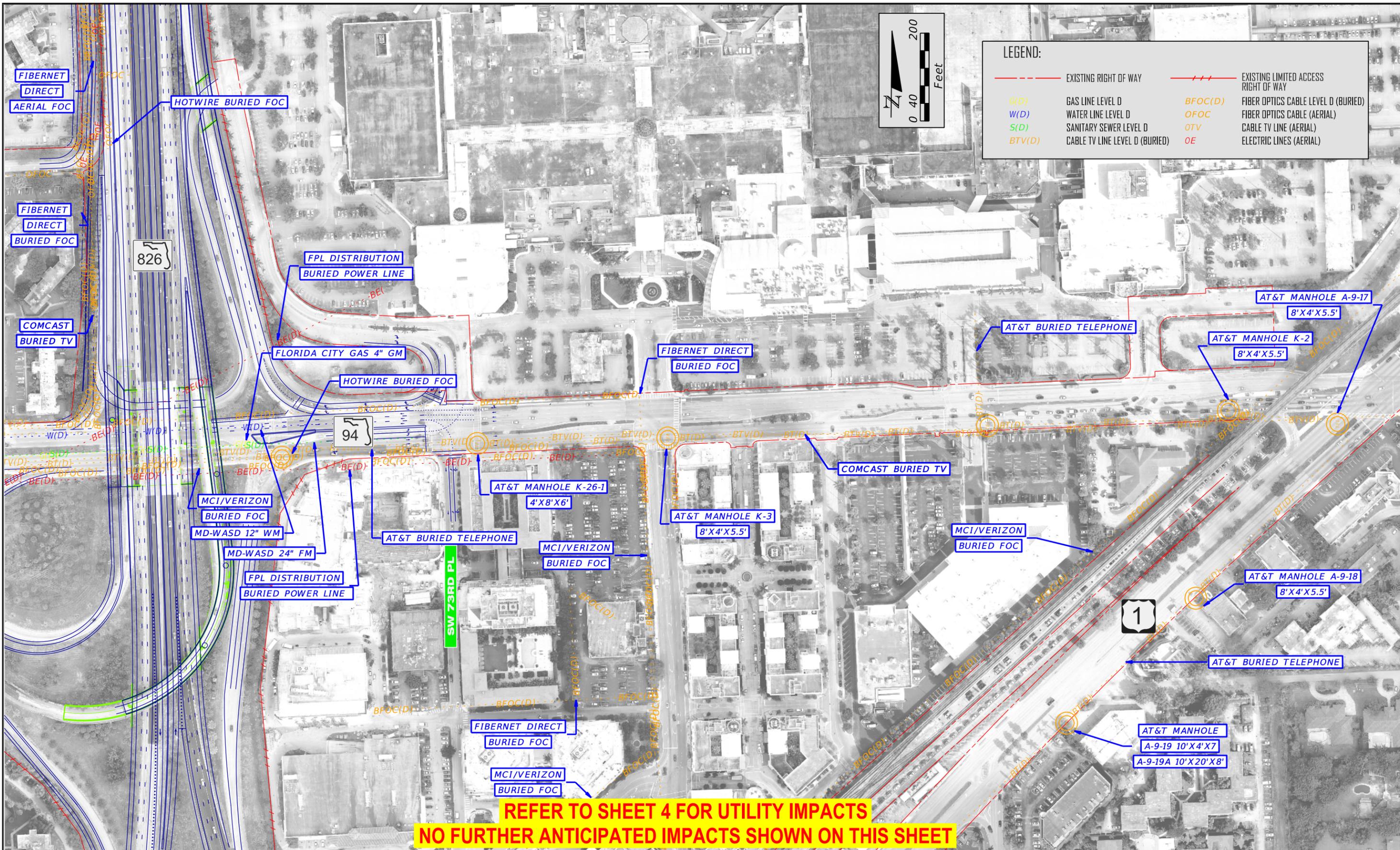
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ALTERNATIVE 2**

SHEET NO.  
17



**LEGEND:**

	EXISTING RIGHT OF WAY		EXISTING LIMITED ACCESS RIGHT OF WAY
	G(D) GAS LINE LEVEL D		BFOC(D) FIBER OPTICS CABLE LEVEL D (BURIED)
	W(D) WATER LINE LEVEL D		OFOC FIBER OPTICS CABLE (AERIAL)
	S(D) SANITARY SEWER LEVEL D		OTV CABLE TV LINE (AERIAL)
	BTV(D) CABLE TV LINE LEVEL D (BURIED)		OE ELECTRIC LINES (AERIAL)

**REFER TO SHEET 4 FOR UTILITY IMPACTS  
NO FURTHER ANTICIPATED IMPACTS SHOWN ON THIS SHEET**

**LEGEND:**

	EXISTING RIGHT OF WAY		EXISTING LIMITED ACCESS RIGHT OF WAY
	G(D) GAS LINE LEVEL D		BFOC(D) FIBER OPTICS CABLE LEVEL D (BURIED)
	W(D) WATER LINE LEVEL D		OFOC FIBER OPTICS CABLE (AERIAL)
	S(D) SANITARY SEWER LEVEL D		OTV CABLE TV LINE (AERIAL)
	BTV(D) CABLE TV LINE LEVEL D (BURIED)		OE ELECTRIC LINES (AERIAL)



**REFER TO SHEET 6 FOR UTILITY IMPACTS  
NO FURTHER ANTICIPATED IMPACTS SHOWN ON THIS SHEET**



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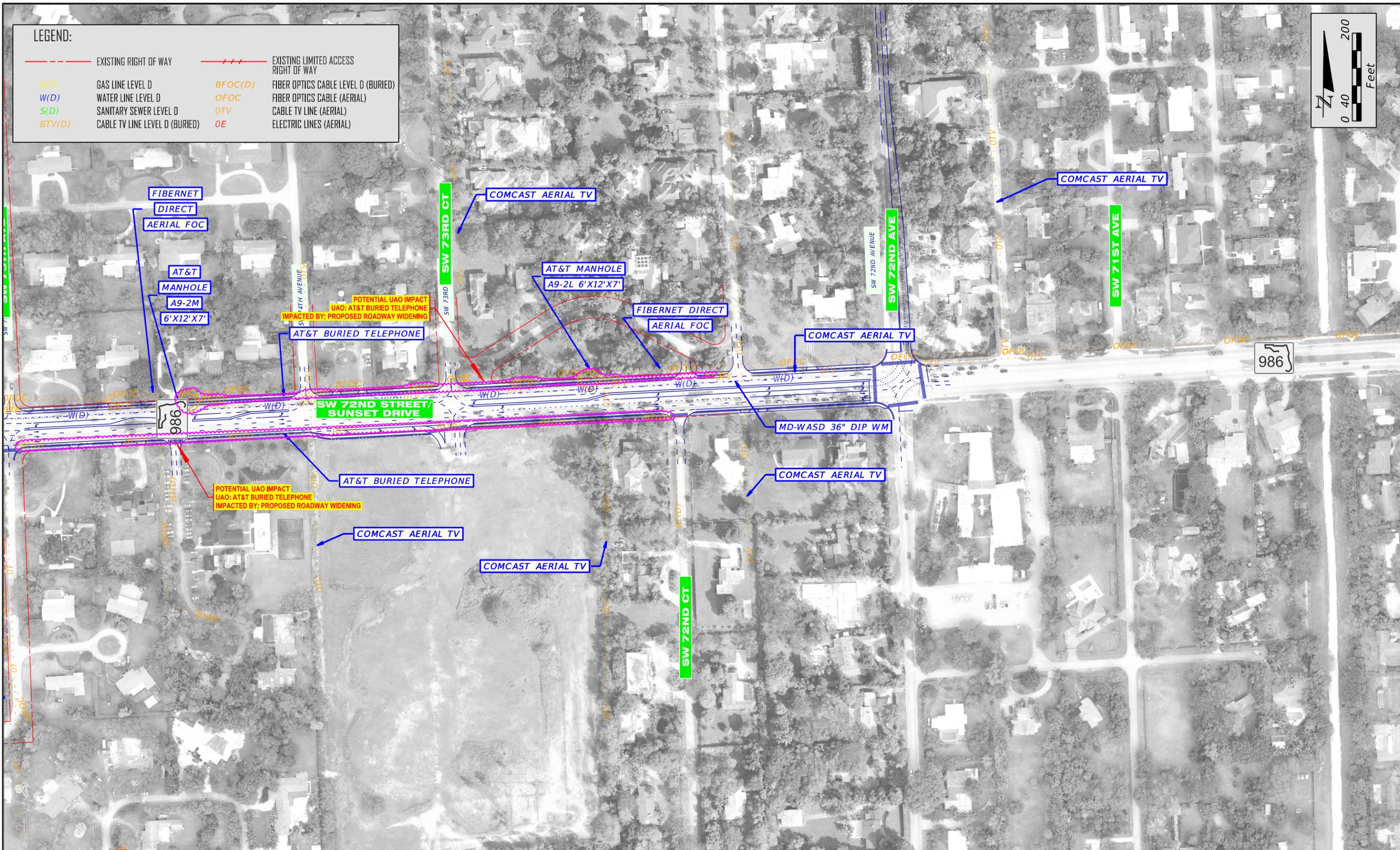
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ALTERNATIVE 2**

SHEET NO.  
**19**

**LEGEND:**

	EXISTING RIGHT OF WAY		EXISTING LIMITED ACCESS RIGHT OF WAY
	GAS LINE LEVEL D		FIBER OPTICS CABLE LEVEL D (BURIED)
	WATER LINE LEVEL D		FIBER OPTICS CABLE (AERIAL)
	SANITARY SEWER LEVEL D		CABLE TV LINE (AERIAL)
	CABLE TV LINE LEVEL D (BURIED)		ELECTRIC LINES (AERIAL)



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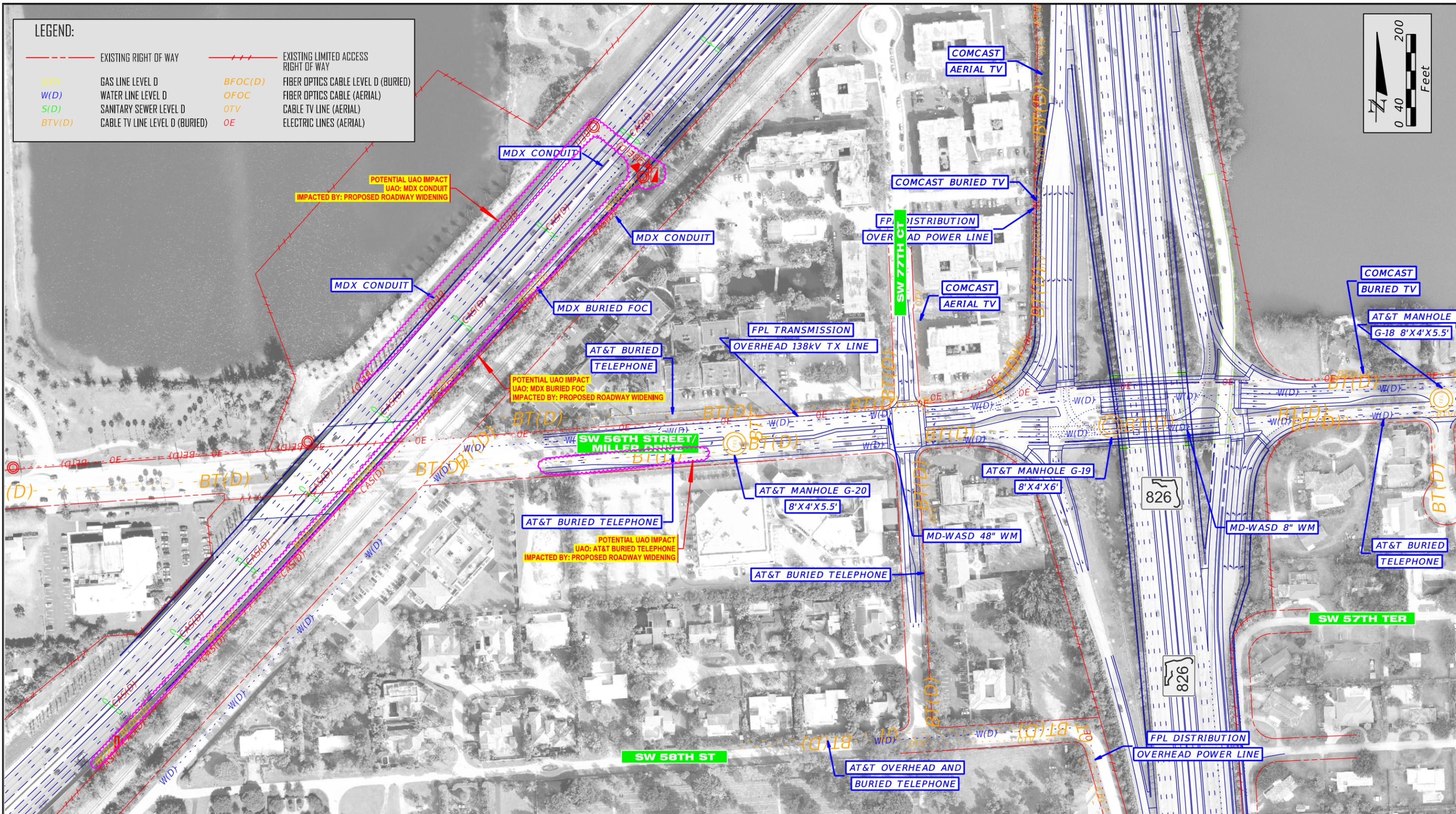
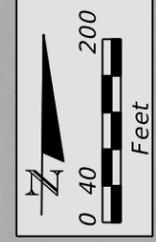
**SR 826/PALMETTO EXPRESSWAY  
 UTILITY IMPACT ASSESSMENT  
 ALTERNATIVE 2**

SHEET NO.

20

**LEGEND:**

	EXISTING RIGHT OF WAY		EXISTING LIMITED ACCESS RIGHT OF WAY
	GAS LINE LEVEL D		BFOC(D) FIBER OPTICS CABLE LEVEL D (BURIED)
	WATER LINE LEVEL D		OFOC FIBER OPTICS CABLE (AERIAL)
	SANITARY SEWER LEVEL D		OTV CABLE TV LINE (AERIAL)
	CABLE TV LINE LEVEL D (BURIED)		OE ELECTRIC LINES (AERIAL)



**REFER TO SHEET 8 FOR ADDITIONAL UTILITY IMPACTS AT INTERCHANGE  
ADDITIONAL ANTICIPATED IMPACTS ARE SHOWN ON THIS SHEET**



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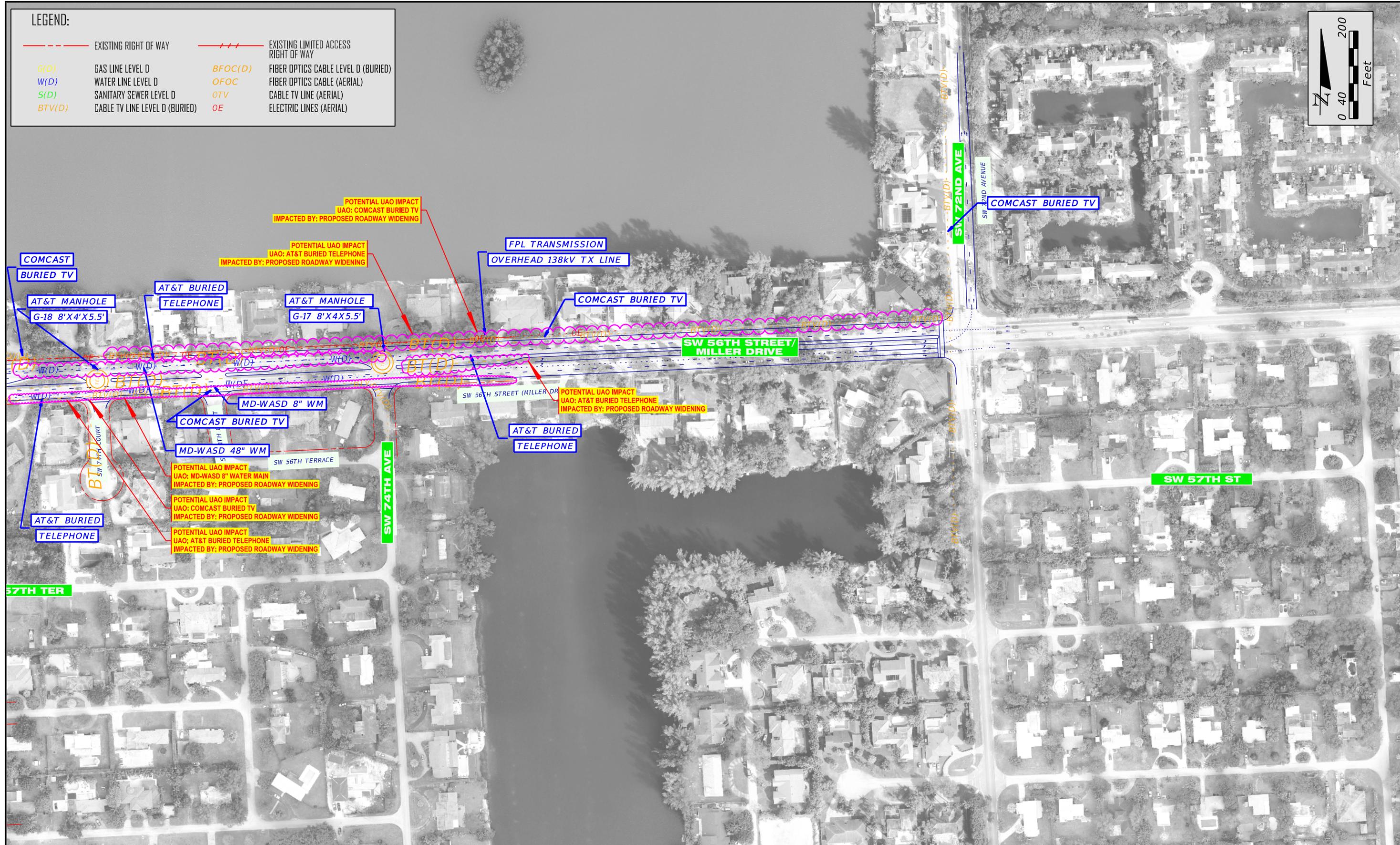
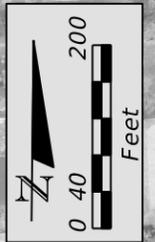
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UTILITY IMPACT ASSESSMENT  
ALTERNATIVE 2**

SHEET NO.  
**21**

**LEGEND:**

	EXISTING RIGHT OF WAY		EXISTING LIMITED ACCESS RIGHT OF WAY
	G(D) GAS LINE LEVEL D		BFOC(D) FIBER OPTICS CABLE LEVEL D (BURIED)
	W(D) WATER LINE LEVEL D		OFOC FIBER OPTICS CABLE (AERIAL)
	S(D) SANITARY SEWER LEVEL D		OTV CABLE TV LINE (AERIAL)
	BTVD(C) CABLE TV LINE LEVEL D (BURIED)		OE ELECTRIC LINES (AERIAL)



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 UTILITY IMPACT ASSESSMENT  
 ALTERNATIVE 2**

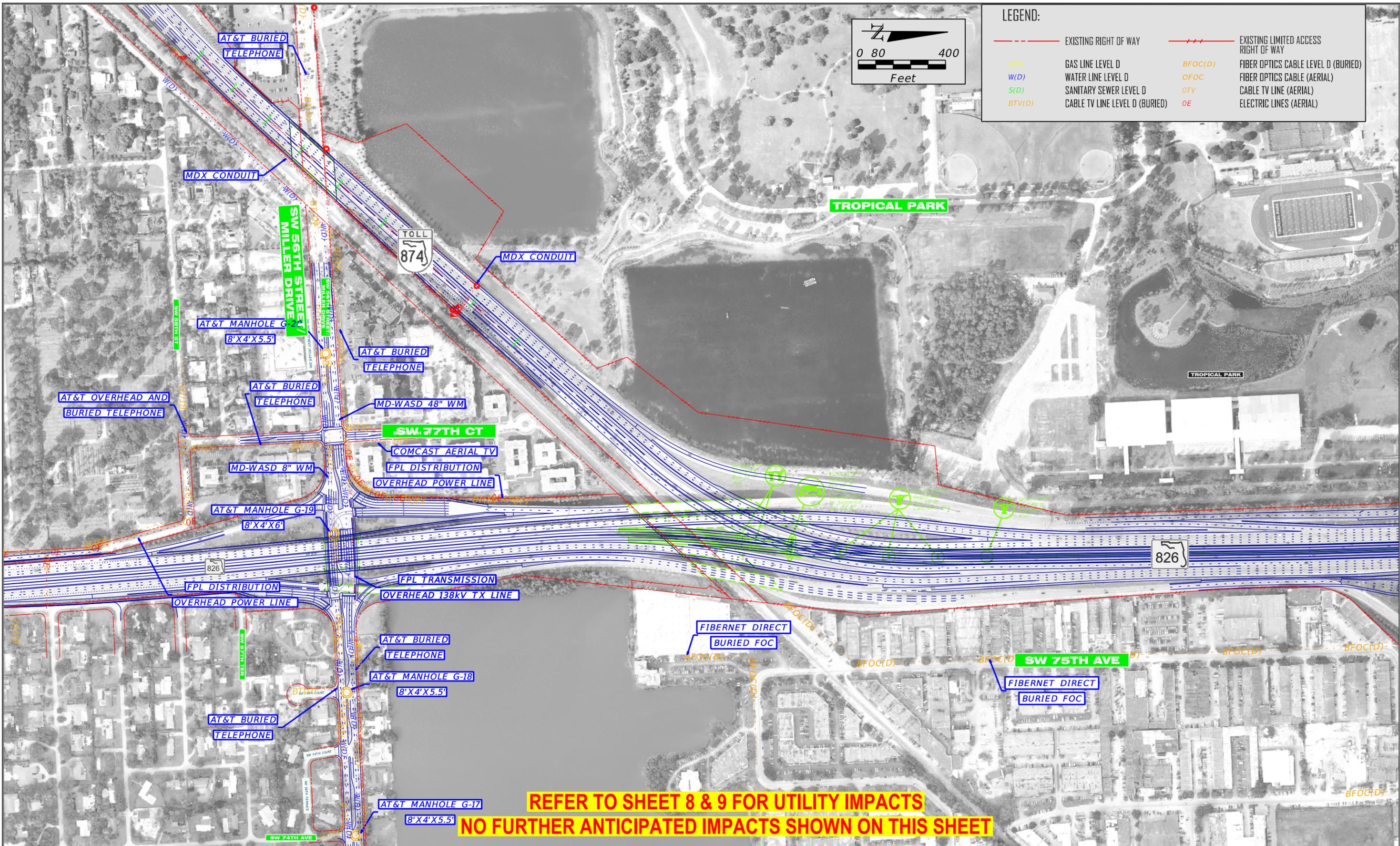
SHEET NO.

22



**LEGEND:**

	EXISTING RIGHT OF WAY		EXISTING LIMITED ACCESS RIGHT OF WAY
	G(D) GAS LINE LEVEL D		BFOC(D) FIBER OPTICS CABLE LEVEL D (BURIED)
	W(D) WATER LINE LEVEL D		OFOC FIBER OPTICS CABLE (AERIAL)
	S(D) SANITARY SEWER LEVEL D		OTV CABLE TV LINE (AERIAL)
	BTV(D) CABLE TV LINE LEVEL D (BURIED)		OE ELECTRIC LINES (AERIAL)



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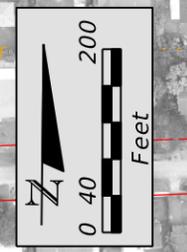
SHEET NO.  
 23

JULY 2019



**LEGEND:**

	EXISTING RIGHT OF WAY		EXISTING LIMITED ACCESS RIGHT OF WAY
	GAS LINE LEVEL D		FIBER OPTICS CABLE LEVEL D (BURIED)
	WATER LINE LEVEL D		FIBER OPTICS CABLE (AERIAL)
	SANITARY SEWER LEVEL D		CABLE TV LINE (AERIAL)
	CABLE TV LINE LEVEL D (BURIED)		ELECTRIC LINES (AERIAL)



**REFER TO SHEET 10 FOR UTILITY IMPACTS  
NO FURTHER ANTICIPATED IMPACTS SHOWN ON THIS SHEET**



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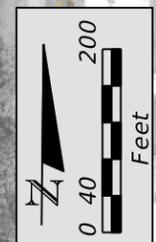
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ALTERNATIVE 2**

SHEET NO.  
**24**

**LEGEND:**

	EXISTING RIGHT OF WAY		EXISTING LIMITED ACCESS RIGHT OF WAY
	G(D) GAS LINE LEVEL D		BFOC(D) FIBER OPTICS CABLE LEVEL D (BURIED)
	W(D) WATER LINE LEVEL D		OFOC FIBER OPTICS CABLE (AERIAL)
	S(D) SANITARY SEWER LEVEL D		OTV CABLE TV LINE (AERIAL)
	BT(D) CABLE TV LINE LEVEL D (BURIED)		OE ELECTRIC LINES (AERIAL)



**REFER TO SHEET 10 FOR UTILITY IMPACTS  
NO FURTHER ANTICIPATED IMPACTS SHOWN ON THIS SHEET**



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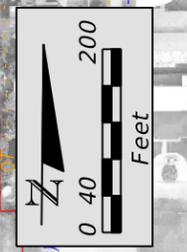
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ALTERNATIVE 2**

SHEET NO.  
**25**

**LEGEND:**

	EXISTING RIGHT OF WAY		EXISTING LIMITED ACCESS RIGHT OF WAY
	GAS LINE LEVEL D		BFOC(D) FIBER OPTICS CABLE LEVEL D (BURIED)
	WATER LINE LEVEL D		OFOC FIBER OPTICS CABLE (AERIAL)
	SANITARY SEWER LEVEL D		OTV CABLE TV LINE (AERIAL)
	CABLE TV LINE LEVEL D (BURIED)		OE ELECTRIC LINES (AERIAL)



**REFER TO SHEET 12 FOR ADDITIONAL UTILITY IMPACTS AT INTERCHANGE  
 ADDITIONAL ANTICIPATED IMPACTS ARE SHOWN ON THIS SHEET**



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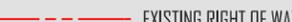


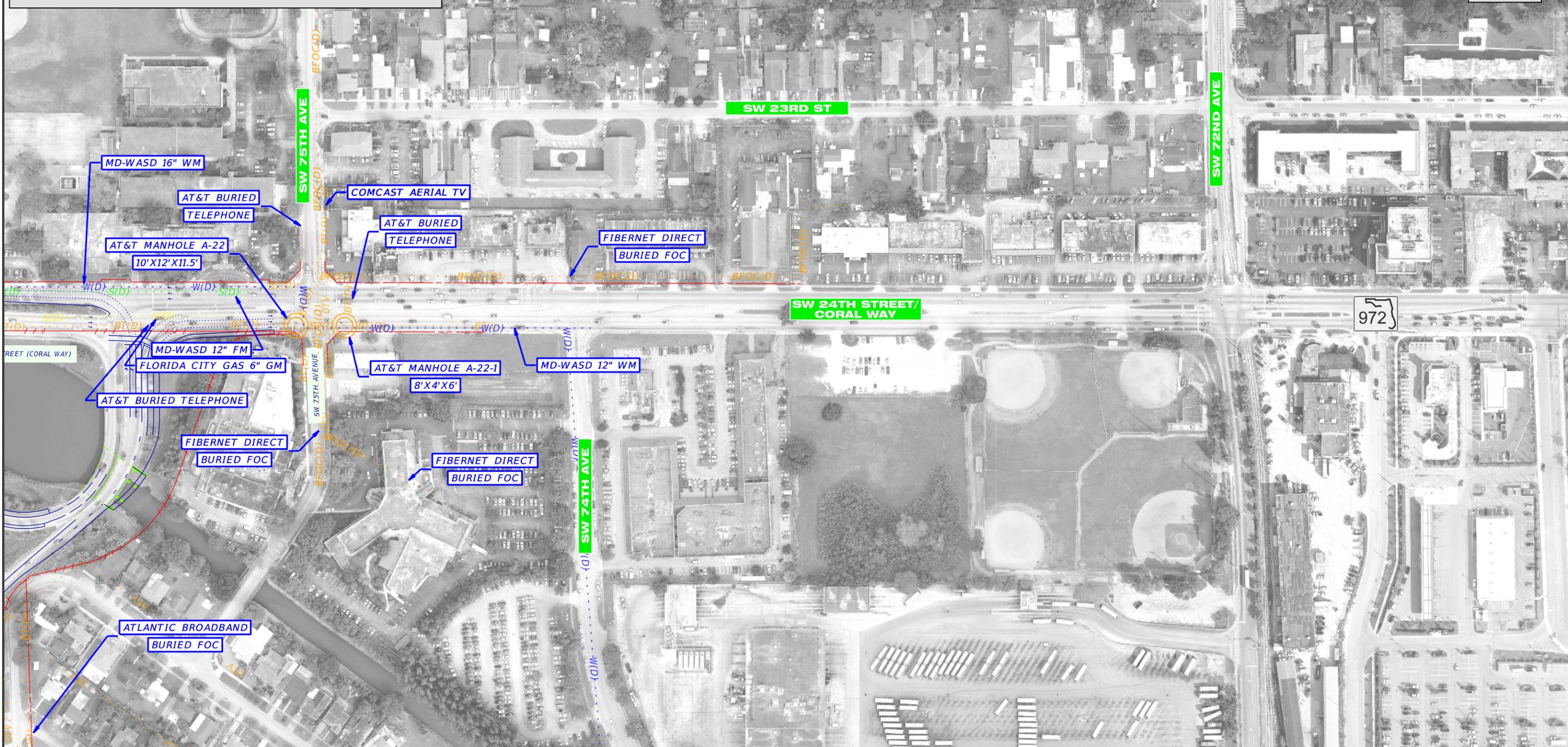
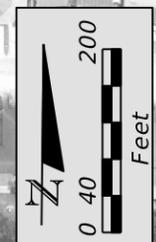
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 ALTERNATIVE 2**

SHEET NO.  
**26**

**LEGEND:**

- |                                                                                   |                                |                                                                                   |                                             |
|-----------------------------------------------------------------------------------|--------------------------------|-----------------------------------------------------------------------------------|---------------------------------------------|
|  | EXISTING RIGHT OF WAY          |  | EXISTING LIMITED ACCESS RIGHT OF WAY        |
|  | GAS LINE LEVEL D               |  | BFOC(D) FIBER OPTICS CABLE LEVEL D (BURIED) |
|  | WATER LINE LEVEL D             |  | OFOC FIBER OPTICS CABLE (AERIAL)            |
|  | SANITARY SEWER LEVEL D         |  | OTV CABLE TV LINE (AERIAL)                  |
|  | CABLE TV LINE LEVEL D (BURIED) |  | OE ELECTRIC LINES (AERIAL)                  |



**REFER TO SHEET 12 FOR UTILITY IMPACTS AT INTERCHANGE  
NO FURTHER ANTICIPATED IMPACTS SHOWN ON THIS SHEET**



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ALTERNATIVE 2

SHEET NO.

27

**LEGEND:**

	EXISTING RIGHT OF WAY		EXISTING LIMITED ACCESS RIGHT OF WAY
	G(D) GAS LINE LEVEL D		BFOC(D) FIBER OPTICS CABLE LEVEL D (BURIED)
	W(D) WATER LINE LEVEL D		OFOC FIBER OPTICS CABLE (AERIAL)
	S(D) SANITARY SEWER LEVEL D		OTV CABLE TV LINE (AERIAL)
	BTVD(D) CABLE TV LINE LEVEL D (BURIED)		OE ELECTRIC LINES (AERIAL)



**POTENTIAL UAO IMPACT**  
 UAO: OVERHEAD ELECTRIC (UNIDENTIFIED UAO)  
 IMPACTED BY: PROPOSED ROADWAY WIDENING

**REFER TO SHEET 14 FOR ADDITIONAL UTILITY IMPACTS AT INTERCHANGE**  
**ADDITIONAL ANTICIPATED IMPACTS ARE SHOWN ON THIS SHEET**



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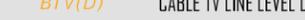


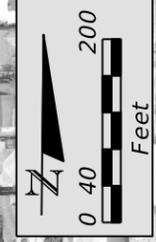
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**ALTERNATIVE 2**

SHEET NO.  
 28

**LEGEND:**

- |                                                                                   |                                |                                                                                   |                                             |
|-----------------------------------------------------------------------------------|--------------------------------|-----------------------------------------------------------------------------------|---------------------------------------------|
|  | EXISTING RIGHT OF WAY          |  | EXISTING LIMITED ACCESS RIGHT OF WAY        |
|  | GAS LINE LEVEL D               |  | BFOC(D) FIBER OPTICS CABLE LEVEL D (BURIED) |
|  | WATER LINE LEVEL D             |  | OFOC FIBER OPTICS CABLE (AERIAL)            |
|  | SANITARY SEWER LEVEL D         |  | OTV CABLE TV LINE (AERIAL)                  |
|  | CABLE TV LINE LEVEL D (BURIED) |  | OE ELECTRIC LINES (AERIAL)                  |



POTENTIAL UAO IMPACT  
UAO: MD-WASD 24" WATER MAIN  
IMPACTED BY: PROPOSED ROADWAY WIDENING

POTENTIAL UAO IMPACT  
UAO: MD-WASD 12" WATER MAIN  
IMPACTED BY: PROPOSED ROADWAY WIDENING

**REFER TO SHEET 14 FOR ADDITIONAL UTILITY IMPACTS AT INTERCHANGE  
ADDITIONAL ANTICIPATED IMPACTS ARE SHOWN ON THIS SHEET**



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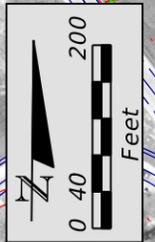


SR 826 PROJECT DEVELOPMENT & ENVIRONMENT STUDY  
From US 1/SR 5/Dixie Highway to SR 836/Dolphin Expressway  
FPID No.: 432639-1-22-02  
ETDM No.: 14308

SR 826/PALMETTO EXPRESSWAY  
UTILITY IMPACT ASSESSMENT  
ALTERNATIVE 2

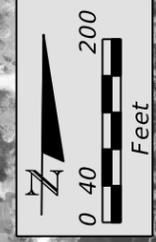
SHEET NO.  
29

**REFER TO SHEET 15 FOR ADDITIONAL UTILITY IMPACTS AT INTERCHANGE  
 ADDITIONAL ANTICIPATED IMPACTS ARE SHOWN ON THIS SHEET**



**LEGEND:**

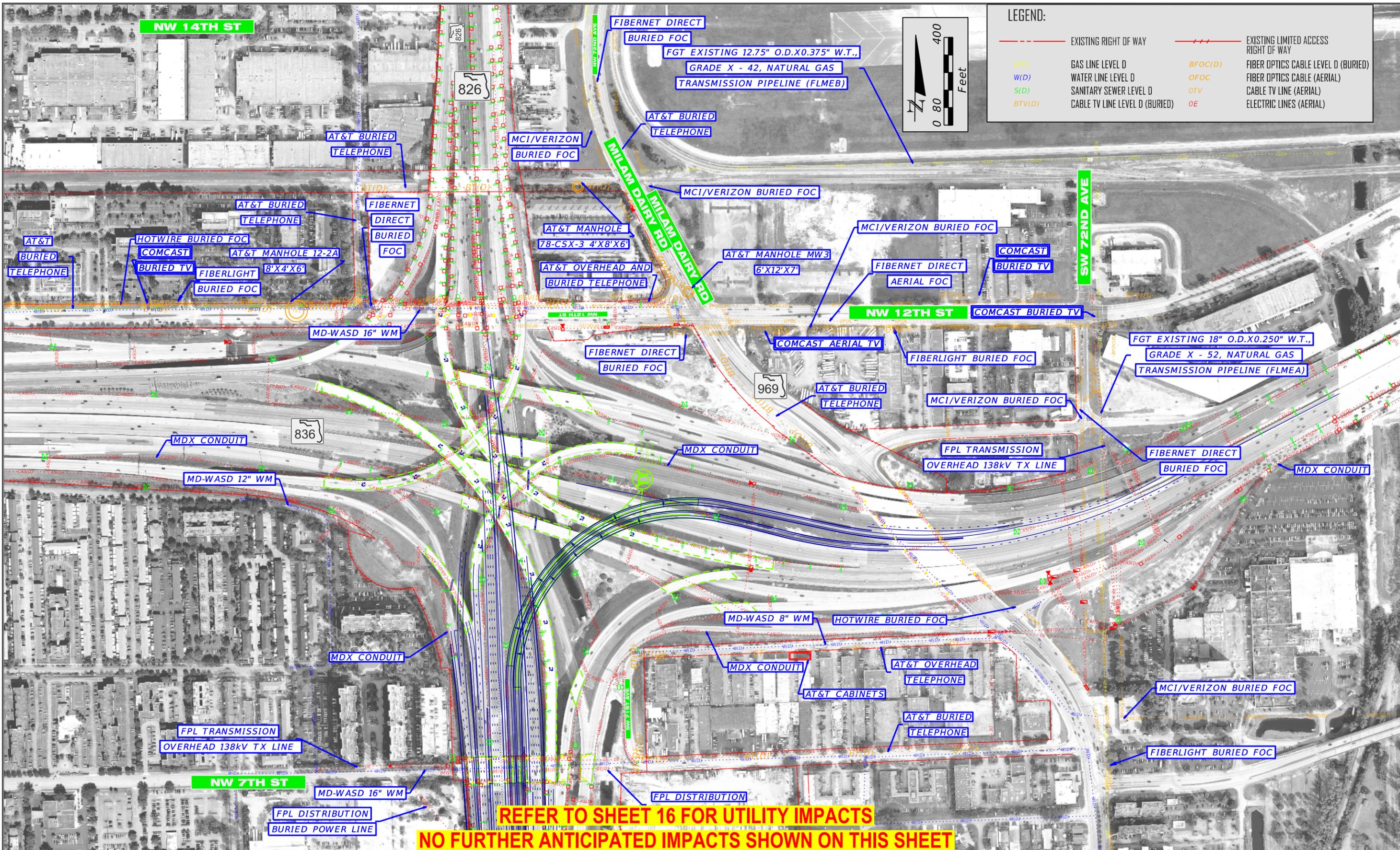
	EXISTING RIGHT OF WAY		EXISTING LIMITED ACCESS RIGHT OF WAY
	GAS LINE LEVEL D		FIBER OPTICS CABLE LEVEL D (BURIED)
	WATER LINE LEVEL D		FIBER OPTICS CABLE (AERIAL)
	SANITARY SEWER LEVEL D		CABLE TV LINE (AERIAL)
	CABLE TV LINE LEVEL D (BURIED)		ELECTRIC LINES (AERIAL)



**REFER TO SHEET 15 FOR ADDITIONAL UTILITY IMPACTS AT INTERCHANGE  
NO UTILITY IMPACTS ANTICIPATED ON THIS SHEET**

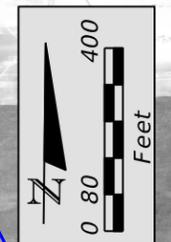
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	W(D) WATER LINE LEVEL D		OFOC FIBER OPTICS CABLE (AERIAL)
	S(D) SANITARY SEWER LEVEL D		OTV CABLE TV LINE (AERIAL)
	BT(V) CABLE TV LINE LEVEL D (BURIED)		OE ELECTRIC LINES (AERIAL)



**LEGEND:**

	EXISTING RIGHT OF WAY		EXISTING LIMITED ACCESS RIGHT OF WAY
	G(D) GAS LINE LEVEL D		BFOC(D) FIBER OPTICS CABLE LEVEL D (BURIED)
	W(D) WATER LINE LEVEL D		OFOC FIBER OPTICS CABLE (AERIAL)
	S(D) SANITARY SEWER LEVEL D		OTV CABLE TV LINE (AERIAL)
	BTVD(D) CABLE TV LINE LEVEL D (BURIED)		OE ELECTRIC LINES (AERIAL)



**REFER TO SHEET 16 FOR UTILITY IMPACTS  
NO FURTHER ANTICIPATED IMPACTS SHOWN ON THIS SHEET**



FLORIDA DEPARTMENT OF TRANSPORTATION  
DISTRICT SIX  
1000 NW 11TH AVENUE  
MIAMI, FL 333172



**SR 826 PROJECT DEVELOPMENT & ENVIRONMENT STUDY**  
From US 1/SR 5/Dixie Highway to SR 836/Dolphin Expressway  
FPID No.: 432639-1-22-02  
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**SR 826/PALMETTO EXPRESSWAY  
UTILITY IMPACT ASSESSMENT  
ALTERNATIVE 2**

SHEET NO.  
**32**

JULY 2019

APPENDIX N  
Transportation Management Plan

## 1.0 TRANSPORTATION MANAGEMENT PLAN (TMP)

A conceptual Transportation Management Plan (TMP) was developed as part of this PD&E study to determine constructability and the ability to maintain traffic for the recommended alternative for mainline maintenance of traffic (MOT). Impacts on traffic, traffic handling options, constructability, and design features/constraints were evaluated for the recommended alternative. Many of the components required to develop a TMP will be developed in accordance with the FDOT standards during the subsequent phases of the project (design & construction). Measures to be considered for implementation of the plan are discussed below.

A TMP includes three components:

- 1) Temporary Traffic Control (TTC) Plan
- 2) Transportation Operations
- 3) Public Information

A Transportation Management Plan (TMP) is required for minimizing activity-related traffic delay and crashes. A TMP consists of strategies to manage the work zone impacts of a project. The goal of a TMP is congestion relief during the construction phase of a project by managing traffic flow and balancing traffic demand with highway capacity through the project area. TMPs are required for significant projects which are defined as:

- 1) A project that, alone or in combination with other concurrent projects nearby, is anticipated to cause sustained work zone impacts.
- 2) All Interstate system projects within the boundaries of a designated Transportation Management Area (TMA) that occupy a location more than three days either intermittent or continuous lane closures.

An effective TMP provides safe construction zones with minimum delay for all traffic modes; including motor vehicles, transit, bicyclists, and pedestrians. The TMP must comply with the requirements contained in the following documents:

- 1) Manual on Uniform Traffic Control Devices for Streets and Highway (MUTCD), Part VI, Temporary Traffic Control
- 2) Policy on Geometric Design of Highway and Streets, AASHTO
- 3) Roadside Design Guide, AASHTO, Chapter 9
- 4) FDOT Standard Plans, Indexes 102-100, 102-110, 102-120, the 102-600 Series, and 711-002
- 5) FDOT Standard Specifications for Road and Bridge Construction (Standard Specifications
- 6) Basis of Estimates Manual
- 7) FDOT accessing Transit Handbook, Chapter 4.6
- 8) AASHTO Guide for the Development of Bicycle Facilities, 4<sup>th</sup> Edition, Chapter 7

As the conceptual TMP was developed, the goal of this PD&E study was to determine a preliminary sequence of construction for the mainline that would minimize traffic delays along the corridor and maintain traffic to the maximum extent possible during project implementation. The existing number of lanes will remain open at all times along the mainline corridor during construction, however, short lane closures will be necessary during off-peak hours to change construction phases. Communication strategies, such as the use of local news media, will be implemented to inform affected road users, the general public, business owners, residents, and appropriate public entities of expected work zone impacts and the changing conditions of the project in the affected area.

## 1.1 ROADWAY TRANSPORTATION MANAGEMENT PLAN

Several roadway typical sections were identified and evaluated throughout the corridor as part of the development of this conceptual TMP. The following locations are discussed for constructability and MOT purposes.

- **US 1/South Dixie Highway**
  - **Typical Section 1** – Between SW 104<sup>th</sup> Street and SW 98<sup>th</sup> Street
  - **Typical Section 2** – Between SW 98<sup>th</sup> Street and SR 94/SW 88<sup>th</sup> Street
- **SR 826/Palmetto Expressway**
  - **Typical Section 3** – Between Snapper Creek Expressway and SR 986/SW 72<sup>nd</sup> Street (Sunset Drive)
  - **Typical Section 4** – Between SR 986/SW 72<sup>nd</sup> Street (Sunset Drive) and SW 56<sup>th</sup> Street (Miller Drive)
  - **Typical Section 5** – At SR 976/SW 40<sup>th</sup> Street (Bird Road)
  - **Typical Section 6** – Between SR 976/SW 40<sup>th</sup> Street (Bird Road) and SW 24<sup>th</sup> Street (Coral Way)
  - **Typical Section 7** – Between SW 24<sup>th</sup> Street (Coral Way) and US 41/SW 8<sup>th</sup> Street (Tamiami Trail)
  - **Typical Section 8** – Between US 41/SW 8<sup>th</sup> Street (Tamiami Trail) and SR 968/West Flagler Street
  - **Typical Section 9** – Between SR 968/W Flagler Street and SR 836/Dolphin Expressway

The following discusses the conceptual MOT phases at the identified locations mentioned previously. to show the typical sections during construction.

## **Typical Section 1 – Between SW 104<sup>th</sup> Street and SW 98<sup>th</sup> Street (Figure 1.1)**

### Phase 1 – Widen US 1 Southbound (Figure 1.2)

- Reduce travel lane widths on the southbound approach to 11 feet (except for the outside lane which is reduced to 10 feet).
- Remove outside curb and gutter.
- Provide a 2-foot shoulder on the outside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110).
- Provide 1-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Construct the proposed widening section.
- Construct new curb and gutter.
- Re-grade outside ground.

### Phase 2 – Median Construction Zone (Figure 1.3)

- Shift lanes to newly widened roadway.
- Reduce all southbound lanes to 10-feet.
- Provide a 2-foot shoulder on the inside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110).
- Provide 1-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Construct proposed widening and median modification.

### Phase 3 – Shift travel lanes to final location (Figure 1.4)

- Overbuild the pavement and make cross slope/crown point adjustments as necessary.
- Resurface the pavement corridor to final grade.

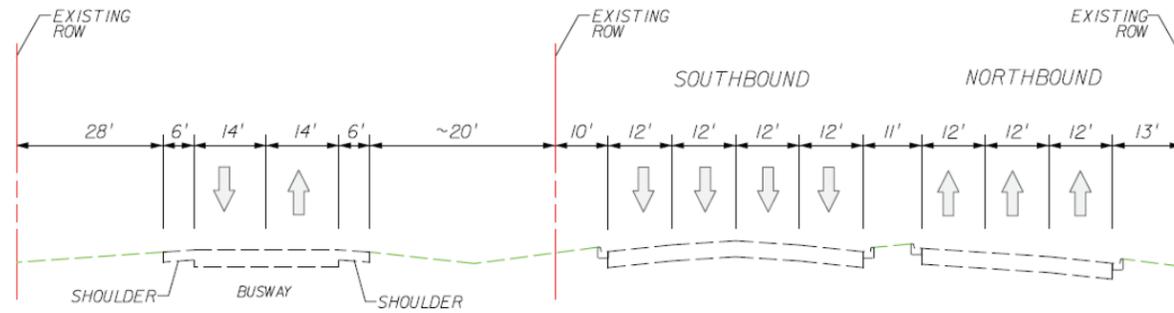


Figure 1.1 Existing Roadway Typical Section 1

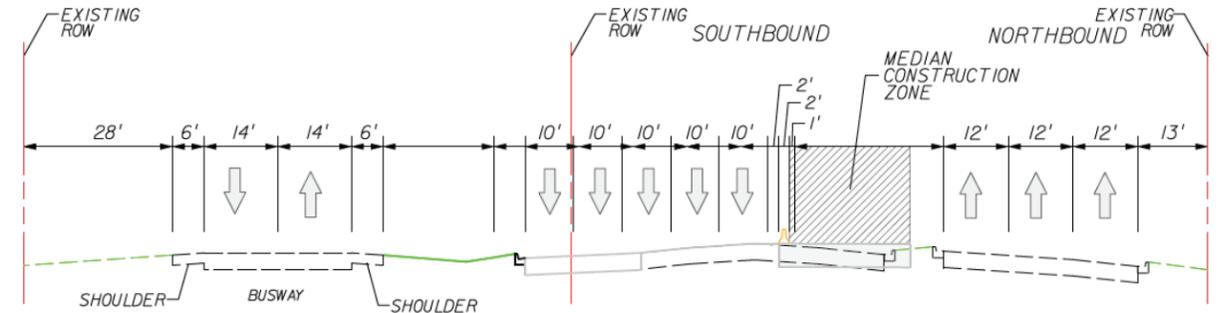


Figure 1.3 Typical Section 1 - Phase 2

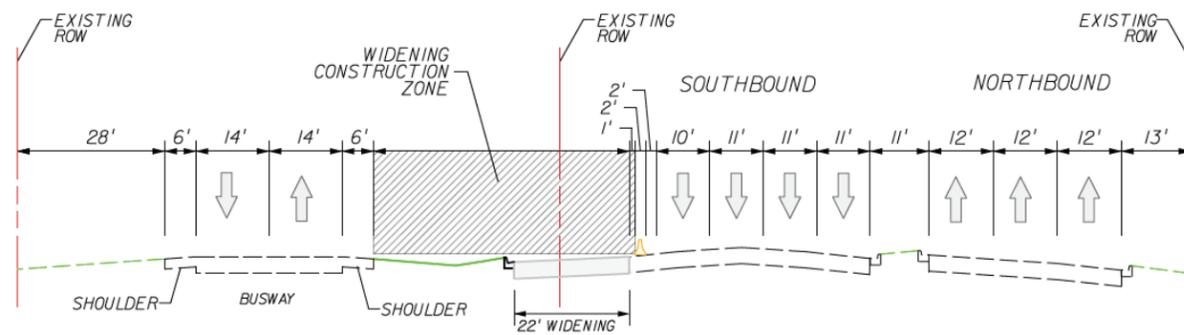


Figure 1.2 Typical Section 1 - Phase 1

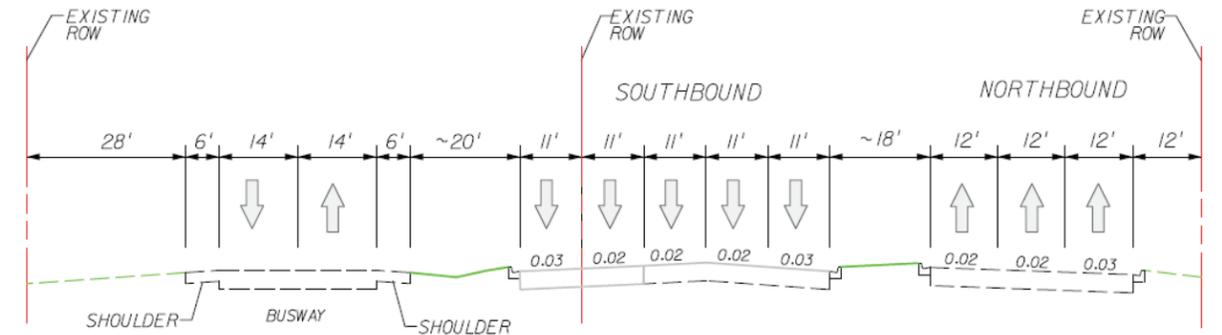


Figure 1.4 Typical Section 1 - Phase 3

**Typical Section 2 – Between SW 98<sup>th</sup> Street and SR 94/SW 88<sup>th</sup> Street (Figure 1.5)**

**Phase 1 – Widen SR 826 Southbound (Figure 1.6)**

- Reduce travel lane widths on the southbound approach to 11 feet.
- Remove outside shoulder and guardrail.
- Provide a 2-foot shoulder on the outside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110).
- Provide 1-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Construct the proposed widening section and shoulder.
- Construct new guardrail.
- Re-grade outside ground.

**Phase 2 – Shift lanes to outside of southbound approach and construct MSE wall/Proposed Busway (Figure 1.7)**

- Shift lanes to newly widened roadway.
- Reduce travel lane widths on southbound approach to 11 feet.
- Reduce outside shoulder width to 10-feet.
- Provide a 2-foot shoulder on the inside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110).
- Provide 1-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Provide temporary retaining system for MSE wall construction and embankment excavation for proposed busway.
- Construct MSE wall.
- Remove temporary retaining wall system.
- Construct remaining pavement for proposed busway.

**Phase 3 – Shift travel lanes to final location (Figure 1.8)**

- Overbuild the pavement and make cross slope/crown point adjustments as necessary.
- Resurface the pavement corridor to final grade.

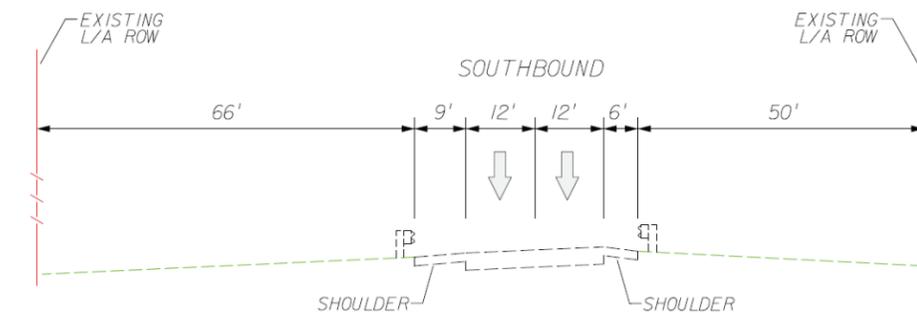


Figure 1.5 Existing Roadway Typical Section 2

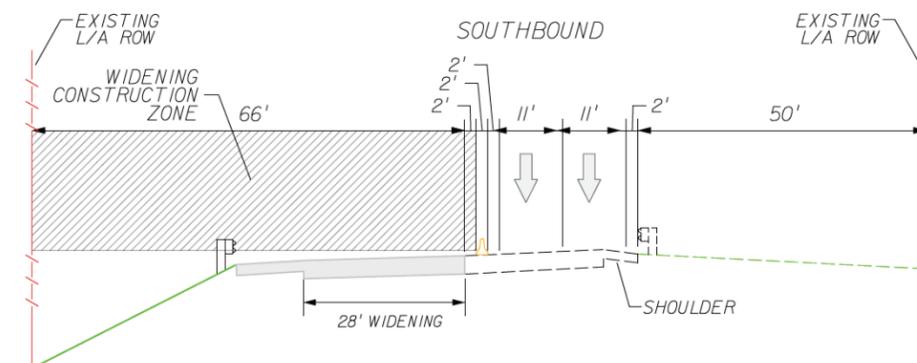


Figure 1.6 Typical Section 2 - Phase 1

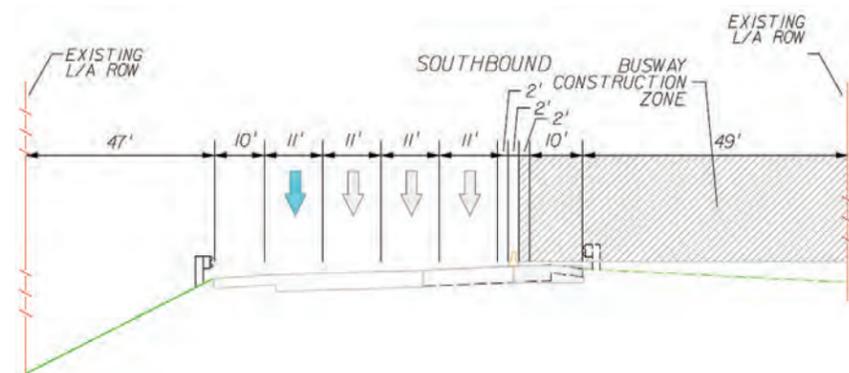


Figure 1.7 Typical Section 2 - Phase 2

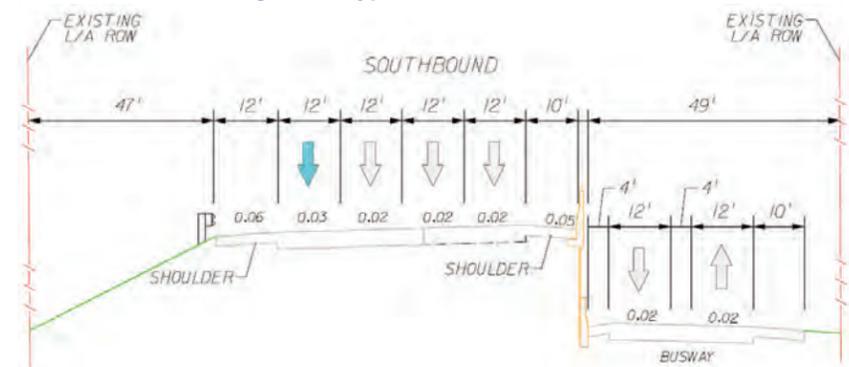


Figure 1.8 Typical Section 2 - Phase 3

**Typical Section 3 – Between Snapper Creek Expressway and SR 986/SW 72<sup>nd</sup> Street (Sunset Drive) (Figure 1.9)**

**Phase 1 – Widen Outside of SR 826 on Southbound & Northbound Approach (Figure 1.10)**

- Reduce travel lane widths to 11 feet on the Southbound and Northbound approach.
- Reduce inside shoulders to 8 feet.
- Provide a 2-foot shoulder on the outside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110).
- Provide 1-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Construct the proposed widening section, shoulder, and roadside barriers.

**Phase 2 – Shift lanes to outside and perform median reconstruction for express lanes (Figure 1.11)**

- Shift all lanes on both approaches to newly widened roadway.
- Reduce travel lane widths to 11 feet on the southbound and northbound approach.
- Reduce outside shoulder to 8 feet on southbound approach and 9 feet on northbound approach.
- Provide a 2-foot shoulder on the inside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110).
- Provide 1-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Construct proposed widening section, shoulder, and median barrier wall along the median.

**Phase 3 – Shift travel lanes to final location (Figure 1.12)**

- Stripe buffered separation and express lanes in the median.
- Install express lane markers.
- Overbuild the pavement and make cross slope/crown point adjustments as necessary.
- Resurface the pavement corridor to final grade.

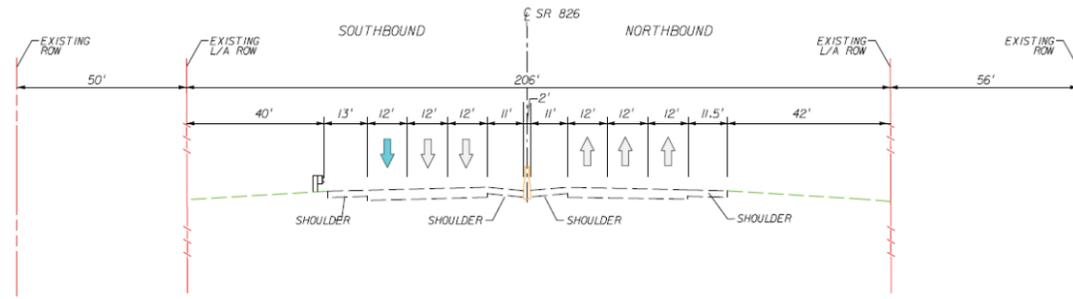


Figure 1.9 Existing Roadway Typical Section 3

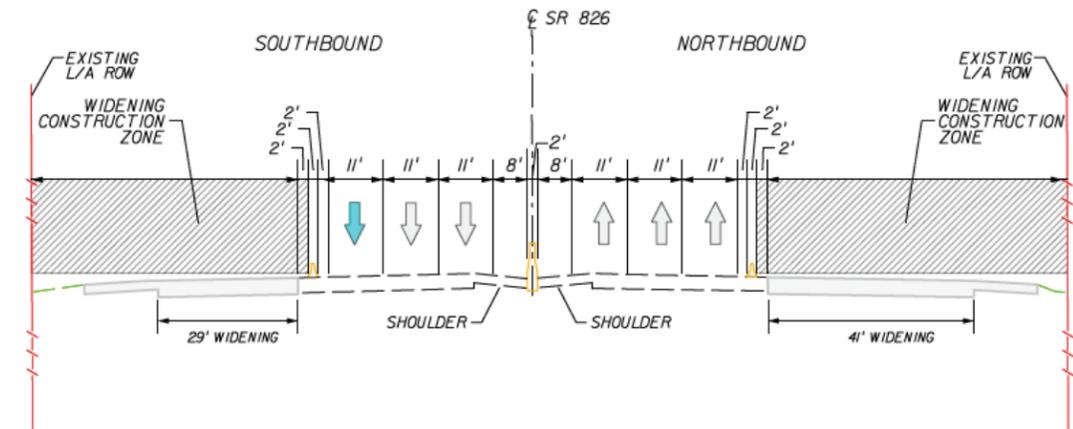


Figure 1.10 Typical Section 3 - Phase 1

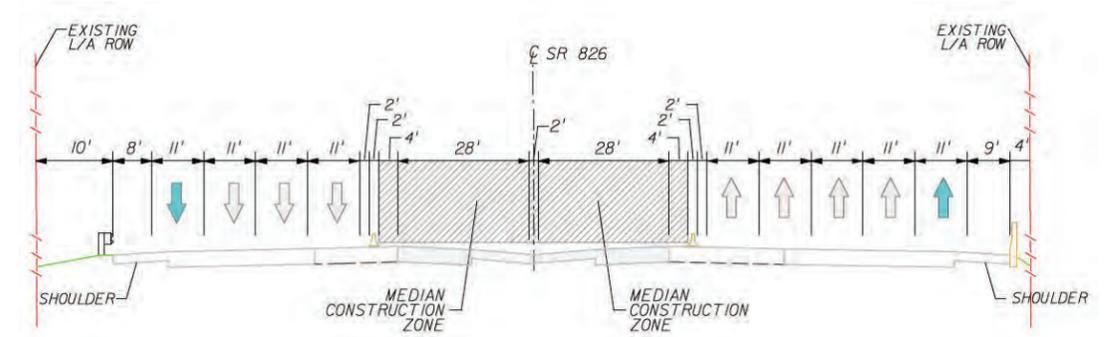


Figure 1.11 Typical Section 3 - Phase 2

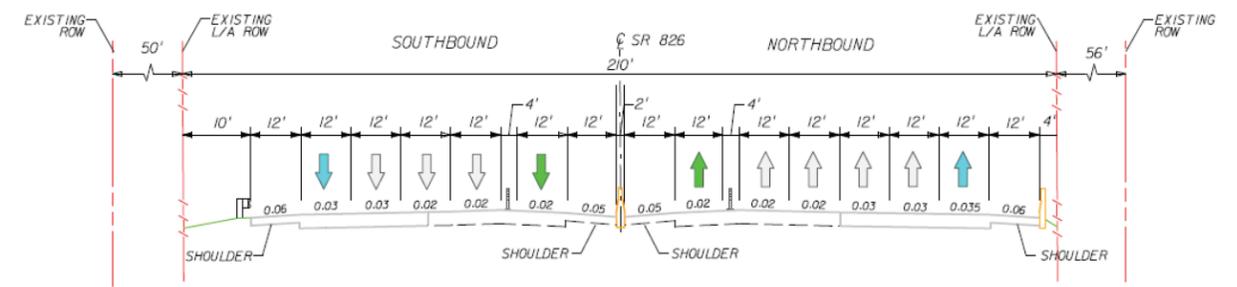


Figure 1.12 Typical Section 3 - Phase 3

**Typical Section 4 – Between SR 986/SW 72<sup>nd</sup> Street (Sunset Drive) and SW 56<sup>th</sup> Street (Miller Drive) (Figure 1.13)**

**Phase 1 – Widen Outside of SR 826 on Southbound & Northbound Approach (Figure 1.14)**

- Reduce travel lane widths to 11 feet on the Southbound and Northbound approach.
- Reduce inside shoulder on southbound approach to 6.5 feet and on northbound approach to 7 feet.
- Provide a 2-foot shoulder on the outside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110).
- Provide 1-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Construct the proposed widening section, shoulder, and roadside barriers.

**Phase 2 – Shift lanes to outside and perform median reconstruction for express lanes (Figure 1.15)**

- Shift all lanes on both approaches to newly widened roadway.
- Provide travel lane widths of 12 feet on the southbound and northbound approach.
- Provide 12-foot outside shoulders on both approaches.
- Provide a 6-foot shoulder on the inside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Free-Standing (Standard Plans Index 102-110).
- Provide 4-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Construct proposed widening section, shoulder, and median barrier wall along the median.

**Phase 3 – Shift travel lanes to final location (Figure 1.16)**

- Stripe buffered separation and express lanes in the median.
- Install express lane markers.
- Overbuild the pavement and make cross slope/crown point adjustments as necessary.

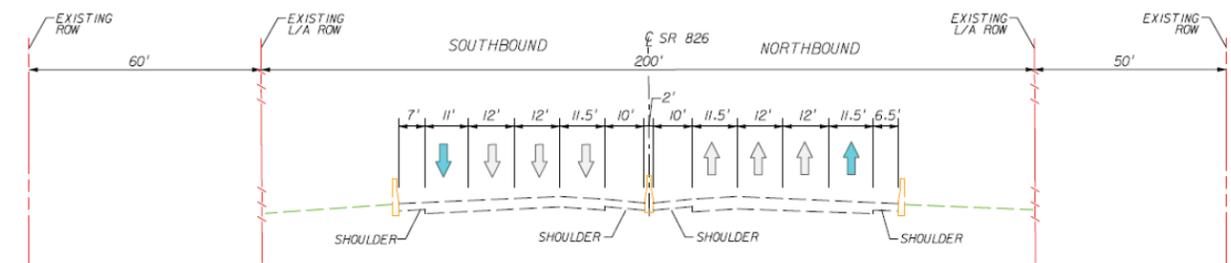


Figure 1.13 Existing Roadway Typical Section 4

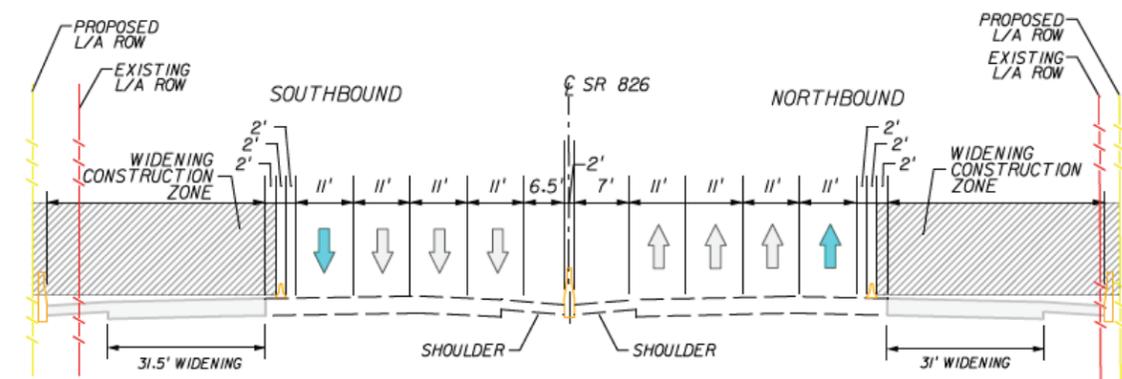


Figure 1.14 Typical Section 4 - Phase 1

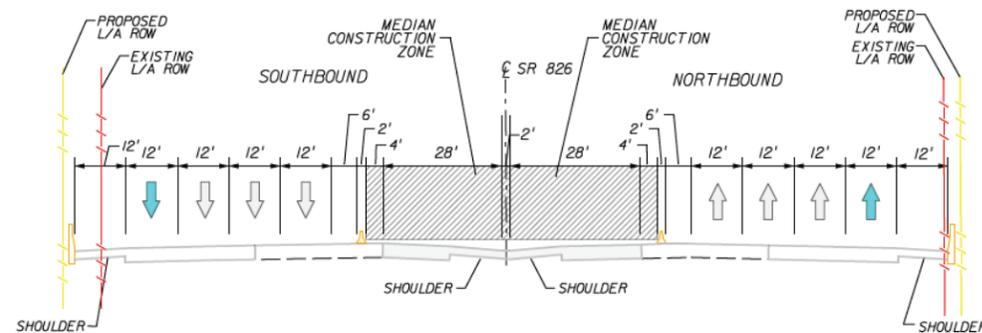


Figure 1.15 Typical Section 4 - Phase 2

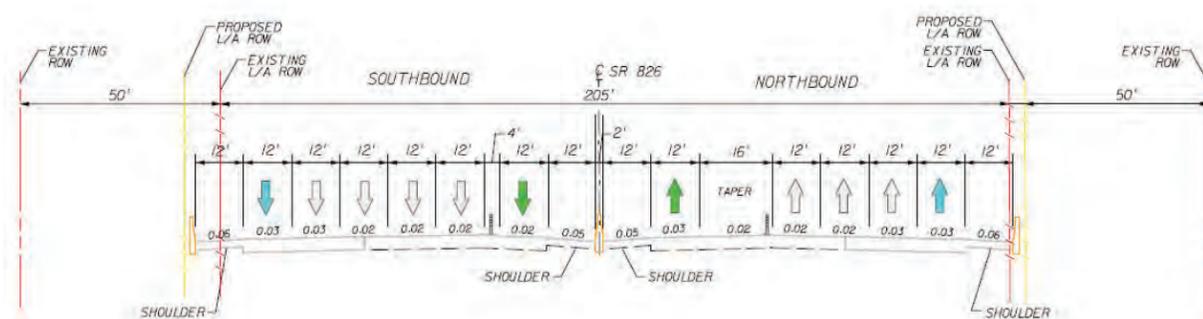


Figure 1.16 Typical Section 4 - Phase 3

**Typical Section 5 – At SR 976/SW 40<sup>th</sup> Street (Bird Road) (Figure 1.17)**

**Phase 1 – Widen Outside of SR 826 on Southbound & Northbound Approach (Figure 1.18)**

- Reduce travel lane widths to 11 feet on the Southbound and Northbound approach except for inside lanes.
- Reduce inside travel lane width to 12.5 feet, 13.5 feet on southbound and northbound approaches respectively.
- Provide a 2-foot shoulder on the outside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110).
- Provide 1-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Provide temporary retaining system on outside of corridor for MSE wall construction.
- Construct MSE wall.
- Remove temporary retaining wall system.
- Construct the proposed widening section, shoulder, and roadside barriers.

**Phase 2 – Shift lanes to outside and perform median reconstruction for express lanes (Figure 1.19)**

- Shift all lanes on both approaches to newly widened roadway.
- Reduce travel lane widths of 11 feet on the southbound and northbound approach.
- Provide 2-foot outside shoulders on both approaches.
- Provide a 2-foot shoulder on the inside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110).
- Provide 2-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Construct bridge substructure foundations and erect columns for elevated express lanes.
- Construct bridge superstructure for elevated express lanes.
- Construct proposed widening section, shoulder, and median barrier wall along the median.

**Phase 3 – Shift travel lanes to final location (Figure 1.20)**

- Stripe buffered separation and express lanes in the median at-grade.
- Install express lane markers at-grade.
- Overbuild the pavement and make cross slope/crown point adjustments as necessary at-grade.
- Stripe express lanes on elevated bridge structure.

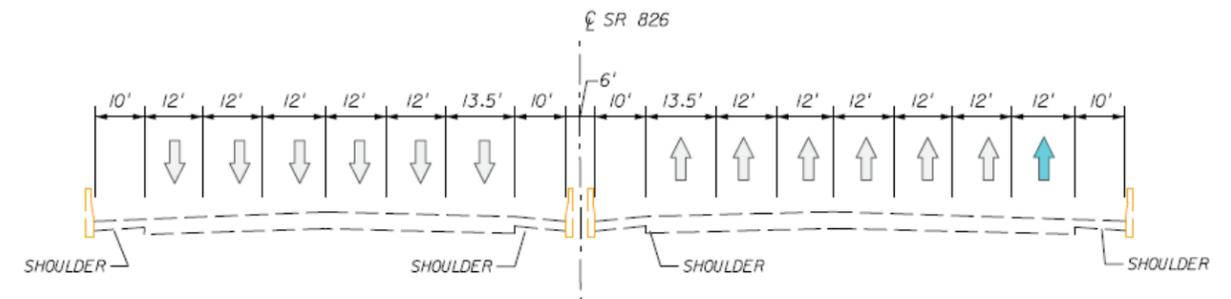


Figure 1.17 Existing Roadway Typical Section 5

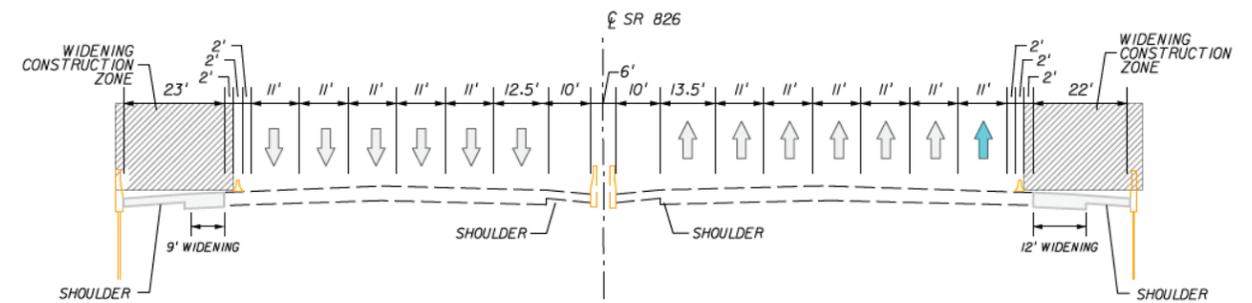


Figure 1.18 Typical Section 5 - Phase 1

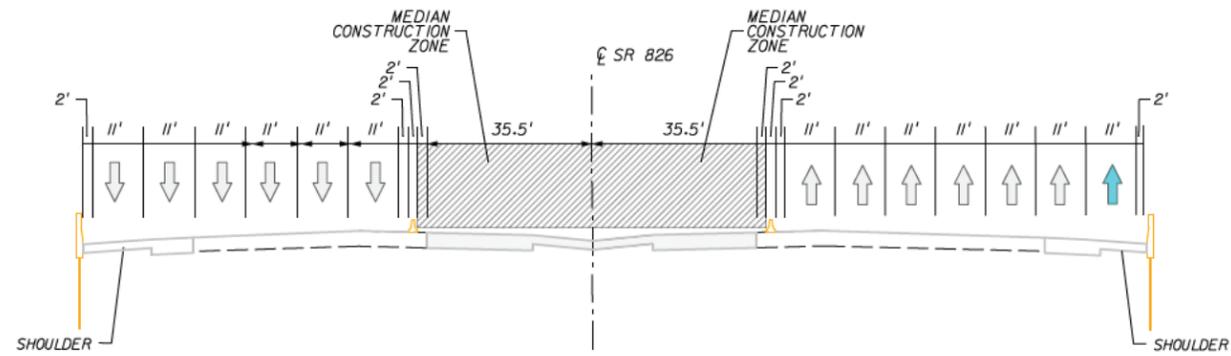


Figure 1.19 Typical Section 5 - Phase 2

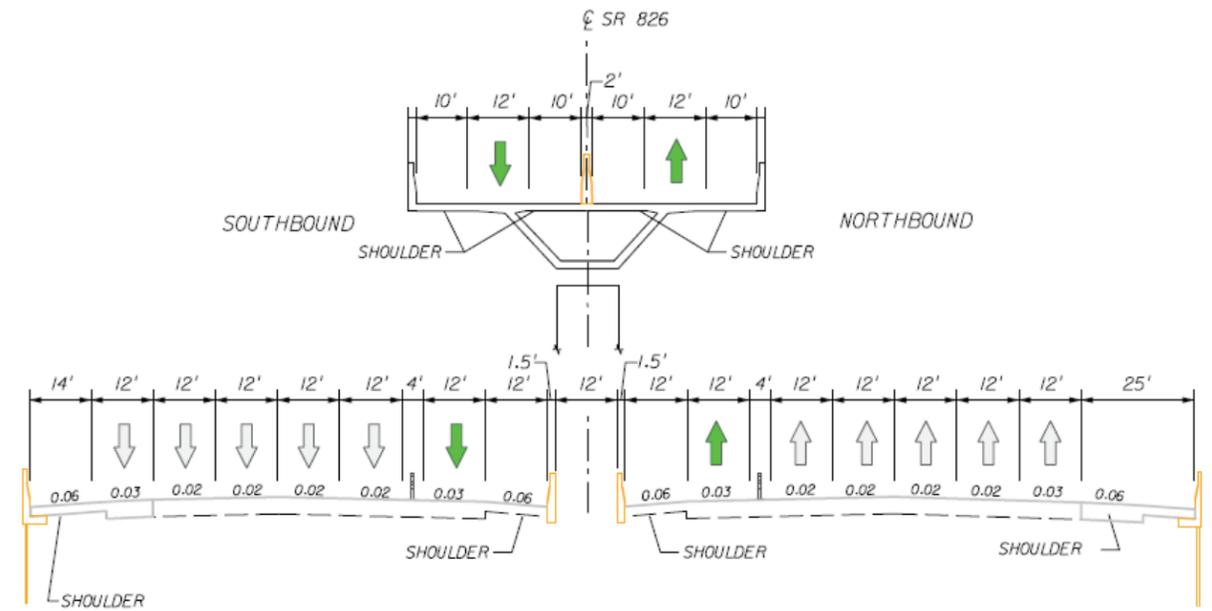


Figure 1.20 Typical Section 5 - Phase 3

**Typical Section 6 – Between SR 976/SW 40<sup>th</sup> Street (Bird Road) and SW 24<sup>th</sup> Street (Coral Way) (Figure 1.21)**

**Phase 1 – Widen Outside of SR 826 on Southbound Approach (Figure 1.22)**

- Reduce travel lane widths to 11 feet on the southbound approach.
- Reduce inside shoulder to 7.5 feet on the southbound approach.
- Provide a 2-foot shoulder on the outside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110).
- Provide 1-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Construct the proposed widening section, shoulder, and roadside barriers.

**Phase 2 – Shift lanes to outside and perform median reconstruction for express lanes (Figure 1.23)**

- Shift southbound lanes to newly widened roadway.
- Provide travel lane widths of 11 feet on the southbound and northbound approach.
- Provide a 6-foot outside shoulder on the southbound approach.
- Provide a 5.5-foot shoulder on the inside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Free-Standing (Standard Plans Index 102-110).
- Provide 4-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Provide temporary retaining system on the median of corridor for MSE wall construction.
- Construct MSE wall.
- Remove temporary retaining wall system.
- Construct proposed widening section, shoulder, and median barrier on the MSE wall & at-grade sections.

**Phase 3 – Shift travel lanes to final location (Figure 1.24)**

- Stripe buffered separation and express lanes in the median.
- Install express lane markers.
- Overbuild the pavement and make cross slope/crown point adjustments as necessary.

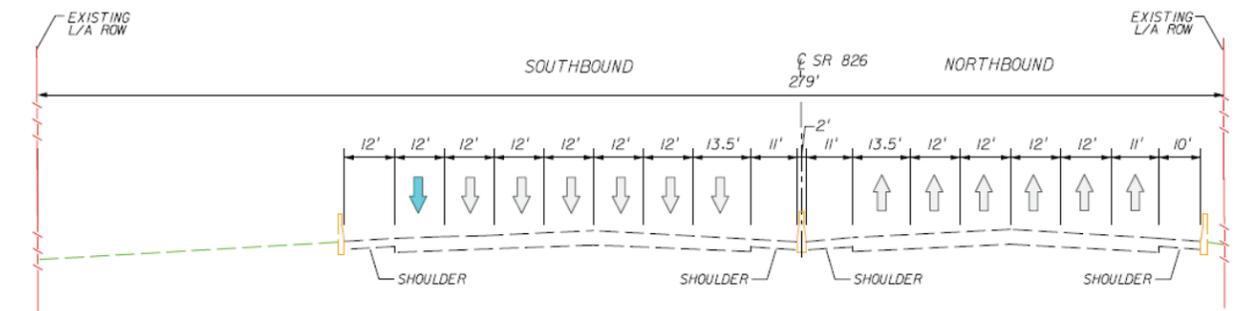


Figure 1.21 Existing Roadway Typical Section 6

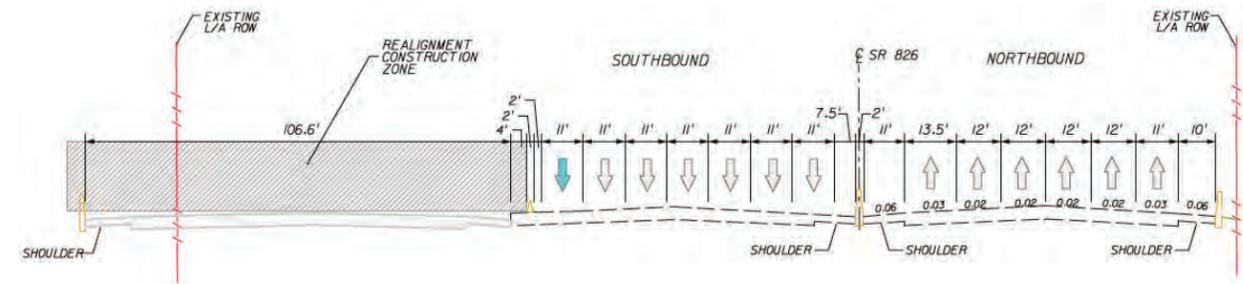


Figure 1.22 Typical Section 6 - Phase 1

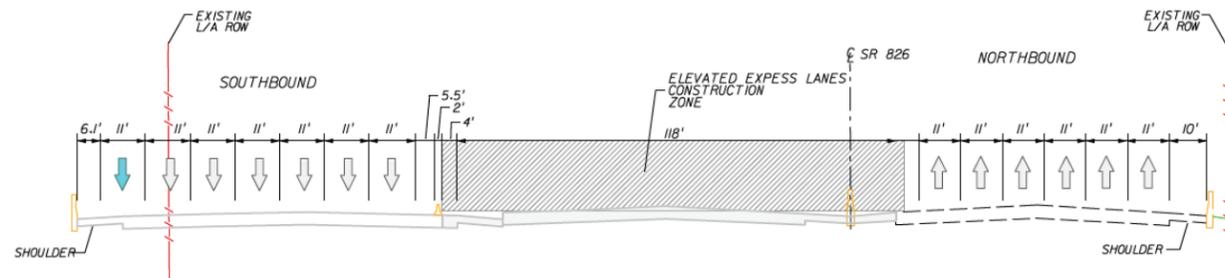


Figure 1.23 Typical Section 6 - Phase 2

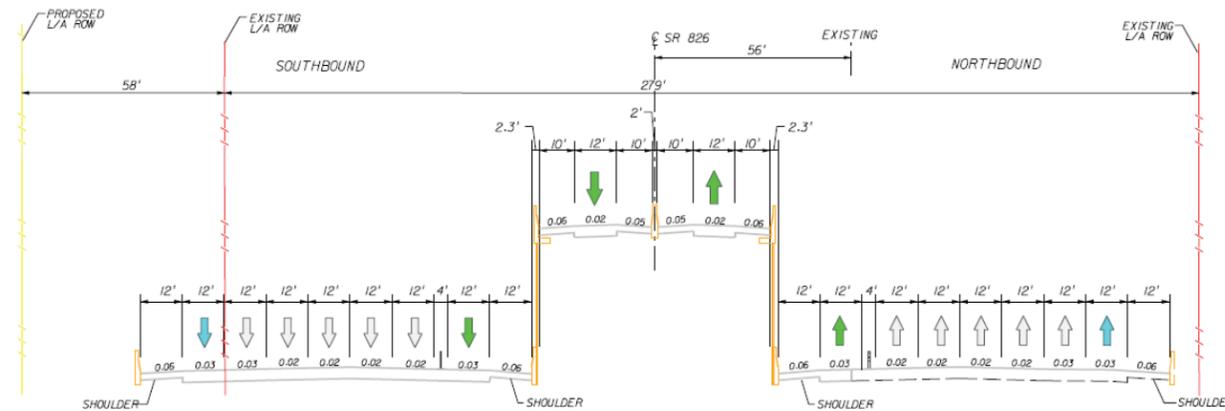


Figure 1.24 Typical Section 6 - Phase 3

**Typical Section 7 – Between SW 24<sup>th</sup> Street (Coral Way) and US 41/SW 8<sup>th</sup> Street (Tamiami Trail) (Figure 1.25)**

**Phase 1 – Widen Outside of SR 826 on Southbound & Northbound Approach (Figure 1.26)**

- Reduce travel lane widths to 11 feet on the southbound and northbound approach.
- Reduce inside shoulder on southbound approach to 10 feet and on northbound approach to 10.5 feet.
- Provide a 2-foot shoulder on the outside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110).
- Provide 1-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Provide temporary retaining system on the outside of the northbound approach adjacent to existing canal for MSE wall construction.
- Construct MSE wall.
- Remove temporary retaining wall system.
- Construct the proposed widening section, shoulder, and roadside barriers

**Phase 2 – Shift lanes to outside and perform median reconstruction for express lanes (Figure 1.27)**

- Shift all lanes on both approaches to newly widened roadway.
- Provide travel lane widths of 11 feet on the southbound and northbound approach.
- Provide 8-foot outside shoulder on southbound approach and 12-foot shoulder on northbound approach.
- Provide a 2-foot shoulder on the inside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110).
- Provide 4-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Construct proposed widening section, shoulder, and median barrier wall along the median.

**Phase 3 – Shift travel lanes to final location (Figure 1.28)**

- Stripe buffered separation and express lanes in the median.
- Install express lane markers.

- Overbuild the pavement and make cross slope/crown point adjustments as necessary.

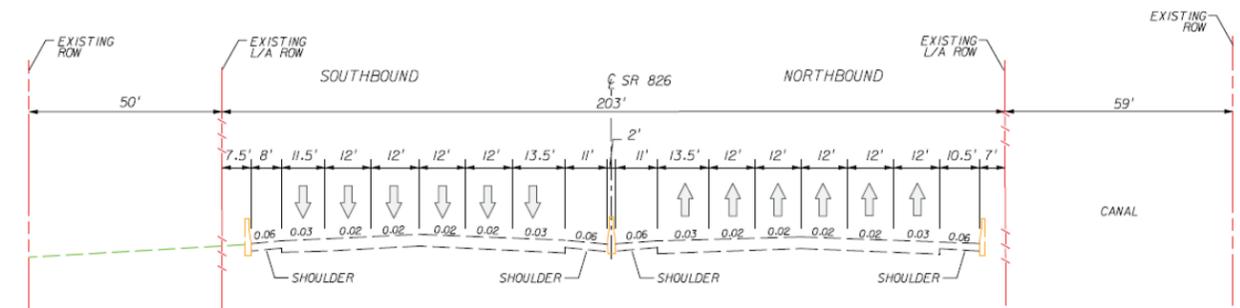


Figure 1.25 Existing Roadway Typical Section 7

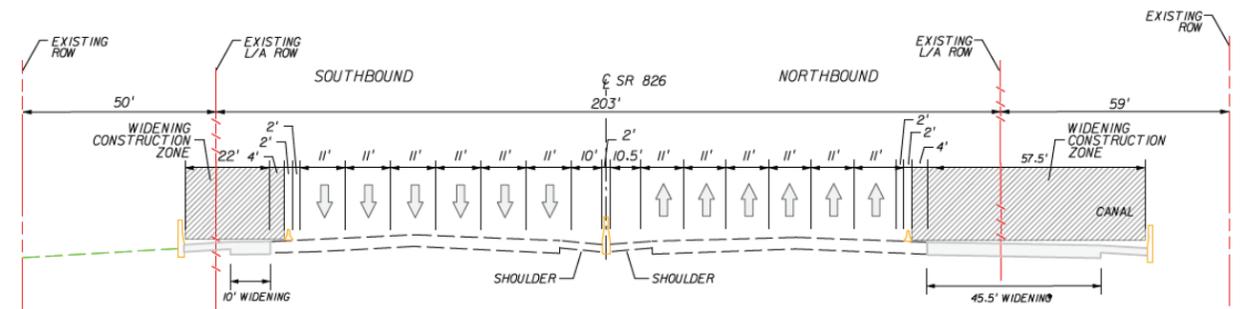


Figure 1.26 Typical Section 7 - Phase 1

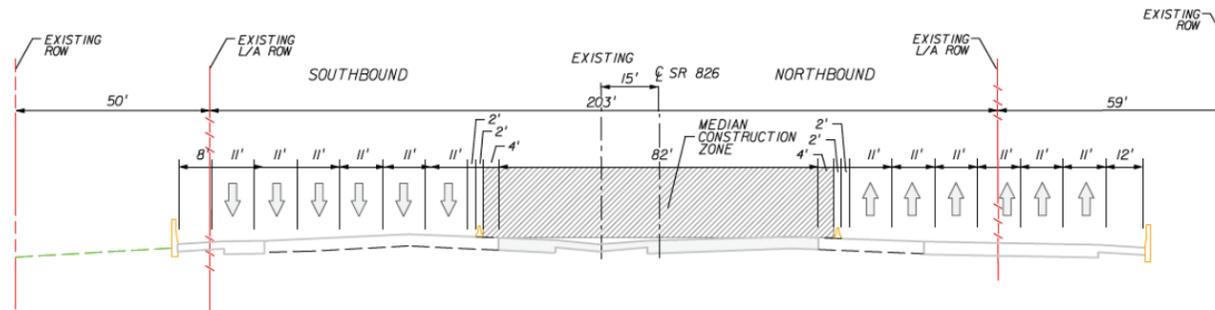


Figure 1.27 Typical Section 7 - Phase 2

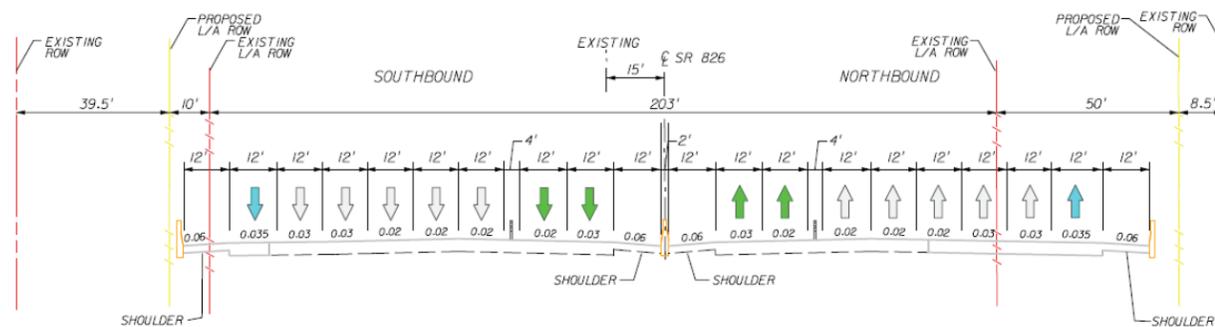


Figure 1.28 Typical Section 7 - Phase 3

**Typical Section 8 – Between US 41/SW 8<sup>th</sup> Street (Tamiami Trail) and SR 968/West Flagler Street (Figure 1.29)**

**Phase 1 – Widen Outside of SR 826 on Southbound & Northbound Approach (Figure 1.30)**

- Reduce travel lane widths to 11 feet on the southbound approach except for the inside lane, auxiliary lane, and lane adjacent to existing gore.
- Reduce travel lane widths to 11 feet on the northbound approach.
- Reduce inside shoulder on the northbound approach to 13 feet.
- Provide a 2-foot shoulder on the outside traveled way.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110).
- Provide 1-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Provide temporary retaining system on the outside of the northbound approach for MSE wall construction.
- Construct MSE wall.
- Remove temporary retaining wall system.
- Construct the proposed widening section, shoulder, and roadside barriers.

**Phase 2 – Shift lanes to outside and perform median reconstruction for express lanes (Figure 1.31)**

- Shift all lanes on both approaches to newly widened roadway.
- Provide travel lane widths of 12 feet on the southbound and 11 feet on the northbound approach.
- Provide 15-foot outside shoulder on southbound approach and 2-foot shoulder on northbound approach.
- Provide a 10-foot shoulder on the inside traveled way of the southbound approach and a 3-foot shoulder on the northbound approach.
- Place Type K Temporary Concrete Barrier System installed as Free Standing (Standard Plans Index 102-110).
- Provide 4-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Construct proposed widening section, adjacent local road, shoulder, and median barrier wall along the median.





**Typical Section 9 – Between SR 968/W Flagler Street and SR 836/Dolphin Expressway (Figure 1.34)**

**Phase 1 – Southbound/Northbound Shoulder & Barrier Construction (Figure 1.35)**

- Reduce travel lanes to 11 feet on the southbound and northbound approaches.
- Provide a 2-foot shoulder on the outside mainline traveled way and on the inside of the adjacent CD systems.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110) on the outside shoulder of the mainline traveled way and inside shoulder of the adjacent CD systems.
- Provide 1-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Construct the proposed widening section.

**Phase 2 – Outside/Median Widening (Figure 1.36)**

- Redistribute/shift travel lanes.
- Utilize existing pavement in the median for construction of express lanes.
- Provide a 2-foot shoulder on the mainline traveled way.
- Place Type K Temporary Concrete Barrier System installed as Anchored (Standard Plans Index 102-110) the mainline traveled way.
- Provide 1-foot pavement/asphalt width behind the anchored Type K Temporary Concrete Barrier System.
- Construct the proposed widening section.

**Phase 3 – Shift travel lanes to final location (Figure 1.37)**

- Stripe buffered separation and express lanes in the median.
- Install express lane markers.
- Overbuild the pavement and make cross slope/crown point adjustments as necessary.

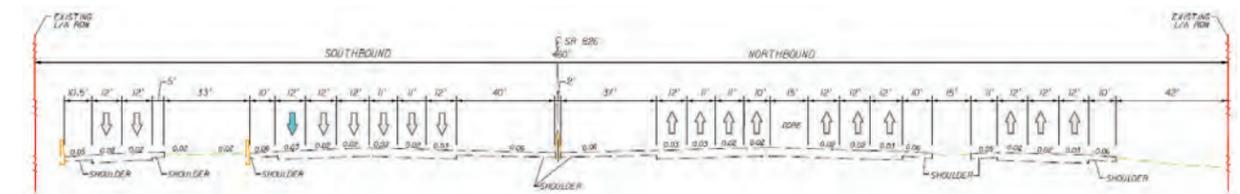


Figure 1.34 Existing Roadway Typical Section 9

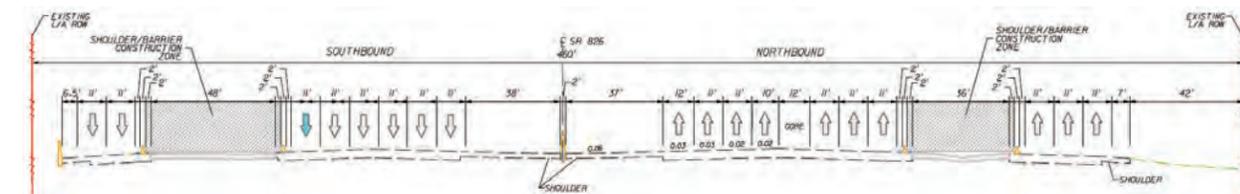


Figure 1.35 Typical Section 9 - Phase 1

## 1.2 STRUCTURES TRANSPORTATION MANAGEMENT PLAN

The conceptual transportation management plan for the implementation of the recommended alternative structures improvements proposes to keep the same number of travel lanes open to traffic during construction. Certain locations along the corridor require unique considerations to achieve a seamless construction of the bridge structures.

Mainline bridge widenings over Kendall Drive (SW 88<sup>th</sup> St.), Snapper Creek Canal, Miller Drive (SW 56<sup>th</sup> Street), Bird Road (SW 40<sup>th</sup> Street), Coral Way (SW 24<sup>th</sup> Street), Tamiami Trail (SW 8<sup>th</sup> Street), Flagler Street and NW 7<sup>th</sup> Street will require three Maintenance of Traffic phases. Refer to [Figure 1.38](#) to [Figure 1.47](#).

### Phase I – Shift traffic to the inside

- Reduce the inside shoulder width.
- Reduce the travel lanes to 11' (Except for the center lane).
- Install K-Type barriers along the cutline on the outside of the bridge.
- Construct outside bridge widening (widenings of crossings at Kendal drive, Snapper Creek Canal, Tamiami Trail and NW 7<sup>th</sup> Street require prior relocation and reconstruction of adjacent ramp bridges to allow space for the proposed widening).

### Phase II – Reconstruct existing Median Barrier (at Flagler St and Coral Way)

- Relocate temporary traffic barriers.
- Shift traffic to the outside.
- Reconstruct median barrier, where required.

### Phase III – Shift traffic to final condition

- Remove temporary barriers.
- Stripe lanes for final condition.
- Open bridge to traffic.

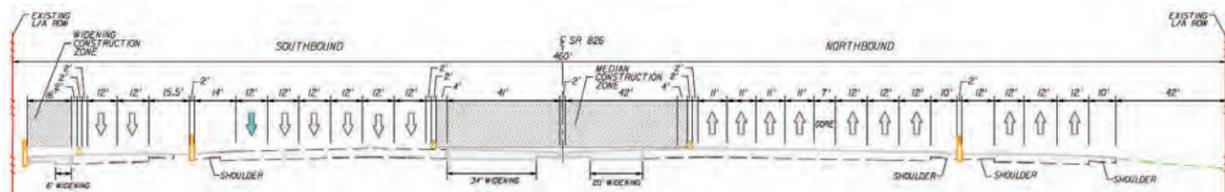


Figure 1.36 Typical Section 9 - Phase 2

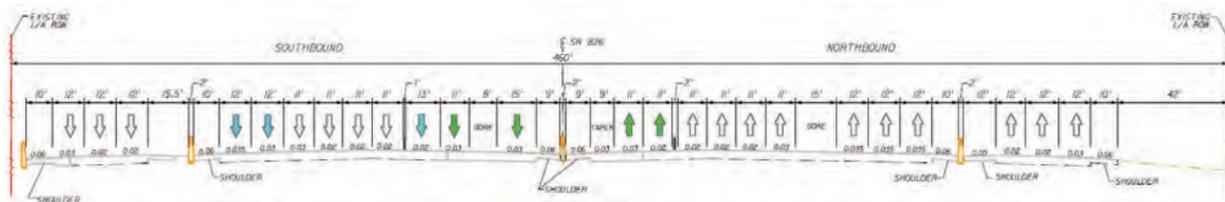


Figure 1.37 Typical Section 9 - Phase 3

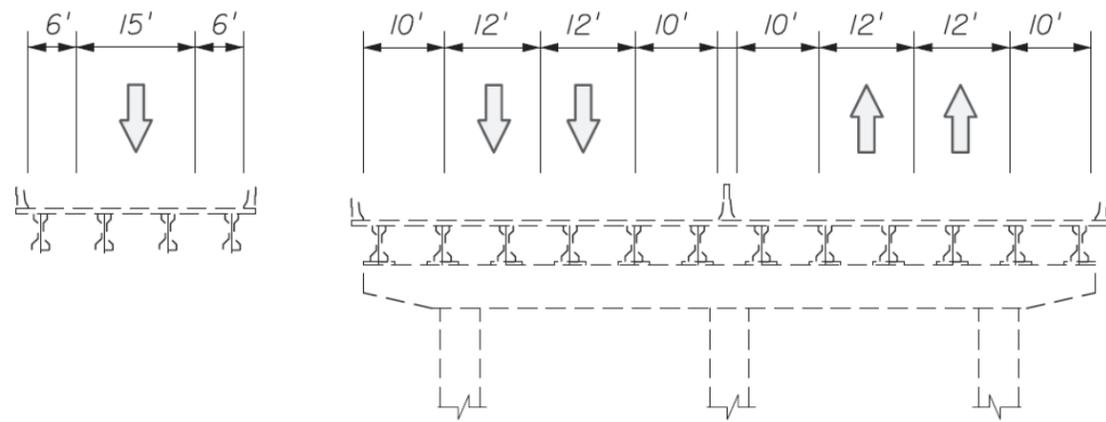


Figure 1.38 Existing Bridge Over SW 88<sup>th</sup> Street/Kendall Drive

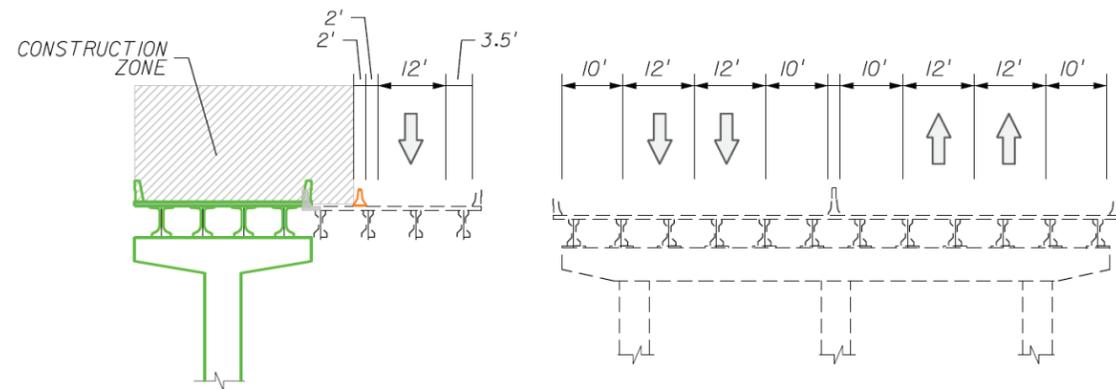


Figure 1.39 Bridge Over SW 88<sup>th</sup> Street/Kendall Drive- Partial Removal/New Bridge Construction

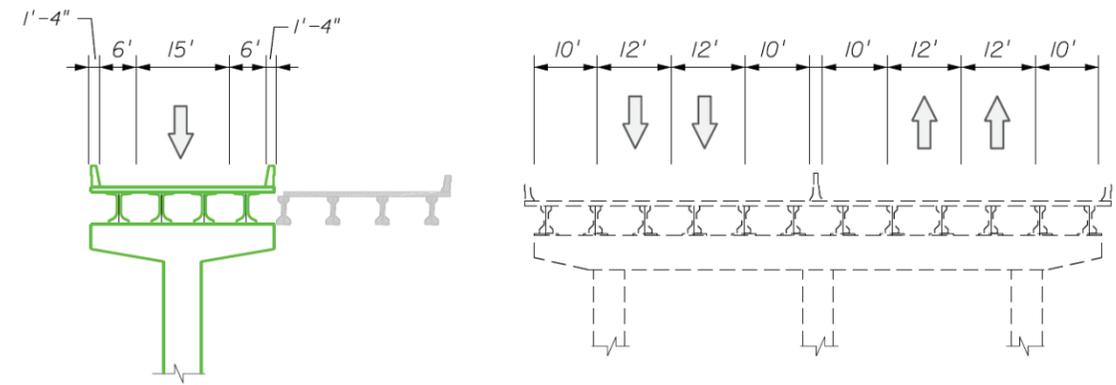


Figure 1.40 Bridge Over SW 88<sup>th</sup> Street/Kendall Drive - Remaining Bridge Removal

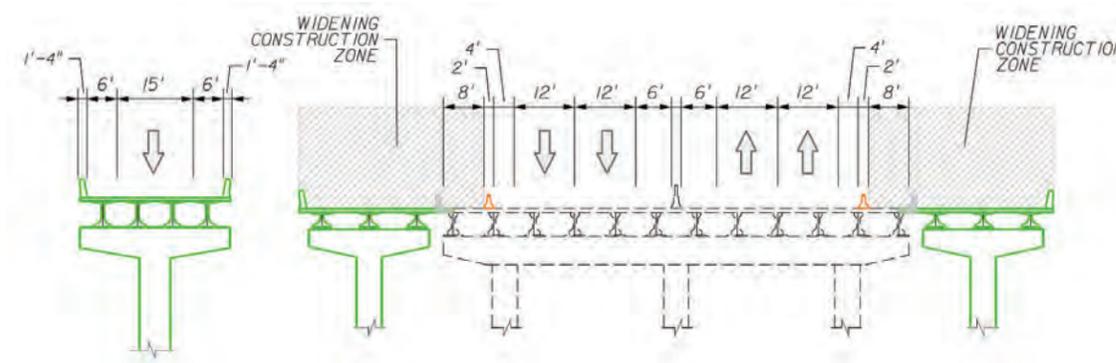


Figure 1.41 Bridge Over SW 88<sup>th</sup> Street/Kendall Drive - Outside Widening

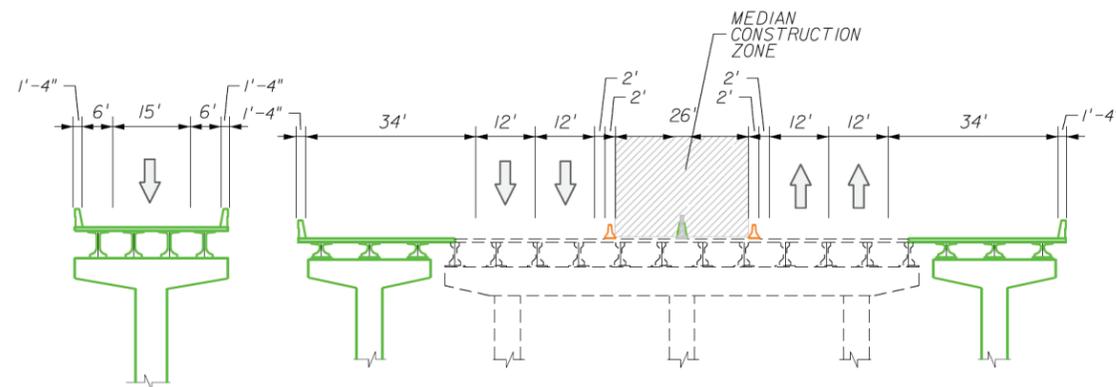


Figure 1.42 Bridge Over SW 88<sup>th</sup> Street/Kendall Drive - Median Reconstruction

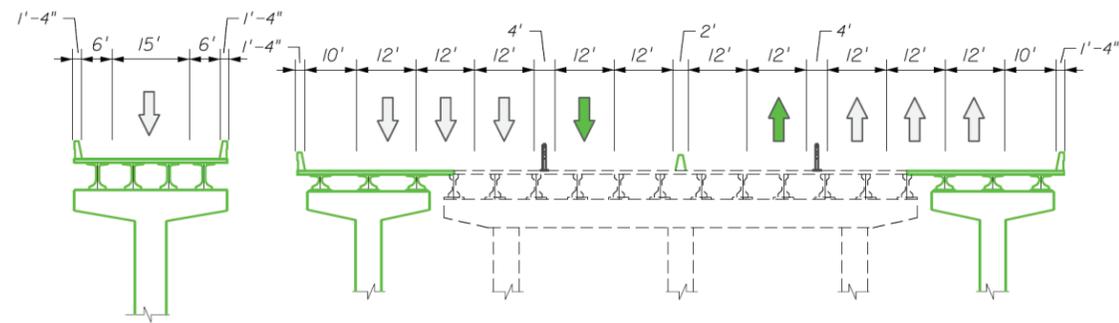


Figure 1.43 Proposed Bridge Over SW 88th Street/Kendall Drive Typical Section

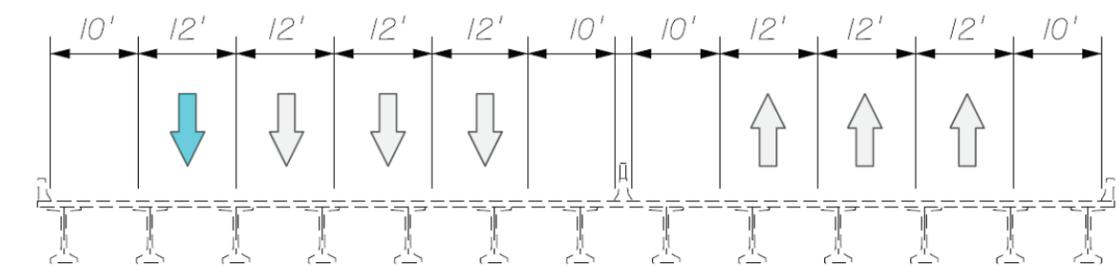


Figure 1.44 Existing Bridge Over SW 56th Street/Miller Drive

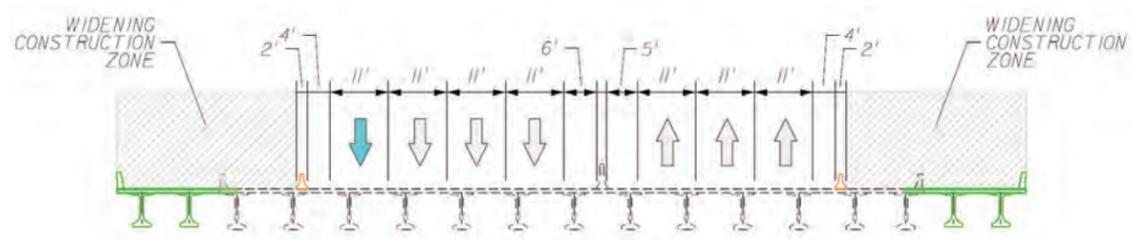


Figure 1.45 Bridge Over SW 56th Street/Miller Drive - Outside Widening

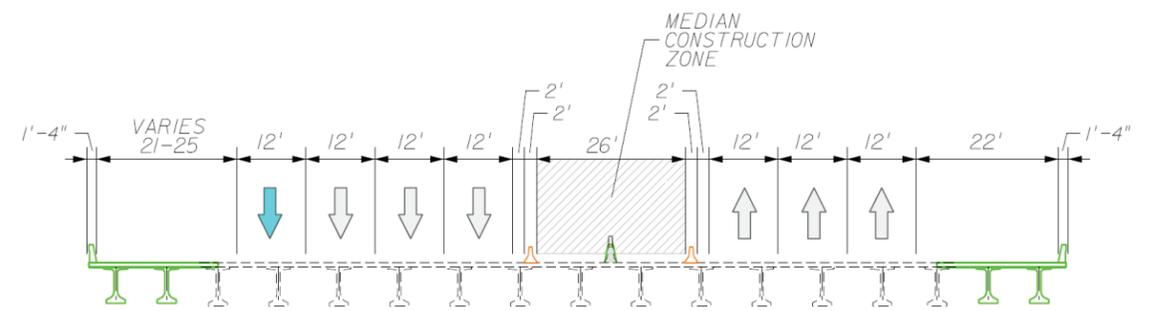


Figure 1.46 Bridge Over SW 56th Street/Miller Drive - Median Reconstruction

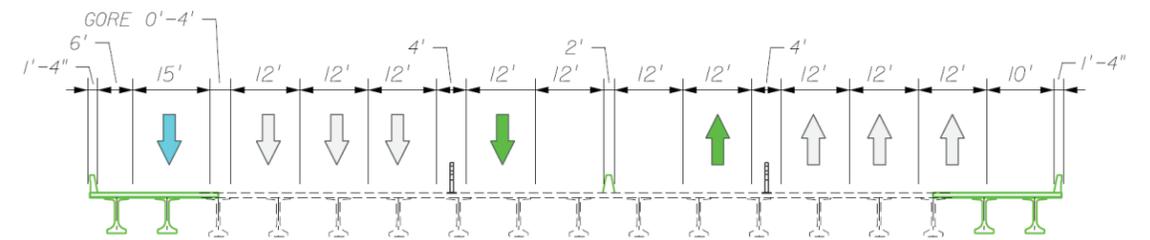


Figure 1.47 Proposed Bridge Over SW 56th Street/Miller Drive Typical Section

**Bridge No. 870129 - SR826 over SW 98th Street**

The replacement of Bridge No. 870129 will require the construction of a portion of the proposed bridge along the west side of the existing bridge to shift the two lanes of traffic while the existing bridge structure is demolished. This will require shifting the travel lanes to the east with the reduction of the existing shoulder to 1 foot, and the existing travel lanes from 12'-0" to 11'-0", thus allowing the removal of the existing overhang to accommodate the first phase of the bridge replacement. The remaining portion of the proposed bridge can be constructed after the demolition of the existing bridge and finalized by the shifting of the travel lanes to their final condition.

Two straddle bents over the existing busway corridor will be required to be built underneath the existing bridge to support the phased construction of the proposed bridge.

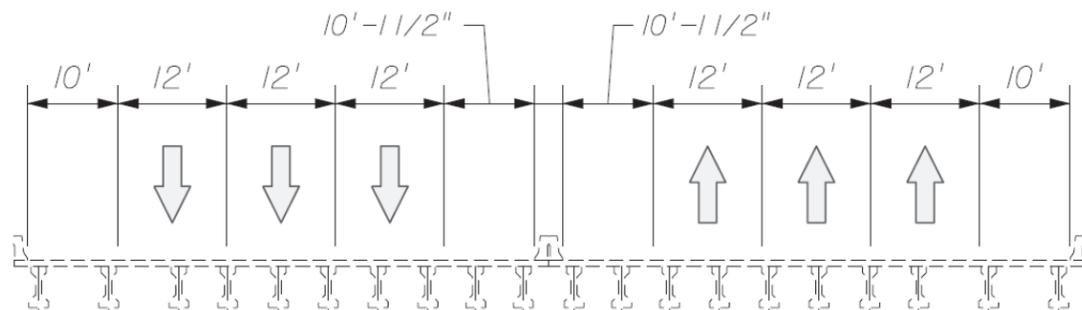
**Bridge 871017 - SB-SR826 over SR874 and Bridge 871018 - NB SR826 over SR874**

These two bridges will require a partial widening to the outside prior to the partial demolition of the bridge to accommodate the construction of the bridge

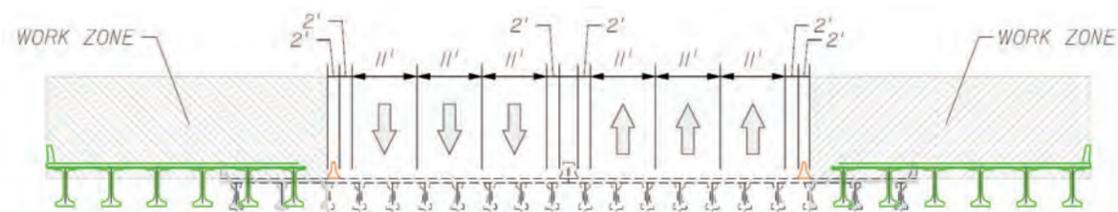
structure carrying the proposed express lanes through the interchange. At each of these bridges traffic would be shifted to the inside while the widening is constructed to the outside. Then traffic will be shifted to the outside of the widened bridge to perform the partial demolition and reconstruction of the inside portion of the bridge.

**Bridge 870132 - SB-SR826 over SW 72<sup>nd</sup> St. and Bridge 870287 - NB SR826 over SW 72<sup>nd</sup> St.**

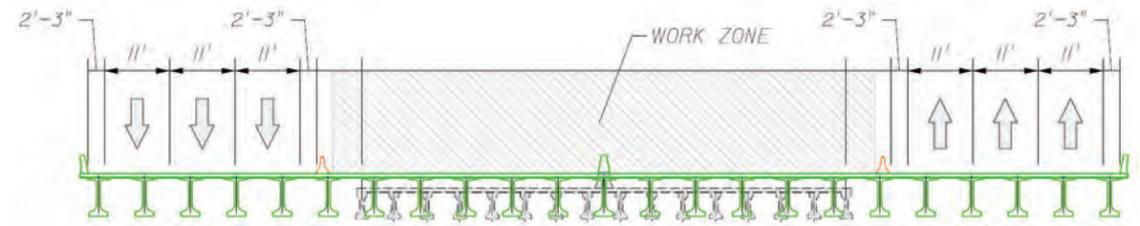
Phased construction of these two bridges will facilitate their replacement while maintaining the same level of traffic. In doing so, the traffic will be shifted to the inside after the lanes and shoulders are reduced in width. Thereafter, the demolition of the outside portion of the bridge structure can be performed followed by the construction of part of the new bridge structure. Then, the traffic can be shifted to the newly built portion of the new bridge, thus allowing for the demolition of the remaining existing bridge and construction of the rest of the new bridge. Refer to [Figure 1.48](#) to [Figure 1.51](#).



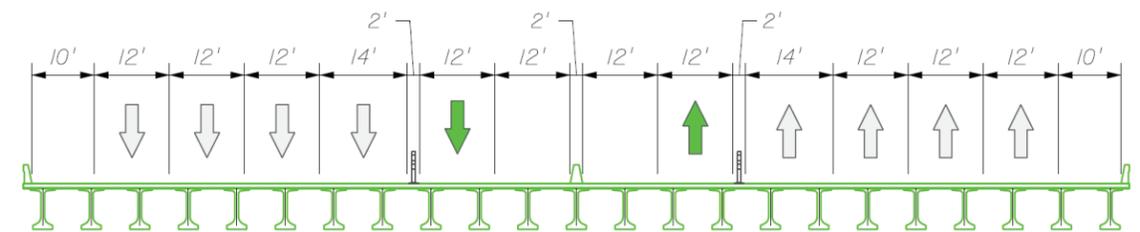
**Figure 1.48 Existing Bridge Over SW 72<sup>nd</sup> Street**



**Figure 1.49 Existing Partial Removal/Partial Construction of Bridge Replacement**



**Figure 1.50 Removal of Remaining Existing Bridge/Complete Bridge Replacement**



**Figure 1.51 Bridge Replacement Proposed Typical Section**

## SR 826/PALMETTO EXPRESSWAY PD&E STUDY

From US 1/SR 5/Dixie Highway to SR 836/Dolphin Expressway  
Miami-Dade County, Florida

