



FPID #430029-2-21-01

ETDM No. 14413



**Atlantic Isle at West of SR A1A
Bridge No. 874218
Project Development and Environment
Study**

Draft Individual Section 4(f) Evaluation

FDOT District Six
1000 NW 111th Avenue
Miami, Florida 33172

Atlantic Isle Bridge (Bridge No. 874218)
Atlantic Avenue, Sunny Isles Beach, FL
Miami-Dade County, FL

January 2024



DRAFT Individual Section 4(f) Evaluation

Florida Department of Transportation

District Six

Atlantic Isle at West of SR A1A, Bridge No. 874218

Atlantic Avenue, 0.25 miles west of SR A1A

Miami-Dade County, Florida

Financial Management Number: 430029-2-21-01

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January 2024

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. § 327 and a Memorandum of Understanding dated May 26, 2022, and executed by the Federal Highway Administration and FDOT.

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ACRONYMS AND ABBREVIATIONS

APE	area of potential effect
BBAP	Biscayne Bay Aquatic Preserve
ca.	circa
CAMA	Office of Coastal and Aquatic Management Areas
CFR	Code of Federal Regulations
CIP	cast-in-place
CRAS	Cultural Resource Assessment Survey
DOI	U.S. Department of the Interior
EFH	Essential Fish Habitat
ETDM	Efficient Transportation Decision Making
FDEP	Florida Department of Environmental Protection
FDOT	Florida Department of Transportation
FHWA	Federal Highway Administration
FMSF	Florida Master Site File
HAER	Historic American Engineering Record
HALS	Historic American Landscape Survey
HAPC	Habitat Area of Particular Concern
HPB	Historic Preservation Board (City of Sunny Isles)
NPS	National Park Service
NRE	Natural Resources Evaluation
NRHP	National Register of Historic Places
OEM	Office of Environmental Management (FDOT)
OGC	Office of General Counsel
OWJ	official(s) with jurisdiction
PD&E	Project Development and Environment
ROW	right-of-way
SHPO	State Historic Preservation Officer
SR	state road
TSMO	Transportation Systems Management and Operations
TTC	temporary traffic control
USC	United States Code
USDOT	U.S. Department of Transportation

1.0 INTRODUCTION

The Florida Department of Transportation (FDOT), District Six, is conducting a Project Development and Environment (PD&E) Study to address the deficiencies of the existing Atlantic Isle Bridge (Bridge No. 874218). The Atlantic Isle Bridge is a historic bridge located on Atlantic Island just west of State Road (SR) A1A (Collins Avenue), within the City of Sunny Isles Beach in Miami-Dade County, Florida. The limits of the proposed project encompass the bridge (along Atlantic Avenue) and approaches for a distance of approximately 0.009 mile. Figure 1-1 presents the Project Location Map. The purpose of this study is to evaluate alternatives for the potential rehabilitation or replacement of the Atlantic Isle Bridge.

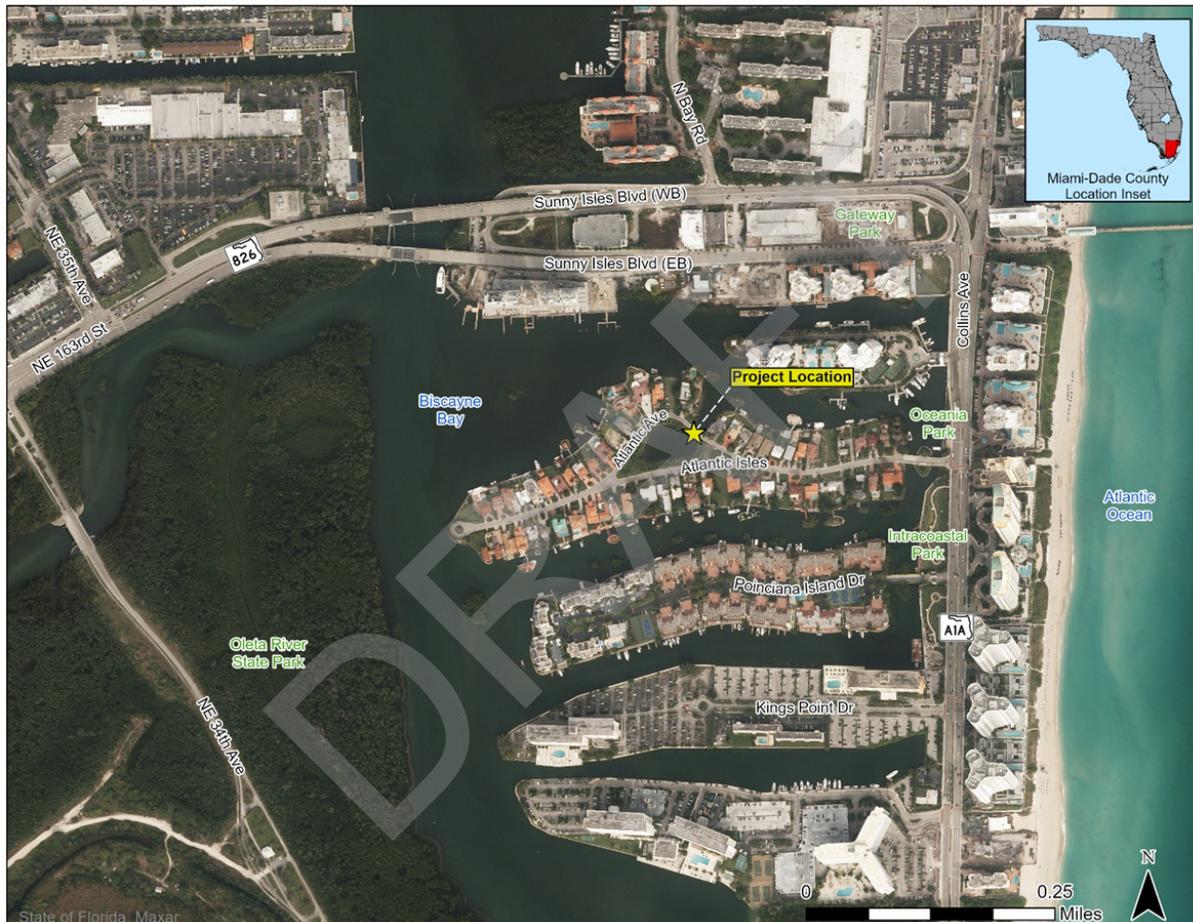


Figure 1-1 Project Location Map

The PD&E Study evaluates the need for improvements and provides documented environmental and engineering analyses to assist FDOT in reaching a decision on location and conceptual design for improvements to the Atlantic Isle Bridge. Additional products of the PD&E Study include preliminary engineering conceptual plans, environmental studies, a public outreach program, and other information that can be directly used in the final design of the project.

1.1 PROJECT DESCRIPTION

The Atlantic Isle Bridge was constructed circa 1925 and is located on Atlantic Avenue, an approximate 0.25-mile-long, one-way eastbound undivided roadway that serves residential traffic and service vehicles. Atlantic Isle is a two-way, east–west residential roadway that intersects with Atlantic Avenue and is located on the south side of the Atlantic Isle Lagoon. There are approximately 14 residential properties along Atlantic Avenue that use the bridge to access their properties on the one-way roadway. The functional classification for both facilities is local road. The roadways on Atlantic Island—including the Atlantic Isle Bridge—are owned, operated, and maintained by the City of Sunny Isles Beach.

The Atlantic Isle Bridge spans approximately 60 feet over a narrow channel between the Lake of the Isles (Atlantic Isle Lagoon) and Biscayne Bay. The west and east bridge approaches are approximately 16 feet wide. The bridge typical section is approximately 20 feet wide with one 10-foot-wide travel lane in the center, and includes a planter easement, curbs, and barrier walls on both sides. Bicyclists and pedestrians must share the 10-foot-wide travel lane to cross the bridge as no sidewalks are provided on the existing facility.

The bridge is open to vehicular traffic that meets posted weight restrictions and is used for access to the residential properties on Atlantic Avenue. The Atlantic Avenue roadway typical section east and west of the bridge consists of 16 feet of pavement used by one-way traffic with curb and gutter on the outside. The posted speed limit along Atlantic Isle and Atlantic Avenue is 20 miles per hour. Figure 1-2 shows the current traffic pattern at the project location, as well as the project study area. The project study area is within the historic triangular landscape of the Atlantic Island Park [Florida Master Site File (FMSF) No. 8DA6433], which is both privately and publicly owned.



Figure 1-2. Project Study Area

1.2 PURPOSE AND NEED

The purpose of the project is to address the structural and functional deficiencies of the existing bridge to provide a safe and functional route for the surrounding community/traveling public.

According to a bridge inspection conducted on September 29, 2023, the Atlantic Isle Bridge (Bridge Identification Number 874218) has been determined to be 'functionally obsolete', with a sufficiency rating of 40.9 and a health index of 60.39. The sufficiency rating and health index values vary from 0 (worst) to 100 (best). Existing functional deficiencies observed during the bridge inspection include substandard traffic barriers, multi-directional cracks in the asphalt overlay, and missing oolitic limestone (coral rock) on some areas of the north face of the arch. The southwest corner along the underside edge and the south side of the arch have spalls and delamination with exposed steel and areas of corrosion stains throughout the length of the arch along the fallen coral rock. In addition, the arch underside has a core hole at the center of the mid-span and exhibits delamination at random locations.

The bridge also has weight restrictions and limitations with an existing Bridge Load Posting Sign for single unit and Class 1 trucks at 12 tons and 21 tons, respectively. The load posting on the bridge poses a significant issue for the residents of Atlantic Isle since garbage trucks, as well as trucks transporting concrete, building materials/demolition debris, and other urban goods, may not be within an adequate weight range to cross the bridge. As trucks are restricted to smaller loads when crossing the bridge and are forced to make several circuitous trips to transport freight, unnecessary truck traffic is being added to the surrounding roadway network. In some cases, fire trucks, emergency vehicles, delivery or moving vans, and construction vehicles also exceed the posted bridge load limit. Overweight vehicles accessing neighboring properties must complete a crossover requiring special procedures, such as the use of flagmen to proceed. Given these conditions, the bridge does not meet the current transportation needs of the community.

1.3 REQUIREMENTS AND APPLICABILITY OF SECTION 4(f)

1.3.1 Section 4(f) Applicability

Per the Federal Highway Administration (FHWA) *Section 4(f) Policy Paper*, Section 4(f) of the U.S. Department of Transportation (USDOT) Act of 1966 applies to all FDOT transportation projects that use federal aid funds or require the approval of a USDOT agency and involve the "use" of a Section 4(f) property or resource (FHWA 2012). In addition, for the statute to apply to a proposed project there are four conditions that must all be true:

1. Must be a transportation project that utilizes federal aid funds;
2. Must require an approval from a USDOT agency to proceed;
3. Must require the use of land from property protected under Section 4(f) (refer to 23 USC § 138(a) and 49 USC § 303(a)); and
4. None of the regulatory applicability rules or exceptions applies (refer to 23 CFR 774.11 and 13).

Section 4(f) protects the following properties of national, state, and local significance:

- Parks and recreational areas of national, state, or local significance that are both publicly owned and open to the public
- Publicly owned wildlife and waterfowl refuges of national, state, or local significance that are open to the public to the extent that public access does not interfere with the primary purpose of the refuge
- Historic sites of national, state, or local significance in public or private ownership regardless of whether they are open to the public

The proposed project would receive FHWA funding and requires the "use" of properties protected by Section 4(f) as defined in 23 CFR 774.17. Therefore, documentation of compliance with Section 4(f) is required.

1.3.2 Section 4(f) Use Definitions

As defined in 23 CFR 774.17, the "use" of a protected Section 4(f) property occurs when any of the following conditions are met:

Direct Use: A direct use of a Section 4(f) property occurs when property is permanently incorporated into a proposed transportation project. This may occur as a result of partial or full acquisition of a fee simple interest, permanent easement, or temporary easement that exceeds regulatory limits.

Temporary Occupancy: A temporary use of a Section 4(f) property occurs when there is a temporary occupancy of property that is considered adverse in terms of the preservation purposes of the Section 4(f) statute. A temporary occupancy of property does not constitute a use of a Section 4(f) resource when all of the following conditions are satisfied:

- Duration must be temporary (that is, less than the time needed for construction of the project) and there should be no change in ownership of the land;
- Scope of the work must be minor (that is, both the nature and the magnitude of the changes to the Section 4(f) property are minimal);
- There are no anticipated permanent adverse physical impacts, nor will there be interference with the protected activities, features, or attributes of the property, on either a temporary or permanent basis;
- The land being used must be fully restored (that is, the property must be returned to a condition which is at least as good as that which existed prior to the project); and
- There must be documented agreement of the official(s) with jurisdiction over the Section 4(f) resource regarding the above conditions.

Constructive Use: A constructive use of a Section 4(f) property occurs when a transportation project does not incorporate land from the resource, but the proximity of the project results in impacts so severe that the protected activities, features, or attributes that qualify the resource for protection under Section 4(f) are substantially impaired (23 CFR 774.15).

1.3.3 Section 4(f) Approval Options

An Individual Section 4(f) Evaluation must be completed when approving a project that requires the use of Section 4(f) property if the use results in a greater than *de minimis* impact and a Programmatic Section 4(f) Evaluation cannot be applied to the situation (23 CFR 774.3). Because potential alternatives may have an adverse effect on the Atlantic Island Resource Group (8DA19241) and the Atlantic Island Bridge (8DA6433), a programmatic bridge approval does not apply. A *de minimis* impact can only be applied when a project does not impact or result in adverse effects to historic resources, and does not adversely affect parks, recreation area, and wildlife and waterfowl refuges protected under Section 4(f). The impact is not Section 4(f) *de minimis* in nature, and therefore an Individual Section 4(f) Evaluation must be prepared.

The Individual Section 4(f) Evaluation documents the evaluation of the proposed use of Section 4(f) properties in the project area for all potential alternatives. The Individual Section 4(f) Evaluation requires two findings as follows:

1. That there is no feasible and prudent alternative that completely avoids the use of Section 4(f) property.
2. That the project includes all possible planning to minimize harm to the Section 4(f) property resulting from the transportation use.

2.0 DESCRIPTION OF SECTION 4(f) PROPERTIES

The 2022 Cultural Resource Assessment Survey (CRAS) resulted in the identification of four historic properties within the project area of potential effect (APE): the Atlantic Island (also known as Isle) Bridge (8DA6433), the Atlantic Island Resource Group (8DA19241), with two contributing resources, the Lake of the Isles (8DA15824) and Atlantic Island Park (8DA15825) (Table 2-1). In a letter dated February 2, 2022, the State Historic Preservation Officer (SHPO) concurred with the determinations of the 2022 CRAS (Janus Research 2022), which is included in Appendix A. The properties listed in Table 2-1 were determined NRHP-eligible by SHPO and, therefore, are determined applicable as Section 4(f) resources.

Table 2-1. Identified Significant Historic Properties within the Historic Resources APE

FMSF No.	Site Name	Resource Type/Style	Year Built	Owner/Maintaining Agency	National Register of Historic Places (NRHP) Evaluation
8DA6433	Atlantic Island Bridge	Historic Bridge	ca. 1925	City of Sunny Isles Beach	Individually Eligible and Eligible as a Contributing Resource 8DA19241
8DA15824	Lake of the Isles	Designed Historic Landscape	ca. 1925	City of Sunny Isles Beach	Eligible as a Contributing Resource to 8DA19241
8DA15825	Atlantic Island Park	Designed Historic Landscape Publicly Owned Park/Recreational Area	ca. 1925	City of Sunny Isles Beach/Atlantic Island Civic Association	Eligible as a Contributing Resource to 8DA19241
8DA19241	Atlantic Island Resource Group	Designed Historic Landscape	ca. 1925	City of Sunny Isles Beach/Atlantic Island Civic Association	Eligible

Figure 2-1 presents the locations of the four NRHP-eligible properties in relation to the study's APE. Brief narratives and photographs of the four properties follow on succeeding pages.

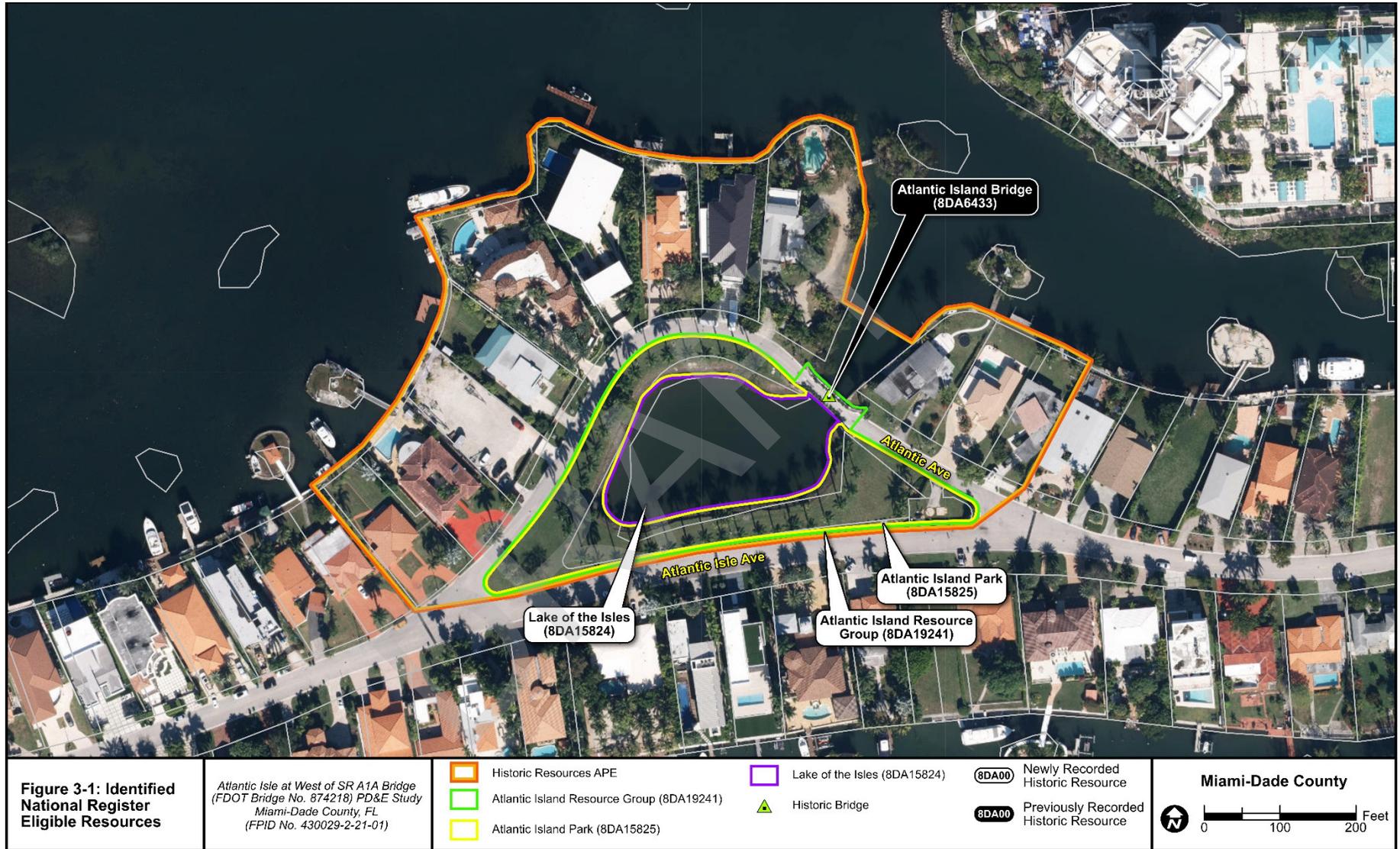


Figure 2-1. Identified National Register Eligible Resources

2.1 ATLANTIC ISLAND RESOURCE GROUP (8DA19241)

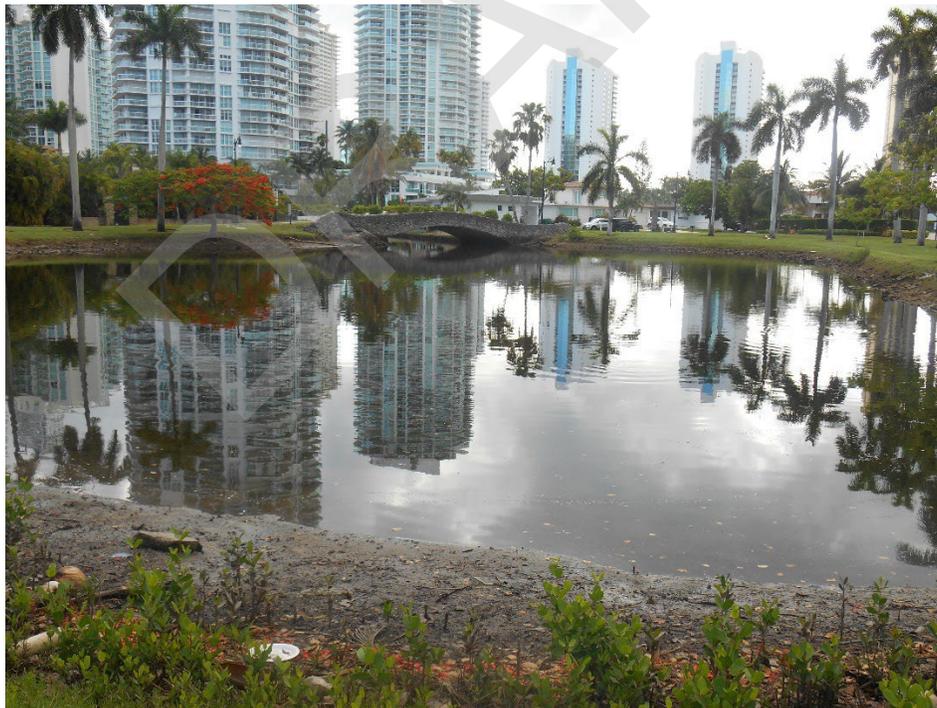
The Atlantic Island Resource Group (8DA19241) is a designed historic landscape comprised of the NRHP-eligible Atlantic Isle Bridge (8DA6433), the artificial Lake of the Isles (8DA15824), and the surrounding triangular-shaped Atlantic Island Park (8DA15825), all of which were constructed circa 1925 and are directly connected spatially and historically (Figures 2-2 through 2-4).



**Figure 2-2. The Atlantic Island Bridge (8DA6433), ca. 1925.
Determined NRHP-eligible, Facing Northeast**



**Figure 2-3. The Lake of the Isles (8DA15824), ca. 1925,
Determined NRHP-eligible, Facing Southwest**



**Figure 2-4. Atlantic Island Park (8DA15825), ca. 1925,
Determined NRHP-eligible, Facing East**

Atlantic Island, like several of the islands in Sunny Isles Beach, was a subdivision created in the mid-1920s as the result of a dredge-and-fill project funded by New York transplant Henry Graves. Graves purchased 2.26 square miles of land from the Model Land Company in 1920 (part of which would become Sunny Isles and known today as Sunny Isles Beach) and marketed his development as “Sunny Isles-The Venice of America” (Bramson 2007). Sunny Isles included land on a natural barrier island, several smaller natural landforms in Biscayne Bay, and a series of artificial dredged islands in Biscayne Bay. A newspaper advertisement from 1925 depicts an ad of the planned development of artificial finger islands in Figure 2-5 (*Miami Herald* 1925). Graves envisioned the area as a community with a mixture of residences and resorts and began development with the construction of a number of gently sloping concrete bridges surfaced with limestone. At least three islands were dredged and filled during this period, and named by Graves as Fairyland Island, Atlantic Island, and Poinciana Island. Graves also oversaw the construction of a bathhouse, casino, and pier in Sunny Isles (Bramson 2007).

Many artificial waterways were designed and built as a part of Sunny Isles to help sell lots and beautify the associated islands (Lennox 1989). The Lake of the Isles (8DA15824), located in the center of Atlantic Island was described in a 1925 newspaper promotional about Sunny Isles as, “A scenic little lake in the very heart of the Atlantic Island Subdivision, giving many lots a double water frontage” (*Miami Daily News* 1925). A gazebo was originally part of the park that surrounds the lake but has since been demolished (City of Sunny Isles Beach Historic Preservation Board 2005). The park includes an open grassy area with palm trees lining the lakeshore.

The first subdivision within Sunny Isles was the Bella Vista subdivision located north of Atlantic Island, built circa 1922 and platted in 1927 (*Miami Daily Metropolis* 1922). Of the other subdivisions within the Sunny Isles development, only the Bella Vista subdivision also contained artificial lakes. Atlantic Island was constructed in 1925 and platted in 1928, with the Lake of the Isles (8DA15824) on the center of the island with parcels surrounding the lagoon and park and Atlantic Avenue extending around the subdivision and crossing the Atlantic Island Bridge (Figure 2-6). The financial bust that began in 1926 left Graves’ developments only partially realized. Atlantic Island was purchased by the North Miami Beach Corporation, under the leadership of Milwaukee magnate Kurtis Froedtert in 1936 and construction of luxury homes resumed (Janus Research 2016).

Froedtert completed three more subdivisions within Grave’s vision: Poinciana Island, Royal Palm Island (now King’s Court), and Bayview Point. Froedtert rebranded Sunny Isles as “the American Riviera,” which featured Lake of the Isles, the park and gazebo, and luxury homes found on Atlantic Island (City of Sunny Isles Beach Historic Preservation Board 2005).

THE NEW ATLANTIC ISLAND SUBDIVISION

SUNNY ISLES OCEAN BEACH COMPANY

LOTS NOW PLACED ON SALE

Sunny Isles Ocean Beach Co. ATLANTIC ISLAND SUBDIVISIONS

Charming vistas from every homesite. Views include Oleta and Biscayne Rivers, Bella Vista Bay, Atlantic Ocean, six miniature lakes, ten artificial waterways, sixty palm, mangrove, summer br and tea garden islands.

EVERY LOT A WATER LOT

H. B. Graves' Sunny Isles properties include 2,365 acres, 10 miles Ocean, Bay and River frontage. Ocean to Federal Highway; Biscayne Bay from Miami Shores to N. Shore of Dunning Bay. A Venetian development in the making without a parallel.

OCEAN SURF FROM ONE TO EIGHT MINUTES' WALK FROM EVERY HOMESITE. PRESENT MAIL ADDRESS, LEMON CITY OR FULFORD, FLA.

ONLY ONE SUNNY ISLES

Map of ATLANTIC ISLAND SUNNY ISLES April 1, 1925.

Computed and Traced from W.C. Swann's Shoreline Map of June 1912. The Builders' Parcel Sketch of July 1916 and W.C. Swann's Map of March 1923.

Aug. 28, 1926. George F. Young, Inc. Engineers, Architects.

With the exception of a few irregular lots, all lots are approximately equal in size. One family residence may be erected on a 1/2 acre lot. A combination of two or more lots, with or without water, may be purchased for \$10,000.00 or more. Prices subject to change without notice.

EVERY LOT A WATER LOT

SUNNY ISLES OCEAN BEACH CO. MIAMI, FLA.

Atlantic Island building plots 60, 90, 120, 150 or more feet wide, average 155 feet or more deep. 60 feet Boulevard.

An island subdivision from the Atlan-

Obtained from Newspapers.com archive; The Miami Herald (Miami, Florida) 1 April 1925, Wed Page 42

Figure 2-5. A 1925 Newspaper Advertisement for the Atlantic Island Subdivision

Source: Newspapers.com

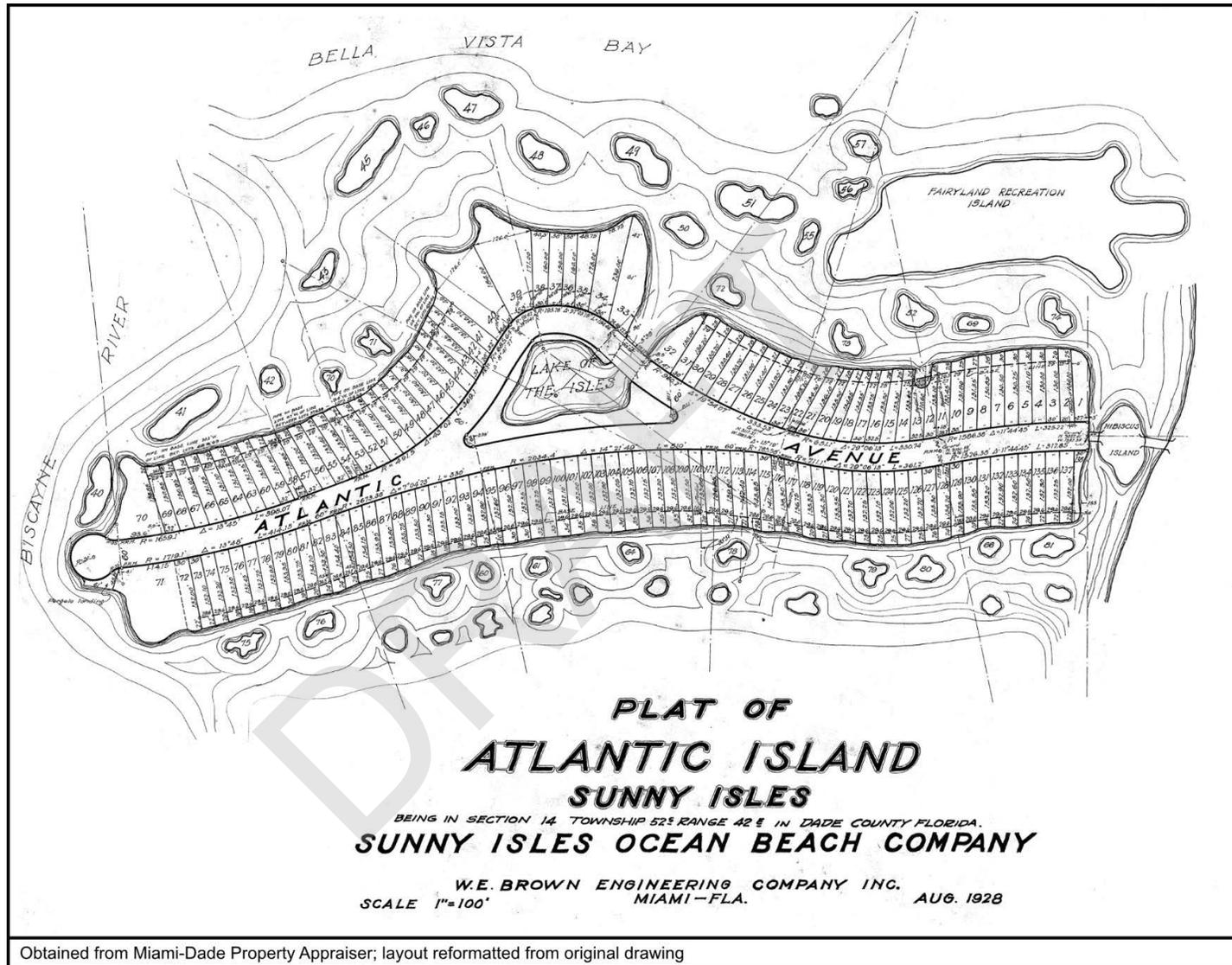


Figure 2-6. The 1928 Plat of Atlantic Island Subdivision with Lake of the Isles Depicted in the Center of the Island
Source: Miami-Dade Property Appraiser

An illustrated postcard of Sunny Isles from the 1930s depicts an oolitic limestone bridge and water feature, which could be the Lake of the Isles (8DA15824) and adjacent Atlantic Island Bridge (8DA6433). The illustration does not include enough specific information to confirm the location depicted in the drawing but is representative of the design principles and guidelines used when developing Sunny Isles (Figure 2-7). A historic photo of the Atlantic Island entrance bridges depicts the towers that were historically featured in the corners of all of the bridges throughout the development (Figure 2-8). An illustrated aerial of Sunny Isles from 1940s depicts the development including several man-made finger islands and a road system. Atlantic Island is seen in the center of the illustration (Figure 2-9).

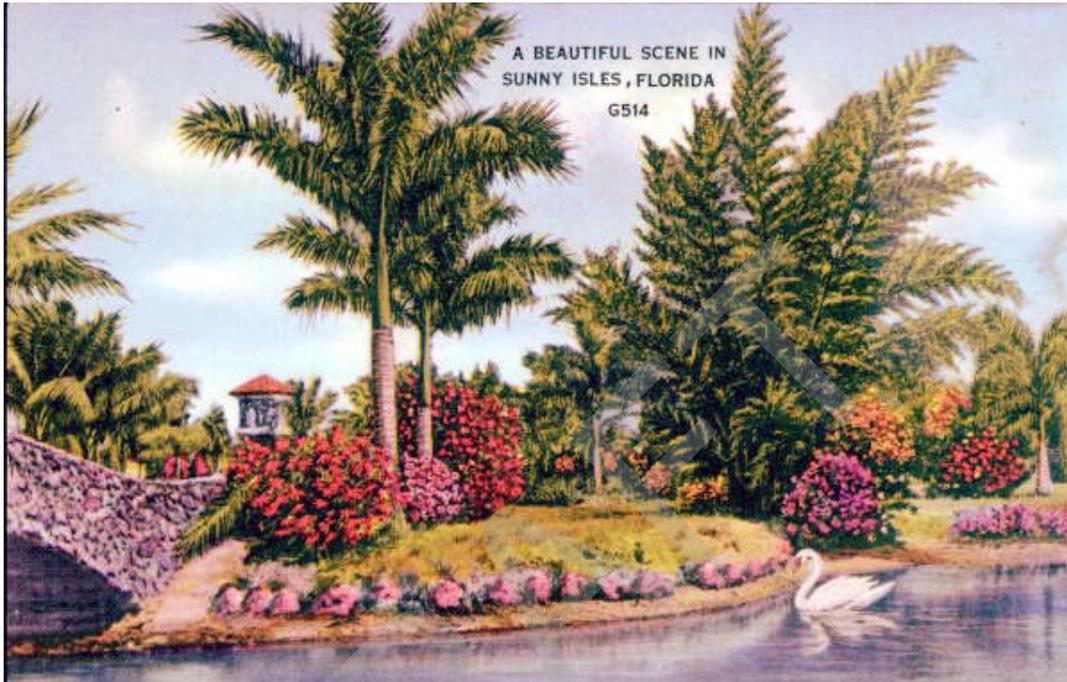


Figure 2-7. A Circa 1930s Postcard of the Sunny Isles Development, Featuring a Bridge with an Oolitic Limestone Exterior Wall Crossing Water

Source: Florida Memory (n.d.)



Figure 2-8. Historic Photograph of the Entrance Bridges to the Atlantic Island Development

Source: Bramson (2007)



Figure 2-9. A Circa 1940 Illustrated Aerial of the Sunny Isles Development, with Atlantic Island Depicted in the Center of the Image

Source: Florida Memory (1940)

In 1984, the Atlantic Island Bridge (8DA6433) along with the two entrance bridges to Atlantic Island located east of the project area were designated as historic sites by the Metropolitan Dade County Historic Preservation Board. The bridges were noted as “tangible examples of the beautifying features of the early development of Atlantic Island” and as significant for their unique architectural design (Metropolitan Dade County Historic Preservation Board 1984). However, the two entrance bridges were reconstructed circa 1995, leaving the Atlantic Island Bridge (8DA6433) as the only bridge original to the development. In 2005, the Atlantic Island Bridge (8DA6433) and two reconstructed Atlantic Island entrance bridges were redesignated as historic sites by the City of Sunny Isles Beach (City of Sunny Isles Beach Historic Preservation Board 2005). The Lake of the Isles (8DA15824) was also historically known as Atlantic Isles

Lagoon according to the City of Sunny Isles historic designation plaque found on the Atlantic Island Bridge (City of Sunny Isles Beach Historic Preservation Board 2005). Based on analysis of historic aerials, the artificial lakes in the Bella Vista subdivision were filled in the 1950s and 1960s, leaving the Lake of the Isles (8DA15824) as the only remaining original artificial lake in the Sunny Isles development. Atlantic Island Park (8DA15825) retains its historic design and layout with a palm tree court, comprised of replaced plant material, surrounding the lakeshore.

The Atlantic Island Bridge (8DA6433), Lake of the Isles (8DA15824), and Atlantic Island Park (8DA15825) are intrinsically linked and retain their historic footprint and spatial relationships. The historic bridge and landscape elements are contributing elements to the Atlantic Island Resource Group (8DA19241). Descriptions and additional photographs of each significant resource are included in the following text, as well as an analysis of NRHP eligibility.

2.2 ATLANTIC ISLAND BRIDGE (8DA6433)

The Atlantic Island Bridge (8DA6433), constructed circa 1925, carries Atlantic Avenue over the Ocean Canal between the Lake of the Isles (8DA15824) and Biscayne Bay in Section 14 of Township 52 South, Range 42 East on the North Miami (1988) USGS quadrangle map (Figure 2-10). The bridge is an approximately 50-foot-long concrete arch deck bridge and the outer walls of the bridge are covered in oolitic limestone. The inner walls of the bridge are treated with irregular whitewashed stucco. Non-historic alterations include the addition of curbs and concrete flowerpots with rubber plants (Figure 2-11). The bridge is currently one lane wide and carries one-way eastbound traffic, with a weight restriction. The Atlantic Island Bridge (8DA6433), along with the Lake of the Isles (8DA15824) and Atlantic Island Park (8DA15825), is one of three resources that comprise the Atlantic Island Resource Group (8DA19241).

The Atlantic Island Bridge (8DA6433) is the only remaining historic bridge of the three originally constructed for the island in 1925 and one of the few remaining elements of pre-World War II development in Sunny Isles Beach. The character-defining elements of the Atlantic Island Bridge (8DA6433) include its oolitic limestone surface on its exterior, the irregular whitewashed stucco on the interior of the bridge, and its low, gentle slope. Oolitic limestone was quarried in southern Miami-Dade County beginning in the mid-nineteenth century and was used as a material in a number of historic buildings throughout the county (City of Miami 2011). While oolitic limestone was a common material for chimney stacks or architectural embellishment during the early twentieth century in Miami-Dade County, the application of oolitic limestone to the exterior of the Atlantic Island Bridge (8DA6433) is unique.

Although the four towers that originally decorated its corners were removed after 1989, the bridge retains historic integrity and it continues to convey its significance as a rare example of pre-World War II development in Sunny Isles Beach. On August 23, 2016, the SHPO determined the Atlantic Island Bridge to be individually NRHP eligible under Criteria A and C in the areas of Community Planning and Development and Architecture for its association with the development of the Atlantic Island Subdivision and Sunny Isles Beach, as well as its unique design (Janus Research 2016). In 2022, the SHPO determined that the Atlantic Island Bridge was also NRHP eligible as a contributing resource to the newly recorded Atlantic Island Resource Group (8DA19241) (Janus Research 2022).



Figure 2-10. The Atlantic Island Bridge (8DA6433), circa 1925, Determined NRHP-eligible, Facing East



Figure 2-11. The Atlantic Island Bridge (8DA6433), circa 1925, Determined NRHP-eligible, Facing North

2.3 LAKE OF THE ISLES (8DA15824)

The Lake of the Isles (8DA15824) is located in the center of Atlantic Island on an approximately 0.51-acre property between Atlantic Avenue to the west and the Atlantic Isle Bridge (DA6433) to the east in Section 14 of Township 52 South, Range 42 East on the North Miami (1988) USGS quadrangle map, in the City of Sunny Isles Beach, Miami-Dade County, Florida (Figure 2-12). The artificial water feature is an approximately 275-foot-long and 170-foot-wide crescent-shaped lagoon surrounded by the triangular-shaped open grassy area with palm trees lining the lakeshore. Designed historic landscapes are recorded in the FMSF under the National Register category of historic district or site. The Lake of the Isles is categorized as a site per *National Register Bulletin 15*, which specifies “designed landscape” as an example of a historic site (National Park Service 1995). The Lake of the Isles (8DA15824), along with the Atlantic Island Bridge (8DA6433) and Atlantic Island Park (8DA15825), is one of three resources comprising the Atlantic Island Resource Group (8DA19241), which were all determined NRHP-eligible by SHPO in 2022. The Lake of the Isles (8DA15824) is one of few original beautifying features left from the Sunny Isles development and an extant example of an artificial water. The Lake of the Isles (8DA15824) has retained its historic design and layout and remains a central feature of the Atlantic Island Subdivision.



Figure 2-12. The Lake of the Isles (8DA15824), circa 1925, Determined NRHP-eligible, Facing Southwest

2.4 ATLANTIC ISLAND PARK (8DA15825)

Atlantic Island Park (8DA15825) is a designed historic landscape feature located in the center of Atlantic Island in Section 14 of Township 52 South, Range 42 East on the North Miami (1988) USGS quadrangle map, in the City of Sunny Isles Beach, Miami-Dade County, Florida (Figure 2-13). The outer portion of the park that abuts Atlantic Avenue is owned by the City of Sunny Isles Beach, while the inner portion of the park is owned by the Atlantic Island Civic Association. The triangular-shaped park was constructed circa 1925 and features an open grassy area with a palm tree court lining the Lake of the Isles (8DA15824). Designed historic landscapes are recorded in the FMSF under the National Register category of historic district or site. Atlantic Island Park is categorized as a site per *National Register Bulletin 15*, which specifies

“designed landscape” as an example of a historic site (National Park Service 1995). The Atlantic Island Park (8DA15825), along with the Atlantic Island Bridge (8DA6433) and the Lake of the Isles (8DA15824), is one of three resources that comprise the Atlantic Island Resource Group (8DA19241), which were determined NRHP-eligible by SHPO in 2022. The grassy park surrounding the Lake of the Isles (8DA15824) is one of the original beautifying features left from the Sunny Isles development and was used as an advertising and promotional feature of the Sunny Isles development during the 1930s, when the development was completed by Kurtis Froedtert. While an original gazebo has been removed from the park, the lake and surrounding landscape have retained their historic design and layout in relation. Although the existing palm tree court surrounding the lakeshore and other landscaped materials have been replaced over time, the replacements have been made with in-kind plants and vegetation and are consistent with early descriptions of the park. A small 0.05-acre portion at the southeastern corner of the park was deeded to the Miami-Dade County Water and Sewer Department by the City of Sunny Isles Beach in 2010 and features a county water and sewage pump station. Because the park is NRHP-eligible, it is protected by Section 4(f) as a historic site. However, the park is also publicly owned by the City of Sunny Isles Beach and is used for recreation. Because the park is recreational and significant to the community of Atlantic Island, it is also protected by Section 4(f) as a publicly owned park/recreation area.



Figure 2-13. Atlantic Island Park (8DA15825), circa 1925, Determined NRHP-eligible, Facing East

2.5 ATLANTIC ISLAND RESOURCE GROUP (8DA19241)

The Atlantic Island Resource Group (8DA19241) is a designed historic landscape located in the center of Atlantic Island in Section 14 of Township 52 South, Range 42 East on the North Miami (1988) USGS quadrangle map, in the City of Sunny Isles Beach, Miami-Dade County, Florida (Figure 2-14). The designed historic landscape consists of three contributing resources: the NRHP-eligible Atlantic Isle Bridge (DA6433), the artificial Lake of the Isles (8DA15824), and Atlantic Island Park (8DA15825). Designed historic landscapes are recorded in the FMSF under the National Register category of historic district or site. The Atlantic Island Resource Group (8DA19241) is categorized as a district because it contains three distinct historic resources “united historically or aesthetically by plan or physical development” (National Park Service 1995).



Figure 2-14. The Atlantic Island Resource Group (8DA19241), Determined NRHP-eligible, Facing East

Per *National Register Bulletin 18*, a designed historic landscape is defined as “a landscape that has significance as a design of work or art; was consciously designed and laid out by a master gardener, landscape architect, architect, or horticulturist to a design principle, or an owner or other amateur using a recognized style or tradition in response or reaction to a recognized style or tradition; has a historical association with a significant person, trend, event, etc. in landscape gardening or landscape architecture; or a significant relationship to the theory or practice of landscape architecture” (Keller and Keller 1987). Select examples of designed historic landscapes include “plaza/square/green/mall or other public spaces, subdivisions and planned communities/resorts, parks, (local, state and national), grounds designed and developed for outdoor recreation, or bodies of water and fountains” (Keller and Keller 1987).

The components of the Atlantic Island Resource Group (8DA19241) are extant examples of designed features associated with the beginnings of the Sunny Isles development and luxury residential development trends during the 1920s. The resources were intentionally sited in the physical center of the artificial Atlantic Island and designed with the goal of beautifying the development. Alterations to the resources include replaced material on the bridge and the replacement of landscaped materials within the park such as replaced palm trees and grass. The Atlantic Island Bridge (8DA6433) has previously been determined NRHP eligible. The Lake of the Isles (8DA15824), which is the only remaining artificial lake within Sunny Isles, and the surrounding Atlantic Island Park (8DA15825) are considered NRHP eligible as part of the current study. The Atlantic Island Resource Group (8DA19241) features the three extant designed central features of the Atlantic Island Subdivision dating to the 1920s and retains a high degree of integrity including location, design intent, setting, feeling, and association. Therefore, the Atlantic Island Resource Group (8DA19241) was determined to be NRHP-eligible under Criteria A and C in the areas of Community Planning and Development and Landscape Architecture by the SHPO in 2022 (Janus Research 2022).

2.6 POTENTIAL SECTION 4(f) PROPERTIES

The Biscayne Bay Aquatic Preserve (BBAP) surrounds Atlantic Island, and directly connects to Lake of the Isles (8DA15824) via the canal underneath the Atlantic Island Bridge (8DA6433), as presented in Figures 1-1 and 1-2. The BBAP is comprised of 67,000 submerged acres between the Oleta River in Miami-Dade County and Card Sound Road Bridge in Monroe County. BBAP is managed by the Office of Coastal and Aquatic Management Areas (CAMA) under the Florida Department of Environmental Protection (FDEP). CAMA manages sites in Florida for the conservation and protection of natural and historic resources and resource-based public use that is compatible with the conservation and protection of lands. BBAP provides habitat for a wide variety of juvenile and adult marine species as well as wading birds. In addition, BBAP offers water-oriented recreational opportunities to the metropolitan areas of Southeast Florida and the Keys. The mission of BBAP is to protect the natural resources for the benefit of future generations and at the same time allow for traditional uses.

The BBAP Management Plan (FDEP 2013) was developed to protect the long-term health of the ecosystems and their resources, and four management programs were created: 1) managing natural and cultural resources, 2) ecosystem science including monitoring and sampling in the bay, 3) education and outreach programs, and 4) promoting and managing public use that supports the protection of the resources of the preserve. As documented in the BBAP Management Plan, the primary function of the BBAP is not for recreational purposes or functions of the property are not for park or recreational use, nor for refuge purposes and it does not represent a significant historic site. Therefore, Section 4(f) does not apply to this resource.

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3.0 ALTERNATIVES CONSIDERED

Initial alternatives considered included Transportation Systems Management and Operations (TSMO) alternatives, multimodal alternatives, a tunnel alternative, a new alignment alternative, and a conversion of the bridge to a pedestrian bridge. Alternatives that did not meet the project's purpose and need or that had constructability issues, significant right-of-way (ROW) impacts, or significant adverse impacts to natural, social, cultural, and physical environmental resources were not considered viable and were eliminated from detailed consideration.

The only TSMO option applicable to this project is to continue limited repairs to the existing bridge. Because the bridge would continue to deteriorate even with routine maintenance and repairs, the TSMO alternative would not preserve the aesthetic facade or the historic integrity of the bridge long term. Therefore, the TSMO alternative was eliminated from detailed consideration as it does not address the bridge's structural and functional deficiencies. There are no multimodal alternatives that are consistent with the project's purpose and need that would address the bridge's structural and functional deficiencies or maintain vehicular access for residences of the island. Therefore, multimodal alternatives were eliminated from detailed consideration. A tunnel also was considered but eliminated because of the significant social, natural, cultural, and physical impacts. A tunnel would result in demolition of the existing NRHP-eligible bridge and have significant impacts to the NRHP-eligible resources, Lake of the Isles and Atlantic Island Park.

Alternatives on new alignments were also considered to avoid adverse effects to the NRHP-eligible Atlantic Island Bridge. A new alignment to the north of the existing bridge (Biscayne Bay side) would require realigning part of Atlantic Avenue northeast and reconstructing a new bridge over Biscayne Bay. This alternative would significantly impact multiple residences along Atlantic Avenue, as well as the sensitive natural resources associated with Biscayne Bay. A new alignment to the south of the existing bridge (lagoon side) would require realigning part of Atlantic Avenue southwest and reconstructing a new bridge over the lagoon. This alternative would significantly impact the NRHP-eligible resources, Lake of the Isles and Atlantic Island Park. While both alternatives avoid the NRHP-eligible Atlantic Island Bridge, they would have an adverse effect on the Atlantic Island Resource Group as well as residential properties and natural resources, as well as additional construction costs. Therefore, an alternative on a new alignment was eliminated from further consideration.

A Pedestrian Bridge Alternative was also considered to avoid an adverse effect to the Atlantic Island Bridge. This alternative would maintain the existing bridge structure as a pedestrian bridge and prohibit all motor vehicle access on the bridge. The existing bridge typical section would remain. This alternative would require widening Atlantic Avenue to two lanes (one lane per direction) to provide two-way travel to maintain access to the existing properties. Additionally, implementation of turnaround areas west and east of the bridge would be required to prohibit vehicular traffic from entering the bridge. The proposed turnaround areas east and west of the bridge would be approximately 40 feet wide and have a turning radius of 20 feet. The turnaround areas end with a low-profile barrier or similar barrier as used on the renovated bridges at the entrance of the island. A permanent gravity wall would be required for the turnout area west of the bridge. The required roadway widening and turnaround areas would result in permanent ROW impacts to the NRHP-eligible resources, Atlantic Isle Lagoon and Atlantic Island Park. The turnaround area west of the bridge and associated gravity wall may become a focal point for the Atlantic Island Park because these would protrude (both horizontally and vertically) to the edge of the lake, resulting in significant viewshed impacts. Although this alternative avoids the NRHP-eligible Atlantic Island Bridge, the permanent ROW and viewshed impacts to the Atlantic Isle Lagoon and Atlantic Island Park would likely result in an adverse effect to the Atlantic Island Resource Group. The service life of the existing bridge may be extended without vehicular loads, but predicting its longevity is difficult. To determine the remaining service life of the bridge as a pedestrian bridge, materials testing and location of foundations would be required. However, both of these evaluations would likely have an adverse effect to the bridge. Further, the structure and exterior limestone facade would continue to require repairs as the bridge continues to deteriorate. Although this

alternative was previously eliminated from further consideration for reasons unrelated to Section 4(f), it is being reconsidered as part of the Section 4(f) process.

3.1 AVOIDANCE ALTERNATIVES EVALUATED

Initial alternatives considered avoidance of the Section 4(f) resources. Avoidance alternatives were evaluated against the Section 4(f) feasible and prudent avoidance alternative criteria (23 CFR 774.17), as well as the evaluation criteria used to screen all potential alternatives. The following evaluation criteria were used to screen initial alternatives considered and to identify alternatives for detailed study:

- Reasonable expectation of serving traffic needs identified in the project purpose and need
- Degree to which each alternative meets the project purpose and need
- Consideration of future safety and operational problems
- Constructability
- Magnitude of adverse impacts to natural, social, cultural, and physical environmental resources after consideration of reasonable mitigation
- ROW impacts
- Cost feasibility based on construction, maintenance, and operational costs

Because of the constrained ROW near the historic resource group, a limited number of alternatives qualify as avoidance alternatives: No-Action Alternative and an alignment shift to the north alternative. Table 3-1 summarizes the initial screening evaluation criteria results for each avoidance alternative. The following subsections describes these avoidance alternatives in further detail.

Table 3-1. Initial Screening Evaluation Criteria Results for Avoidance Alternatives

Evaluation Criteria	Unit of Measure	No-Action Alternative	Alignment Shift to the North Alternative
Meets the Traffic Needs Identified in the Project Purpose and Need	Yes/No	No	Yes
Meets the Project Purpose and Need	Yes/No	No	Yes
Enhances Safety and Operations	Yes/No	No	Yes
Constructability Issues	High/Medium/Low	N/A	Low
Magnitude of Adverse Impacts to Environmental Resources	High/Medium/Low	None	High
ROW Impacts	High/Medium/Low	None	High
Construction, Maintenance, and Operational Costs	High/Medium/Low	Low (requires continued structural and non-structural maintenance)	High

3.1.1 No-Action Alternative

The No-Action Alternative maintains the existing bridge and roadway approaches in their existing condition and includes no rehabilitation of the existing bridge superstructure or substructure. The No-Action Alternative involves minor maintenance repairs to extend the functional use of the bridge as recommended by routine bridge inspections until future inspections require reduced loading capacity or bridge closure. In the existing condition, the bridge is functionally obsolete. The bridge rating is below a sufficiency rating of 50 and is eligible for replacement per FHWA policy. The bridge displays exposed rebar and multiple instances of cracking, delamination, and spalls, which vary in size and severity on the soffit and sides of the bridge. The exterior oolitic-limestone-covered walls also show cracks up to 1 inch wide. The posted weight restrictions would be maintained in the No-Action Alternative and increased as needed based on future maintenance inspections. In the No-Action Alternative, emergency vehicles, larger delivery and moving vans, and heavy vehicles would continue to be prohibited to cross the bridge. Additionally, overweight vehicles would be required to continue use of flagging staff and special crossover procedures.

A geotechnical investigation performed in March 2021 was initiated to determine the size and type of the existing foundations; however, the investigation was inconclusive, and the bridge was classified as having “unknown foundations” (FDOT 2021). The remaining service life of the bridge is unknown because of the age of the structure (approximately 95 years) and the bridge would continue to deteriorate even with routine maintenance. The design service life of this bridge is unknown. However, modern-day bridges are designed with a 75-year lifespan (FDOT 2024). This bridge has exceeded modern-day requirements and is nearing the end of its service life. Additionally, the aesthetic appearance (oolitic limestone) would continue to deteriorate without continued maintenance. As noted previously, the City of Sunny Isles Beach owns and maintains the bridge. The City noted that the 2023 budgeted maintenance costs for the combined infrastructure (bridge, roadways, drainage, etc.) in the community is approximately \$60,000, and that separate bridge maintenance costs are not specifically quantified.

The No-Action Alternative would not preserve the aesthetic facade or the historic integrity of the bridge, and has the following advantages and disadvantages:

Advantages:

- No construction cost
- No temporary noise or vibration impacts
- No disruption of existing travel patterns

Disadvantages:

- The bridge life has exceeded the modern-day bridge life of 75 years and has reached its limit; there is greater risk to lose a historic resource if no significant rehabilitation is performed.
- Bridge structural components would continue to deteriorate even with routine maintenance and would eventually require closure.
- Heavy vehicles would continue to be restricted with posted weight restrictions.
- Bridge would remain functionally obsolete.
- Aesthetic appearance and historic integrity of limestone (oolitic) facade would continue to deteriorate.
- Continued bridge maintenance would be needed to maintain the structural and non-structural components; maintenance would be expected to increase as the bridge continues to deteriorate.

3.1.2 Alignment Shift to the North

Shifting the alignment to the north of the existing bridge (Biscayne Bay side) would require realigning part of Atlantic Avenue northeast and reconstructing a new bridge over Biscayne Bay. This alternative addresses the structural and functional deficiencies of the existing bridge and provides a safe and functional route for the surrounding community/traveling public, while avoiding use of the Atlantic Island Bridge (8DA6433), Lake of the Isles (8DA15824), and the Atlantic Island Park (8DA15825). However, ROW in the area is limited because of the existing residential properties adjacent to the roadway. Further, this alternative would prohibit all motor vehicle access on the existing bridge and the existing bridge typical section would remain. The structural elements of the existing bridge would continue to require repairs as the bridge continues to deteriorate, although not at the same rate as the No-Action Alternative. A comprehensive restoration of the exterior limestone façade would reduce the need for continued non-structural maintenance but would depend on the City's commitment to maintain the aesthetics of the bridge.

With the realignment of Atlantic Avenue (and bridge) to the north, this alternative would likely impact at least three residential properties (minimum). Two residential properties would likely sustain impacts to their front yards and driveways. The other residential property (located east of the bridge) would likely sustain an impact to the property structure and seawall because of its proximity to the existing bridge and roadway. This property would likely require a full take and result in a relocation.

Additionally, this alternative would require a new bridge over the BBAP, which is an Outstanding Florida Water. The BBAP is also a Marine Protected Area as defined by National Oceanic and Atmospheric Administration and provides habitat for several imperiled species, such as the Florida manatee, smalltooth sawfish, and American crocodile (FDEP 2013). The U.S. Fish and Wildlife Service also designates the BBAP as critical habitat for the West Indian manatee. Additionally, impacts to submerged aquatic vegetation could occur as a result of shading from the new bridge. Therefore, impacts to the BBAP and the associated natural resources would be significant. The existing mangroves located on the northeast side of the bridge would also be impacted. The mangroves provide Essential Fish Habitat and act as natural rip rap for scour and erosion prevention.

ROW costs are expected to be significant because of the impacts to two residential properties and a relocation of another residential property. Further, construction costs for this alternative would also be significant.

The Alignment Shift to the North Alternative has the following advantages and disadvantages:

Advantages:

- Maintains a safe and functional route for the surrounding community/traveling public.
- Historical bridge is retained.
- New rated traffic barriers and bridge typical section improve functionality and safety.
- New arch soffit facilitates easier inspections and maintenance.
- New bridge provides a 75-year design life.
- No continued maintenance of the existing structural and non-structural bridge elements required (typical bridge maintenance of a new bridge expected).
- Posted weight restrictions would be removed.

Disadvantages:

- Significant construction and ROW costs anticipated, when compared to other alternatives considered.

- New bridge and realignment of Atlantic Avenue may aesthetically impact the overall historical character of the island, as they will protrude (both vertically and horizontally) into the BBAP.
- Permanent impacts to at least three residential properties and one potential relocation.
- Significant impacts to the BBAP and associated natural resources anticipated.
- Temporary traffic control includes temporary impacts to the Atlantic Island Park (8DA15825), which is NRHP-eligible as a contributing resource to Atlantic Island Resource Group (8DA19241).
- Aesthetic appearance and historic integrity of limestone (oolitic) facade (of existing bridge) would continue to deteriorate.
- Continued bridge maintenance would be needed to maintain the structural and non-structural components; maintenance would be expected to increase as the bridge continues to deteriorate.

3.2 FEASIBLE AND PRUDENT AVOIDANCE ALTERNATIVE ANALYSIS

A feasible and prudent avoidance alternative is one that avoids using Section 4(f) property and does not cause other severe problems of a magnitude that substantially outweigh the importance of protecting the Section 4(f) property.

The No-Action Alternative avoids the use of the Atlantic Island Bridge (8DA6433), Lake of the Isles (8DA15824), and the Atlantic Island Park (8DA15825) but does not correct the situation that causes the bridge to be considered structurally deficient or significantly deteriorated. These deficiencies may lead to eventual structural failure/collapse. Because of the unknown foundations and the extent of deterioration, normal maintenance is not considered adequate to address the both the structural and functional bridge deficiencies. These deficiencies may lead to safety hazards to the traveling public or place unacceptable restrictions on transport and travel. Further, the No-Action Alternative does not meet the project's purpose and need and, therefore, is not a prudent avoidance alternative, as defined in 23 CFR 774.17.

While shifting the alignment of Atlantic Avenue to the north and constructing a new bridge over the Biscayne Bay avoids use of Section 4(f) resources, it is anticipated to cause significant environmental impacts and would cause major disruption to this established community. This alternative is anticipated to result in ROW and construction costs of "extraordinary magnitude" as compared to other alternatives. For these reasons, this avoidance alternative is not considered prudent, as defined in 23 CFR 774.17.

3.3 ALTERNATIVES THAT REQUIRE USE OF SECTION 4(f) PROPERTIES

3.3.1 Build Alternative #1 - Rehabilitation Alternative

The Rehabilitation Alternative (as presented in Figure 3-1) involves rehabilitation of the existing bridge superstructure, providing a new cast-in-place (CIP) reinforced concrete arch structure, and maintaining one-way travel on the bridge. The roadway width would be maintained, but the typical section and vertical roadway geometry would be impacted to accommodate the retrofitted structure depth. Because of the age, unknown size, and type of the existing bridge foundations, this alternative is anticipated to require the new arch to be supported on new deep foundations. The proposed new arch would extend beyond the ends of the existing concrete arch and foundations to avoid the existing foundation removal costs and the associated risks that could impact the adjacent residential property foundations and structures. A new bridge substructure (abutments and foundations) would be constructed to support the rehabilitated bridge superstructure. During construction, the existing substructure and the superstructure would remain to support the existing concrete arch and exterior limestone facade.

The existing structure—including the architectural facade and bridge structure—could be damaged during the rehabilitation. Additionally, the unknown nature of the existing foundations presents added risk of field changes during construction. As previously noted, the geotechnical investigation in March 2021 was initiated to determine the size and type of the existing foundations; however, the investigation was inconclusive, and the bridge was classified as having “unknown foundations.”

The Rehabilitation Alternative does not address the bridge’s functional deficiencies (substandard traffic barriers) because that would require removal and replacement of the arch spandrel walls, which could compromise the integrity of the already deteriorating bridge. The existing roadway limerock base and pavement would be removed and replaced with a concrete riding surface provided by the new arch structure. With the bridge rehabilitation, one-way travel on the bridge would be maintained. The rehabilitated bridge typical section would remain as is, consisting of a single 10-foot-wide travel lane, 8-inch-wide curbs, 2.5-foot-wide planter easements, and 1-foot, 10-inch-wide barriers on each side of the bridge. The vertical direction of the typical section would be impacted because the roadway profile would be higher at the bridge section to accommodate the additional thickness of the new structural arch.

The Rehabilitation Alternative requires removal of portions of the existing bridge, including the existing overlay and fill material. The demolition work and the construction of the new bridge components pose risks to the existing structure, including damage to the architectural facade, such as cracking, breakage, or loss of the oolitic limestone facade material; cracking or loss of stucco surfacing on the underside of the existing arch; damage to the deteriorating bridge structure; and excessive settlement of the existing foundations supporting the existing bridge during construction. Construction of the Rehabilitation Alternative requires the existing foundations to support the existing arch and facade throughout construction. The unknown nature of the existing foundations may require temporary shoring under the bridge to support the existing arch and facade until the rehabilitation is complete. The need for such temporary shoring also would satisfy the need for falsework to support the wet concrete for the new CIP arch, which would be in close contact with the top of the existing arch.

The proposed arch and foundations would provide a new load-carrying structure that meets design live load requirements in accordance with current FDOT guidelines and would allow the posted bridge loading restrictions to be removed. The proposed arch and new foundations also would support the load of the existing portions of the bridge remaining in place. The new structural arch would connect to the existing arch and facade from above the existing foundations, rendering the existing foundations redundant and eliminating the inherent uncertainty of the unknown load-carrying capacity of the existing foundations. Therefore, future deterioration of the existing foundations would have no adverse impact on the rehabilitated bridge. The design life of the new arch and foundations of the rehabilitated bridge would be 75 years. Construction activities to accomplish the rehabilitation pose risks to the existing bridge, including damage to the architectural facade and potential damage to the structure and substructure. Additionally, it is unknown if the current bridge possesses hidden damages since its construction in 1925.

The longevity of the retained portions of the existing bridge would depend on the commitment of the City to repair and maintain the mostly non-structural oolitic limestone facade and underside stucco of the existing arch. Estimates of the extent of the spall and crack repairs are based on experience and engineering judgement but would require additional field work during final design to accurately quantify. Future maintenance needs of the Rehabilitation Alternative (non-structural oolitic limestone facade and underside stucco of the existing arch) are expected to be less than maintaining all of the existing bridge components. The City noted that maintenance costs for the existing bridge are not specifically quantified but budgeted \$60,000 in 2023 for the entire subdivision. Therefore, future maintenance costs are expected to be lower than \$60,000 for the Rehabilitation Alternative (in 2023 dollars).

The Rehabilitation Alternative requires temporary roadway widening and a turnout along Atlantic Avenue to maintain two-way access during construction. The turnout would be temporary and removed after rehabilitation of the bridge is complete. The temporary roadway turnout is proposed west of the bridge to accommodate temporary traffic control (TTC). The temporary turnout would require temporary walls (either gravity or sheet pile wall-types). All wall options would require excavation of the soil or installation via driving

or vibratory methods near the waterline of the Atlantic Isle Lagoon. The wall is considered temporary and could be removed following completion of the bridge construction work and elimination of the temporary turnaround area.

Figure 3-1 presents the proposed Rehabilitation Alternative Typical Section.

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Figure 3-1. Rehabilitation Alternative Typical Section

The Rehabilitation Alternative has the following advantages and disadvantages:

Advantages:

- Portions of the historical bridge may be retained.
- Service life of new structural arch and foundations of the rehabilitated bridge is 75 years.
- Posted weight restrictions would be removed.

Disadvantages:

- Relatively longer construction time compared to the Replacement Alternative because of the temporary shoring needed during construction to prevent damage to the existing structure as well as time related to potential emergency corrections related to the many existing bridge unknowns.
- Increased risk of damage to the existing bridge historic features during construction.
- Potential for settlement of the existing bridge during construction.
- Does not address functional obsolescence of the existing bridge typical section or traffic barriers.
- Continued maintenance for the non-structural oolitic limestone facade and underside stucco of the existing arch compared to the Replacement Alternative (maintenance of structural components similar to the Replacement Alternative).
- Would likely result in an adverse effect to the NRHP-eligible Atlantic Island Bridge (8DA6433) and the Atlantic Island Resource Group (8DA19241).
- Temporary traffic control includes temporary impacts to the Atlantic Island Park (8DA15825), which is NRHP-eligible as a contributing resource to Atlantic Island Resource Group (8DA19241).

The Rehabilitation Alternative corrects the situation that causes the bridge to be considered structurally deficient or significantly deteriorated but does not correct the situation that causes the bridge to be considered functionally/geometrically deficient. These deficiencies may lead to safety hazards to the traveling public or place unacceptable restrictions on transport and travel.

3.3.2 Build Alternative #2 - Replacement Alternative

The Replacement Alternative involves replacing the entire bridge to address the structural and functional deficiencies of the existing superstructure and substructure to enhance operations and remove load restrictions. This would require demolition of the existing bridge and replacement of the bridge at the same location to minimize overall environmental impacts. The proposed bridge typical section would be approximately 27 feet wide to accommodate one 10-foot-wide travel lane, one 8-foot-wide shared-use path, 3-foot-wide shoulders, and concrete traffic railings on both sides. A raised sidewalk would separate pedestrians from vehicular traffic.

New approach retaining walls would replace the existing retaining walls. A new, non-structural oolitic limestone facade would be placed along the exterior faces of the traffic railings and retaining walls to provide aesthetics similar to the existing bridge. A slightly longer bridge span may be required to span portions of the existing unknown foundations that may not be able to be removed to eliminate potential conflicts and enhance constructability.

Limestone rock fill with roadway pavement would be placed on the new arch structure. New approach retaining walls would replace the existing retaining walls. In addition, a new rubble oolitic limestone facade would be placed along the exterior faces of the vertical shape barriers and retaining walls to mimic the existing structure. The limestone could be obtained from the original source used to construct the original

bridge, or the limestone from the existing bridge could be reused and incorporated into the new bridge. New bridge approach slabs are anticipated and would be the standard length of 20 feet each.

The Replacement Alternative requires temporary roadway widening and a turnout along Atlantic Avenue to maintain two-way access during construction. The turnout would be temporary and removed after rehabilitation of the bridge is complete. The temporary roadway turnout is proposed west of the bridge to accommodate TTC. The temporary turnout would require temporary walls (either gravity or sheet pile wall-types). All wall options would require excavation of the soil or installation via driving or vibratory methods near the waterline of the Lake of the Isles (Atlantic Isle Lagoon). The wall is considered temporary and could be removed following completion of the bridge construction work and elimination of the temporary turnaround areas.

The Replacement Alternative has the following advantages and disadvantages:

Advantages:

- Rated traffic barriers and bridge typical section improve bridge functionality and safety.
- Addition of a shared-use path provides a safe and comfortable experience for pedestrians and bicyclists.
- New arch soffit facilitates easier inspections and maintenance.
- New bridge provides a 75-year design life.
- No continued maintenance of the existing structural and non-structural bridge elements required (typical bridge maintenance of a new bridge expected).
- Posted weight restrictions would be removed.

Disadvantages:

- New bridge arch may have a slightly different profile than the existing bridge.
- Permanent impacts from the removal of the NRHP-eligible Atlantic Island Bridge (8DA6433) and associated Atlantic Island Resource Group (8DA19241) are expected and would result in an adverse effect to the Atlantic Island Bridge (8DA6433) and Atlantic Island Resource Group (8DA19241).
- Minor permanent impacts are expected to the NRHP-eligible Lake of the Isles (8DA15824) and Atlantic Island Park (8DA15825); no adverse effect is expected to these resources.
- Temporary traffic control includes temporary impacts to the Atlantic Island Park (8DA15825), which is NRHP-eligible as a contributing resource to Atlantic Island Resource Group (8DA19241).

The Replacement Alternative corrects the situation that causes the bridge to be considered structurally deficient or significantly deteriorated and corrects the situation that causes the bridge to be considered functionally/geometrically deficient.

Figures 3-2 and 3-3 present the proposed Replacement Alternative Typical Elevation and Typical Section, respectively.

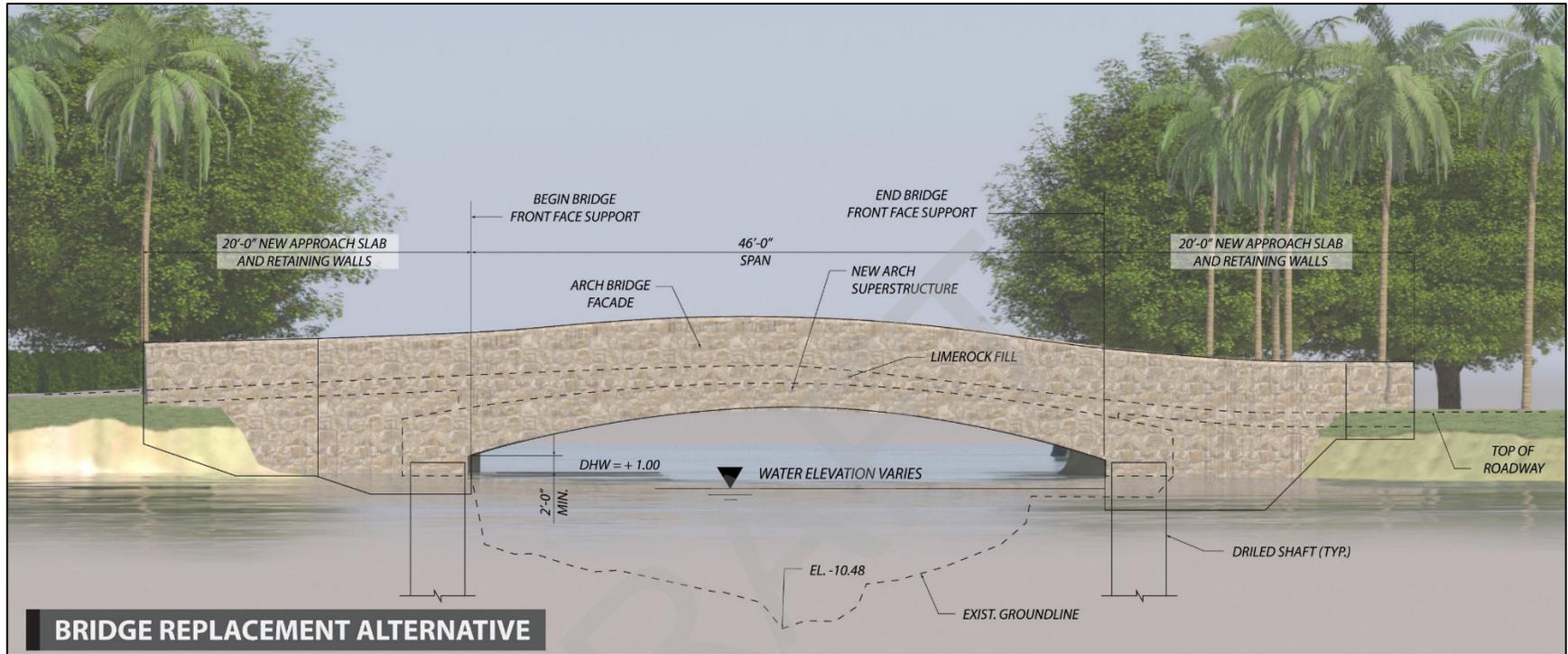


Figure 3-2. Replacement Alternative (Build Alternative #2) Elevation View

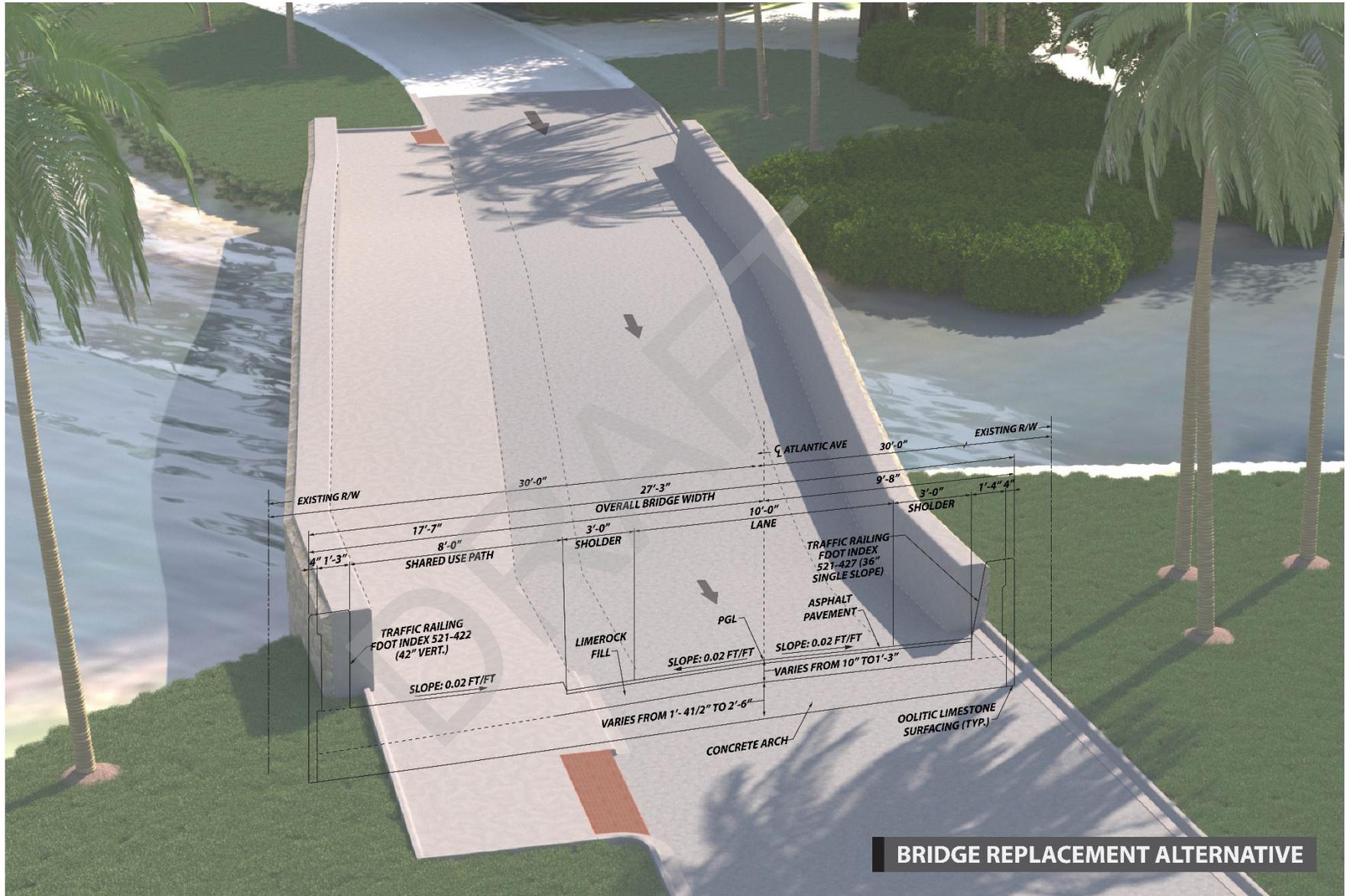


Figure 3-3. Replacement Alternative (Build Alternative #2) Typical Section

3.3.3 Pedestrian Bridge Alternative

A Pedestrian Bridge Alternative was also considered to potentially extend the service life of the bridge. This alternative would maintain the existing bridge structure as a pedestrian bridge and prohibit all motor vehicle access on the bridge. The existing bridge typical section would remain. Based on constructability evaluations, no improvements to the existing bridge typical section or superstructure are feasible without bridge replacement or rehabilitation. Additionally, improvements to the bridge substructure (new piers and foundations) would require bridge rehabilitation outside the limits of the existing substructure.

Because the bridge approaches would be closed in this alternative, Atlantic Avenue would be required to be permanently widened within the existing ROW to two lanes (one lane per direction), as the existing pavement width of 16 feet is not wide enough to maintain two-way travel for access to the existing properties. The FDOT Florida Greenbook criteria, Chapter 3, Section C.7.b.1, require minimum lane widths for local urban roads in residential areas with limited ROW to be 9 feet wide minimum. Further, per the FDOT Florida Greenbook (Chapter 16, Section C.6.a), implementation of turnaround areas west and east of the bridge would be required to prohibit vehicular traffic from entering the bridge and would be approximately 40 feet wide and have a turning radius of 20 feet to accommodate large vehicles (i.e., garbage trucks, moving vans, emergency vehicles, etc.) (FDOT 2018a). The turnarounds end with a low-profile barrier or similar barrier as the one used on the replaced bridges at the entrance of the island. A permanent gravity wall would be required for the turnout area west of the bridge. Although this alternative avoids the use of the Section 4(f) resource, Atlantic Island Bridge (8DA6433), it requires the use of Section 4(f) resources Lake of the Isles (8DA15824) and Atlantic Island Park (8DA15825). The service life of the existing bridge may be extended without vehicular loads, but predicting its longevity is difficult. To determine the remaining service life of the bridge as a pedestrian bridge, materials testing and location of foundations would be required. However, both of these evaluations would likely have an adverse effect to the bridge. Further, the structural elements of the bridge would continue to require repairs as the bridge continues to deteriorate, although not at the same rate as the No-Action Alternative. A comprehensive restoration of the exterior limestone façade would reduce the need for continued non-structural maintenance but would depend on the City's commitment to maintain the aesthetics of the bridge.

The Pedestrian Bridge Alternative has the following advantages and disadvantages:

Advantages:

- Historical bridge is retained.
- Service life of the Pedestrian Bridge is extended, but duration is unknown because of the unknown foundations.
- Posted weight restrictions would be removed.

Disadvantages:

- Continued maintenance needed for the existing structural and non-structural bridge components; maintenance would be expected to increase as the bridge continues to deteriorate (though not at the same rate as the No-Action Alternative).
- Permanent impacts (from turnaround areas) are expected to the NRHP-eligible Atlantic Island Park (8DA15825) and Lake of the Isles (8DA15824) and would likely result in an adverse effect Atlantic to the Island Park (8DA15825) and Atlantic Island Resource Group (8DA19241).

The Pedestrian Bridge Alternative does not address the existing bridge's structural and functional deficiencies but does maintain a functional vehicle route for the surrounding community and traveling public. While this alternative does not impact the NRHP-eligible bridge, direct use of Lake of the Isles (8DA15824) and Atlantic Island Park (8DA15825) would be required and result in approximately 0.11 acres of permanent ROW impacts. Figure 3-4 presents the preliminary concept developed for the Pedestrian Bridge Alternative.



Figure 3-4. Pedestrian Bridge Alternative – Preliminary Concept

4.0 LEAST OVERALL HARM ANALYSIS

A least overall harm analysis was conducted to determine which of the potential feasible and prudent alternatives that use Section 4(f) property, have the “least overall harm” in accordance with 23 CFR § 774.3(c)(1) and “includes all possible planning to minimize harm to Section 4(f) property” or reasonable measures to minimize harm or mitigate adverse effects to the resource as a result of the use.

A comparison of the following factors set forth in 23 CFR 774.3(c)(1) is required when evaluating which of the alternatives may result in the least overall harm:

1. The ability to mitigate adverse impacts to each Section 4(f) property (including any measures that result in benefits to the property)
2. The relative severity of the remaining harm, after mitigation, to the protected activities, attributes, or features that qualify each Section 4(f) property for protection
3. The relative significance of each Section 4(f) property
4. The views of the official(s) with jurisdiction (OWJ) over each Section 4(f) property
5. The degree to which each alternative meets the purpose and need for the project
6. After reasonable mitigation, the magnitude of any adverse impacts to resources not protected by Section 4(f)
7. Substantial differences in costs among the alternatives

The following text provides details on the least overall harm analysis for each of the potential feasible and prudent alternatives.

4.1 ABILITY TO MITIGATE ADVERSE IMPACTS TO EACH SECTION 4(f) PROPERTY

This following text describes the measures used to avoid, minimize, or mitigate adverse impacts to Section 4(f) property.

4.1.1 Build Alternative #1 – Rehabilitation Alternative

The Rehabilitation Alternative was developed to minimize impacts to the Atlantic Island Bridge (8DA6433) and maintain some of the historical elements of the bridge including the limestone facade. Construction activities to accomplish the rehabilitation pose risks to this resource, including potential damage to the architectural facade and the existing structure and substructure. Temporary shoring under the bridge to support the existing arch and facade would mitigate some of the risks until the rehabilitation is complete. However, because of the unknown foundations and potential unknown hidden damages inside the bridge, construction activities could have an adverse effect on the bridge.

The Rehabilitation Alternative also requires temporary use of the Atlantic Island Park (8DA15825) property for TTC but would not impact the Lake of the Isles (8DA15824). Impacts to the Atlantic Island Park (8DA15825) may be mitigated through restoration of the grassy areas associated with the property and avoiding and minimizing impacts to the existing palm trees to the greatest extent possible.

4.1.2 Build Alternative #2 – Replacement Alternative

The Replacement Alternative requires demolishing the Atlantic Island Bridge (8DA6433) resulting in an adverse impact to the resource. Potential mitigation options include incorporating the existing oolitic limestone into the new bridge or sourcing new limestone from the original source used to construct the original bridge. In addition, the new bridge may be designed to mimic the shape of the existing structure.

The Replacement Alternative also requires temporary use of the Atlantic Island Park (8DA15825) property for TTC (similar to the Rehabilitation Alternative). Impacts to this resource may be mitigated through restoration of the grassy areas associated with the property and avoiding and minimizing impacts to the existing palm trees to the greatest extent possible.

Further, impacts to the Atlantic Island Bridge (8DA6433) and the associated impacts to the Atlantic Island Resource Group could be mitigated through documentation of the resources in accordance with the standards and guidelines of the Historic American Landscape Survey (HALS) and Historic American Engineering Record (HAER), as well as a use of a State Historic Marker (hereinafter, “the Marker”) to educate future generations.

4.1.3 Pedestrian Bridge Alternative

The Pedestrian Bridge Alternative avoids the use of the Atlantic Island Bridge (8DA6433) but requires permanent widening of Atlantic Avenue as well as turnaround areas west and east of the bridge. Therefore, this alternative results in permanent alterations and impacts to the Lake of the Isles (8DA15824) and Atlantic Island Park (8DA15825) and ultimately to the Atlantic Island Resource Group. Mitigation of impacts to these Section 4(f) properties may be accomplished by restoring the landscaping to the greatest extent possible.

4.2 RELATIVE SEVERITY OF REMAINING HARM AFTER MITIGATION

The relative severity of remaining harm after mitigation includes the remaining use of Section 4(f) resources for each alternative. Table 4-1 summarizes the remaining use (by acreage) of Section 4(f) resources for each alternative, while Table 4-2 summarizes the determination of effect on Section 4(f) resources by alternative.

Table 4-1. Remaining Use of Section 4(f) Resources by Alternative

Alternative	Acres of Use (Acres Remaining after Use) by Section 4(f) Resource				
	Atlantic Island Bridge (8DA6433)	Lake of the Isles (8DA15824)	Atlantic Island Park (8DA15825)	Atlantic Island Park (8DA15825) (Temporary Use)	Atlantic Island Resource Group (8DA19241)
Total Resource Area (in acres)	0.047	0.782	1.053	1.053	1.882
Build Alternative #1 (Rehabilitation)	0.026 (0.021)	0.000 (0.782)	0.000 (1.053)	0.032 (1.021)	0.058 (1.824)
Build Alternative #2 (Replacement)	0.047 (0.000)	0.011 (0.771)	0.009 (1.044)	0.032 (1.021)	0.099 (1.783)
Pedestrian Bridge Alternative	0.000 (0.047)	0.002 (0.78)	0.103 (0.95)	0.000 (1.053)	0.105 (1.777)

Table 4-2. Determination of Effect to Section 4(f) Resources by Alternative

Alternative	Atlantic Island Bridge (8DA6433)	Lake of the Isles (8DA15824)	Atlantic Island Park (8DA15825)	Atlantic Island Resource Group (8DA19241)
Build Alternative #1 (Rehabilitation)	Adverse Effect	No Adverse Effect	No Adverse Effect	Adverse Effect
Build Alternative #2 (Replacement)	Adverse Effect	No Adverse Effect	No Adverse Effect	Adverse Effect
Pedestrian Bridge Alternative	No Adverse Effect	No Adverse Effect	Adverse Effect	Adverse Effect

4.2.1 Build Alternative #1 – Rehabilitation Alternative

The Rehabilitation Alternative would result in direct use of the Atlantic Island Bridge (8DA6433). While this alternative retains the bridge facade, it provides a new permanent concrete arch structure that alters the original bridge structure. Because the bridge is a contributing resource to the Atlantic Island Resource Group (8DA19241), this alternative also results in direct use of this resource group. This alternative also requires temporary use of the Atlantic Island Park (8DA15825) for TTC during construction. Figure 4-1 presents the anticipated use of contributing resources resulting from the Rehabilitation Alternative.

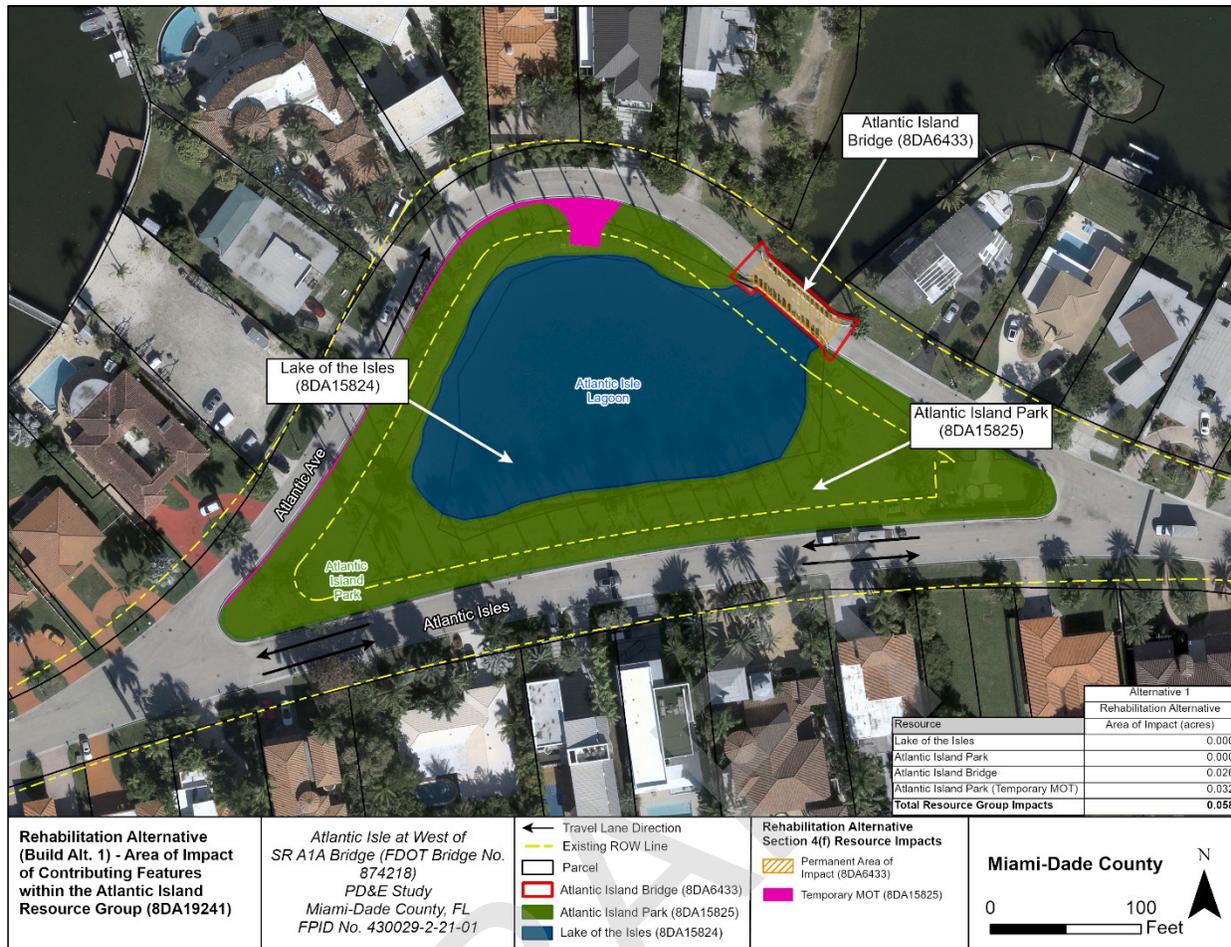


Figure 4-1. Rehabilitation Alternative (Build Alternative #1) – Use of Section 4(f) Resources

4.2.2 Build Alternative #2 – Replacement Alternative

The Replacement Alternative would result in direct use of the Atlantic Island Bridge (8DA6433), as the bridge would be demolished and replaced with a new bridge. Because the bridge is a contributing resource to the Atlantic Island Resource Group (8DA19241), this alternative also results in direct use of this resource group. This alternative also requires direct use of the Lake of the Isles (8DA15824) and Atlantic Island Park (8DA15825), albeit very small: 0.01 acre of the Lake of the Isles (8DA15824) and 0.01 acre of the Atlantic Island Park (8DA15825). Temporary use of the Atlantic Island Park (8DA15825) for TTC would also be required during construction. Figure 4-2 shows the anticipated use of contributing resources resulting from the Replacement Alternative.

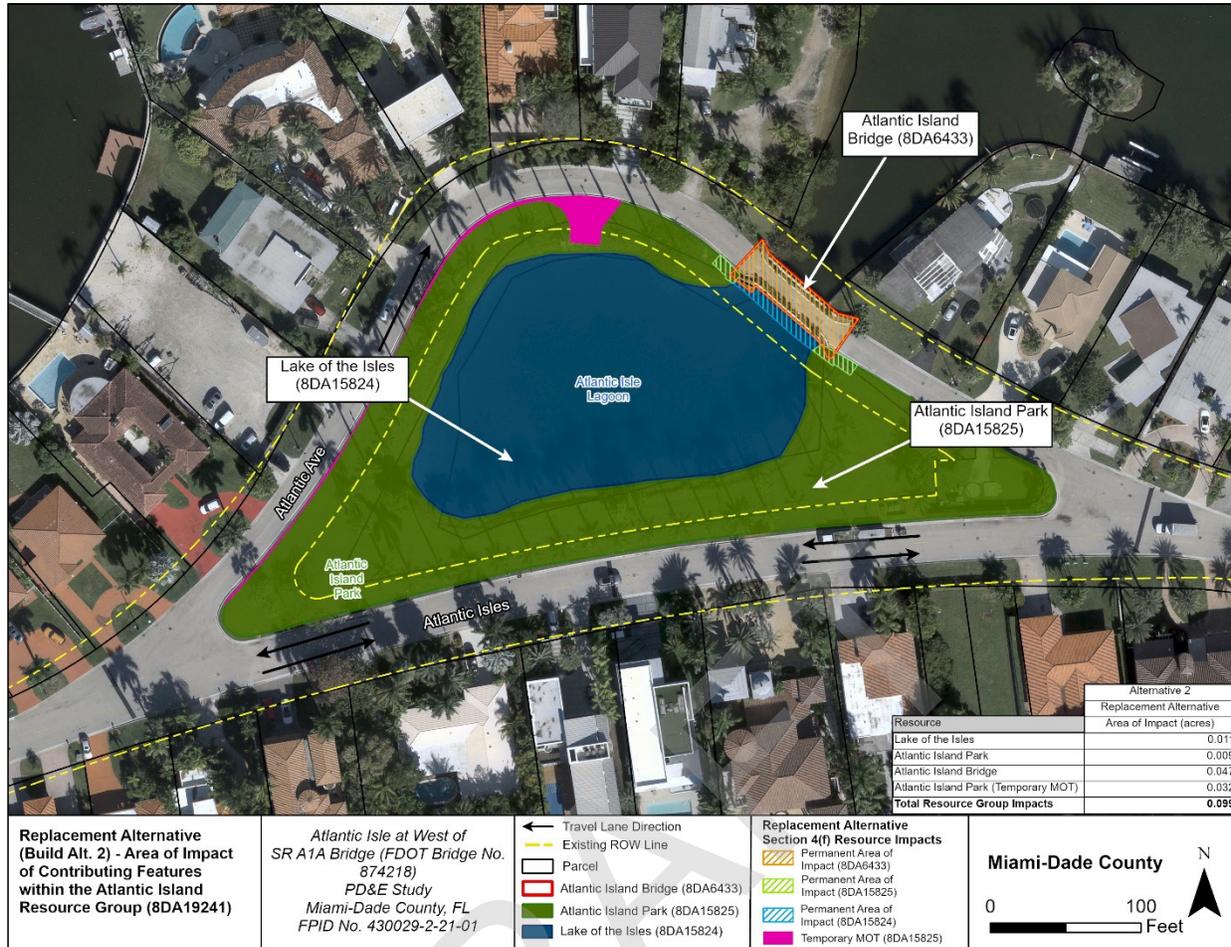


Figure 4-2 Replacement Alternative (Build Alternative #2) – Use of Section 4(f) Resources

4.2.3 Pedestrian Bridge Alternative

The Pedestrian Bridge Alternative would result in direct use of the Lake of the Isles (8DA15824) and Atlantic Island Park (8DA15825), resulting from widening Atlantic Avenue and the addition of permanent turnaround areas east and west of the bridge. Because Lake of the Isles (8DA15824) and Atlantic Island Park (8DA15825) are contributing resources to the Atlantic Island Resource Group (8DA19241), this alternative also results in direct use of this resource group. This alternative avoids the use of Atlantic Island Bridge (8DA6433). Figure 4-3 presents the anticipated use of contributing resources resulting from Pedestrian Bridge Alternative.



Figure 4-3. Pedestrian Bridge Alternative – Use of Section 4(f) Resources

4.3 VIEWS OF THE OFFICIAL WITH JURISDICTION

The OWJ for this project include the SHPO and the City of Sunny Isles Beach as they own and maintain Atlantic Avenue, the bridge, and a portion of the park. Multiple meetings were held with the City for input on the concepts as well as the effects to the Section 4(f) resources (reference Appendix B). Because the City maintains the roadway, bridge, and portions of the park, their preference is the Replacement Alternative as this would greatly reduce their maintenance costs. The City engaged their Historic Preservation Board (HPB), who generally agreed the bridge should be replaced but requested that the new bridge mimic the old bridge as much as possible to maintain the look and character of the community.

The SHPO did not provide many comments or opinions during consultation on the alternatives. As noted previously, they concurred with the *Final Section 106 Determination of Effects Case Study Report*, which presented the Replacement Alternative as the Preferred Alternative (FDOT 2023). The signed concurrence provided by SHPO is provided in Appendix A.

4.4 DEGREE TO WHICH EACH ALTERNATIVE MEETS PROJECT PURPOSE AND NEED

Table 4-3 compares how each of the alternatives meets the project’s purpose and need. The Replacement Alternative is the only alternative that meets the project’s purpose and need.

Table 4-3. Purpose and Need Comparison by Alternative

Evaluation Criteria	Build Alternative #1 Rehabilitation	Build Alternative #2 Replacement	Pedestrian Bridge Alternative
Structural Deficiencies			
Replaces Existing Foundation	Yes	Yes	No
Replaces Superstructure and Substructure	Yes	Yes	No
Removes Weight Limit Restrictions	Yes	Yes	No
Bridge Life	75 years (remaining bridge elements 15–25 years)	75 years	Unknown
Functional Deficiencies			
Addresses Functional Deficiencies	No	Yes	No
Maintains Functional Route			
Maintains Functional Route for Traveling Public	Yes	Yes	Yes
Provides Enhanced Operations and Safety	No	Yes	No
Meets Project Purpose and Need			
Does This Alternative Satisfy the Project Purpose and Need	No	Yes	No

The following sections further describe how each alternative meets the purpose and need of the project.

4.4.1 Build Alternative #1 – Rehabilitation Alternative

The Rehabilitation Alternative addresses the existing bridge's structural deficiencies and would provide a safe route for the surrounding community/traveling public as it removes load restrictions, while also providing a 75-year design life. The Rehabilitation Alternative does not address the bridge's functional deficiencies (substandard traffic barriers) because that would require removal and replacement of the arch spandrel walls, which could compromise the integrity of the already deteriorating bridge. This alternative attempts to retain the historical elements of the bridge but includes increased risk of damaging the architectural facade and bridge structure, increased project costs associated with the emergency corrective repairs and other unknowns likely to be identified only during construction, and continued maintenance associated with repairing the mostly non-structural oolitic limestone facade and underside stucco of the existing arch. As Build Alternative #1 does not address the bridge's functional deficiencies, it does not satisfy the project's Purpose and Need.

4.4.2 Build Alternative #2 – Replacement Alternative

The Replacement Alternative meets the project's purpose and need, as it addresses the existing bridge's structural and functional deficiencies and would provide a safe and functional route for the surrounding community/traveling public. It also removes the load restrictions, while also providing a 75-year design life. This alternative also includes the addition of a shared-use path that would provide a safe and comfortable experience for pedestrians and bicyclists.

4.4.3 Pedestrian Bridge Alternative

Although the Pedestrian Bridge Alternative maintains a safe and functional route for the surrounding community/traveling public, it does not meet the project's purpose and need of addressing the existing bridge's structural and functional deficiencies. While the service life of the existing bridge may be extended without vehicular loads, it does not meet current pedestrian bridge design criteria and would involve continued repairs and inspections. Further, because of the unknown foundations, the service life as a pedestrian bridge is also unknown and may require future closures to protect public safety.

4.5 IMPACTS AND BENEFITS TO NON-SECTION 4(f) RESOURCES

The least overall harm analysis also includes consideration of impacts and benefits to resources not protected by Section 4(f), such as natural, physical, and sociocultural resources. Table 4-4 summarizes the anticipated impacts to non-Section 4(f) resources for each alternative.

Table 4-4. Impacts to Non-Section 4(f) Resources by Alternative

Non-Section 4(f) Resources	Build Alternative #1 Rehabilitation	Build Alternative #2 Replacement	Pedestrian Bridge Alternative
Potential ROW Impacts (Temporary) (acres)	0.03	0.03	0.00
Potential ROW Impacts (Permanent) (acres)	0.00	0.00	0.04
Community Use: No. of Parcels Impacted (Temporary)	1	1	0
Community Use: No. of Parcels Impacted (Permanent)	0	0	1
Residential Parcels Impacted	0	0	0
Aesthetic Effects (viewshed)	Low	Medium	High
Potential Wetland Impacts (acres)	0.1	0.1	0.1
Potential Surface Water Impacts (acres)	0.1	0.1	0.1
Increased Shading Impacts	No	Yes	No
Potential Species Habitat Impacts (acres)	0.11	0.11	0.1

Among the three alternatives that require use of Section 4(f) resources, none are not expected to impact residential parcels. These three alternatives are also expected to have nearly identical impacts to the wetlands (0.1 acres), other surface waters (0.1 acres), and protected species habitat (0.1 acres).

The following subsections describe the impacts and benefits that differ among the three alternatives.

4.5.1 Build Alternative #1 – Rehabilitation Alternative

The Rehabilitation Alternative maintains a safe vehicle route for the surrounding community and traveling public. It also would allow the posted bridge loading restrictions to be removed, making it more convenient for delivery and moving trucks to reach the residential properties as well as garbage trucks and emergency vehicles. This alternative also maintains the aesthetics of the area by preserving the bridge, lake, and park as they are today.

The Rehabilitation Alternative is expected to have minimal impacts to natural resources. The Preferred Alternative (Replacement Alternative) was documented in the Natural Resources Evaluation (NRE) for this study, and effects from the Rehabilitation Alternative are anticipated to be similar to the Replacement Alternative. The Replacement Alternative is anticipated to have an effect determination of “May Affect, Not Likely to Adversely Affect” for the West Indian manatee, eastern indigo snake, smalltooth sawfish, giant manta ray, hawksbill sea turtle, loggerhead sea turtle, Kemp’s ridley sea turtle, and green sea turtle. The remainder of the species are determined to have “No Effect”, “No effect anticipated”, or “No adverse effect anticipated.”

Similar to the Replacement Alternative, impacts would also include construction of temporary materials within the lagoon, while indirect impacts such as sedimentation should be minimized using turbidity barriers and using minimal impact construction methods. Because in-water work is expected to be minimal, impacts to wetlands are also anticipated to be minimal. Mangroves are not anticipated to be impacted. Because there are permanent, indirect impacts to seagrass Essential Fish Habitat (EFH) and potential seagrass habitat, and temporary, direct impacts to potential seagrass habitat, algal communities, and oysters, adverse impacts to EFH are anticipated to be Moderate. Any impacts to EFH and HAPCs will be mitigated as appropriate. Last, proper drainage to protect the canal (and associated habitat) will be included in the project plans.

4.5.2 Build Alternative #2 – Replacement Alternative

The Replacement Alternative maintains both a safe and functional vehicle route for the surrounding community and traveling public. It would also allow the posted bridge loading restrictions to be removed making it more convenient for services to reach the residential properties. Additionally, this alternative includes a shared-use path that would provide a safe and comfortable experience for pedestrians and bicyclists while on the bridge.

This alternative would have minor effects to the aesthetics of the area, as the new bridge would not look exactly like the old bridge but would preserve the lake and park as they are today. As documented in the NRE for this study, the use of U.S. Fish and Wildlife Service’s most current guidance for standard protection measures during construction reduces the effect determination for this alternative to “May Affect, Not Likely to Adversely Affect” for the West Indian manatee, eastern indigo snake, smalltooth sawfish, giant manta ray, hawksbill sea turtle, loggerhead sea turtle, Kemp’s ridley sea turtle, and green sea turtle. The remainder of the species are determined to have “No Effect”, “No effect anticipated”, or “No adverse effect anticipated” from the project. Additionally, the Replacement Alternative would have increased shading on the Outstanding Florida Water because of the new bridge’s shared-use path.

4.5.3 Pedestrian Bridge Alternative

The Pedestrian Bridge Alternative also provides a safe and functional vehicle route for the surrounding community and traveling public, but the bridge closure requires turnarounds that make it less convenient for services to reach the residential properties. The turnarounds east and west of the bridge would be a substantial change for the community as they are required to accommodate large vehicles and use approximately 10% of the park area. Additionally, the permanent gravity wall needed for the turnaround west of the bridge would protrude (both horizontally and vertically) to the edge of the lake and become a focal point for the area. Therefore, significant impacts are expected to the area viewshed and community character and may detract from the community’s existing focal points.

The impacts to natural resources, such as wetlands, federally listed and/or state listed species, EFHs, and HAPCs, are anticipated to be minimal. Permanent impacts from the permanent turnaround areas would result in higher ROW needs than both the Replacement and Rehabilitation Alternatives.

4.6 COST COMPARISON

Table 4-5 summarizes total construction costs for each alternative that require use of Section 4(f) resources. Among the alternatives considered for this Section 4(f) evaluation, the Rehabilitation Alternative

has the highest estimated construction cost while the Pedestrian Bridge Alternative has the lowest estimated construction cost. The Rehabilitation Alternative includes increased costs associated with the emergency corrective repairs and other unknowns likely to be identified only during construction. The TTC costs are included in the cost estimates. TTC costs for the Rehabilitation Alternative would be higher (approximately \$50,000 more) as compared to the Replacement Alternative because of the increased construction time and costs.

Both the Rehabilitation Alternative and Pedestrian Bridge Alternative would require continued maintenance. Maintenance is not included in the construction cost estimates and would be the responsibility of the City. Maintenance for these two alternatives includes the following when compared to the Replacement Alternative:

- Rehabilitation Alternative – continued maintenance would be needed for the mostly non-structural oolitic limestone facade and underside stucco of the existing arch (maintenance of the structural components would be similar to the Replacement Alternative).
- Pedestrian Alternative – continued maintenance would be needed for the structural and non-structural components of the bridge maintenance (though not at the same rate as the No-Action Alternative).

Table 4-5. Total Construction Cost Comparison by Alternative

Alternative	Total Estimated Costs (in 2023 dollars)	Requires Continued Maintenance
Build Alternative #1 (Rehabilitation)	\$1,656,371	Yes
Build Alternative #2 (Replacement)	\$1,504,295	No
Pedestrian Bridge Alternative	\$631,569	Yes

4.7 SUMMARY OF LEAST OVERALL HARM ANALYSIS

Based on the least overall harm analysis, the feasible and prudent alternative that provides the least overall harm is the Build Alternative #2 – Replacement Alternative. All alternatives evaluated would result in use of the Atlantic Island Resource Group (8DA19241). While the Replacement Alternative requires direct use of the entire Atlantic Island Bridge (8DA6433), it requires minimal use of the Lake of the Isles (8DA15824) and Atlantic Island Park (8DA15825). Temporary impacts to the Atlantic Island Park (8DA15825) could be mitigated through restoration of the grassy areas and avoiding and minimizing impacts to the existing palm trees. Impacts to the Atlantic Island Bridge (8DA6433) and the associated impacts to the Atlantic Island Resource Group (8DA19241) could be mitigated through documentation of the resources in accordance with the standards and guidelines of the HALS and HAER, as well as a use of a Marker. The Replacement Alternative also is favored by the OWJ (i.e., City of Sunny Isles Beach).

The Replacement Alternative meets the project's purpose and need by correcting the situation that causes the bridge to be structurally and functionally deficient and provides a safe and functional route for the surrounding community/traveling public. It also removes the load restrictions, making it more convenient for service and emergency vehicles to reach the residential properties. Impacts to non-Section 4(f) resources would be minimal. Further, the Replacement Alternative has the second highest costs but will not require regular maintenance. As such, it is anticipated to have the lowest long-term cost of the alternatives evaluated.

5.0 PUBLIC INVOLVEMENT

5.1.1 Alternatives Public Workshop

On Thursday, June 23, 2022, an Alternatives Public Workshop was held. A total of 17 people attended the meeting: 10 in person and 7 online. The Alternatives Public Workshop was held to show existing bridge deficiencies, existing roadway and bridge typical sections on Atlantic Avenue, initial alternatives considered, No-Action Alternative, and graphical representations of the proposed typical section, elevation view, and plan view for each of the Build Alternatives. A video of a rendering of Build Alternative 2 was also presented to give attendees an idea of what this alternative would look like in the community. Temporary traffic control impact considerations, and alternative characteristics and impacts evaluation matrixes were also discussed as well as the natural resources, ROW considerations, physical environment, agency coordination, the cultural resources in the study area, and the alternative impact evaluation matrix. Appendix B includes the meeting summary and list of attendees. A summary of public comments received during the Alternatives Public Workshop, as they relate to Section 4(f), are summarized as follows:

- A participant stated that the wooden rafters under the bridge are in poor condition and was concerned about heavy vehicles using the bridge.
- A participant asked if the existing bridge can be converted to a pedestrian bridge, and a new bridge be constructed adjacent to the existing bridge for vehicles.

5.1.2 Public Hearing

A Public Hearing is tentatively scheduled for March 2024.

6.0 COORDINATION WITH OFFICIAL(S) WITH JURISDICTION AND OTHER CONSULTING PARTIES

Throughout this project, consultation and coordination has taken place with FDOT District Six, FDOT OEM, the City of Sunny Isles Beach, the Florida SHPO, Miami-Dade County, and local residents. In addition, several Affected Parties Consultation meetings have occurred throughout this study as part of the Section 106 process. Table 6-1 summarizes coordination with OWJ and affected parties throughout the project.

Table 6-1. Summary of Coordination with OWJ and Other Consulting Parties

Meeting Date	No. of Attendees	Itinerary
October 27, 2020	35	SHPO/City of Sunny Isles Beach/Sunny Isles Beach Historic Preservation Board/Miami-Dade County – To provide a project overview and gain input on potential environmental issues and alternatives.
February 9, 2022	6	City of Sunny Isles Beach – To provide a project overview and discuss potential alternatives and refinements.
June 13, 2022	25	SHPO/City of Sunny Isles Beach/Sunny Isles Beach Historic Preservation Board/ Miami-Dade County – Affected Parties Consultation Meeting, as part of the Section 106 process, to provide a project overview and to discuss the cultural resources and associated process.
October 11, 2022	21	City of Sunny Isles Beach/Sunny Isles Beach Historic Preservation Board – To discuss the preliminary concepts of the project and potential mitigation measures for Section 4(f) resources as part of the Section 106 process.
May 11, 2023	20	City of Sunny Isles Beach/Sunny Isles Beach Historic Preservation Board - To discuss impacts resulting from each potential alternative and potential mitigation measures for Section 4(f) resources as part of the Section 106 process.
July 21, 2023	22	SHPO/City of Sunny Isles Beach/Sunny Isles Beach Historic Preservation Board/ Miami-Dade County – Affected Parties Consultation Meeting, as part of the Section 106 process, to discuss impacts resulting from each potential alternative and potential mitigation measures for Section 4(f) resources.

An agency kick-off meeting with elected officials and agency officials was held on October 27, 2020. Attendees at the meeting included FDOT District Six, Florida SHPO staff, Miami-Dade County Historic Preservation Program staff, and the City of Sunny Isles Beach Planning and Zoning Director who is also on the City of Sunny Isles HPB.

On November 19, 2020, the staff of the Miami-Dade Historic Preservation Program were contacted for any information regarding cultural resources, as a component of the development of the CRAS. The information shared by the County staff the following day was integrated into the CRAS report.

On February 9, 2022, a meeting with the City of Sunny Isles Beach staff was held to discuss potential alternatives and TTC refinements to reduce environmental impacts.

On June 13, 2022, an Affected Parties Consultation meeting was held to discuss potential project alternatives. Attendees at the meeting included residents from the City of Sunny Beach HPB, staff from the City of Sunny Isles Beach, staff from the Florida SHPO, staff from the Miami-Dade County Historic Preservation Program, the City of Sunny Isles Beach HPB, staff from the City of Sunny Isles, residents of Sunny Isles Beach, staff from the FDOT OEM, FDOT District Six staff, Jacobs, and Janus Research. Ms. Elizabeth Morales (a member of the HPB) asked if the planters and limestone facing would be included in the design of a replacement bridge. Mr. Nicholas Danu (FDOT Project Manager) explained that the new design would integrate many of the aesthetics of the original bridge. Ms. Morales also asked about the design of the shared-use path and the shading issues. Ms. Castro (Stantec) explained that the increased shading would impact fish habitat and would need further study to determine whether that would impact the viability of the alternative. Ms. Ivette Francolla (a member of the HPB) asked if the bridge could be rehabilitated as a pedestrian bridge. Mr. Danu said that it could not because that would not meet the purpose and need of the project. Ms. Susan Simpson (City of Sunny Isles Beach) asked what the age of the planters were. Mr. Danu responded that they were 10-15 years old. Ms. Francolla and Mr. Norman Edelcup (a member of the HPB) discussed whether the bridge would still be historic if it was replaced. Ms. Simpson inquired about construction schedule and funding. Mr. Danu explained the process. Ms. Francolla asked FDOT if they were not interested in maintaining the original structure and continuing using it for vehicular traffic. Mr. Danu responded in the affirmative that the Rehabilitation Alternative would keep the bridge usage for vehicular traffic. Comments made at this meeting were integrated into the design of the replacement bridge. Appendix B includes the meeting summary.

A presentation to the City of Sunny Isles Beach HPB was held on October 11, 2022. Staff members of the FDOT District Six, Jacobs, Stantec, and Janus Research presented the project to the members of the HPB and answered questions regarding the design of the project and the resources identified in the project APE. During the meeting, the HPB expressed concern about the safety of the sidewalk on the Replacement Alternative. They would prefer a barrier (wall or railing) along the path to protect the pedestrians from the vehicular traffic. The FDOT explained that the design was developed with the City and that due to the speed limit of the roadway, barriers were not required. The HPB requested that a potential mitigation might be sharing the research conducted on the Atlantic Island Resource Group with the HPB so that local designation or National Register-listing could be pursued. The HPB discussed if the existing bridge could be utilized as a pedestrian bridge and any impacts that may have on the traffic. The FDOT also reviewed the temporary construction impacts and TTC during construction. The HPB was concerned about the impact on a tree for the TTC during construction. FDOT replied that they would study how to minimize the impacts. The HPB reported that they would develop further comments and report back to FDOT. Appendix B includes the meeting minutes.

On May 11, 2023, a presentation to the City of Sunny Isles Beach HPB was held. Staff members from FDOT District Six, Jacobs, Stantec, and Janus Research presented the project to the members of the HPB and answered questions regarding the design of the project and potential mitigation measures to Section 4(f) resources. Appendix B includes the meeting minutes.

A second Affected Parties Consultation meeting was held on Friday, July 21, 2023, via Microsoft Teams to discuss adverse effects to the significant resources and the potential mitigation measures. There was a total of 22 attendees, and they were given the opportunity to ask questions. Comments that were made during the question-and-answer session include questions as to whether there is an education component included in the mitigation measures, as well as if there had been communication with the City regarding its preferences. Appendix B includes the meeting minutes.

7.0 SECTION 106 CONSULTATION

A Section 106 Determination of Effects Case Study Report was prepared to document the potential effects of the Replacement Alternative to the four significant historic properties within the project APE. The Case Study concluded that the Replacement Alternative would result in an adverse effect to the Atlantic Island Bridge (8DA6433). Because the Replacement Alternative would result in the removal of a contributing resource to the Atlantic Island Resource Group (8DA19241), the resource group would also be adversely affected. Based on the Replacement Alternative of the proposed project, there would be no adverse effect to the remaining two significant properties: the Lake of the Isles (8DA15824) and Atlantic Island Park (8DA15825). On May 12, 2023 (Appendix A), the SHPO concurred with the determinations of the 2023 Section 106 Determination of Effects Case Study Report.

Following the assessment of effects and additional consultation, a *Draft Memorandum of Agreement* between FDOT and the SHPO, was developed to document the measures to minimize and mitigate adverse effects to the Atlantic Island Bridge (8DA6433) and the Atlantic Island Resource Group (8DA19241) as part of the Section 106 process. As documented in the *Draft Memorandum of Agreement*, potential mitigation measures include bridge design considerations, documentation of the Atlantic Island Resource Group (8DA19241) and Atlantic Island Bridge (8DA6433), as well as public education. The following sections describe in detail the potential mitigation measures proposed during the Section 106 consultation process. Appendix C includes the *Draft Memorandum of Agreement*.

7.1 BRIDGE DESIGN

FDOT will take into consideration the historic materials, visual profile, and design elements of the historic Atlantic Island Bridge when designing the replacement bridge. FDOT will also provide the City of Sunny Isles and the SHPO opportunity to comment on the 60% and 90% design plans. All comments received will be considered during development of the replacement bridge design.

7.2 HISTORIC RESOURCE DOCUMENTATION

Prior to bridge demolition and per guidance provided by the National Park Service (NPS), FDOT will ensure that the resources are documented in accordance with the standards and guidelines of the HALS and HAER. For organizational purposes, all documentation will be grouped under HALS, but some components (as noted below) will follow HAER guidelines. The HALS documentation for the Atlantic Island Resource Group (8DA9241) and Atlantic Island Bridge (8DA6433) will be prepared as follows:

1. Written historical and descriptive data prepared in accordance with outline format guidelines placing the Atlantic Island Resource Group in the local and regional historical context and covering significant landscape design elements and features. The data will also contain significant construction history of the Atlantic Island Bridge, including the history of the bridge type, an architectural description of the resource including alterations, and a description of the site and changes; and
2. A site plan; and
3. Reproduction of selected historical photographs, if available; and
4. Large-format (4" x 5" or larger negative size) photographs processed for archival permanence in accordance with HALS photographic specifications. Views will include:
 - a. Significant landscape design elements and features.
 - b. Views and vistas.
 - c. Circulation patterns.
 - d. Spatial organization.

5. At least one color digital photograph of the overall landscape design and select views in color to be included as figures at the end of the report, as well as at least one color digital photograph of the Atlantic Island Bridge resource and its setting; and
6. Photo locations keyed to the site plan and included with the "Index to Photographs."

HAER documentation for the Atlantic Island Bridge (8DA6433) will include, but is not limited to:

1. Written historical and descriptive data prepared in accordance with outline format guidelines containing a construction history of the bridge including the history of the bridge type, an architectural description of the resource including alterations, and a description of the site and changes; and
2. Reproduction of selected historical photographs, if available; and
3. Large-format (4" x 5" or larger negative size) photographs processed for archival permanence in accordance with HAER photographic specifications; and
4. At least one color digital photograph of the resource and its setting; and
5. Photo locations keyed to the site plan and included with the "Index to Photographs."

FDOT shall provide draft HALS documentation (non-archival format, electronic version) to the NPS and SHPO for concurrent review. Unless otherwise agreed to by the NPS or the SHPO, FDOT will ensure that all large-scale photographs of the resources are completed and accepted in writing by the NPS and SHPO prior to demolition.

7.3 PUBLIC EDUCATION

FDOT will assist with the development and funding of one State Historical Marker to be placed in proximity to the bridge location. The Marker will be located in an area that is safe and accessible to the public. The draft Marker text and location will be coordinated with the SHPO for review.

8.0 CONCLUSION

Once OEM approves this Individual Section 4(f) Evaluation for public availability, the District will circulate the document to the OWJ and the U.S. Department of the Interior (DOI). The OWJ and DOI will have at least 45 days to review and comment on the document. The document will be presented to the public at the Public Hearing, and comments on the project alternatives and the use Section 4(f) resources will be solicited.

After completion of the circulation and public comment period, the District will submit the Final Individual Section 4(f) Evaluation to OEM for review and comment, and to the FDOT Office of General Counsel (OGC) for legal sufficiency review. OGC must certify that the evaluation is legally sufficient before the Section 4(f) Evaluation can be approved by the Director of OEM as part of the NEPA document. Once approved, the District will distribute copies of the signed document to the OWJ and DOI.

Upon final alternative selection, the provision of Section 4(f) and 36 CFR Part 800 will be fully satisfied.

DRAFT

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APPENDIX A
CORRESPONDENCE WITH OFFICIALS
WITH JURISDICTION

DRAFT



Florida Department of Transportation

RON DESANTIS
GOVERNOR

1000 NW 111th Avenue
Miami, FL 33172-5800

KEVIN J. THIBAUT, P.E.
SECRETARY

January 27, 2022

Timothy A. Parsons, Ph.D.
Director, Division of Historical Resources, and
State Historic Preservation Officer
R.A. Gray Building
500 S. Bronough Street
Tallahassee FL 32399-0250

Attn: Ms. Marsha Welch, Transportation Compliance Review Program

Re: Cultural Resource Assessment Survey for the Atlantic Isle at West Bridge (FDOT Bridge No. 874218) Project Development and Environment (PD&E) Study in the city of Sunny Isles Beach, Miami-Dade County, Florida (Financial Project ID [FPID] No. 430029-2-21-01)

Dear Dr. Parsons,

At the request of the Florida Department of Transportation (FDOT) District 6, Janus Research conducted the Cultural Resource Assessment Survey (CRAS) for the Atlantic Isle at West Bridge (FDOT Bridge No. 874218) Project Development and Environment (PD&E) Study in the city of Sunny Isles Beach, Miami-Dade County, Florida (Financial Project ID [FPID] No. 430029-2-21-01). The purpose of this CRAS was to locate and evaluate archaeological and historic resources within the area of potential effect (APE) and to assess their eligibility for inclusion in the National Register of Historic Places (National Register) according to the criteria set forth in 36 CFR Section 60.4. The current survey is being conducted for the PD&E Study to address a permanent solution for the Atlantic Isle Bridge (FDOT Bridge No. 874218), also known as the Atlantic Island Bridge (Florida Master Site File [FMSF] No. 8DA6433).

This assessment complies with Section 106 of the National Historic Preservation Act (NHPA) of 1966 (Public Law 89-665, as amended), as implemented by 36 CFR 800 -- Protection of Historic Properties (incorporating amendments effective August 5, 2004); Stipulation VII of the Programmatic Agreement among the Federal Highway Administration (FHWA), the Advisory Council on Historic Preservation (ACHP), the Florida Division of Historical Resources (FDHR), the State Historic Preservation Officer (SHPO), and the FDOT Regarding Implementation of the Federal-Aid Highway Program in Florida (Section 106 Programmatic Agreement, effective March 2016, amended June 7, 2017); Section 102 of the National Environmental Policy Act (NEPA) of 1969, as

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Timothy A. Parsons, Ph.D.

January 27, 2022

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amended (42 USC 4321 et seq.), as implemented by the regulations of the Council on Environmental Quality (CEQ) (40 CFR Parts 1500–1508); Section 4(f) of the Department of Transportation Act of 1966, as amended (49 USC 303 and 23 USC 138); the revised Chapter 267, Florida Statutes (F.S.); and the standards embodied in the Florida Division of Historical Resources' (FDHR) Cultural Resource Management Standards and Operational Manual (February 2003), and Chapter 1A-46 (Archaeological and Historical Report Standards and Guidelines), Florida Administrative Code. In addition, this report was prepared in conformity with standards set forth in Part 2, Chapter 8 (Archaeological and Historical Resources) of the FDOT Project Development and Environment Manual. All work also conforms to professional guidelines set forth in the Secretary of Interior's Standards and Guidelines for Archaeology and Historic Preservation (48 FR 44716, as amended and annotated).

No previously recorded archaeological sites were located within the APE, nor within a one-mile buffer encompassing the APE. Subsurface testing within the corridor was not possible or necessary within the APE due to the artificial nature of the island landform and the ubiquity of paved roadway, buried utilities, and hardscaping. The desktop analysis and pedestrian survey determined that the archaeological APE exhibits a low potential for containing intact archaeological sites. No Miami-Dade County-designated archaeological sites or zones are located within the APE

The historic resources survey resulted in the identification of 12 historic resources within the historic resources APE, one of which was previously recorded. The previously recorded Atlantic Island Bridge (8DA6433) was documented in 2016 and determined eligible for listing in the National Register by the SHPO on August 23, 2016 under Criteria A and C in the areas of Community Planning and Development and Architecture for its association with the development of the Atlantic Island subdivision and Sunny Isles Beach, as well as its unique design. No changes to the bridge were observed since it was last recorded and the FMSF form was not updated during the current survey.

The 11 newly recorded historic resources include eight historic buildings (8DA15822-8DA15823, 8DA19157-8DA19162), two historic designed landscape features (8DA15824-8DA15825), and one historic designed landscape (8DA19241). The Atlantic Island Resource Group (8DA19241), a designed landscape, is considered eligible for listing in the National Register under Criteria A and C in the areas of Community Planning and Development and Landscape Architecture. The two landscape features, the Lake of the Isles (8DA15824) and Atlantic Island Park (8DA15825), are considered a contributing part of the resource group, along with the previously recorded National Register-eligible Atlantic Island Bridge (8DA6433).

The eight newly recorded historic buildings (8DA15822-8DA15823, 8DA19157-8DA19162) exhibit common architectural styles and design types found in South Florida. Many of the structures feature alterations or modifications which diminish their historic physical integrity including replaced windows, doors, or exterior material, the addition of non-historic exterior ornament, or additions to the historic structure.

Timothy A. Parsons, Ph.D.
January 27, 2022
Page 3

Research conducted during this study did not identify known associations with significant people or historical events.

Analysis of aerial photographs revealed that the area surrounding the project APE was not largely developed until the 1960s, with more than half of the lots in the subdivision containing the APE remaining undeveloped by 1968. While every lot within the subdivision is now developed, this construction mainly occurred after the early 1970s. Furthermore, a later wave of development in the 1990s and 2000s resulted in several adjacent historic parcels with large additions which have altered the appearance of any historic buildings or contain modern buildings constructed as infill. Based on field observations, it does not appear that there are any potential residential historic districts that may contain any of the buildings within the APE at this time. Therefore, these eight newly recorded historic resources are considered ineligible for listing in the National Register, either individually or as part of a historic district.

We kindly request that this cover letter and the enclosed document are reviewed, and concurrence is provided by your office. This information is provided in accordance with the provisions contained in 36 CFR, Part 800, as well as the provisions contained in the revised F.S. Chapter 267. If you have any questions regarding the subject project, please contact me at Steven.james@dot.state.fl.us or (305) 470-5221.

Sincerely,

Steven Craig James, RLA
District Environmental Manager

DocuSigned by:
Steven James
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Timothy A. Parsons, Ph.D.

January 27, 2022

Page 4

The Florida State Historic Preservation Officer finds the attached Cultural Resource Assessment Survey Report complete and sufficient and concurs / does not concur with the recommendations and findings provided in this cover letter for SHPO/FDHR

Project File Number 2022-518. Or, the SHPO finds the attached document contains _____ insufficient information.

In accordance with the Programmatic Agreement among the ACHP, SHPO and FDOT regarding Implementation of the Federal-Aid Highway Program in Florida, if providing concurrence with a finding of No Historic Properties Affected for a project as a whole, or to No Adverse Effect on a specific historic property, SHPO shall presume that FDOT may approve the project as de minimis use under Section 4(f) under 23 CFR 774.

SHPO Comments:

Kelly L. Chase

Digitally signed by Kelly L. Chase
DN: cn=Kelly L. Chase, o, ou,
email=kelly.chase@dos.myflorida.com, c=US
Date: 2022.02.04 13:07:35 -0500

2/4/2022

Timothy A. Parsons, Director, and
State Historic Preservation Officer
Florida Division of Historical Resources

[DATE]



Florida Department of Transportation

RON DESANTIS
GOVERNOR

1000 NW 111 Avenue
Miami, FL 33172

JARED W. PERDUE, P.E.
SECRETARY

May 4, 2022

Alissa S. Lotane
Director, Division of Historical Resources, and
State Historic Preservation Officer
R.A. Gray Building
500 S. Bronough Street
Tallahassee FL 32399-0250

Attn: Ms. Marsha Welch, Transportation Compliance Review Program

Re: Section 106 Determination of Effects Case Study Report for the Atlantic Isle at West Bridge (FDOT) Bridge No. 874218) Project Development and Environment (PD&E) Study in the City of Sunny Isles Beach, Miami-Dade County, Florida (Financial Project ID [FPID] No. 430029-2-21-01)

Dear Ms. Lotane,

At the request of the Florida Department of Transportation (FDOT) District 6, Janus Research prepared the Section 106 Determination of Effects Case Study Report for the Atlantic Isle at West Bridge (FDOT Bridge No. 874218) Project Development and Environment (PD&E) Study in the City of Sunny Isles Beach, Miami-Dade County, Florida (Financial Project ID [FPID] No. 430029-2-21-01). In accordance with the provisions of Section 106 of the *National Historic Preservation Act (NHPA) of 1966* (Public Law 89-665, as amended), as implemented by 36 CFR 800 -- *Protection of Historic Properties* (incorporating amendments effective August 5, 2004), this case study report documents potential effects of the proposed project to the National Register of Historic Places (National Register) eligible resources identified during the *Cultural Resources Assessment Survey (CRAS) for the Atlantic Isle at West Bridge* (Janus Research 2022).

The CRAS resulted in the identification of the previously recorded Atlantic Island Bridge (8DA6433) which was documented in 2016 and determined eligible for listing in the National Register by the State Historic Preservation Officer (SHPO) on August 23, 2016 under Criteria A and C in the areas of Community Planning and Development and Architecture for its association with the development of the Atlantic Island subdivision and Sunny Isles Beach, as well as its unique design. The Atlantic Island Resource Group (8DA19241), a designed landscape, was determined eligible for listing in the National Register under Criteria A and C in the areas of Community Planning and Development and Landscape Architecture. The two landscape features, the Lake of the Isles

(8DA15824) and Atlantic Island Park (8DA15825), are a contributing part of the resource group, along with the previously recorded National Register-eligible Atlantic Island Bridge (8DA6433). The SHPO concurred with the findings of the CRAS in a letter signed on February 4, 2022.

The Section 106 Process thus far has identified historic properties within the project APE, and this report presents the evaluation of the potential effects that the proposed project activities may have on these National Register-eligible properties. The Criteria of Adverse Effect, as defined in 36 CFR Part 800.5, were applied to the significant historic properties to determine project effects on each of the eligible historic properties. In consideration of available project information, the Preferred Alternative, Build Alternative #2, will have an adverse effect on the Atlantic Island Bridge (8DA6433) since it will be removed. With the removal of the bridge, the Atlantic Island Resource Group (8DA19241) will also be adversely impacted. The Preferred Alternative will have no adverse effect on the Lake of the Isles (8DA15824), and Atlantic Island Park (8DA15825). The Preferred Alternative incorporates a new low-profile bridge with a structural arch and non-structural oolitic limestone along the exterior faces to acknowledge the form and aesthetics of the existing bridge.

Affected parties' consultation during the Section 106 process among FDOT, SHPO, interested parties, and the public took place at meetings with project stakeholders in June 2022 and October 2022. Further consultation will take place in order to develop mitigation and a Memorandum of Agreement (MOA).

We kindly request that this cover letter and the enclosed document are reviewed, and concurrence is provided by your office. This information is provided in accordance with the provisions contained in 36 CFR, Part 800, as well as the provisions contained in the revised F.S. Chapter 267. If you have any questions regarding the subject project, please contact Steven Craig James, District Environmental Manager at Steven.james@dot.state.fl.us or (305) 470-5221.

Sincerely,

DocuSigned by:

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Victoria Vogt, M.S.
Environmental Specialist III

The Florida State Historic Preservation Officer finds the attached Section 106 Determination of Effects Case Study Report complete and sufficient and concurs/ does not concur with the recommendations and findings provided in this cover letter for SHPO/FDHR Project File Number 2022-0518B Or, the SHPO finds the attached document contains insufficient information.

In accordance with the Programmatic Agreement among the FHWA, ACHP, FDHR, SHPO, and FDOT Regarding Implementation of the Federal-Aid Highway Program in Florida, if providing concurrence with a finding of No Historic Properties Affected for a project as a whole, or to No Adverse Effect on a specific historic property, SHPO shall presume that FHWA will proceed with a *de minimis* Section 4(f) finding at its discretion for the use of land from the historic property.

SHPO Comments:

Kelly A Chase
for

5.12.2023

Alissa S. Lotane, Director, and
State Historic Preservation Officer
Florida Division of Historical Resources

[DATE]

DRAFT

APPENDIX B
COORDINATION WITH AGENCIES AND
STAKEHOLDERS

DRAFT



Affected Parties Consultation Meeting Summary Report

**SR A1A/Atlantic Isles Lagoon Bridge
(Bridge No. 874218)
City of Sunny Isles Beach
Miami-Dade County, Florida**

Project Identification Number: 430029-2-21-01

Wednesday, June 8, 2022 | 3 p.m. to 4:30 p.m.

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Meeting Summary

SR A1A/Atlantic Isles Lagoon Bridge Affected Parties Meeting - Summary

Monday, June 13, 2022 | 3 p.m.
GoToWebinar

Staff:

Jacobs

John Flora, Consultant Project Manager

Colleen Ross, Deputy Consultant Project Manager

Alex Meitin, Roadway Lead

Bhushan Godbole, Structure Lean

Tara Jones, Section 4(f) Lead

Janus Research

Amy Streeleman

Ken Hardin

Stantec

Joy Castro

FDOT

Nicholas Danu, Project Manager

Barbara Culhane, District 6 Cultural Resources Coordinator

Katherine Bernabeo

MD Hussain

Tish Burgher

Infinite Source Communications

Monica Diaz

Affected Parties attendees are presented in the Attendee Log as an attachment.

Purpose of the meeting:

The Affected Parties meeting was held to consult with affected parties on the potential alternatives to improve the existing Atlantic Isles Lagoon Bridge (Bridge No. 874218) The meeting was held virtually. Invitations were emailed to affected parties on May 9, 2022 by the FDOT District 6 Public Information Office.

Key Items discussed:

Introduction

Community Outreach Specialist Monica Diaz, of Infinite Source Communications, welcomed the attendees and allowed each project team member to introduce themselves. Ms. Diaz also helped with introductions for each agency representative.

The following members of the project's affected parties were in attendance: Ms. Elizabeth Morales a resident and a member of the Historic Board, Mr. Michael McDaniel with the Florida Department of Transportation (FDOT) Central office, Ms. Alyssa McManus with the State Historic Preservation Office, Ms. Adrienne Burke with the Miami-Dade County of Historic Preservation, Ms. Lindsay Rothrock with FDOT Office of Environmental Management, Ms. Susan Simpson Deputy City Manager of City of Sunny Isles Beach and Mr. Jeff Ransom Miami-Dade County Archeologist.

Ms. Diaz stated the rules of engagement, reviewed the FDOT Title Six policies and provided a brief explanation of the outreach conducted for the meeting.

Mr. Danu presented each slide and provided a detailed explanation of the following slides: Agenda, What is a PD&E Study, Project Location Map, Project Study Area, Project Background, Project Timeline, Bridge Deficiencies, Purpose and Need, Project Goals, Existing Bridge Typical Section, and Atlantic Avenue Existing Typical Section.

Ms. Strelman continued the presentation and discussed Section 106 of the National Historic Preservation Act Process, Cultural Resources – Area of Potential Effect (APE), Atlantic Isles Historic Importance, Section 106 of the National Historic Preservation Act, and Affected Parties Consultation.

Mr. Danu continued by presenting the Initial Alternatives Considered, Viable Alternatives, No Action Alternative, Build Alternative 1 – Rehabilitation (Typical Section, Elevation View, Plan View), Build Alternative 2 – Replacement (Typical Section, Elevation View, Plan View), Temporary Maintenance of Traffic Impact Considerations, Alternatives Characteristics Evaluation Matrix, and Alternative Impact Evaluation Matrix.

Ms. Strelman talked about the project's next steps and Mr. Danu finalized the presentation with the Project Schedule.

Question & Answer Session

- Ms. Morales stated the planters are part of the historic character of the bridge. She noted the planters are shown on the rehabilitation alternative; however, they are not part of the replacement alternative. Ms. Morales asked the team why the replacement alternative did not include the planters.
 - Mr. Danu responded current design standards do not allow for planters on bridges.
- Ms. Morales asked if the Department can reuse the limestone on the current bridge during the replacement to replicate the historic façade.
 - Mr. Danu responded the Department's goal is to mimic the historic bridge as much as possible. He added the details and materials will be analyzed later in the project.
- Ms. Morales asked what increased shading means.
 - Mr. Danu responded that because Build Alt 2 includes a shared use path, it has a wider footprint and would decrease sunlight to the lagoon; therefore, increasing the shade.
 - Ms. Castro stated that more shading could affect the fish habitats as they require sunlight. FDOT is going to coordinate with National Marine Fisheries Service to review if this Alternative is viable for the marine life.
- Ms. Ivette Francolla asked if the bridge could be rehabilitated as a pedestrian bridge only.
 - Mr. Danu responded no, it would not meet the purpose and need of the project.
- Ms. Susan Simpson asked if the planters on the bridge are historic.
 - Mr. Norman replied they are not historic and were placed about 10-15 years ago.
- Ms. Francolla asked if the City and the residents would want the bridge to be historic.
 - Mr. Norman responded that the bridge is historic right now; however, if it is replaced then it would have to be reapplied to the replacement.

- Ms. Susan Simpson asked if the PD&E moves forward, when would construction commence and what would be the cost.
 - Mr. Danu stated the City would be responsible for 25% of the total construction costs and further noted that federal funding for all off-system bridges is coordinated with FDOT Central Office. Mr. Danu also noted that the Department is developing a Memorandum of Agreement between the City and FDOT to specify responsibilities for costs and maintenance.
 - Ms. Streelman asked if that is what the City did with the two bridges that were replaced in the front of the neighborhood; Mr. Norman replied yes.
- Ms. Francolla stated FDOT is not interested in maintaining the original structure, but they are using it for vehicles.
 - Mr. Danu responded yes, the bridge will be used for vehicles.
- Ms. Streelman asked if the members from the SHPO and OEM had any questions.
 - Ms. Rothrock (OEM) said that she had no questions and that the team was doing a good job following the process

Ms. Diaz thanked the attendees for their participation and provided the team's contact information and invited them to the public meeting on the Thursday, June 23, 2022.

DRAFT

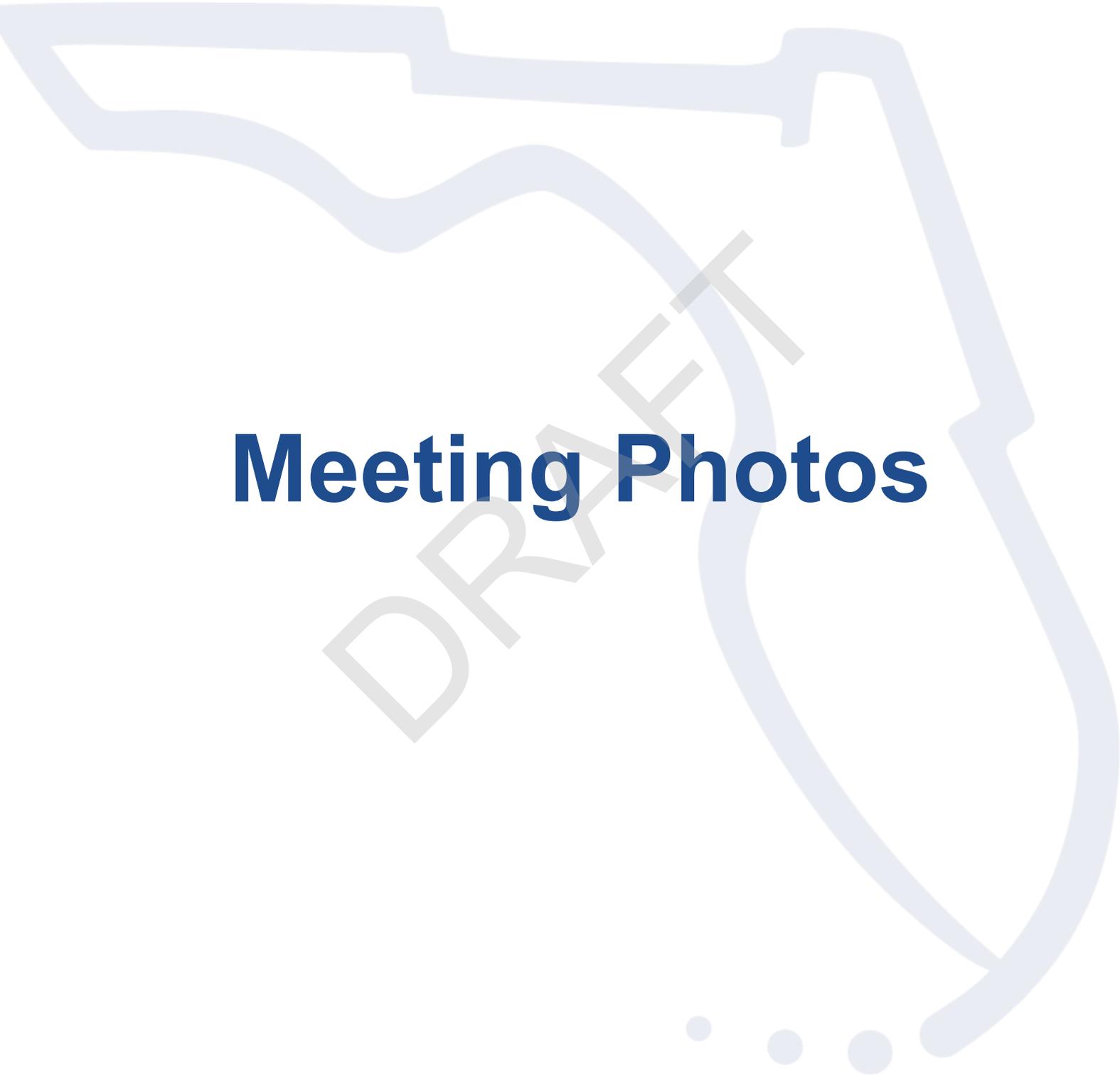
Attendee Log

DRAFT

Virtual Sign-In

Name	Representing
Kelly Chase	DHR
Alyssa McManus	SHPO
Lyndsay Rothrock	FDOT OEM
Michael McDaniel	FDOT OEM
Richard Labinsky	SIB City Engineer
Susan Simpson	SIB Deputy City Manager
Jeff Ransom	Miami Dade County Archaeologist
Ivette Francolla	member of SIB Historical Board
Adrienne Burk	MDC Office of Historic Preservation
Elizabeth Morales	Member of the Historic Preservation Board
Norman Edelcup	Member of SIB Historical Board, Former SIB Mayor, and lives in Atlantic Isle

DRAFT

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Meeting Photos



Meeting Purpose and Speakers



Nick Danu
FDOT



Amy Strelman
Janus Research



John Flora
Jacobs



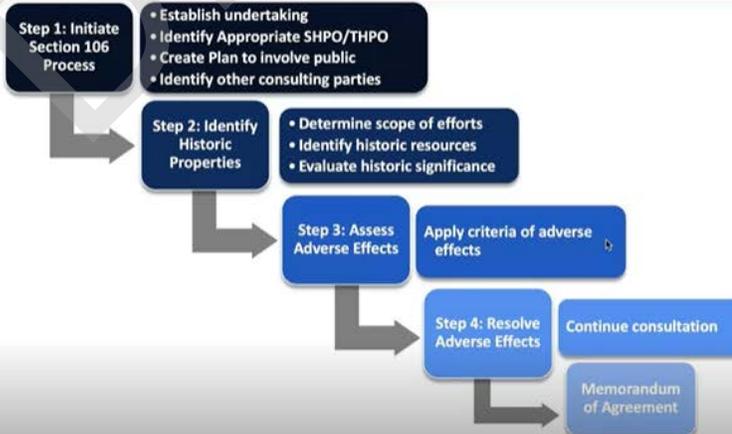
Barbara Culhane
FDOT



Ken Hardin
Janus Research



Section 106 of the National Historic Preservation Act Process



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Invitation Letter

DRAFT



From: Cynthia.Turcios@dot.state.fl.us <Cynthia.Turcios@dot.state.fl.us>
Sent: Monday, May 9, 2022 5:48 PM
To: Monica Diaz <monica@iscprgroup.com>
Subject: PD&E Consultation Meeting: for Atlantic Isle at West Bridge



Florida Department of Transportation

RON DESANTIS
GOVERNOR

1000 N.W. 111 Avenue
Miami, Florida 33172

JARED W. PERDUE, P.E.
SECRETARY

May 9, 2022

SUBJECT: Affected Parties Consultation for the Atlantic Isle at West Bridge (FDOT Bridge No. 874218)
Project Development and Environment (PD&E) Study in the City of Sunny Isles Beach, Miami-Dade County, Florida
(Financial Project ID [FPID] No. 430029-2-21-01)

Dear Ms. Diaz,

The Florida Department of Transportation (FDOT) is inviting you to participate in an affected parties consultation meeting for the above-mentioned project. This meeting is an important component of the United States Code Section 106 of the National Historic Preservation Act of 1966. The purpose of the consultation is to ensure that the range of stakeholder views regarding possible improvements to the Atlantic Isle Bridge, the Atlantic Island Resource Group, a designed landscape, and the two landscape features, the Lake of the Isles and Atlantic Island Park, are taken into consideration as project alternatives are developed. Each of these resources are eligible for inclusion in the National Register of Historic Places (National Register). The meeting will provide information to assist in understanding the Section 106

<https://outlook.office.com/mail/id/AAMkAGZkYzJZWQ5LTlmYzctNGI0Ni04ODFLWU0ZTYyZGQ0N2I1M1M1QBGAAAAACICUtlL%3B3RS5zMAC9GQyyEB...> 1/2



1/8/24, 8:14 PM

Mail - Monica Diaz - Outlook

process and project and to help the project team explore ways to address issues and needs that may be identified through the study.

Four significant historic resources were identified as part of the Cultural Resources Assessment Survey: the National Register-eligible Atlantic Island Bridge (8DA6433) and the Lake of the Isles (8DA15824), Atlantic Island Park (8DA15825), and the Atlantic Island Resource Group (8DA19241). The Atlantic Island Resource Group (8DA19241) is a designed historic landscape comprised of the National Register-eligible Atlantic Isle Bridge (DA6433), the man-made Lake of the Isles (8DA15824), and the surrounding triangular shaped Atlantic Island Park (8DA15825), all of which were constructed circa 1925 and are directly connected spatially and historically.

Our virtual meeting is scheduled for Wednesday, June 8, 2022, from 3:00 p.m. to 4:30 p.m.

Please join the meeting from your computer, tablet or smartphone visit the link:

<https://meet.goto.com/787699741>

Participants can also call in by dialing: +1 (408) 650-3123; Access Code: 787-699-741

Your contact for scheduling attendance to this affected parties meeting will be Ms. Monica Diaz. She can be reached by phone at telephone number 305-984-2715 or via e-mail at monica@iscprgroup.com. For general questions regarding the project, please contact Mr. Nicholas Danu, P.E., Project Development Manager at telephone number 305-470-5219 or via email at Nicholas.Danu@dot.state.fl.us. We look forward to working with you and thank you in advance for dedicating time to this project.

Sincerely,
Nicholas Danu, P.E.
District Six Project Development Manager

- [Project Factsheet](#)
- [Project Webpage](#)
- [Unsubscribe from email alerts](#)

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Affected Parties										
Contact Name	Agency	Position	Appointed By/Two-Year Term: 1/20 to 12/21	Street Address	City	State	Zip	Phone number	Email	Note
Warren J. Stamm	City of Sunny Isles Beach Historic Preservation Board	Chairperson	Mayor Goldman	16001 Collins Avenue #3204	Sunny Isles Beach	FL	33160	904-868-1635	wjslegal@aol.com	Confirmed 5/2/22
Raul Rodriguez	Atlantic Isles HOA	Atlantic Isles HOA President						303-778-4023		
Claudia Hasbun	City of Sunny Isles Beach Historic Preservation Board	Staff Liaison/ Director, Planning & Zoning	Planning and Zoning/Historical Preservation Board	18070 Collins Avenue	Sunny Isles Beach	FL	33160	305-792-1757	chasbun@sibfl.et	Confirmed 09/09/20
	City of Sunny Isles	Staff Liaison/ Director, Planning & Zoning								
Susan Simpson	City of Sunny Isles Beach Historic Preservation Board	Assistant City Manager	City Manager's Office	18070 Collins Avenue	Sunny Isles Beach	FL	33160	305-792-1800	ssimpson@sibfl.net	Confirmed 09/09/20
Robin Chicly	City of Sunny Isles Beach Historic Preservation Board	Member	Commissioner Stuyvesant	19380 Collins Avenue	Sunny Isles Beach	FL	33160	(786) 436-1171	rc438@cornell.edu	Confirmed 5/2/22
Norman Edelcup	City of Sunny Isles Beach Historic Preservation Board	Member	At-Large	244 Atlantic Isle	Sunny Isles Beach	FL	33160	(786) 202-1119	nsedelcup@aol.com	Confirmed 5/2/22
Ivette Francola	City of Sunny Isles Beach Historic Preservation Board	Member	Vice Mayor Viscarra	305 Poinciana Island Drive	Sunny Isles Beach	FL	33160	(305) 945-9784	ifrancolla@yahoo.com	Confirmed 5/2/22
Joel Levine	City of Sunny Isles Beach Historic Preservation Board	Member	Commissioner Lama	16445 Collins Avenue, Unit 524	Sunny Isles Beach	FL	33160	(516) 381-5618	wb2bmh@atlanticbb.net	Confirmed 5/2/22
Elizabeth Morales	City of Sunny Isles Beach Historic Preservation Board	Member	Commission Joseph	19380 Collins Avenue	Sunny Isles Beach	FL	33160	(305) 733-5915	liza.morales@alliedsolutions.net	Confirmed 5/2/22
Mark Snyder	City of Sunny Isles Beach Historic Preservation Board	Member	At-Large	16425 Collins Avenue, Unit 2912	Sunny Isles Beach	FL	33160	(786) 593-2471	doraceus@aol.com	Confirmed 5/2/22
Michael McDaniel	FDOT Office of Environmental Management	Project Delivery Coordinator		605 Suwannee Street	Tallahassee	FL	32399	850-414-5329	michael.mcdaniel@dot.state.fl.us	Confirmed 04/11/22
Lindsay S Rothrock	FDOT Office of Environmental Management	Cultural and Historic Resource Specialist		605 Suwannee Street	Tallahassee	FL	32399	850-414-5269	lindsay.rothrock@dot.state.fl.us	Confirmed 04/8/22
Kelly Chase	Florida State Historic Preservation Officer (SHPO) / Division of Historical Resources	Compliance and Review Supervisor		500 South Bronough Street 4th Floor	Tallahassee	FL	32399	850-245-6344	Kelly.Chase@dos.myflorida.com	Confirmed 04/8/22
Alyssa McManus	Florida State Historic Preservation Officer (SHPO) / Division of Historical Resources	Architectural Historian		500 South Bronough Street 4th Floor	Tallahassee	FL	32399	850-245-6368	alyssa.mcmanus@dos.myflorida.com	Confirmed 04/8/22
Marsha Welch	Florida State Historic Preservation Officer (SHPO) / Division of Historical Resources	Historic Preservationist		500 South Bronough Street 4th Floor	Tallahassee	FL	32399	850.245.6315	Marsha.Welch@DOS.MyFlorida.Com	Confirmed 04/8/22
Andrew A Kizlauskas	U.S. Army Corps of Engineers	Chief Panama City Permitting Section		415 Richard Jackson Boulevard Suite 411	Panama City	FL	32407	904-304-9095	andrew.a.kizlauskas@usace.army.mil	Confirmed 09/09/20
Michelle Gilbert	U.S. Army Corps of Engineers			415 Richard Jackson Boulevard Suite 411	Panama City	FL	32407	904-304-9095	michelle.l.gilbert@usace.army.mil	Confirmed 04/11/22
Sara Cody	Miami-Dade County Historic Preservation Board	Chief		111 NW 1st Street 12th Floor	Miami	FL	33128	305-375-4438	scody@miamidadecounty.gov	Confirmed 04/08/22
Christine Rupp	Dade Heritage Trust	Executive Director		190 Southeast 12 Terrace	Miami	FL	33131	305-358-9572	chris@dadeheritagetrust.org	Confirmed 04/08/22



Alternatives Workshop Meeting Report

SR A1A/Atlantic Isles Lagoon Bridge
(Bridge No. 874218)
City of Sunny Isles Beach
Miami-Dade County, Florida

Project Identification Number: 430029-2-21-01

Thursday, June 23, 2022 | 6 p.m.

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Meeting Summary

SR A1A/Atlantic Isles Lagoon Bridge Alternatives Public Workshop - Summary

Thursday, June 23, 2022 | 6 p.m.

In Person: Sunny Isles Beach Gateway Center

151 Sunny Isles Blvd

Sunny Isles Beach, FL 33160

Virtual: GoToWebinar

Staff:

Stantec

Joy Castro – In-Person

Jacobs

Bhushan Godbole - Virtual

Alex Meitin – In-Person

Colleen Ross - Virtual

John Flora – In-Person

Michael Baker - Virtual

Janus Research

Ken Hardin – In-Person

FDOT

Nicholas Danu – In-Person

Katherine Bernabeo – In-Person

Barbara Culhane – In-Person

Tish Burgher – In-Person

Md Hossain – In-Person

Infinite Source Communications

Monica Diaz – In-Person

Alexander Coicou – In-Person

Andre Souza – In-Person

Meeting attendees from the public are presented in the Attendee Log as an attachment.

Purpose of the workshop:

The Alternatives Public Workshop was to provide the community an opportunity to review and comment on the proposed alternatives for the Atlantic Isles Lagoon Bridge (Bridge No. 874218)

The Alternatives Public Workshop was held in a hybrid meeting format. The hybrid meeting included two options for interested parties to attend, either in-person or virtual. The in-person option took place at the Sunny Isles Beach Gateway Center, which is approximately one quarter mile from the Atlantic Isle community. The virtual option was held on the GoToWebinar platform using the following registration link: <https://register.gotowebinar.com/register/3105107021843563531>. Once participants arrived at the meeting, they could view project displays and a fact sheet both in-person and virtually online. The meeting began with key staff members at a table at the head of the meeting room to present an overview of the project. Cameras and microphones were in the meeting room so that virtual attendees could hear and watch the same presentation. A formal question and answer session took place after the presentation where both in-person and virtual attendees could ask questions or make comments. Project team members were available both in person and virtually to answer questions and provide assistance.

Key Items discussed:

Introduction

Community Outreach Specialist Monica Diaz, of Infinite Source Communications, welcomed the attendees and stated the rules of engagement. She also reviewed the Florida Department of Transportation (FDOT) Title Six policies and provided a brief explanation of the outreach conducted for the workshop. Ms. Diaz recognized the Elected Officials in attendance: City Clerk Mr. Mauricio Betancur of City of Sunny Isles Beach, Commissioner, Seat One Jerry Joseph of City of Sunny Isles Beach, Commissioner, Seat Three Fabiola Stuyvesant of City of Sunny Isles Beach, City Engineer Rick Labensky of City of Sunny Isles and Ms. Margie Robinson from Miami-Dade County District 4 Commissioner Sally Heyman's office.

FDOT District Project Development Manager Mr. Nicholas Danu introduced the project team and presented the agenda.

Ms. Diaz advised attendees to use their cellphones or computers to participate in a polling activity using the Slido Application and explained how to access the questions. Ms. Diaz stated the polling results will help provide preliminary input to the team but are not the only factor used in developing potential alternatives. Ms. Diaz presented two ice breaker questions to ensure the participants were familiarized with Slido. All questions and responses are provided at the end of the summary.

Mr. Danu presented slides and details on the: project location map, project study area, project background, what is a PD&E study, purpose and need and project goals.

Throughout the presentation, Ms. Diaz used the Slido Application to ask project polling questions of the meeting attendees. The polling questions and results are presented on Page 7 of this summary.

Mr. Danu continued the presentation with existing bridge deficiencies, existing roadway and bridge typical sections on Atlantic Avenue, initial alternatives considered, no action alternative, Graphical representations of the proposed typical section, elevation view, and plan view were shown for each of the Build Alternatives A video of a rendering of Build Alternative 2 in the community, was presented to give attendees an idea of what this alternative would look like in the community. Mr. Danu then presented the temporary maintenance of traffic impact considerations, and alternative characteristics evaluation matrix. Mr. Danu then introduced Ms. Joy Castro, Environmental Scientist.

Ms. Castro discussed the natural resources, right-of-way considerations, physical environment, and agency coordination. Ms. Castro then introduced Mr. Ken Hardin, Cultural Resources Specialist.

Mr. Hardin presented the cultural resources, Atlantic Isle historic importance, section 106 of the National Historic Act process, and what is section 4(f)?

Mr. Danu then presented the alternative impact evaluation matrix.

Mr. Danu concluded the presentation with the project schedule and public involvement initiatives.

Ms. Diaz provided the participants with information on how to remain engaged during the project and contact information for the team.

Question & Answer Session

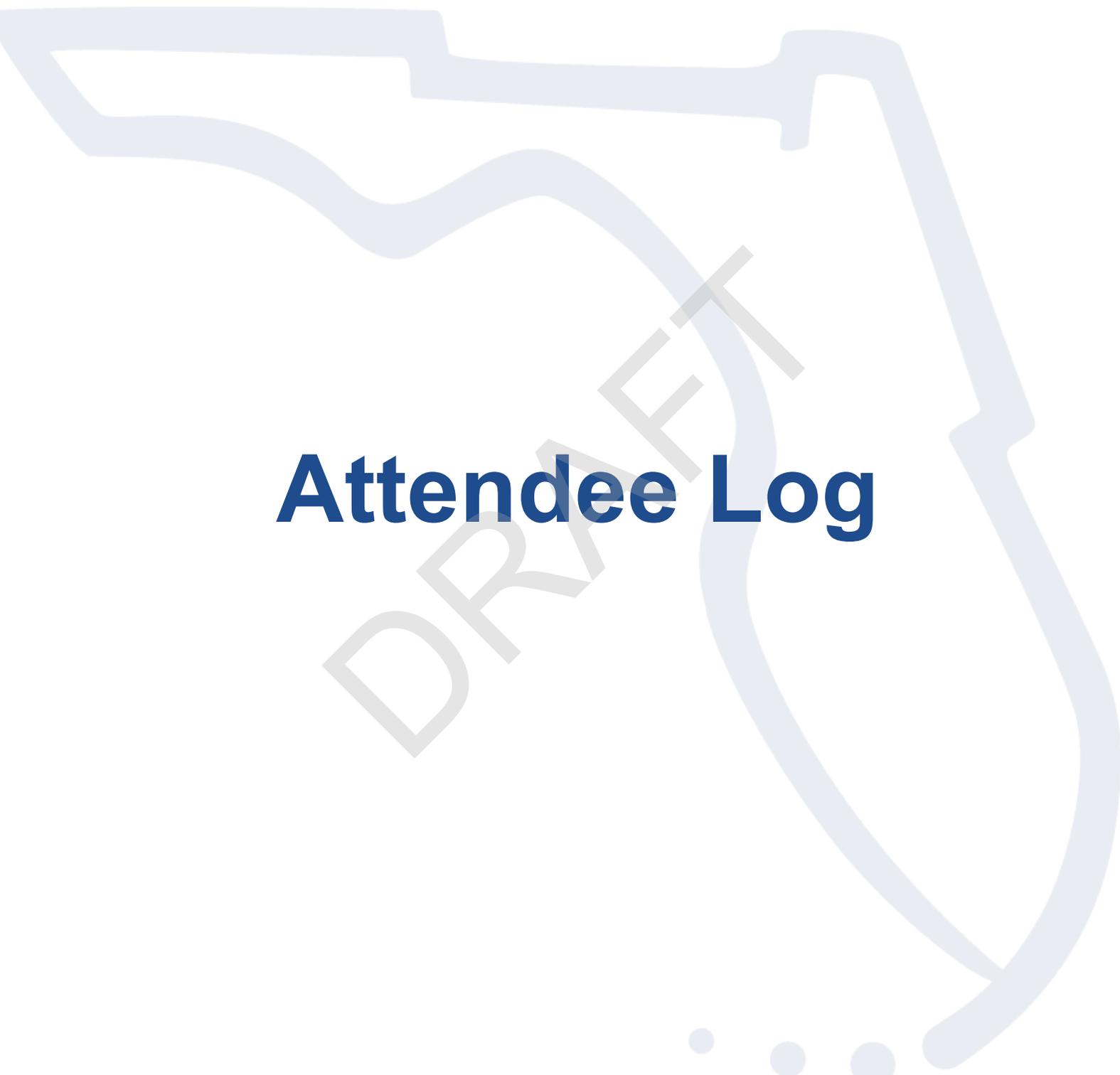
Ms. Diaz moderated questions from both virtual and in-person attendees.

- Mr. Arie Stager, resident of 262 Atlantic Island, stated that the wooden rafters under the bridge are in poor conditions. He suggested the Department sends a crew to conduct a site inspection and noted his concerns with the weight of the garbage trucks traveling across the bridge.
- Mr. Tom O'Connell, resident of 283 Atlantic Isle, stated there is a green heron that nests yearly on the northwest side.
 - Mr. Castro asked for clarification of where the green heron's nest is located.
 - Mr. O'Connell confirmed that it is on northwest side of the bridge and that it just had babies.
- Mr. O'Connell asked if the proposed bridge will be navigable for vessels.
 - Mr. Danu responded the bridge will not be navigable for coast guard permits; however, a small kayak or canoe can go through.
- Mr. Kent Percy, resident of Sunny Isles Beach, stated the Alternative Two bridge is wider than the existing bridge. Mr. Percy asked if another alternative was considered so that the existing bridge could remain as a pedestrian bridge and therefore extend its life expectancy. He suggested the possibility of a third alternative that would include adding a new travel lane/bridge for cars, next to the existing bridge to bypass the existing bridge.
 - Mr. Danu replied the Department reviewed the bridge's existing alignment and noted that to add an additional travel lane for cars, the existing roadway alignment would have to be shifted significantly to the south in order to meet current geometrical standards.
 - Mr. Percy asked that this be analyzed further as a pedestrian bridge because it is a historic monument, and it means a lot to the residents.
 - Mr. Danu noted that maintaining the existing bridge as a pedestrian bridge does not meet the purpose and need of the project.
 - Mr. Bhushan Godbole, Structural Engineer stated that adding a new travel lane/bridge next to the existing bridge would affect the aesthetics of the existing bridge as it would not be as visible as it is today.

Ms. Diaz thanked the attendees for their participation and provided the team's contact information and project website.

Slido Interactive Poll Questions – Summary of Results

1. What is your favorite season?
 - 33% Spring
 - 44% Fall
 - 11% Winter
 - 11% Summer
2. What group best represents you?
 - 60% Agency
 - 20% Residents
 - 20% Elected Officials
 - 0% Business Owner
 - 0% Interested Party
3. Which of these projects goals is most important to you?
 - 13% Minimize environmental impacts
 - 25% Minimize effects to significant cultural resources
 - 63% All of the above
 - 0% Improve mobility
 - 0% Enhance safety
4. Of the two build alternative presented this evening, which one best serves the community's needs? Build Alternative one, the rehabilitation alternative, Or Build Alternative two, the replacement alternative? Or something else?
 - 17% Build Alternative 1 – Bridge Rehabilitation
 - 83% Build Alternative 2 – Bridge Replacement
 - Other
 - Unsure
5. In a few words, do you have any additional thoughts about this project?
 - Necessary. Ideal to keep

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Attendee Log

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Virtual Attendees - GoToWebinar

Atlantic Isles at West Bridge PD&E Study - Alternatives Workshop		
Last Name	First Name	Email Address
Bejar	Frank	frankbejar@gmail.com
Dawson	Richard	radawson007@gmail.com
Diaz	Frank	fdianz1321@gmail.com
Levine	Joel	wn2bmh@atlanticbb.net
Robinson	Margie	amadorm@miamidade.gov
Hall	Truett	truettthall@policastralaw.com
Marchesani	Alessandro	alessandro.marchesani@miamidade.gov

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In-Person Attendees - Sign In Cards

SIGN-IN CARD
Alternatives Workshop
 06/23/22
 Atlantic Isle at West of State Road (SR) A1A (Bridge No. 874218)
 in the City of Sunny Isles Beach
 Financial Management Number: 430029-2-21-01
 Miami-Dade County, Florida

NAME: MARICRU ESTANCUER

REPRESENTING: _____
Sunny Isles Beach

ADDRESS: _____
19070 COLLINS AVENUE

PHONE NUMBER: _____
(305) 792-1703

EMAIL: _____
mestanur@sibfl.net

SIGN-IN CARD
Alternatives Workshop
 06/23/22
 Atlantic Isle at West of State Road (SR) A1A (Bridge No. 874218)
 in the City of Sunny Isles Beach
 Financial Management Number: 430029-2-21-01
 Miami-Dade County, Florida

NAME: _____
LEWIS THALER

REPRESENTING: _____
19333 COLLINS AVE

ADDRESS: _____
788 208 1122

PHONE NUMBER: _____
MRTENNISLT@AOL.COM

EMAIL: _____

SIGN-IN CARD
Alternatives Workshop
 06/23/22
 Atlantic Isle at West of State Road (SR) A1A (Bridge No. 874218)
 in the City of Sunny Isles Beach
 Financial Management Number: 430029-2-21-01
 Miami-Dade County, Florida

NAME: NORMAN S. EDELEP

REPRESENTING: _____
Historic Preservation Board

ADDRESS: _____
244 Atlantic Isle

PHONE NUMBER: _____
786-202-1119

EMAIL: _____
nseDELAY@aol.com

SIGN-IN CARD
Alternatives Workshop
 06/23/22
 Atlantic Isle at West of State Road (SR) A1A (Bridge No. 874218)
 in the City of Sunny Isles Beach
 Financial Management Number: 430029-2-21-01
 Miami-Dade County, Florida

NAME: _____
FABIOLA STUYVESANT

REPRESENTING: _____
Commissioner

ADDRESS: _____
326 Poinciana Dr, SIB, FL 33160

PHONE NUMBER: _____
786 291 6571

EMAIL: _____
FABISTUY@OUTLOOK.COM

Margie Robinson
Commissioner of her

SIGN-IN CARD
Alternatives Workshop
 06/23/22
 Atlantic Isle at West of State Road (SR) A1A (Bridge No. 874218)
 in the City of Sunny Isles Beach
 Financial Management Number: 430029-2-21-01
 Miami-Dade County, Florida

NAME: _____
COMMISSIONER JERRY JOSEPH

REPRESENTING: _____
18560 N BAY RD

ADDRESS: _____
SIB FL 33160

PHONE NUMBER: _____
518 229 8228

EMAIL: _____
JJOSEPH@SIBFL.NET

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Alternatives Workshop
 06/23/22
 Atlantic Isle at West of State Road (SR) A1A (Bridge No. 874218)
 in the City of Sunny Isles Beach
 Financial Management Number: 430029-2-21-01
 Miami-Dade County, Florida

NAME: _____
RICK LABINSKY

REPRESENTING: _____
City

ADDRESS: _____
305-792-1817

PHONE NUMBER: _____
rlabinsky@staff.net

EMAIL: _____

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Alternatives Workshop
06/23/22
 Atlantic Isle at West of State Road (SR) A1A (Bridge No. 874218)
 in the City of Sunny Isles Beach
 Financial Management Number: 430029-2-21-01
 Miami-Dade County, Florida

NAME: _____
 REPRESENTING: Tom O'Connell
 ADDRESS: 283 ATLANTIC ISLE
 PHONE NUMBER: 305-947-1242
 EMAIL: _____

SIGN-IN CARD
Alternatives Workshop
06/23/22
 Atlantic Isle at West of State Road (SR) A1A (Bridge No. 874218)
 in the City of Sunny Isles Beach
 Financial Management Number: 430029-2-21-01
 Miami-Dade County, Florida

NAME: _____
 REPRESENTING: Ken Percy
 ADDRESS: _____
 PHONE NUMBER: _____
 EMAIL: Ken@KPercy.me

SIGN-IN CARD
Alternatives Workshop
06/23/22
 Atlantic Isle at West of State Road (SR) A1A (Bridge No. 874218)
 in the City of Sunny Isles Beach
 Financial Management Number: 430029-2-21-01
 Miami-Dade County, Florida

NAME: _____
 REPRESENTING: ARIE STEIGER
 ADDRESS: 262 ATLANTIC ISLE
 PHONE NUMBER: 305 710 1230
 EMAIL: STEIGER@EBELSON.COM

SIGN-IN CARD
Alternatives Workshop
06/23/22
 Atlantic Isle at West of State Road (SR) A1A (Bridge No. 874218)
 in the City of Sunny Isles Beach
 Financial Management Number: 430029-2-21-01
 Miami-Dade County, Florida

NAME: _____
 REPRESENTING: Danny Iglesias
 ADDRESS: 270 ATLANTIC AVE, Sunny Isles Beach FL 33160
 PHONE NUMBER: 305-505-9809
 EMAIL: Danny.Iglesias@att.net

City of SIB Historic Board Preservation Meeting Notes (Colleen Ross, Jacobs)

10/11/22

GoTo Meeting Platform

5:30 PM to 7 PM

Staff Attendees: Thu Clark, Lindsey Rothrock, MD Hossain, Monica Diaz, Katherine Bernabeo, Amy Streelman, Colleen Ross, John Flora, Nick Danu, Randy Mock, Joy Castro, Craig James

Board Attendees: Mauricio Betencur (City Clerk), Warren Stand (president), mayor?, Elizabeth Morales, Yvette Francola, Planning & building director for the City (name?), Ronni Adili, Joel Levine, Valerie Vicente (City Attorney)

Historic Board Preservation Board started their meeting per their protocol and then they joined our meeting. Monica noted that the meeting is being recorded.

- Nick Danu and Amy Streelman went through the presentation and then discussion began
- Board: It was noted by a member that the community placed the planters on the bridge shoulders approximately 7 to 10 years ago to make it a one-way bridge; it was coordinated with FDOT; they did this to force heavy trucks off of the bridge; it was noted that it's an inconvenience for some to have to go around; they asked if the new bridge would allow for 2-way traffic to be restored
 - Nick noted that when the project came to us it was one-way facility and that the project does not allow for adding capacity to the roadway;
- Board asked if the replacement has enough room for 2-way traffic
 - Nick noted that the replacement alternative does not meet criteria for a 2-way facility
- Board: Asked if any private properties would be impacted by the replacement
 - Nick noted no, and that only the park areas would be impacted
- Board: Expressed concern about safety (Replacement Alternative) of the pathway next to the road on the bridge and would like a barrier wall or railing along the path to protect ped/bike from cars
 - It was explained that the path is raised and with the speed limit of the roadway, it should be sufficient for safety; however Board did not agree; Nick explained that the bridge typical section was coordinated with City staff and this is what they agreed to; Board noted that they will discuss with City
- Board: Asked team to help them better understand the significance of the park and lagoon and if a designation would be made
 - Amy explained that the FDOT does not coordinate historic designation but that the information would be give to them so that they could coordinate
 - Amy asked the Board to think about mitigation efforts and suggested that part of the mitigation could include the historical information FDOT provides so that the community could be pursue a local designation for the Resource Group – similar to what they did for the other bridges
 - The Board said they do want the historical information so they could pursue future designation

- Craig clarified that FDOT treats all of these resources as “eligible” as if they are already NRHP-listed; but it is the responsibility of the community to gain the listed designation
 - Board noted that if we replace the bridge it will no longer be NRPH-registered; Amy clarified that the bridge is not currently registered but yes, if the bridge is replaced it is no longer NRHP-eligible
- Board: amongst the Board discussion someone asked if it could be a pedestrian bridge; another member stated that it would be an inconvenience to some of the residences to make the turn arounds
- Craig showed the MOT impacts slide (Slide 36) to ensure the Board understood the temporary impacts to the park area
 - The only concern they had was a tree could be impacted at the temporary turn around; Craig noted that was important feedback and the team will review tree impacts as a result of MOT
 - Craig further noted that FDOT is happy to answer their questions, but their feedback is very important to the Department and this process; Amy reiterated that and gave some examples of typical mitigation they could consider and asked them to take some time to review these ideas and provide feedback at a later date
- Valerie Vicente (City Attorney) wanted to understand the specifics on what FDOT is asking for
 - Amy noted that there isn't a specific requirement from the Board to give feedback, but we would like to get an idea of what mitigation they would like for purposes of the project and so that it can be documented in the Section 106 MOA
 - Board: Noted that they are going to schedule another meeting where all board members could be present to discuss these items in detail and give FDOT feedback afterwards
- Board: thanked FDOT for explaining the alternatives and answering their questions



HISTORIC PRESERVATION BOARD MEETING SUMMARY

Atlantic Isles Lagoon Bridge (Bridge No. 874218) PD&E Study

Located in Atlantic Avenue, City of Sunny Isles Beach, FL

Miami-Dade County, Florida

FPID# 430029-2 – In-House Project

05/11/23

6 PM

Meeting Location: City of Sunny Isles Beach Government Center

18070 Collins Avenue

4th Floor Conference Room

Sunny Isles Beach, FL 33160

Virtual [Zoom Webinar](#)

In-person Attendees: Mauricio Betancur (SIB; City Clerk), Marline PierreSaint, (SIB; Assistant City Clerk), Richard Labinsky (SIB; City Engineer), Alain Boileau (SIB; Deputy City Attorney), Amy Canales (SIB; Planning and Zoning Director), Warren Stamm (HPB Chair), Elizabeth Morales (HPB), Norman Edelcup (HPB), Mark Snyder (HPB), Ivette Francola (HPB), Nicholas Danu (FDOT), Max Adriel Imberman (Janus), Victoria Vogt (FDOT), Monica Diaz (Infinite)

Virtual Attendees: Steven Craig James (FDOT), Amy Streelman (Janus), Joy Castro (Stantec), Alex Meitin (Jacobs), Colleen Ross (Jacobs), Michael Baker (Jacobs)

1) *Presentation*

- a) Nick and Amy presented the study presentation to the City of Sunny Isles Beach Historic Preservation Board (HPB).

2) *Discussion*

- a) Nick stated that the primary impacts from the project are to cultural resources. Because this project will be using federal funds, the study is required to follow the federal processes associated with the National Environmental Policy Act or NEPA; the Section 106 process is part of NEPA.
- b) HPB member stated that pedestrians use the bridge and that more lighting around the bridge would be better. If there are any trees that have to be removed, they would like those preserved and replanted. Requested that the Atlantic Island Resource Group to be listed in the federal register. A historic marker referring to the 1920's construction of the island would also be appropriate.
- c) HPB member asked if the four corners of the bridge can be preserved so that the oolitic limestone can be shown as you enter or exit the bridge.
 - i) Study team said that these details could be worked out during the design phase but preserving the existing corners in place may be difficult.
- d) HPB member stated that they want the replacement bridge to be a close replica of the existing bridge and would like to see some of the materials from the existing bridge used on the new bridge (i.e., oolitic limestone). Further noted that manatees have been spotted in the lagoon during high-tide.
 - i) Victoria and Joy Castro stated that they also do a full environmental evaluation and have documented the existence of sea turtles, manatees, and seagrass. Environmental features and impacts are being coordinated with the USFWS.



- e) HPB member also stated that they wanted to preserve the bridge's historic character and asked about the cost.
 - i) Nick stated that the total project cost is approx. \$4.2 million, and that the City is responsible for 25% of the total costs.
- f) HPB member asked what the timeline for construction to be completed.
 - i) Nick stated that construction takes approx. 1 ½ years.
- g) HPB member stated that they are in favor of the oolitic limestone from the existing bridge be repurposed on the replacement bridge.
- h) HPB member asked of the bridge can be removed and preserved at a different location.
 - i) Nick stated that federal funding dictates only replacement of the existing bridge which includes demolishing the existing bridge.
- i) HPB member stated that they would like some of the oolitic limestone to be preserved by the plaque or as an end cap on the replacement bridge, or if a piece could be preserved for safe keeping during construction.
- j) HPB member asked if the bridge would be designed like the two entrance bridges at the entrance of the community.
 - i) Nick stated that the bridge design details would be worked out in the design phase and that the design was expected to start in July, but FDOT requires the City to pay 25% of the costs and sign a Locally Funded Agreement prior to starting design.
- k) HPB member asked who would be maintaining the bridge during the design phase.
 - i) Nick stated that the City would continue to maintain the bridge until construction begins.
- l) HPB member asked what the Area of Potential Effect (APE) and the other colored lines in the presentation represent.
 - i) Amy explained the details of the APE, as well as the cultural resource group and its elements.
- m) HPB member asked if the area around the bridge would be historically registered through the National Register of Historic Places (NRHP).
 - i) Amy explained that FDOT does not do NRHP designations but the board has the ability to begin that process. FDOT can provide the all the supporting documents related to the historic resource group. Amy further noted that if the bridge is removed, then that bridge would no longer be NRHP-eligible.
 - ii) Amy further noted that as part of mitigation, FDOT will prepare the Historic American Engineering Record (HAER) documents and the Section 106 memorandum of agreement (MOA). The Section 106 MOA requires FDOT requires to implement the mitigation prescribed in the MOA.
- n) HPB member asked if there any other properties that would be taken as part of this project.
 - i) Nick confirmed that there are no impacts to the adjacent property owners.
- o) HPB member asked once the project is advanced, what would be the involvement of this Board.
 - i) Nick stated that during the design phase, public involvement continues and includes coordinating with the public and this Board.
 - ii) HPB member stated that they want to make sure that the Board still has a say in the design of the of the replacement bridge.
 - iii) HPB member asked for provisions in the MOA that makes sure the Board is involved throughout the design process.
 - iv) Amy stated that the MOA is between the State Historic Preservation Officer (SHPO), and FDOT; the City could be a consulting party. Ongoing consultation will be required with this Board and the City.
 - v) Colleen stated that the SHPO is also considered an affected party as well.
- p) HPB member stated that they have been asked to sign a Certificate of Appropriateness (COA) presented by the City staff today and would like the Preferred Alternative reflected in the COA



- i) Colleen stated that as part of the NEPA process, they are still evaluating the alternatives. The study team has identified a Preferred Alternative, but it is not the Recommended Alternative at this time; once the study goes through a public hearing, receives concurrence from the SHPO, NMFS, and other agencies, then a Recommended Alternative will be approved by FDOT to move forward to design.
- ii) Colleen further noted that this meeting is to help define the mitigation measures and for the Board to provide input on the mitigation.
- q) HPB member asked where the construction staging would occur.
 - i) Nick stated that the bridge is not very long, but a staging area would be identified during design and construction.
- r) The HPB stated that they are excited about the improvements to the bridge, but that they will not sign the COA presented by the City staff today as they did not have time to process information provided in the COA.
- s) The City stated that the Board is only being asked to sign the COA, not the MOA and further noted that it is only an approval agreeing that the bridge does need to be improved.
 - i) The HPB stated that they would not sign this because they were concerned they would lose their leverage in the future with the City to have say over what happens to the bridge during future phases.
 - ii) The City stated that the Board has the option to not sign the COA and that City staff will go to the Commissioners for approval.
 - iii) The Board stated that this is the first time they have seen the COA and they need time to review. The Board is asking for a week from the City to review the COA.
 - iv) The City stated to the Board that they are not asking for a signature today but need approval from the HPB for the City to move forward in funding the bridge replacement.
- t) An HPB member asked if FDOT developed this language.
 - i) Colleen stated that the COA is a City document and was not developed or provided by FDOT.
 - ii) Nick stated that in most cases, the Locally Funded Agreement is signed prior to the study beginning.
- u) City asked if the Board can be provided additional time to review the COA.
 - i) Nick stated that the project is already being pushed back because the funding is not in place.
- v) The HPB and City agreed that the Board chairman will work with the City attorney to revise and review the COA.
 - i) The HPB voted and passed the motion to designate the HPB chairman to work with the City attorney in revisions and approval of the COA.

Meeting Concluded



AFFECTED PARTIES CONSULTATION #2 MEETING SUMMARY
Atlantic Isles Lagoon Bridge (Bridge No. 874218) PD&E Study
Located in Atlantic Avenue, City of Sunny Isles Beach, FL
Miami-Dade County, Florida
FPID# 430029-2 – In-House Project
07/21/23 via MS Teams

Staff Attendees: Victoria Vogt (FDOT), MD Hussain (FDOT), Steven Craig James (FDOT), Tish Burgher (FDOT), Amy Strelman (Janus), Joy Castro (Stantec), Ken Hardin (Janus), Colleen Ross (Jacobs), Michael Baker (Jacobs), Tara Jones (Jacobs), Alex Meitin (Jacobs), Max Imberman (Janus)

APC Attendees: Alyssa McManus (SHPO), Lindsay Rothrock (FDOT OEM), Marsha Welch (SHPO), Paola Martinez (Consultant Management), Rick Labensky (City of Sunny Isles Beach), Sarah Cody (Miami-Dade County Historic Preservation Officer), Susan Simpson (City of Sunny Isles Beach), Tamara McDonald (Miami-Dade County), Warren Stem (SIB Historic Preservation Board), Amy Canales (City of Sunny Isles Beach)

1. Introductions

- a. Introductions of the study team and APC attendees were made.

2. Presentation

- a. Monica, Tori, and Amy provided an update regarding the Atlantic Isles PD&E Study, the Section 106 process, adverse effects to the significant resources, and potential mitigation measures for the adverse effects to the Atlantic Island Bridge and resource group.

3. Question and Answer Session

- a. Marsha Welch stated that she does not have comments and enjoyed the presentation.
- b. Alyssa McManus stated that SHPO agrees with the mitigation measures presented.
- c. Lyndsay Rothrock asked if there is an education component included in the mitigation measures.
 - i. Amy stated that the historical marker would serve as an education component.
 - ii. Lyndsay agreed.
- d. Warren Stem asked if any communication with the City regarding the city's preferences and mitigation.
 - i. Tori stated that they have coordinated
 - ii. Warren stated that a representative from the City spoke with him recently and he wanted to see if there are provisions in the Certificate of Appropriateness for the HPB to be involved in the design phase.
 - 1. Tori stated that she will follow up with the City regarding those provisions.
 - iii. Warren stated that he missed the next steps slide and asked if those can be discussed again.
 - 1. Tori presented the next steps in the Section 106 process.
 - 2. Warren asked if there is anything still needed from the HPB at this time.
 - 3. Rick stated the city's next steps is to get the HPB to sign the COA.



- 4. Warren stated that he appreciates the information presented today and will be sure to follow-up with any additional questions.
 - iv. Tori confirmed that they will reach out to Warren following this meetign to ensure his feedback is received.
- 4. Closeout**
- a. Tori provided an overview of the schedule and next steps.
- 5. Meeting Concluded**
- 6. New Action Items**

New Action Items					
Item No.	Description	Assigned	Due	Completed	Notes

DRAFT

**APPENDIX C
DRAFT MEMORANDUM OF
AGREEMENT**

DRAFT

**MEMORANDUM OF AGREEMENT
BETWEEN THE FLORIDA DEPARTMENT OF TRANSPORTATION AND
THE FLORIDA STATE HISTORIC PRESERVATION OFFICER
REGARDING THE ATLANTIC ISLES LAGOON BRIDGE (FDOT BRIDGE NO.
874218) PROJECT DEVELOPMENT AND ENVIRONMENT STUDY, MIAMI-DADE
COUNTY, FLORIDA**

WHEREAS, Pursuant to 23 United States Code (U.S.C.) § 327 and the implementing Memorandum of Understanding (MOU) executed on May 26, 2022, the Florida Department of Transportation (FDOT) has assumed Federal Highway Administration's (FHWA) responsibilities under the National Environmental Policy Act (NEPA) for highway projects on the State Highway System (SHS) and Local Agency Program (LAP) projects off the SHS; and

WHEREAS, in accordance with the MOU, FDOT's assumption of FHWA's responsibilities under NEPA for highway projects includes assumption of responsibilities for compliance with 36 CFR Part 800, regulations implementing Section 106 of the National Historic Preservation Act (54 U.S.C. § 306108); and

WHEREAS, FDOT proposes to provide federal financial assistance for the Atlantic Isles Lagoon Bridge (FDOT Bridge No. 874218) (FPID No. 430029-2-21-01) (**the Project**) through a Locally Funded Agreement with the City of Sunny Isles Beach; and

WHEREAS, FDOT has determined that **the Project** represents an undertaking in accordance with 36 CFR § 800.3(a); and

WHEREAS, FDOT has defined **the Project's** area of potential effects (APE) for archaeological resources as the footprint of proposed ground disturbance and for historic resources as all parcels immediately adjacent to the proposed improvements.

WHEREAS, FDOT has consulted with the Florida State Historic Preservation Officer (SHPO) pursuant to the requirements of 36 CFR Part 800 and has determined that **the Project** will have an adverse effect on the Atlantic Island Bridge (8DA6433) and the Atlantic Island Resource Group (8DA19241), which are eligible for listing in the National Register of Historic Places (NRHP); and

WHEREAS, FDOT has consulted with the City of Sunny Isles Beach, City of Sunny Isles Beach Historic Preservation Board, and Miami-Dade County Office of Historic Preservation regarding the effects of **the Project** on historic properties; and

WHEREAS, FDOT has provided opportunities for public review and comment regarding the effects of **the Project** on historic properties; and

WHEREAS, in accordance with 36 CFR § 800.6(a)(1) FDOT has notified the Advisory Council on Historic Preservation (ACHP) of the adverse effect determination with specified documentation and has invited the ACHP to comment and participate in consultation, and the ACHP has chosen not to participate pursuant to 36 CFR § 800.6(a)(1)(iii); and

NOW, THEREFORE, FDOT and the SHPO agree that **the Project** shall be implemented in accordance with the following stipulations to consider the effect of **the Project** on historic properties.

STIPULATIONS

Prior to initiating any ground disturbing or demolition work associated with **the Project**, FDOT shall ensure that the following measures are carried out:

I. BRIDGE DESIGN

- A. FDOT will take into consideration the historic materials, visual profile, and design elements of the historic Atlantic Island Bridge when designing the replacement bridge.
- B. FDOT will provide the City of Sunny Isles and the SHPO opportunity to comment on the 60% and 90% design plans. All comments received will be considered during development of the replacement bridge design.

II. DOCUMENTATION OF THE ATLANTIC ISLAND RESOURCE GROUP (8DA19241) AND ATLANTIC ISLAND BRIDGE (8DA6433)

A. Prior to bridge demolition and per guidance provided by the National Park Service (NPS), FDOT will ensure that the resources are documented in accordance with the standards and guidelines of the Historic American Landscape Survey (HALS) and Historic American Engineering Record (HAER). For organizational purposes, all documentation will be grouped under HALS, but some components (as noted below) will follow HAER guidelines. The HALS documentation for the Atlantic Island Resource Group (8DA9241) and Atlantic Island Bridge (8DA6433) will be prepared as follows:

1. Written historical and descriptive data prepared in accordance with outline format guidelines placing the Atlantic Island Resource Group in the local and regional historical context and covering significant landscape design elements and features. The data will also contain significant construction history of the Atlantic Island Bridge, including the history of the bridge type, an architectural description of the resource including alterations, and a description of the site and changes; and
2. A site plan; and
3. Reproduction of selected historical photographs, if available; and
4. Large-format (4" x 5" or larger negative size) photographs processed for archival permanence in accordance with HALS photographic specifications. Views will include:
 - i. Significant landscape design elements and features.
 - ii. Views and vistas.
 - iii. Circulation patterns.

iv. Spatial organization.

5. At least one color digital photograph of the overall landscape design and select views in color to be included as figures at the end of the report, as well as at least one color digital photograph of the Atlantic Island Bridge resource and its setting; and

6. Photo locations keyed to the site plan and included with the “Index to Photographs.”

B. HAER documentation for the Atlantic Island Bridge (8DA6433) will include, but is not limited to:

1. Written historical and descriptive data prepared in accordance with outline format guidelines containing a construction history of the bridge including the history of the bridge type, an architectural description of the resource including alterations, and a description of the site and changes; and

2. Reproduction of selected historical photographs, if available; and

3. Large-format (4” x 5” or larger negative size) photographs processed for archival permanence in accordance with HAER photographic specifications; and

4. At least one color digital photograph of the resource and its setting; and

5. Photo locations keyed to the site plan and included with the “Index to Photographs.”

C. FDOT shall provide draft HALS documentation (non-archival format, electronic version) to the NPS and SHPO for concurrent review. SHPO shall have 30 days, after receipt of the draft documentation, for review, as per Stipulation VIII.

D. FDOT shall make requested edits and provide final copies of the HALS documentation, completed in accordance with Stipulation II.A, as follows:

1. An archival copy to the NPS Southeast Regional Office for review and approval prior to salvage and demolition of the structure, per HALS guidelines; and

2. An archival copy to the SHPO for inclusion in the Florida Master Site File and the State Archives of Florida; and

3. Non-archival copies and electronic copies to the City of Sunny Isles Beach, HistoryMiami, and the Florida Room of the Miami-Dade County Main Library.

E. Unless otherwise agreed to by the NPS or the SHPO, FDOT will ensure that all large-scale photographs of the resources are completed and accepted in writing by the NPS and SHPO prior to demolition.

III. HISTORICAL MARKER

A. FDOT will assist with the development and funding of one State Historic Marker (Marker) to be placed in proximity to the bridge location. The Marker will be located in an area that is safe and accessible to the public. The draft Marker text and location will be coordinated with the SHPO for review, as described in Stipulation VIII.

IV. PROFESSIONAL STANDARDS

A. All architectural history work carried out pursuant to this Agreement shall be conducted by, or under the direct supervision of, a person or persons meeting the Secretary of the Interior's Professional Qualifications Standards for Architectural History (62 FR 33708 – 33723, June 20, 1997) and all archaeological work shall be carried out by, or under the direct supervision of, a person or persons meeting the Secretary of the Interior's Professional Qualifications Standards for Archaeology (62 FR 33708 – 33723, June 20, 1997).

V. DURATION

A. This Memorandum of Agreement (MOA) will expire if its terms are not carried out within 5 years from the date of execution or project acceptance, whichever should come first. Prior to expiration, the parties may agree to extend the timeframe for fulfillment of the terms by letter agreement.

VI. MOA DOCUMENTATION

A. The FDOT shall prepare an Annual Report documenting actions carried out pursuant to this MOA. The reporting period shall be the fiscal year from July 1st to June 30th. The Annual Report shall be distributed to the consulting parties to this MOA for review as per Stipulation VIII. The distribution of the Annual Report shall occur by September 1st each year. The Annual Report shall address issues and describe actions and accomplishments over the past year, including, as applicable:

1. status of mitigation activities;
2. any issues that are affecting or may affect the ability of the FDOT to continue to meet the terms of this MOA; and
3. any disputes and objections received, and how they were resolved.

B. A final document will be prepared to summarize the implementation of the MOA after all stipulations have been fulfilled. This document will be submitted to the FDOT Office of Environmental Management (OEM) and SHPO for their files within six (6) months after completion of all MOA stipulations.

VII. POST-REVIEW DISCOVERIES

In accordance with 36 CFR § 800.13, FDOT will take the following actions if a post-review discovery is made:

A. If previously unidentified historic properties are discovered, or if the potential to affect previously identified historic properties changes after FDOT has completed their appropriate reviews under this Agreement, but before construction has started, FDOT shall reinstate consultation under Section 106 and Chapter 267, F.S.

B. If previously unidentified historic properties are discovered during construction or if unanticipated impacts to known or previously unidentified historic properties occur during construction, the following procedures shall be followed:

1. All construction-related activity in the vicinity of the discovery shall stop and the contractor shall immediately notify the Project Engineer and the FDOT District 6 Cultural Resources Coordinator of the discovery. Necessary security measures will be taken to protect the discovery as appropriate.
2. FDOT will notify the SHPO of the discovery and invite them to accompany FDOT staff (or consultants) to the location within forty-eight (48) hours of the discovery.
3. FDOT will immediately notify any Federally Recognized Native American Tribe (Tribe) that might attach religious and cultural significance to the affected property within forty-eight (48) hours of the discovery.
4. FDOT shall consult with the SHPO/THPO and appropriate consulting parties to document and evaluate the project effects and the need, if any, for further investigation within forty-eight (48) hours of the SHPO/THPO receipt of notification.
5. If FDOT determines that the discovery does not warrant further investigation, FDOT will provide written notification to the SHPO outlining FDOT's reasons and requesting their concurrence within two (2) business days of the visit to the discovery location. The SHPO/THPO and Tribes will have two (2) business days after receipt to respond. If no comments are received within this period, concurrence will be assumed, and project construction may resume.
6. If FDOT determines that the site warrants further investigation, a scope of work will be developed within forty-eight (48) hours of the site visit. The scope of work will be submitted to the SHPO and, as appropriate, the Tribes. The SHPO/THPO and Tribes will have two (2) business days after receipt to review and comment. If no comments are received within this period, concurrence will be assumed and work will be implemented in accordance with the scope. If comments are received, FDOT shall take them into account and carry out the scope of work. Upon completion and acceptance of the work, construction may proceed as planned. A report of the investigations will be completed within the time frame established by the scope of work and copies provided to all consulting parties. Should any party object to the proposed work plan or results, FDOT will proceed in accordance with Stipulation IX.
7. When the discovery consists of human remains, graves, or grave-associated artifacts or other properties that federally recognized Tribes with ancestral ties to Florida may ascribe with a traditional cultural or religious significance, FDOT-OEM will notify the Tribes.

FDOT will comply with Section 1.6 of the current version of the FDOT Standard Specifications for Road and Bridge Construction and the procedures for inadvertent discovery of human remains contained in Section 872.05, F.S. and Rule 1A-44 of the Florida Administrative Code.

VIII. REVIEW STIPULATION

FDOT shall afford the SHPO and other consulting parties, including the federally-recognized Tribes affiliated with Florida, a 30-day period for review and comment following the receipt of delivery of those submittals and reviews described above. If no comments are received by FDOT at the end of these 30 days, FDOT will presume there are no objections. Any objections to the findings or plans proposed in these submittals will be addressed in accordance with Stipulation IX, below.

IX. DISPUTE RESOLUTION

Should any signatory to this MOA object at any time to any actions proposed or the manner in which the terms of this MOA are implemented, FDOT shall consult with such party to resolve the objection. If FDOT determines that such objection cannot be resolved, FDOT will:

A. Forward all documentation relevant to the dispute, including FDOT's proposed resolution, to the ACHP. The ACHP shall provide FDOT with its advice on the resolution of the objection within thirty (30) days of receiving adequate documentation. Prior to reaching a final decision on the dispute, FDOT shall prepare a written response that considers any timely advice or comments regarding the dispute from the ACHP, signatories, and concurring parties, and provide them with a copy of this written response. FDOT will then proceed according to its final decision.

B. Make a final decision on the dispute and proceed accordingly if the ACHP does not provide its advice regarding the dispute within thirty (30) days. Prior to reaching such a final decision, FDOT shall prepare a written response that considers any timely comments regarding the dispute from the signatories to the MOA, and provide them and the ACHP with a copy of the written response.

C. Fulfill its responsibility to carry out all other actions subject to the terms of this MOA that are not the subject of the dispute remain unchanged.

X. AMENDMENTS

This MOA may be amended when such an amendment is agreed to in writing by all signatories. All signatories must signify their acceptance of the proposed changes to the MOA in writing within 30 days of their receipt. The amendment will be effective on the date a copy signed by all signatories is filed with the ACHP. In accordance with 36 CFR § 800.6(b)(7), if the ACHP was not a signatory to the original agreement and the signatories execute an amended agreement, FDOT shall file the amended agreement with the ACHP.

XI. TERMINATION

If any signatory to this MOA determines that its terms will not or cannot be carried out, that party shall immediately consult with the other signatories in an effort to amend the MOA per Stipulation X, above. If within thirty (30) days (or another time agreed to by all signatories) an amendment cannot be reached, any signatory may terminate the MOA upon written notification to the other signatories.

Once the MOA is terminated, and prior to work continuing on **the Project**, FDOT must either (a) execute an MOA pursuant to 36 CFR § 800.6 or (b) request, consider, and respond to the comments of the ACHP under 36 CFR § 800.7. FDOT shall notify the signatories as to the course of action it will pursue.

Execution of this MOA by FDOT and SHPO and implementation of its terms is evidence that FDOT has considered the effects of this undertaking on historic properties per the requirements of Section 106 (Public Law 113-287 [Title 54 U.S.C. 306108]), and 36 CFR Part 800 (Protection of Historic Properties).

SIGNATORIES:

FLORIDA STATE HISTORIC PRESERVATION OFFICER

_____ Date _____
Alissa S. Lotane
Director, Division of Historical Resources
State Historic Preservation Officer

FLORIDA DEPARTMENT OF TRANSPORTATION

_____ Date _____
Jennifer Marshall, P.E.
Director, Office of Environmental Management

FLORIDA DEPARTMENT OF TRANSPORTATION, DISTRICT 6

_____ Date _____
Daniel Iglesias, P.E.
Director, Transportation Development