



APPENDIX OCR-C: I-195 CPS, EXISTING CONDITIONS REPORT



EXISTING CONDITIONS REPORT I-195 Corridor Planning Study

I-95/NW 12th Avenue to Alton Road Miami-Dade County, Florida

Financial Management Number: 440228-1-22-01



Prepared for:



Prepared by:
BCC Engineering, Inc.
6401 SW 87th Avenue, Suite 200
Miami, FL 33173

October 2018

EXISTING CONDITIONS REPORT



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Project Study Limits:

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TABLE OF CONTENTS

1.0 INTRODUCTION 1

1.1 Project Description 1

1.2 Study Objectives..... 1

1.3 Study Area 1

1.4 Municipalities..... 3

2.0 TRANSPORTATION PLANS AND PREVIOUS STUDIES 4

2.1 Transportation Plans 4

2.1.1 Miami-Dade TPO 2040 Long Range Transportation Plan (LRTP) 4

2.1.2 Miami-Dade Transportation Improvement Program (TIP) 5

2.1.3 FDOT District 6 – Five Year Work Program 6

2.1.4 FDOT SIS – Ten Year Plan..... 7

2.1.5 FDOT SIS – Cost Feasible Plan 7

2.1.6 Southeast Florida’s Express Lanes Network RCTO 8

2.1.7 Strategic Miami Area Rapid Transit (SMART) Plan..... 10

2.1.8 Brightline Rail Project..... 11

2.1.9 Proposed North Beach Master Plan 11

2.2 Transportation Studies 12

2.2.1 I-95 Corridor Planning Study..... 12

2.2.2 FDOT Bicycles on Limited Access Facilities Pilot Program 14

2.2.3 Feasibility Study for Shared Use Path Along SR 112/I-195 Julia Tuttle Causeway 14

2.2.4 Safety Study for the Intersection of SR 907/Alton Road at 43rd Street 15

2.2.5 Analysis of Design District Connections to I-95 Express – White Paper 15

2.2.6 Biscayne Boulevard Safety Study from NE 32nd Street to NE 38th Street 16

2.2.7 Middle Beach Intermodal Center Feasibility Study 17

3.0 EXISTING LAND USE..... 18

3.1 Existing Land Use City of Miami 18

3.2 Existing Land Use City of Miami Beach 20

4.0 ROADWAY CHARACTERISTICS 21

4.1 Existing Typical Sections..... 21

4.1.1 I-195 from NW 14th Avenue to NW 10th Avenue 21

4.1.2 I-195 from NW 10th Avenue to NW 3rd Avenue 22

4.1.3 I-195 from NW 3rd Avenue to Biscayne Boulevard 23

4.1.4 I-195 from Biscayne Boulevard to Bridge No. 870314 24

4.1.5 I-195 from Bridge No. 870314 to Bridge No. 870302 25

4.1.6 I-195 from Bridge No. 870302 to Alton Road 26

4.1.7 I-95 from NW 22nd Street to NW 29th Street 26

4.1.8 I-95 from NW 29th Street to NW 43rd Street 26

4.1.9 I-95 from NW 43rd Street to NW 50th Street 27

4.1.10 I-95 from NW 50th Street to NW 54th Street 27

4.2 Existing Horizontal Alignment 27

4.3 Existing Vertical Alignment 27

4.4 Overhead Signage..... 27

4.5 Existing Structures Characteristics 32

4.5.1 Type of Structure	32
4.5.2 Condition of Existing Structures	32
4.5.3 Vertical Clearance.....	39
4.5.4 Horizontal Clearance.....	39
4.6 Intelligent Transportation Systems (ITS) Equipment	40
4.6.1 Pan-Tilt-Zoom (PTZ) Closed Circuit Television (CCTV) Cameras	40
4.6.2 Dynamic Message Signs (DMS).....	40
4.6.4 ITS Power Service Subsystem	40
4.7 Utilities	41
4.7.1 AT&T Florida	42
4.7.2 Atlantic Broadband	42
4.7.3 AT&T Transmission (AT&T Corp/TCA, LLC)	43
4.7.4 Centurylink / Level 3.....	43
4.7.5 City of Miami Beach	43
4.7.6 Comcast Cable	44
4.7.7 Crown Castle/Fibernet Direct	44
4.7.8 Florida Power & Light – Distribution.....	45
4.7.9 Florida Power & Light – Transmission.....	45
4.7.10 Hotwire Communications	46
4.7.11 Miami-Dade Water & Sewer Department	46
4.7.12 TECO People’s Gas	47
4.8 Pedestrian and Bicycle Facilities.....	49
4.9 Drainage System.....	54
4.9.1 I-195 (SR 112/Julia Tuttle Causeway) from NW 16 th Avenue to Alton Road	54
4.9.2 I-95 from NW 26 th Street to NW 62 nd Street	54
4.9.3 US 27 (NW 36 th Street) from NW 1 st Avenue to NE 5 th Avenue.....	54
4.9.4 US 1 (Biscayne Boulevard) from NE 35 th Terrace to NE 38 th Street.....	55
4.9.5 Alton Rd North and South from Chase Ave Mt. Sinai Driveway and 43rd St to Chase Ave55	
4.9.6 Regional Drainage Basin.....	55
4.9.7 Federal Emergency Management Agency (FEMA) Floodplain	55
5.0 EXISTING TRANSIT.....	56
5.1 Transit Ridership.....	61
6.0 TRAFFIC DATA.....	63
6.1 Origin-Destination Data Collection	63
6.2 72-Hour Traffic Station Volumes.....	68
6.3 72-Hour Classification Counts	69
6.4 Turning Movement Counts.....	69
6.4.1 Peak Spreading Review	69
6.4.2 5-Hour TMC Data Collection	69
6.5 Travel Time Runs	70
6.6 Traffic Signals	71
6.7 Field reviews	73

7.0 SAFETY ANALYSIS	76
7.1 Overall Corridor Review	76
7.2 Crash Analysis For Segments, Ramps And Clusters.....	81
7.2.1 Segment Crash Analysis	81
7.2.2 Ramp Crash Analysis.....	101
7.2.3 Cluster Analysis.....	104
8.0 ENVIRONMENTAL	110
8.1 Historic Resources	110
8.2 Parks and Recreational Facilities	110
8.3 Planned Community Growth/Land Use	111
8.4 Social	111
8.5 Contamination.....	112
8.6 Navigable Waterway.....	112
8.7 Water Quality and Wetlands.....	112
8.8 Endangered and Threatened Species, Wildlife and Critical Habitats.....	112
8.9 Essential Fish Habitat (EFH)	113
8.10 Floodplains.....	113
8.11 Air Quality and Noise	113

LIST OF EXHIBITS

Exhibit 1-1: Project Study Area	2
Exhibit 2-1: Southeast Florida Express Lane Network.....	9
Exhibit 2-2: I-95 Corridor Planning Study – Study Area	13
Exhibit 2-3: Proposed Julia Tuttle Typical Section with Shared Use Path on Bridges	15
Exhibit 2-4: Proposed 95 Express Lanes Connections to Design District	16
Exhibit 3-1: Existing Land Use Map – City of Miami	19
Exhibit 3-2: Existing Land Use Map – City of Miami Beach.....	20
Exhibit 4-1: Typical Section - I-195 from NW 14 th Avenue to NW 10 th Avenue	21
Exhibit 4-2: Typical Section - I-195 from NW 10 th Avenue to NW 3 rd Avenue	22
Exhibit 4-3: Typical Section - I-195 from NW 3 rd Avenue to Biscayne Boulevard.....	23
Exhibit 4-4: Typical Section - I-195 from Biscayne Boulevard to Bridge No. 870314	24
Exhibit 4-5: Typical Section - I-195 from Bridge No. 870314 to Bridge No. 870301	25
Exhibit 4-6: Typical Section - I-195 from Bridge No. 870301 to Alton Road	26
Exhibit 4-7: Location of Overhead Signage in Study Area	31
Exhibit 4-8: Location of Existing Bridges	36
Exhibit 4-9: Location of Existing Bridges	37
Exhibit 4-10: Location of Existing Bridges.....	38
Exhibit 4-11: Existing Bicycle Features - City of Miami.....	50
Exhibit 4-12: Existing Pedestrian Features - City of Miami	51
Exhibit 4-13: Existing Bicycle Features - City of Miami Beach	52
Exhibit 4-14: Existing Pedestrian Features - City of Miami Beach.....	53
Exhibit 5-1: Miami-Dade Bus Transit Routes In City of Miami	57
Exhibit 5-2: City of Miami Trolley Routes.....	58
Exhibit 5-3: Miami-Dade Bus Transit Routes In City of Miami Beach	59
Exhibit 5-4: City of Miami Beach Trolley Routes	60
Exhibit 6-1: Existing Traffic Data Collection Plan – Sheet 1 of 4	64
Exhibit 6-2: Existing Traffic Data Collection Plan – Sheet 2 of 4	65
Exhibit 6-3: Existing Traffic Data Collection Plan – Sheet 3 of 4	66
Exhibit 6-4: Existing Traffic Data Collection Plan – Sheet 1 of 4	67
Exhibit 6-5: Location of Existing Traffic Signals	72
Exhibit 7-1: Overall Crashes By Type (2011-2015)	78
Exhibit 7-2: Overall Crash Locations (2011-2015)	79
Exhibit 7-3: Overall Fatal Crash Locations (2011-2015)	80
Exhibit 7-4: Crash Summary and Segment Map (2011-2015)	82
Exhibit 7-5: Study Segment 1 Crashes By Type (2011-2015)	85
Exhibit 7-6: Study Segment 2 Crashes By Type (2011-2015)	88
Exhibit 7-7: Study Segment 3 Crashes By Type (2011-2015)	91
Exhibit 7-8: Study Segment 4 Crashes By Type (2011-2015)	94
Exhibit 7-9: Study Segment 5 Crashes By Type (2011-2015)	97
Exhibit 7-10: Study Segment 6 Crashes By Type (2011-2015).....	100
Exhibit 7-11: Location of Crash Clusters (2011-2015)	105

LIST OF TABLES

Table 2-1: Summary of Relevant TIP Projects	6
Table 4-1: Summary of Overhead Signage in Study Area.....	28
Table 4-2: Summary of Overhead Signage in Study Area (Contd.)	29
Table 4-3: Summary of Overhead Signage in Study Area (Contd.)	30
Table 4-4: Existing Bridge Characteristics Summary.....	33
Table 4-5: Existing Bridge Characteristics Summary.....	34
Table 4-6: Existing Bridge Characteristics Summary.....	35
Table 4-7: Existing Utility Agency Owners and Contact Information.....	41
Table 5-2: Transit Service Ridership.....	61
Table 6-1: I-195 Travel Time Summary	70
Table 6-2: Field Review Summary	73
Table 6-3: Field Review Summary	74
Table 6-4: Field Review Summary	75
Table 7-1: Overall Crash Statistics Summary (2011-2015)	77
Table 7-2: Segment 1: Confidence Levels (2011-2015)	83
Table 7-3: Study Segment 1 Crash Statistics Summary (2011-2015)	84
Table 7-4: Segment 2: Confidence Levels (2011-2015)	86
Table 7-5: Study Segment 2 Crash Statistics Summary (2011-2015)	87
Table 7-6: Segment 3: Confidence Levels (2011-2015)	89
Table 7-7: Study Segment 3 Crash Statistics Summary (2011-2015)	90
Table 7-8: Segment 4: Confidence Levels (2011-2015)	92
Table 7-9: Study Segment 4 Crash Statistics Summary (2011-2015)	93
Table 7-10: Segment 5: Confidence Levels (2011-2015)	95
Table 7-11: Study Segment 5 Crash Statistics Summary (2011-2015).....	96
Table 7-12: Segment 6: Confidence Levels (2011-2015)	98
Table 7-13: Study Segment 6 Crash Statistics Summary (2011-2015).....	99
Table 7-14: Ramps Crash Summary (2011-2015).....	102
Table 7-15: Crash Clusters Summary (2011-2015).....	104
Table 7-16: Crash Clusters Analysis (2011-2015).....	106
Table 7-17: Probable Causes.....	108
Table 7-18: Probable Causes.....	109
Table 8-1: Summary of Park and Recreational Facilities.....	111

1.0 INTRODUCTION

This Existing Conditions Report summarizes the existing data that have been collected or compiled for the I-195 Corridor Planning Study (CPS). Where noted, the report also contains an evaluation of key data elements including aerial photography, current transportation plans, typical cross sections, right-of-way, ITS equipment, utility infrastructure, drainage system, and public transportation data in regard to their relevance and the base information they will provide in developing improvements to address the future needs of the I-195 study corridor. The assessment of existing geometric (horizontal and vertical) conditions has been conducted at a level of detail consistent with the format of data obtained from the Florida Department of Transportation (FDOT) District 6 office.

1.1 Project Description

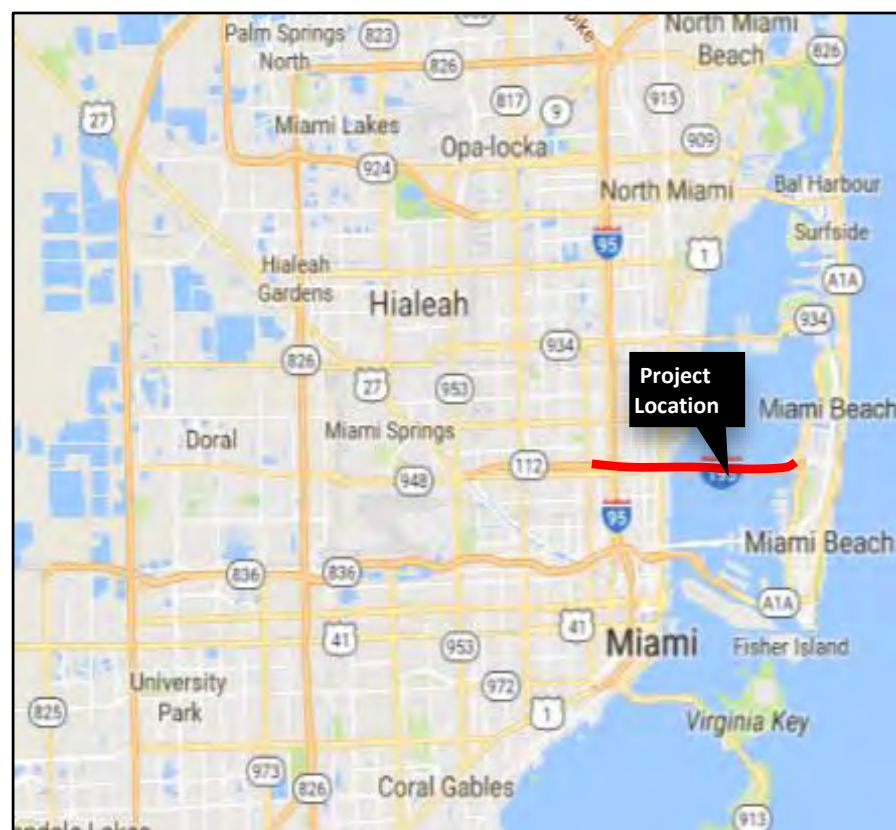
Interstate 195 (I-195) is an important limited access facility in Miami-Dade County providing a direct connection between Miami International Airport (via State Road 112 [SR 112]), Interstate 95, and the densely populated areas of Miami Beach. One of two limited access facilities in Miami-Dade County connecting the mainland to the barrier island, I-195/SR 112 carries approximately 130,000 vehicles daily. The corridor provides interchange access to several neighborhoods recently experiencing significant growth including the Design District, Midtown, and Wynwood in the City of Miami. Travel demand in this corridor, is expected to increase over the next 30 years due to continued growth that is anticipated within both the City of Miami and the City of Miami Beach. Opportunities for geometric expansion along the corridor are constrained due to limited right-of-way.

1.2 Study Objectives


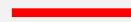
The primary study objectives are to evaluate existing conditions and deficiencies, identify needs, and develop and evaluate improvement concepts. The study includes the evaluation of study interchanges, interchange influence areas, and ramp junctions to identify deficiencies focusing on recurring bottlenecks and development of a series of proposed improvements to address existing and future long-term demands of the corridor. As necessary, multiple improvement alternatives will be developed for the mainline and system-to-system connections. The alternatives developed from this planning study are being coordinated and evaluated with the final I-95 Corridor Planning Study recommendations to ensure consistent operations. The I-195 CPS study is also evaluating the feasibility of implementing a long-term solution to provide a dedicated bicycle/pedestrian connection between the City of Miami Beach and the City of Miami.

1.3 Study Area

The following interchanges exist within the study limits: NW 12th Avenue (partial), Interstate 95 (system-to-system), North Miami Avenue (partial), Biscayne Boulevard (full), and SR 907/Alton Road (full). **Exhibit 1-1** on the next page, shows the project location and study limits.



LEGEND

-  Study limits
-  I-195/SR 112 Study Corridor

Project Name:



 **I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road** 
FM No. 440228-1-22-01

Exhibit Name:

Project Study Area

Report Title:

Existing Conditions Report

Exhibit No. 1-1

Page No.

Date: 09/04/18

1.4 Municipalities

The project corridor traverses the following municipalities within the Miami-Dade County: City of Miami and City of Miami-Beach.

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2.0 TRANSPORTATION PLANS AND PREVIOUS STUDIES

Transportation plans and previous studies relevant to the I-195 corridor were reviewed. The remainder of this section presents an overview of these plans and studies as well as an assessment of their relevance to the I-195 CPS.

2.1 Transportation Plans

The Transportation plans reviewed as part of this study include: the Miami-Dade Transportation Planning Organization (TPO) 2040 Long Range Transportation Plan (LRTP), Miami-Dade TPO Transportation Improvement Program (TIP), FDOT District 6 5-Year Work Program, FDOT Strategic Intermodal System (SIS) – Ten Year Plan, FDOT SIS – Cost Feasible Plan, Southeast Florida’s Express Lanes Network Regional Concept of Traffic Operations (RCTO), Miami-Dade Strategic Miami Area Rapid Transit (SMART) Plan and the City of Miami Beach Proposed North Beach Master Plan.

2.1.1 Miami-Dade TPO 2040 Long Range Transportation Plan (LRTP)

The LRTP is the Long-Range Transportation Plan that address the planning needs of the Miami-Dade Urban planning area within a 20-year planning horizon. The Plan establishes the funding priorities for cost-feasible multimodal transportation projects and is updated every 5 years to reflect the long-range initiatives consistent with federal, state and local goals. The 2040 LRTP was adopted in October 2014. The LRTP establishes the revenue forecasts for the funding priorities according to the following schedule:

- Priority I: TIP (2015-2019) and 2020
- Priority II: 2021-2025
- Priority III: 2026-2030
- Priority IV: 2031-2040

The following projects within the project study limits were identified using the LRTP Interactive Project Tool:

- Biscayne Boulevard Bicycle / Pedestrian Improvements between NE 36th Street and NE 38th / 39th Street. Project funding priority and phasing are identified as follows, Priority II – Construction.
- NE 2nd Avenue Bicycle / Pedestrian Improvements between NE 20th Street to NE 36th Street. Project funding priority and phasing are identified as follows, Priority II – Construction.
- NW 2nd Avenue Bicycle / Pedestrian Improvements between NW 20th Street and NW 79th Street. Project funding priority and phasing are identified as follows, Priority II – Construction.
- Interchange at I-95 at I-195 roadway ramp signage improvements congestion management project.
- I-95 improvement project from south of SR 836/I-395 to Broward County line. Preliminary Engineering phase is Priority I in the LRTP.
- N Miami Avenue Arterial Collector Roadway Improvements from NW 14th Street to Miami City Limits. Project funding priority and phasing are identified as follows, Priority IV – Preliminary Engineering, Construction and Operations & Maintenance.

- NW 7th Avenue Enhanced Bus Premium limited stop transit service from Downtown Miami to Golden Glades Interchange Terminal. Project funding priority and phasing are identified as follows, Priority II – Right-of-way / Construction, Priority III – Construction / Operations and Maintenance, Priority IV – Operations and Maintenance.
- I-195 ramps in Midtown (NE 36th & NE 38th Streets) - Ramp reconstruction/reconfiguration of ramps between I-195 and NE 36th and NE 38th Streets as a Priority III project for Preliminary Engineering and Construction.

2.1.2 Miami-Dade Transportation Improvement Program (TIP)

The Miami-Dade TPO's Transportation Improvement Program (TIP), outlines the programmed transportation improvements within the planning region slated for the next five years and is consistent with the projects as well as priorities in the currently adopted LRTP. Federally funded projects as well as other major projects which are part of the TPO's program of improvements (but not federally funded), are also included in the TIP. The current TIP encompasses the period from October 1, 2018 to September 30, 2023 and was approved on June 21, 2018 by the Miami-Dade TPO governing Board. **Table 2-1** on the next page presents a summary of projects within the project study limits that are in the current TIP:

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Table 2-1: Summary of Relevant TIP Projects

Project ID*	Implementing Agency	Project Description	Funding and Phasing
XA11212	MDX	SR 112 Ramp Improvements at NW 37th Avenue - New partial interchange at SR 112 and NW 37th Avenue. New movements include eastbound on-ramp at NW 37th Avenue as well as a westbound off-ramp at NW 37th Avenue.	<ul style="list-style-type: none"> o Planning - Fiscal Years 2018 and 2019. o Right-of-way - Fiscal Years 2018, 2019 and 2020. o Design/Build – Fiscal Years 2019, 2020 and 2021.
DT4293003	FDOT	SR 9A / I-95, From NW 29th Street to NW 131st Street, rigid pavement reconstruction.	o Design/Build – Fiscal Year 2018
DT4154561	FDOT	SR 9A / I-95 Express lane improvements to add special use lane, From north of SR 836 / I-395 to Golden Glades Interchange.	o Design/Build phase completed in prior Fiscal Year 2018
DT4308171	FDOT	SR 7 / NW 7th Avenue resurfacing from NW 36th Street to north of NW 78th Street	o Construction phase completed in prior Fiscal Year 2018
PW000925	M-D DTPW	N Miami Avenue resurfacing from NE 17th Street to NE 87th Street	o Construction phase completed in prior Fiscal Year 2018
DT4358431 [†]	FDOT	SR 112 / I-195 Frontage Road & Ramp Realignment and new road construction (Miami Design District).	o Construction – Fiscal Year 2020
PW000982	M-D DTPW	NE 2nd Avenue and NE 36th Street Intersection Improvement	o Construction phase completed in prior Fiscal Year 2018
PW000306a	M-D DTPW	NE 2nd Avenue from NE 20th Street to NE 36th Street, traffic operational improvements.	<ul style="list-style-type: none"> o Preliminary Engineering - Fiscal Years 2018, 2019, and 2020. o Construction – Fiscal Years 2018, 2019 and 2020.
DT4291931	FDOT	SR 907 / Alton Road flexible pavement reconstruction, from Michigan Avenue to south of Ed Sullivan Drive / 43rd Street.	o Construction – Fiscal Year 2021
MU000055	Miami Beach	41st Street Corridor Improvements from Alton Road to Collins Avenue	Not Available

Notes:

* "XA" = Miami-Dade Expressway Authority, "DT" = Florida Department of Transportation, "PW" = Miami-Dade Department of Transportation and Public Works, "MU" = Municipality (City of Miami-Beach).

† It should be noted that this project would be dropped if Miami-Dade County receives approval to move forward with another project to convert the partial interchange at N Miami Avenue to a full interchange by adding ramps to and from the east.

2.1.3 FDOT District 6 – Five Year Work Program

FDOT's District 6 Work Program contains a list of funded transportation projects that are programmed for implementation within District 6 (including Miami-Dade and Monroe counties) during the next five years. It is developed annually with projects identified and scheduled based on priorities and allocated funds. In each cycle, a new fifth year is added, and the first year drops off as projects are completed. A new project only begins to move forward after it is funded and then placed in the Work Program's fifth year. Projects in the FDOT Work Program are incorporated into the 5-year Miami-Dade TIP as part of the annual TIP Development process. All the FDOT projects listed in the preceding Section 2.1.2 (as noted in the TIP) are also included in the current FDOT Five-Year Work Program.

2.1.4 FDOT SIS – Ten Year Plan

FDOT's SIS Ten Year Plan produced by the FDOT Systems Implementation Office, represents a subset of the SIS Funding Strategy which includes the SIS Work Program (1st Five-Year Plan), SIS 2nd Five-Year Plan, SIS Cost Feasible Plan, and the SIS Multimodal Unfunded Needs Plan. The combination of the 1st Five-Year and 2nd Five-Year Plans is known as the SIS 10-Year Plan. The First Five Year Plan illustrates projects on the SIS that are funded by the legislature in the Work Program (Year 1) and projects that are programmed for proposed funding in the next 2 to 5 years. The Second Five Year Plan illustrates projects that are planned to be funded in the five years (Years 6 through 10) beyond the Adopted Work Program. Projects in the Second Five Year Plan can move forward into the First Five Year Plan as funds become available. Projects in the SIS Ten Year Plan include:

- The PD&E phase for improvements to I-95 from south of SR 836/I-395 to Broward County Line is included in Fiscal Years 2019 and 2023 as part of the District 6 SIS Interstate Plan.
- SR 112/I-195 Frontage Road & Ramp Realignment (Miami Design District) with funding in Fiscal Years 2019, 2020 and 2021 for preliminary engineering as part of the District 6 SIS Non-Interstate Plan.

2.1.5 FDOT SIS – Cost Feasible Plan

The FDOT SIS Cost Feasible Plan illustrates projects on the SIS that are considered financially feasible during the last 15 years (years 11 to 25) of the State's Long-Range Plan, based on current revenue forecasts. Projects in this plan can move forward into the Second Five as funds become available or back into the SIS 2045 Multimodal Unfunded Needs Plan if revenues fall short of projections. The SIS Cost Feasible Plan is typically updated every 3 to 5 years as new revenue forecasts become available. Projects in the SIS Cost Feasible Plan include:

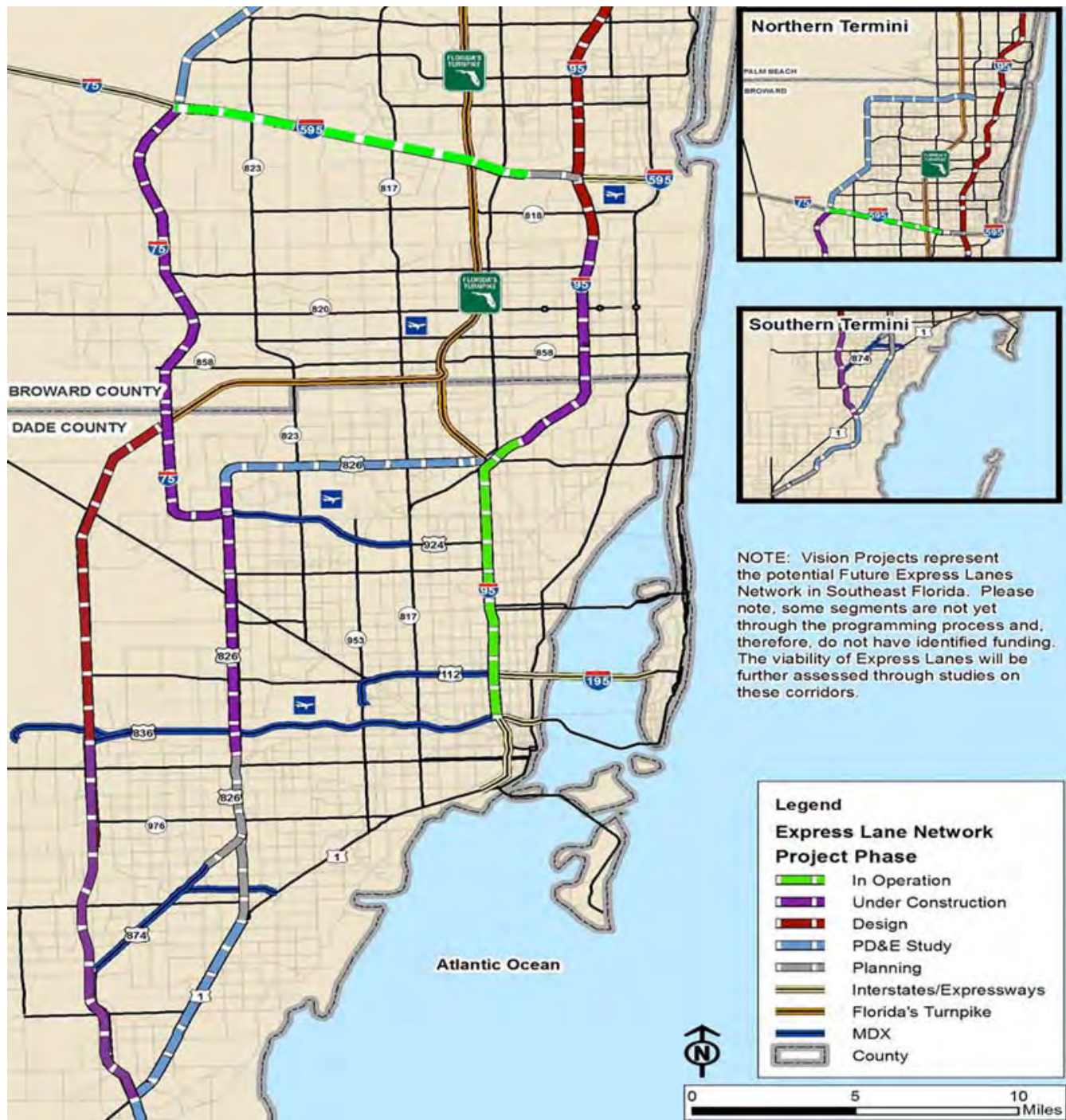
- The Preliminary Engineering (PE) phase for the managed lanes capacity improvements to I-95 from U.S. 1 to Broward County Line is included in Fiscal Years 2029 to 2045.

2.1.6 Southeast Florida's Express Lanes Network RCTO

The Southeast Florida's Express Lanes Network (ELN) Regional Concept for Transportation Operations (RCTO) is intended to reflect the policies, operational guidelines and goals for how the ELN will operate regionally and how to achieve those mutually agreed upon goals. The RCTO is a multiagency initiative that develops and refines the roles and responsibilities of each agency and how the regional system will operate. A key goal of the ELN is to optimize the performance of the roadway corridors, enhance safety, meet the needs of ELN customers, and reduce congestion regionally. The ELN is intended to provide choices for the traveling public, increase trip reliability and additional travel options for Bus Rapid Transit (BRT) service along the network. **Exhibit 2-1** on the next page, shows Southeast Florida's ELN including those parts of the network that are either in operation, under construction, under design, in the PD&E phase or are currently being planned. The map shows that I-95 has express lanes that are in operation running through the interchange north and south of I-195.

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Exhibit 2-1: Southeast Florida Express Lane Network



NOTE: Vision Projects represent the potential Future Express Lanes Network in Southeast Florida. Please note, some segments are not yet through the programming process and, therefore, do not have identified funding. The viability of Express Lanes will be further assessed through studies on these corridors.

Source: South Florida Express Lanes Network website: <http://www.95express.com/pages/related-info/south-florida-express-lanes-network>

2.1.7 Strategic Miami Area Rapid Transit (SMART) Plan

The SMART Plan was adopted on April 21, 2016 by the Miami-Dade TPO Board as a top priority to promote the advancement of rapid transit corridors and transit supportive projects across the county. The SMART Plan will expand transit options in Miami-Dade County along six critical corridors that are linked to regional, state, national, and global economic markets. Another critical component of the SMART Plan is a network of Express Buses, known as Bus Express Rapid Transit (BERT), which will connect the SMART rapid transit corridors on limited access facilities, promoting the active expansion of South Florida's Express Lanes network. Following is a summary of the six corridors that are being planned:

- **Beach Corridor** from Midtown Miami to the Miami Beach Convention Center. The Miami-Dade Department of Transportation and Public Works (DTPW) is the lead agency for a PD&E study that is being conducted for the implementation of the Beach Corridor Rapid Transit Project.
- **East-West Corridor** from Miami Intermodal Center to Florida International University. The Miami-Dade DTPW is the lead agency for a PD&E study that is being conducted for the implementation of the East-West Corridor Rapid Transit Project.
- **Kendall Corridor** from Dadeland area Metrorail Stations to Krome Avenue. The FDOT District 6 is the lead agency for a PD&E study that is being conducted for the implementation of the Kendall Corridor Rapid Transit Project.
- **North Corridor** from Martin Luther King, Jr. Metrorail Station to NW 215th Street. The FDOT District 6 is the lead agency for a PD&E study that is being conducted for the implementation of the North Corridor Rapid Transit Project.
- **Northeast Corridor** from Downtown Miami to the City of Aventura. The FDOT Districts 6 and 4 are the lead agencies for a PD&E study for the implementation of the Northeast Corridor Rapid Transit Project. This PD&E study is currently on hold.
- **South Dade Transitway** from Dadeland South Metrorail Station to SW 344th Street Transit Terminal (Florida City). The Miami-Dade DTPW is the lead agency for a PD&E study that is being conducted for the implementation of the South Dade Transitway Rapid Transit Project.

Appendix A, shows the proposed SMART plan network including the planned 6 rapid transit corridors. Of the six rapid transit projects, the Northeast Corridor route will have a direct impact within the study area running along I-95 through the interchange with I-195.

Along with the six rapid transit corridors, a complementary network of BERT services is planned to include the Beach Express (divided into North, Central and South routes), the Flagler Corridor, Florida's Turnpike Express (divided into North and South routes), Northwest Miami-Dade Express, South Miami-Dade Express and Southwest Miami-Dade Express. **Appendix A** shows the proposed BERT network. The Beach Express route will have a direct impact within the study area running between the Miami Beach Convention Center and the Golden Glades along I-95 via I-195 and the Julia Tuttle Causeway.

2.1.8 Brightline Rail Project

The All Aboard Florida (AAF) Passenger Rail Project (aka Brightline) got underway in 2012/2013 with the objective of implementing a privately owned and operated intercity passenger high-speed rail service connecting Miami to Orlando, with intermediate stops in Fort Lauderdale and West Palm Beach, Florida. Service implementation was proposed in two phases:

- Phase I would provide passenger rail service along the 66.5 miles of the Florida East Coast Rail (FECR) Corridor connecting Miami, Fort Lauderdale, and West Palm Beach. Service would run nonstop between three newly constructed stations in Downtown Miami, Downtown Fort Lauderdale and Downtown West Palm Beach.
- Phase II would focus on the remaining section from West Palm Beach the Orlando

Following a comprehensive planning effort, stakeholder coordination, project approval and station construction, Phase 1 of the Brightline Service between Miami and West Palm Beach was launched on May 19, 2018. The Miami station known as MiamiCentral, is a 9-acre mixed used complex that includes 3 million square feet of residential, office, commercial, and retail development. The station is in the Government Center district of Downtown, Miami, to the south of the I-195 Corridor Planning Study area. Transit connections are provided at the MiamiCentral station to several Miami-Dade transit routes that run through the I-195 planning study area including, MDT route #s 2, 3, 9, 21, 77, 93, 95 and 246 (ref. **Exhibit 5-1** in **Section 5.0** of this report). Brightline service includes 16 round trips on Monday through Friday, eight on Saturdays and seven on Sundays, between Miami and West Palm Beach. Albeit with trains traveling at high speeds, the Brightline service will lead to increased rail crossings at study intersections of NE 36th Street with Federal Highway and at NE 39th Street with Federal Highway.

2.1.9 Proposed North Beach Master Plan

The City of Miami Beach proposes an amendment to its Comprehensive Plan to increase density in the North Beach Town Center area via a Revitalization Overlay. The comprehensive plan amendment would increase the Floor Area Ratio (FAR) to 3.5 for the Town Center Core Category (TC-1), TC-2, and TC-3 zoning districts. This represents an increase in the FAR for TC-1, which is currently between 2.25 and 2.75; an increase for TC-2 which is currently 2.0; and an increase for TC-3 which is currently 1.25. The proposed density limits for TC-1 are 150 dwelling units per acre; for TC-2 the density limits are 100 dwelling units per acre; and for TC-3 the density limits are 60 dwelling units per acre. The North Beach Town Center Revitalization Overlay is generally bounded by 69th Street to the south, Indian Creek Drive to the west, 72nd Street to the north, and Collins Avenue to the east. It consists of approximately 22.6 acres.

2.2 Transportation Studies

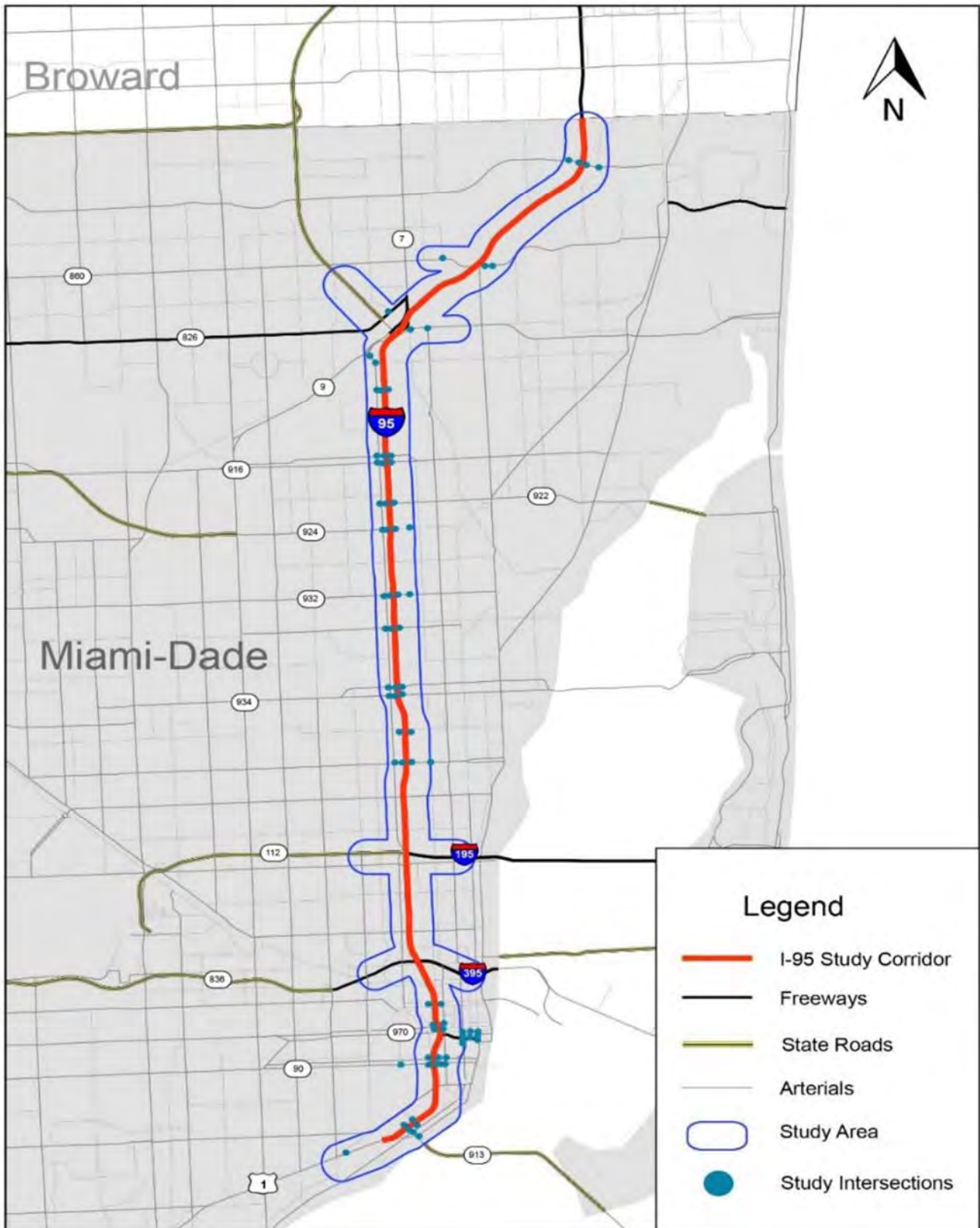
The Transportation studies reviewed as part of this study include: the ongoing I-95 Corridor Planning Study, FDOT Bicycles on Limited Access Facilities Pilot Program report, Safety Study for the Intersection of SR 907/Alton Road at 43rd Street, Analysis of Design District Connections to I-95 Express – White Paper, Biscayne Boulevard Safety Study from NE 32nd Street to NE 38th Street and the Middle Beach Intermodal Center Feasibility Study conducted for the City of Miami Beach.

2.2.1 I-95 Corridor Planning Study

I-95 is the busiest corridor in South Florida with over 250,000 vehicles traversing the corridor daily in Miami-Dade County. The I-95 Corridor Planning Study (CPS) is ongoing and a detailed planning-level analysis is being conducted for the I-95 corridor within Miami-Dade County in order to develop improvement concepts. The southern project limit is SR-5/US 1/Dixie Highway at SW 16th Avenue in the City of Miami, approximately one-half mile south of the beginning of I-95. The northern project limit is the Broward/Miami-Dade County line. This project overlaps the I-195 CPS planning area within the I-95 and I-195 interchange. **Exhibit 2-2** on the following page shows the study area of the I-95 CPS.

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Exhibit 2-2: I-95 Corridor Planning Study – Study Area



Source: I-95 Corridor-wide Planning for Operational Deficiencies U.S. 1/SR 5 to Broward County Line Final Report | Cambridge Systematics, April 3, 2017

2.2.2 FDOT Bicycles on Limited Access Facilities Pilot Program

During the 2012 Florida legislative session, law makers established a pilot program in Florida Statute 316.091, to study the feasibility of allowing bicyclists to travel on limited access bridges. The law required that FDOT establish a two-year pilot program, in three separate urban areas, allowing bicycle access on highway approaches and bridge segments of limited access highways crossing bodies of water. Prior to this legislation, bicycles were prohibited from operating on limited access roadways or bridge sections.

FDOT considered the population within five miles of the highway approach and bridge segment, lack of bicycle access by other means, cost, safety and operational impacts when selecting the pilot projects. Three corridors were selected for the pilot project including:

- Pineda Causeway (SR 404), Brevard County/Cape Canaveral National Seashore,
- William Lehman Causeway (SR 856), Aventura/Sunny Isles Beach, Miami-Dade County and,
- Julia Tuttle Causeway (I-195), Miami Beach/Miami, Miami-Dade County.

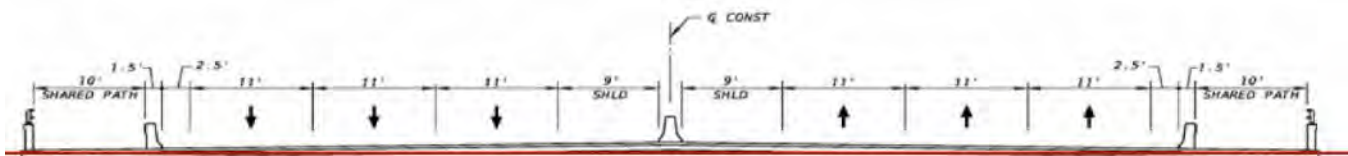
The selection was based on unique characteristics of these limited access corridors, making them good candidates for added bicycle facilities. The selected pilot corridors featured existing bicycle activity prior to bicycles being permitted on each facility. The corridor selection was supported by local cycling interest groups and the respective metropolitan planning organizations.

The three pilot corridors were modified in 2012-2013 to add bicycle lane pavement markings and signs showing bicycle travel was permitted. Each of the corridors was opened to bicycle travel by March 2013. Counts of bicycle activity were conducted before, during, and after the corridors were upgraded for bicycle use. The pilot project on the Julia Tuttle is still ongoing and may remain in effect until a more permanent solution is developed to address the future Bicycle needs along the corridor.

2.2.3 Feasibility Study for Shared Use Path Along SR 112/I-195 Julia Tuttle Causeway

In December 2015, a study was performed by C.H. Perez & Associates for FDOT to determine the feasibility of installing Shared-Used Path facilities along I-195 / SR-112 / Julia Tuttle Causeway from SR-5 / US-1 / Biscayne Boulevard to SR-907 / Alton Road. The study evaluated existing field conditions, existing typical section(s), horizontal and vertical geometry, and volume of pedestrians/bicyclists movements to determine the feasibility of providing shared used paths within the existing typical section(s) or to provide alternatives that would provide a safer corridor for pedestrians and bicyclists. The study recommended a directional shared use path along the north and south sides of I-195 behind the existing guardrail. At the bridge approaches, a break in the guardrail was proposed to allow the path to align with the existing paved shoulder along the roadway, transitioning onto the bridges. Where the path transitions onto the shoulder, a barrier was recommended to separate the motorists from the bicyclists and pedestrians. To accommodate a 10' shared path, 1.5' barrier wall, and 2.5' shoulder, the existing 12' lanes were proposed to be reduced to 11' in width and the inside shoulder were proposed to be reduced from 10' to 9'. The inclusion of a bike box at the northbound approach to the Alton Road at 43rd Street intersection was recommended for analysis during the final design. **Exhibit 2-3** on the following page illustrates the proposed typical section that was recommended from that study.

Exhibit 2-3: Proposed Julia Tuttle Typical Section with Shared Use Path on Bridges



Source: Feasibility Study to Include Shared Use Path Along SR 112 / I-195 / Julia Tuttle Causeway | CH Perez & Associates, December 17, 2015

Appendix A, contains the concept layout for the proposed shared use path. Since the competition of this feasibility study, no project development activities have been initiated towards the implementation of any of the recommendations identified in the study. The further evaluation of potential bicycle and pedestrian facilities along the Causeway is being conducted as part of the I-195 CPS.

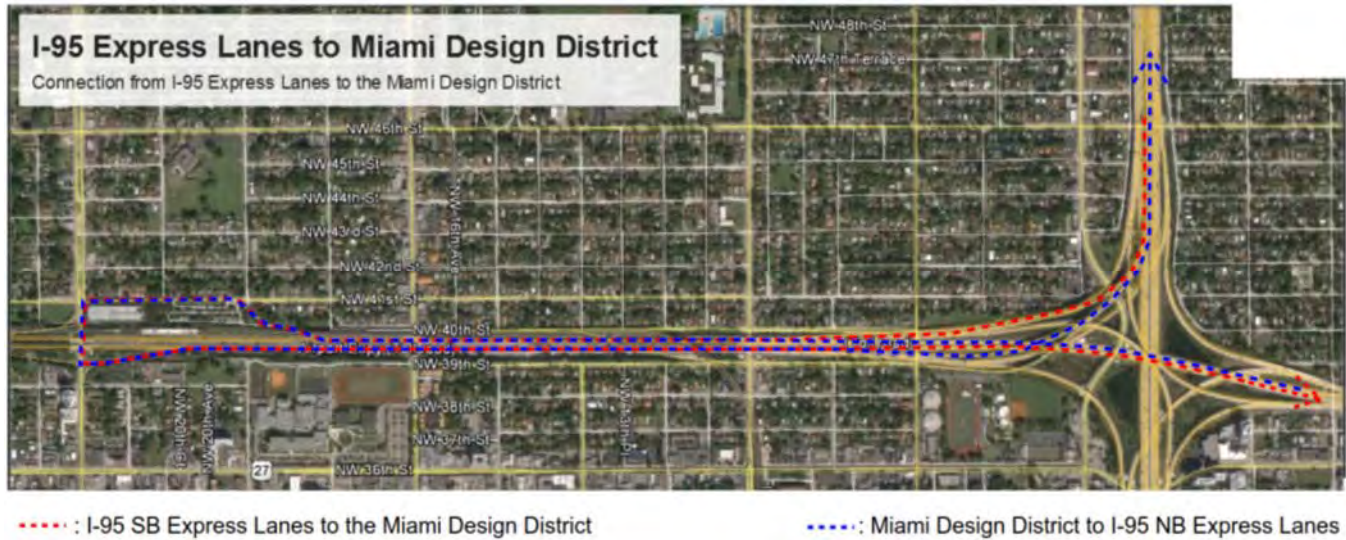
2.2.4 Safety Study for the Intersection of SR 907/Alton Road at 43rd Street

In April 2016, a safety study was performed for the intersection of SR 907/Alton Road and 43rd Street, located in the City of Miami Beach, Miami-Dade County, Florida. The study limits also included the segment of southbound Alton Road from 43rd Street to the on-ramp for westbound I-195. The study was initiated following complaints raised by representatives of Mount Sinai Medical Center regarding traffic congestion experienced at the study intersection. The study investigated potential safety and operational improvements at the Alton Road/43rd Street intersection and evaluated the feasibility of widening the on-ramp to westbound I-195 from one lane to two lanes. The study recommended that the FDOT consider moving forward towards the widening of the westbound on-ramp from Alton Road to I-195 from one to two lanes. The recommendations from this study resulted in the initiation of project development activities for the SB Alton Road to WB I-195 ramp improvement project which is programmed under FM#4304443, with construction beginning in May 2021.

2.2.5 Analysis of Design District Connections to I-95 Express – White Paper

In February 2016, a white paper was prepared to investigate the feasibility of providing access between the Design District and the I-95 Express lanes utilizing SR 112 and I-195. Two connections were evaluated. The first connection included a Texas U-Turn on the east side of NW 10th Avenue via a new alignment that would originate from westbound SR 112 immediately after the existing pier for the eastbound SR 112 connecting to the northbound I-95 Express fly-over. The second connection included a Texas U-Turn like the one described for the opposite movement. In this case, the Texas U-Turn lane would be provided along NW 12th Avenue by lengthening the existing westbound SR 112 bridge. **Exhibit 2-4** on the next page, is a schematic depiction of both connections as presented in the white paper.

Exhibit 2-4: Proposed 95 Express Lanes Connections to Design District



Source: Analysis of Design District Connections to 95 Express | AECOM, February 17, 2016

The white paper concluded that further evaluation was needed to confirm the feasibility of these connections prior to moving forward. The scope of this further evaluation is being reviewed as part of the I-195 CPS.

2.2.6 Biscayne Boulevard Safety Study from NE 32nd Street to NE 38th Street

In January 2014, a safety study was performed for SR 5/Biscayne Boulevard from north of NE 32nd Street to south of NE 38th Street in Miami-Dade County. This study was initiated subsequent to a review of the 2011 5 percent Segment High Crash List. To mitigate crash patterns that were identified as part of the safety review, the following major improvements were identified:

- Build a raised median along SR 5 between south of NE 32nd Street and NE 36th Street.
- Physically restrict northbound/southbound left-turn, and eastbound/westbound left-turn/through movements at NE 34th Street and NE 35th Terrace.
- Physically restrict southbound left-turn, eastbound through/left-turn, and westbound through movements at NE 35th Street.
- Provide a standard 4' traffic separator, along the east leg of the intersection of SR 5 and NE 36th Street.

Other minor safety improvements were identified to complement the implementation of the aforementioned improvements which while addressing existing safety issues, had the effect of modifying the existing circulation. **Appendix A** shows the improvements that were proposed as part of this safety study. The study resulted in a construction project (FM#4330592) which was completed in April 2018. It should be noted that at the time of writing this report, a neighborhood meeting was held with the FDOT construction team in which numerous questions and concerns related to the increased circuitry of the circulation that has resulted from these improvements.

2.2.7 Middle Beach Intermodal Center Feasibility Study

The City of Miami Beach conducted a study to explore the feasibility of constructing an intermodal center adjacent to Julia Tuttle Causeway (I-195/ SR 112) at Alton Road. The 4.8-acre site is owned by the State of Florida and is used by the FDOT as a debris management site. Miami Beach wishes to transform the site into a central transportation hub for regional and local transit connections to enhance mobility and connectivity in the region. The following summarizes key elements of the proposed facility:

- Transit hub with 10 transit bays (six for 60-foot articulated buses and four for standard buses) and a central passenger platform
- Covered passenger waiting area
- Driver comfort station
- Passenger comfort station
- Retail kiosks on the passenger platform
- Bicycle parking and lockers, with room for bike sharing operation
- Taxi/rideshare parking area
- Kiss-and-Ride drop-off/waiting area
- Handicapped parking
- Surface parking spaces (75 minimum)
- Future 1,000 space parking garage
- Carshare spaces

The study generated a conceptual design plan and the Middle Beach Intermodal Center functional requirements but did not identify a preferred design as FDOT's approval to use the site has not been finalized. Consensus among the study's stakeholders about the required functionality of the intermodal center are reflected in the feasibility study. However, it should be noted that FDOT District Six determined that the site was not available for this proposed intermodal center since it serves as a key staging area for debris processing as well as construction activities related to three Alton Road reconstruction projects. The unavailability of this site was communicated to the City of Miami Beach by FDOT in a letter dated July 11, 2017.

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3.0 EXISTING LAND USE

As noted in **Section 1.4** of this report, the study corridor is located within two municipalities including the City of Miami on the west end and the City of Miami Beach on the east end. Information from the Planning and Zoning Department in each municipality was compiled and is summarized in this section of the report.

3.1 Existing Land Use City of Miami

The portion of the study area that falls within the City of Miami comprises various land use types including residential, commercial retail, office and industrial uses. The area east of Biscayne Boulevard and south of I-195 mainly comprises, high-rise residential developments including the Blue and Charter Club Condominiums. The area between Biscayne Boulevard and N Miami Avenue comprises, the Miami Design District, the historic Buena Vista neighborhood, the mixed-use Midtown Miami urban infill development, and industrial land uses. The area, between N Miami Avenue and just west of NW 12th Avenue mainly consists of single-family residential neighborhoods. Various boundaries are created by transportation networks traversing this portion of the study area which limit the interaction between some of the land uses. North-south mobility between land uses is constrained by the presence of the I-195 corridor running through the center of the study area. East-west mobility towards the west of the study area is constrained by the I-95 corridor. East-west vehicular and pedestrian mobility (on the east side just west of the Intracoastal Waterway) between the Biscayne Boulevard and areas to the west, are constrained by the presence of the FEC rail corridor which runs parallel to Federal Highway. It is anticipated that the burgeoning design district together with Midtown Miami, a major mixed-use development of local and regional significance, will continue to generate increased attractions to the area. A plethora of redevelopment activity continues within the study area and its environs which will result in increased demands on the transportation system. **Exhibit 3-1** on the next page shows the land uses within the area.

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Source: City of Miami Planning and Zoning

Legend

Existing Land Use

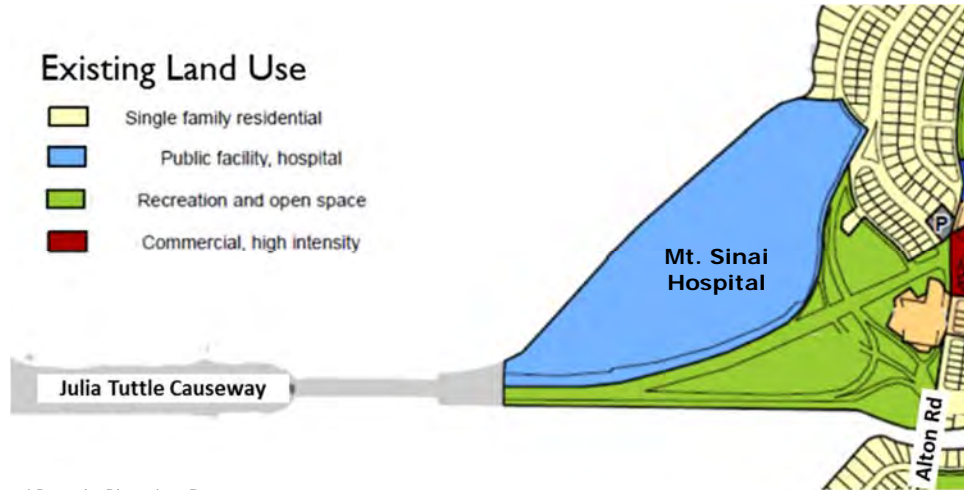
- Single-Family
- Two-Family Duplexes
- Mobile Home Parks
- Townhouses
- Low-Density Multi-Family
- High-Density Multi-Family
- Residential-Government Owned Housing
- Transient-Residential (Hotels, Motels)
- Commercial, Shopping Centers, Stadia
- Office
- Mixed Use-Business/Residential
- Institutional
- Industrial Extraction
- Industrial
- Industrial Intensive, Office type of use
- Industrial intensive, Commercial Condominium type of use

Study Area

3.2 Existing Land Use City of Miami Beach

The portion of the study area that falls within the City of Miami Beach comprises various land use types including residential, commercial retail, Public Facility/Hospital and Recreational. **Exhibit 3-2** below, depicts the land uses within the limits of the study area that surround the SR 112 / Alton Road Interchange. The main traffic generator around the interchange is the Mt. Sinai Hospital.

Exhibit 3-2: Existing Land Use Map – City of Miami Beach



Source: City of Miami Beach, Planning Department

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4.0 ROADWAY CHARACTERISTICS

An inventory of various roadway characteristics data was performed utilizing FDOT’s Roadway Characteristics Inventory (RCI) database, existing records drawings as well as other sources where noted. To facilitate the documentation of much of the roadway characteristics information that were gathered, aerial photography was compiled and cut into several plan sheets at a 1" = 100' scale to develop base maps that were used as a basis for plotting various data necessary for the study. **Appendix B** contains the Base Maps that were developed. Following is an assessment of the data obtained.

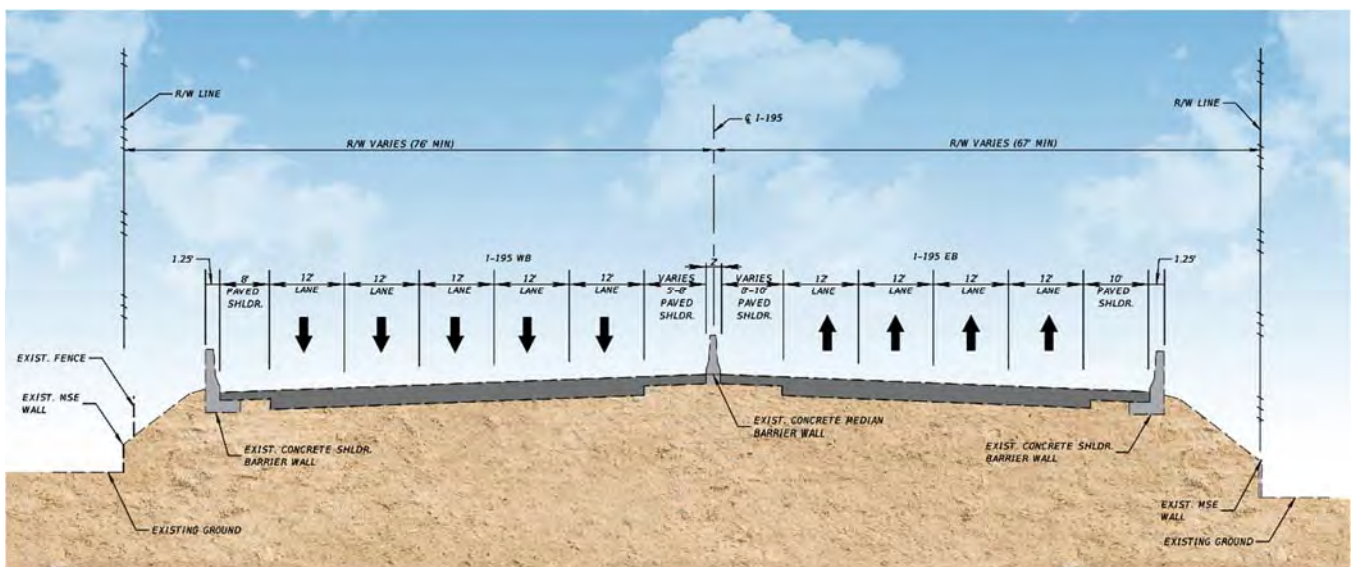
4.1 Existing Typical Sections

Existing typical sections were identified for several segments along I-195 and I-95 within the project study limits. The following is a more in-depth review of the Typical Sections identified.

4.1.1 I-195 from NW 14th Avenue to NW 10th Avenue

The segment of SR 112 / I-195 from NW 14th Avenue to NW 10th Avenue has an existing typical section that consists of 4 lanes in the eastbound direction and 5 lanes in the westbound direction. The typical section varies near the I-95 interchange, as auxiliary lanes and gore areas for interchange ramps are introduced (Refer to Existing Typical Section Exhibit in **Appendix B** for more details). The section has 12-foot wide lanes in each direction, 10' paved outside and 8' to 10' paved inside shoulders in the eastbound direction, and 8' paved outside and 5' to 8' paved inside shoulders in the westbound direction. The travel lanes are separated by a median concrete barrier wall and shoulder concrete barrier wall is used along the outside. The existing ground behind the shoulder barrier wall slopes down to Mechanically Stabilized Earth (MSE) walls located at the right-of-way limits. The existing right-of-way in this area varies, with a minimum width of 143'. **Exhibit 4-1** below, presents an illustration of the typical section for this segment.

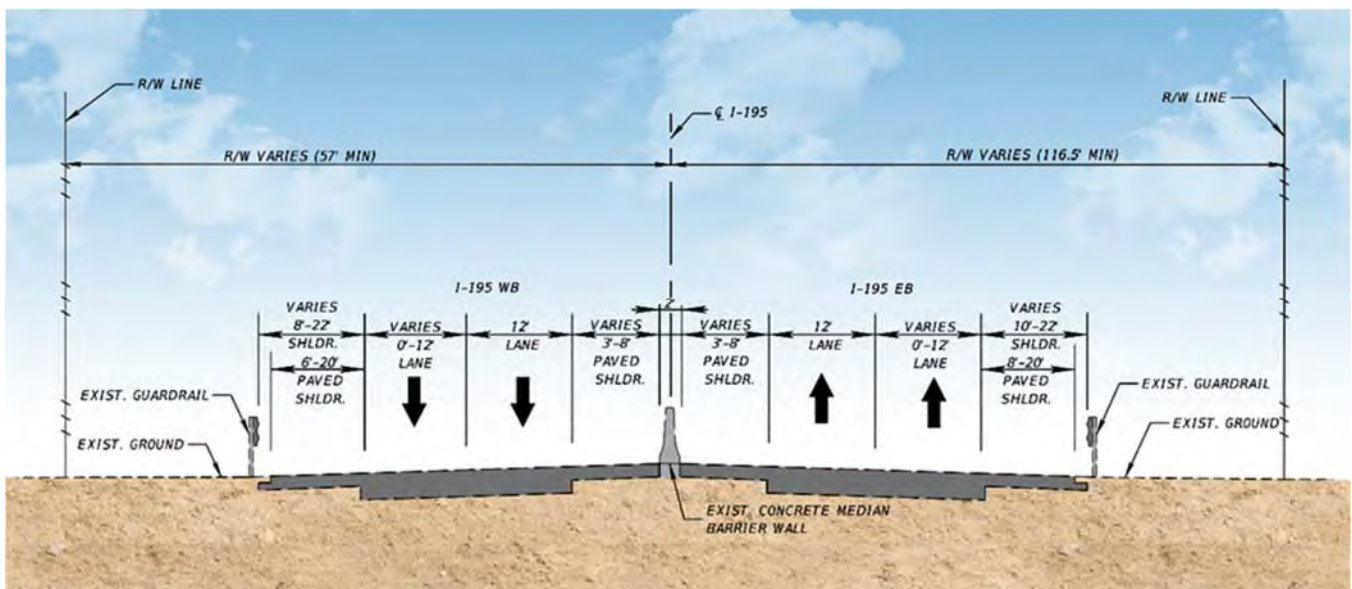
Exhibit 4-1: Typical Section - I-195 from NW 14th Avenue to NW 10th Avenue



4.1.2 I-195 from NW 10th Avenue to NW 3rd Avenue

The segment of SR 112 / I-195 from NW 10th Avenue to NW 3rd Avenue has an existing typical section that consists of two lanes in each direction. The section has 12-foot wide lanes in each direction, 3' to 8' paved inside shoulders, 6'-20' paved outside shoulders in the westbound direction, and 8'-22' outside shoulders in the eastbound direction. The travel lanes are separated by a median concrete barrier wall. The outside roadside protection varies within these limits, with the use of guardrail throughout most of the limits and pier protection barrier wall at existing bridge column locations within the interchange. The existing right-of-way in this area varies, with a minimum width of 174'. **Exhibit 4-2** below, presents an illustration of the typical section for this segment.

Exhibit 4-2: Typical Section - I-195 from NW 10th Avenue to NW 3rd Avenue

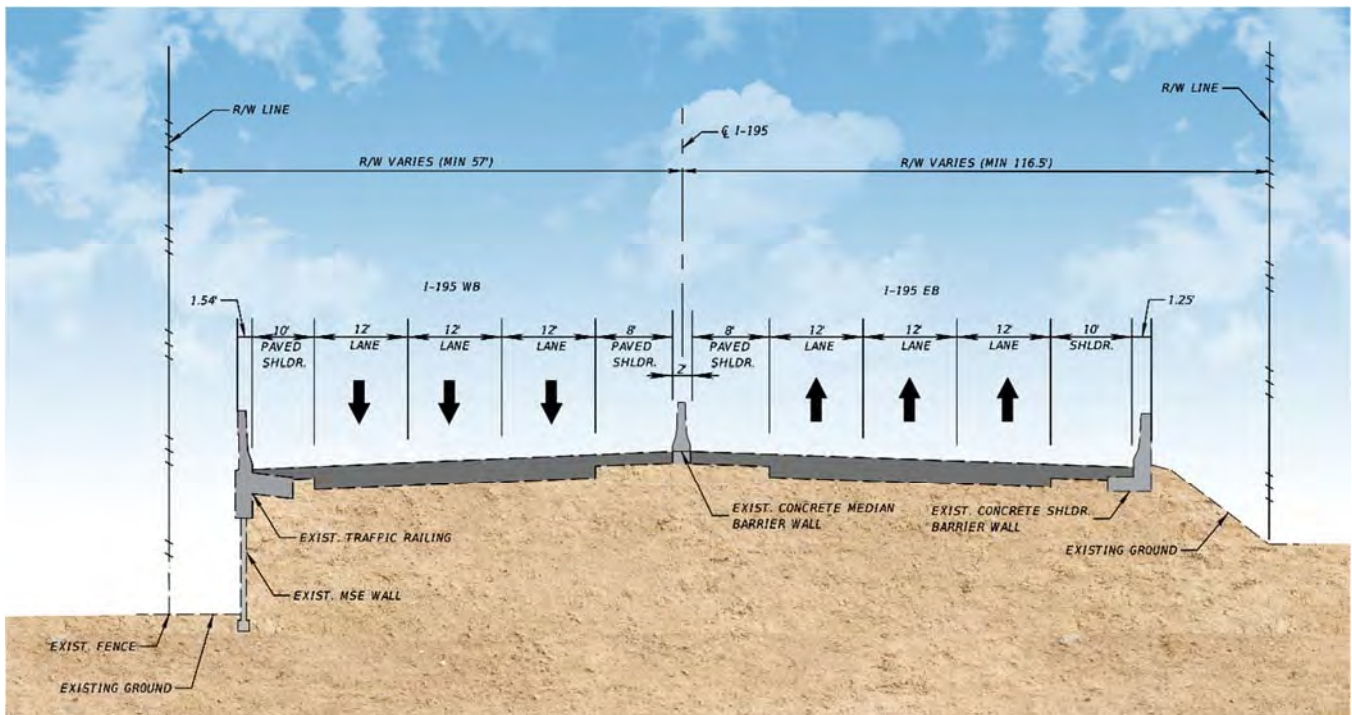


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4.1.3 I-195 from NW 3rd Avenue to Biscayne Boulevard

The segment of SR 112 / I-195 from NW 3rd Avenue to N Miami Avenue has an existing typical section that consists of three lanes in each direction. The section does vary, however, at the I-95 interchange and N Miami Avenue to allow for auxiliary and ramp lanes (Refer to Existing Typical Section **Exhibit in Appendix B** for more details). The section has 12-foot wide lanes in each direction, 8' paved inside shoulders, and 10' paved outside shoulders. The travel lanes are separated by a median concrete barrier wall and either shoulder barrier or MSE walls with traffic railing are used along the outside. The existing right-of-way in this area varies, with a minimum width of 174'. **Exhibit 4-3** below, presents an illustration of the typical section for this segment.

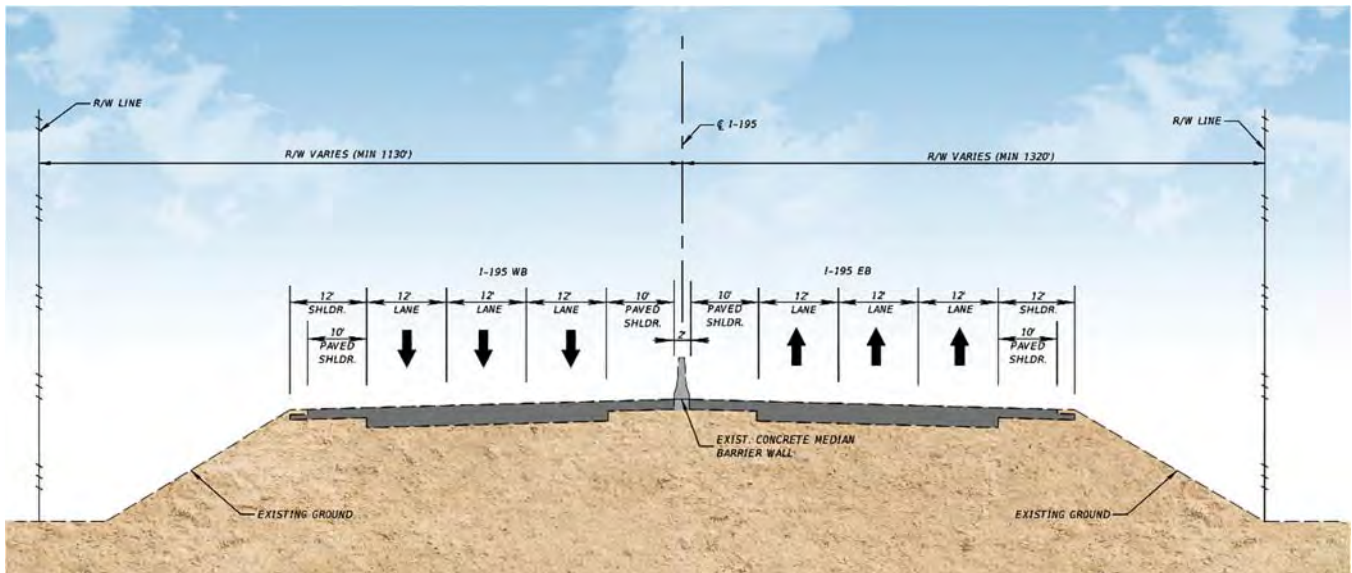
Exhibit 4-3: Typical Section - I-195 from NW 3rd Avenue to Biscayne Boulevard



4.1.4 I-195 from Biscayne Boulevard to Bridge No. 870314

The segment of SR 112 / I-195 from Biscayne Boulevard to Bridge No. 870314 has an existing typical section that consists of three lanes in each direction. The section has auxiliary lanes at the Biscayne Boulevard interchange (Refer to Existing Typical Section Exhibit in **Appendix B** for more details). The section has 12' wide lanes in each direction, 10' paved inside shoulders, and 10' paved / 12' full open shoulders along the outside. The travel lanes are separated by a median concrete barrier wall. The existing right-of-way in this area varies, with a minimum width of 2,450' **Exhibit 4-4** below, presents an illustration of the typical section for this segment.

Exhibit 4-4: Typical Section - I-195 from Biscayne Boulevard to Bridge No. 870314

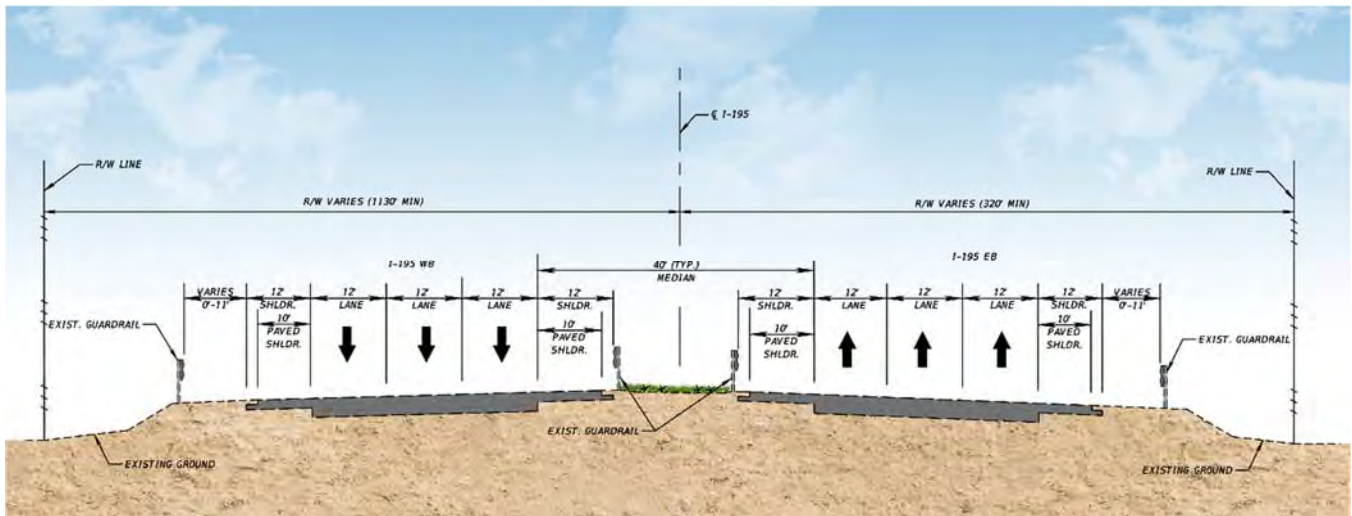


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4.1.5 I-195 from Bridge No. 870314 to Bridge No. 870302

The segment of SR 112 / I-195 from Bridge No. 870314 to Bridge No. 870302 has an existing typical section that consists of three lanes in each direction. The section has 12' wide lanes in each direction, 10' paved / 12' full shoulders with guardrail along the inside, and 10' paved / 12' full open shoulders along the outside. The travel lanes are separated by a 40' typical width landscaped median. There is also guardrail along the outside that varies from 0' to 11' from the existing shoulders. The existing right-of-way in the area varies, with a minimum width of 1,450'. **Exhibit 4-5** below, presents an illustration of the typical section for this segment.

Exhibit 4-5: Typical Section - I-195 from Bridge No. 870314 to Bridge No. 870301

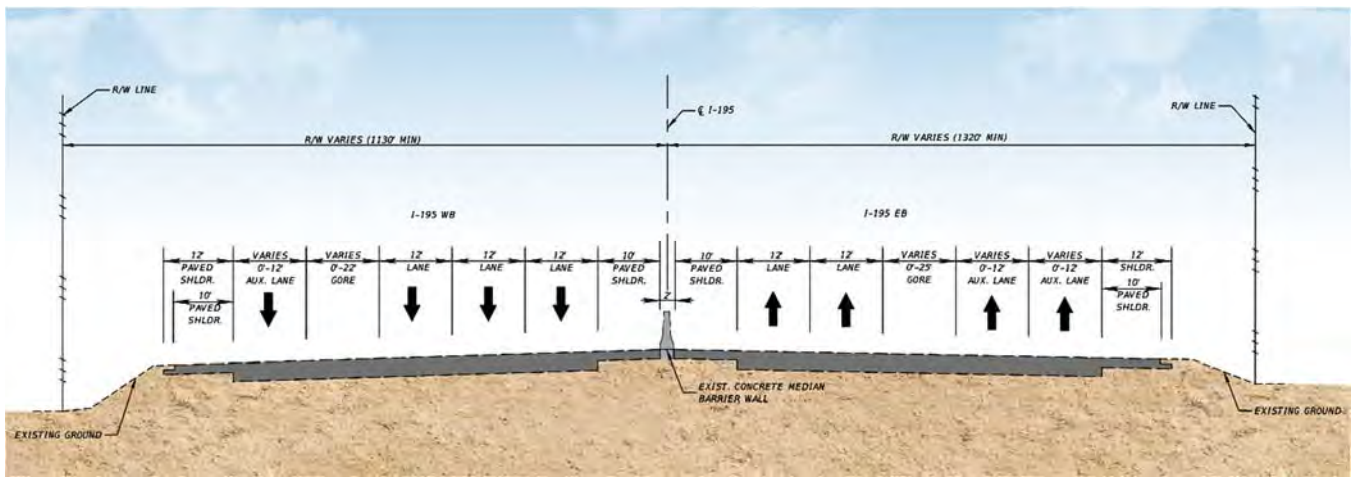


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4.1.6 I-195 from Bridge No. 870302 to Alton Road

The segment of SR 112 / I-195 from Bridge No. 870302 to Alton Road has an existing typical section that consists of two lanes in each direction, with a third auxiliary lane in each direction to and from the Alton Road ramps. The section has 12' wide lanes in each direction, 10' paved inside shoulders, and 10' paved / 12' full open shoulders along the outside. The travel lanes are separated by a median concrete barrier wall. The existing right-of-way varies, with a minimum width of 2,450'. **Exhibit 4-6** below, presents an illustration of the typical section for this segment.

Exhibit 4-6: Typical Section - I-195 from Bridge No. 870301 to Alton Road



4.1.7 I-95 from NW 22nd Street to NW 29th Street

The segment of SR 9 / I-95 from NW 22nd Street to NW 29th Street has an existing typical section that consists of 6 lanes in each direction. The section has 11' wide lanes in each direction, 5' to 8' paved inside shoulders, and 8' paved outside shoulders with shoulder gutter and guardrail. The travel lanes are separated by a median concrete barrier wall. The existing right-of-way varies, with a minimum width of 205'.

4.1.8 I-95 from NW 29th Street to NW 43rd Street

The segment of SR 9 / I-95 from NW 29th Street to NW 43rd Street has an existing typical section that consists of three general purpose lanes and two express lanes in the southbound direction, and five general purpose lanes in the northbound direction. The section has 11' wide lanes in each direction, a 1' buffer for the express lane delineators in the southbound direction, 5' to 8' paved inside shoulders, and 8' paved outside shoulders with shoulder gutter and guardrail. The travel lanes are separated by a median concrete barrier wall. The existing right-of-way varies, with a minimum width of 246'.

4.1.9 I-95 from NW 43rd Street to NW 50th Street

The segment of SR 9 / I-95 from NW 43rd Street to NW 50th Street has an existing typical section that consists of three general purpose lanes and two express lanes in each direction. It also consists of an elevated 1-lane express lane ramp in each direction in the median. The mainline section has 11' wide lanes in each direction, a 1' buffer for the express lane delineators, 6' paved inside shoulders, and 8' to 10' paved outside shoulders. There is traffic railing with barrier mounted noise walls and MSE walls along the outside and half walls along the inside that are against the MSE walls for the elevated express lane ramps from SR 112 / I-195. The elevated ramps consist of one 15' lane in each direction and 4' paved inside and outside shoulders. The express lane ramps are separated by a median concrete barrier wall. The existing right-of-way varies, with a minimum width of 240'.

4.1.10 I-95 from NW 50th Street to NW 54th Street

The segment of SR 9 / I-95 from NW 50th Street to NW 54th Street has an existing typical section that consists of five general purpose lanes and two express lanes in each direction. It also has an additional auxiliary lane in the express lanes that carries the ramp traffic from SR 112 / I-195. The section has 11' wide lanes in each direction, the express ramp lanes vary from 0' to 15', a 1' buffer for the express lane delineators, 6' paved inside shoulders, and 10' paved outside shoulders. There is traffic railing with barrier mounted noise walls and MSE wall along the outside. The travel lanes are separated by a median concrete barrier wall. The existing right-of-way varies, with a minimum width of 240'.

4.2 Existing Horizontal Alignment

The as-built plans provided by FDOT do not include sufficient horizontal alignment information along the study corridor. Based on the straight-line diagram and field observations, the horizontal alignment is composed of tangents and horizontal curves within the project limits.

4.3 Existing Vertical Alignment

The as-built plans provided by FDOT do not include sufficient vertical alignment information along the study corridor. Based on field observations, the longitudinal grades remain relatively flat, except where vertical curves are introduced at the interchange with I-95, at surface street crossings, and at bridge crossings over the bay.

4.4 Overhead Signage

Within the project limits, there is an abundance of overhead signage due to the number of roadways and destinations in the area. Existing signage primarily includes guide signs for ramp movements to adjacent roadways. In the eastbound and westbound directions along SR 112/I-195, the signage includes overhead guide signs leading to I-195 east – Miami Beach, I-95 North – Fort Lauderdale, and I-95 South – Downtown. There are also guide signs for local exits, including N Miami Avenue, Biscayne Boulevard and Alton Road north and south, NW 12th Avenue, and NW 17th Avenue. Several warning signs exist in both directions along the project signifying lane ends and merge conditions. Dynamic Message Signs (DMSs) can also be found along the corridor for general information and tolling rates. **Tables 4-1** through **4-3** as well as **Exhibit 4-7** on the following pages, list the existing signs by sign type and location in the study area.

Table 4-1: Summary of Overhead Signage in Study Area

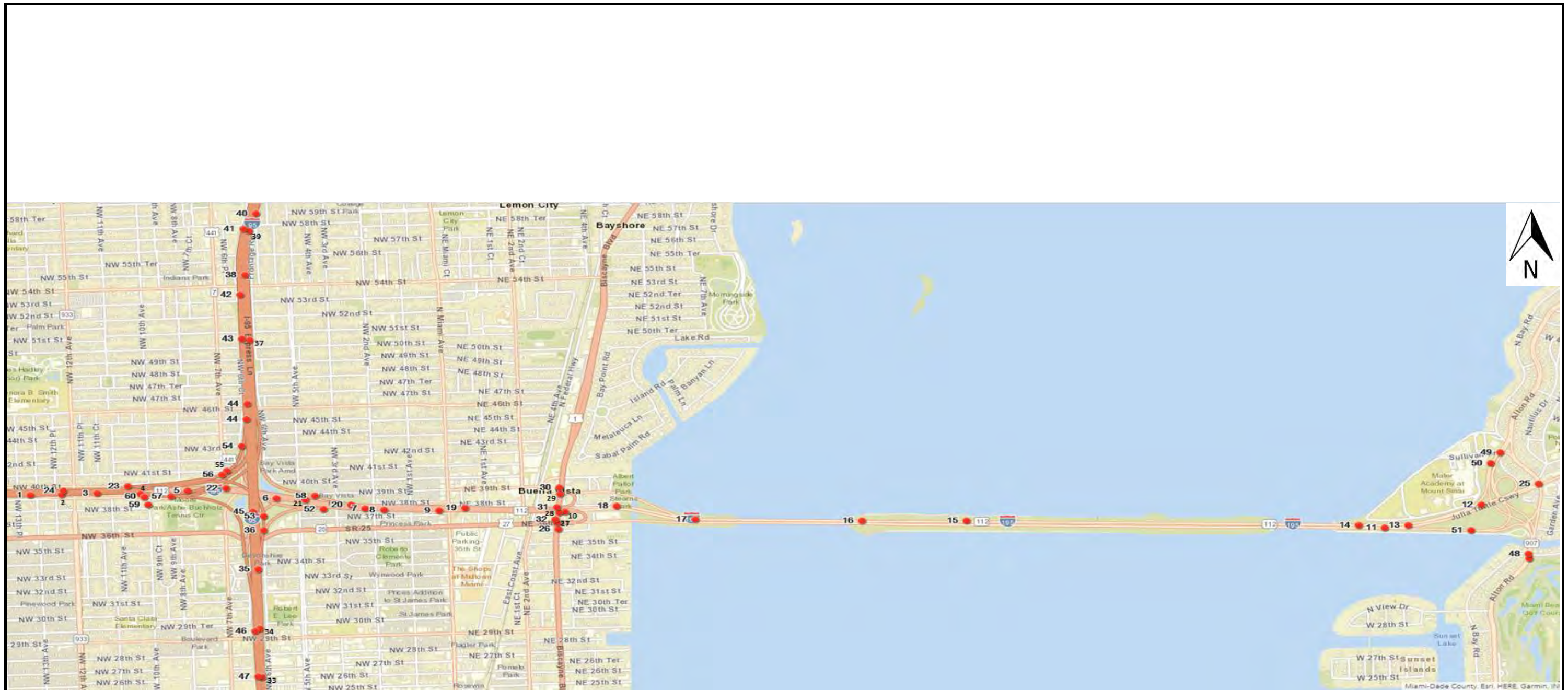
Location No.	Roadway ID	Roadway Name	Direction of Travel	Mile Post	Sign Type
1	87003000	SR-112/ NW-I-195	Eastbound	3.825	Guidesign
2	87003000	SR-112/ NW-I-195	Eastbound	3.93	Guidesign
3	87003000	SR-112/ NW-I-195	Eastbound	4.05	Guidesign
4	87004000	SR 112/Julia Tuttle Causeway	Eastbound	0.11	Guidesign
5	87004000	SR 112/Julia Tuttle Causeway	Eastbound	0.275	Warning Sign
6	87004000	SR 112/Julia Tuttle Causeway	Eastbound	0.58	Warning Sign
7	87004000	SR 112/Julia Tuttle Causeway	Eastbound	0.885	DMS
8	87004000	SR 112/Julia Tuttle Causeway	Eastbound	0.945	Guidesign
9	87004000	SR 112/Julia Tuttle Causeway	Eastbound	1.135	Guidesign
10	87004000	SR 112/Julia Tuttle Causeway	Eastbound	1.56	Guidesign
11	87004000	SR 112/Julia Tuttle Causeway	Eastbound	4.36	Guidesign
12	87004000	SR 112/Julia Tuttle Causeway	Westbound	4.705	Guidesign
13	87004000	SR 112/Julia Tuttle Causeway	Westbound	4.44	Guidesign
14	87004000	SR 112/Julia Tuttle Causeway	Westbound	4.275	DMS
15	87004000	SR 112/Julia Tuttle Causeway	Westbound	2.94	Guidesign
16	87004000	SR 112/Julia Tuttle Causeway	Westbound	2.585	Guidesign
17	87004000	SR 112/Julia Tuttle Causeway	Westbound	2.02	Guidesign
18	87004000	SR 112/Julia Tuttle Causeway	Westbound	1.74	Guidesign
19	87004000	SR 112/Julia Tuttle Causeway	Westbound	1.225	Guidesign
20	87004000	SR 112/Julia Tuttle Causeway	Westbound	0.835	Guidesign

Table 4-2: Summary of Overhead Signage in Study Area (Contd.)

Location No.	Roadway ID	Roadway Name	Direction of Travel	Mile Post	Sign Type
21	87004000	SR 112/Julia Tuttle Causeway	Westbound	0.68	Warning Sign
22	87004000	SR 112/Julia Tuttle Causeway	Westbound	0.405	Warning Sign
23	87004000	SR 112/Julia Tuttle Causeway	Westbound	0.075	Guidesign
24	87003000	SR-112/ NW-I-195	Westbound	3.987	Guidesign
25	87004000	SR 112/Julia Tuttle Causeway		4.92	Mast Arm Signal
26	87030000	US 1/SR 5	North	13.461	Guidesign
27	87030000	US 1/SR 5		13.486	Mast Arm Signal
28	87030000	US 1/SR 5	North	13.531	Guidesign
29	87030000	US 1/SR 5	North	13.601	Guidesign
				13.605	Mast Arm Signal
30	87030000	US 1/SR 5	South	13.664	Guidesign
31	87030000	US 1/SR 5	South	13.589	Guidesign
32	87030000	US 1/SR 5	South	13.534	Guidesign
33	87030000	I-95	North	4.205	DMS
34	87270000	I-95	North	4.41	Guidesign
35	87270000	I-95	North	4.655	Guidesign
36	87270000	I-95 off ramp	North	4.82	Guidesign
37	87270000	I-95	North	5.61	Guidesign
38	87270000	I-95	North	5.88	Guidesign
39	87270000	I-95	North	6.07	DMS
40	87270000	I-95	North	6.14	Guidesign

Table 4-3: Summary of Overhead Signage in Study Area (Contd.)

Location No.	Roadway ID	Roadway Name	Direction of Travel	Mile Post	Sign Type
41	87270000	I-95	South	5.975	DMS
42	87270000	I-95	South	5.695	Guidesign
43	87270000	I-95	South	5.515	Guidesign
44	87270000	I-95	South	5.24	Guidesign
		I-95	South	5.18	Guidesign
45	87270000	I-95	South	4.79	Guidesign
46	87270000	I-95	South	4.295	Guidesign
47	87270000	I-95	South	4.105	DMS
48	87037000	SR 907/Alton Road	North	2.46	DMS
				2.475	Mast Arm Signal
49	87037000	SR 907/Alton Road		2.96	Mast Arm Signal
50	87037000	SR 907/Alton Road	South	2.894	Guidesign
51	87004024	SR 907/Alton Road exit off ramp	Eastbound		Guidesign
52	87270179	I-95 N to I-195 East OFR	Northbound		Guidesign
53	87004003	I-95 N to SR 112 West ONR	Northbound		Guidesign
54	87270180	I-95 S OFR to I-195/SR 112 at Split	Southbound		Guidesign
55		I-95 S OFR to SR 112 West	Southbound		Guidesign
56	87270513	I-95 N Express Lanes OFR to SR 112 West	Northbound		Guidesign
57	87004002	SR 112 East OFR to I-95 at Split	Eastbound		Guidesign
58	87004004	I-195 West OFR to I-95 at split	Westbound		Guidesign
59		10th Ave to I-95/ SR 826/ Turnpike			Guidesign
60		39th St to I-95/ SR 826/ Turnpike			Guidesign



Legend
Overhead_Signage_Structures



Project Name:



FM No. 440228-1-22-01

Exhibit Name:

Location of Overhead Signage in Study Area

Report Title:

Existing Conditions Report

Exhibit No. 4-7

Page No.

Date: 09/04/18

4.5 Existing Structures Characteristics

As part of this planning study, 32 bridges along the I-195 project corridor were reviewed. The following elements were reviewed for each bridge including Type of Structure, Condition of Existing Structure, Vertical Clearance and Horizontal Clearance.

4.5.1 Type of Structure

The bridge superstructures studied are made up of a variety of types of superstructures involving American Association of State Highway and Transportation Officials (AASHTO) beams, steel plate girders, AASHTO-steel combinations, or flat slabs. The substructures studied consist of multicolumn piers, hammerhead piers, pile bents, straddle bents, and cantilever piers.

4.5.2 Condition of Existing Structures

As required by the Federal Highway Administration's (FHWA) *National Bridge Inventory (NBI) and Structural Inventory and Appraisal Program*, bi-annual inspections and evaluations are carried out by the FDOT on all bridge structures under its jurisdiction. The results of these inspections and evaluations are documented in Bridge Inspection Reports (BIR) that use specific jargon to communicate the current conditions of various bridges. Structural deficiency is used to indicate whether a bridge is structurally deficient, functionally obsolete, or not deficient at all. Bridges that are deemed structurally deficient must undergo repairs or be replaced within six years of being designated as such. Functionally obsolete bridges are bridges found to be structurally adequate but do not meet current roadway design standards. A bridge's health index indicates its current overall condition and serves to gauge the amount of work required to bring the bridge's condition to an ideal level; the lower the health index, the more work required. To determine whether a bridge deemed functionally obsolete or structurally deficient will be repaired or replaced, the sufficiency rating is used. The sufficiency rating is calculated based on the bridge's structural adequacy and safety, serviceability and functional obsolescence, essentiality for public use, and other special reductions. The Bridge load Rating indicates the reserved capacity of the bridge to carry live loads. Of the three different stress levels used in load rating analyses, Operating Rating (OR) and Inventory Rating (IR) were used for the purposes of this planning study.

The latest BIR were obtained for the existing bridges and a review of the reports indicated that Sufficiency Ratings vary from 64 to 97.5 for the bridges studied. Health indexes ranged from 60.78 to 99.98. Several bridges were determined to be Functionally Obsolete. NBI ratings for bridge decks, superstructures, and substructures were observed to be greater than or equal to the satisfactory level, except for Bridge 870301, whose superstructure was rated fair, and Bridge 870314, whose substructure was rated fair. **Tables 4-4** through **4-6** as well as **Exhibits 4-8** through **4-10** on the following pages, summarize the existing bridge characteristics and location of those bridges within the study area.



Table 4-4: Existing Bridge Characteristics Summary

Location					Structure Type		Geometrics							Condition							
No.	Bridge ID No.	Bridge Name	Direction	Facility Crossed	Super Structure Type	Sub-Structure Type	Bridge Width (ft.)	Rdwy Width (ft.)	Length (ft.)	No. of Spans	Span Lengths (ft.)	Min. Vertical Clearance ⁴ (ft.)	Min. Horizontal Clearance (ft.)	Year Built / Reconstructed	Sufficiency Rating (2018)	Health Index	Inspection Date	Bridge Load Rating (Tons)		Deficiency (2018)	Owner
																		IR (Year)	OR (Year)		
1	870021	COMP SR-7	NB/SB	I-195 (SR-112)	AASHTO	Multi-Column	65.25	52.00	191.81	4	48.42, 47.48, 50.63, 45.28	15.00	5.88 (P2), 10 (P1, P3)	1960	78	89.44	2/22/2018	43.6 (2012)	73.4 (2012)	Functionally Obsolete	FDOT
2	870022	SR 112 OVER SR 933 (NW 12 AVE)	EB/WB	SR 933 (NW 12 Ave)	AASHTO	Multi-Column/HH	144.75	139.83	170.00	3	40, 90, 40	15.00	6	1962/1989	93.7	99.67	10/5/2016	36 (2011)	53.6 (2011)	Not Deficient	FDOT
3	870023	US 441	NB/SB	I-95 Ramps NW 36 St	AASHTO	Multi-Column	65.25	52.00	352.22	7	2 @ 41.25, 53.75, 61.76, 58.43, 41.79, 54	15.00	6 (P2, P5), 10 (P1, P6)	1960	81	99.74	9/19/2016	55.4 (2007)	92.4 (2007)	Not Deficient	FDOT
4	870301	JULIA TUTTLE CAUSEWAY	EB/WB	Biscayne Bay	AASHTO/Steel Plate	Multi-Column/HH	116.75	112.00	2150.23	27	65.11, 7 @ 65, 5 @ 98, 130, 5 @ 98, 7 @ 65, 65.12	55.00	90	1959/1990	73.9	98.94	10/24/2017	35.1 (2016)	58.5 (2016)	Not Deficient	FDOT
5	870302	JULIA TUTTLE CAUSEWAY	EB/WB	Biscayne Bay	AASHTO	Multi-Column/HH	116.75	112.00	1138.07	17	64.91, 7 @ 65, 98, 7 @ 65, 65.16	35.00	75	1959/1991	85	99.46	10/26/2017	42.9 (2016)	71.6 (2016)	Not Deficient	FDOT
6	870303	JULIA TUTTLE CAUSEWAY	EB/WB	Alton Rd.	AASHTO	Multi-Column	104.58	99.25	309.41	7	29.71, 47.92, 48.03, 48.08, 48.05, 47.80, 39.81	14.00	6 (P2, P5), 10 (P5), 12 (P1), 13.5 (P6), 17.5 (P4)	1959/1991	81	99.82	2/14/2017	45.9 (2006)	76.4 (2006)	Functionally Obsolete	FDOT
7	870304	SR-112 OVER NW 10TH AVE	EB/WB	NW 10th Ave	AASHTO	Multi-Column	86.86 Min. (Varies)	66.28 Min. (Varies)	144.92	3	39.83, 63.25, 41.83	15.00	6	1960	93.4	99.21	1/16/2018	53.4 (2000)	84.3 (2000)	Not Deficient	FDOT
8	870305	SR-112 EB TO I-95 NB	EB/NB	SR-112	Steel Plate	Multi-Column/HH	37.33	33.00	366.96	4	89.74, 113.85, 96.63, 66.73	15.00	10 (P1, P3)	1961/1976	91.1	99.60	1/16/2018	40.6 (2013)	67.8 (2013)	Functionally Obsolete	FDOT
9	870310	I-195 OVER NW 2 AVE	EB/WB	NW 2nd Ave	AASHTO	Multi-Column/HH	135.08	130.00	149	3	43, 63, 43	16.75	8.24 (P2), 8.27 (P3)	1961/2012	91.3	99.64	10/24/2017	50.8 (2017)	67.0 (2017)	Not Deficient	FDOT
10	870311	I-195 OVER NW 1 AVE	EB/WB	NW 1st Ave	AASHTO	Multi-Column/HH	156.23 Min. (Varies)	151.15 Min. (Varies)	147	3	42, 63, 42	15.00	12.23 (P2), 12.39 (P3)	1961/2012	85	99.69	10/24/2017	38.9 (2017)	50.4 (2017)	Not Deficient	FDOT
11	870312	I-195 OVER N MAIMI AVE	EB/WB	North Miami Ave	AASHTO	Multi-Column/HH	103.08	98.00	160.67	3	42, 76.67, 42	15.60	7.71 (P2), 8.02 (P3)	1960/2012	83	87.82	7/26/2016	45.2 (2000)	75.4 (2000)	Not Deficient	FDOT
12	870313	I-195 OVER US-1	EB/WB	SR-5 & FEC RR	AASHTO/Steel Plate	Multi-Column/HH	90.17 Min. (Varies)	80 Min. (Varies)	1393.42	20	38, 55, 61.19, 7 @ 73, 75, 64.47, 88, 56, 64.09, 3 @ 70, 60.5, 110.17	15 22.67 (RR)	5.28 (P14), 5.92 (P15), 6 (P2), 6.01 (P3), 9.92 (P20), 10.04 (Abutment), 11.72 (P11, P12), 12 (P13), 18.02 (P14)	1961/2000	81	86.87	5/23/2018	37.4 (2006)	62.4 (2006)	Functionally Obsolete	FDOT



Table 4-5: Existing Bridge Characteristics Summary

Location					Structure Type		Geometrics							Condition							
No.	Bridge ID No.	Bridge Name	Direction	Facility Crossed	Super Structure Type	Sub-Structure Type	Bridge Width (ft.)	Rdwy Width (ft.)	Length (ft.)	No. of Spans	Span Lengths (ft.)	Min. Vertical Clearance ⁴ (ft.)	Min. Horizontal Clearance (ft.)	Year Built / Reconstructed	Sufficiency Rating (2018)	Health Index	Inspection Date	Bridge Load Rating (Tons)		Deficiency (2018)	Owner
																		IR (Year)	OR (Year)		
13	870314	I-195 OVER WESTSHORE WW	EB/WB	Westshore Waterway	Flat Slab	Pile Bent	114.56 Min. (Varies)	109.81 Min. (Varies)	160.22	5	33.70, 30.67, 30.79, 30.92, 34.14	1.41 / 3.91*	27.14	1961/1991	72	96.24	5/10/2017	43.3 (2008)	72.2 (2008)	Not Deficient	FDOT
14	870324	I-95 SB OVER I-195	SB	I-195 (SR-112)	AASHTO	Multi-Column	70.88	67.79	172.01	4	45.14, 45.06, 44.81, 37.01	15.70	5.5 (P5), 10 (P4, P6)	1961	84.4	92.46	3/15/2018	49.7 (2010)	55.8 (2010)	Functionally Obsolete	FDOT
15	870325	I-195 WB TO I-95 SB	WB/SB	I-195 (SR-112)	AASHTO	Multi-Column/HH	47.58	44.50	335.23	4	59.43, 85.57, 101.63, 83.63	15.25	10.00	1961	64	99.14	5/10/2017	74.1 (2000)	99 (2000)	Functionally Obsolete	FDOT
16	870326	I-195 OVER NW 3 AVE	EB/WB	NW 3RD AVE	AASHTO	Multi-Column	82.05 Min. (Varies)	78.51 Min. (Varies)	140.88	3	39.56, 63.12, 38.2	15.29	7.18 (P5), 8.68 (P6)	1961/2012	81.8	97.43	3/13/2017	30.6 (2017)	55.8 (2017)	Not Deficient	FDOT
17	870331	I-95 SB TO I-95 EB	SB/EB	I-195 & I-95 & RAMPS	AASHTO/Steel Plate	Multi-Column	34.17	28.00	1104	15	56.51, 51.26, 65.41, 2 @ 75, 64.80, 158.93, 139.98, 4 @ 60, 64.10, 59.61, 53.42	16.00	6 (P18), 8 (P32), 10 (P19, P31)	1959/2004	87.5	99.98	12/14/2016	45.7 (2010)	79.9 (2010)	Not Deficient	FDOT
18	870332	36 ST INTERCHANGE	NB/WB	I-95 & I-195 & RAMPS	AASHTO/Steel Plate	Multi-Column	34.17	28.00	1126.2	16	64.77, 52.64, 55, 60, 53.98, 73.55, 52.07, 60, 137.67, 156.25, 3 @ 60, 58.97, 60.48, 60.79	16.00	6 (P1), 8 (P5, P6, P17), 10 (P2, P16)	1961/2010	93.8	99.98	9/26/2017	39.6 (2010)	69.5 (2010)	Not Deficient	FDOT
19	870333	I-195 EB RMP OVER NW 3 AVE	EB	NW 3 AVE	AASHTO	Multi-Column/HH	51.28 Min. (Varies)	49.73 Min. (Varies)	139.3	3	38.4, 63.36, 37.54	15.29	7.18 (P1), 8.68 (P2)	1961/2012	92.9	99.57	3/13/2017	53.6 (2009)	59.4 (2009)	Not Deficient	FDOT
20	870335	I-95 SB TO SR-112 WB	WB	NW 10TH AVE	AASHTO	Multi-Column	75.96 Min. (Varies)	70 Min. (Varies)	145.15	3	39.92, 63.31, 41.92	16.27	6.00	1962/1989	95.7	98.94	1/16/2018	53.7 (2000)	87.2 (2000)	Not Deficient	FDOT
21	870336	I-195 (SR-112) EB TO I-95 SB	EB/SB	US-27 (NW 36 ST) & NW 35 ST	AASHTO	Multi-Column	34.17 Min. (Varies)	28 Min. (Varies)	342.94	6	50.80, 55.51, 44.42, 56.48, 81.72, 54.02	15.00	6.00	1960	91.1	99.53	3/20/2017	58.5 (2001)	97.5 (2001)	Functionally Obsolete	FDOT
22	870341	I-95 NB TO I-195 EB	NB/EB	US 27 (NW 36 ST) & NW 35 ST	AASHTO	Multi-Column	46.17	40.00	327.43	6	38.71, 55.8, 46.25, 44.71, 83.09, 58.87	15.00	6.00	1962	70.2	99.21	3/20/2017	41.7 (2008)	69.5 (2008)	Functionally Obsolete	FDOT
23	870342	I-195 WB TO I-95 SB	WB/SB	US 27 (SR 948 / NW 36 ST)	AASHTO	Multi-Column	34.17	28.00	158.88	3	39.01, 73.93, 45.94	15.00	6.00	1960	74	99.62	12/14/2016	60.3 (2006)	100.5 (2006)	Not Deficient	FDOT
24	870343	I-95 EB TO I-95 SB	EB/SB	US 441 (NW 7 AVE)	AASHTO	Multi-Column	34.17	28.00	176.72	3	34.16, 93.16, 49.41	15.00	6.00	1962	90.3	99.31	3/13/2017	64.8 (2008)	99 (2008)	Not Deficient	FDOT
25	870375	I-195 WB TO NE 38 ST	WB/NE	WESTSHORE WATERWAY	Flat Slab	Pile Bent	42.75	40.00	145.30	5	29.12, 3 @ 29.02, 29.12	0.5 / 3*	26.00	1959/1990	93	93.54	5/10/2017	48.2 (2017)	62.6 (2017)	Not Deficient	FDOT



Table 4-6: Existing Bridge Characteristics Summary

Location					Structure Type		Geometrics							Condition							
No.	Bridge ID No.	Bridge Name	Direction	Facility Crossed	Super Structure Type	Sub-Structure Type	Bridge Width (ft.)	Rdwy Width (ft.)	Length (ft.)	No. of Spans	Span Lengths (ft.)	Min. Vertical Clearance ⁴ (ft.)	Min. Horizontal Clearance (ft.)	Year Built / Reconstructed	Sufficiency Rating (2018)	Health Index	Inspection Date	Bridge Load Rating (Tons)		Deficiency (2018)	Owner
																		IR (Year)	OR (Year)		
26	870376	NW 36TH TO I-195 EB	EB	WESTSHORE WATERWAY	Flat Slab	Pile Bent	42.75	40.00	129.79	5	25.97, 25.95, 25.98, 25.96, 25.94	1.86 / 4.36*	22.95	1959/1990	97.5	95.97	5/10/2017	44.5 (2016)	74.1 (2016)	Not Deficient	FDOT
27	870377	EAST TO NORTH RAMP ALTON	EB/NB	NORTHBOUND ALTON ROAD	AASHTO	Multi-Column	34.25	31.50	127	3	37, 48, 42	14.00	6 (P2), 10 (P1)	1959/1991	82	95.53	10/25/2017	29.7 (2011)	49.5 (2011)	Functionally Obsolete	FDOT
28	870440	I-95 NB OVER I-195	NB	I-195 (SR-112)	AASHTO	Multi-Column	70.63	67.54	172.01	4	45.14, 45.06, 44.81, 37.01	15.70	5.5 (P2), 10 (P1, P3)	1961/2009	86.5	93.71	3/15/2018	50 (2010)	55.4 (2010)	Functionally Obsolete	FDOT
29	870713	I-95 FLYOVER RAMP	NB/SB	I-95 SB	AASHTO/Steel Plate	Multi-Column/HH/Concrete & Steel Straddle/Cantilever	53.00	48.08	1731	17	2 @ 117, 96, 119, 96, 5 @ 84, 130, 170, 130, 4 @ 84	16.54	1.25 (P22), 1.4 (P21)	1991/2010	79.2	99.98	6/21/2017	21.6 (2003)	78 (2003)	Not Deficient	FDOT
30	870775	I-95 SB TO SR-112 WB SUNPASS RMP	SB/WB	SR-112	AASHTO/Steel Plate	Multi-Column/HH/Concrete Straddle	26.75	24.00	1072.4	10	5 @ 86, 147, 163, 147, 2 @ 92.68	23.34	2.09 (P7X), 2.10 (P7X), 2.23 (P9X), 2.38 (P6X), 7.37 (P10X), 9.01 (P5X), 9.54 (P8X)	1990	95	99.87	10/31/2017	44.7 (2007)	77.1 (2007)	Functionally Obsolete	FDOT
31	870776	SR-112 EB TO I-95 NB SUNPASS RMP	EB/NB	SR-112	AASHTO/Steel Plate	HH/ Multi-Column	26.75 Min. (Varies)	24 Min. (Varies)	1631.5	18	4 @ 82, 133, 165, 133, 2 @ 94, 41, 96, 3 @ 86, 90, 61, 93, 45.5	28.76	2.02 (P12Y), 2.10 (6Y), 2.23 (P11Y), 2.41 (P18Y), 3.34 (P16Y), 6.33 (P8Y), 8.22 (P17Y), 9.54 (P15Y)	1991	95.3	99.98	10/29/2017	53.7 (2007)	89.6 (2007)	Functionally Obsolete	FDOT
32	870777	I-95 HOV RAMP D	EB/NB	I-95/I-195 & RAMPS	AASHTO/Steel Plate	HH/ Multi-Column	26.75	24.00	906	7	3 @ 95, 170, 185, 170, 96	16.52	2.02 (P6D), 2.05 (P4D), 2.09 (P5D), 2.44 (P7D)	1991	88.8	99.83	3/15/2018	26.3 (2017)	35.6 (2017)	Not Deficient	FDOT

DWSE = Design Water Surface Elevation
 OWSE = Optimum Water Surface Elevation
 HH = Hammerhead
 MHW = Mean High Water
 P# = Pier #
 RMP = Ramp
 RR = Rail Road
 WW = Waterway

Notes:
 1 Geometric data obtained from existing bridge plans.
 2 Location and condition data obtained from bridge inspection reports from inspection date listed.
 3 Structure type data obtained from existing bridge plans and bridge inspection reports.
 4 Controlling substructure for horizontal clearances shown in parenthesis.
 5 Minimum vertical clearances for structures spanning waterways are with respect to the MHW elevation.
 * OWSE / DWSE



■ Concrete Bridge
■ Concrete-Steel Combination Bridge
■ Steel Bridge

Please refer to Existing Bridge Characteristics Summary

Project Name:



**I-195 Corridor Planning Study from
 I-95/NW 12th Avenue to SR 907/Alton Road**
 FM No. 440228-1-22-01



Exhibit Name:

Location of Existing Bridges

Report Title:

Existing Conditions Report

Exhibit No. 4-8

Page No.

Date: 12/22/17



- Concrete Bridge
- Concrete-Steel Combination Bridge
- Steel Bridge

Please refer to Existing Bridge Characteristics Summary

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

Location of Existing Bridges

Report Title:

Existing Conditions Report

Exhibit No.	4-9
Page No.	
Date:	12/22/17



- Concrete Bridge
- Concrete-Steel Combination Bridge
- Steel Bridge

Please refer to Existing Bridge Characteristics Summary

4.5.3 Vertical Clearance

Primarily, vertical clearance standards are a means of ensuring safe passage beneath structures to tall design vehicles. Per the January 2018 FDOT Plans Preparation Manual (PPM), Table 260.6.1, the minimum vertical clearance for construction affecting existing bridge structures is 16 feet. The observed vertical clearances for the bridges in question (which span above roadways) range from 14 to 28.76 feet.

For bridges over waterways, section 260.8 of the January 2018 FDOT PPM specifies that at least 6 feet of vertical clearance is required for navigational purposes, and at least 1 foot of vertical clearance must be provided between coastal bridges and the 100-year design wave crest elevation. The bridges involved in this study which span over waterways range in vertical clearance from 3 to 55 feet to the Mean High Water (MHW) level. **Tables 4-4** through **4-6** on the preceding pages, present a summary of existing vertical clearances.

4.5.4 Horizontal Clearance

Horizontal clearance as it pertains to bridge structures that span roadways, is the clear zone measured horizontally from the edge of the traveled way, auxiliary lane, or ramp to the nearest bridge pier or abutment. Horizontal clearances for such cases in the current study, range from 1.25 to 18.02 feet.

Horizontal clearance, as it pertains to bridge structures that span waterways, is the minimum distance between piers, fender systems, culvert walls, etc. The bridges involved in this study which span over waterways, range from 22.95 to 90 feet in horizontal clearance. **Tables 4-4** through **4-6** on the preceding pages, present a summary of existing horizontal clearances.

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4.6 Intelligent Transportation Systems (ITS) Equipment

The I-195 corridor is currently monitored, analyzed and managed from the FDOT District 6 Transportation Management Center (TMC). The SunGuideSM software is used to control and monitor the ITS subsystems. This section further describes the existing ITS components:

4.6.1 Pan-Tilt-Zoom (PTZ) Closed Circuit Television (CCTV) Cameras

CCTV cameras currently provide coverage of the project corridor and enables traffic monitoring and early incident detection capabilities. Within or approaching the study limits, the District 6 SunGuideSM TMC operates seven 7 CCTV cameras. Existing CCTVs are scheduled to be upgraded under the ITS Device Replacement Project - Contract Number: E6L51 - Financial Project Number: 430291-2-92-01 & 430291-2-92-02 with GovComm GC-IMPO Series. Plan sheets showing the locations of the existing CCTV cameras are included in **Appendix B**.

4.6.2 Dynamic Message Signs (DMS)

DMSs are currently deployed along the corridor to inform motorists of current traffic conditions and incidents such as crashes, disabled vehicles, road work, car fires, hazmat spills, evacuations as well as the America's Missing Broadcast Emergency Response (AMBER) alerts. The District 6 SunGuideSM TMC currently operates two 2 General Purpose Lane DMSs and two 2 Arterial DMS signs within or approaching the study limits. Existing DMS signs are monochromatic. The existing eastbound General-Purpose Lane DMS is scheduled to be upgraded under the ITS Device Replacement Project - Contract Number: E6L51 - Financial Project Number: 430291-2-92-01 & 430291-2-92-02 with full color DMS sign. Plan sheets showing the locations of the existing DMS Signs are included in **Appendix B**.

4.6.3 Vehicle Detection System

Microwave Vehicle Detection System (MVDS) sensors are part of the District 6 Vehicle Detection System. These devices are non-intrusive, mounted on poles along the shoulders and collect volume, vehicle type, average speed and long vehicle count data. Within the study limits, the District 6 SunGuideSM TMC currently operates 17 MVDS sensors along I-195. MVDS are a mixture of Wavetronix and ImageSensing System MVDS models. There are no loop detectors within the study corridor. Plan sheets showing the locations of the existing MVDS sensors are included in **Appendix B**.

4.6.4 ITS Power Service Subsystem

Seven Existing Services energize the ITS field devices along the corridor. Plan sheets showing the locations of the existing Power Service Points are included in **Appendix B**.

4.7 Utilities

There are 18 Utility Agency Owners (UAO) with facilities within the study area. **Table 4-7** below shows the list of utility agency owners and utility contact data obtained from Sunshine State One Call of Florida (SSOCOF).

Table 4-7: Existing Utility Agency Owners and Contact Information

No.	Utility Agency Owner	Facility Type	Contact Person	Phone	Email
1	Atlantic Broadband	Cable TV	Pete Freytag	(305) 861-8069 x5208	pfreytag@atlanticbb.com
2	AT&T Florida	Telecommunications	Steve Low	(305) 222-8745	sl4504@att.com
3	AT&T Transmission (PEA, Inc.)	Telecommunications	Craig Petrie	(407) 578-8000 x10	cpetrie@pea-inc.net
4	Centurylink / Level 3	Telecommunications	Jack Brady	(786) 495-2170	jack_brady@centurylink.com
5	City of Miami Beach	Water & Sewer	Ricky Falls	(305) 673-7080	Rickyfalls@miamibeachfl.gov
6	Comcast Cable	Cable TV	Leonard Maxwell-Newbold	(954) 447-8405	leonard_maxwell-newbold@cable.comcast.com
7	Crown Castle	Wireless / Fiber	Rick Sandala	(742) 416-2180	rick_sandala@crowncastle.com
8	Fibernet Direct	Telecommunications	Danny Haskett	(786) 610-7073	danny.haskett@crowncastle.com
9	Florida Power & Light - Distribution	Electric	Angel Vargas	(305) 442-5129	angel.vargas@fpl.com
10	Florida Power & Light - Transmission	Electric	Michael Foley		michael.foley@fpl.com
11	Hotwire Communications	Telecommunications	Walter Sancho-Davila	(954) 248-7396	walter_sancho-davila@hotwirecommunications.com
12	Miami-Dade Water & Sewer Dept.	Water & Sewer	Patrick Chong	(786) 552-4416	patrick.chong@miamidade.gov
13	TECO People's Gas	Gas - Distribution	Angel Quant	(954) 453-0805	alquant@tecoenergy.com
14	Teleport Comm America (PEA, Inc.)	Telecommunications	Craig Petrie	(407) 578-8000 x10	cpetrie@pea-inc.net
15	Sprint	Telecommunications	Jon Baker	(321) 280-9596	jon.baker@sprint.com
16	Strome	Telecommunications	Scott Drake	(404) 932-4156	sdrake@strome-networks.com
17	Verizon / MCI	Telecommunications	Todd Mars	(305) 626-2802	todd.mars@verizon.com
18	Windstream Communications	Telecommunications	Gary Cary	(386) 462-6530	gary.cary@windstream.com

All utility companies contacted have responded, including six of the 18 who responded to indicate they have no facilities within the study limits. These included Fibernet Direct, Teleport Comm America (PEA, Inc.), Sprint, Strome, Verizon / MCI and Windstream Communications. The remainder of this section presents a summary of the utility information gathered. **Appendix B** includes plots of the following major utilities; FP&L Distribution/Transmission, AT&T Florida, Miami-Dade Water & Sewer Department, City of Miami Beach, and TECO People's Gas.

4.7.1 AT&T Florida

AT&T Florida operates the following facilities within the study limits:

- A duct bank with 9-4" PVC ducts running along the east side of NW 3rd Avenue and crossing below I-195 eastbound and westbound.
- A 18-4" Multiple Tile Ducts (MTD) duct bank running along the east side of North Miami Avenue crossing below I-195. These ducts run into an existing AT&T manhole located on the northeast quadrant of the N Miami Avenue intersection with NW 38th Street, then continue northward. A 1-4" PVC duct then stems westward from the manhole, and additional 6-4" PVC and 1-3.5" Transit Ducts (TRD) running eastward along the north side of NW 38th Street.
- A 1-4" PVC duct runs along the eastside of NE 2nd Avenue and crosses below I-195.
- Two duct banks cross below I-195 along the east side of Federal Highway. These contain 8-4" PVC ducts and a parallel 4-4" MTD duct run.
- A 1-4" PVC duct with buried copper runs along the north side of NE 37th Avenue crossing Biscayne Boulevard and terminating at the I-195 westbound on-ramp from Biscayne Boulevard.
- Overhead copper cable attached to the existing FP&L-Distribution pole line along the south edge of I-195 from Biscayne Boulevard and terminating at the westernmost Julia Tuttle Causeway limits.
- AT&T owns several structures at the I-195 EB / 41st Street easternmost limits west of Alton Road, including two manhole structures at each side of the entrance to the 41st Street, westbound to North Bay Road, northbound connector to Alton Road. with 3-4" PVC ducts crossing under N Bay Road and 3-4" MTD ducts running northward along the west side of N Bay Road.
- A large AT&T manhole structure is also located along the northeast quadrant of the Alton Road and 41st Street intersection with 4-4" PVC ducts running westward from the structure along the northbound Alton Road travel lanes; a 4-3.5" TRD duct run going westward from the structure; and a 4-4" MTD duct run heading eastward from the structure along the north side of 41st Street, and continuing eastward past Garden Avenue.
- At the Alton Road and 43rd Street intersection, AT&T owns 2-4" PVC ducts crossing the south leg of the intersection, and an additional 2-4" duct run along the westside of Alton Road and continuing northward past the intersection.
- AT&T Florida owns an 8-4" PVC and 9-4" MTD duct bank crossing I-95 at NW 28th Street.

4.7.2 Atlantic Broadband

Atlantic Broadband operates the following facilities within the study limits:

- Aerial fiber cables attached the existing FP&L-Distribution pole line located along the western side of the North Bay Road connector to northbound Alton Road.
- Aerial cable crossing attached to FP&L-Distribution poles crossing 41st Street along the east side of North Meridian Avenue.

4.7.3 AT&T Transmission (AT&T Corp/TCA, LLC)

AT&T Transmission operates the following facilities within the study limits:

- A 2-4" High Density Polyethylene (HDPE) with fiber optic cable operated by AT&T Corp. Long Lines located along the west side of NW 3rd Avenue and crossing below I-195.
- A 16-1.5" HDPE duct bank operated by TCA, LLC crossing I-195 along the west side of Biscayne Boulevard.

4.7.4 Centurylink / Level 3

Centurylink/Level 3 operates the following facilities within the study limits:

- Fiber optic cables located within a joint-use duct bank located within the FEC railway along the eastern border of the right-of-way. This is a newly constructed duct bank built by the FEC Railway Company to accommodate double-tracking for the Brightline Passenger Rail. The duct bank is owned by FEC, individual fiber/conduit is leased to the joint users, among them Centurylink/Level 3, among them Verizon/MCI.

4.7.5 City of Miami Beach

The City of Miami Beach operates the following facilities within the study limits:

- 36" water main (WM) running along the south side of I-195 west of Alton Road and onto the Julia Tuttle Causeway. This WM runs westward along Julia Tuttle Causeway until it meets the point-of-connection with the Miami-Dade Water & Sewer Department Infrastructure from which it is fed.
- 8" terracotta gravity sewer and manhole structures crossing 41st Street west of Alton Road and running along the south side of 41st Street. This system feeds into an 18" connector crossing the intersection of Alton Road and 41st Street which itself feeds into a 21" gravity sewer system at the eastern leg of the intersection of Alton Road and 41st Street.
- 18" terracotta gravity sewer and manhole structures running north-south along Alton Road. This gravity sewer reduces into a 15" terracotta gravity sewer system just north of 43rd Street.
- Miami Beach also operates a storm drainage system crossing the I-195 to Alton Road northbound off-ramp, and the Alton Road to I-195 westbound on-ramp at two locations; one north of the mainline and on south of the mainline. This same system crosses the I-195 eastbound and westbound mainline and continues due south along Alton Road.
- The storm drain pipeline also crosses the south leg of the 41st Street at Alton Road intersection and continues eastward along the southern curb line for 41st Street.
- 12" WM running south along the west side of Alton Road.

4.7.6 Comcast Cable

Comcast operates the following facilities within the study limits:

- Aerial cable attached to existing FP&L-Distribution poles along the north side of NW 40 Street from NW 13th Place to east of NW 12th Avenue.
- Buried facilities located on the east side of NW 10th Avenue and crossing I-195.
- Buried facilities located on the west side of NW 2th Avenue and crossing I-195.
- Aerial cable attached to existing FP&L-Distribution poles along the north side of NW 38th Street from NW 2nd Avenue to NW 1st Avenue.
- Buried facilities located on the west side of Federal Highway and crossing under I-195.
- Aerial cable attached to existing FP&L-Distribution poles located along the south side of I-195 between Federal Highway and NE 5th Avenue then going subgrade east of NE 5th Avenue.
- Subgrade cable facilities along the east side of North Bay Road north of 41st Street.
- Subgrade facilities along the west side of Alton Road crossing 41st Street.
- Subgrade facilities along the north side of 41st Street from North Bay Road to Alton Road.
- Subgrade facilities along the north side of 41st Street from Alton Road to North Meridian Avenue.
- Subgrade facilities along the west side of the Alton Road southbound on-ramp to I-195 westbound and continuing north along the west side of Alton Road north of North Bay Road.
- Subgrade facilities along the east side of Alton Road northbound crossing 34th Street.
- Subgrade facilities crossing I-95 along the north side of NW 29th Street.
- Subgrade facilities crossing I-95 along the south side of NW 35th Street.
- Subgrade facilities crossing I-95 along the south side of NW 53rd Street.
- Aerial cable attached to existing FP&L-Distribution poles along the west side of NW 6 Court from NW 56th Street to NW 58th Street.

4.7.7 Crown Castle/Fibernet Direct

Crown Castle/Fibernet Direct operate the following facilities within the study limits:

- Aerial fiber crossing 41st Street along the east side of Alton Rd. and continuing north and south along Alton Road.
- Fiber Optic ducts crossing I-95 along the south side of NW 71st Street.
- Fiber Optic ducts crossing I-95 along the south side of NW 54th Street.
- Fiber Optic ducts crossing I-95 along the north side of NW 29th Street.
- Fiber Optic ducts crossing I-195 along the east side of Federal Highway.
- Fiber Optic ducts crossing I-195 along the west side of the Alton Road southbound connector.

4.7.8 Florida Power & Light – Distribution

FP&L-Distribution operates the following facilities within the study limits:

- 13kV overhead electric (OE) crossing NW 40th Street along NW 12th Place with pole, down guy, and luminaire along the south side of NW 40 Street.
- 13kV OE crossing NW 12th Avenue along the north side of NW 40th Street.
- 13kV OE along the east and west side of NW 12 Avenue south of the I-195 eastbound and westbound overpass, with luminaire.
- 480v OE along the north side of NW 40th Street.
- 13kV buried electric (BE) duct bank crossing I-195 along the east side of NW 10th Avenue.
- 13kV OE crossing I-195 along the west side of NW 3rd Avenue.
- 13kV OE along the north side of NW 38th Street from west of NW 2nd Avenue to east of NW 1 Avenue with luminaires attached to poles.
- 13kV BE along North Miami Avenue crossing I-195 then running eastward along NW 38th Street up to a point just east of NE 1st Avenue.
- 13kV BE crossing I-195 along Federal Highway.
- 13kV OE along the south side of I-195 from Federal Highway to NE 5th Avenue and crossing over the I-195 east bound to Biscayne Boulevard off-ramp.
- 13kV BE and manhole structures at the intersection of Alton Road and 41st Street. 13kV BE running northward from the intersection along the NB lanes of Alton Road, and two 13kV BE running eastward from the intersection along the EB and WB lanes of 41st Street respectively.
- 13kV BE crossing Alton Road just south of 43rd Street.
- 13kV OE running along the east side of Alton Road north of 43rd Street.
- 13kV OE crossing I-95 at NW 28th Street.
- 13kV OE crossing I-95 at NW 35th Street.
- 13kV OE crossing I-95 north of NW 47th Terrace.
- 13kV OE crossing I-95 at NW 51st Street.
- 480v and 13kV OE along NW 6th Court from south of NW 56th Street to NW 58th Street with a 13kV OE crossing I-95 at NW 58th Street.
- 13kV OE along NW 6 Avenue from NW 58th Street to NW 60th Street.
- 13kV OE and 13kV BE along NW 35th Street crossing I-95.

4.7.9 Florida Power & Light – Transmission

FP&L-Transmission operates the following facilities within the study limits:

- 138kV OE crossing I-95 along the west side of NW 3rd Avenue.
- 69kV BE crossing I-95 along NW 3rd Avenue.
- 138kV OE crossing I-95 along the north side of NW 35th Street.
- 138kV OE running along the east side of NW 6th Court from NW 58th Street to NW 60th Street and then crossing over I-95 along NW 60th Street.
- 138kV OE crossing I-95 along the north side of NW 35th Street.

4.7.10 Hotwire Communications

Hotwire operates the following facilities within the study limits:

- A 1-2" HDPE crossing I-195 along the east side of N. Miami Avenue.
- A 1-2" HDPE crossing I-195 along the east side of Federal Highway.

4.7.11 Miami-Dade Water & Sewer Department

Miami-Dade Water & Sewer Department operates the following facilities within the study limits:

Water Infrastructure

- A 16" WM crossing I-195 at Biscayne Boulevard.
- 6" WM along north side of I-195 from Biscayne Boulevard to Federal Highway.
- 4" WM crossing I-195 at Federal Highway.
- 2" WM along south side of I-195 running eastward at NE 5th Avenue.
- 16" WM crossing I-195 at NE 2nd Avenue.
- Abandoned 6" WM crossing I-195 at NE 2nd Avenue.
- 2" WM crossing NE 1st Court.
- 36" WM along NE 36th Street running eastward until the City of Miami Beach connection point.
- 1" WM along southern border of the I-95 to I-195 east-bound ramp.
- 20" WM crossing I-195 at NW 3rd Avenue.
- 48" WM crossing I-195 at NW 2nd Avenue.
- 8" WM crossing I-195 at NW 2nd Avenue.
- 12" WM crossing I-195 at NE 1st Avenue.
- 12" WM along NE 38th Street from North Miami Avenue to NE 2nd Avenue with an abandoned 12" WM along the same limits.
- 8" WM along south side of I-195 from NE 1st Avenue to NE 1st Court.
- 12" WM crossing I-195 at North Miami Avenue.
- WM crossing at NW 1st Avenue.
- 8" WM within a 16" casing crossing I-195 at NW 8th Avenue.
- 30" WM and 6" WM crossing I-195/SR-112 at NW 10th Avenue.
- 16" WM crossing I-195/SR-112 at NW 12th Avenue.
- 6" WM along NW 39th Street from NW 10th Avenue to NW 12th Avenue.
- 4" WM along NW 39th Street from NW 12th Avenue to NW 14th Avenue.
- 8" WM along NW 40th Street from NW 10 Avenue to NW 14th Avenue.
- 48" WM along NW 6th Court between NW 54th Street and NW 53rd Street and crossing I-95 at NW 53rd Street.
- 6" WM along NW 6th Avenue from NW 53rd Street to a point near NW 40th Street.
- 6" WM along NW 6th Court from NW 48th Street to a point near NW 43rd Street.
- 20" WM crossing I-95 at NW 54th Street.
- 8" WM crossing I-95 at NW 50th Street.
- 12" WM crossing I-95 at NW 47th Terrace.
- 6" WM crossing I-95 at NW 46th Street.
- 20" WM crossing I-95 at NW 36th Street.
- 6" WM crossing I-95 at NW 32nd Street.
- 8" WM crossing I-95 at NW 29th Street.

- 36" WM crossing I-95 at NW 28th Street.
- 8" WM along NW 6th Avenue from NW 35th Street to NW 29th Street then reducing to a 6" WM from NW 29th Street to NW 28th Street.
- 6" WM along NW 6th Court from NW 32nd Street to NW 30th Street.
- An existing 6" WM, 12" WM, and 36" WM running along NE 36th Street from NE 5th Avenue to NE 7th Avenue.
- A 6" WM and 12" WM continuing westward along NE 36th Street to a point west of Biscayne Boulevard.

Sewer Infrastructure:

- An 8" gravity sewer along NE 36th Street between Biscayne Boulevard and NE 7th Avenue with a 10" gravity sewer in proximity.
- 60" gravity sewer crossing I-195 at Biscayne Boulevard with 8" gravity sewers running along either side.
- 36" FM crossing I-195 at Federal Highway.
- 8" gravity sewer crossing I-195 at NE 2nd Avenue.
- 8" gravity sewer crossing I-195 at NE 1st Court.
- 12" gravity sewer crossing I-195 at NW 3rd Avenue.
- 8" gravity sewer along the south side of the I-95 ramp to I-195 eastbound.
- 8" gravity sewer along NE 38th Street from NE 1 Avenue to NE 2 Avenue.
- 8" gravity sewer along NW 40th Street from NW 10th Avenue to NW 14th Avenue.
- 8" gravity sewer along NW 39th Street from NW 10th Avenue to NW 14th Avenue.
- 15" gravity sewer and 8" gravity sewer crossing I-95 at NW 54th Street.
- 10" gravity sewer crossing I-95 at NW 53rd Street.
- 10" gravity sewer crossing I-95 at NW 46th Street.
- 10" gravity sewer along NW 6th Court from NW 50th Street to NW 46th Street.
- 8" gravity sewer along NW 6th Avenue from NW 34th Street to NW 33rd Street.
- 10" gravity sewer along NW 6th Avenue from NW 33rd Street to NW 32nd Street.
- 12" gravity sewer crossing I-95 at NW 32nd Street.
- 8" gravity sewer along NW 6th Court from NW 32nd Street to NW 30th Street.
- 21" gravity sewer crossing I-95 at NW 29th Street.
- Encased gravity sewer crossing I-95 at NW 28th Street.

4.7.12 TECO People's Gas

TECO People's Gas operates the following facilities within the study limits:

- 6" steel Gas Main (GM) along Alton Road. crossing 41st Street/Arthur Godfrey Road.
- 3" steel GM along North Meridian Ave. crossing 41st Street/Arthur Godfrey Road.
- 6" plastic GM crossing Alton Road at Sullivan Drive.
- 4" plastic GM in 8" Cast Iron Pipe (CIP) casing along Federal Highway crossing I-195.
- 3" galvanized iron GM along NE 2nd Avenue crossing I-195.
- 2" plastic GM along NE 1st Court crossing I-195.
- 4" plastic GM in 8" CIP casing along NE 1st Avenue crossing I-195.
- 1.25" plastic GM in 2" galvanized iron casing along NE 38th Street from Biscayne Boulevard to NE 6th Avenue.
- 2" iron pipe along alleyway south of NE 38th Street from NE 5th Court to NE 6th Avenue.

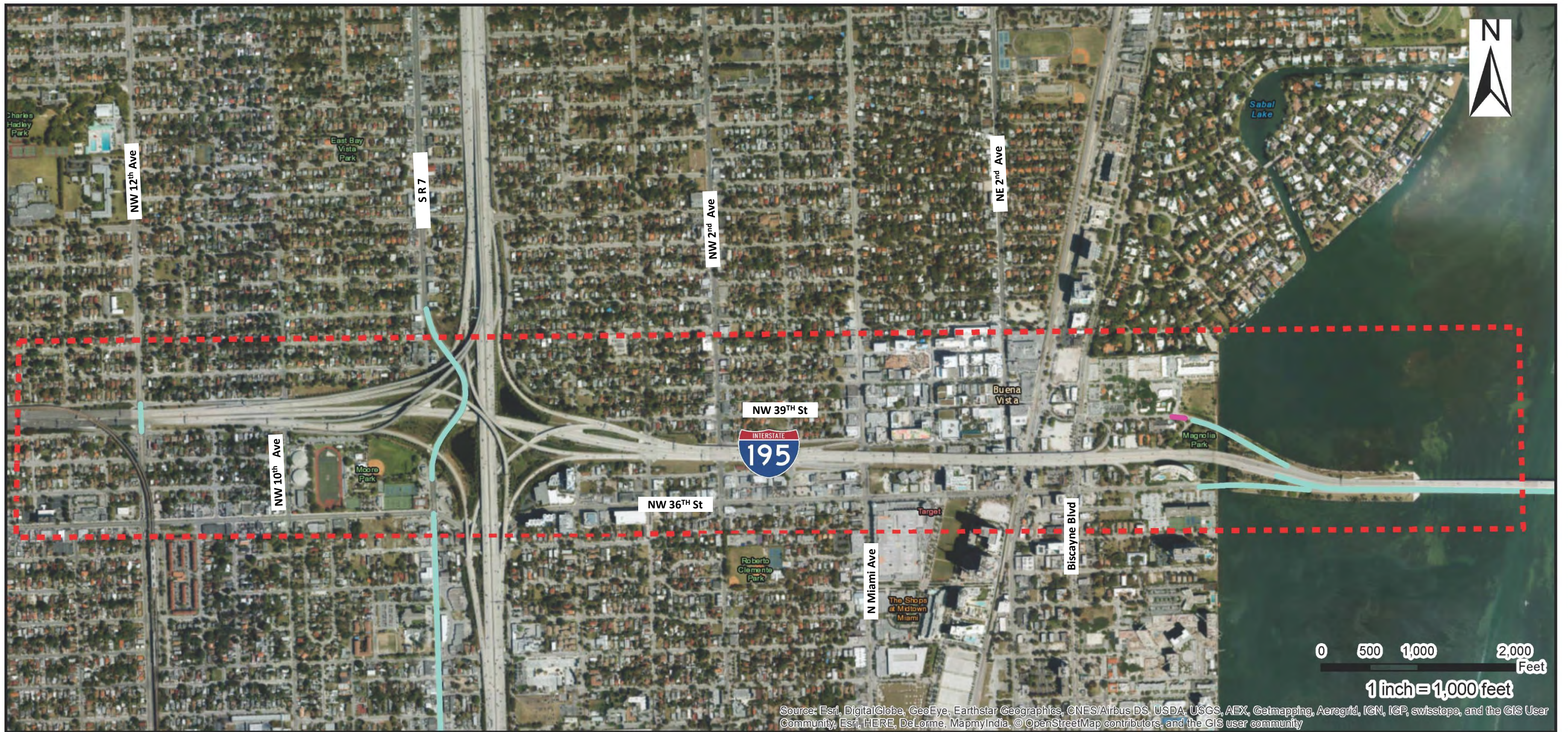
- 2" galvanized iron GM and 6" steel GM along NE 36 Street from Biscayne Boulevard to NE 7th Avenue.
- Abandoned 2" galvanized iron GM crossing Biscayne Boulevard along NE 36th Street.
- 2" plastic GM along NE 36th Street from Biscayne Boulevard to Federal Highway.
- 4" plastic GM in 8" CIP casing along NE 36th Street from Federal Highway to NW 2 Avenue.
- 4" plastic GM in 10" CIP casing along NW 36th Street from NW 2nd Avenue to NW 5th Avenue.
- 4" CIP GM along NW 37 Street from North Miami Avenue to NW 2nd Avenue.
- Abandoned 2" galvanized iron GM along NW 37th Street at NW 5th Avenue running westward.
- 4" steel GM crossing I-195 at NW 3rd Avenue with regulator station located in proximity to the northwest edge of the I-195 overpass.
- 4" plastic GM along NW 54th Street crossing under I-95.
- 2" steel GM along NW 46th Street crossing I-95.
- 3" steel GM with 2" plastic liner crossing SR-112 along NW 10th Avenue.
- 6" steel GM crossing along NW 12th Avenue crossing under SR-112.
- 2" plastic GM within 4" CIP casing crossing I-95 at NW 29th Street.

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4.8 Pedestrian and Bicycle Facilities

FDOT's RCI database was reviewed for information related to existing bicycle and pedestrian features within the study area. The information contained in the database for bicycle (bike) features include bike lanes and bike slots or "Keyhole Lanes" which are bike lanes transitioning between a thru lane and a right turn lane. For sidewalk features, the database contains general information of sidewalks maintained by FDOT in addition to which of those sidewalks include barriers that separate them from motorized vehicle lanes (eg. On-street parking, physical traffic barriers, guardrail, trees, etc). **Exhibits 4-11** through **4-14** depict the spatial layout of bicycle and pedestrian features in the Cities of Miami and Miami Beach within the limits of the study area.

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Source: FDOT Transportation Data and Analytics Office RCI Database <http://www.fdot.gov/statistics/gis/>

Legend

-  Study Area
-  Bike Slot
-  Bike Lane

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No.440228-1-22-01



Exhibit Name:

Existing Bicycle Features - City of Miami

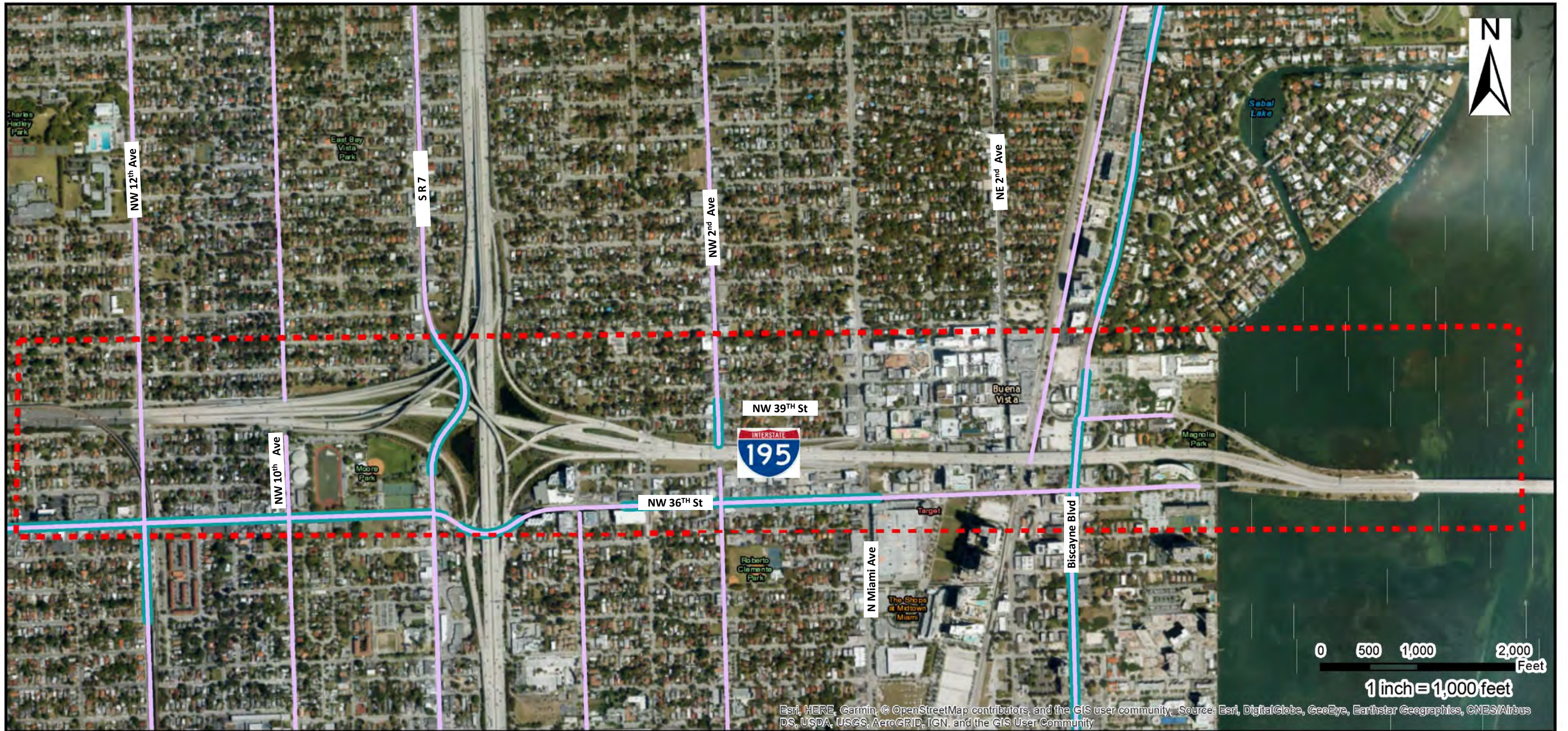
Report Title:

Existing Conditions Report

Exhibit No. 4-11

Page No.

Date: 05/31/18



Esri, HERE, Garmin, © OpenStreetMap contributors, and the GIS user community. Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

Source: FDOT Transportation Data and Analytics Office RCI Database <http://www.fdot.gov/statistics/gis/>

Legend

- Study Area
- Sidewalk Barrier
- Sidewalks Maintained by FDOT

<p>Project Name:</p> <p>I-95 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road FM No.440228-1-22-01</p>	<p>Exhibit Name:</p> <p style="text-align: center;">Existing Pedestrian Features - City of Miami</p>	<p>Report Title:</p> <p style="text-align: center;">Existing Conditions Report</p>	<p>Exhibit No. 4-12</p> <p>Page No.</p> <p>Date: 05/31/18</p>
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Source: FDOT Transportation Data and Analytics Office RCI Database <http://www.fdot.gov/statistics/gis/>

Legend

- Study Area
- Bike Lane
- Bike Slot

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No.440228-1-22-01



Exhibit Name:

Existing Bicycle Features - City of Miami Beach

Report Title:

Existing Conditions Report

Exhibit No. 4-13

Page No.

Date: 05/31/18



Source: FDOT Transportation Data and Analytics Office RCI Database <http://www.fdot.gov/statistics/gis/>

Legend

- Study Area
- Sidewalk Barrier
- Sidewalks Maintained by FDOT

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No.440228-1-22-01



Exhibit Name:

Existing Pedestrian Features - City of Miami Beach

Report Title:

Existing Conditions Report

Exhibit No. 4-14

Page No.

Date: 05/31/18

4.9 Drainage System

The drainage systems for the following roadway segments were reviewed as part of this planning study:

- I-195 (SR 112/Julia Tuttle Causeway) from NW 16th Avenue to Alton Road
- I-95 from NW 26th Street to NW 62nd Street
- US 27 (NW 36th Street) from NW 1st Avenue to NE 5th Avenue
- US 1 (Biscayne Boulevard) from NE 35th Terrace to NE 38th Street
- Alton Road North- from Chase Avenue to Sullivan Drive/Mt. Sinai Driveway.
- Alton Road South- from 43rd Street to Chase Avenue/North Bay Road

A public record request was made to the FDOT District Six Office and several plans sets for the roads included in the study area were obtained and evaluated. The offices of the Drainage Department and the South Dade Maintenance within FDOT D6 were also contacted but no additional information was obtained. A field visit was conducted during the study after several days of heavy rain to observe any existing ponding or drainage deficiencies in the project area.

4.9.1 I-195 (SR 112/Julia Tuttle Causeway) from NW 16th Avenue to Alton Road

The existing drainage system includes conveyance of storm-water runoff collected mainly by inlets (including Type S, Type B, barrier wall and valley gutter inlets) located on both sides of the road. The runoff is treated and discharged to several different 24-inch deep wells and French drains. No data were available to confirm the efficiency and current operation of the system in terms of capacity or its current effectiveness in meeting water quality and peak discharge. No flooding or existing drainage issues were observed during the field visit.

4.9.2 I-95 from NW 26th Street to NW 62nd Street

The existing drainage system includes conveyance of storm-water runoff collected mainly by inlets (including Type B, Type C and Barrier Wall Inlets) located on both sides of the road. The runoff is treated and attenuated by French Drains. No data were available to confirm the efficiency and current operation of the system in terms of capacity or its current effectiveness in meeting water quality and peak discharge. No flooding or existing drainage issues were observed during the field visit.

4.9.3 US 27 (NW 36th Street) from NW 1st Avenue to NE 5th Avenue

US 27 is a curb and gutter road with a drainage system consisting of a pipe and inlet (Type P-2, P-5, P-6 inlets) network. The runoff is discharged to several different 24-inch deep wells. No data were available to confirm the efficiency and current operation of the system in terms of capacity or its current effectiveness in meeting water quality and peak discharge. No flooding or existing drainage issues were observed during the field visit.

4.9.4 US 1 (Biscayne Boulevard) from NE 35th Terrace to NE 38th Street

This segment of Biscayne Boulevard is a curb and gutter road with the drainage system consisting of a pipe and inlet (Type P-3, P-5, P-6, P-7 inlets) network. The runoff is collected and pumped to a 24-inch deep well by way of a pump station located on the NE corner of Biscayne Boulevard and NW 17th Terrace. No data were available to confirm the efficiency and current operation of the system in terms of capacity or its current effectiveness in meeting water quality and peak discharge. No flooding or existing drainage issues were observed during the field visit.

4.9.5 Alton Rd North and South from Chase Ave Mt. Sinai Driveway and 43rd St to Chase Ave

Alton Road within these limits is a curb and gutter road with the drainage system consisting of a pipe and inlet network. No information was available to confirm the outfall provided for the existing system. No data were available to confirm the efficiency and current operation of the system in terms of capacity or its current effectiveness in meeting water quality and peak discharge. Minor gutter ponding but no flooding or other existing drainage issues were observed during field visit.

4.9.6 Regional Drainage Basin

The project limits lie within the C-7, C-6 and the Intercoastal South Florida Water Management District (SFWMD) Drainage Basin. A copy of the SFWMD drainage map, is included in **Appendix B**. The northern limits of the study area are located inside the C-7 basin. The C-7 basin discharges into the Little River Canal (WBID 3287) via the S-27 control structure to the Biscayne Bay. The Little River canal is listed in the Florida Department of Environmental Protection's Statewide comprehensive verified list of impaired waters due to fecal coliform.

The west and south segments of the intersection of I-95 and SR 112 are located within the C-6 basin. The C-6 basin discharges into the Miami Canal via the S-26 control structure to Biscayne Bay. The Miami canal is listed in the Florida Department of Environmental Protection's Statewide comprehensive verified list of impaired waters due to fecal coliform.

Finally, the eastern segments of the study area lie inside the Intercoastal SFWMD basin. The Intercoastal basin outfalls to the Biscayne Bay Intracoastal (ICWW) (WBDID 3226 H), which is listed in the Florida Department of Environmental Protection's Statewide comprehensive verified list of impaired waters due to Nutrients (Chlorophyll-a).

4.9.7 Federal Emergency Management Agency (FEMA) Floodplain

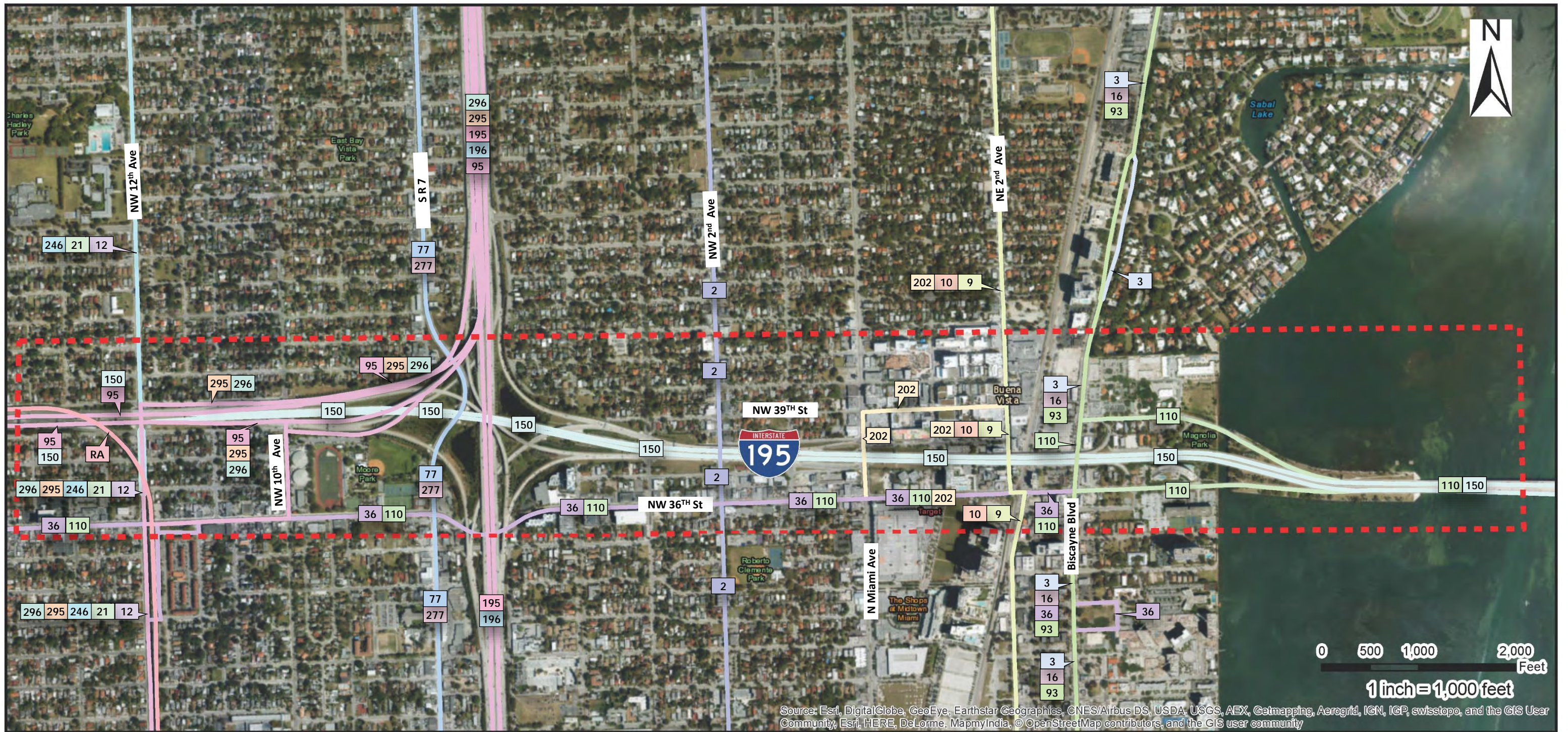
The project corridor lies within the FEMA Flood Insurance Rate Map (FIRM) Panel No. 12086C0304L, 12086C0308L, 12086C0309L, 12086C0312L, 12086C0316L and 12086C0317L effective September 11, 2009. The eastern segments of the project area is identified as being in the Special flood hazard areas subject to inundation by the 1 percent annual chance flood (100-yr flood) (Zone AE) with a base flood elevation ranging from 7 to 10 feet (NGVD). In addition, the north, south and west segments of the project area lie in areas determined to be outside of the 0.2 percent annual chance floodplain (Zone X). Refer to **Appendix B** for the FEMA Map FIRMETTES.

5.0 EXISTING TRANSIT

The study area is currently served by several transit service bus routes that either operate within the cities of Miami and Miami Beach for intra city travel or between the cities of Miami and Miami Beach for intercity travel. Miami-Dade Transit (a division of the Miami-Dade Department of Transportation and Public Works - DTPW) is the agency responsible for operating a majority of the existing transit service in the study area followed by the City of Miami which operates three trolley routes and the City of Miami Beach which operates one trolley route. **Exhibits 5-1** through **5-4** on the following pages, depict the transit routes operating within the study area.

It should be noted that given the plethora of overlapping Miami-Dade Transit (MDT) routes within the study area, the chromatic options that are available to effectively differentiate between these routes are severely limited. Labels were introduced to help delineate the different overlapping routes. However, it is anticipated that the individual Miami-Dade Transit route maps that are included in **Appendix C**, can help clarify the paths of the various transit routes if the reader desires more detailed information.

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Legend

Study Area	MDT Route 9	MDT Route 21	MDT Route 95	MDT Route 196	MDT Route 295
Route Name	MDT Route 10	MDT Route 36	MDT Route 110	MDT Route 202	MDT Route 296
MDT Route 2	MDT Route 12	MDT Route 77	MDT Route 150	MDT Route 246	MDT Route RA*
MDT Route 3	MDT Route 16	MDT Route 93	MDT Route 195	MDT Route 277	

Project Name:

I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road
FM No.440228-1-22-01



Exhibit Name:

Miami-Dade Bus Transit Routes in City of Miami

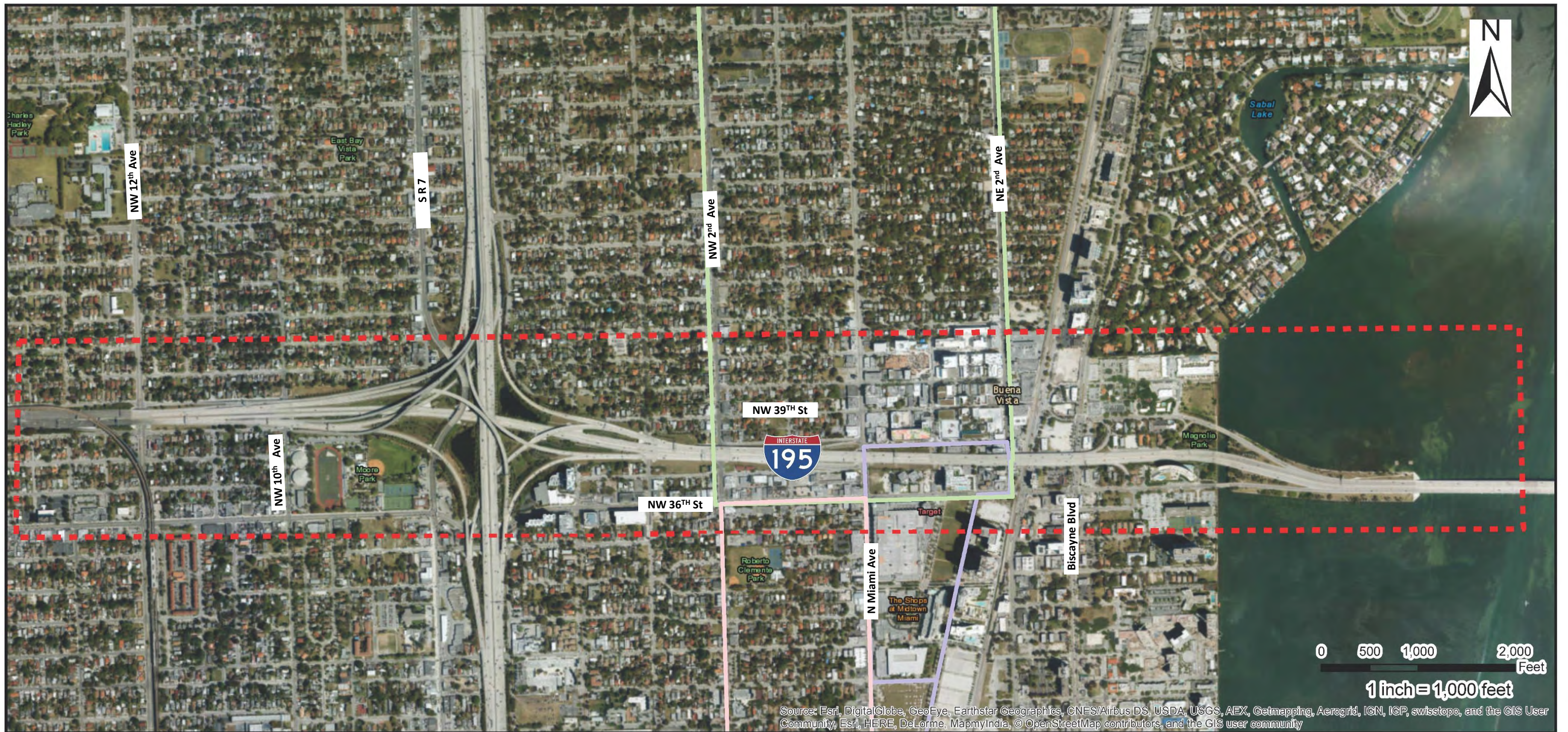
Report Title:

Existing Conditions Report

Exhibit No. 5-1

Page No.

Date: 05/31/18



Legend

Study Area

Route Name

Route Biscayne

Route Little Haiti

Route Wynwood

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No.440228-1-22-01



Exhibit Name:

City of Miami Trolley Routes

Report Title:

Existing Conditions Report

Exhibit No. 5-2

Page No.

Date: 05/31/18



Legend

Study Area

Route Name

MDT Route 103

MDT Route 110

MDT Route 113

MDT Route 115

MDT Route 150

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No.440228-1-22-01



Exhibit Name:

Miami-Dade Bus Transit Routes in City of Miami Beach

Report Title:

Existing Conditions Report

Exhibit No. 5-3

Page No.

Date: 05/31/18



Legend

-  Study Area
-  Route Mid-Beach Loop Trolley

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No.440228-1-22-01



Exhibit Name:

City of Miami Beach Trolley Routes

Report Title:

Existing Conditions Report

Exhibit No. 5-4

Page No.

Date: 05/31/18

5.1 Transit Ridership

Available transit ridership data for existing bus service were gathered from the maintaining agencies. **Table 5-2** below, presents a summary of the information gathered.

Table 5-2: Transit Service Ridership

Transit Service	Average Daily Ridership ⁷							
	Weekday		Saturday		Sunday		Monthly	
	Volume	Δ % Change ²	Volume	Δ % Change ²	Volume	Δ % Change ²	Volume	Δ % Change ²
MDT 2	2,312	-11.8%	1,470	-3.2%	1,064	-3.2%	62,477	-11.7%
MDT 3	5,380	-16.2%	6,149	-7.5%	4,545	-10.8%	167,288	-14.1%
MDT 9	5,385	-12.7%	3,365	-9.5%	2,579	3.5%	145,622	-12.6%
MDT 10	2,211	-19.4%	1,916	-7.5%	1,410	-1.5%	63,869	-17.2%
MDT 12	2,546	-3.6%	-	-	-	-	56,015	-7.8%
MDT 16	1,904	-27.9%	1,651	-19.4%	1,003	-13.2%	54,147	-26.4%
MDT 21	1,756	-3.3%	-	-	-	-	38,640	-7.5%
MDT 36	2,110	-9.7%	1,380	43.1%	1,060	3.2%	57,555	-6.7%
MDT 77	7,707	-16.7%	4,572	-9.8%	3,054	-5.6%	204,625	-16.8%
MDT 93 Biscayne Max	3,577	-8.0%	-	-	-	-	78,704	-12.0%
MDT 95	2,020	1.0%	-	-	-	-	44,448	-3.4%
MDT 103	429	-82.4%	289	-83.9%	200	-87.7%	11,674	-83.3%
MDT 110 Route J³	2,409	12.6%	1,818	23.9%	1,363	-1.2%	67,535	11.5%
MDT 113	773	16.6%	499	69.4%	362	-2.6%	20,954	17.0%
MDT 115 Mid-North Beach Connection	122	-70.6%	93	-52.5%	68	-68.2%	3,409	-69.4%
MDT 150 Miami Beach Airport Express	1,810	5.3%	1,588	24.6%	1,580	-8.0%	54,082	5.0%
MDT 202 Little Haiti Connection	156	-12.3%	53	-11.0%	27	-61.5%	3,808	-25.0%
MDT 246 Night Owl	15	-72.5%	18	-73.1%	12	-81.6%	468	-73.7%
MDT 277								

Notes:

1. Source; Miami-Dade Department of Transportation and Public Works Ridership Technical Report | March 2018.
2. Source; year over year change reported in the March 2018 Technical Report.
3. This route was included in the Miami-Dade DTPW "Top 5 Bus Routes Average Weekday Ridership Performance Year Over Year Comparisons" reported in the March 2018 Technical Report.

As can be seen from **Table 5-2**, several of the routes operating within the study area, experienced a reduction in year over year ridership with the MDT 103 on Miami Beach, MDT 246 Night Owl and MDT 115 Mid-North Beach Connection reporting the largest reductions of 83.3 percent, 73.7 percent and -69.4 percent respectively on an average monthly basis. MDT 110 Route J, MDT 113 and MDT 150 Miami Beach Airport Express were the three routes that experienced a year over year increase in ridership of 11.5 percent, 17.0 percent and 5.0 percent respectively.

MDT 110 Route J was reported as being in the top five bus routes for average weekday ridership year over year performance, signaling a high demand for this existing transit service. Ridership data excerpted from the Miami-Dade Department of Transportation and Public Works Ridership Technical Report, March 2018 are included in **Appendix C** of this report.

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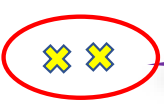
6.0 TRAFFIC DATA

Traffic data were collected along the project corridor at various mid-block locations and major intersections. The data collected included the following: 72-hour average daily traffic (ADT) counts, turning movement counts, and travel time runs. In addition to the traffic field data collected, historical annual average daily traffic (AADT) data from the FDOT Traffic Online Database was also obtained to supplement and provide traffic data along the corridor. **Exhibits 6-1 through 6-4**, illustrate the data collection plan that was approved by the FDOT District 6 Planning and Environmental Management Office (PLEMO). The remainder of this section describes the traffic data obtained. A separate traffic data collection report prepared by Caltran Engineering Group, is provided in **Appendix D**.

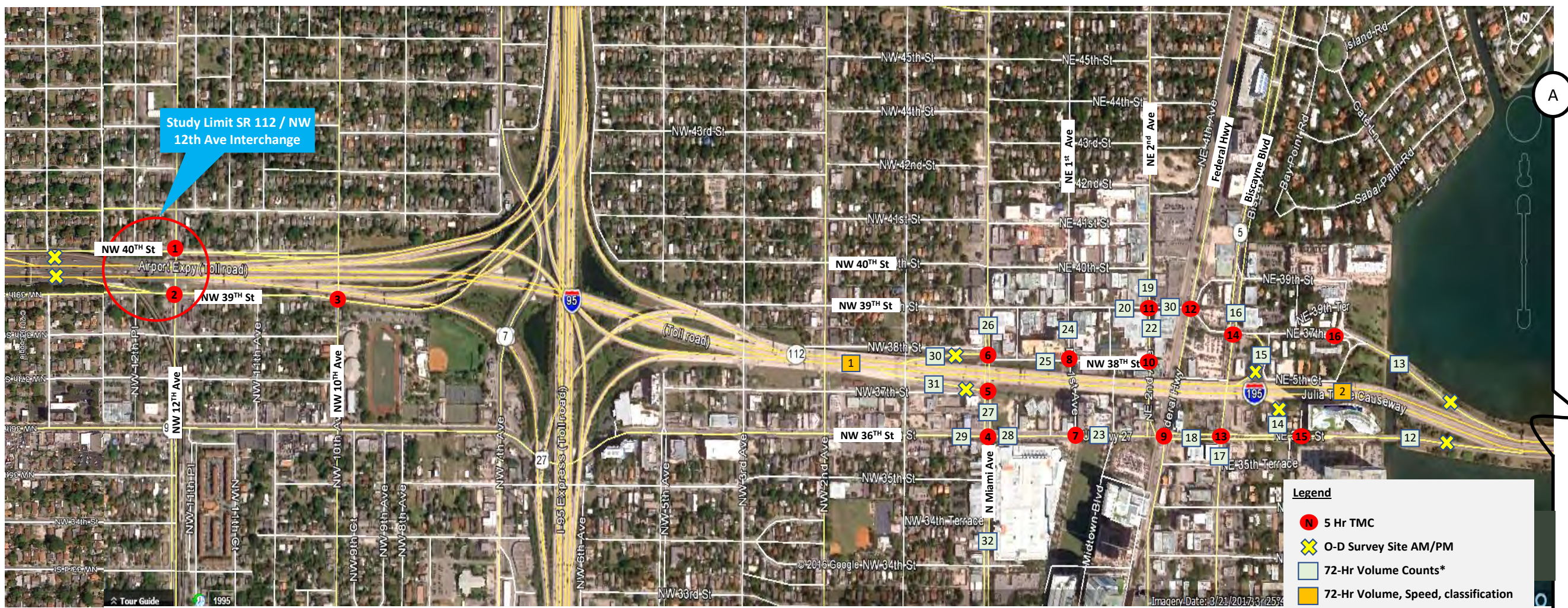
6.1 Origin-Destination Data Collection

Origin-destination data were collected to capture the route choice between origins and destinations within the study area on I-195 and I-95. Twelve BlueTOAD (Bluetooth Travel-time Origin and Destination) devices were placed to capture the vehicles entering or exiting the study area, two of them were installed on I-95 close to Golden Glades Interchange (GGI) to capture the traffic moving between Miami Beach and North Miami Dade towards Broward County. The data collection locations are shown in **Exhibits 6-1 through 6-4**. Further data collection information with summaries, is included in the detailed traffic data collection report in **Appendix D**.

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Set O-D Stations for I-95 NB and SB near GGI (not shown) to capture percentage of trips to and from the Beach that could potentially use express lanes if there was a connection. See Sheet 4 for location details.



Project Name:



I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

Existing Traffic Data Collection Plan (Sheet 1 of 4)

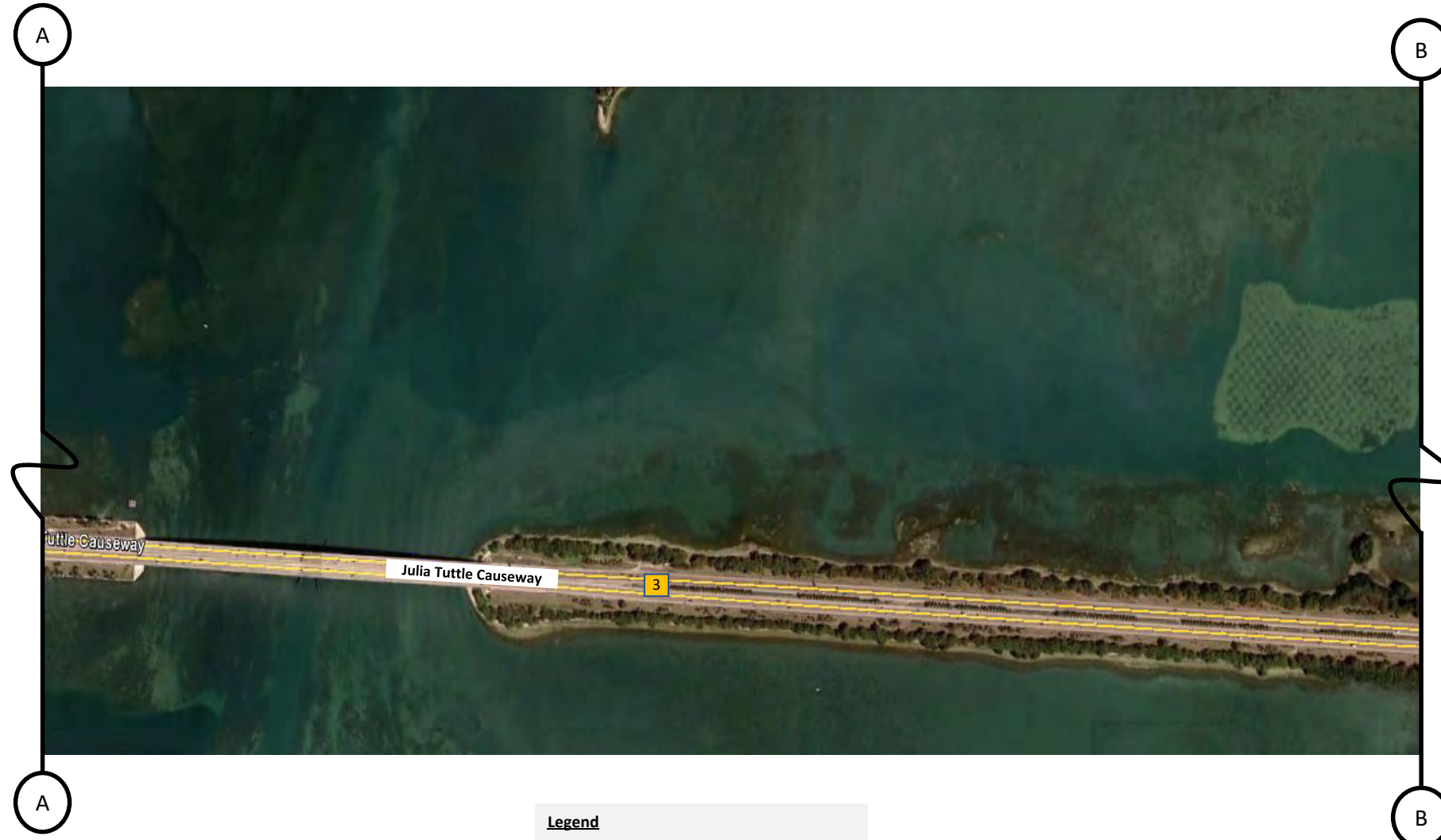
Report Title:

Existing Conditions Report

Exhibit No. 6-1

Page No.

Date: 09/12/18



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- 5 Hr TMC
- ✕ O-D Survey Site AM/PM
- 72-Hr Volume Counts*
- 72-Hr Volume, Speed, classification

* Count may be bi-directional based on location

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



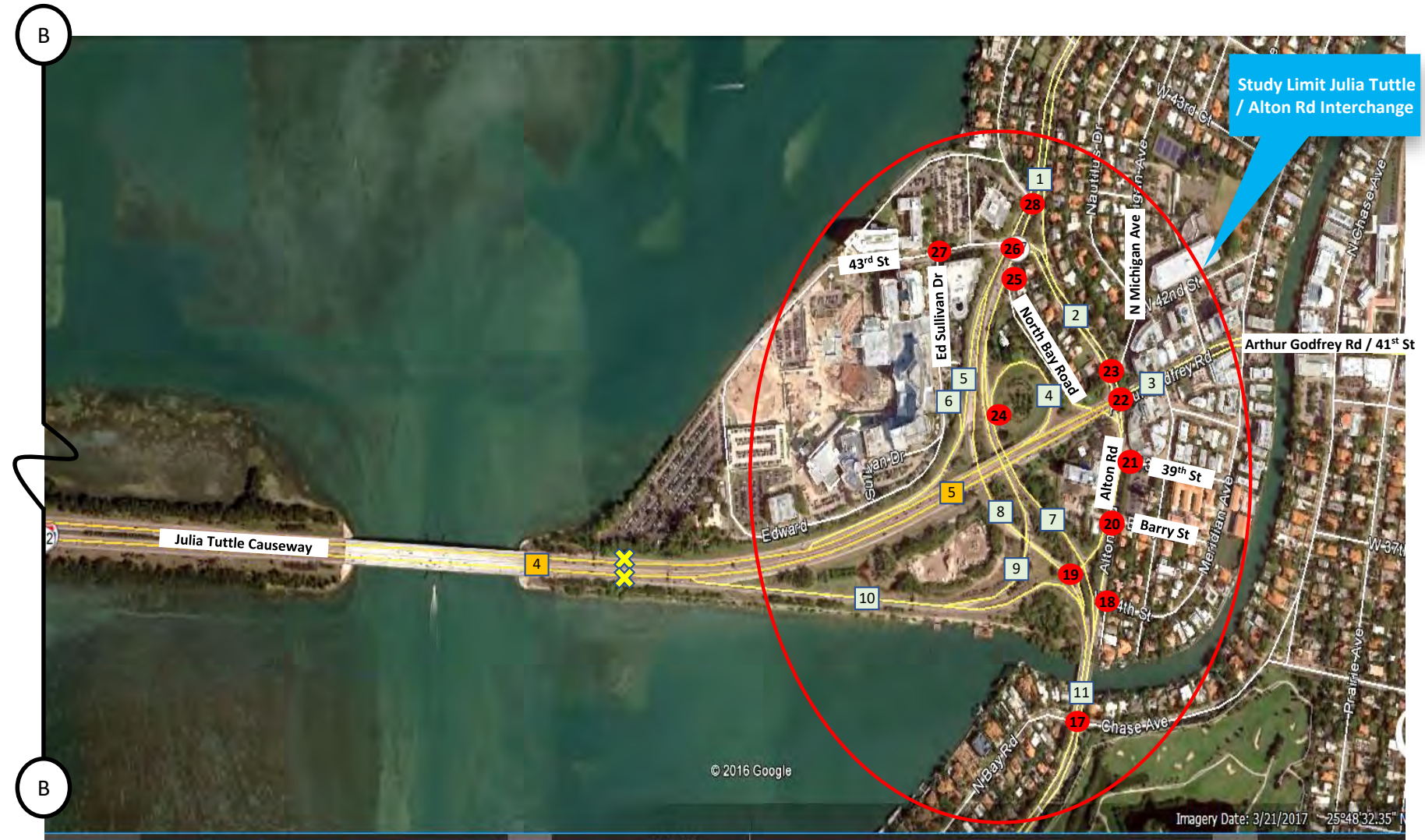
Exhibit Name:

Existing Traffic Data Collection Plan (Sheet 2 of 4)

Report Title:

Existing Conditions Report

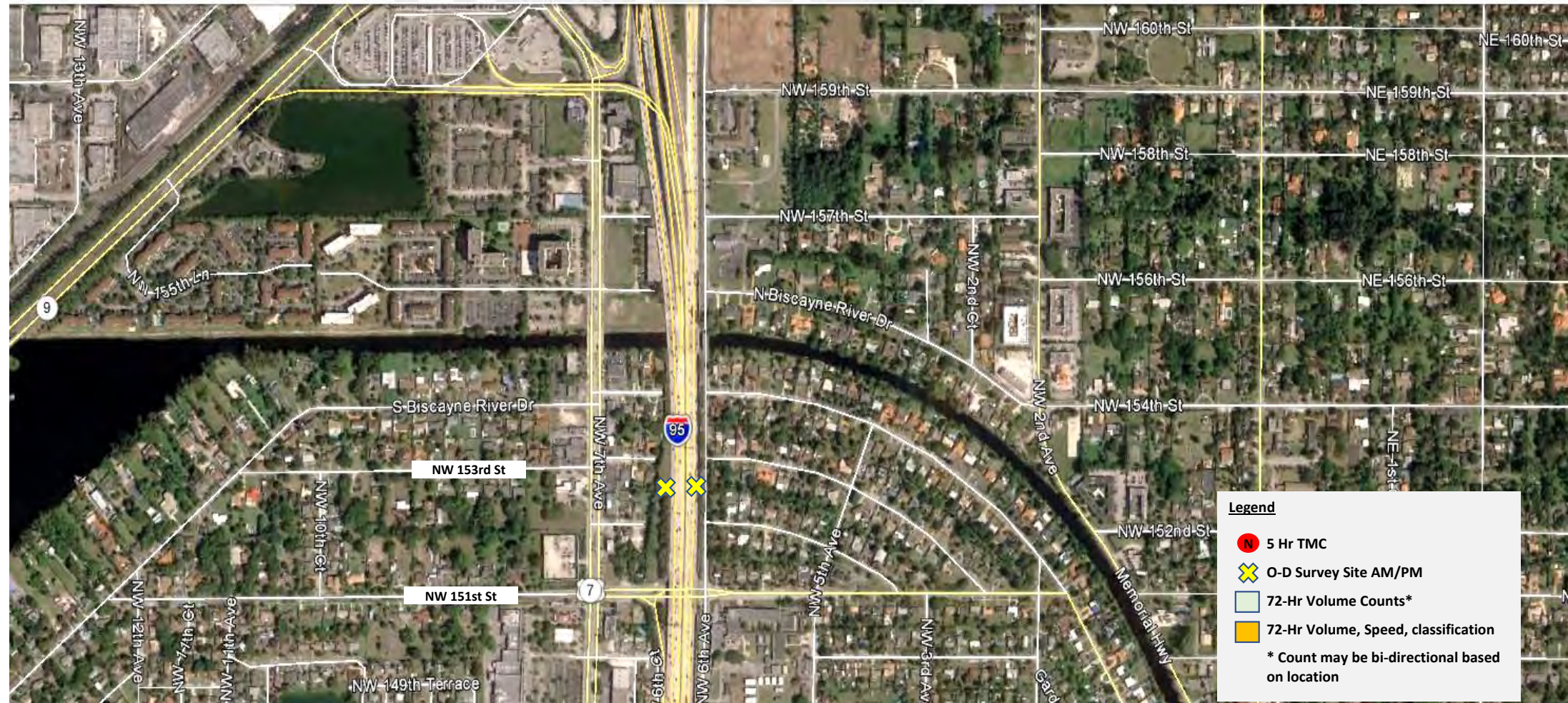
Exhibit No.	6-2
Page No.	
Date:	09/12/18



Legend

- 5 Hr TMC
- O-D Survey Site AM/PM*
- 72-Hr Volume Counts*
- 72-Hr Volume, Speed, classification

* Count may be bi-directional based on location



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

Existing Traffic Data Collection Plan (Sheet 4 of 4)

Report Title:

Existing Conditions Report

Exhibit No. 6-4

Page No.

Date: 09/12/18

6.2 72-Hour Traffic Station Volumes

72-hour bi-directional machine counts on arterials and one-way counts on ramps were recorded in 15-minute intervals at 32 locations. Data were collected for three consecutive days as follows: October 17th, 2017 through October 19th, 2017 for 11 stations, October 24th, 2017 through October 26th, 2017 for 14 stations and October 31st, 2017 through November 2nd, 2017 for 6 stations. The traffic station locations (shown on **Exhibits 6-1** through **6-4**) are as follows:

1. Alton Road, north of North Bay Road and south of W 44th Street
2. Alton Road, north of Nautilus Road
3. 41st Street, east of Alton Road
4. Ramp 87037202
5. Southbound Alton Road to westbound I-195
6. Ramp from Mount Sinai Medical Center to westbound I-195
7. Northbound Alton Road, 300' south of I-195 and about 1400' north of Chase Avenue
8. Southbound Alton Road, south of I-195
9. Ramp 87037201: Ramp 87004024 to northbound Alton Road
10. Ramp 87004024: eastbound I-195 to Alton Road
11. Alton Road, North of Chase Avenue
12. Ramp 87004023: NE 36th Street to eastbound I-195
13. Ramp 87004022: Westbound I-195 to NE 38th Street
14. Ramp 87004021: Eastbound I-195 to Westbound NE 36th Street
15. Ramp 87004020: NE 38th Street to Westbound I-195
16. US-1, north of NE 38th Street
17. US-1, south of NE 36th Street (Under Construction)
18. NE 36th Street, East of Federal Highway
19. NE 2nd Avenue, north of NE 39th Street
20. NE 39th Street west of Northwest 2nd Avenue
21. NE 39th Street east of NE 2nd Avenue
22. NE 2nd Avenue south of NE 39th Street
23. NE 36th Street, east of NE 1st Avenue
24. NE 1st Avenue, north of NE 38th Street
25. NE 38th Street, west of NE 1st Avenue
26. North Miami Avenue, north of NE 38th Street
27. North Miami Avenue, north of NE 36th Street
28. NE 36th Street, east of North Miami Avenue
29. NE 36th Street, west of North Miami Avenue
30. Ramp 87004018: North Miami Avenue to Westbound I-195
31. Ramp 87004019: Eastbound SR-112 to north Miami Avenue
32. North Miami Avenue, north of NE 34th Street

The 72-Hour volume data were supplemented with additional traffic obtained from the Florida Traffic Online database portable traffic monitoring sites (PTMS) for the ramps on the interchange of I-95 and I-195. Further data collection information with summaries is included in the traffic data collection report in **Appendix D**.

6.3 72-Hour Classification Counts

72-Hour traffic counts were recorded at five locations along I-195. Vehicles were classified into 14 categories from motorcycles to multi-axle multi-trailer trucks and vehicle speeds were recorded. These counts were recorded in 15-minute intervals on three consecutive days from October 24th, 2017 through October 26th, 2017 at the following five stations:

1. SR-112, east of Northwest 2nd Avenue
2. SR-112, east of US 1
3. SR-112, east of Intercostal Waterway Bridge
4. SR-112, east of Biscayne Bay Bridge
5. SR-112, west of Alton Road.

Further data collection information with summaries, is included in the traffic data collection report in **Appendix D**.

6.4 Turning Movement Counts

Turning movement counts were conducted at the major study intersections and ramp termini after conducting a review of existing traffic data (from the Florida Online Traffic Database – FTI) to determine peak periods and peak spreading of traffic volumes. The following sections describe the review of the peak periods as well as the efforts to collect turning movement counts.

6.4.1 Peak Spreading Review

A review of the peak spreading (aka duration of congestion) around the AM and PM peak periods was conducted within the study area considering the traffic analysis needs of the study. Existing 24-hour traffic volume profiles based on Florida FTI data were evaluated to determine the extent to which the AM and PM peak demands spread over multiple hours. Based on the FTI data, the morning peak period was between 7:00 AM and 9:00 AM but the PM peak period was between 3:00 PM and 6:00 PM. Upon approval from the FDOT District 6 PLEMO, 5-hour Turning Movement Counts were conducted at the study intersections. Further information documenting the peak spreading review, coordination and approval is included in **Appendix D**.

6.4.2 5-Hour TMC Data Collection

Five-hour vehicle TMCs were obtained for the 28 intersections in the study area. The TMC consists of 2 hours from 7:00 AM to 9:00 AM in the morning (AM) peak period and 3 hours from 3:00 PM to 6:00 PM in the evening (PM) peak period on the dates of October 17th, 2017 for 13 intersections and on October 24th, 2017 for 15 locations. This collection effort included the information for trucks, pedestrians and bicycle counts along with the passenger car counts for each movement at the intersections. TMCs were counted at the following intersections:

1. NW 12th Avenue & NW 40th Street
2. NW 12th Avenue & NW 39th Street
3. NW 10th Avenue & NW 39th Street
4. North Miami Avenue & NW 36th Street
5. North Miami Avenue & SR-112 Eastbound Off-Ramp
6. North Miami Avenue & NW 38th Street
7. NE 1st Avenue & NE 36th Street
8. NE 1st Avenue & NE 38th Street
9. North Federal Highway & NE 36th Street
10. NE 2nd Avenue & NE 38th Street
11. NE 2nd Avenue & NE 39th Street
12. North Federal Highway & NE 39th / 38th Street
13. US-1 Biscayne Boulevard & NE 36th Street
14. US-1 Biscayne Boulevard & NE 38th Street
15. NE 5th Avenue & NE 36th Street
16. NE 6th Avenue & NE 38th Street
17. Alton Road and Chase Avenue
18. Alton Road and West 34th Street
19. SB Alton at EB off-ramp (87004024)
20. Alton Road and Barry Street
21. Alton Road and West 39th Street
22. Alton Road and 41st Street
23. Alton Road and Nautilus Road
24. NB Alton at WB on-ramp (87037202)
25. North Bay Road
26. Alton Road and 43rd Street
27. Mount Sinai Emergency Entrance and Sullivan Drive
28. Alton Road at North Bay Road.

6.5 Travel Time Runs

Travel time runs were performed to obtain the travel time, average speed and delay statistics along I-195 mainline (both eastbound and westbound) during the AM and PM peak hours. The runs were performed on the dates of February 14th, 2018 and February 15th, 2018 from 7:00 AM to 9:00 AM in the morning peak period and from 3:00 PM to 6:00 PM in the evening peak period. **Table 6-1** summarizes the output statistics for the eastbound and westbound runs. Additional travel time information is included in the traffic data collection report in **Appendix D**.

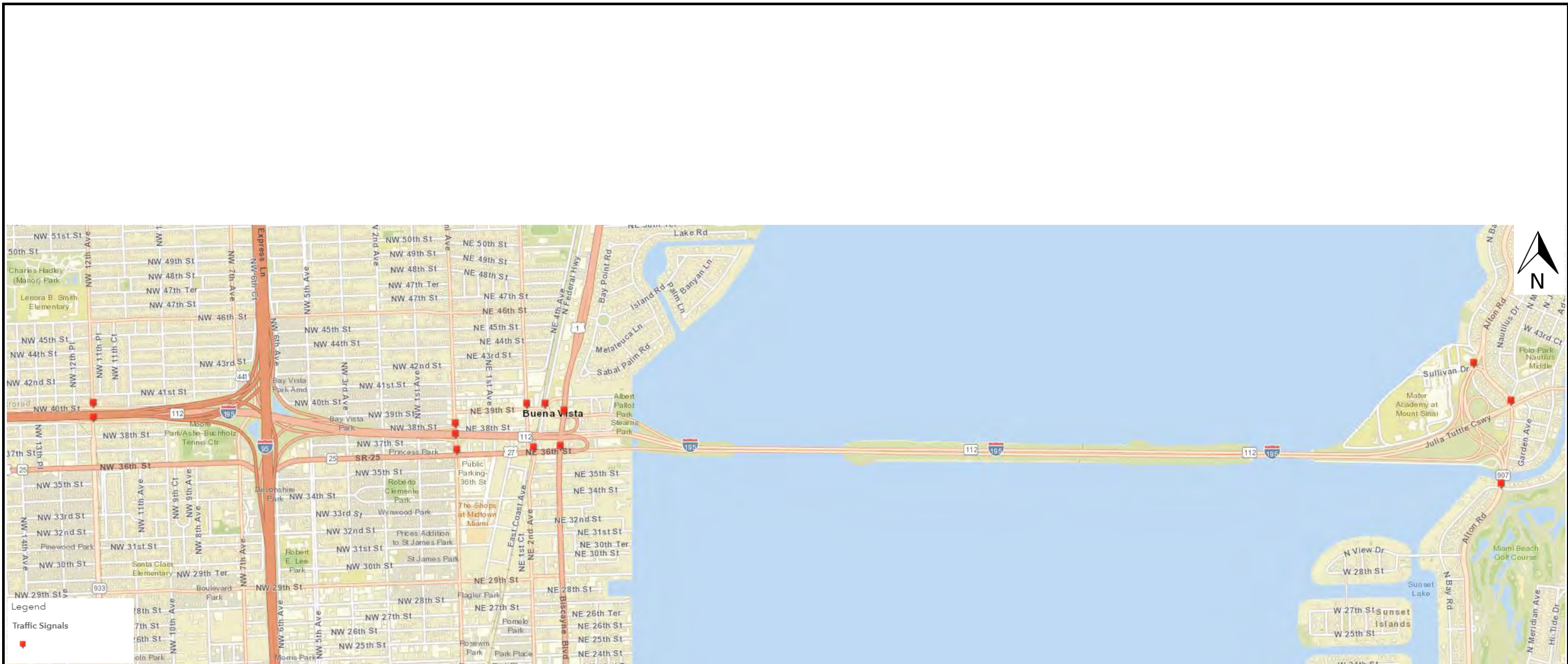
Table 6-1: I-195 Travel Time Summary

Direction	Limits	Peak Period	Travel Time (Sec)	Average Speed (MPH)
Eastbound	From NW 13th Avenue to Alton Road	AM	619.7	30.3
		PM	493	38
Westbound	From Alton Road to NW 13th Avenue	AM	360.5	51.2
		PM	719.7	25.7

6.6 Traffic Signals

There are 13 signalized intersections within the study area. The timing plans for the signalized intersections were obtained from the Miami-Dade Traffic Signs & Signals Division database and are included in **Appendix D. Exhibit 6-5** shows the location of the signalized intersections. The existing signal timing data will be used in the existing traffic operations analysis which is being documented in a separate report as part of the I-195 CPS.

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Project Name:



Exhibit Name:

Location of Traffic Signals in Study Area

Report Title:

Existing Conditions Report

Exhibit No. 6-5

Page No.

Date: 09/12/18

6.7 Field reviews

Field visits were conducted during the weekday AM and PM peak periods to qualitatively assess traffic operations at the study intersections and document existing bottlenecks or choke points throughout the study network. The AM and PM field reviews were performed on Tuesday, October 17th, 2017, and Wednesday, October 18th, 2017 (clear days) from 7:00 AM to 9:00 AM and 3:00 PM to 6:00 PM respectively. The findings are summarized in **Tables 6-2** through **6-4**.

Table 6-2: Field Review Summary

Intersection	AM Peak Period (7:00 AM to 9:00 AM)	PM Peak Period (3:00 PM to 6:00 PM)
NW 12th Avenue at NW 40th Street (I-195 WB Off-Ramp)	<ul style="list-style-type: none"> o Peak movement on NW 12th Avenue was observed to be southbound through and left turn movements comprising vehicles heading towards the I-195 westbound on-ramp. o Light queues were observed on the I-195 westbound off-ramp. 	<ul style="list-style-type: none"> o Peak movement on NW 12th Avenue was observed to be northbound movement, with queues extending until upstream intersection of NW 12th Ave with NW 39th Street. o Moderate traffic was observed on the westbound off-ramp as well as for the southbound movements.
NW 12th Avenue at NW 39th Street	<ul style="list-style-type: none"> o Peak direction at NW 12th Avenue was southbound through movement. o Heavy eastbound off-ramp right-turning movement queues were observed. 	<ul style="list-style-type: none"> o Peak direction on NW 12th Avenue was the northbound movement with observed queues extending to at least 300 feet. o Moderate queue build up was observed on the eastbound approach due to traffic arriving from the I-195 eastbound off-ramp.
N Miami Avenue at N 36th Street	<ul style="list-style-type: none"> o Peak direction on N Miami Avenue was the southbound direction. Heavy queues were observed in this direction as a result of vehicles exiting from I-195 eastbound off-ramp north of this intersection. o Heavy eastbound through movement queues were observed. Left-turning vehicles were obstructed from making the turn during some cycles because of the queues on the N Miami Ave from the northbound left-turning vehicles to I-195 westbound on-ramp that backed up into this intersection. Moderate queues were observed on the westbound direction. o Pedestrian signal heads were observed to be inoperable at this intersection during the field reviews. 	<ul style="list-style-type: none"> o Peak movement on N Miami Avenue was observed to be northbound through movement with queues of at least 600 feet. o Heavy westbound through queues were observed on N 36th Street. o Northbound through, eastbound left and westbound right-turning vehicles were unable to fully clear the intersection during some cycles due to northbound queue backups from northbound left-turning vehicles at the I-195 westbound on ramp extending into the intersection. o Heavy pedestrian activity was observed at the intersection during this period.
N Miami Avenue at I-195 Eastbound Off-Ramp	<ul style="list-style-type: none"> o Heavy traffic was observed on N Miami Avenue in the northbound direction due to vehicles heading towards the I-195 westbound on-ramp. Vehicles were stopped in the NB direction even during the green signal phase during heavy westbound on-ramp volumes at the downstream intersection where queues extended all the way back into the intersection at N 36th Street. o Heavy I-195 eastbound off-ramp queues were observed extending as far back as the freeway exit junction. All the vehicles in the queues were not being cleared in a cycle. Eastbound left-turning movements were sometimes unable to cross the intersection because of queues from the northbound left turn movement at the downstream I-195 westbound on-ramp which extended back into this intersection. 	<ul style="list-style-type: none"> o Peak direction on N Miami Avenue was observed to be northbound with queues extending to the upstream intersection with NE 36th Street. o Heavy eastbound off-ramp queues were observed. Some of the eastbound left-turning vehicles were unable to clear the intersection because of queues from the northbound left-turning vehicles at the intersection of I-195 westbound on-ramp and NE 38th Street which extended back into the intersection.

Table 6-3: Field Review Summary

Intersection	AM Peak Period (7:00 AM to 9:00 AM)	PM Peak Period (3:00 PM to 6:00 PM)
N Miami Avenue at NW 38th Street/ I-195 Westbound On-Ramp	<ul style="list-style-type: none"> o Heavy queues were observed associated with the northbound left turns on to the I-195 westbound on-ramp. These queues extended back until the N 36th Street intersection. The queues resulted from the northbound left turns having to yield to the heavy southbound through vehicles as well southbound right turns headed towards the westbound on-ramp. Vehicles were observed using small gaps to make this northbound left-turn movement. o Moderate traffic was observed on N 38th Street where queues generally ranged from 3 to 6 vehicles. 	<ul style="list-style-type: none"> o Peak movement on N Miami Avenue was observed to be the northbound through movement with queues extending past the intersection with the I-195 eastbound off-ramp into the upstream intersection of N 36th Street. o Queues of 7 to 9 vehicles were observed in each cycle on the westbound approach of NE 38th Street. Frequent vehicle backups from the I-195 westbound mainline were observed on the westbound on-ramp.
Federal Highway and NE 36th Street and NE 2nd Avenue	<ul style="list-style-type: none"> o The peak movement on NE 36th Street was observed to be the eastbound through movement with queues extending approximately 500 feet. o Heavy queues of at least 600 feet were observed on the southeast bound through movement from NE 2nd Avenue extending to the upstream intersection of NE 2nd Ave at NE 38th Street. o Northbound right-turning queues were observed on Federal Highway. 	<ul style="list-style-type: none"> o Peak movement on Federal Highway was observed to be northbound movement with observed queues of at least 600 feet. o Heavy queues were observed for both eastbound and westbound movements. Eastbound queues from the downstream intersection of US 1 with NE 36th Street were observed to block traffic trying to clear this intersection. Westbound queues at this intersection were observed to extend all the way to the upstream intersection of US 1 and NE 36th Street. o Queues from the southeast bound through movement from NE 2nd Avenue extending to the upstream intersection of NE 2nd Ave at NE 38th Street were observed during some cycles.
US-1/ Biscayne Boulevard at NE 36th Street	<ul style="list-style-type: none"> o Heavy queues were observed in the eastbound, northbound through, southbound left and westbound movements. Eastbound left-turn demand was not met during some cycles because of northbound queues extending from the downstream intersection of US-1 and NE 38th Street. Heavy westbound right-turn demand was also observed. o Heavy northbound through movements were observed with queue backups extending until the upstream intersection at NE 33rd Street. The northbound exclusive rightmost lane was under construction during the field review. o Occasional conflicts were observed between bicyclists and eastbound/southbound right-turning vehicles. 	<ul style="list-style-type: none"> o Heavy queues were observed on all the movements. o Eastbound left turns were prevented from clearing the intersection as a result of spillback from northbound queues at the intersection of US 1 with NE 38th Street. Heavy eastbound through queues were observed extending to the upstream intersection at Federal Highway with NE 36th Street. o Traffic demand from the I-195 eastbound loop off-ramp was heavy leading to long queues on the westbound approach at the intersection as well as heavy queues on the ramp itself which extended back to the I-195 mainline. o Heavy southbound left turn demand was observed resulting in queues of at least 300 to 400 feet. o Peak direction on US1 was northbound with observed queues extending three blocks to the upstream signalized intersection of US 1 with NE 33rd Street. Similar to the AM peak, the exclusive right turn lane was under construction likely contributing to the breakdown in operations on the northbound approach during this period.
US-1/ Biscayne Boulevard at NE 38th Street	<ul style="list-style-type: none"> o Heavy traffic was observed on all intersection approaches. o Southbound left-turning vehicles headed towards the I-195 westbound on-ramp were backing up for at least 800 feet. o Westbound traffic approaching the intersection from the east was obstructed from clearing the intersection due to southbound queues from the downstream intersection of US-1 and NE 36th Street that extended into this intersection. 	<ul style="list-style-type: none"> o Heavy queues were observed on the northbound approach a majority of which comprised right-turning vehicles destined for the I-195 eastbound loop on-ramp. Northbound queues extended to the upstream intersection of US 1 at NE 36th Street. o Heavy southbound left-turning movement queues of at least 350 to 450 feet were observed. o Heavy traffic demand was observed on the westbound approach which mostly comprised vehicles exiting from the I-195 eastbound loop off-ramp leading to queues of at least 400 feet. Most of the vehicles were observed to clear the intersection in the given green time.

Table 6-4: Field Review Summary

Intersection	AM Peak Period (7:00 AM to 9:00 AM)	PM Peak Period (3:00 PM to 6:00 PM)
Alton Road and Arthur Godfrey Road	<ul style="list-style-type: none"> o Heavy eastbound traffic was observed with queues of at least 800 feet. Occasionally, heavy westbound through traffic was also observed during some cycles but these vehicles cleared the intersection during the given green time. o Queues on the southbound approach were observed to block the unsignalized upstream intersection of Magnolia Avenue/ Nautilus Road. o Moderate traffic was observed on northbound and southbound movements. o Moderate pedestrian activity was observed at the intersection. 	<ul style="list-style-type: none"> o Peak direction on Alton Road was northbound with observed queues extending for at least 400 feet blocking the upstream unsignalized intersection at W 39th Street. o Queues on the southbound approach were observed to block the unsignalized upstream intersection of Magnolia Avenue/ Nautilus Road. o Moderate queues were observed on eastbound and westbound approaches with queues extending up to approximately 400 feet in each direction. o Heavy bus and pedestrian traffic were observed at the intersection.
Alton Road and Sullivan Drive and 43rd Street	<ul style="list-style-type: none"> o Heavy northbound left-turn demand was observed turning into the Mount Sinai Hospital. o Buses were seen traveling to and from the hospital. o Heavy southbound through queues of at least 600 feet were observed on Alton Road blocking the unsignalized upstream intersection at N Bay Road. o Very few vehicles were observed exiting N Bay Road on to Alton Road. 	<ul style="list-style-type: none"> o Long queues were observed on the southbound approach. o Heavy westbound through demand was observed with observed queues from at least 700 to 800 feet. o Queues from the I-195 westbound on-ramp (southwest of the intersection) extended back into the intersection blocking eastbound traffic exiting the hospital as well as southbound through vehicles even during the green phase of the cycle. o Northbound through queues started to build during this time extending a distance from at least 350 to 400 feet.
I-195 eastbound off-ramp to Alton Road	<ul style="list-style-type: none"> o Queues backing up onto the eastbound I-195 mainline were observed. 	<ul style="list-style-type: none"> o No major issues were observed with the operation of this off-ramp during the PM peak period.
I-195 westbound off-ramp to US-1	<ul style="list-style-type: none"> o Long queues extending to NE 6th Avenue were observed requiring 2 to 3 signal cycles for westbound exiting vehicles to enter US-1. 	<ul style="list-style-type: none"> o No major issues were observed with the operation of this off-ramp during the PM peak period.
I-195 eastbound off-ramp on to NE 36 St / US-1	<ul style="list-style-type: none"> o Moderate queues were observed on this exit ramp. 	<ul style="list-style-type: none"> o Heavy demand was observed on this exit ramp during the PM peak period with frequent backups.

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7.0 SAFETY ANALYSIS

A safety analysis along Interstate 195 (I-195) has been conducted to identify crash patterns at interchanges, ramps, and other influence areas within the corridor. Locations with high crash rates and trends as well as general countermeasures to address multi-modal crash patterns have been identified. Crash data for a five-year study period (January 2011 to December 2015) were reviewed and summarized for the following study sections:

- Section 87003000/SR-112 from MP 3.810 (NW 14th Avenue) to MP 4.132 (NW 11th Avenue)
- Section 87004000/SR-112/I-195 from MP 0.000 (NW 11th Avenue) to MP 4.910 (Alton Road).

The crash data were obtained from FDOT’s Crash Analysis Reporting System (CARS). Crash data at interchanges, interchange influence areas, and ramp junctions were collected. The study area was divided into six segments along the mainline to account for the change in the lane configuration, crash rate category, and ADT along the SR-112 / I-195 study corridor. A cluster analysis of the crash frequencies for the mainline and ramps was conducted to identify the concentration of crashes within the study limits. The crash statistics for the entire study limits are presented first, followed by the statistics for the segments, ramps, and crash clusters.

7.1 Overall Corridor Review

Overall, there were a total of 2,395 crashes during the five-year study period with 371 crashes in 2011, 409 crashes in 2012, 459 crashes in 2013, 501 crashes in 2014, and 655 crashes in 2015. The leading crash types along the study limits were rear end with 1,206 crashes (50 percent), sideswipe with 336 crashes (14 percent), angle with 317 crashes (13 percent). There were 29 collisions with pedestrian crashes (1 percent) and 6 collisions with bicycle crashes (<1 percent) during the five-year period. There were 679 (28 percent) nighttime crashes, and 350 (15 percent) wet-pavement-surface conditions crashes. The percentage of crashes occurring at nighttime was below the Department’s Districtwide average (29 percent) for all the roadways. The wet pavement crash percentage for the study limits was also below the Department’s Districtwide average of 16 percent for all roadways. Based on crash severity, there were 696 (29 percent) injury type crashes, and 1684 (70 percent) were property damage only crashes. There were 15 fatal crashes (<1 percent) reported during the referenced five-year period. It is worth noting that the number of crashes has increased from 2011 to 2015, with an average rate of increase of 71 crashes per year, equivalent to 31 percent increase per year. The crash statistics associated with all the crash types for the study limits have been summarized in **Table 7-1**. Furthermore, a detailed graph illustrating the crash types is presented in **Exhibit 7-1**. The crash summaries for the entire study corridor are presented in **Appendix E**.

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Table 7-1: Overall Crash Statistics Summary (2011-2015)

Segment/Spot with No Expected Values Available		Number of Crashes								
		Year					5 Year Total Crashes	Mean Crashes Per Year	%	
		2011	2012	2013	2014	2015				
CRASH TYPE	Rear End	184	194	226	242	360	1206	241.20	50.4%	
	Head On	0	0	0	2	0	2	0.40	0.1%	
	Angle	46	53	50	72	96	317	63.40	13.2%	
	Left Turn	8	10	18	12	16	64	12.80	2.7%	
	Right Turn	0	1	2	2	2	7	1.40	0.3%	
	Sideswipe	45	58	68	73	92	336	67.20	14.0%	
	Backed Into	1	0	2	0	2	5	1.00	0.2%	
	Coll. w/ Parked Car	4	7	2	2	3	18	3.60	0.8%	
	Coll. w/ Pedestrian	5	5	8	3	8	29	5.80	1.2%	
	Coll. w/ Bicycle	2	0	1	2	1	6	1.20	0.3%	
	Fixed Object	26	34	39	40	30	169	33.80	7.1%	
	Ran Off Road	1	0	0	1	0	2	0.40	0.1%	
	Overtuned	7	2	4	3	2	18	3.60	0.8%	
	Other	42	45	39	47	43	216	43.20	9.0%	
	Total Crashes	371	409	459	501	655	2395	479.00	100.0%	
SEVERITY	PDO Crashes	217	295	327	363	482	1684	336.80	70.3%	
	Fatal Crashes	2	4	3	3	3	15	3.00	0.6%	
	Injury Crashes	152	110	129	135	170	696	139.20	29.1%	
LIGHTING CONDITIONS	Daylight	274	292	329	357	463	1715	343.00	71.6%	
	Dusk	4	6	12	20	18	60	12.00	2.5%	
	Dawn	2	7	9	10	6	34	6.80	1.4%	
	Dark	91	104	108	114	168	585	117.00	24.4%	
	Unknown	0	0	1	0	0	1	0.20	0.0%	
SURFACE CONDITIONS	Dry	320	342	378	427	577	2044	408.80	85.3%	
	Wet	51	67	81	74	77	350	70.00	14.6%	
	Others	0	0	0	0	1	1	0.20	0.0%	
MONTH OF YEAR	January	17	22	26	40	50	155	31.00	6.5%	
	February	17	45	40	52	51	205	41.00	8.6%	
	March	36	42	39	49	53	219	43.80	9.1%	
	April	23	34	46	40	57	200	40.00	8.4%	
	May	27	43	49	29	64	212	42.40	8.9%	
	June	35	29	35	36	50	185	37.00	7.7%	
	July	33	33	36	34	59	195	39.00	8.1%	
	August	39	35	37	41	52	204	40.80	8.5%	
	September	47	37	33	39	28	184	36.80	7.7%	
	October	32	27	38	36	58	191	38.20	8.0%	
	November	34	26	39	48	57	204	40.80	8.5%	
	December	31	36	41	57	76	241	48.20	10.1%	
DAY OF WEEK	Sunday	54	67	72	72	83	348	69.60	14.5%	
	Monday	46	44	62	73	88	313	62.60	13.1%	
	Tuesday	56	59	62	78	106	361	72.20	15.1%	
	Wednesday	61	57	66	60	101	345	69.00	14.4%	
	Thursday	61	75	82	96	128	442	88.40	18.5%	
	Friday	44	59	70	56	86	315	63.00	13.2%	
	Saturday	49	48	45	66	63	271	54.20	11.3%	
HOUR OF DAY	00:00-06:00	60	55	45	56	51	267	53.40	11.1%	
	06:00-09:00	43	58	60	68	70	299	59.80	12.5%	
	09:00-11:00	42	41	53	58	81	275	55.00	11.5%	
	11:00-13:00	42	45	47	47	79	260	52.00	10.9%	
	13:00-15:00	45	51	60	61	76	293	58.60	12.2%	
	15:00-18:00	75	90	98	106	148	517	103.40	21.6%	
	18:00-24:00	64	69	96	105	150	484	96.80	20.2%	

Exhibit 7-1: Overall Crashes By Type (2011-2015)

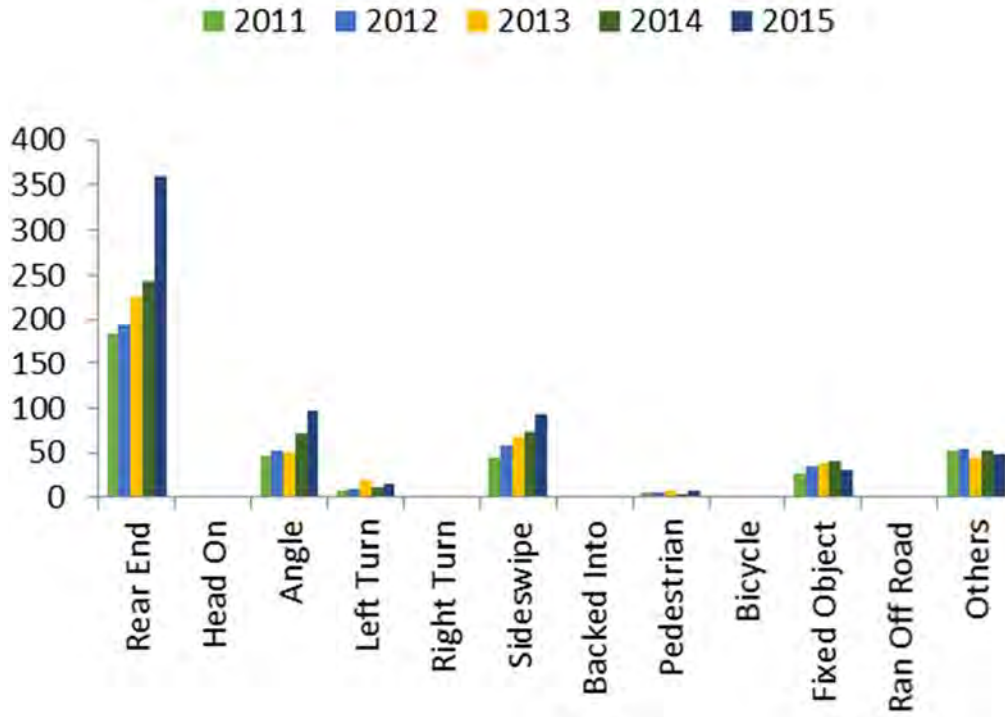
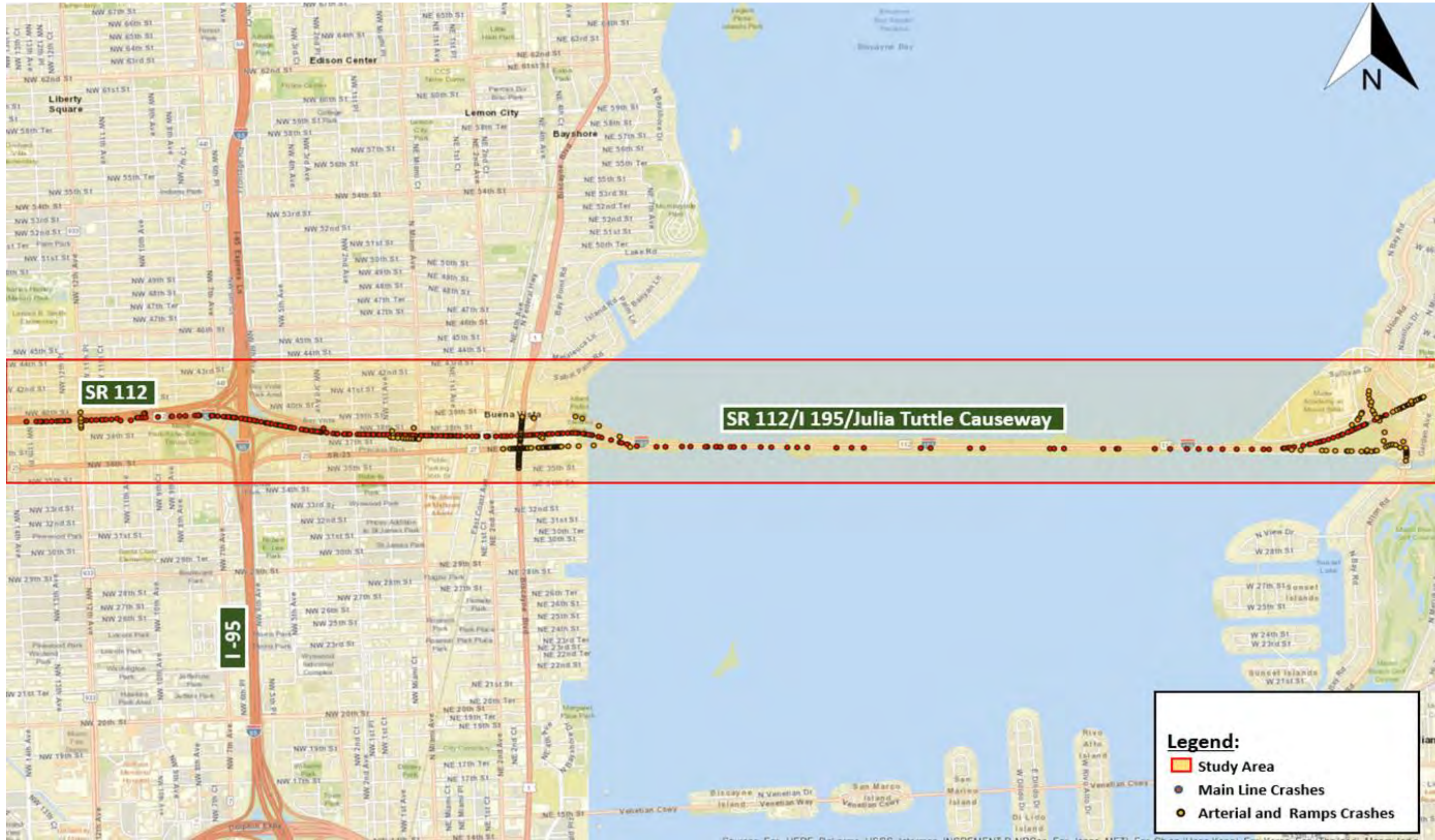


Exhibit 7-2 on the next page, illustrates all crashes within study limits for five-years period. In addition, **Exhibit 7-3** on the following pages depicts the fatal crashes only:

The following is a summary of findings for all fatal crashes:

- A total of 15 fatal crashes were reported.
- Seven crashes (47 percent of all fatal crashes) involved a collision with a pedestrian.
- Three crashes (20 percent of all fatal crashes) were fixed-object collisions.
- Two crashes (13 percent of all fatal crashes) were rear end crashes.
- Two crashes (13 percent of all fatal crashes) involved unknown or other type of crashes.
- One crash (7 percent of all fatal crashes) was a sideswipe.
- Twelve (80 percent of all fatal crashes) of all fatal crashes occurred during nighttime conditions.



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 97/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

Crash Summary and Segment Map (2011-2015)

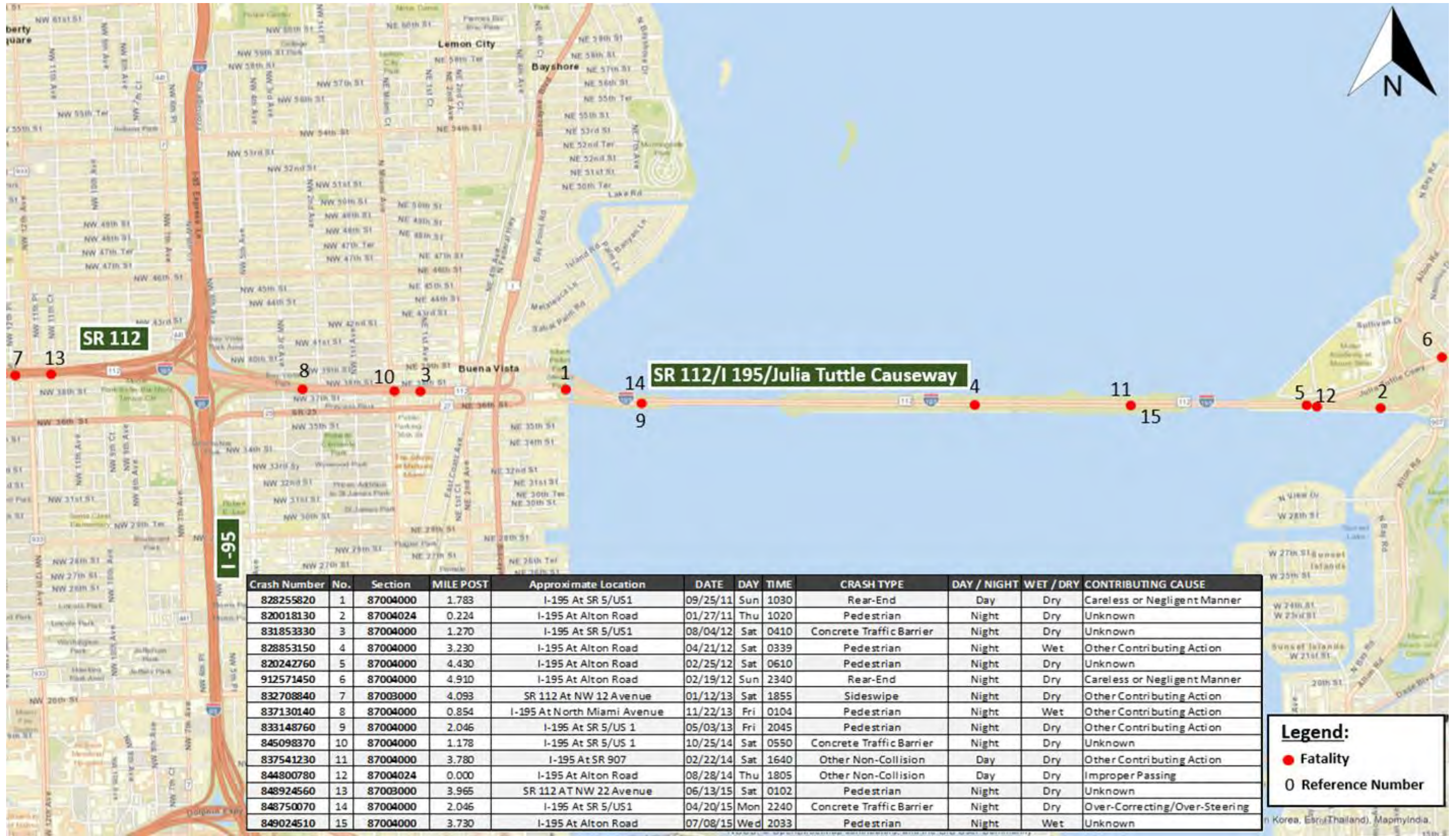
Report Title:

Existing Conditions Report

Exhibit No. 7-2

Page No.

Date: 9/7/18



Project Name:



I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

Overall Fatal Crash Locations (2011-2015)

Report Title:

Existing Conditions Report

Exhibit No. 7-3

Page No.

Date: 9/7/18

7.2 Crash Analysis For Segments, Ramps And Clusters

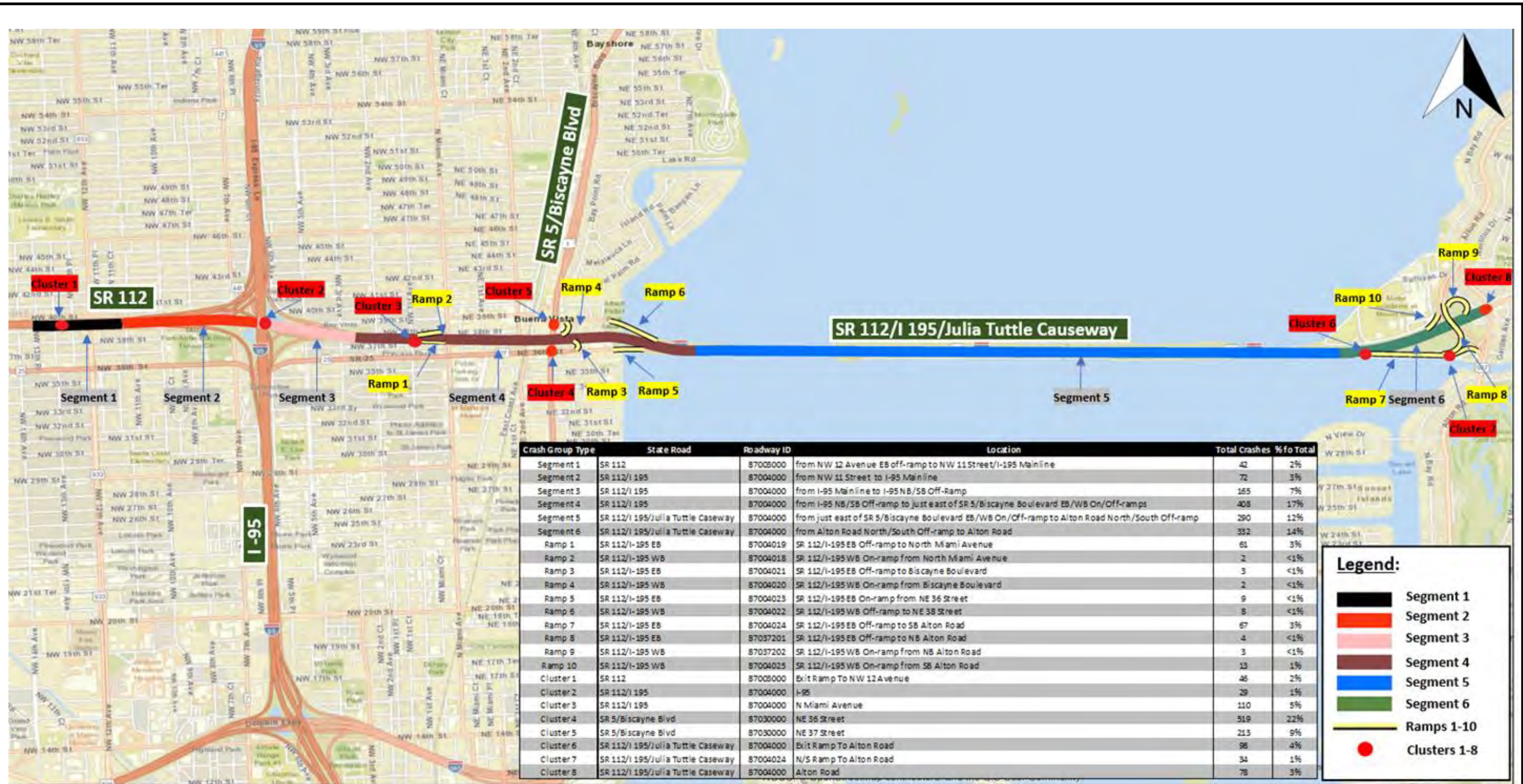
The crash analysis within the study limit was divided into several segments along the mainline (SR 112 and SR I 195). Furthermore, a ramp and cluster crash analyses were performed separately. **Exhibit 7-4** illustrates the data analysis summary along the study limits for segments, ramps and crash cluster analysis.

7.2.1 Segment Crash Analysis

Due to changes in the lane configuration, crash rate category, and ADT along SR 112 and I-195 Sections, the study limits were further divided into the following six segments:

- Segment 1: SR 112 (Section 87003000) from NW 12 Avenue eastbound off-ramp (MP 3.810) to NW 11th Street/I-195 Mainline (MP 4.132). Total length of 0.322 miles. The segment has an estimated AADT of 112,500 (with total of eight or more lanes). The crash category for this segment is classified as Toll Road Urban.
- Segment 2: SR 112/I-195 Mainline (Section 87004000) from NW 11th Street (MP 0.000) to I-95 Mainline (MP 0.486). Total length of 0.486 miles. The segment has an estimated AADT of 110,000 (with total of four lanes). The crash category for this segment is classified as Toll Road Urban.
- Segment 3: SR 112/I-195 (Section 87004000) from I-95 Mainline (MP 0.486) to I-95 NB/SB Off Ramp (MP 0.838). Total length of 0.352 miles. The segment has an estimated AADT of 113,000 (with total of three to four lanes). The crash category for this segment is classified as Interstate Urban.
- Segment 4: SR 112/I-195 (Section 87004000) from I-95 NB/SB Off-ramp (MP 0.838) to just east of SR 5/Biscayne Boulevard EB/WB On/Off-ramps (MP 2.046). Total length of 1.208 miles. The segment has an estimated AADT of 128,000 (with a total of six to seven lanes). The crash category for this segment is classified as Interstate Urban.
- Segment 5: SR 112/I-195 (Section 87004000) from just east of SR 5/Biscayne Boulevard EB/WB On/Off-ramp (MP 2.046) to Alton Road North/South Off-ramp (MP 4.351). Total length of 2.305 miles. The segment has an estimated AADT of 114,000 (with a total of six to seven lanes). The crash category for this segment is classified as Interstate Urban.
- Segment 6: SR 112/Julia Tuttle Causeway/I-195 (Section 87004000) from Alton Road North/South Off-ramp (MP 4.351) to Alton Road (4.910). Total length of 0.559 miles. Total length of 2.305 miles. The segment has an estimated AADT of 113,915 (with a total of four to five lanes). The crash category for this segment is classified as Interstate Urban.

The crash summaries for all six segments are presented in **Appendix E**.



Project Name:



I-95 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

Crash Summary and Segment Map (2011-2015)

Report Title:

Existing Conditions Report

Exhibit No. 7-4

Page No.

Date: 9/7/18

Segment 1 - [SR 112 (Section 87003000) from NW 12th Avenue eastbound off-ramp (MP 3.810) to NW 11th Avenue/I-195 Mainline Section 87004000 (MP 4.132)].

In Segment 1, there were a total of 42 crashes during the five-year study period with 7 crashes in 2011, 3 crashes in 2012, 13 crashes in 2013, 7 crashes in 2014, and 12 crashes in 2015. The three leading crash types along Segment 1 were rear end with 23 crashes (55 percent), sideswipe with 9 crashes (21 percent), fixed object with 3 crashes (7 percent). There were 14 (33 percent) and 7 (17 percent) crashes which occurred during nighttime and wet pavement surface conditions, respectively. The nighttime crash percent was higher than the Department’s Districtwide average of 29 percent, and the wet pavement crash percent was higher than the Department’s Districtwide average of 16 percent for all roadways. Based on crash severity, there were 16 (38 percent) injury type crashes, and 24 (57 percent) property damage only crashes. There were four fatal crashes (5 percent) reported during the referenced five-year period. The confidence levels for Segment 1 were calculated and summarized in **Table 7-2**.

Table 7-2: Segment 1: Confidence Levels (2011-2015)

Year	2011	2012	2013	2014	2015
Number of crashes	7	3	13	7	12
Average Daily Traffic (ADT)	110,000	100,500	104,000	115,000	112,500
Actual Crash Rate (ACR)	0.541	0.254	1.064	0.518	0.908
District 6 Average Crash Rate (A)	1.435	1.673	1.884	1.834	1.629
Average Vehicle Exposure (M)	12.928	11.812	12.223	13.516	13.222
Critical Crash Rate (CCR)	2.493	2.869	3.135	3.009	2.746
Safety Ratio	0.217	0.089	0.339	0.172	0.33
Statistical Significance	-2.566	-3.658	-1.986	-3.472	-1.948
Confidence Level	50.00%	50.00%	50.00%	50.00%	50.00%

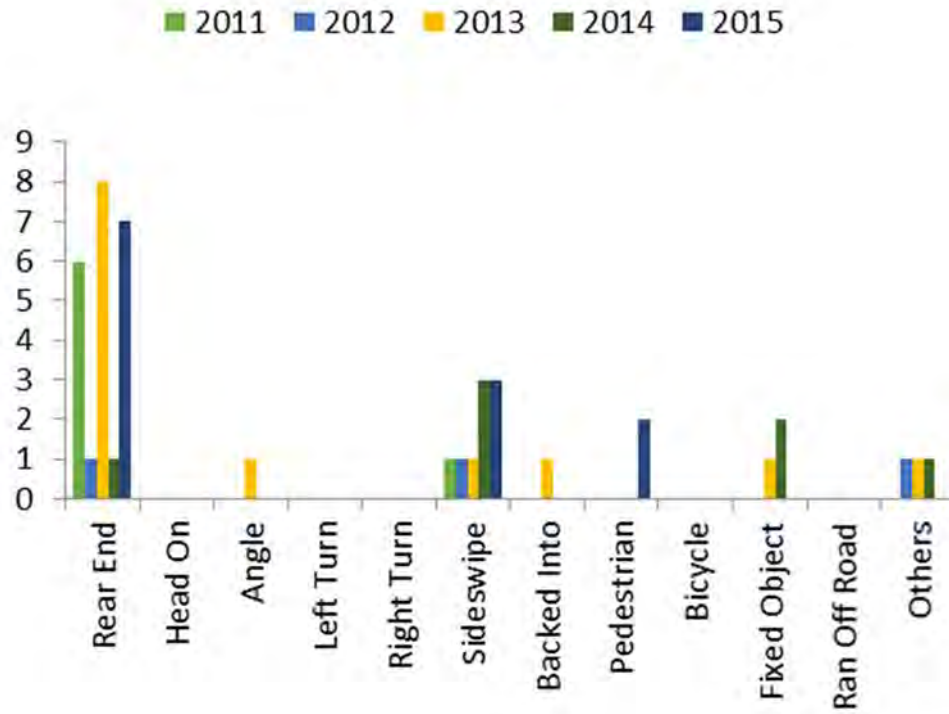
As presented in **Table 7-2**, Segment 1 is not considered to have an abnormally high number of crashes throughout the five years period, since the confidence level is lower than 99.95 percent. Based on the Highway Safety Improvement Program (HSIP), urban intersections or segments with a confidence level of 99.95 percent or greater are considered locations with abnormally high crashes.

The crash statistics associated with all the crash types for Segment 1 have been summarized in **Table 7-3** as well as in a detailed chart in **Exhibit 7-5** illustrating the crash types.

Table 7-3: Study Segment 1 Crash Statistics Summary (2011-2015)

SR 112 (Section 87003000) from NW 12 Avenue EB off-ramp (MP 3.810) to NW 11 Avenue/I-195 Mainline (MP 4.132)		Number of Crashes							
		Year					5 Year Total Crashes	Mean Crashes Per Year	%
		2011	2012	2013	2014	2015			
Segment/Spot with No Expected Values Available		2011	2012	2013	2014	2015	5 Year Total Crashes	Mean Crashes Per Year	%
CRASH TYPE	Rear End	6	1	8	1	7	23	4.60	54.8%
	Head On	0	0	0	0	0	0	0.00	0.0%
	Angle	0	0	1	0	0	1	0.20	2.4%
	Left Turn	0	0	0	0	0	0	0.00	0.0%
	Right Turn	0	0	0	0	0	0	0.00	0.0%
	Sideswipe	1	1	1	3	3	9	1.80	21.4%
	Backed Into	0	0	1	0	0	1	0.20	2.4%
	Coll. w/ Parked Car	0	0	0	0	0	0	0.00	0.0%
	Coll. w/ Pedestrian	0	0	0	0	2	2	0.40	4.8%
	Coll. w/ Bicycle	0	0	0	0	0	0	0.00	0.0%
	Fixed Object	0	0	1	2	0	3	0.60	7.1%
	Ran Off Road	0	0	0	0	0	0	0.00	0.0%
	Overtuned	0	1	0	0	0	1	0.20	2.4%
	Other	0	0	1	1	0	2	0.40	4.8%
Total Crashes		7	3	13	7	12	42	8.40	100.0%
SEVERITY	PDO Crashes	3	1	7	6	7	24	4.80	57.1%
	Fatal Crashes	0	0	1	0	1	2	0.40	4.8%
	Injury Crashes	4	2	5	1	4	16	3.20	38.1%
LIGHTING CONDITIONS	Daylight	4	3	8	5	8	28	5.60	66.7%
	Dusk	0	0	0	0	2	2	0.40	4.8%
	Dawn	0	0	0	0	0	0	0.00	0.0%
	Dark	3	0	5	2	2	12	2.40	28.6%
	Unknown	0	0	0	0	0	0	0.00	0.0%
SURFACE CONDITIONS	Dry	5	1	12	6	11	35	7.00	83.3%
	Wet	2	2	1	1	1	7	1.40	16.7%
	Others	0	0	0	0	0	0	0.00	0.0%
MONTH OF YEAR	January	1	0	2	0	0	3	0.60	7.1%
	February	0	0	2	0	1	3	0.60	7.1%
	March	1	1	1	1	1	5	1.00	11.9%
	April	0	1	0	0	4	5	1.00	11.9%
	May	0	0	1	1	1	3	0.60	7.1%
	June	2	0	0	2	1	5	1.00	11.9%
	July	1	1	3	0	0	5	1.00	11.9%
	August	1	0	2	0	0	3	0.60	7.1%
	September	1	0	0	0	1	2	0.40	4.8%
	October	0	0	2	0	1	3	0.60	7.1%
	November	0	0	0	1	2	3	0.60	7.1%
	December	0	0	0	2	0	2	0.40	4.8%
DAY OF WEEK	Sunday	2	2	0	3	1	8	1.60	19.0%
	Monday	0	1	4	0	3	8	1.60	19.0%
	Tuesday	1	0	1	3	2	7	1.40	16.7%
	Wednesday	1	0	1	0	0	2	0.40	4.8%
	Thursday	0	0	2	0	2	4	0.80	9.5%
	Friday	0	0	3	1	3	7	1.40	16.7%
	Saturday	3	0	2	0	1	6	1.20	14.3%
HOUR OF DAY	00:00-06:00	2	0	0	1	1	4	0.80	9.5%
	06:00-09:00	1	0	0	1	6	8	1.60	19.0%
	09:00-11:00	1	0	1	0	0	2	0.40	4.8%
	11:00-13:00	0	1	0	2	1	4	0.80	9.5%
	13:00-15:00	0	1	1	1	1	4	0.80	9.5%
	15:00-18:00	1	1	5	1	1	9	1.80	21.4%
	18:00-24:00	2	0	6	1	2	11	2.20	26.2%

Exhibit 7-5: Study Segment 1 Crashes By Type (2011-2015)



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Segment 2 - [SR 112/I-195 Mainline (Section 87004000) from NW 11th Avenue (MP 0.000) to I-95 Mainline (MP 0.486)].

In Segment 2, there were a total of 72 crashes during the five-year study period with 13 crashes in 2011, 14 crashes in 2012, 13 crashes in 2013, 17 crashes in 2014, and 15 crashes in 2015. The three leading crash types along Segment 2 were rear-end with 30 crashes (42 percent), sideswipe with 19 crashes (26 percent), Other with 12 crashes (17 percent). Based on the review of the data, most crashes classified as other appear to be either rear-end or sideswipe crashes. There were 19 (26 percent) and 10 (14 percent) crashes which occurred during nighttime and wet pavement surface conditions, respectively. The nighttime crash percent was lower than the Department’s Districtwide average of 29 percent, and the percentage of crashes occurring under wet pavement conditions was also lower than the Department’s Districtwide average of 16 percent for all roadways. Based on crash severity, there were 28 (39 percent) injury type crashes, and 44 (61 percent) property damage only crashes. There were no fatal crashes reported during the referenced five-year period. The confidence levels for Segment 2 were calculated and summarized in **Table 7-4**.

Table 7-4: Segment 2: Confidence Levels (2011-2015)

Year	2011	2012	2013	2014	2015
Number of crashes	13	14	13	17	15
Average Daily Traffic (ADT)	100,004	98,075	101,521	113,266	110,111
Actual Crash Rate (ACR)	0.733	0.805	0.722	0.846	0.768
District 6 Average Crash Rate (A)	1.435	1.673	1.884	1.834	1.629
Average Vehicle Exposure (M)	17.74	17.398	18.009	20.092	19.533
Critical Crash Rate (CCR)	2.343	2.665	2.921	2.803	2.554
Safety Ratio	0.313	0.302	0.247	0.302	0.301
Statistical Significance	-2.37	-2.707	-3.507	-3.187	-2.893
Confidence Level	50.00%	50.00%	50.00%	50.00%	50.00%

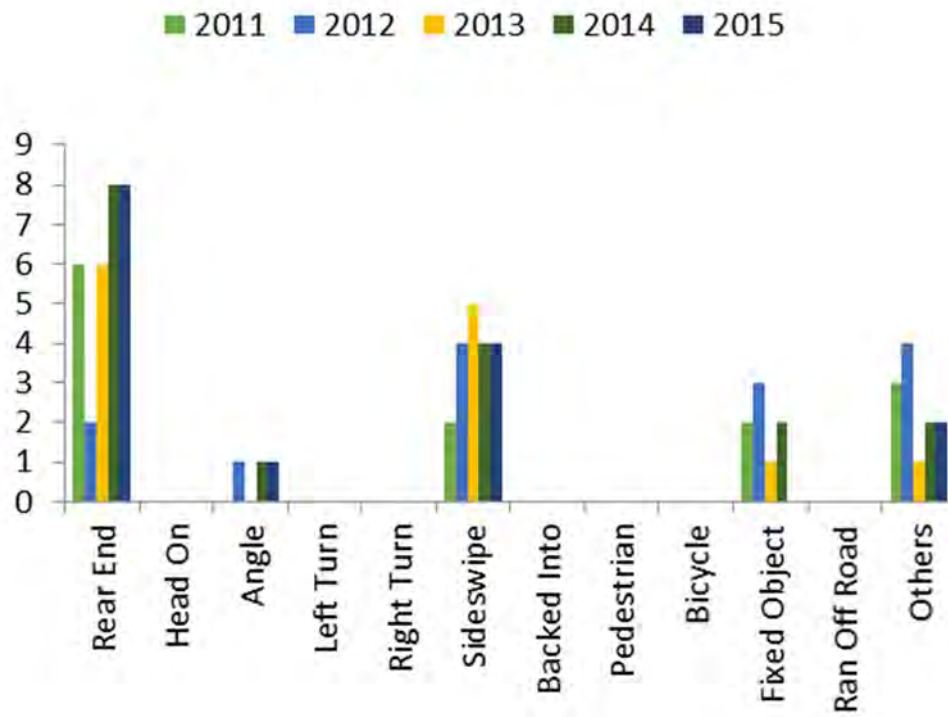
As presented in **Table 7-4**, Segment 2 is not considered to have an abnormally high number of crashes, since the confidence level is lower than 99.95 percent. Based on the HSIP, urban intersections or segments with a confidence level of 99.95 percent or greater are considered as locations with abnormally high crashes.

The crash statistics associated with all the crash types for Segment 2 have been summarized in **Table 7-5** as well as in a detailed chart in **Exhibit 7-6** illustrating the crash types.

Table 7-5: Study Segment 2 Crash Statistics Summary (2011-2015)

SR 112/I-195 Mainline (Section 87004000) from NW 11 Avenue (MP 0.000) to I-95 Mainline (MP 0.486)		Number of Crashes								
		Year					5 Year Total Crashes	Mean Crashes Per Year	%	
		2011	2012	2013	2014	2015				
Segment/Spot with No Expected Values Available										
CRASH TYPE		6	2	6	8	8	30	6.00	41.7%	
	Rear End	6	2	6	8	8	30	6.00	41.7%	
	Head On	0	0	0	0	0	0	0.00	0.0%	
	Angle	0	1	0	1	1	3	0.60	4.2%	
	Left Turn	0	0	0	0	0	0	0.00	0.0%	
	Right Turn	0	0	0	0	0	0	0.00	0.0%	
	Sideswipe	2	4	5	4	4	19	3.80	26.4%	
	Backed Into	0	0	0	0	0	0	0.00	0.0%	
	Coll. w/ Parked Car	0	0	0	0	0	0	0.00	0.0%	
	Coll. w/ Pedestrian	0	0	0	0	0	0	0.00	0.0%	
	Coll. w/ Bicycle	0	0	0	0	0	0	0.00	0.0%	
	Fixed Object	2	3	1	2	0	8	1.60	11.1%	
	Ran Off Road	0	0	0	0	0	0	0.00	0.0%	
	Overtuned	0	0	0	0	0	0	0.00	0.0%	
	Other	3	4	1	2	2	12	2.40	16.7%	
	Total Crashes	13	14	13	17	15	72	14.40	100.0%	
SEVERITY		5	11	8	10	10	44	8.80	61.1%	
	PDO Crashes	5	11	8	10	10	44	8.80	61.1%	
	Fatal Crashes	0	0	0	0	0	0	0.00	0.0%	
	Injury Crashes	8	3	5	7	5	28	5.60	38.9%	
LIGHTING CONDITIONS		10	11	10	12	10	53	10.60	73.6%	
	Daylight	10	11	10	12	10	53	10.60	73.6%	
	Dusk	0	0	0	0	0	0	0.00	0.0%	
	Dawn	0	0	0	1	0	1	0.20	1.4%	
	Dark	3	3	3	4	5	18	3.60	25.0%	
	Unknown	0	0	0	0	0	0	0.00	0.0%	
SURFACE CONDITIONS		10	11	12	15	14	62	12.40	86.1%	
	Dry	10	11	12	15	14	62	12.40	86.1%	
	Wet	3	3	1	2	1	10	2.00	13.9%	
	Others	0	0	0	0	0	0	0.00	0.0%	
MONTH OF YEAR		2	1	0	2	0	5	1.00	6.9%	
	January	2	1	0	2	0	5	1.00	6.9%	
	February	0	3	0	3	1	7	1.40	9.7%	
	March	0	0	1	1	2	4	0.80	5.6%	
	April	1	2	2	1	2	8	1.60	11.1%	
	May	1	5	3	1	1	11	2.20	15.3%	
	June	0	0	1	1	0	2	0.40	2.8%	
	July	2	0	3	1	1	7	1.40	9.7%	
	August	4	0	0	2	3	9	1.80	12.5%	
	September	1	1	1	1	3	7	1.40	9.7%	
	October	1	0	2	1	1	5	1.00	6.9%	
	November	0	0	0	1	1	2	0.40	2.8%	
	December	1	2	0	2	0	5	1.00	6.9%	
DAY OF WEEK		6	1	1	2	0	10	2.00	13.9%	
	Sunday	6	1	1	2	0	10	2.00	13.9%	
	Monday	0	2	3	5	2	12	2.40	16.7%	
	Tuesday	3	1	0	2	2	8	1.60	11.1%	
	Wednesday	0	4	2	0	3	9	1.80	12.5%	
	Thursday	2	1	5	5	3	16	3.20	22.2%	
	Friday	1	0	1	2	3	7	1.40	9.7%	
	Saturday	1	5	1	1	2	10	2.00	13.9%	
HOUR OF DAY		3	2	1	3	4	13	2.60	18.1%	
	00:00-06:00	3	2	1	3	4	13	2.60	18.1%	
	06:00-09:00	1	1	2	2	0	6	1.20	8.3%	
	09:00-11:00	1	4	1	3	2	11	2.20	15.3%	
	11:00-13:00	4	3	4	2	1	14	2.80	19.4%	
	13:00-15:00	1	2	1	1	2	7	1.40	9.7%	
	15:00-18:00	3	1	2	4	4	14	2.80	19.4%	
	18:00-24:00	0	1	2	2	2	7	1.40	9.7%	

Exhibit 7-6: Study Segment 2 Crashes By Type (2011-2015)



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Segment 3 - [SR 112/I-195 (Section 87004000) from I-95 Mainline (MP 0.486) to I-95 NB/SB Off-Ramp (MP 0.838)].

In Segment 3, there were a total of 165 crashes during the five-year study period with 36 crashes in 2011, 28 crashes in 2012, 30 crashes in 2013, 28 crashes in 2014, and 43 crashes in 2015. The three leading crash types along Segment 3 were rear-end with 91 crashes (55 percent), sideswipe with 31 crashes (19 percent), and fixed object with 19 crashes (12 percent). There were 53 (32 percent) and 20 (12 percent) crashes which occurred during nighttime and wet pavement surface conditions, respectively. The nighttime crash percent was higher than the Department’s Districtwide average of 29 percent, and the wet pavement crash percent was lower than the Department’s Districtwide average of 16 percent for all roadways. Based on crash severity, there were 56 (34 percent) injury type crashes, and 109 (66 percent) property damage only crashes. There were no fatal crashes reported during the referenced five-year period. The confidence levels for Segment 3 were calculated and summarized in **Table 7-6**.

Table 7-6: Segment 3: Confidence Levels (2011-2015)

Year	2011	2012	2013	2014	2015
Number of Crashes	36	28	30	28	43
Average Daily Traffic (ADT)	110,000	100,500	106,746	115,000	113,766
Actual Crash Rate (ACR)	2.547	2.168	2.187	1.895	2.942
District 6 Average Crash Rate (A)	1.400	1.546	1.854	2.018	2.641
Average Vehicle Exposure (M)	14.133	12.912	13.715	14.775	14.617
Critical Crash Rate (CCR)	2.400	2.646	3.028	3.200	4.006
Safety Ratio	1.061	0.820	0.723	0.592	0.734
Statistical Significance	3.758	1.911	1.006	-0.241	0.788
Confidence Level	99.99%	95.00%	80.00%	50.00%	75.00%

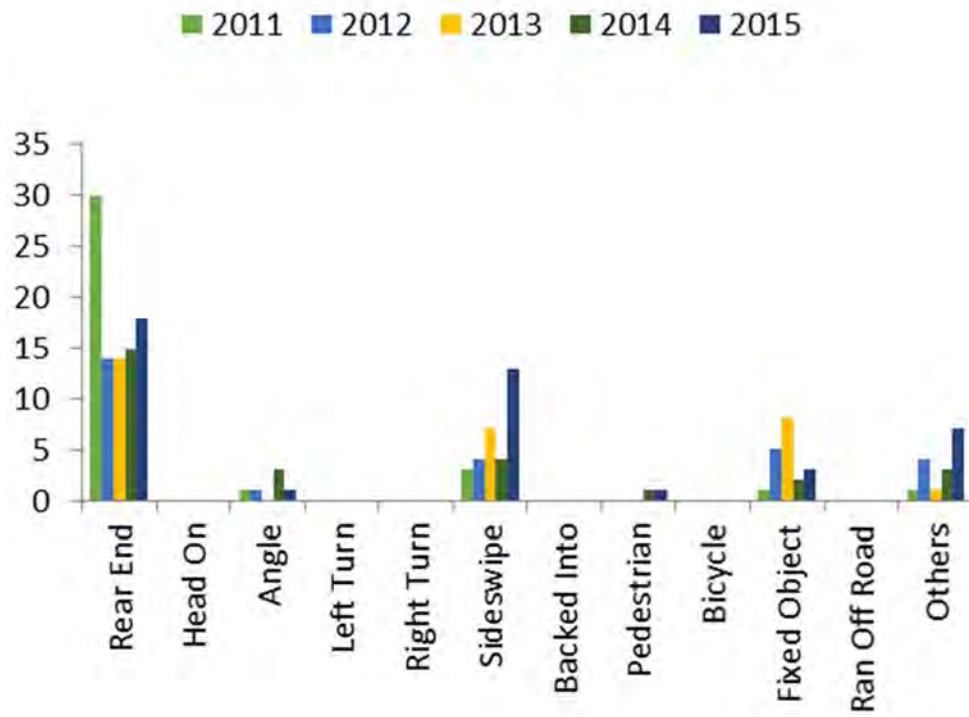
As presented in **Table 7-6**, Segment 3 is considered to have an abnormally high number of crashes in the year 2011, since the confidence level is greater than or equal to 99.95 percent. Based on the HSIP, urban intersections or segments with a confidence level of 99.95 percent or greater are considered as locations with abnormally high crashes.

The crash statistics associated with all the crash types for Segment 3 have been summarized in **Table 7-7** as well as in a detailed chart in **Exhibit 7-7** illustrating the crash types.

Table 7-7: Study Segment 3 Crash Statistics Summary (2011-2015)

SR 112/I-195 (Section 87004000) from I-95 Mainline (MP 0.486) to I-95 NB/SB Off Ramp (MP 0.838)		Number of Crashes								
		Year					5 Year Total Crashes	Mean Crashes Per Year	%	
		2011	2012	2013	2014	2015				
Segment/Spot with No Expected Values Available		2011	2012	2013	2014	2015	5 Year Total Crashes	Mean Crashes Per Year	%	
CRASH TYPE	Rear End	30	14	14	15	18	91	18.20	55.2%	
	Head On	0	0	0	0	0	0	0.00	0.0%	
	Angle	1	1	0	3	1	6	1.20	3.6%	
	Left Turn	0	0	0	0	0	0	0.00	0.0%	
	Right Turn	0	0	0	0	0	0	0.00	0.0%	
	Sideswipe	3	4	7	4	13	31	6.20	18.8%	
	Backed Into	0	0	0	0	0	0	0.00	0.0%	
	Coll. w/ Parked Car	0	2	0	0	0	2	0.40	1.2%	
	Coll. w/ Pedestrian	0	0	0	1	1	2	0.40	1.2%	
	Coll. w/ Bicycle	0	0	0	0	0	0	0.00	0.0%	
	Fixed Object	1	5	8	2	3	19	3.80	11.5%	
	Ran Off Road	0	0	0	0	0	0	0.00	0.0%	
	Overtuned	0	0	0	0	0	0	0.00	0.0%	
Other	1	2	1	3	7	14	2.80	8.5%		
Total Crashes		36	28	30	28	43	165	33.00	100.0%	
SEVERITY	PDO Crashes	18	18	21	21	31	109	21.80	66.1%	
	Fatal Crashes	0	0	0	0	0	0	0.00	0.0%	
	Injury Crashes	18	10	9	7	12	56	11.20	33.9%	
LIGHTING CONDITIONS	Daylight	29	17	17	16	33	112	22.40	67.9%	
	Dusk	0	1	2	5	2	10	2.00	6.1%	
	Dawn	0	1	0	0	0	1	0.20	0.6%	
	Dark	7	9	11	7	8	42	8.40	25.5%	
Unknown	0	0	0	0	0	0	0.00	0.0%		
SURFACE CONDITIONS	Dry	33	27	21	23	41	145	29.00	87.9%	
	Wet	3	1	9	5	2	20	4.00	12.1%	
	Others	0	0	0	0	0	0	0.00	0.0%	
MONTH OF YEAR	January	1	5	3	3	5	17	3.40	10.3%	
	February	2	3	1	0	3	9	1.80	5.5%	
	March	1	1	4	3	3	12	2.40	7.3%	
	April	3	2	3	0	2	10	2.00	6.1%	
	May	6	3	3	1	5	18	3.60	10.9%	
	June	5	1	3	2	3	14	2.80	8.5%	
	July	5	0	3	3	2	13	2.60	7.9%	
	August	2	3	1	2	2	10	2.00	6.1%	
	September	8	4	2	4	2	20	4.00	12.1%	
	October	0	1	4	4	5	14	2.80	8.5%	
	November	2	2	1	2	5	12	2.40	7.3%	
	December	1	3	2	4	6	16	3.20	9.7%	
DAY OF WEEK	Sunday	4	5	9	4	7	29	5.80	17.6%	
	Monday	7	4	4	4	5	24	4.80	14.5%	
	Tuesday	8	5	4	3	6	26	5.20	15.8%	
	Wednesday	6	2	4	5	10	27	5.40	16.4%	
	Thursday	4	3	7	6	9	29	5.80	17.6%	
	Friday	4	5	0	3	1	13	2.60	7.9%	
	Saturday	3	4	2	3	5	17	3.40	10.3%	
HOUR OF DAY	00:00-06:00	5	8	5	2	2	22	4.40	13.3%	
	06:00-09:00	5	3	3	5	7	23	4.60	13.9%	
	09:00-11:00	4	1	3	4	8	20	4.00	12.1%	
	11:00-13:00	5	1	0	1	0	7	1.40	4.2%	
	13:00-15:00	4	2	5	3	7	21	4.20	12.7%	
	15:00-18:00	7	8	6	5	10	36	7.20	21.8%	
	18:00-24:00	6	5	8	8	9	36	7.20	21.8%	

Exhibit 7-7: Study Segment 3 Crashes By Type (2011-2015)



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Segment 4 - [SR 112/I-195 (Section 87004000) from I-95 NB/SB Off-ramp (MP 0.838) to just east of SR 5/Biscayne Boulevard EB/WB On/Off-ramps (MP 2.046)].

In Segment 4, there were a total of 408 crashes during the five-year study period with 88 crashes in 2011, 87 crashes in 2012, 52 crashes in 2013, 92 crashes in 2014, and 89 crashes in 2015. The three leading crash types along Segment 4 were rear-end with 237 crashes (58 percent), fixed object with 58 crashes (14 percent), sideswipe with 55 crashes (14 percent). There were 123 (30 percent) and 65 (16 percent) crashes which occurred during nighttime and wet pavement surface conditions, respectively. The nighttime crash percent was higher than the Department’s Districtwide average of 29 percent, and percentage of crashes occurring under wet pavement conditions was the same as the Department’s Districtwide average of 16 percent for all roadways. Based on crash severity, there were 150 (37 percent) injury type crashes, and 254 (62 percent) property damage only crashes. There were four fatal crashes (1 percent) reported during the referenced five-year period. The confidence levels for Segment 4 were calculated and summarized in **Table 7-8**.

Table 7-8: Segment 4: Confidence Levels (2011-2015)

Year	2011	2012	2013	2014	2015
Number of Crashes	88	87	52	92	89
Average Daily Traffic (ADT)	113,170	113,533	118,441	122,925	128,050
Actual Crash Rate (ACR)	1.764	1.738	0.996	1.697	1.576
District 6 Average Crash Rate (A)	1.400	1.546	1.854	2.018	2.641
Average Vehicle Exposure (M)	49.899	50.059	52.223	54.200	56.460
Critical Crash Rate (CCR)	1.941	2.114	2.465	2.644	3.344
Safety Ratio	0.908	0.822	0.404	0.642	0.471
Statistical Significance	2.230	1.149	-4.504	-1.614	-4.882
Confidence Level	97.50%	85.00%	50.00%	50.00%	50.00%

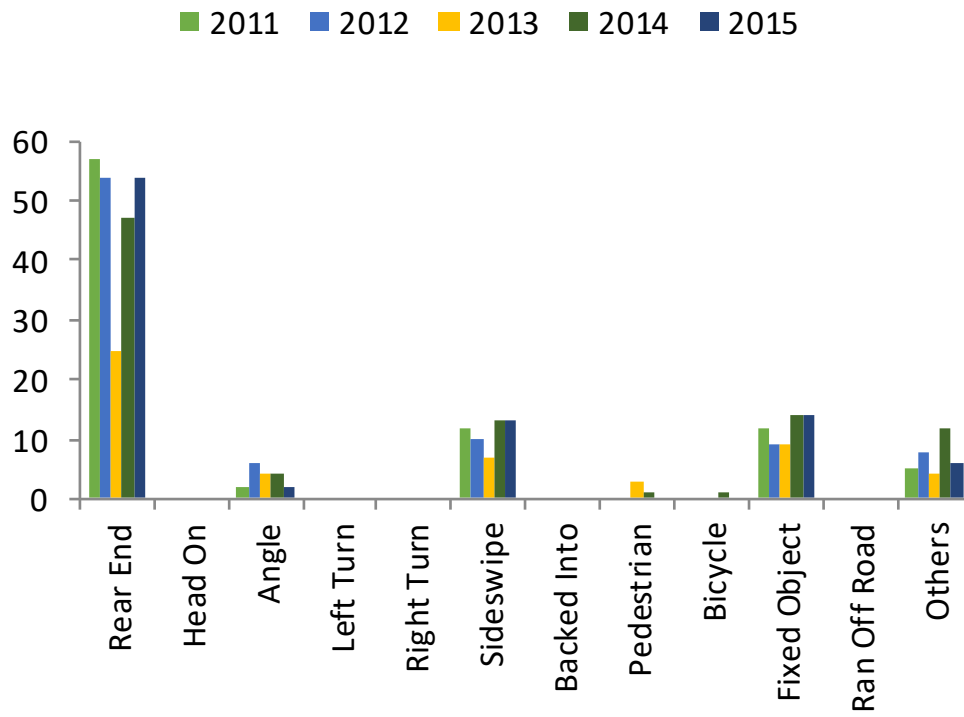
As presented in **Table 7-8**, Segment 4 is not considered to have an abnormally high number of crashes, since the confidence levels are lower than 99.95 percent. Based on the HSIP, urban intersections or segments with a confidence level of 99.95 percent or greater are considered as locations with abnormally high crashes.

The crash statistics associated with all the crash types for Segment 4 have been summarized in **Table 7-9** as well as in a detailed chart in **Exhibit 7-8** illustrating the crash types.

Table 7-9: Study Segment 4 Crash Statistics Summary (2011-2015)

SR 112/I-195 from I-95 NB/SB Off-ramp (MP 0.838) to just east of SR 5/Biscayne Boulevard EB/WB On/Off-ramps (MP 2.046). Segment/Spot with No Expected Values Available		Number of Crashes					5 Year Total Crashes	Mean Crashes Per Year	%
		Year							
		2011	2012	2013	2014	2015			
CRASH TYPE	Rear End	57	54	25	47	54	237	47.40	58.1%
	Head On	0	0	0	0	0	0	0.00	0.0%
	Angle	2	6	4	4	2	18	3.60	4.4%
	Left Turn	0	0	0	0	0	0	0.00	0.0%
	Right Turn	0	0	0	0	0	0	0.00	0.0%
	Sideswipe	12	10	7	13	13	55	11.00	13.5%
	Backed Into	0	0	0	0	0	0	0.00	0.0%
	Coll. w/ Parked Car	0	0	0	1	0	1	0.20	0.2%
	Coll. w/ Pedestrian	0	0	3	1	0	4	0.80	1.0%
	Coll. w/ Bicycle	0	0	0	1	0	1	0.20	0.2%
	Fixed Object	12	9	9	14	14	58	11.60	14.2%
	Ran Off Road	0	0	0	0	0	0	0.00	0.0%
	Overtuned	0	0	0	0	1	1	0.20	0.2%
	Other	5	8	4	11	5	33	6.60	8.1%
	Total Crashes	88	87	52	92	89	408	81.60	100.0%
SEVERITY	PDO Crashes	46	58	34	56	60	254	50.80	62.3%
	Fatal Crashes	1	1	1	1	0	4	0.80	1.0%
	Injury Crashes	41	28	17	35	29	150	30.00	36.8%
LIGHTING CONDITIONS	Daylight	62	62	37	63	61	285	57.00	69.9%
	Dusk	0	2	0	6	2	10	2.00	2.5%
	Dawn	1	1	0	1	1	4	0.80	1.0%
	Dark	25	22	15	22	25	109	21.80	26.7%
	Unknown	0	0	0	0	0	0	0.00	0.0%
SURFACE CONDITIONS	Dry	79	69	39	77	79	343	68.60	84.1%
	Wet	9	18	13	15	10	65	13.00	15.9%
	Others	0	0	0	0	0	0	0.00	0.0%
MONTH OF YEAR	January	1	4	3	4	11	23	4.60	5.6%
	February	2	4	4	7	12	29	5.80	7.1%
	March	11	8	0	8	7	34	6.80	8.3%
	April	5	7	4	9	3	28	5.60	6.9%
	May	7	10	10	7	8	42	8.40	10.3%
	June	4	11	6	5	2	28	5.60	6.9%
	July	4	12	2	5	12	35	7.00	8.6%
	August	14	12	4	9	5	44	8.80	10.8%
	September	12	5	6	13	3	39	7.80	9.6%
	October	8	8	2	6	7	31	6.20	7.6%
	November	15	3	7	9	10	44	8.80	10.8%
	December	5	3	4	10	9	31	6.20	7.6%
DAY OF WEEK	Sunday	12	13	7	12	11	55	11.00	13.5%
	Monday	12	9	4	16	8	49	9.80	12.0%
	Tuesday	11	14	9	11	14	59	11.80	14.5%
	Wednesday	18	12	6	14	24	74	14.80	18.1%
	Thursday	15	13	8	16	17	69	13.80	16.9%
	Friday	12	19	12	12	10	65	13.00	15.9%
	Saturday	8	7	6	11	5	37	7.40	9.1%
HOUR OF DAY	00:00-06:00	14	15	9	10	11	59	11.80	14.5%
	06:00-09:00	8	16	4	16	13	57	11.40	14.0%
	09:00-11:00	16	6	10	13	10	55	11.00	13.5%
	11:00-13:00	10	7	4	6	10	37	7.40	9.1%
	13:00-15:00	5	10	5	8	7	35	7.00	8.6%
	15:00-18:00	18	20	10	17	20	85	17.00	20.8%
	18:00-24:00	17	13	10	22	18	80	16.00	19.6%

Exhibit 7-8: Study Segment 4 Crashes By Type (2011-2015)



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Segment 5 - [SR 112/I-195 (Section 87004000) from just east of SR 5/Biscayne Boulevard EB/WB On/Off-ramp (MP 2.046) to Alton Road North/South Off-ramp (MP 4.351)].

In Segment 5, there were a total of 290 crashes during the five-year study period with 42 crashes in 2011, 48 crashes in 2012, 60 crashes in 2013, 48 crashes in 2014, and 92 crashes in 2015. The three leading crash types along Segment 5 were rear-end with 177 crashes (61 percent), fixed object with 38 crashes (13 percent), sideswipe with 26 crashes (9 percent). There were 124 (43 percent) and 73 (25 percent) crashes which occurred during nighttime and wet pavement surface conditions, respectively. The nighttime crash percent was higher than the Department’s Districtwide average of 29 percent, and percentage of crashes occurring under wet pavement conditions was also higher than the Department’s Districtwide average of 16 percent for all roadways. Based on crash severity, there were 110 (38 percent) injury type crashes, and 175 (60 percent) property damage only crashes. There were five fatal crashes (2 percent) reported during the referenced five-year period. The confidence levels for Segment 5 were calculated and summarized in **Table 7-10**.

Table 7-10: Segment 5: Confidence Levels (2011-2015)

Year	2011	2012	2013	2014	2015
Number of Crashes	42	48	60	48	92
Average Daily Traffic (ADT)	103,091	102,506	105,721	108,487	114,801
Actual Crash Rate (ACR)	0.484	0.557	0.675	0.526	0.953
District 6 Average Crash Rate (A)	1.400	1.546	1.854	2.018	2.641
Average Vehicle Exposure (M)	86.733	86.241	88.946	91.273	96.585
Critical Crash Rate (CCR)	1.812	1.981	2.324	2.502	3.180
Safety Ratio	0.267	0.281	0.290	0.210	0.300
Statistical Significance	-7.163	-7.346	-8.130	-9.998	-10.180
Confidence Level	50.00%	50.00%	50.00%	50.00%	50.00%

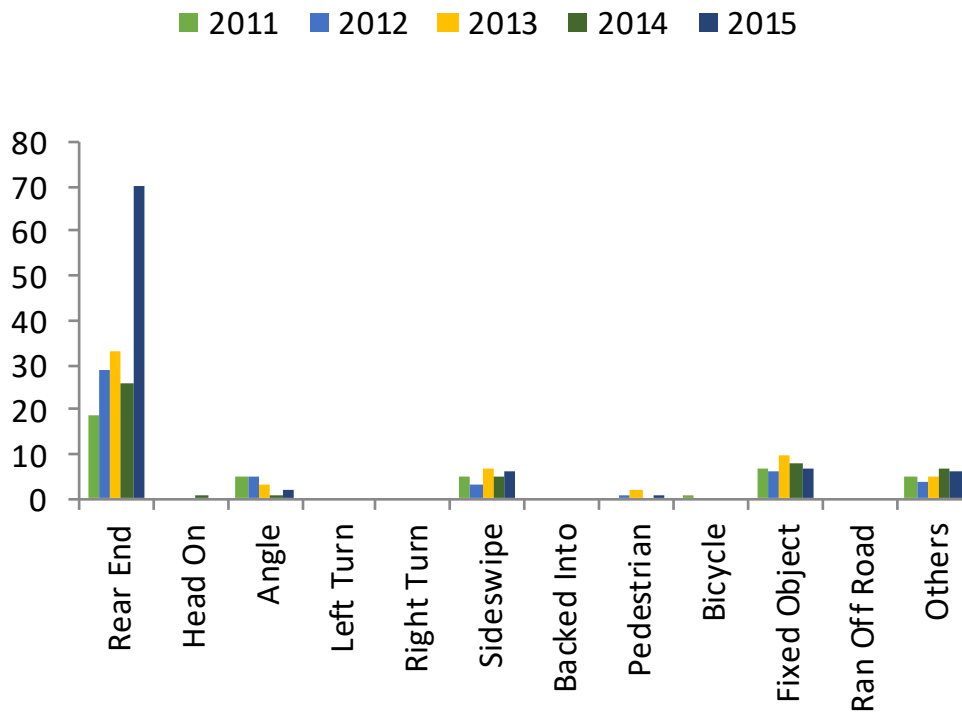
As presented in **Table 7-10**, Segment 5 is not considered to have an abnormally high number of crashes, since the confidence levels are lower than 99.95 percent. Based on the HSIP, urban intersections or segments with a confidence level of 99.95 percent or greater are considered as locations with abnormally high crashes.

The crash statistics associated with all the crash types for Segment 5 have been summarized in **Table 7-11** as well as in a detailed chart in **Exhibit 7-9** illustrating the crash types.

Table 7-11: Study Segment 5 Crash Statistics Summary (2011-2015)

SR 112/I-195 (Section 87004000) from just east of SR 5/Biscayne Boulevard EB/WB On/Off-ramps (MP 2.046) to Alton Road North/South Off-ramp (MP 4.351). Segment/Spot with No Expected Values Available		Number of Crashes					5 Year Total Crashes	Mean Crashes Per Year	%
		Year							
		2011	2012	2013	2014	2015			
CRASH TYPE	Rear End	19	29	33	26	70	177	35.40	61.0%
	Head On	0	0	0	1	0	1	0.20	0.3%
	Angle	5	5	3	1	2	16	3.20	5.5%
	Left Turn	0	0	0	0	0	0	0.00	0.0%
	Right Turn	0	0	0	0	0	0	0.00	0.0%
	Sideswipe	5	3	7	5	6	26	5.20	9.0%
	Backed Into	0	0	0	0	0	0	0.00	0.0%
	Coll. w/ Parked Car	0	0	0	0	1	1	0.20	0.3%
	Coll. w/ Pedestrian	0	1	2	0	1	4	0.80	1.4%
	Coll. w/ Bicycle	1	0	0	0	0	1	0.20	0.3%
	Fixed Object	7	6	10	8	7	38	7.60	13.1%
	Ran Off Road	0	0	0	0	0	0	0.00	0.0%
	Overtuned	1	0	1	3	1	6	1.20	2.1%
Other	4	4	4	4	4	20	4.00	6.9%	
	Total Crashes	42	48	60	48	92	290	58.00	100.0%
SEVERITY	PDO Crashes	19	29	37	30	60	175	35.00	60.3%
	Fatal Crashes	0	1	1	1	2	5	1.00	1.7%
	Injury Crashes	23	18	22	17	30	110	22.00	37.9%
LIGHTING CONDITIONS	Daylight	24	26	37	23	56	166	33.20	57.2%
	Dusk	1	3	5	1	3	13	2.60	4.5%
	Dawn	0	1	3	1	2	7	1.40	2.4%
	Dark	17	18	15	23	31	104	20.80	35.9%
	Unknown	0	0	0	0	0	0	0.00	0.0%
SURFACE CONDITIONS	Dry	35	33	44	34	71	217	43.40	74.8%
	Wet	7	15	16	14	21	73	14.60	25.2%
	Others	0	0	0	0	0	0	0.00	0.0%
MONTH OF YEAR	January	3	2	5	6	6	22	4.40	7.6%
	February	3	5	8	9	4	29	5.80	10.0%
	March	7	5	4	1	10	27	5.40	9.3%
	April	3	6	6	6	7	28	5.60	9.7%
	May	5	5	9	2	9	30	6.00	10.3%
	June	5	3	4	2	13	27	5.40	9.3%
	July	7	6	4	4	6	27	5.40	9.3%
	August	3	3	5	4	10	25	5.00	8.6%
	September	2	4	3	2	2	13	2.60	4.5%
	October	1	2	5	2	3	13	2.60	4.5%
	November	1	1	2	4	7	15	3.00	5.2%
	December	2	6	5	6	15	34	6.80	11.7%
DAY OF WEEK	Sunday	3	5	7	5	14	34	6.80	11.7%
	Monday	8	4	8	8	13	41	8.20	14.1%
	Tuesday	7	4	8	5	24	48	9.60	16.6%
	Wednesday	3	6	10	5	7	31	6.20	10.7%
	Thursday	5	10	9	13	21	58	11.60	20.0%
	Friday	6	12	12	6	7	43	8.60	14.8%
	Saturday	10	7	6	6	6	35	7.00	12.1%
HOUR OF DAY	00:00-06:00	13	10	9	12	9	53	10.60	18.3%
	06:00-09:00	4	3	12	4	9	32	6.40	11.0%
	09:00-11:00	3	5	3	2	9	22	4.40	7.6%
	11:00-13:00	1	1	3	2	4	11	2.20	3.8%
	13:00-15:00	7	3	9	4	6	29	5.80	10.0%
	15:00-18:00	6	14	13	12	28	73	14.60	25.2%
	18:00-24:00	8	12	11	12	27	70	14.00	24.1%

Exhibit 7-9: Study Segment 5 Crashes By Type (2011-2015)



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Segment 6 - [SR 112/Julia Tuttle Causeway/I-195 (Section 87004000) from Alton Road North/South Off-ramp (MP 4.351) to Alton Road (4.910)]

In Segment 6, there were a total of 332 crashes during the five-year study period with 35 crashes in 2011, 33 crashes in 2012, 85 crashes in 2013, 78 crashes in 2014, and 101 crashes in 2015. The three leading crash types along Segment 6 were rear-end with 248 crashes (75 percent), sideswipe with 31 crashes (9 percent), Other with 15 crashes (5 percent). Based on a preliminary review of the data, most crashes classified as other appear to be either rear-end or sideswipe crashes. There were 84 (25 percent) and 47 (14 percent) crashes which occurred during nighttime and wet pavement surface conditions, respectively. The nighttime crash percent was lower than the Department’s Districtwide average of 29 percent, and percentage of crashes occurring under wet pavement conditions was also lower than the Department’s Districtwide average of 16 percent for all roadways. Based on crash severity, there were 127 (38 percent) injury type crashes, and 203 (61 percent) property damage only crashes. There were two fatal crashes (<1 percent) reported during the referenced five-year period. The confidence levels for Segment 6 were calculated and summarized in **Table 7-12**.

Table 7-12: Segment 6: Confidence Levels (2011-2015)

Year	2011	2012	2013	2014	2015
Number of Crashes	35	33	85	78	101
Average Daily Traffic (ADT)	101,165	100,664	104,779	107,473	113,915
Actual Crash Rate (ACR)	1.696	1.607	3.976	3.557	4.345
District 6 Average Crash Rate (A)	1.400	1.546	1.854	2.018	2.641
Average Vehicle Exposure (M)	20.641	20.539	21.379	21.928	23.243
Critical Crash Rate (CCR)	2.233	2.425	2.800	2.994	3.729
Safety Ratio	0.759	0.663	1.420	1.188	1.165
Statistical Significance	1.228	0.310	7.285	5.149	5.120
Confidence Level	85.00%	50.00%	99.99%	99.99%	99.99%

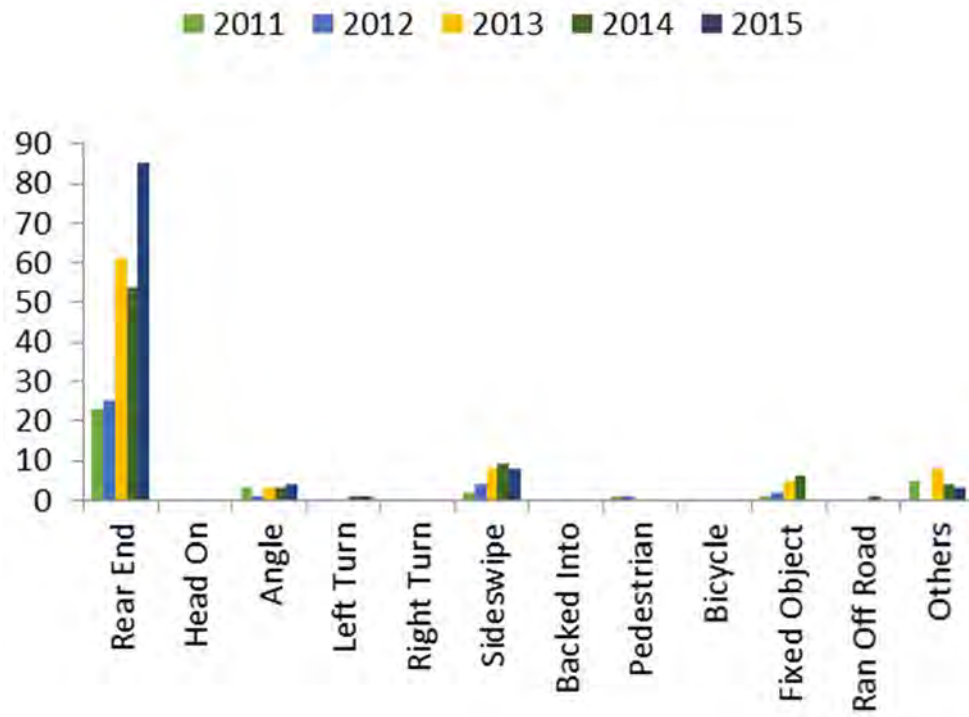
As presented in **Table 7-12**, Segment 6 is considered to have an abnormally high number of crashes in years 2013, 2014, and 2015, since the confidence levels are higher than 99.95 percent. Based on the HSIP, urban intersections or segments with a confidence level of 99.95 percent or greater are considered as locations with abnormally high crashes.

The crash statistics associated with all the crash types for Segment 5 have been summarized in **Table 7-13** as well as in a detailed chart in **Exhibit 7-10** illustrating the crash types.

Table 7-13: Study Segment 6 Crash Statistics Summary (2011-2015)

SR 112/Julia Tuttle Causeway/I-195 (Section 87004000) from Alton Road North/South Off-ramp (MP 4.351) to Alton Road (4.910).		Number of Crashes					5 Year Total Crashes	Mean Crashes Per Year	%
		Year							
		2011	2012	2013	2014	2015			
Segment/Spot with No Expected Values Available									
CRASH TYPE	Rear End	23	25	61	54	85	248	49.60	74.7%
	Head On	0	0	0	0	0	0	0.00	0.0%
	Angle	3	1	3	3	4	14	2.80	4.2%
	Left Turn	0	0	0	1	1	2	0.40	0.6%
	Right Turn	0	0	0	0	0	0	0.00	0.0%
	Sideswipe	2	4	8	9	8	31	6.20	9.3%
	Backed Into	0	0	0	0	0	0	0.00	0.0%
	Coll. w/ Parked Car	1	0	1	0	0	2	0.40	0.6%
	Coll. w/ Pedestrian	1	1	0	0	0	2	0.40	0.6%
	Coll. w/ Bicycle	0	0	0	0	0	0	0.00	0.0%
	Fixed Object	1	2	5	6	0	14	2.80	4.2%
	Ran Off Road	0	0	0	1	0	1	0.20	0.3%
	Overtuned	1	0	2	0	0	3	0.60	0.9%
	Other	3	0	5	4	3	15	3.00	4.5%
Total Crashes		35	33	85	78	101	332	66.40	100.0%
SEVERITY	PDO Crashes	11	20	56	49	67	203	40.60	61.1%
	Fatal Crashes	0	2	0	0	0	2	0.40	0.6%
	Injury Crashes	24	11	29	29	34	127	25.40	38.3%
LIGHTING CONDITIONS	Daylight	30	22	60	62	74	248	49.60	74.7%
	Dusk	1	0	2	2	3	8	1.60	2.4%
	Dawn	0	1	2	0	0	3	0.60	0.9%
	Dark	4	10	21	14	24	73	14.60	22.0%
	Unknown	0	0	0	0	0	0	0.00	0.0%
SURFACE CONDITIONS	Dry	32	28	73	63	89	285	57.00	85.8%
	Wet	3	5	12	15	12	47	9.40	14.2%
	Others	0	0	0	0	0	0	0.00	0.0%
MONTH OF YEAR	January	4	1	2	6	6	19	3.80	5.7%
	February	5	4	8	11	9	37	7.40	11.1%
	March	8	8	12	7	8	43	8.60	13.0%
	April	2	1	7	8	7	25	5.00	7.5%
	May	1	0	8	4	11	24	4.80	7.2%
	June	6	2	5	8	8	29	5.80	8.7%
	July	1	1	6	5	8	21	4.20	6.3%
	August	3	3	7	6	5	24	4.80	7.2%
	September	3	3	5	2	4	17	3.40	5.1%
	October	1	2	7	4	16	30	6.00	9.0%
	November	1	3	9	6	7	26	5.20	7.8%
	December	0	5	9	11	12	37	7.40	11.1%
DAY OF WEEK	Sunday	5	5	13	12	15	50	10.00	15.1%
	Monday	0	6	12	9	10	37	7.40	11.1%
	Tuesday	1	5	8	16	13	43	8.60	13.0%
	Wednesday	6	4	12	6	11	39	7.80	11.7%
	Thursday	10	5	13	18	24	70	14.00	21.1%
	Friday	4	4	16	8	17	49	9.80	14.8%
	Saturday	9	4	11	9	11	44	8.80	13.3%
HOUR OF DAY	00:00-06:00	2	3	11	6	7	29	5.80	8.7%
	06:00-09:00	9	3	9	15	6	42	8.40	12.7%
	09:00-11:00	3	4	16	12	13	48	9.60	14.5%
	11:00-13:00	2	3	6	5	9	25	5.00	7.5%
	13:00-15:00	5	5	10	14	18	52	10.40	15.7%
	15:00-18:00	11	8	18	14	28	79	15.80	23.8%
	18:00-24:00	3	7	15	12	20	57	11.40	17.2%

Exhibit 7-10: Study Segment 6 Crashes By Type (2011-2015)



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7.2.2 Ramp Crash Analysis

The crash data for a five-year study period (January 2011 to December 2015) were summarized for all ramps within the study area; however, no confidence levels analysis is provided, since there is no crash rate data available for ramps neither at the Districtwide nor Statewide level. The following present the list of ramps and major findings for the ramps analysis (from west to east):

- **Ramp 1:** SR 112/I-195 EB Off-ramp (87004019) to North Miami Avenue.
- **Ramp 2:** SR 112/I-195 WB On-ramp (87004018) from North Miami Avenue.
- **Ramp 3:** SR 112/I-195 EB Off-ramp (87004021) to Biscayne Boulevard.
- **Ramp 4:** SR 112/I-195 WB On-ramp (87004020) from Biscayne Boulevard.
- **Ramp 5:** SR 112/I-195 EB On-ramp (87004023) from NE 36th Street.
- **Ramp 6:** SR 112/I-195 WB Off-ramp (87004022) to NE 38th Street.
- **Ramp 7:** SR 112/I-195 EB Off-ramp (87004024) to SB Alton Road
- **Ramp 8:** SR 112/I-195 EB Off-ramp (87037201) to NB Alton Road
- **Ramp 9:** SR 112/I-195 WB On-ramp (87037202) from NB Alton Road.
- **Ramp 10:** SR 112/I-195 WB On-ramp (87004025) from SB Alton Road.

A detailed summary of documented crashes is presented for all ramps in **Table 7-14** on the next page.

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Table 7-14: Ramps Crash Summary (2011-2015)

Crash Statistics Description	Ramp 1: SR 112/I-195 EB Off-ramp (87004019) to North Miami Avenue		Ramp 2: SR 112/I-195 WB On-ramp (87004018) from North Miami Avenue		Ramp 3: SR 112/I-195 EB Off-ramp (87004021) to Biscayne Boulevard		Ramp 4: SR 112/I-195 WB On-ramp (87004020) from Biscayne Boulevard		Ramp 5: SR 112/I-195 EB On-ramp (87004023) from NE 36 Street		Ramp 6: SR 112/I-195 WB Off-ramp (87004022) to NE 38 Street		Ramp 7: SR 112/I-195 EB Off-ramp (87004024) to SB Alton Road		Ramp 8: SR 112/I-195 EB Off-ramp (87037201) to NB Alton Road		Ramp 9: SR 112/I-195 EB Off-ramp (87037202) to NB Alton Road		Ramp 10: SR 112/I-195 WB On-ramp (87004025) from SB Alton Road	
	5-Years	%	5-Years	%	5-Years	%	5-Years	%	5-Years	%	5-Years	%	5-Years	%	5-Years	%	5-Years	%	5-Years	%
Rear End	34	55.7%	0	0.0%	2	66.7%	2	100.0%	6	66.7%	2	25.0%	49	73.1%	1	25.0%	1	33.3%	5	38.5%
Head On	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%
Angle	10	16.4%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	1	12.5%	3	4.5%	0	0.0%	0	0.0%	0	0.0%
Left Turn	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%
Right Turn	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%
Sideswipe	10	16.4%	1	50.0%	0	0.0%	0	0.0%	2	22.2%	1	12.5%	3	4.5%	0	0.0%	1	33.3%	2	15.4%
Backed Into	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%
Coll. w/ Parked Car	1	1.6%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%
Coll. w/ Pedestrian	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	1	1.5%	0	0.0%	0	0.0%	0	0.0%
Coll. w/ Bicycle	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%
Fixed Object	2	3.3%	1	50.0%	1	33.3%	0	0.0%	0	0.0%	3	37.5%	7	10.4%	2	50.0%	1	33.3%	2	15.4%
Ran Off Road	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%
Overtuned	0	0.0%	0	0.0%	0	0.0%	0	0.0%	1	11.1%	0	0.0%	1	1.5%	0	0.0%	0	0.0%	0	0.0%
Other	4	6.6%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	1	12.5%	3	4.5%	1	25.0%	0	0.0%	4	30.8%
Total Crashes	61	100%	2	100%	3	100%	2	100%	9	100%	8	100%	67	100%	4	100%	3	100%	13	100%
SEVERITY																				
PDO Crashes	51	83.6%	2	100.0%	3	100.0%	2	100.0%	6	66.7%	4	50.0%	43	64.2%	4	100.0%	2	66.7%	10	76.9%
Fatal Crashes	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	2	3.0%	0	0.0%	0	0.0%	0	0.0%
Injury Crashes	10	16.4%	0	0.0%	0	0.0%	0	0.0%	3	33.3%	4	50.0%	22	32.8%	0	0.0%	1	33.3%	3	23.1%
LIGHTING CONDITIONS																				
Daylight	44	72.1%	1	50.0%	2	66.7%	2	100.0%	6	66.7%	3	37.5%	57	85.1%	2	50.0%	2	66.7%	9	69.2%
Night	17	27.9%	1	50.0%	1	33.3%	0	0.0%	3	33.3%	5	62.5%	10	14.9%	2	50.0%	1	33.3%	4	30.8%
Unknown	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%
SURFACE Conditions																				
Dry	56	91.8%	1	50.0%	2	66.7%	2	100.0%	6	66.7%	6	75.0%	58	86.6%	2	50.0%	3	100.0%	10	76.9%
Wet	5	8.2%	1	50.0%	1	33.3%	0	0.0%	3	33.3%	2	25.0%	9	13.4%	2	50.0%	0	0.0%	3	23.1%
Others	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%

Notes:

Leading Type of Crashes

The following findings are summarized from a review of **Table 7-14**:

- Overall, the three-leading type of crashes at the study ramps are rear end, angle and sideswipe crashes. In the case of ramp crashes, the rear-end and sideswipe crashes are typical of congested roadways with frequent lane changes, and aggressive maneuvers. On the other hand, angle crashes may be associated to lack of visibility at signalized intersections or other signal operations issues at the arterial level. Of the three-leading type of crashes, rear-end crashes account for most crashes with a total of 102 crashes (59 percent, total for all ramp crashes).
- The two ramps with the highest number of crashes, are Ramp 1 (EB Off-ramp to North Miami Avenue), and Ramp 7 (EB Off-ramp to Alton Road) with a total of 61 crashes (35 percent) and 67 crashes (39 percent), respectively. Rear end is also the top leading type of crash at the two locations.
- Nighttime crashes and wet-pavement-surface conditions crashes when combining statistics for all ramps along study limits are 25 percent and 15 percent, respectively. Overall, nighttime and wet-pavement-surface conditions crashes do not appear to be high based on the Department's Districtwide average for all roadways.
- Two fatalities were reported at Ramp 7 (EB Off-ramp to SB Alton Road). Based on review of crash data the first fatality involved a collision with pedestrian type of crash. The second state "Other non-Collision crashes;" however, the contributed cause of the crash indicates "Improper passing.

The crash summaries for all ramps reviewed, are presented in **Appendix E**.

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7.2.3 Cluster Analysis

Given the substantial number of crashes during the analysis period, a Geographic Information System (GIS) density map was prepared to visualize the clustering of all crashes as presented in **Table 7-15** below and **Exhibit 7-11** on the next page, also a detailed summary of documented crashes indicates that there are 8 distinct cluster locations within the study limits. Crashes are mainly concentrated at the following intersections:

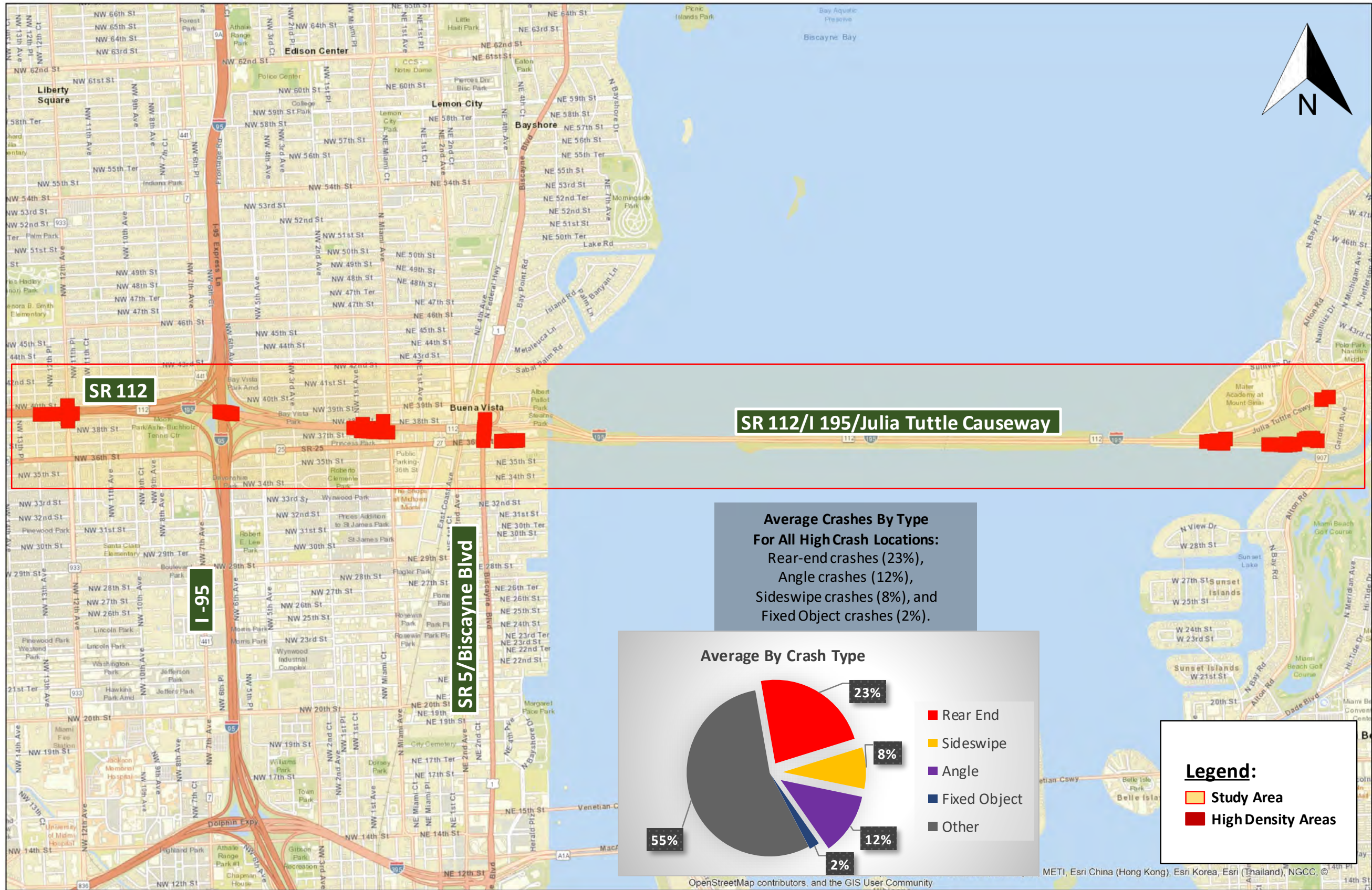
- **Cluster 1:** SR 112 at NW 12th Avenue (Section 87003000, MP 4.049).
- **Cluster 2:** SR 112/I-195 at I-95 (Section 87004000, MP 0.5107).
- **Cluster 3:** SR 112/I-195 at North Miami Avenue (Section 87004000, MP 1.055).
- **Cluster 4:** SR 5/Biscayne Boulevard at NE 36th Street (Section 87030000, MP 13.495).
- **Cluster 5:** SR 5/Biscayne Boulevard at NE 37th Street (Section 87030000, MP 13.615).
- **Cluster 6:** SR 112/I-195 at Exit Ramp to Alton Road (Section 87004000, MP 4.455).
- **Cluster 7:** SR 112/I-195 at N/S Exit Ramp to Alton Road (Section 87004000, MP 0.315).
- **Cluster 8:** SR 112/I-195 EB/WB at Alton Road (Section 87004000, MP 4.910).

Table 7-15: Crash Clusters Summary (2011-2015)

Cluster	S.R. ID	Main S. R.	Intersecting Street	MP	2010	2011	2012	2013	2014	5-Yr Crashes	Percent %
1	87003000	112	Exit Ramp To NW 12 Avenue	4.049	9	3	9	12	13	46	1.9%
2	87004000	112	I-95	0.517	5	6	7	0	11	29	1.2%
3	87004000	112	N Miami Avenue	1.055	19	20	19	29	23	110	4.6%
4	87030000	5	NE 36 Street	13.495	65	96	90	115	153	519	21.7%
5	87030000	5	NE 37 Street	13.615	29	41	44	38	61	213	8.9%
6	87004000	112	Exit Ramp To Alton Road	4.455	17	17	25	16	23	98	4.1%
7	87004024	112	N/S Ramp To Alton Road	0.315	5	0	4	7	18	34	1.4%
8	87004000	112	Alton Road	4.877	14	7	22	14	21	78	3.3%

Note: The percent presented was calculated based on the total number of crashes along the entire study segment.

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Project Name:



I-195 Corridor Planning Study from
 I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01



Exhibit Name:

Location of Crash Clusters (2011-2015)

Report Title:

Existing Conditions Report

Exhibit No. 7-11

Page No.

Date: 9/7/18

The crash statistics for the main eight clusters within the study area are presented in **Table 7-16**. These locations were selected because they experienced high number of crashes during the five-year study period. An Expected Values Analysis (EVA) was performed at the intersections meeting one of the identified 16 intersection types used in the report prepared for FDOT District 6 Traffic Operations Office titled: *Expected Values Analysis*, and dated June 2007. If the cluster under study did not match any of the 16 types used in the EVA report, then a Frequency Analysis was performed.

Table 7-16: Crash Clusters Analysis (2011-2015)

		High Crash Cluster Locations															
Cluster Locations		SR 112 (Section 87003000) at NW 12 Ave (MP 13.495)		SR 112/I-195 (Section 87004000) Under I-95 (MP 0.517)		SR 112/I-195 (Section 87004000) at N Miami Avenue (MP 1.055)		SR 5/Biscayne Boulevard (Section 87030000) at NE 36 Street (MP 13.508)		SR 5/Biscayne Boulevard at NE 37 Street (Section 87030000, MP 13.633)		SR 112/I-195 at Gore approaching Exit Ramp to Alton Rd		SR 112/I-95 Gore approaching N/S Exit Ramp to Alton Rd		SR 112/I-195 EB/WB at Alton Road (Section 87004000, MP 4.910)	
Abnormal Crash Type Analysis -->	Frequency		Frequency		Frequency		Expected Value		Frequency		Frequency		Frequency		Expected Value		
	5-Years	%	5-Years	%	5-Years	%	5-Years	%	5-Years	%	5-Years	%	5-Years	%	5-Years	%	
Rear End	19	41.3%	16	55.2%	66	60.0%	159	30.6%	67	31.5%	67	68.4%	25	73.5%	50	64.1%	
Head On	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	
Angle	10	21.7%	0	0.0%	10	9.1%	181	34.9%	40	18.8%	5	5.1%	1	2.9%	6	7.7%	
Left Turn	2	4.3%	0	0.0%	0	0.0%	38	7.3%	16	7.5%	0	0.0%	0	0.0%	4	5.1%	
Right Turn	0	0.0%	0	0.0%	0	0.0%	5	1.0%	0	0.0%	0	0.0%	0	0.0%	2	2.6%	
Sideswipe	8	17.4%	6	20.7%	18	16.4%	69	13.3%	44	20.7%	7	7.1%	1	2.9%	6	7.7%	
Backed Into	0	0.0%	0	0.0%	0	0.0%	1	0.2%	2	0.9%	0	0.0%	0	0.0%	0	0.0%	
Coll. w/ Parked Car	1	2.2%	0	0.0%	1	0.9%	3	0.6%	2	0.9%	1	1.0%	0	0.0%	3	3.8%	
Coll. w/ Pedestrian	1	2.2%	0	0.0%	0	0.0%	10	1.9%	0	0.0%	2	2.0%	1	2.9%	1	1.3%	
Coll. w/ Bicycle	1	2.2%	0	0.0%	0	0.0%	0	0.0%	2	0.9%	0	0.0%	0	0.0%	0	0.0%	
Fixed Object	2	4.3%	5	17.2%	8	7.3%	1	0.2%	1	0.5%	7	7.1%	4	11.8%	3	3.8%	
Ran Off Road	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	
Overturned	0	0.0%	0	0.0%	0	0.0%	2	0.4%	2	0.9%	3	3.1%	0	0.0%	0	0.0%	
Other	2	4.3%	2	6.9%	7	6.4%	50	9.6%	37	17.4%	6	6.1%	2	5.9%	3	3.8%	
Year(s) Crashes Abnormally High	---		---		---		2011-2015		---		---		---		---		
% of Cluster Crashes to Entire Study Area	1.9%	---	1.2%	---	4.6%	---	21.7%	---	8.9%	---	4.1%	---	1.4%	---	3.3%	---	
Total Intersection Crashes	46	100.0%	29	100.0%	110	100.0%	519	100.0%	213	100.0%	98	100.0%	34	100.0%	78	100.0%	
PDO Crashes	28	60.9%	21	72.4%	84	76.4%	419	80.7%	189	88.7%	53	54.1%	24	70.6%	50	64.1%	
Fatal Crashes	1	2.2%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	2	2.0%	1	2.9%	1	1.3%	
Injury Crashes	17	37.0%	8	27.6%	26	23.6%	100	19.3%	24	11.3%	43	43.9%	9	26.5%	27	34.6%	
Daylight	41	89.1%	26	89.7%	100	90.9%	457	88.1%	197	92.5%	87	88.8%	28	82.4%	63	80.8%	
Night	11	23.9%	11	37.9%	32	29.1%	127	24.5%	46	21.6%	25	25.5%	8	23.5%	18	23.1%	
Unknown	0	0.0%	0	0.0%	0	0.0%	1	0.2%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	
Dry	41	89.1%	26	89.7%	100	90.9%	457	88.1%	197	92.5%	87	88.8%	28	82.4%	63	80.8%	
Wet	5	10.9%	3	10.3%	10	9.1%	62	11.9%	15	7.0%	11	11.2%	6	17.6%	15	19.2%	
Others	0	0.0%	0	0.0%	0	0.0%	0	0.0%	1	0.5%	0	0.0%	0	0.0%	0	0.0%	

Notes:

- Abnormal crashes based on EVA
- Highest frequency crashes when EVA not possible
- Value above districtwide 5 year averages

The crash summaries for the clusters reviewed, are presented in **Appendix E**. The following are major findings of the cluster analysis (from west to east):

- Cluster 1:** rear-end crashes (19 collisions, 41 percent), angle crashes (10 collisions, 22 percent), and sideswipe crashes (8 collisions, 17 percent), are the leading crash types per a crash frequency analysis. Nighttime crashes and wet crashes are below the Department's Districtwide average for all roadways

- **Cluster 2:** rear-end crashes (16 collisions, 55.2 percent), sideswipe crashes (6 collisions, 20.7 percent), and fixed object crashes (5 collisions, 17 percent) are the leading crash types per a crash frequency analysis. Nighttime crashes are above Department's Districtwide average for all roadways. Wet crashes are below the Department's Districtwide average for all roadways.
- **Cluster 3:** rear-end crashes (66 collisions, 60 percent), angle crashes (10 collisions, 9.1 percent), and sideswipe crashes (18 collisions, 16 percent), are the leading crash types per a crash frequency analysis. Nighttime crashes are above Department's Districtwide average for all roadways. Wet crashes are below the Department's Districtwide average for all roadways.
- **Cluster 4:** angle crashes (181 collisions, 34.9 percent), rear-end crashes (159 collisions, 31 percent), sideswipe crashes (69 collisions, 13 percent) are the leading abnormally high type of crashes per EVA analysis. Nighttime and wet crashes are below the Department's Districtwide average for all roadways; however, they also appear to be abnormally high per EVA analysis.
- **Cluster 5:** rear-end crashes (67 collisions, 32 percent), sideswipe crashes (44 collisions, 21 percent), and angle crashes (40 collisions, 19 percent) are the leading crash types per a crash frequency analysis. Nighttime crashes and wet crashes are below the Department's Districtwide average for all roadways
- **Cluster 6:** rear-end crashes (67 collisions, 68 percent), sideswipe crashes (7 collisions, 7 percent), and fixed object crashes (7 collisions, 7 percent), are the leading crash types per a crash frequency analysis. Nighttime crashes and wet crashes are below the Department's Districtwide average for all roadways
- **Cluster 7:** rear-end crashes (25 collisions, 74 percent), fixed object crashes (4 collisions, 12 percent), and other crashes (2 collisions, 6 percent), are the leading crash types per a crash frequency analysis. Nighttime crashes and wet crashes are below the Department's Districtwide average for all roadways
- **Cluster 8:** rear-end crashes (50 collisions, 64 percent) and collision with parked cars crashes (3 collisions, 4 percent) are the leading abnormally high type of crashes per EVA analysis. Wet crashes are above the Department's Districtwide average for all roadways.

SUMMARY OF FINDINGS:

- Most cluster locations have rear-end and sideswipe crashes as their leading frequency crash types during the five-year study period.
- Five of the cluster locations have wet crashes that are either abnormally high or above Department's Districtwide average for all roadway.
- There is a high number of angle crashes at Cluster 4 and Cluster 5, with a total of 181 crashes (35 percent) and 40 crashes (19 percent) respectively. Based on the EVA analysis, angle crashes at Cluster 4 are abnormally high.
- It is worth noticing that Rear-end and Sideswipe type crashes are typical of highly congested roadways with frequent stop-and-go traffic conditions. Also consider that some locations are directly impacted by traffic within influence area of on or off ramps to I-195.
- Some of the locations with considerable number of fixed object crashes are locations with either limited visibility (vertical or horizontal sight distance issues) or at gores approaching exiting ramp due to driver hesitation.

The clusters with identified crash patterns, per Frequency Analysis or EVA are summarized in **Table 7-17** below and **Table 7-18** on the following page. These tables also include a list of probable contributing causes based solely on the review of the crash summaries and aerial photography such as Google Earth.

Table 7-17: Probable Causes

Cluster	Crash Pattern(s)	Probable Cause(s)
SR 112/I 195 at Exit Ramp To NW 12 Avenue	Rear End (41%) (Mostly eastbound)	<ul style="list-style-type: none"> • Congestion (related to mixed traffic: freight, commuters, tourist) • Excessive speed • Ramp Traffic spillover • Lack of Deceleration lane • Construction • Aggressive driving
	Angle (22%)	<ul style="list-style-type: none"> • Crashes may be miscoded • Congestion (related to mixed traffic: freight, commuters, tourist) • Capacity constraints
	Sideswipe (17%)	<ul style="list-style-type: none"> • Congestion • Sign Visibility issues • Abrupt lane changing • Construction • Ramp lane spillover
SR 112/I 195 Under I-95 Overpass	Rear End (55%) (Mostly eastbound)	<ul style="list-style-type: none"> • Congestion • Excessive speed • Narrowing of Lanes (merge lane related)
	Sideswipe (21%)	<ul style="list-style-type: none"> • Excessive speed • Narrowing of Lanes (merge lane related) • Abrupt lane changing
	Fixed Object (17%)	<ul style="list-style-type: none"> • Poor Visibility • Poor Signage • Excessive speed • Narrowing of Lanes (merge lane related) • Poor Lighting Conditions
SR 112/I 195 at N Miami Avenue	Rear End (60%) (Mostly eastbound)	<ul style="list-style-type: none"> • Congestion (due to capacity constrains at arterial level) • Excessive speed • Ramp Traffic spillover • Lack of Deceleration lane
	Sideswipe (16%) (Mostly during peak traffic periods)	<ul style="list-style-type: none"> • Congestion (due to capacity constrains at arterial level) • Excessive speed • Ramp Traffic spillover
	Angle (9%) (Mostly eastbound during PM peak period)	<ul style="list-style-type: none"> • Congestion (due to capacity constrains at arterial level) • Traffic Operational Issues • Sight distance issues

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Table 7-18: Probable Causes

Cluster	Crash Pattern(s)	Probable Cause(s)
SR 5 at NE 36 Street	Angle (35%) (Mostly eastbound)	<ul style="list-style-type: none"> • Congestion • Traffic Operational Issues • Aggressive driving • Limited Capacity • Sight distance issues
	Rear End (31%)	<ul style="list-style-type: none"> • Congestion • Traffic Operational Issues
	Sideswipe (13%) (Mostly eastbound during PM peak period)	<ul style="list-style-type: none"> • Congestion • Roadway Alignment • Traffic Operational Issues
SR 5 At NE 37 Street	Rear End (31%) (Mostly eastbound)	<ul style="list-style-type: none"> • Congestion • Traffic Operational Issues
	Sideswipe (21%) (Mostly during peak traffic periods)	<ul style="list-style-type: none"> • Congestion • Roadway Alignment • Traffic Operational Issues
	Angle (19%) (Mostly eastbound during PM peak period)	<ul style="list-style-type: none"> • Congestion • Traffic Operational Issues • Sight distance issues
SR 112/I 195 at Gore Approaching Exit Ramp To Alton Road	Rear End (68%) (Mostly eastbound)	<ul style="list-style-type: none"> • Aggressive driving • Abrupt lane changing • Congestion (related to ramp spillover) • Horizontal and Vertical Curb issues • Excessive speed • Drivers not aware of access points • Poor Signage
	Sideswipe (7%) (Mostly eastbound)	<ul style="list-style-type: none"> • Congestion • Abrupt lane changing • Aggressive driving
SR 112/I 195 at Gore Approaching Exit Ramp To N/S Alton Road	Rear End (73%) (Mostly eastbound)	<ul style="list-style-type: none"> • Aggressive driving • Abrupt lane changing • Congestion
	Fixed Object (12%) (Mostly eastbound)	<ul style="list-style-type: none"> • Excessive speed • Drivers not aware of access points
SR 112/I 195 at Alton Road	Rear End (64%) (Mostly eastbound)	<ul style="list-style-type: none"> • Congestion • Vertical grade differential • Drivers not aware of access points
	Coll.w/Parked Car (4%) (Mostly eastbound)	<ul style="list-style-type: none"> • Congestion • Aggressive driving • Abrupt lane changing • Capacity constraints

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8.0 ENVIRONMENTAL

This section of the report documents the existing environmental conditions within the study corridor and its environs. Background information was collected, and agency databases were reviewed to determine the potential environmental issues within the existing I-195 and I-95 project area.

The environmental review was conducted using the following sources:

- The Efficient Transportation Decision Making (ETDM) Environmental Screening Tool (EST).
- Florida Geographic Data Library (FGDL)
- Aerial Photographs and Street Maps
- Field visit on July 13, 2018

8.1 Historic Resources

According to the requirements of *Section 106 of the National Historic Preservation Act (NHPA)*, a review was undertaken to identify historic resources that could be impacted depending on the alternatives developed. There are three National Register-eligible Historic Districts and one National Register-listed Site within the study area. The National Register (NR) eligible Bay Vista Park Historic District is adjacent to the east side of I-95 between NW 39th Street and SR 944/NW 54th Street. In addition, the NR-eligible Sunset Lake Historic District is located south of the bridge from the southbound on ramp to Alton Road towards Chase Avenue. The NR-eligible 41st Street Historic District is located along NW 41st Street from SR 907/Alton Road to Pine Tree Drive. The Giller Building, located at 975 West 41st Street is a State Historic Preservation Officer (SHPO) NR Site. There are numerous historic-aged structures and properties documented throughout the project area. No archaeological sites are documented within the project area. A limited Cultural Resource Assessment Survey (CRAS) evaluation of potential impacts to structures and properties and coordination with the SHPO under Stipulation VII of the Section 106 Programmatic Agreement will be performed later as part of this Corridor Planning Study. A more in-depth Section 106 and Section 4 (f) review will most likely be conducted during the project's Project Development and Environment (PD&E) phase.

8.2 Parks and Recreational Facilities

According to the requirements of *Section 4(f) of the US Department of Transportation Act*, a review was undertaken to identify parks and recreational facilities that could be impacted depending on the alternatives developed. Multiple parks and recreational facilities are located within the study area. Two parks are listed in ETDM EST as a Florida Park and Recreation Facility (Sterns Park and Martell Park); however, these properties are transportation facilities, owned by FDOT. The remaining parks and recreational facilities identified within or adjacent to the project study area are listed in the **Table 8-1** on the following page. During the PD&E phase, these facilities will be evaluated to determine if Section 4(f) will apply

Table 8-1: Summary of Park and Recreational Facilities

Category	Name	Location
SCHOOLS	Jose De Diego Middle	3100 NW 5 Avenue
	Miami Edison Senior High	6161 NW 5 Court
	Eneida Massas Hartner Elementary	401 NW 28 Street
	Design & Architecture Senior High	4001 NE 2 Avenue
	Nautilus Middle	4301 N Michigan Avenue
PARKS	Albert Pallot Park	NE 39 Terrace
	Woodson Mini Park	3620 NE 2 Avenue
	Roberto Clemente Park	101 NW 34 Street
	Moore Park	765 NW 36 Street
	North Bay Vista Park	NW 49 Street
	South Bay Vista Park	NW 47 Street
	West Buena Vista Park	4498 NW 11th Plaza
	Miller J Dawkins Mini Park	3625 NE 2 Avenue
	Robert E Lee Park	3000 NW 6 Avenue
	Athalie Range Park	525 NW 62 Street
	Charles Hadley Park	1350 NW 50 Street
	Muss Park	4300 Chase Avenue
Polo Park	4301 N Michigan Avenue	
GULF COURSES	Miami Beach Golf Club	2301 Alton Road

8.3 Planned Community Growth/Land Use

The project area encompasses public/semi-public land, parking, institutional, industrial, residential (private homes and condominiums; high and low density), retail/offices, recreation facilities and rangeland (land portion of Julia Tuttle Causeway).

8.4 Social

The study area includes Midtown Miami and the Miami Design District and continues to the west end of Mid-Beach (Miami Beach). I-195/Julia Tuttle Causeway from NW 12th Avenue to Alton Road is part of the National Highway System (NHS). There are 22 schools, 2 colleges, 8 community centers, 17 daycares, 3 healthcare centers, 2 free standing healthcare clinics, 2 government buildings, 13 parks, 42 religious facilities, 4 social services, the Edison Center Library Branch, Miami Municipal Fire Station #6, and the One-Mind Entertainment Center. A complete list and locations of social and community features are included in **Appendix F**.

8.5 Contamination

A contamination review of the FDOT ETDM EST, which contains, but is not limited to, GIS layers of the US Environmental Protection Agency (USEPA), Florida Department of Environmental Protection (FDEP) and Miami-Dade County Department of Regulatory and Economic Resources, was performed. Based on the review, there are several known contaminated sites within a 1,000-foot buffer of the study area. There are six Compliance and Enforcement Tracking for Hazardous (CHAZ) Facilities, one solid waste site, two dry cleaners and 29 petroleum storage tanks. In addition, one superfund site is within a quarter mile buffer of the corridor. See **Appendix F** for a complete list and figure of locations of contaminated sites.

8.6 Navigable Waterway

The Atlantic Intracoastal Waterway (ICW) crosses the study area at the western most bridge on I-195/Julia Tuttle Causeway. Any changes to the elevation of the bridge or horizontal clearance of the channel which could impact navigation will require coordination with the United States Coast Guard (USCG) and a permit may be required.

8.7 Water Quality and Wetlands

Based on a review of the ETDM EST and desktop aerial review, two detention ponds are located on the southwest and northeast corners of the I-95/I-195 interchange. The coastline along the fill islands of I-195/Julia Tuttle Causeway and the coastline intersecting I-195 contain species of red mangroves (*Rhizophora mangle*) and white mangroves (*Laguncularia racemosa*). The study area crosses Biscayne Bay Aquatic Preserve, which is designated as Outstanding Florida Waters (OFW). Preparation of a Natural Resource Evaluation (NRE) to document wetland impacts will need to be prepared in the PD&E phase of the project.

8.8 Endangered and Threatened Species, Wildlife and Critical Habitats

The ETDM EST identified that the project limits are located within the U.S. Fish and Wildlife Service's (USFWS) consultation area for Johnson's seagrass (*Halophila johnsonii*), West Indian manatee (*Trichechus manatus*), piping plover (*Charadrius melodus*), American crocodile (*Crocodylus acutus*), and Atlantic coast plants (i.e. mangroves species, etc.). The mainland portion of the project falls within the Florida bonneted bat consultation area (*Eumops floridanus*). In addition, the project study area crosses critical habitat for Johnson's seagrass (*Halophila johnsonii*) along the Julia Tuttle Causeway.

Any in-water work may require a benthic survey and coordination with the National Marine Fisheries Service (NMFS). Preparation of a NRE to document listed species and their habitat will be prepared in the PD&E phase and submitted to the USFWS and NMFS for consultation under Section 7 of the Endangered Species Act.

8.9 Essential Fish Habitat (EFH)

Based on a review of the ETDM EST, and the NMFS Essential Fish Habitat Mapper, the study area includes EFH for Snapper Grouper (Serranidae), Spiny Lobster (Palinurus spp.), Black Tip Shark (Carcharhinus limbatus), Bull Shark (Carcharhinus leucas), Sandbar Shark (Carcharhinus plumbeus), Golden crab, Dolphin and wahoo, Sargassum, Shrimp, Skip Jack Tuna (Katsuwonus pelamis), Red Drum (Sciaenops ocellatus), and corals (Anthozoans). Habitat Areas of Special Concern (HAPCs) are defined as subsets of EFH that provide critically important ecological functions or are especially vulnerable to degradation. HAPCs present within the I-195/Julia Tuttle Causeway includes Biscayne Bay, coral, coral reefs and live/hardbottom and seagrass habitat. Based on the field review red mangroves (Rhizophora mangle) and white mangroves (Laguncularia racemosa) are present on the fill islands on I-195 and seagrass is present along the fill islands along I-195. Preparation of a NRE to document impacts to EFH will be prepared in the PD&E phase and submitted to the NMFS for consultation under the Magnuson Stevens Act.

8.10 Floodplains

The project area includes both mainland Miami, Biscayne Bay and Miami Beach. Floodplain area A, AE, and AH are designated for the Miami Beach project area. The mainland includes floodplain designations A, AE, and AH for the area approximately four blocks inland of the coastline.

8.11 Air Quality and Noise

The project is located in an area which is designated attainment for all of the National Ambient Air Quality Standards under the criteria provided in the Clean Air Act. Therefore, the Clean Air Act conformity requirements do not apply. Numerous parks, schools, and residential areas are immediately adjacent to I-195 and I-95. Any scope which involves added capacity and/or the addition of auxiliary lanes will require a noise analysis per 23 CFR 772 to be conducted during the PD&E phase.

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EXISTING CONDITIONS REPORT APPENDIX



I-195 Corridor Planning Study

Project Study Limits:
I-95/NW 12th Avenue to Alton Road
Miami-Dade County, Florida

Financial Management Number: 440228-1-22-01

Prepared for:



Prepared by:

BCC Engineering, Inc.
6401 SW 87th Avenue, Suite 200
Miami, FL 33173
October 2018



TABLE OF CONTENTS

APPENDIX A – TRANSPORTATION PLANS & STUDIES SUPPORTING INFORMATION.....A

- SMART PLAN MAP
- JULIA TUTTLE SHARED USE PATH CONCEPT
- BISCAYNE BLVD SAFETY STUDY IMPROVEMENTS

APPENDIX B – ROADWAY CHARACTERISTICS SUPPORTING INFORMATION B

- PROJECT BASE MAPS
- TYPICAL SECTION DETAILS
- EXISTING ITS EQUIPMENT SUMMARY
- EXISTING MAJOR UTILITIES
- SWFMD DRAINAGE MAP
- FEMA MAP FIRMETTES

APPENDIX C – EXISTING TRANSIT SUPPORTING INFORMATION..... C

- MIAMI DADE TRANSIT ROUTE MAPS
- MDT RIDERSHIP TECHNICAL REPORT

APPENDIX D – TRAFFIC DATA SUPPORTING INFORMATION.....D

- TRAFFIC DATA COLLECTION REPORT
- PEAK SPREADING REVIEW
- TRAFFIC SIGNAL TIMING PLANS

APPENDIX E – SAFETY ANALYSIS SUPPORTING INFORMATION..... E

- STUDY LIMITS CRASH DATA (2011-2015)
- SEGMENTS CRASH DATA (2011-2015)
- RAMP CRASH DATA (2011-2015)
- CLUSTER CRASH DATA (2011-2015)

APPENDIX F – ENVIRONMENTAL REVIEW SUPPORTING INFORMATION.....F

- COMMUNITY FEATURES
- CONTAMINATED SITES





APPENDIX A – TRANSPORTATION PLANS & STUDIES SUPPORTING INFORMATION

- SMART PLAN MAP
- JULIA TUTTLE SHARED USE PATH CONCEPT
- BISCAYNE BLVD SAFETY STUDY IMPROVEMENTS

SMART PLAN MAP

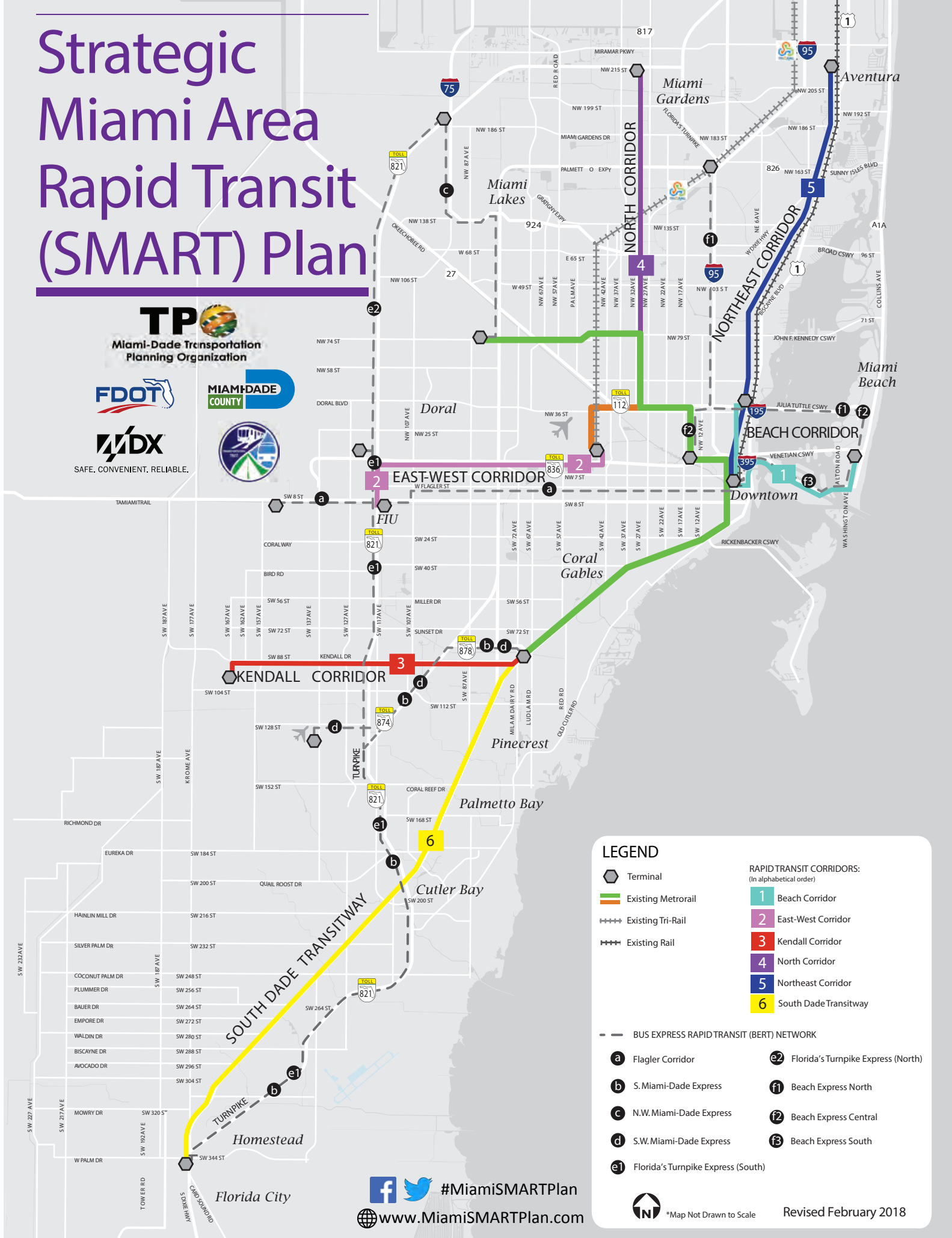
Strategic Miami Area Rapid Transit (SMART) Plan



SAFE. CONVENIENT. RELIABLE.



TAMIAMI TRAIL



LEGEND

- Terminal
- Existing Metrorail
- Existing Tri-Rail
- Existing Rail

RAPID TRANSIT CORRIDORS: (In alphabetical order)

- 1** Beach Corridor
- 2** East-West Corridor
- 3** Kendall Corridor
- 4** North Corridor
- 5** Northeast Corridor
- 6** South Dade Transitway

BUS EXPRESS RAPID TRANSIT (BERT) NETWORK

- a** Flagler Corridor
- b** S. Miami-Dade Express
- c** N.W. Miami-Dade Express
- d** S.W. Miami-Dade Express
- e1** Florida's Turnpike Express (South)
- e2** Florida's Turnpike Express (North)
- f1** Beach Express North
- f2** Beach Express Central
- f3** Beach Express South

*Map Not Drawn to Scale

JULIA TUTTLE SHARED USE PATH CONCEPT



I-195 (Julia Tuttle Causeway)

MATCH LINE - A

**C H Perez and Associates
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 Tel: (305) 552-9170 | Fax: (305) 552-1508
 Carlos H. Perez
 Florida P.E. No. 52080
www.chpe.com chp@chpe.com

JULIA TUTTLE CAUSEWAY SHARED USE PATH

SHEET NO.



MATCHLINE - A

MATCHLINE - B



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JULIA TUTTLE CAUSEWAY SHARED USE PATH

**SHEET
NO.**



MATCH LINE - F

MATCH LINE - G

I-195 (Julia Tuttle Causeway)

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JULIA TUTTLE CAUSEWAY SHARED USE PATH

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JULIA TUTTLE CAUSEWAY SHARED USE PATH

SHEET
NO.



I-195 (Julia Tuttle Causeway)

MATCH LINE - H

MATCH LINE - I

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JULIA TUTTLE CAUSEWAY SHARED USE PATH

SHEET
NO.



MATCHLINE - I

I-195 (Julia Tuttle Causeway)

MATCHLINE - J

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JULIA TUTTLE CAUSEWAY SHARED USE PATH

SHEET
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MATCH LINE - K

I-195 (Julia Tuttle Causeway)

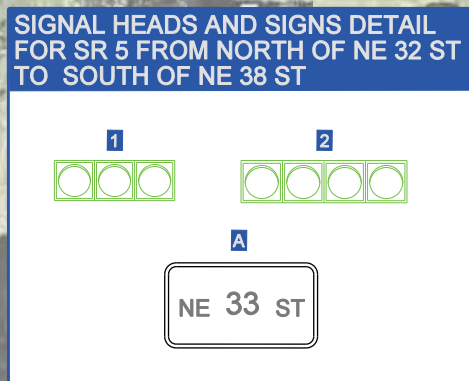
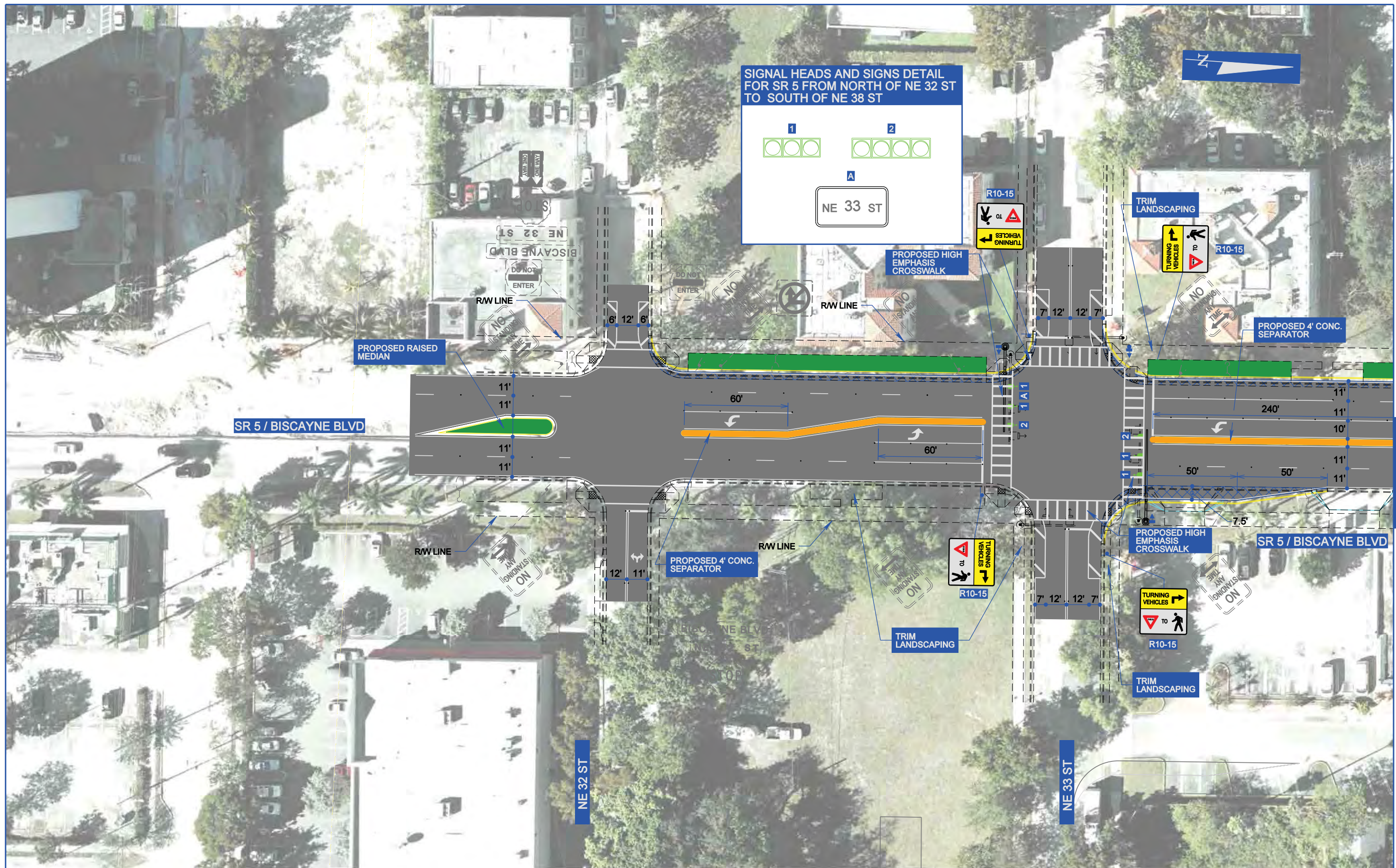


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JULIA TUTTLE CAUSEWAY SHARED USE PATH

SHEET NO.

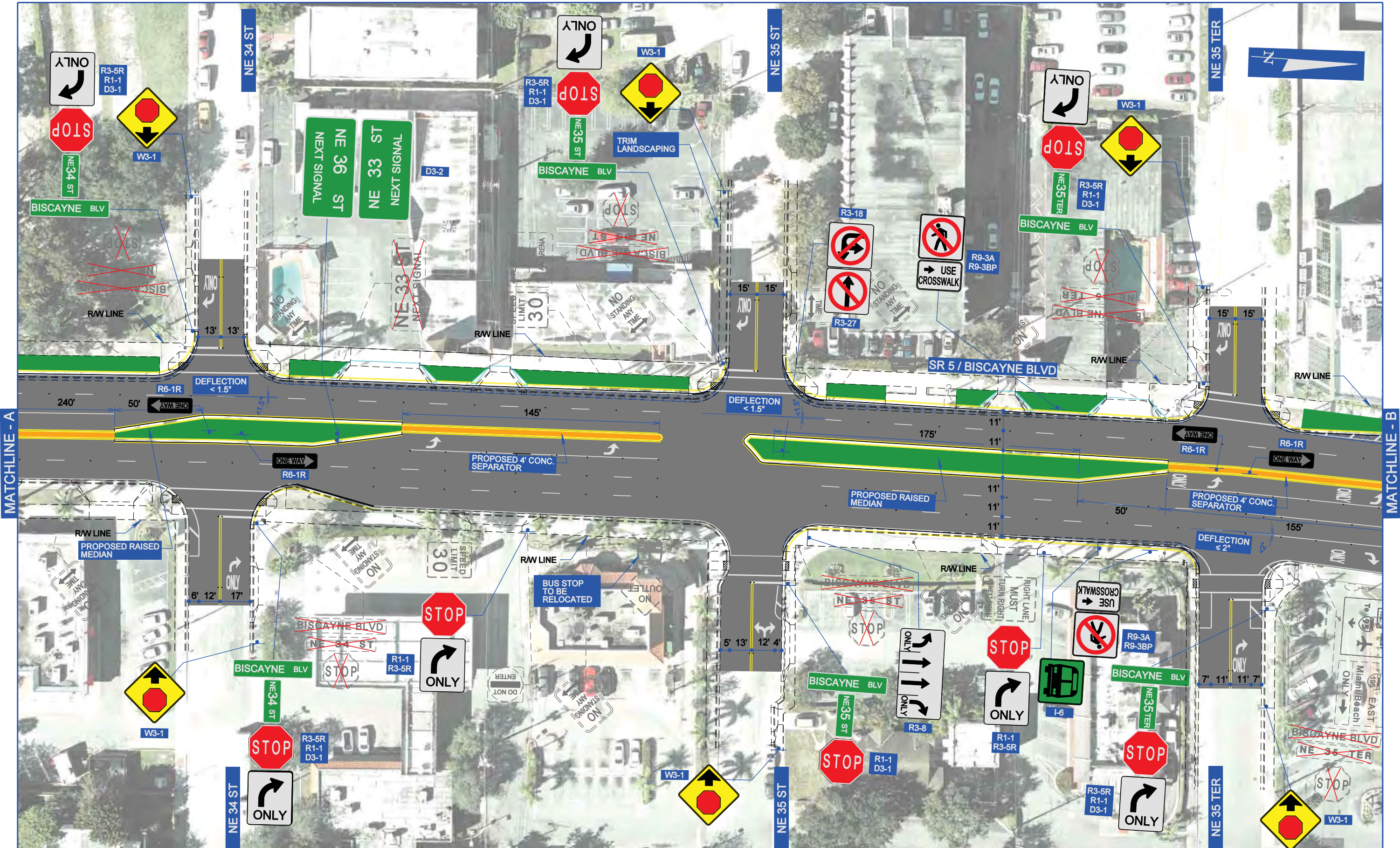
BISCAYNE BLVD SAFETY STUDY IMPROVEMENTS



LEGEND:

**PROPOSED CONDITION DIAGRAM
SR 5 FROM NORTH OF NE 32 ST
TO SOUTH OF NE 38 ST**

FIGURE NO.
10-A



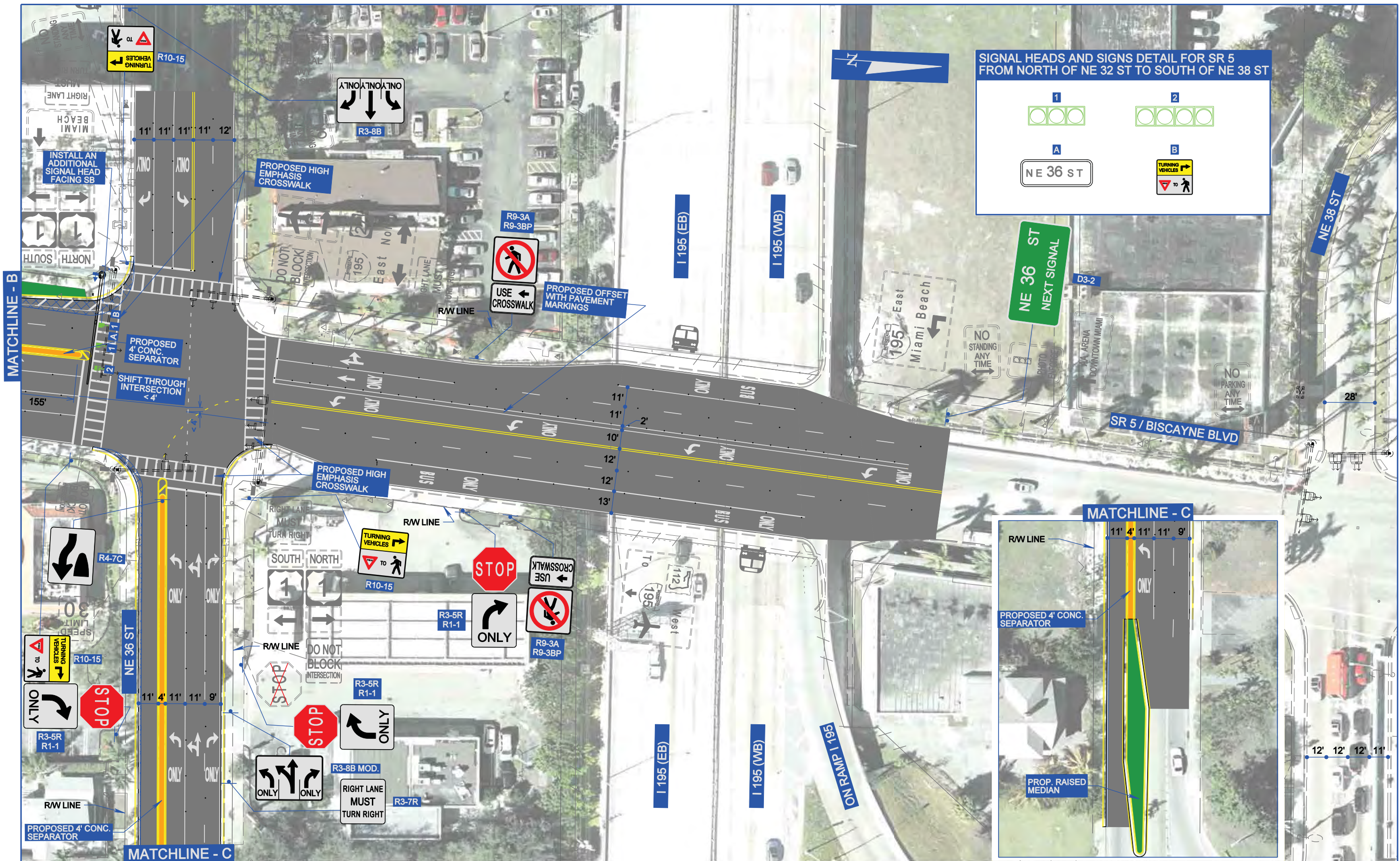
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MATCHLINE - B

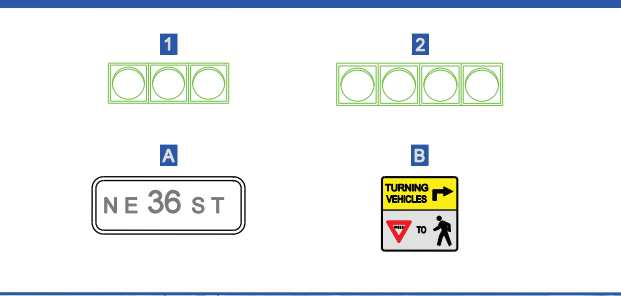
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**PROPOSED CONDITION DIAGRAM
SR 5 FROM NORTH OF NE 32 ST
TO SOUTH OF NE 38 ST**

FIGURE NO.
10-B

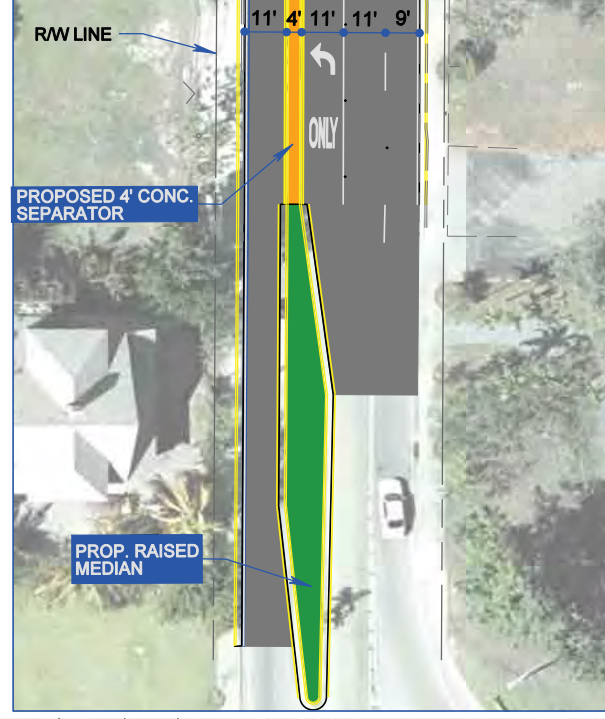


SIGNAL HEADS AND SIGNS DETAIL FOR SR 5 FROM NORTH OF NE 32 ST TO SOUTH OF NE 38 ST



NE 36 ST
NEXT SIGNAL

MATCHLINE - C



LEGEND:

**PROPOSED CONDITION DIAGRAM
SR 5 FROM NORTH OF NE 32 ST
TO SOUTH OF NE 38 ST**

FIGURE NO.
10-C

APPENDIX B – ROADWAY CHARACTERISTICS SUPPORTING INFORMATION

- PROJECT BASE MAPS
- TYPICAL SECTION DETAILS
- EXISTING ITS EQUIPMENT SUMMARY
- EXISTING MAJOR UTILITIES
- SWFMD DRAINAGE MAP
- FEMA MAP FIRMETTES

PROJECT BASE MAPS



PROJECT LAYOUT (SHEET 1 OF 2)

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 1



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet

No: 2



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 3



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 4



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 5



Project Name:

Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet No: 6



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 7



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 8



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

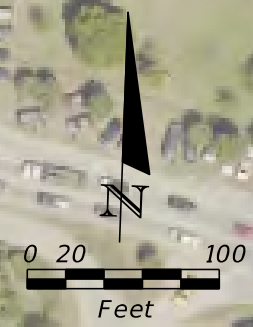
EXISTING CONDITIONS REPORT

Date:

Sheet

No:

9



Project Name:

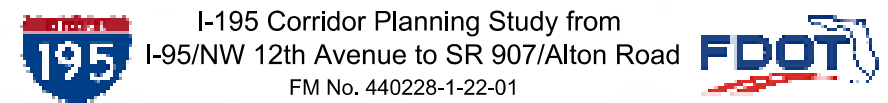


Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet No: 10



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

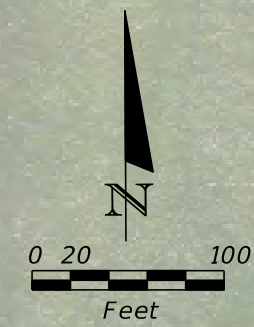
PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 11



MATCHLINE 226+00.00 @ CL195, SHEET NO. 11

MATCHLINE 240+00.00 @ CL195, SHEET NO. 13

PT STA. 226+43.65

230

I-195 WB

235

S 89° 46' 24" E

BRIDGE NO. 870314

I-195 EB

Project Name:



I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

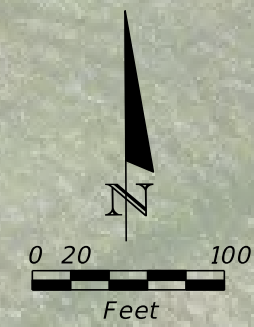
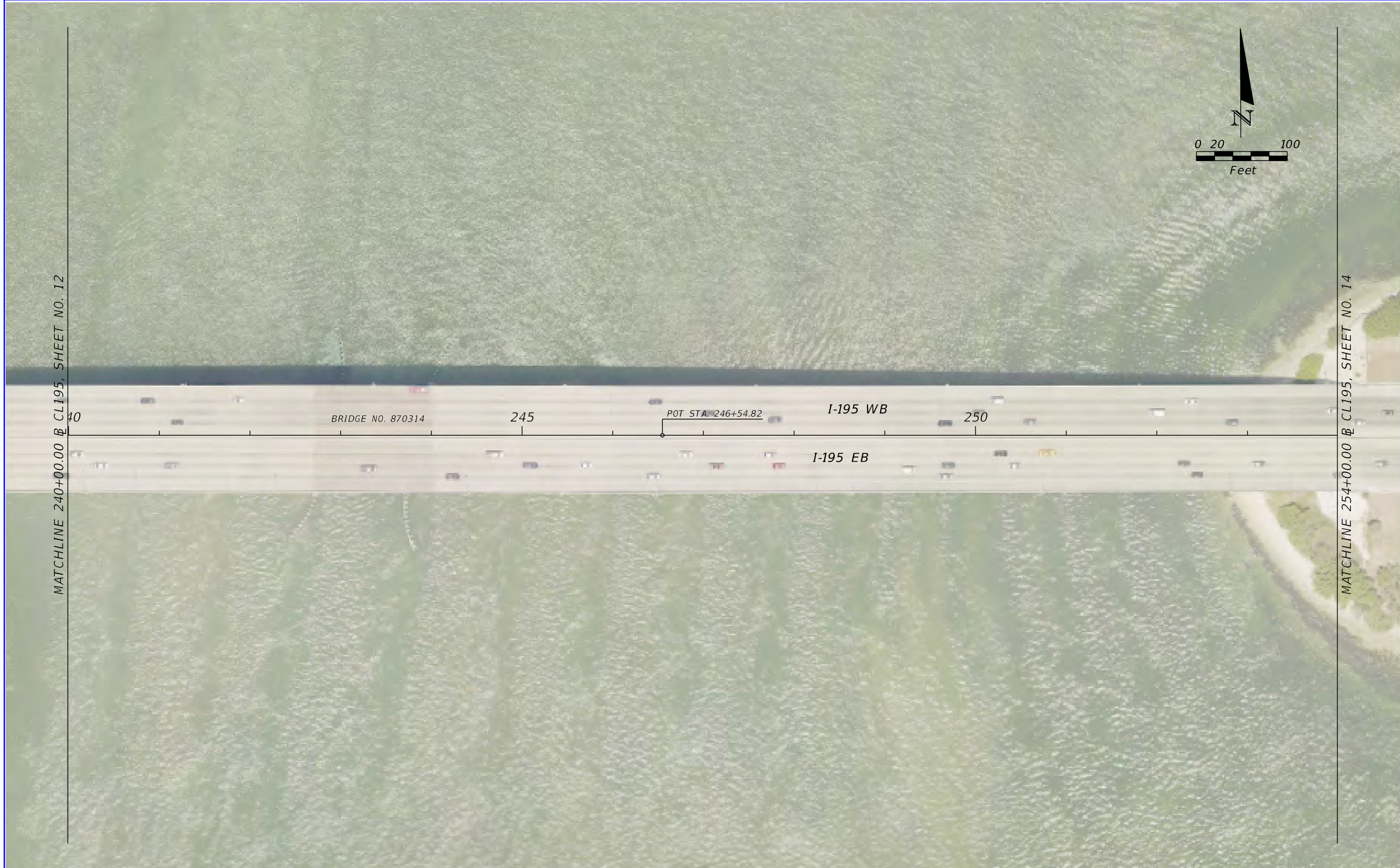
PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT


Date:

Sheet No: 12



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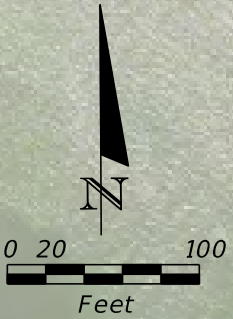
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<p>Project Name: I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road  FM No. 440228-1-22-01</p>	<p>Exhibit Name: PROJECT LAYOUT AND BASE MAPS</p>	<p>Report Name: EXISTING CONDITIONS REPORT</p>	<p>Date: Sheet No: 13</p>
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MATCHLINE 254+00.00 @ CL195, SHEET NO. 13

MATCHLINE 268+00.00 @ CL195, SHEET NO. 15



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

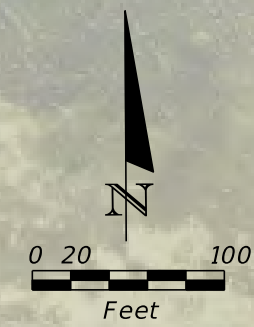
PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 14



MATCHLINE 268+00.00 B CL195, SHEET NO. 14

MATCHLINE 282+00.00 B CL195, SHEET NO. 16



270

275

280

I-195 WB

I-195 EB

Project Name:



I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road FM No. 440228-1-22-01



Exhibit Name:

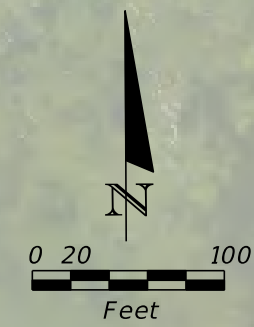
PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet No: 15



MATCHLINE 282+00.00 @ CL195, SHEET NO. 15

MATCHLINE 296+00.00 @ CL195, SHEET NO. 17

I-195 WB

285

290

295

I-195 EB

Project Name:



I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road FM No. 440228-1-22-01



Exhibit Name:

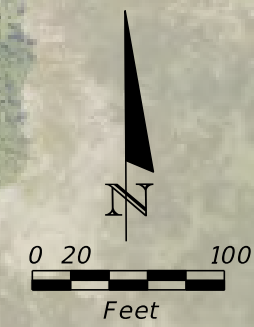
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Report Name:

EXISTING CONDITIONS REPORT

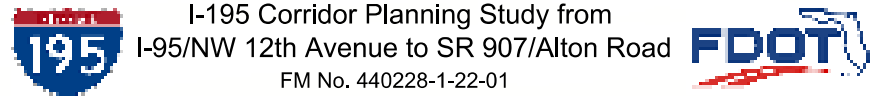
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Sheet No: 16



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MATCHLINE 310+00.00 @ CL195, SHEET NO. 18

Project Name:	 I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road FM No. 440228-1-22-01	Exhibit Name: PROJECT LAYOUT AND BASE MAPS	Report Name: EXISTING CONDITIONS REPORT	Date: Sheet No: 17
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Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

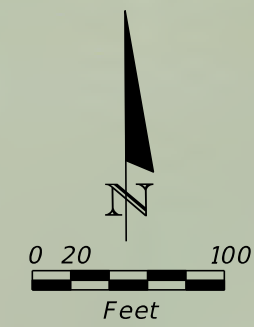
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Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet No: 18



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MATCHLINE 338+00.00 @ CL195, SHEET NO. 20

325

330

I-195 WB

BRIDGE NO. 870302

335

I-195 EB

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet No: 19



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 20



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 21



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



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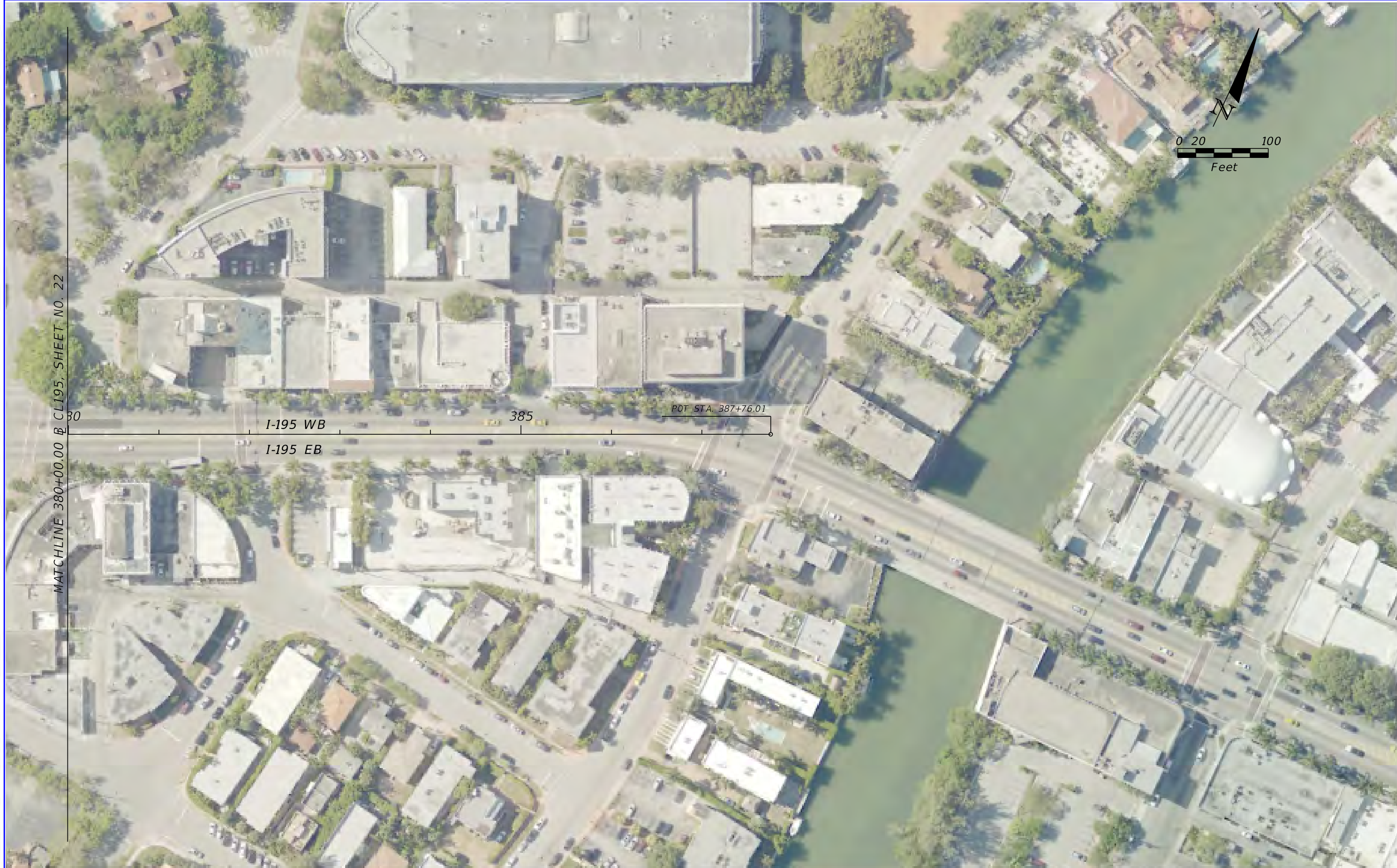
PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 22



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 23



Project Name:



I-95 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

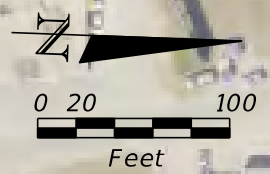
BASE MAPS - I-95 CORRIDOR

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 24



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MATCHLINE 538+00.00 @ CL95, SHEET NO. 26

525

530

535

I-95 SB

I-95 NB

N 2° 08' 49" W

Project Name:



I-95 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

BASE MAPS - I-95 CORRIDOR

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet

No: 25



Project Name:



I-95 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

BASE MAPS - I-95 CORRIDOR

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 26



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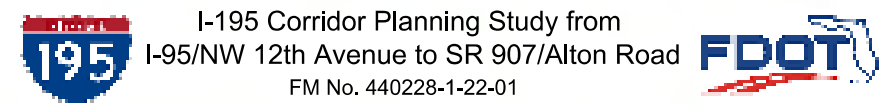


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BASE MAPS - I-95 CORRIDOR

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet No: 27



Project Name:



I-95 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

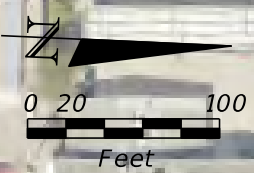
BASE MAPS - I-95 CORRIDOR

Report Name:



EXISTING CONDITIONS REPORT

Date:

Sheet
No: 28



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<p>Project Name:</p>	 <p>I-95 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road FM No. 440228-1-22-01</p> 	<p>Exhibit Name:</p> <p>BASE MAPS - I-95 CORRIDOR</p>	<p>Report Name:</p> <p>EXISTING CONDITIONS REPORT</p>	<p>Date:</p> <hr/> <p>Sheet No: 29</p>
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Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

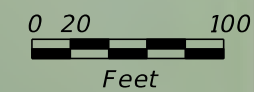
BASE MAPS - SR 907 CORRIDOR

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet No: 30



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

BASE MAPS - SR 907 CORRIDOR

Report Name:

EXISTING CONDITIONS REPORT

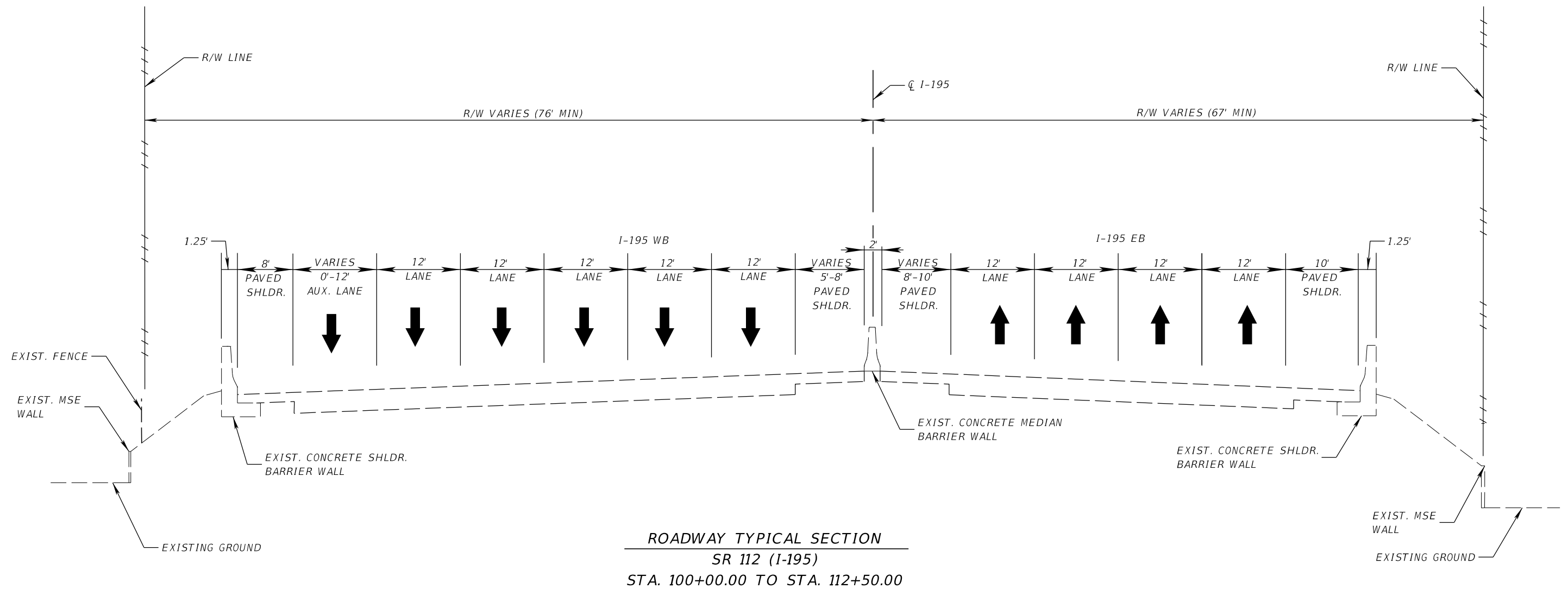
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Sheet

No:

31

TYPICAL SECTION DETAILS



Project Name:



I-195 Corridor Planning Study from
 I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01



Exhibit Name:

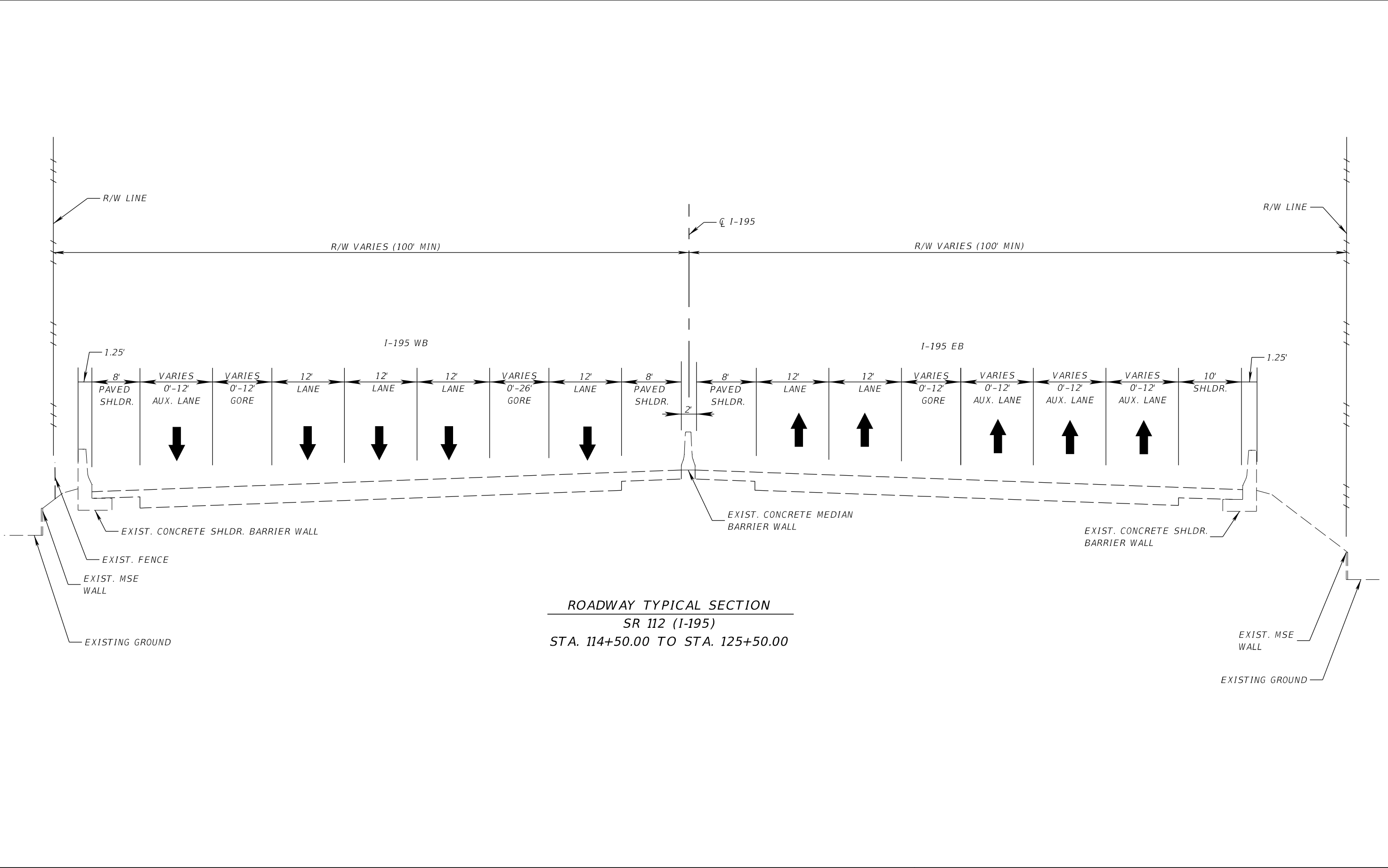
EXISTING TYPICAL SECTION - I-195

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
 No: 1



ROADWAY TYPICAL SECTION
SR 112 (I-195)
STA. 114+50.00 TO STA. 125+50.00

Project Name:



I-195 Corridor Planning Study from
 I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01



Exhibit Name:

EXISTING TYPICAL SECTION - I-195

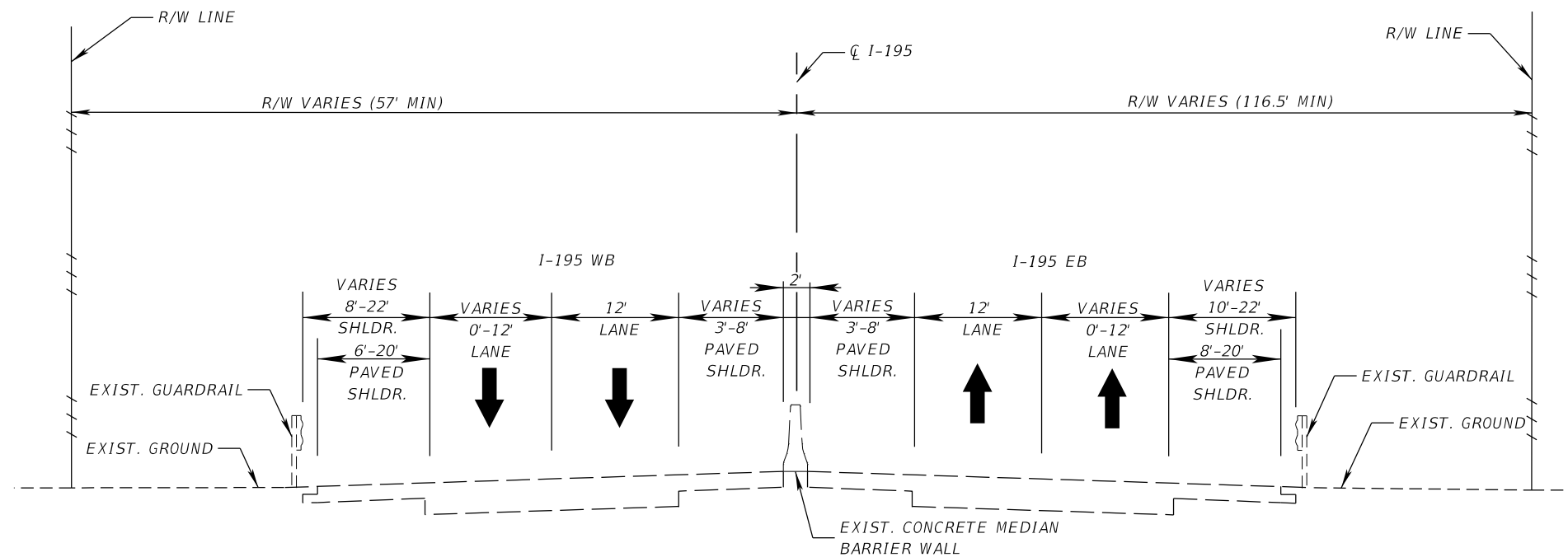
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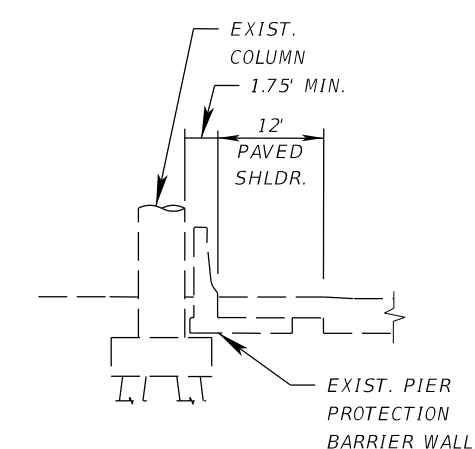
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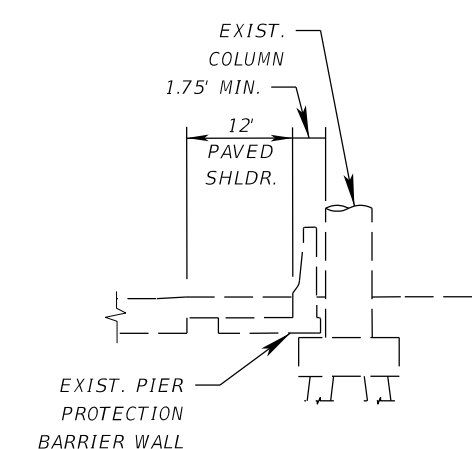
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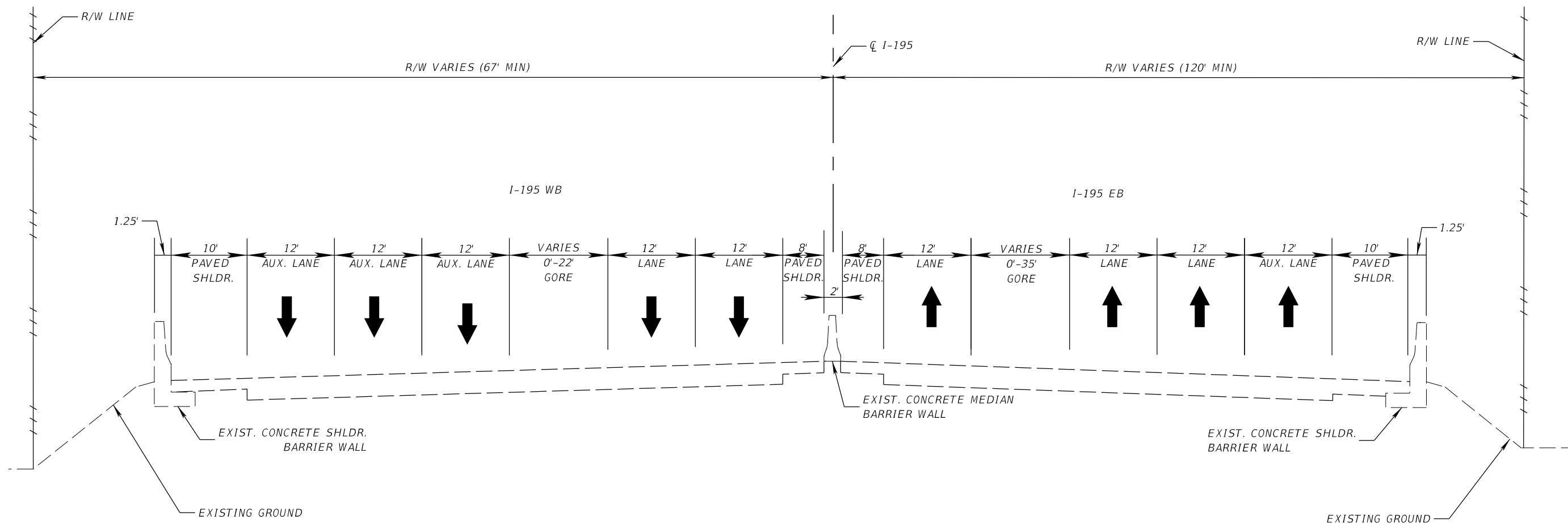
ROADWAY TYPICAL SECTION
SR 112 (I-195)
STA. 127+50.00 TO STA. 160+00.00



STA. 138+50.00 TO STA. 140+50.00
 STA. 142+25.00 TO STA. 146+50.00
 STA. 152+00.00 TO STA. 155+00.00



STA. 136+50.00 TO STA. 137+50.00
 STA. 141+50.00 TO STA. 143+00.00
 STA. 144+75.00 TO STA. 146+00.00
 STA. 150+75.00 TO STA. 152+00.00



ROADWAY TYPICAL SECTION
 SR 112 (I-195)
 STA. 162+50.00 TO STA. 166+50.00

Project Name:



I-195 Corridor Planning Study from
 I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01



Exhibit Name:

EXISTING TYPICAL SECTION - I-195

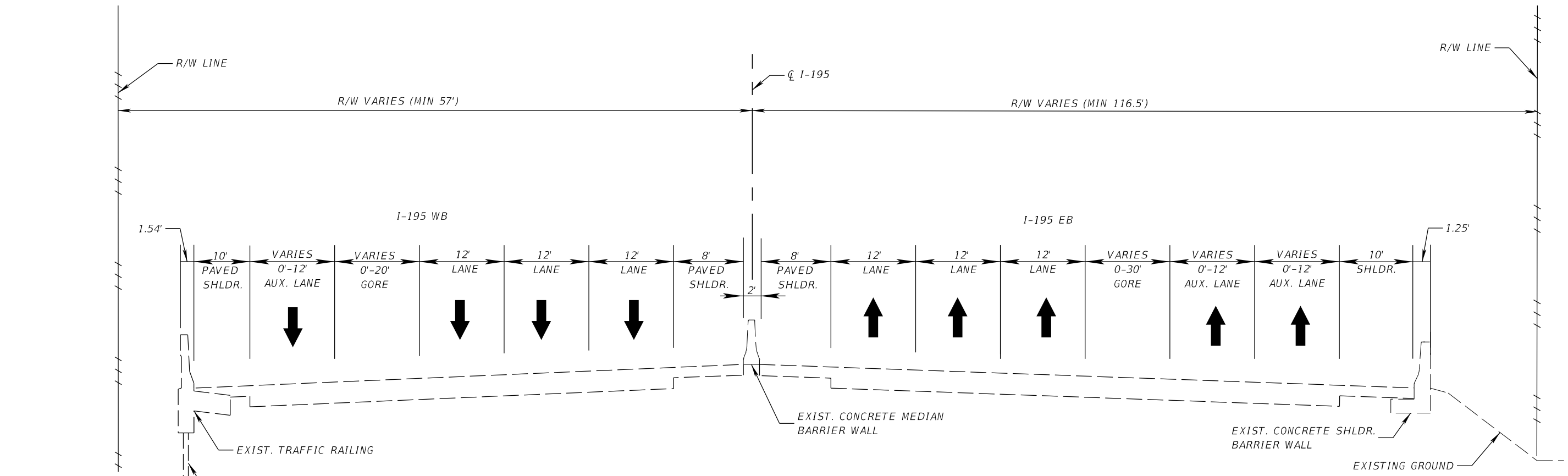
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EXISTING CONDITIONS REPORT

Date:



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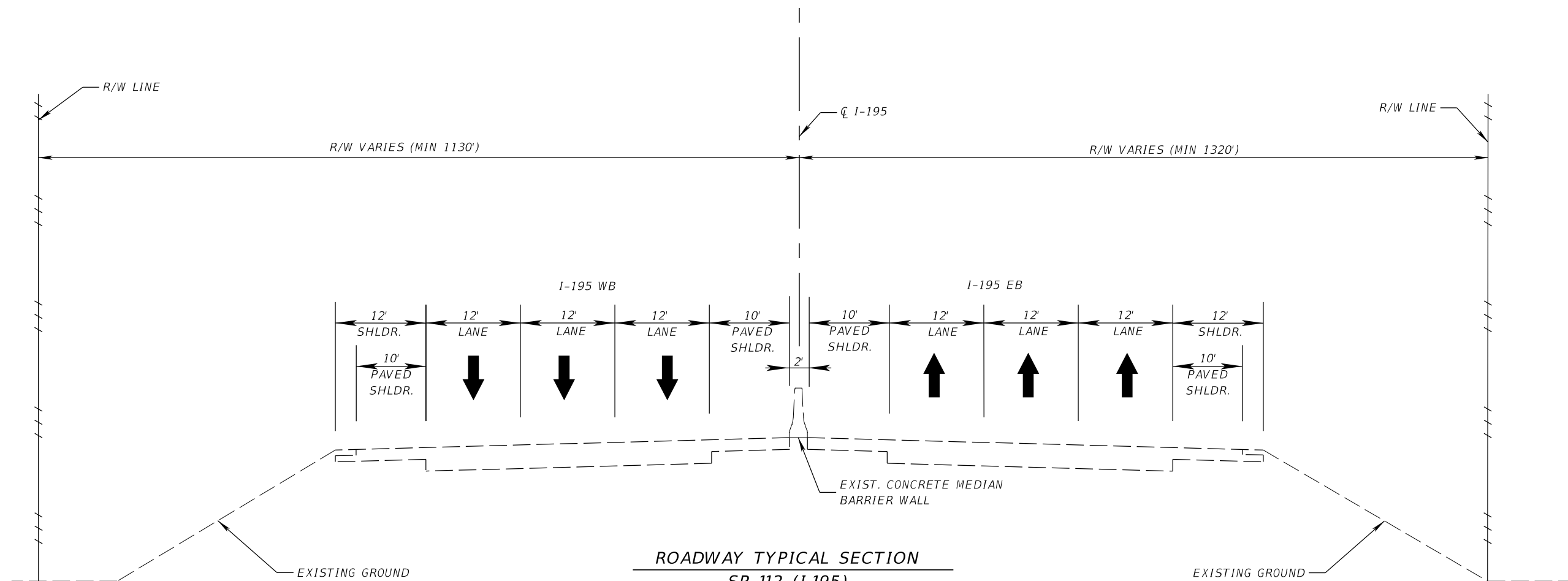
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ROADWAY TYPICAL SECTION
SR 112 (I-195)
 STA. 168+50.00 TO STA. 173+50.00
 STA. 201+50.00 TO STA. 204+00.00

EXIST. FENCE
 EXISTING GROUND
 EXIST. TRAFFIC RAILING
 EXIST. MSE WALL
 EXIST. CONCRETE SHLDR. BARRIER WALL
 EXISTING GROUND
 STA. 175+00.00 TO STA. 180+00.00
 STA. 182+00.00 TO STA. 187+00.00
 STA. 210+00.00 TO STA. 214+75.00

Project Name:	 I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road FM No. 440228-1-22-01 	Exhibit Name: EXISTING TYPICAL SECTION - I-195	Report Name: EXISTING CONDITIONS REPORT	Date: Sheet No: 5
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ROADWAY TYPICAL SECTION
SR 112 (I-195)
 STA. 175+00.00 TO STA. 180+00.00
 STA. 182+00.00 TO STA. 187+00.00
 STA. 204+00.00 TO STA. 232+50.00

Project Name:



I-195 Corridor Planning Study from
 I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01



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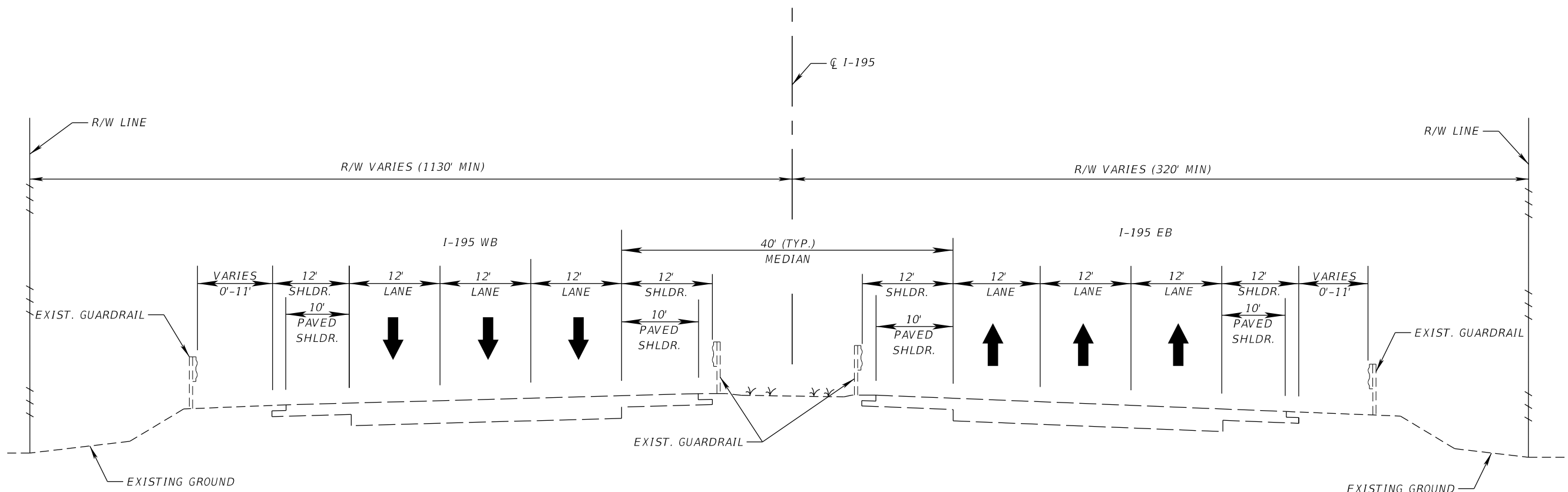
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Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
 No: 6



ROADWAY TYPICAL SECTION
SR 112 (I-195)
STA. 254+50.00 TO STA. 329+00.00

Project Name:



I-195 Corridor Planning Study from
 I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01



Exhibit Name:

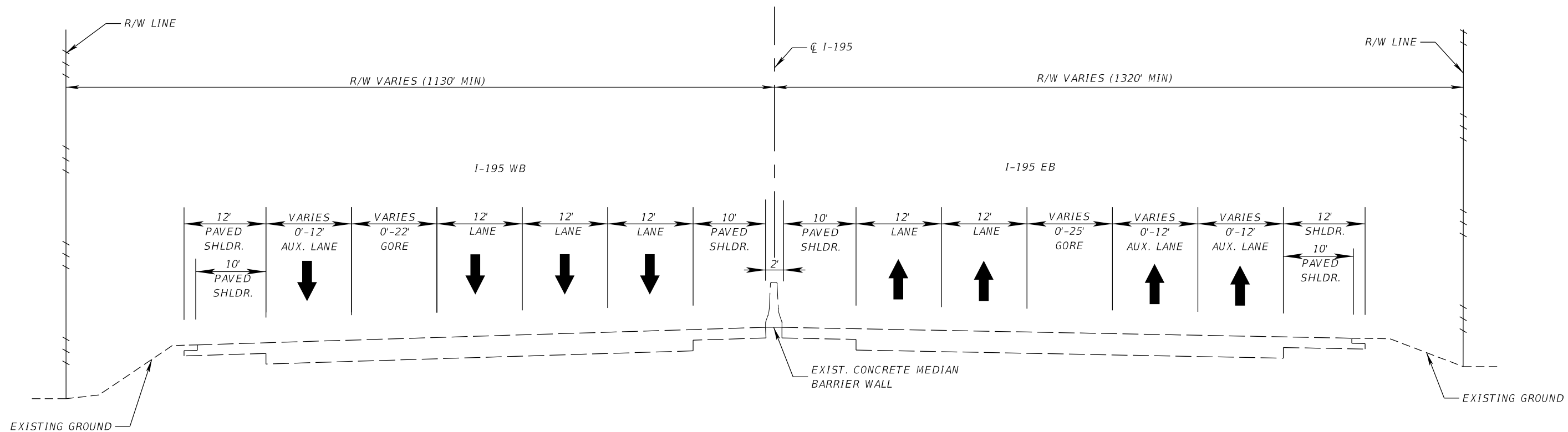
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Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
 No: 7



ROADWAY TYPICAL SECTION
SR 112 (I-195)
 STA. 340+50.00 TO STA. 371+00.00
 STA. 372+00.00 TO STA. 375+00.00

Project Name:



I-195 Corridor Planning Study from
 I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01



Exhibit Name:

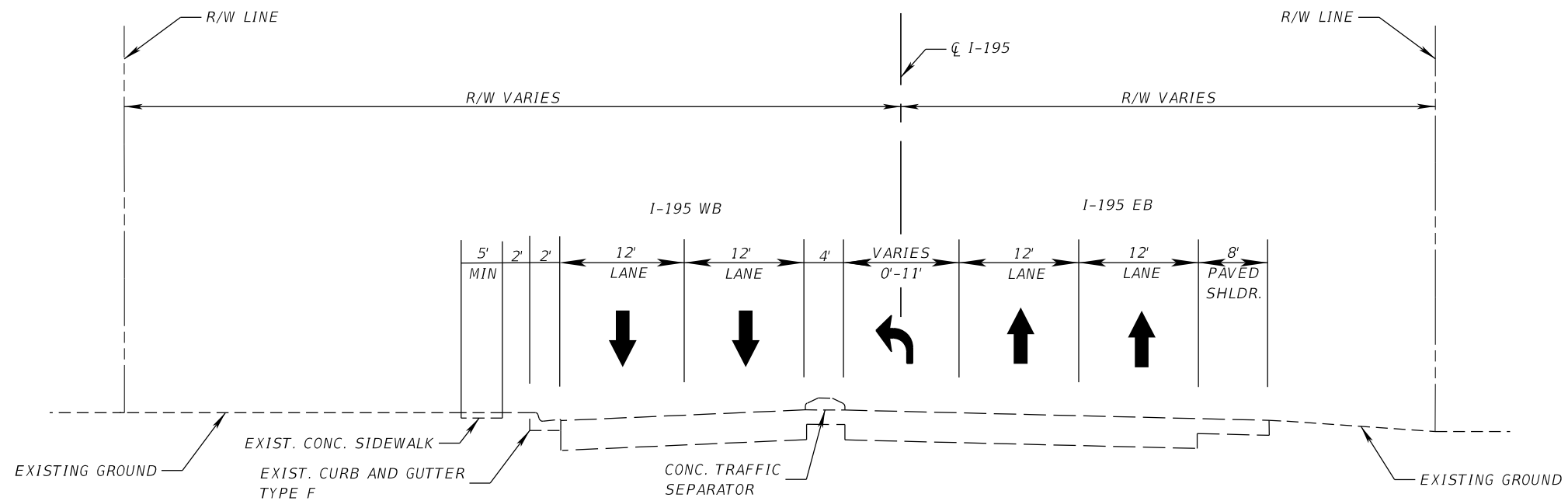
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Report Name:

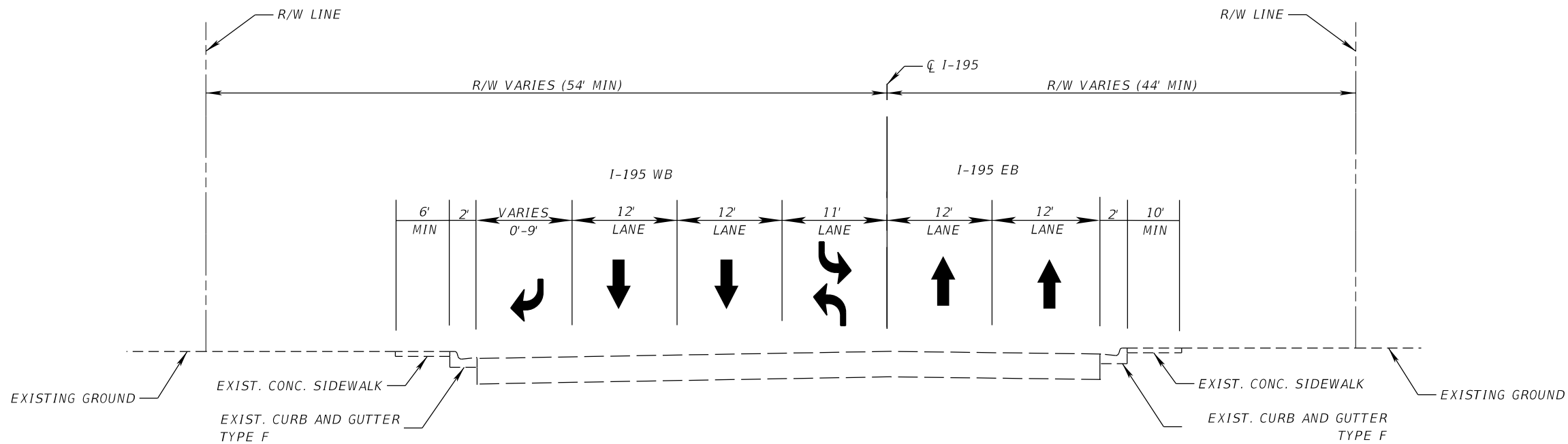
EXISTING CONDITIONS REPORT

Date:

Sheet
 No: 8



ROADWAY TYPICAL SECTION
SR 112 (I-195)
STA. 375+00.00 TO STA. 380+00.00



ROADWAY TYPICAL SECTION
SR 112 (I-195)
STA. 380+00.00 TO STA. 387+00.00

Project Name:



I-195 Corridor Planning Study from
 I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01



Exhibit Name:

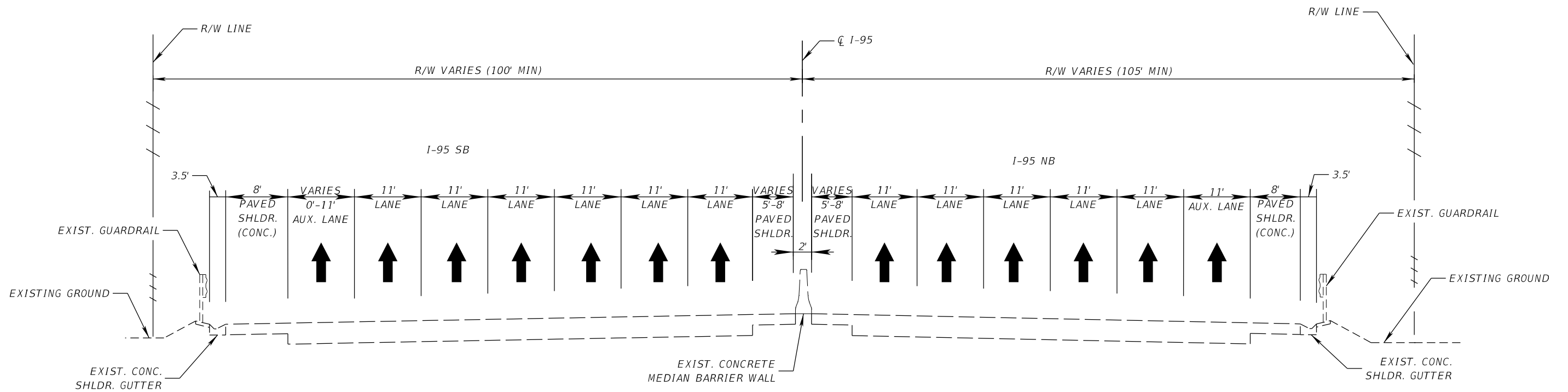
EXISTING TYPICAL SECTION - I-195

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
 No: 9



ROADWAY TYPICAL SECTION
SR 9 (I-95)
STA. 510+00.00 TO STA. 521+50.00

Project Name:



I-195 Corridor Planning Study from
 I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01



Exhibit Name:

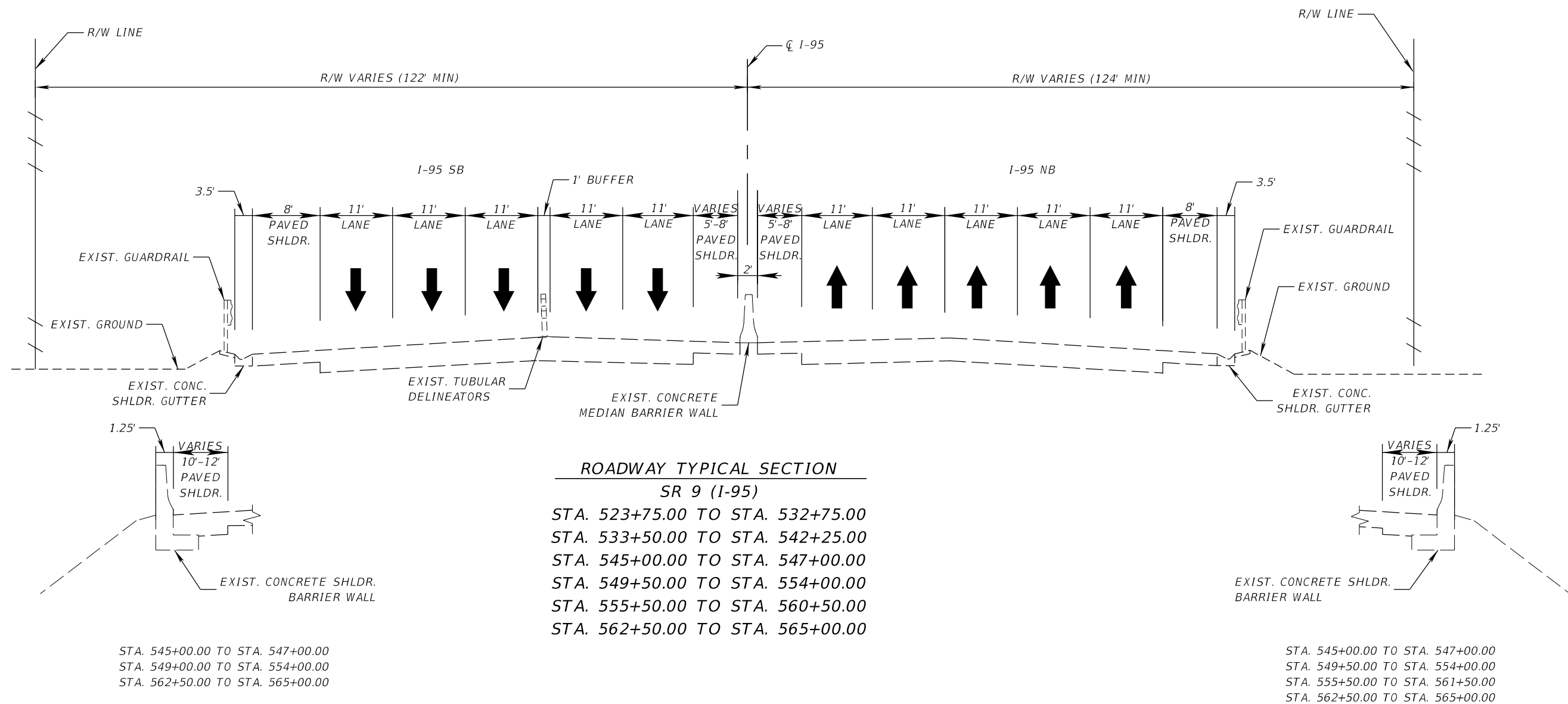
EXISTING TYPICAL SECTION - I-95

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet No: 10



ROADWAY TYPICAL SECTION
SR 9 (I-95)

- STA. 523+75.00 TO STA. 532+75.00
- STA. 533+50.00 TO STA. 542+25.00
- STA. 545+00.00 TO STA. 547+00.00
- STA. 549+50.00 TO STA. 554+00.00
- STA. 555+50.00 TO STA. 560+50.00
- STA. 562+50.00 TO STA. 565+00.00

STA. 545+00.00 TO STA. 547+00.00
 STA. 549+00.00 TO STA. 554+00.00
 STA. 562+50.00 TO STA. 565+00.00

STA. 545+00.00 TO STA. 547+00.00
 STA. 549+50.00 TO STA. 554+00.00
 STA. 555+50.00 TO STA. 561+50.00
 STA. 562+50.00 TO STA. 565+00.00

Project Name:



I-195 Corridor Planning Study from
 I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01



Exhibit Name:

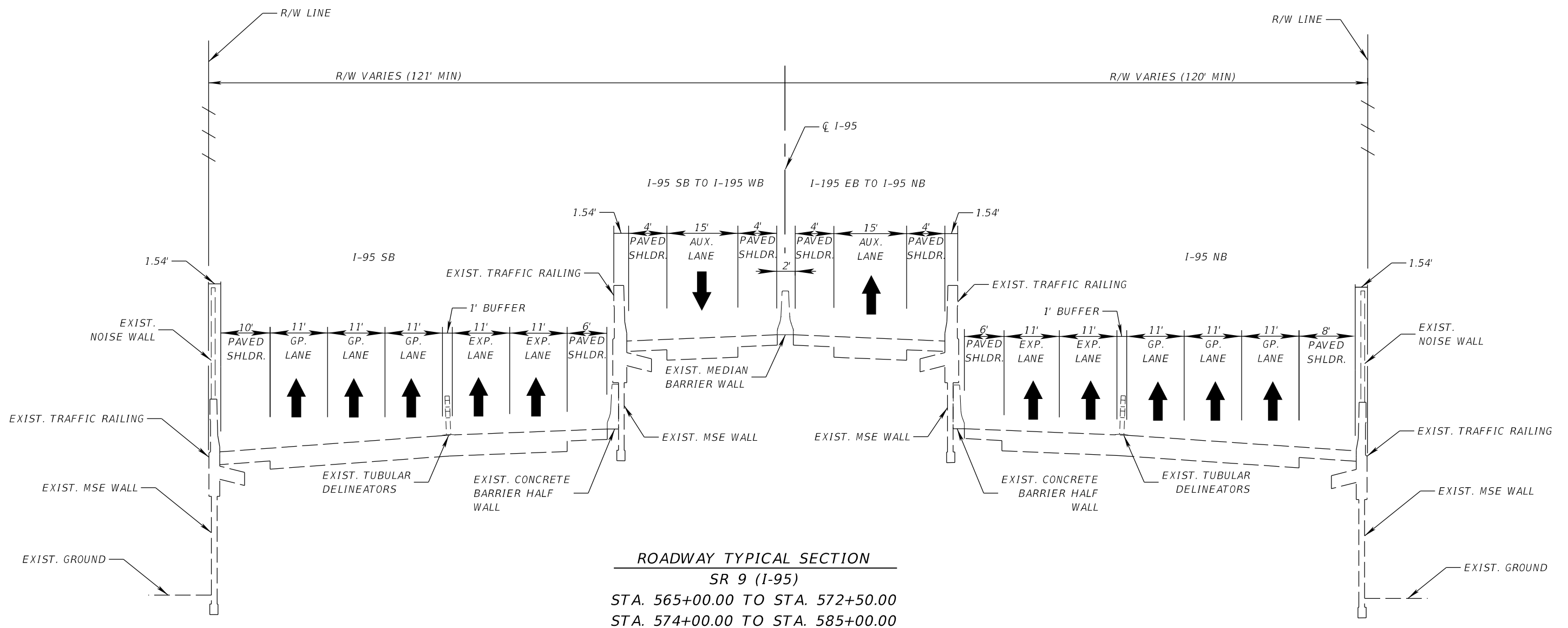
EXISTING TYPICAL SECTION - I-95

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet No: 11



Project Name:



I-195 Corridor Planning Study from
 I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01



Exhibit Name:

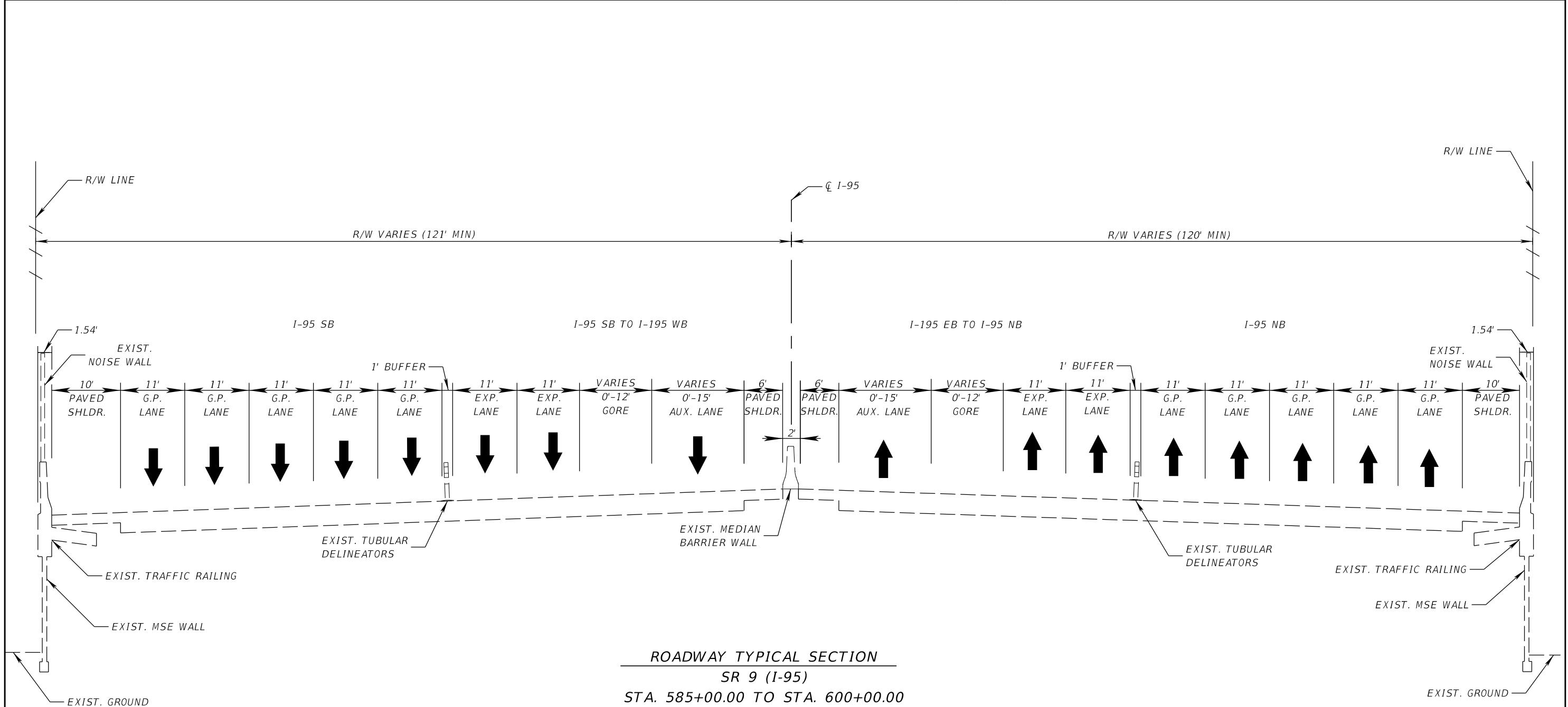
EXISTING TYPICAL SECTION - I-95

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
 No: 12



ROADWAY TYPICAL SECTION
SR 9 (I-95)
STA. 585+00.00 TO STA. 600+00.00

EXISTING ITS EQUIPMENT SUMMARY



I-195 Corridor Planning Study from
 I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01



Existing ITS Equipment Summary

Existing Conditions Analysis

Exhibit No. IT-1
 Page No. IT-1
 Date: 3-6-18



DS-4001W

DMS-57

DS-4001E

POWER SERVICE POINT/METER



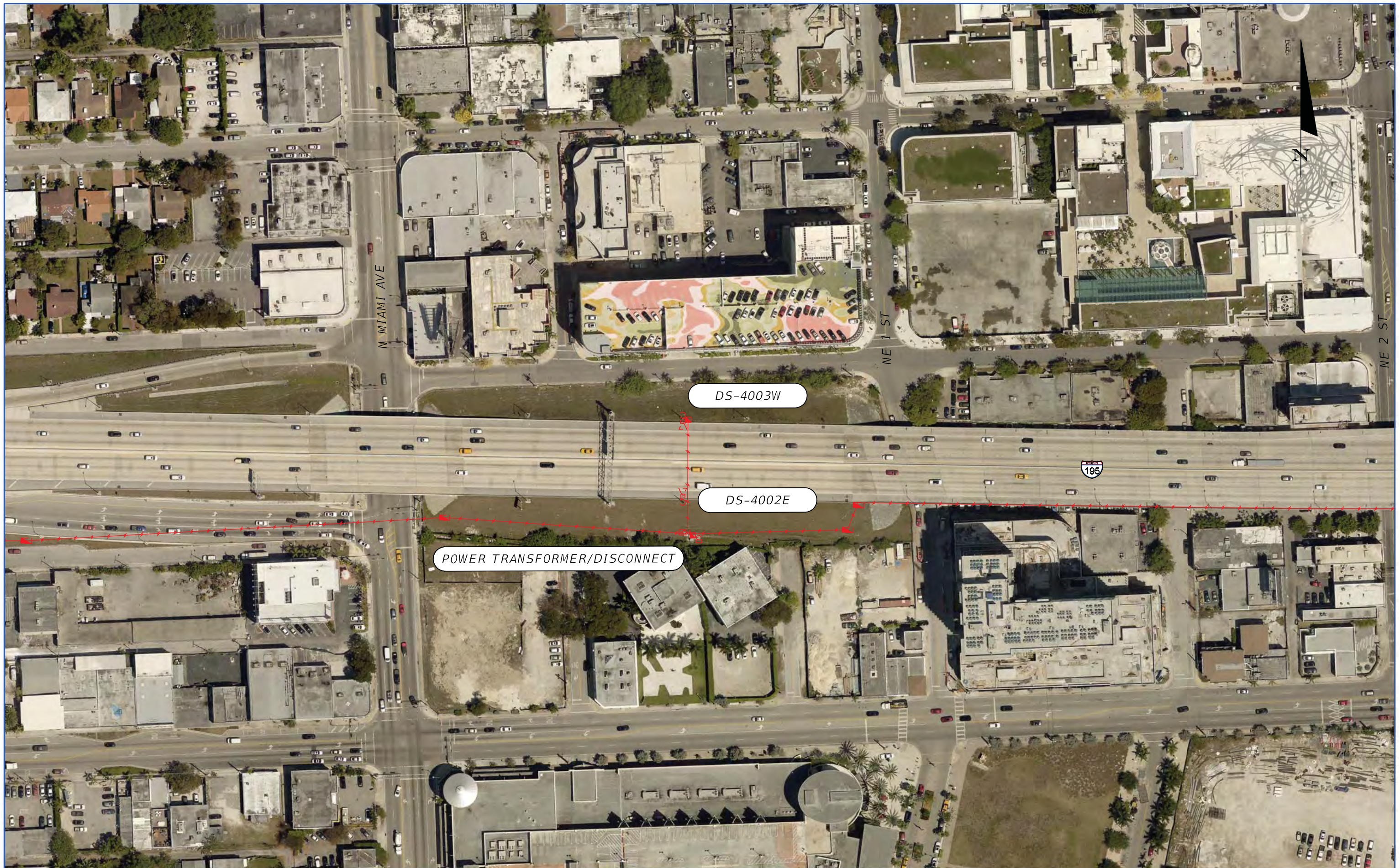
I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01

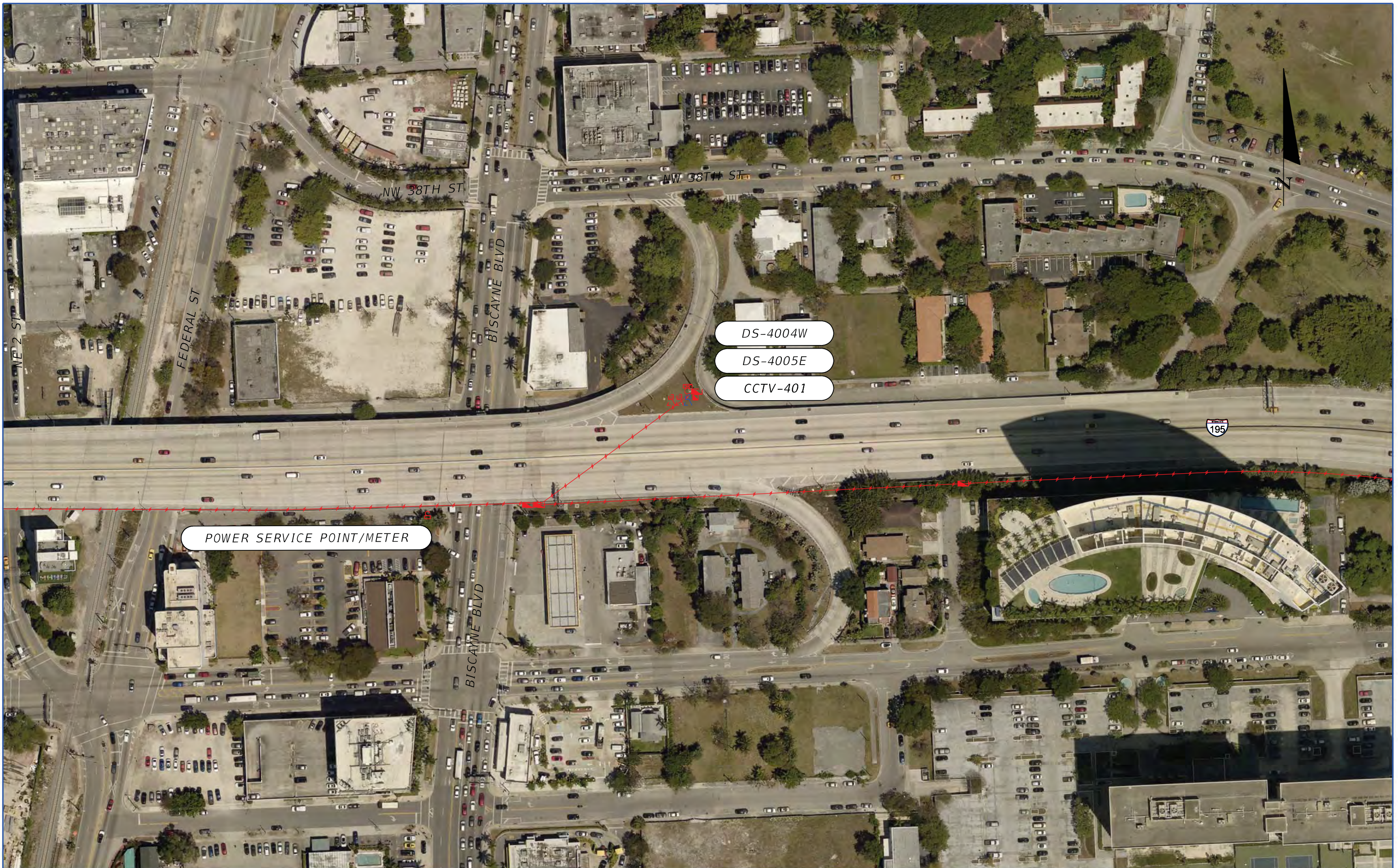


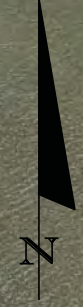
Existing ITS Equipment Summary

Existing Conditions Analysis

Exhibit No. IT-2
Page No. IT-2
Date: 3-6-18







DS-4007W
DS-4008E-ENT
CCTV-402



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



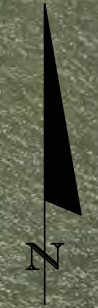
Existing ITS Equipment Summary

Existing Conditions Analysis

Exhibit No.	
Page No.	IT-5
Date:	3-6-18



BISCAYNE BAY



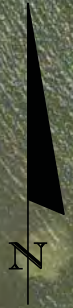




BISCAYNE BAY

DS-4011W

DS-4012E



I-95 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Existing ITS Equipment Summary

Existing Conditions Analysis

Exhibit

No.

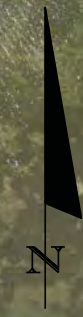
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No. IT-8

Date: 3-6-18



BISCAYNE BAY



POWER TRANSFORMER/DISCONNECT

CCTV-404

DS-4013W

DS-4014E



I-95 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Existing ITS Equipment Summary

Existing Conditions Analysis

Exhibit No.	
Page No.	IT-9
Date:	3-6-18





BISCAYNE BAY



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Existing ITS Equipment Summary

Existing Conditions Analysis

Exhibit No.	
Page No.	IT-11
Date:	3-6-18





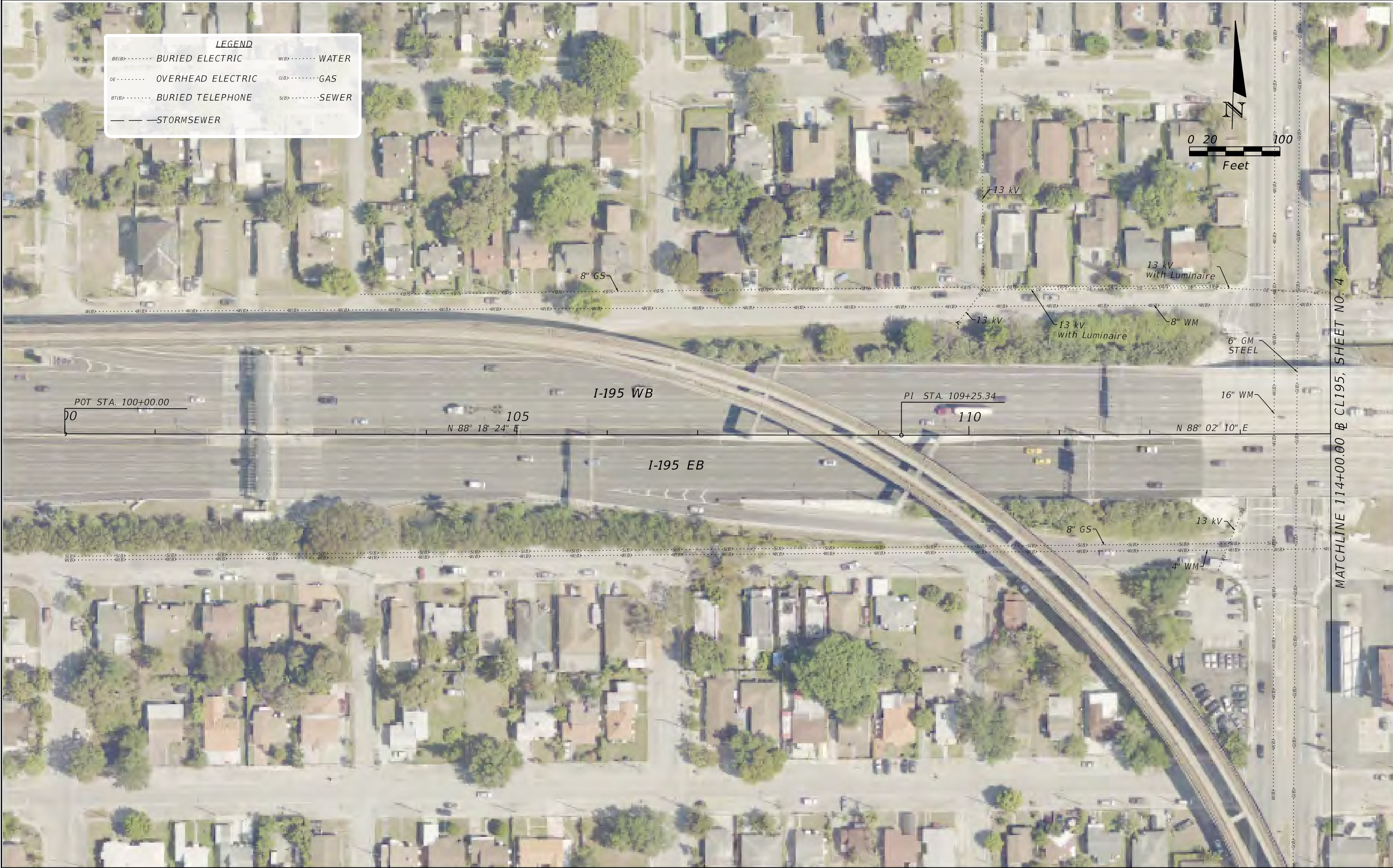






EXISTING MAJOR UTILITIES

LEGEND			
BE(B)-----	BURIED ELECTRIC	WB)-----	WATER
OE-----	OVERHEAD ELECTRIC	G(B)-----	GAS
BT(B)-----	BURIED TELEPHONE	S(B)-----	SEWER
---	STORMSEWER		



MATCHLINE 114+00.00 @ CL195, SHEET NO. 4

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

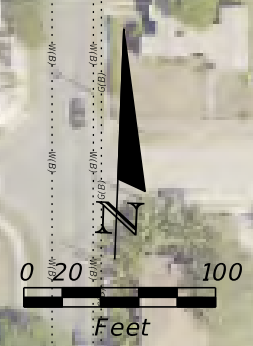
EXISTING CONDITIONS REPORT

Date:

Sheet
No: 1

LEGEND

BE(B)----- BURIED ELECTRIC	WB)----- WATER
OE----- OVERHEAD ELECTRIC	G(B)----- GAS
BT(B)----- BURIED TELEPHONE	S(B)----- SEWER
----- STORMSEWER	



MATCHLINE 114+00.00 @ CL195, SHEET NO. 3

MATCHLINE 128+00.00 @ CL195, SHEET NO. 5





Project Name:  I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01 

Exhibit Name:
PROJECT LAYOUT AND BASE MAPS

Report Name:
EXISTING CONDITIONS REPORT

Date:
 Sheet No: 2

LEGEND

BE(B)----- BURIED ELECTRIC	W(B)----- WATER
OE----- OVERHEAD ELECTRIC	G(B)----- GAS
BT(B)----- BURIED TELEPHONE	S(B)----- SEWER
----- STORMSEWER	

0 20 100
Feet

8" WM
IN 16" CASING

MATCHLINE 128+00.00 @ CL195, SHEET NO. 4

MATCHLINE 142+00.00 @ CL195, SHEET NO. 6

130
N 87° 40' 17" E

I-195 EB

I-195 WB

PC STA. 134+01.92

PCC STA. 138+67.60

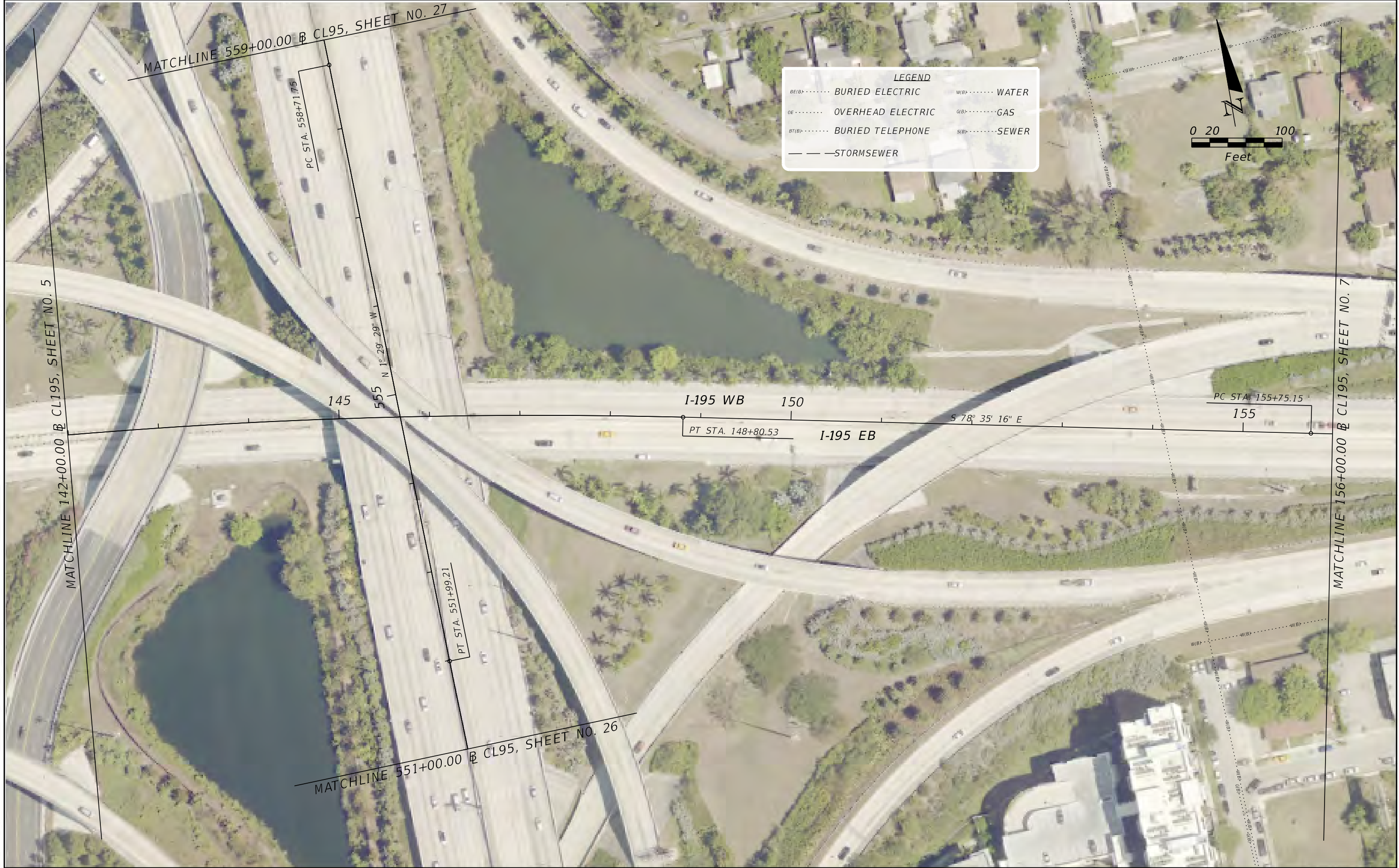
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Project Name: I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01

Exhibit Name: PROJECT LAYOUT AND BASE MAPS

Report Name: EXISTING CONDITIONS REPORT

Date:
 Sheet No: 3



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet
No: 4

LEGEND

- BE(B) BURIED ELECTRIC
- OE OVERHEAD ELECTRIC
- BT(B) BURIED TELEPHONE
- STORMSEWER
- W(B) WATER
- G(B) GAS
- S(B) SEWER

0 20 100
Feet



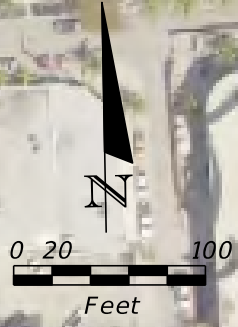
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MATCHLINE 170+00.00 @ CL195, SHEET NO. 8

<p>Project Name:</p> <div style="display: flex; align-items: center;"> <div> <p>I-95 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road FM No. 440228-1-22-01</p> </div> </div>	<p>Exhibit Name:</p> <p style="text-align: center;">PROJECT LAYOUT AND BASE MAPS</p>	<p>Report Name:</p> <p style="text-align: center;">EXISTING CONDITIONS REPORT</p>	<p>Date:</p>
			<p>Sheet No: 5</p>


LEGEND

BE(B)..... BURIED ELECTRIC	WB..... WATER
OE..... OVERHEAD ELECTRIC	GB..... GAS
BT(B)..... BURIED TELEPHONE	SB..... SEWER
— STORMSEWER	

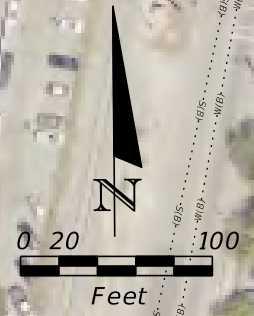


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MATCHLINE 184+00.00 @ CL195, SHEET NO. 9

Project Name:  I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road FM No. 440228-1-22-01	Exhibit Name: PROJECT LAYOUT AND BASE MAPS	Report Name: EXISTING CONDITIONS REPORT	Date:
			Sheet No: 6

LEGEND			
BE(B) ·····	BURIED ELECTRIC	WB ·····	WATER
OE ·····	OVERHEAD ELECTRIC	GB ·····	GAS
BT(B) ·····	BURIED TELEPHONE	SB ·····	SEWER
— ·····	STORMSEWER		



MATCHLINE 184+00.00 B CL195, SHEET NO. 8

MATCHLINE 198+00.00 B CL195, SHEET NO. 10



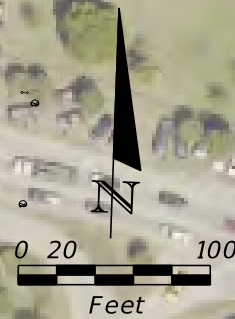
Project Name:  I-95 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road FM No. 440228-1-22-01 

Exhibit Name: PROJECT LAYOUT AND BASE MAPS

Report Name: EXISTING CONDITIONS REPORT


Date: _____

Sheet No: _____



MATCHLINE 198+00.00 B CL195, SHEET NO. 9

MATCHLINE 212+00.00 B CL195, SHEET NO. 11

Project Name:	 I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road FM No. 440228-1-22-01	Exhibit Name: PROJECT LAYOUT AND BASE MAPS	Report Name: EXISTING CONDITIONS REPORT	Date: Sheet No: 8
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Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

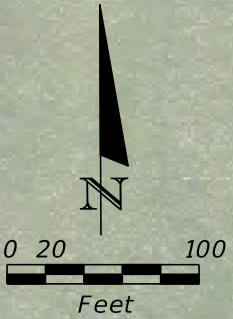
EXISTING CONDITIONS REPORT

Date:

Sheet No: 9

LEGEND

BE(B)..... BURIED ELECTRIC	WB)..... WATER
OE..... OVERHEAD ELECTRIC	GB)..... GAS
BT(B)..... BURIED TELEPHONE	SB)..... SEWER
— — — STORMSEWER	



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

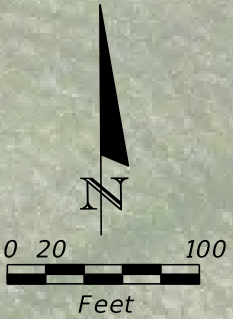
EXISTING CONDITIONS REPORT

Date:

Sheet
No: 10

LEGEND

BE(B)..... BURIED ELECTRIC	W(B)..... WATER
OE..... OVERHEAD ELECTRIC	G(B)..... GAS
BT(B)..... BURIED TELEPHONE	SI(B)..... SEWER
— — — STORMSEWER	



MATCHLINE 240+00.00 B CL195, SHEET NO. 12

MATCHLINE 254+00.00 B CL195, SHEET NO. 14



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

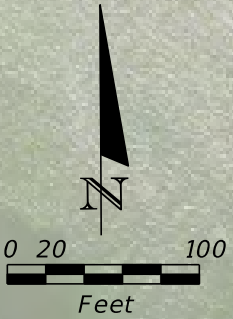
EXISTING CONDITIONS REPORT

Date:

Sheet
No: 11

LEGEND

BE(B)..... BURIED ELECTRIC	WB)..... WATER
OE..... OVERHEAD ELECTRIC	GB)..... GAS
BT(B)..... BURIED TELEPHONE	SB)..... SEWER
— — — STORMSEWER	



MATCHLINE 254+00.00 B CL195, SHEET NO. 13

MATCHLINE 268+00.00 B CL195, SHEET NO. 15



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

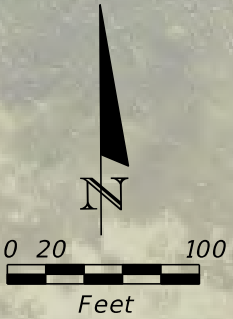
EXISTING CONDITIONS REPORT

Date:

Sheet No: 12

LEGEND

BE(B) - - - - - BURIED ELECTRIC	W(B) - - - - - WATER
OE - - - - - OVERHEAD ELECTRIC	G(B) - - - - - GAS
BT(B) - - - - - BURIED TELEPHONE	S(B) - - - - - SEWER
- - - - - STORMSEWER	



MATCHLINE 268+00.00 B CL195, SHEET NO. 14

MATCHLINE 282+00.00 B CL195, SHEET NO. 16



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

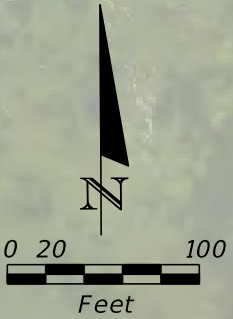
EXISTING CONDITIONS REPORT

Date:

Sheet No: 13

LEGEND

BE(B).....	BURIED ELECTRIC	W(B).....	WATER
OE.....	OVERHEAD ELECTRIC	G(B).....	GAS
BT(B).....	BURIED TELEPHONE	SI(B).....	SEWER
---	STORMSEWER		



MATCHLINE 282+00.00 @ CL195, SHEET NO. 15

MATCHLINE 296+00.00 @ CL195, SHEET NO. 17

I-195 WB

285

290

295

I-195 EB

36" WM

36" WM

36" WM

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

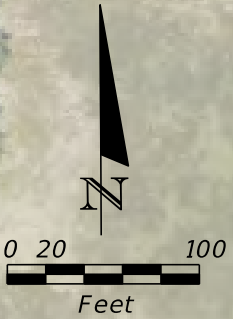
EXISTING CONDITIONS REPORT

Date:

Sheet No: 14

LEGEND

BE(B) ····· BURIED ELECTRIC	WB ····· WATER
OE ····· OVERHEAD ELECTRIC	GB ····· GAS
BT(B) ····· BURIED TELEPHONE	SB ····· SEWER
— — — STORMSEWER	



MATCHLINE 296+00.00 B CL195, SHEET NO. 16

MATCHLINE 310+00.00 B CL195, SHEET NO. 18

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

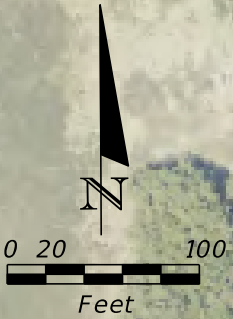
EXISTING CONDITIONS REPORT

Date:

Sheet No: 15

LEGEND

BE(B)----- BURIED ELECTRIC	WB(B)----- WATER
OE----- OVERHEAD ELECTRIC	GB(B)----- GAS
BT(B)----- BURIED TELEPHONE	SB(B)----- SEWER
--- STORMSEWER	



MATCHLINE 310+00.00 B CL195, SHEET NO. 17

MATCHLINE 324+00.00 B CL195, SHEET NO. 19



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

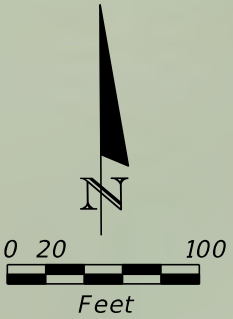
EXISTING CONDITIONS REPORT

Date:

Sheet No: 16

LEGEND

BE(B)..... BURIED ELECTRIC	W(B)..... WATER
OE..... OVERHEAD ELECTRIC	G(B)..... GAS
BT(B)..... BURIED TELEPHONE	SI(B)..... SEWER
— — — STORMSEWER	



MATCHLINE 324+00.00 B CL195, SHEET NO. 18

MATCHLINE 338+00.00 B CL195, SHEET NO. 20

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

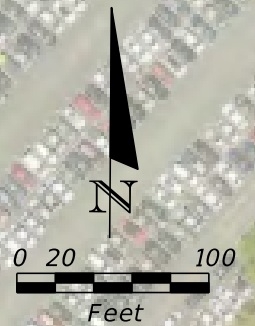
Date:

Sheet No: 17





LEGEND

BE(B).....	BURIED ELECTRIC	WB.....	WATER
OE.....	OVERHEAD ELECTRIC	GB.....	GAS
BT(B).....	BURIED TELEPHONE	SB.....	SEWER
---	STORMSEWER		



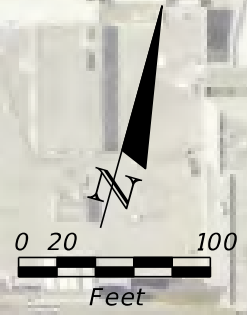
MATCHLINE 338+00.00 @ CL195, SHEET NO. 19

MATCHLINE 352+00.00 @ CL195, SHEET NO. 21

Project Name:  I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road FM No. 440228-1-22-01		Exhibit Name: PROJECT LAYOUT AND BASE MAPS	Report Name: EXISTING CONDITIONS REPORT	Date:
				Sheet No: 18

LEGEND

BE(B)-..... BURIED ELECTRIC	WB(B)-..... WATER
OE..... OVERHEAD ELECTRIC	GB(B)-..... GAS
BT(B)-..... BURIED TELEPHONE	SB(B)-..... SEWER
----- STORMSEWER	



MATCHLINE 352+00.00 @ CL195, SHEET NO. 20

MATCHLINE 366+00.00 @ CL195, SHEET NO. 22



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

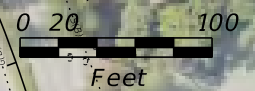
Date:

Sheet No: 19

MATCHLINE 48+00.00 @SR907, SHEET NO. 31

LEGEND

BE(B)..... BURIED ELECTRIC	W(B)..... WATER
OE..... OVERHEAD ELECTRIC	G(B)..... GAS
BT(B)..... BURIED TELEPHONE	SB..... SEWER
— STORMSEWER	



MATCHLINE 366+00.00 @ CL195, SHEET NO. 21

MATCHLINE 380+00.00 @ CL195, SHEET NO. 23



MATCHLINE 39+00.00 @SR907, SHEET NO. 30

Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

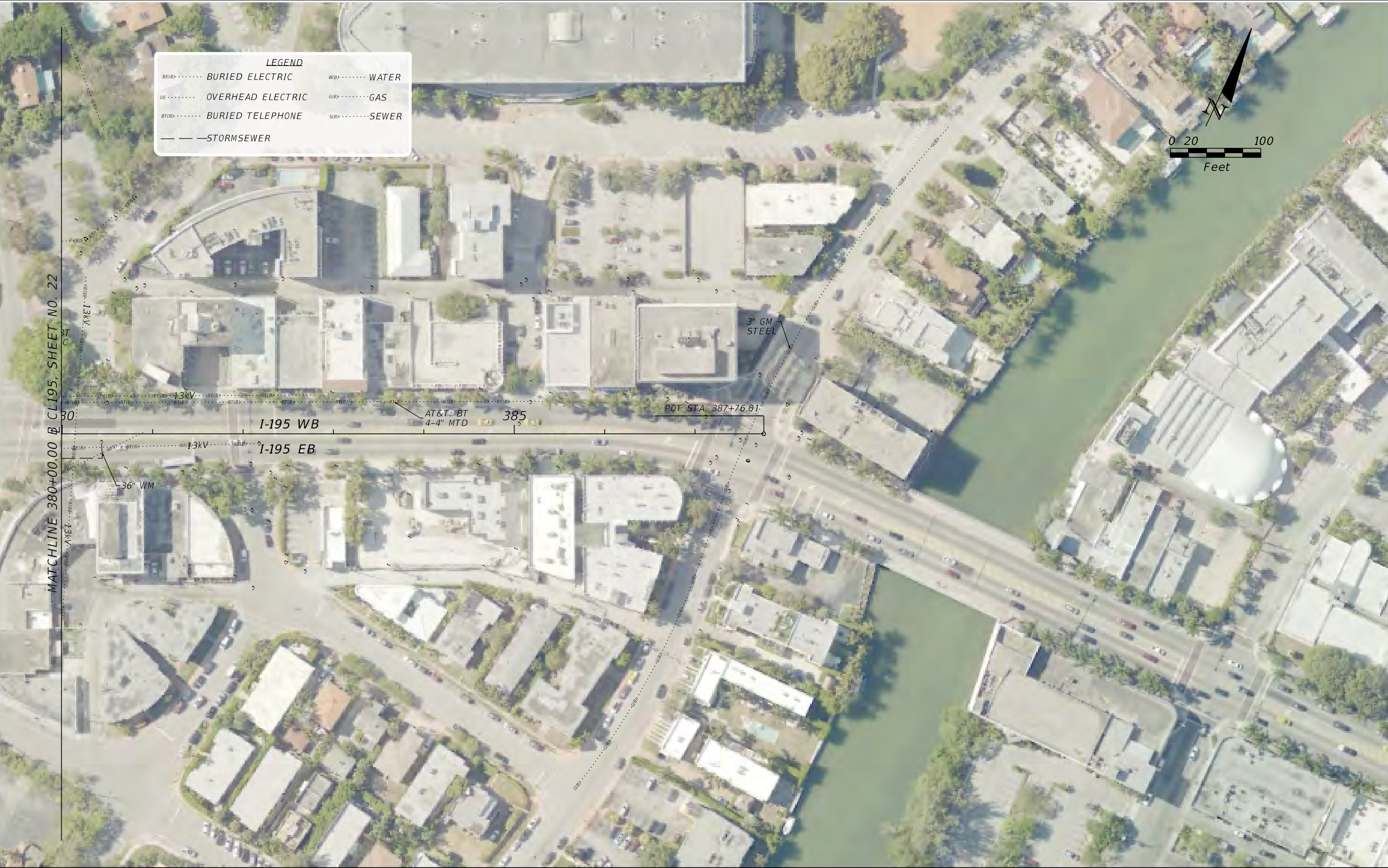
Date:

Sheet
No: 20

LEGEND

BE(B).....	BURIED ELECTRIC	WB).....	WATER
OE.....	OVERHEAD ELECTRIC	GB).....	GAS
BT(B).....	BURIED TELEPHONE	SB).....	SEWER
---	STORMSEWER		

0 20 100
Feet



Project Name: I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01

Exhibit Name: PROJECT LAYOUT AND BASE MAPS

Report Name: EXISTING CONDITIONS REPORT

Date:
 Sheet No: 21

LEGEND

BE(B).....	BURIED ELECTRIC	W(B).....	WATER
OE.....	OVERHEAD ELECTRIC	G(B).....	GAS
BT(B).....	BURIED TELEPHONE	SB(B).....	SEWER
---	STORMSEWER		



Project Name:

Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet No: 22

LEGEND

BE(B)..... BURIED ELECTRIC	WB)..... WATER
OE..... OVERHEAD ELECTRIC	GB)..... GAS
BT(B)..... BURIED TELEPHONE	SB)..... SEWER
— STORMSEWER	



Project Name:



I-95 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet No: 23



LEGEND			
BE(B).....	BURIED ELECTRIC	W(B).....	WATER
OE.....	OVERHEAD ELECTRIC	G(B).....	GAS
BT(B).....	BURIED TELEPHONE	S(B).....	SEWER
---	STORMSEWER		

MATCHLINE 538+00.00 @ CL95, SHEET NO. 25

MATCHLINE 551+00.00 @ CL95, SHEET NO. 6



Project Name:  I-95 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road
 FM No. 440228-1-22-01 

Exhibit Name:
PROJECT LAYOUT AND BASE MAPS

Report Name:
EXISTING CONDITIONS REPORT

Date:
 Sheet No: 24



LEGEND			
BE(B)-	BURIED ELECTRIC	W(B)-	WATER
OE-	OVERHEAD ELECTRIC	G(B)-	GAS
BT(B)-	BURIED TELEPHONE	S(B)-	SEWER
---	STORMSEWER		

MATCHLINE 559+00.00 @ CL95, SHEET NO. 6

MATCHLINE 572+00.00 @ CL95, SHEET NO. 28



Project Name:



I-95 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:

Sheet

No: 25



LEGEND			
BE(B).....	BURIED ELECTRIC	W(B).....	WATER
OE.....	OVERHEAD ELECTRIC	G(B).....	GAS
BT(B).....	BURIED TELEPHONE	S(B).....	SEWER
---	STORMSEWER		



Project Name:



I-95 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

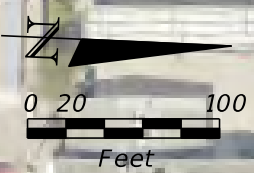
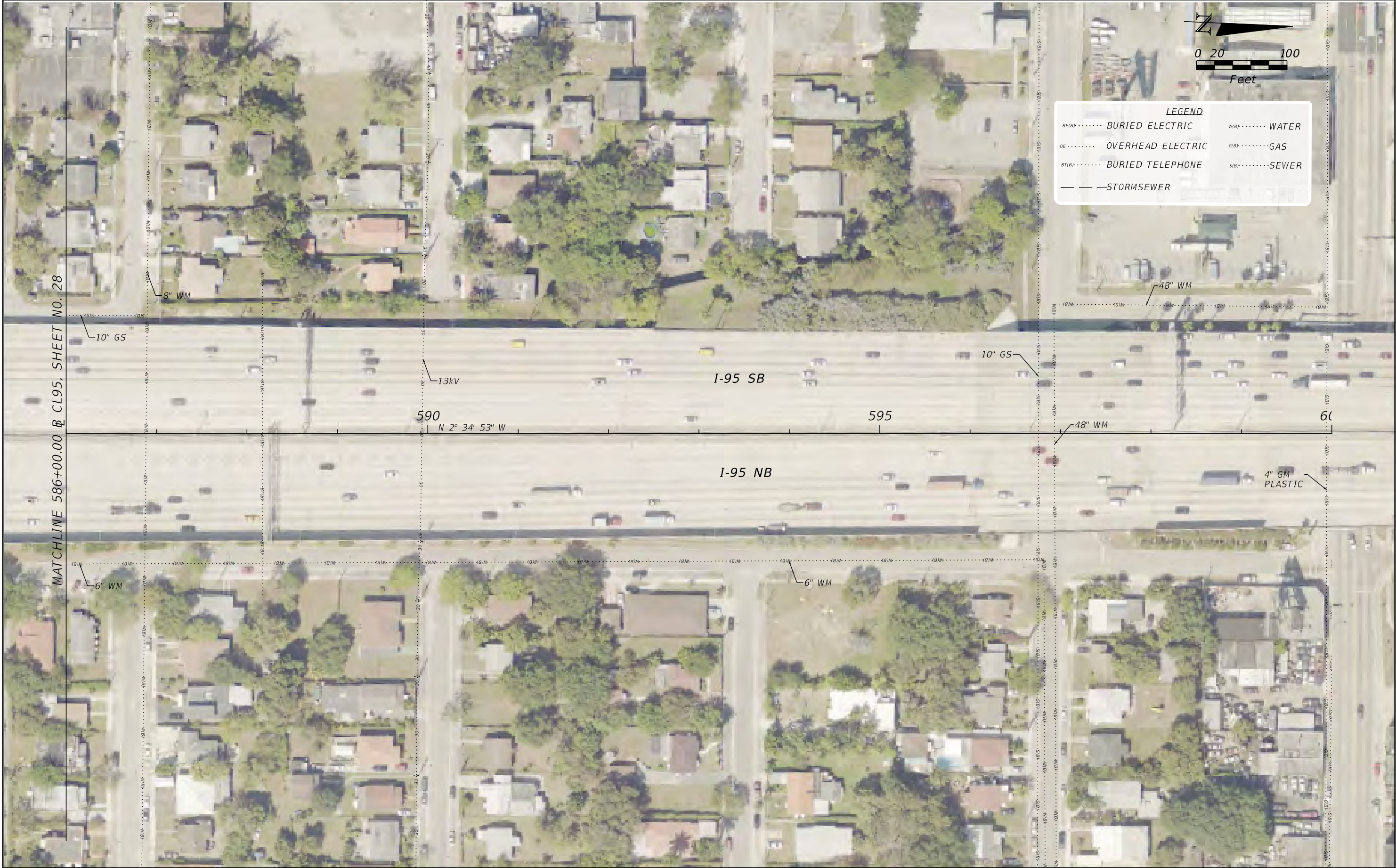
PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT


Date:

Sheet No: 26



LEGEND			
BE(B).....	BURIED ELECTRIC	WB).....	WATER
OE.....	OVERHEAD ELECTRIC	GB).....	GAS
BT(B).....	BURIED TELEPHONE	SB).....	SEWER
---	STORMSEWER		

MATCHLINE 586+00.00 B CL95, SHEET NO. 28

Project Name:  I-95 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road FM No. 440228-1-22-01	Exhibit Name: PROJECT LAYOUT AND BASE MAPS	Report Name: EXISTING CONDITIONS REPORT	Date:
			Sheet No: 27

LEGEND

BE(B)..... BURIED ELECTRIC	W(B)..... WATER
OE..... OVERHEAD ELECTRIC	G(B)..... GAS
BT(B)..... BURIED TELEPHONE	S(B)..... SEWER
— — — — — STORMSEWER	

0 20 100
Feet



Project Name:



I-195 Corridor Planning Study from
I-95/NW 12th Avenue to SR 907/Alton Road
FM No. 440228-1-22-01



Exhibit Name:

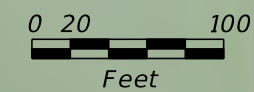
PROJECT LAYOUT AND BASE MAPS

Report Name:

EXISTING CONDITIONS REPORT

Date:


Sheet
No: 28



LEGEND			
BE(B).....	BURIED ELECTRIC	W(B).....	WATER
OE.....	OVERHEAD ELECTRIC	G(B).....	GAS
BT(B).....	BURIED TELEPHONE	S(B).....	SEWER
---	STORMSEWER		



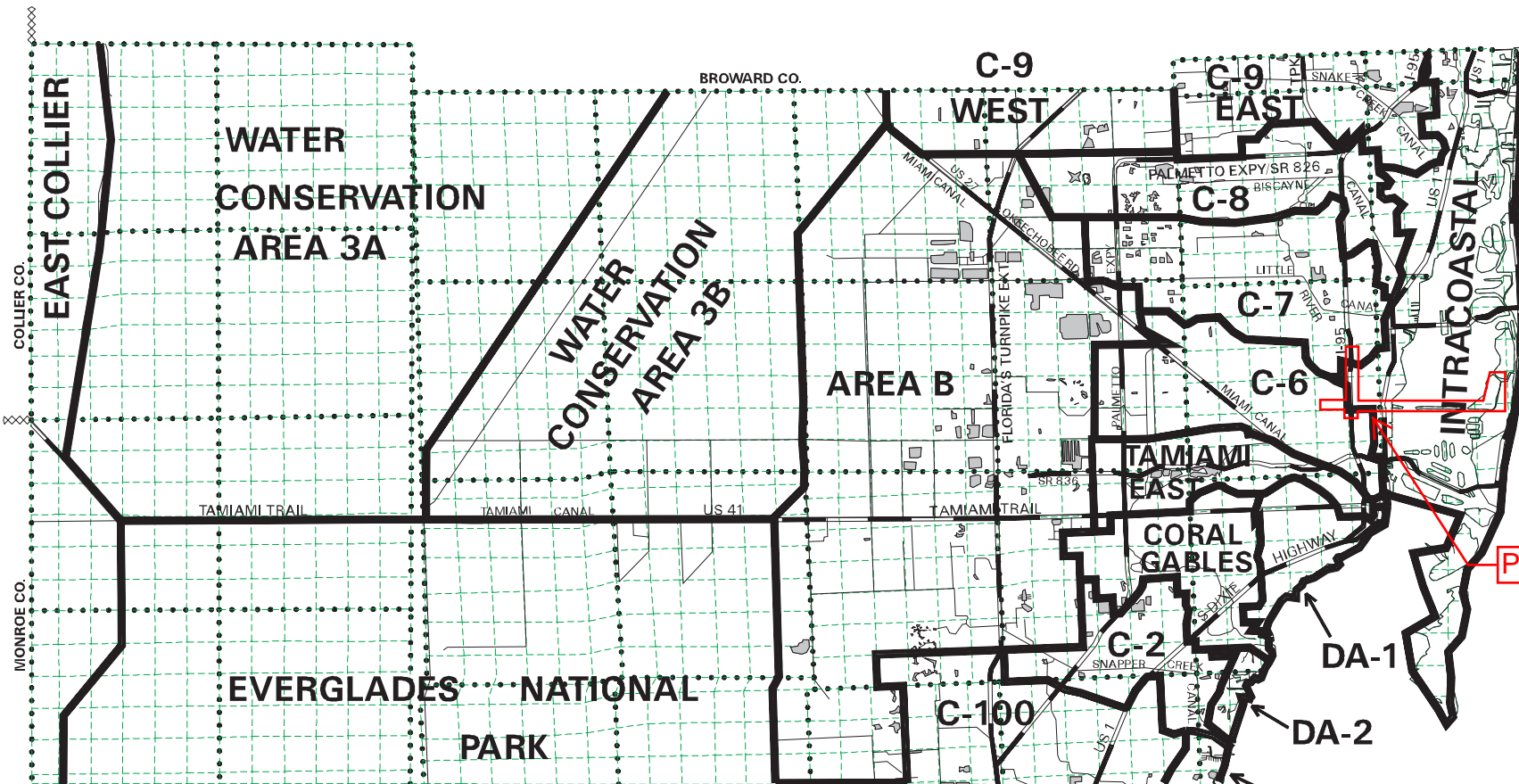
MATCHLINE 48+00.00 @ SR907, SHEET NO. 22

Project Name:  I-195 Corridor Planning Study from I-95/NW 12th Avenue to SR 907/Alton Road FM No. 440228-1-22-01	Exhibit Name: PROJECT LAYOUT AND BASE MAPS	Report Name: EXISTING CONDITIONS REPORT	Date:
			Sheet No: 29

SWFMD DRAINAGE MAP

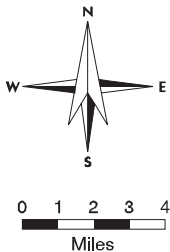
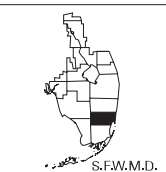
R. 35 E. | R. 36 E. | R. 37 E. | R. 38 E. | R. 39 E. | R. 40 E. | R. 41 E. | R. 42 E.

T. 52 S.
T. 53 S.
T. 54 S.
T. 55 S.

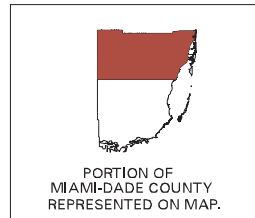


Project Limits

Figure B-11



DRAINAGE BASINS for NORTHERN MIAMI-DADE COUNTY, FL.



FEMA MAP FIRMETTES

NOTES TO USERS

This map is for use in administering the National Flood Insurance Program. It does not necessarily identify all areas subject to flooding, particularly from local drainage sources of small size. The community map repository should be consulted for possible updated or additional flood hazard information.

To obtain more detailed information in areas where **Base Flood Elevations (BFEs)** and/or **floodways** have been determined, users are encouraged to consult the Flood Profiles and Floodway Data and/or Summary of Stillwater Elevations tables contained within the Flood Insurance Study (FIS) report that accompanies this FIRM. Users should be aware that BFEs shown on the FIRM represent rounded whole-foot elevations. These BFEs are intended for flood insurance rating purposes only and should not be used as the sole source of flood elevation information. Accordingly, flood elevation data presented in the FIS report should be utilized in conjunction with the FIRM for purposes of construction and/or floodplain management.

Coastal Base Flood Elevations shown on this map apply only landward of 0.0' National Geodetic Vertical Datum of 1929 (NGVD 29). Users of this FIRM should be aware that coastal flood elevations are also provided in the Summary of Stillwater Elevations tables in the Flood Insurance Study report for this jurisdiction. Elevations shown in the Summary of Stillwater Elevations tables should be used for construction and/or floodplain management purposes when they are higher than the elevations shown on this FIRM.

Boundaries of the **floodways** were computed at cross sections and interpolated between cross sections. The floodways were based on hydraulic considerations with regard to requirements of the National Flood Insurance Program. Floodway widths and other pertinent floodway data are provided in the Flood Insurance Study report for this jurisdiction.

Certain areas not in Special Flood Hazard Areas may be protected by **flood control structures**. Refer to Section 2.4 "Flood Control Structures" of the Flood Insurance Study report for information on flood control structures for this jurisdiction.

The **projection** used in the preparation of this map was Florida State Plane east zone (FIPSZONE 0001). The horizontal datum was NAD 83, GRS80 spheroid. Differences in datum, projection or State Plane zones used in the production of FIRMs for adjacent jurisdictions may result in slight positional differences in map features across jurisdiction boundaries. These differences do not affect the accuracy of this FIRM.

Flood elevations on this map are referenced to the National Geodetic Vertical Datum of 1929. These flood elevations must be compared to structure and ground elevations referenced to the same vertical datum. For information regarding conversion between the National Geodetic Vertical Datum of 1929 and the North American Vertical Datum of 1988, visit the National Geodetic Survey website at <http://www.ngs.noaa.gov> or contact the National Geodetic Survey at the following address:

NGS Information Services
NOAA, NGS12
National Geodetic Survey
SSM-C-3, #9202
1315 East-West Highway
Silver Spring, Maryland 20910-3282
(301) 713-3242

To obtain current elevation, description, and/or location information for **bench marks** shown on this map, please contact the Information Services Branch of the National Geodetic Survey at (301) 713-3242, or visit its website at <http://www.ngs.noaa.gov>.

Base map information shown on this FIRM was provided in digital format by the Miami-Dade County Information Technology Department. These data were compiled at a scale of 1:3,600 from digital orthophotography dated 2001. Additional base map information was provided by the Cities of Aventura, Coral Gables, and Homestead, the Town of Cutler Bay, and Miami-Dade County.

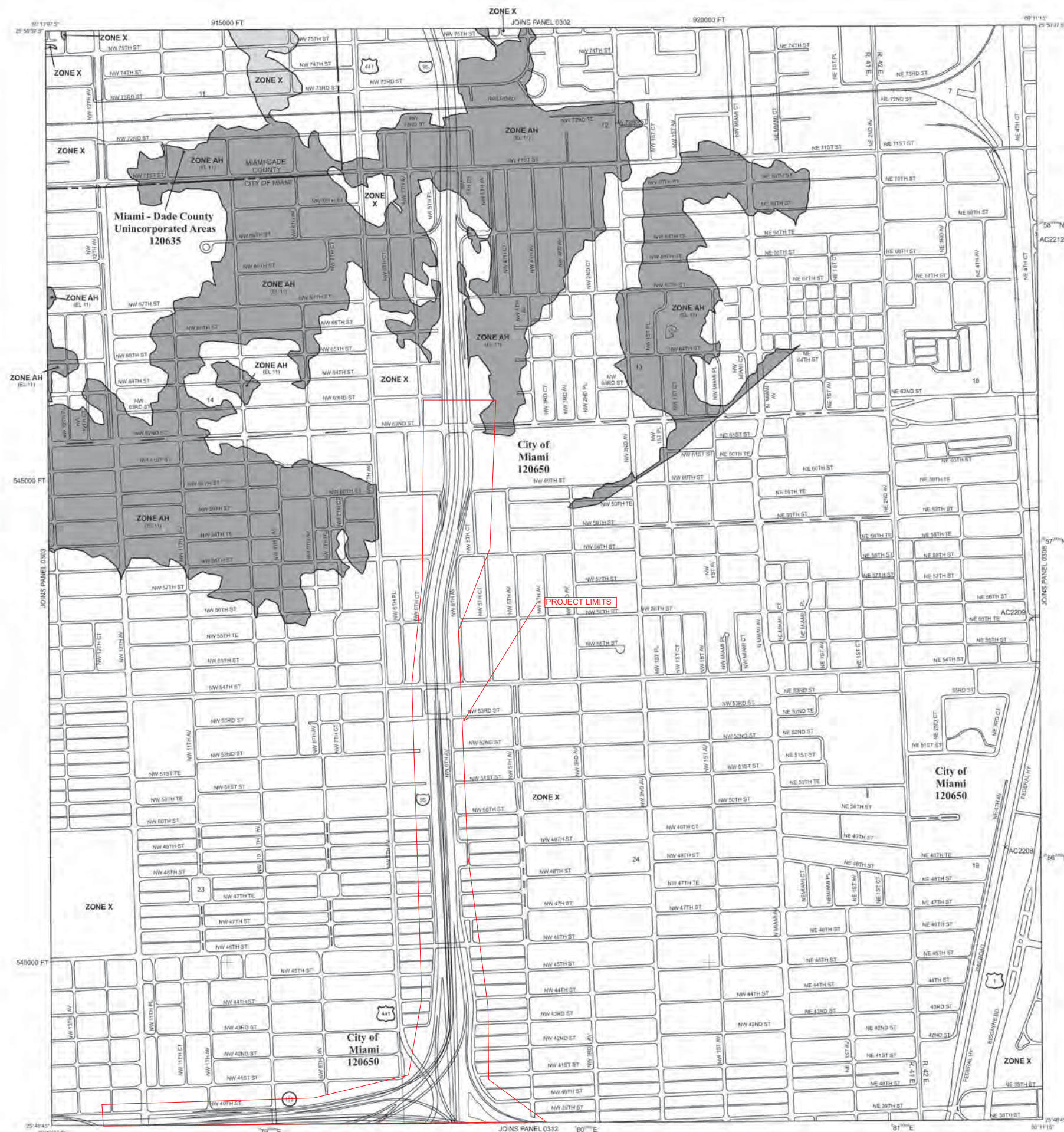
This map reflects more detailed and up-to-date **stream channel configurations** than those shown on the previous FIRM for this jurisdiction. The floodplains and floodways that were transferred from the previous FIRM may have been adjusted to conform to these new stream channel configurations. As a result, the Flood Profiles and Floodway Data tables in the Flood Insurance Study Report (which contains authoritative hydraulic data) may reflect stream channel distances that differ from what is shown on this map.

Corporate limits shown on this map are based on the best data available at the time of publication. Because changes due to annexations or de-annexations may have occurred after this map was published, map users should contact appropriate community officials to verify current corporate limit locations.

Please refer to the separately printed **Map Index** for an overview map of the county showing the layout of map panels, community map repository addresses, and a listing of Communities table containing National Flood Insurance Program dates for each community as well as a listing of the panels on which each community is located.

Contact the **FEMA Map Service Center** at 1-800-359-9616 for information on available products associated with this FIRM. Available products may include previously issued Letters of Map Change, a Flood Insurance Study report, and/or digital versions of this map. The FEMA Map Service Center may also be reached by Fax at 1-800-358-9620 and its website at <http://mfc.fema.gov>.

If you have **questions about this map** or questions concerning the National Flood Insurance Program in general, please call **1-877-FEMA-MAP** (1-877-336-2677) or visit the FEMA website at <http://www.fema.gov>.



LEGEND

SPECIAL FLOOD HAZARD AREAS SUBJECT TO INUNDATION BY THE 1% ANNUAL CHANCE FLOOD

The 1% annual Flood (100-year flood), also known as the base flood, is the flood that has a 1% chance of being equaled or exceeded in any given year. The Special Flood Hazard Area is the area subject to flooding by the 1% annual chance flood. Areas of Special Flood Hazard include Zones A, AE, AH, AO, AR, AV, V, VE, and VE1. The Base Flood Elevation is the water-surface elevation of the 1% annual chance flood.

ZONE A No Base Flood Elevations determined. Base Flood Elevations determined.

ZONE AE Flood depths of 1 to 3 feet (usually areas of ponding). Base Flood Elevations determined.

ZONE AO Flood depths of 1 to 3 feet (usually sheet flow on sloping terrain). Average depths determined. For areas of shallow fast flooding, velocities (as determined).

ZONE AR Special Flood Hazard Area formerly protected from the 1% annual chance flood by a flood control system that was subsequently derelict. Zone AR indicates that the former flood control system is being retained to provide protection from the 1% annual chance or greater flood.

ZONE AV Area to be protected from 1% annual chance flood by a Federal flood protection system under construction; no Base Flood Elevations determined.

ZONE V Coastal flood zone with velocity hazard (wave action). No Base Flood Elevation determined.

ZONE VE Coastal flood zone with velocity hazard (wave action). Base Flood Elevation determined.

FLOODWAY AREAS IN ZONE AE

The floodway is the channel of a stream plus any adjacent floodplain areas that must be kept free of encroachment so that the 1% annual chance flood can be carried without substantial increases in flood heights.

OTHER FLOOD AREAS

ZONE X Areas of 0.2% annual chance flood areas of 1% annual chance flood with average depths of less than 1 foot with average water level less than 1 square mile and areas protected by levees from 1% annual chance flood.

ZONE D Areas in which flood hazards are undetermined, but possible.

COASTAL BARRIER RESOURCES SYSTEM (CBRS) AREAS

OTHERWISE PROTECTED AREAS (OPAs)

CBRS areas and OPAs are normally located within or adjacent to Special Flood Hazard Areas:

- Floodway boundary
- Floodway boundary
- Zone D boundary
- CBRS and OPA boundary
- Boundary dividing Special Flood Hazard Area zones and boundary dividing Special Flood Hazard Areas of different Base Flood Elevations, flood depths or flood velocities.
- Base Flood Elevation line and velocity direction in feet*
- Base Flood Elevation value where within zone elevation in feet*

* Referenced to the National Geodetic Vertical Datum of 1929

- Cross-section line
- Transect line
- Geographic coordinates referenced to the North American Datum of 1983 (NAD 83), Western Hemisphere
- 100-foot Universal Transverse Mercator grid values, zone 17
- 200-foot grid ticks: Florida State Plane coordinate system, East zone (FIPSZONE 0001), Transverse Mercator projection
- Bench mark (see preparation in Notes to Users section of this FIRM panel)
- 1:1.5
- River Mile
- MAP REVISION HISTORY
- Rate of Map Revisions on Map Index
- EFFECTIVE DATE OF COMMUNITY FLOOD INSURANCE RATE MAP
- January 20, 1991
- EFFECTIVE DATES OF REVISIONS TO THIS PANEL
- March 2, 1994 - May 19, 1994 - July 17, 1995 - the description of revisions, see Notes to Users page in the Flood Insurance Study report.
- September 11, 2009 - to reflect revised procedures, to incorporate previously issued Letters of Map Revision, to reflect updated geographic information, to update progress levels, to add and change Base Flood Elevations, to change zone designations, to add roads and road names, and to add and change Special Flood Hazard Areas.

For community map revision history prior to countywide mapping, refer to the Community Map History table located in the Flood Insurance Study report for this jurisdiction.

To determine if flood insurance is available in this community, contact your insurance agent or call the National Flood Insurance Program at 1-800-338-6620.

MAP SCALE 1" = 500'

250 0 500 1000 FEET

150 0 150 300 METERS

NATIONAL FLOOD INSURANCE PROGRAM

PANEL 0304L

FIRM

FLOOD INSURANCE RATE MAP

MIAMI-DADE COUNTY, FLORIDA AND INCORPORATED AREAS

PANEL 304 OF 1031

(SEE MAP INDEX FOR FIRM PANEL LAYOUT)

COMMUNITY	NUMBER	PANEL	SUFFIX
MIAMI-DADE COUNTY	120635	304L	L
MIAMI CITY OF	120650	0304L	L

Refer to User: The Map Number shown below should be used when filing maps with the Community Number shown above should be used on insurance applications for the subject community.

MAP NUMBER 12066C0304L

MAP REVISED SEPTEMBER 11, 2009

Federal Emergency Management Agency

NOTES TO USERS

This map is for use in administering the National Flood Insurance Program. It does not necessarily identify all areas subject to flooding, particularly from local drainage sources of small size. The community map repository should be consulted for possible updated or additional flood hazard information.

To obtain more detailed information in areas where **Base Flood Elevations (BFEs)** and/or **floodways** have been determined, users are encouraged to consult the Flood Profiles and Floodway Data and/or Summary of Stillwater Elevations tables contained within the Flood Insurance Study (FIS) report that accompanies this FIRM. Users should be aware that BFEs shown on the FIRM represent rounded whole-foot elevations. These BFEs are intended for flood insurance rating purposes only and should not be used as the sole source of flood elevation information. Accordingly, flood elevation data presented in the FIS report should be utilized in conjunction with the FIRM for purposes of construction and/or floodplain management.

Coastal Base Flood Elevations shown on this map apply only to landward of D.0' National Geodetic Vertical Datum of 1929 (NGVD 29). Users of this FIRM should be aware that coastal flood elevations are also provided in the Summary of Stillwater Elevations tables in the Flood Insurance Study report for this jurisdiction. Elevations shown in the Summary of Stillwater Elevations tables should be used for construction and/or floodplain management purposes when they are higher than the elevations shown on this FIRM.

Boundaries of the **floodways** were computed at cross sections and interpolated between cross sections. The floodways were based on hydraulic considerations with regard to requirements of the National Flood Insurance Program. Floodway widths and other pertinent floodway data are provided in the Flood Insurance Study report for this jurisdiction.

Certain areas not in Special Flood Hazard Areas may be protected by **flood control structures**. Refer to Section 2.4 "Flood Protection Measures" of the Flood Insurance Study report for information on flood control structures for this jurisdiction.

The **projection** used in the preparation of this map was Florida State Plane east zone (FIPSZONE 0901). The horizontal datum was NAD 83, GRS80 spheroid. Differences in datum, spheroid, projection or State Plane zones used in the production of FIRMs for adjacent jurisdictions may result in slight positional differences in map features across jurisdiction boundaries. These differences do not affect the accuracy of this FIRM.

Flood elevations on the map are referenced to the National Geodetic Vertical Datum of 1929. These flood elevations must be compared to structure and ground elevations referenced to the same vertical datum. For information regarding conversion between the National Geodetic Vertical Datum of 1929 and the North American Vertical Datum of 1988, visit the National Geodetic Survey website at <http://www.ngs.noaa.gov> or contact the National Geodetic Survey at the following address:

NGS Information Services
NOAA, NGS12
National Geodetic Survey
SSM3-3, #6202
1315 East-West Highway
Silver Spring, Maryland 20910-3282
(301) 713-3242

To obtain current elevation, description, and/or location information for **bench marks** shown on this map, please contact the Information Services Branch of the National Geodetic Survey at (301) 713-3242, or visit its website at <http://www.ngs.noaa.gov>.

Base map information shown on this FIRM was provided in digital format by the Miami-Dade County Information Technology Department. These data were compiled at a scale of 1:3,600 from digital orthophotography dated 2001. Additional base map information was provided by the Cities of Aventura, Coral Gables, and Homestead, the Town of Cutler Bay, and Miami-Dade County.

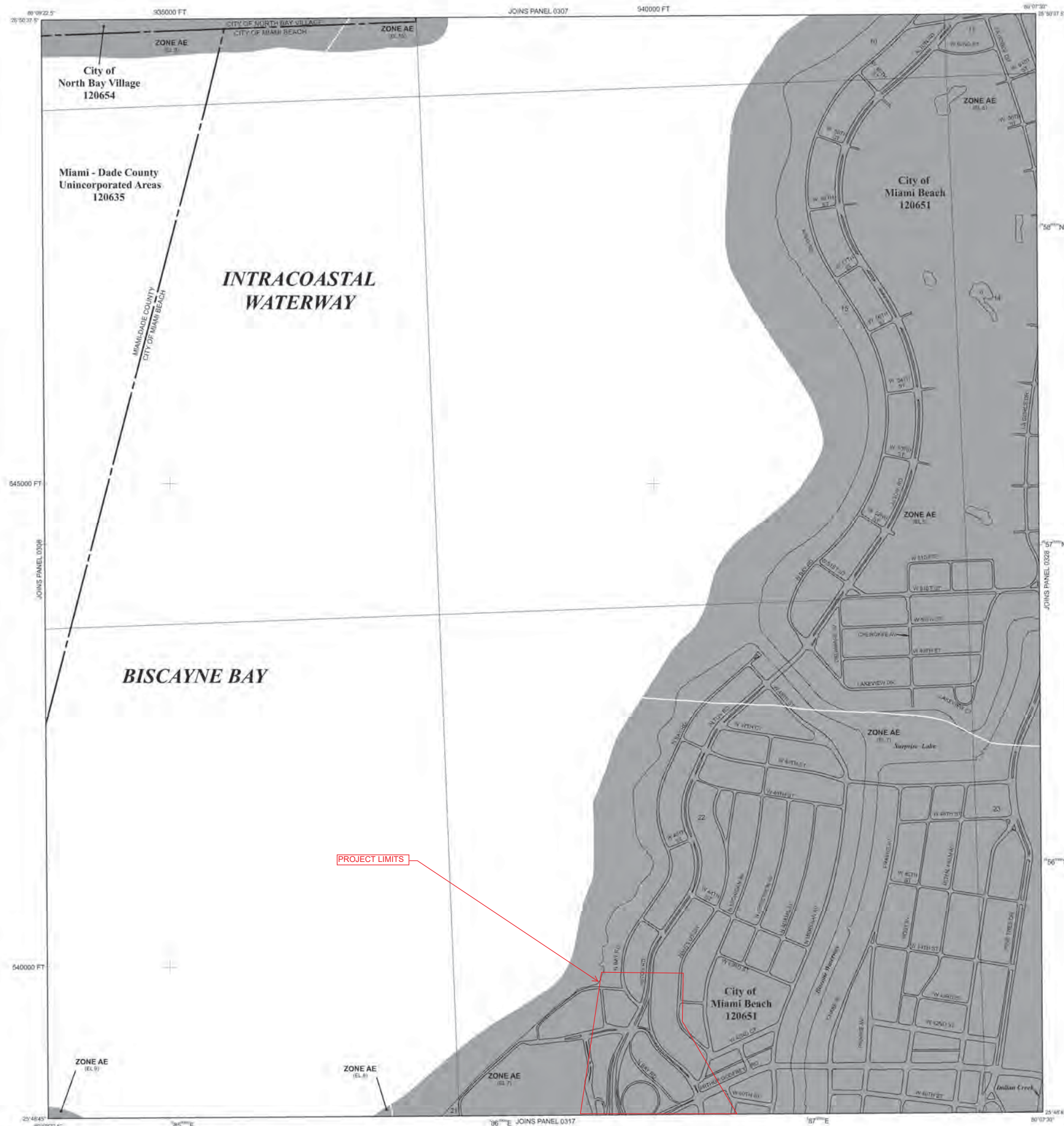
This map reflects more detailed and up-to-date stream channel configurations than those shown on the previous FIRM for this jurisdiction. The floodplains and floodways that were transferred from the previous FIRM may have been adjusted to conform to these new stream channel configurations. As a result, the Flood Profiles and Floodway Data tables in the Flood Insurance Study Report (which contains authoritative hydraulic data) may reflect stream channel distances that differ from what is shown on this map.

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If you have **questions about this map** or questions concerning the National Flood Insurance Program in general, please call 1-877-FEMA-MAP (1-877-336-2627) or visit the FEMA website at <http://www.fema.gov>.



LEGEND

SPECIAL FLOOD HAZARD AREAS SUBJECT TO INUNDATION BY THE 1% ANNUAL CHANCE FLOOD

The 1% annual flood (100-year flood), also known as the base flood, is the flood that has a 1% chance of being equaled or exceeded in any given year. The Special Flood Hazard Area is the area subject to flooding by the 1% annual chance flood. Areas of Special Flood Hazard include Zones A, AE, AH, AO, AR, AV, V, and VE. The Base Flood Elevation is the water-surface elevation of the 1% annual chance flood.

ZONE A No Base Flood Elevations determined. Base Flood Elevations determined.

ZONE AE Flood depths of 1 to 3 feet (usually areas of ponding); Base Flood Elevations determined.

ZONE AH Flood depths of 1 to 3 feet (usually sheet flow on sloping terrain); average depths determined. For areas of shallow fast flooding, velocities (not determined).

ZONE AO Special Flood Hazard Area formerly protected from the 1% annual chance flood by a flood control system that was subsequently deactivated. Zone AO indicates that the former flood control system is being retained to provide protection from the 1% annual chance of greater flood.

ZONE AR Area to be protected from 1% annual chance flood by a Federal flood protection system under construction; no Base Flood Elevations determined.

ZONE AV Coastal flood zone with velocity hazard (wave action); no Base Flood Elevation determined.

ZONE VE Coastal flood zone with velocity hazard (wave action); Base Flood Elevations determined.

FLOODWAY AREAS IN ZONE AE

The floodway is the channel of a stream plus any adjacent floodplain areas that must be kept free of encroachment so that the 1% annual chance flood can be carried without substantial increases in flood heights.

OTHER FLOOD AREAS

ZONE X Areas of 0.2% annual chance flood; areas of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile and areas protected by levees from 1% annual chance flood.

OTHER AREAS

ZONE X Areas determined to be outside the 0.2% annual chance floodplain.

ZONE D Areas in which flood hazards are undetermined, but possible.

COASTAL BARRIER RESOURCES SYSTEM (CBRS) AREAS

OTHERWISE PROTECTED AREAS (OPAs)

CBRS areas and OPAs are normally located within or adjacent to Special Flood Hazard Areas.

Floodplain boundary
Floodway boundary
Zone boundary
Zone D boundary
CBRS and OPA boundary
Boundary dividing Special Flood Hazard Area zones and boundary dividing Special Flood Hazard Areas of different Base Flood Elevations, flood depths or flood velocities.
Base Flood Elevation line and value; elevation in feet
Base Flood Elevation value where within zone; elevation in feet

* Referenced to the National Geodetic Vertical Datum of 1929

○ Cross-section line
--- Transact line
87°07'45" 32"22'30"

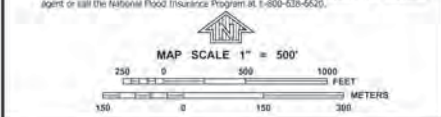
76°N
600000 FT
DX5510 x
M1.5
MAP REPOSITORY
Ruler to listing of Map Repositories on Map Index

EFFECTIVE DATE OF COUNTY-WIDE FLOOD INSURANCE RATE MAP
January 20, 1993

EFFECTIVE DATES OF REVISIONS TO THIS PANEL
March 2, 1994 - May 16, 1994 - July 17, 1995 - for description of revision, see Notes to Users page in the Flood Insurance Study report.
September 11, 2009 - to reflect revised shoreline, to incorporate previously issued Letters of Map Revision, to reflect updated geographic information, to update corporate limits, to add and change Base Flood Elevations, to change zone designations, to add roads and road names, and to add and change Special Flood Hazard Areas.

For community map revision history prior to countywide mapping, refer to the Community Map History table located in the Flood Insurance Study report for this jurisdiction.

To determine if flood insurance is available in this community, contact your insurance agent or visit the National Flood Insurance Program at 1-800-438-9620.



NFIP PANEL 0309L

FIRM
FLOOD INSURANCE RATE MAP

MIAMI-DADE COUNTY,
FLORIDA
AND INCORPORATED AREAS

PANEL 309 OF 1031
(SEE MAP INDEX FOR FIRM PANEL LAYOUT)

CONTAINS

COMMUNITY	NUMBER	PANEL	SUFFIX
MIAMI-DADE COUNTY	120635	0309	L
MIAMI BEACH CITY OF	120651	0309	L
NORTH BAY VILLAGE CITY OF	120654	0309	L

Notes to User: The Map Number shown below should be used when placing map orders. The Community Number shown above should be used on insurance applications for the subject community.

MAP NUMBER
12086C0309L

MAP REVISED
SEPTEMBER 11, 2009

Federal Emergency Management Agency

NOTES TO USERS

This map is for use in administering the National Flood Insurance Program. It does not necessarily identify all areas subject to flooding, particularly from local drainage sources of small size. The community map repository should be consulted for possible updated or additional flood hazard information.

To obtain more detailed information in areas where **Base Flood Elevations (BFEs)** and/or **floodways** have been determined, users are encouraged to consult the Flood Profiles and Floodway Data and/or Summary of Stillwater Elevations tables contained within the Flood Insurance Study (FIS) report that accompanies this FIRM. Users should be aware that BFEs shown on the FIRM represent rounded whole-foot elevations. These BFEs are intended for flood insurance rating purposes only and should not be used as the sole source of flood elevation information. Accordingly, flood elevation data presented in the FIS report should be utilized in conjunction with the FIRM for purposes of construction and/or floodplain management.

Coastal Base Flood Elevations shown on this map apply only to landward of 0' National Geodetic Vertical Datum of 1929 (NGVD 29). Users of this FIRM should be aware that coastal flood elevations are also provided in the Summary of Stillwater Elevations tables in the Flood Insurance Study report for this jurisdiction. Elevations shown in the Summary of Stillwater Elevations tables should be used for construction and/or floodplain management purposes when they are higher than the elevations shown on this FIRM.

Boundaries of the **floodways** were computed at cross sections and interpolated between cross sections. The floodways were based on hydraulic considerations with regard to requirements of the National Flood Insurance Program. Floodway widths and other pertinent floodway data are provided in the Flood Insurance Study report for this jurisdiction.

Certain areas not in Special Flood Hazard Areas may be protected by **flood control structures**. Refer to Section 2.4 "Flood Protection Measures" of the Flood Insurance Study report for information on flood control structures for this jurisdiction.

The **projection** used in the preparation of this map was Florida State Plane east zone (FIPS ZONE 801). The **horizontal datum** was NAD 83, GRS80 spheroid. Differences in datum, spheroid, projection or State Plane zones used in the production of FIRMs for adjacent jurisdictions may result in slight positional differences in map features across jurisdiction boundaries. These differences do not affect the accuracy of this FIRM.

Flood elevations on this map are referenced to the National Geodetic Vertical Datum of 1929. These flood elevations must be compared to structure and ground elevations referenced to the same vertical datum. For information regarding conversion between the National Geodetic Vertical Datum of 1929 and the North American Vertical Datum of 1988, visit the National Geodetic Survey website at <http://www.ngs.noaa.gov> or contact the National Geodetic Survey at the following address:

NGS Information Services
NOAA, NGS12
National Geodetic Survey
SSM3-3, #9202
1315 East-West Highway
Silver Spring, Maryland 20910-3282
(301) 713-3242

To obtain current elevation, description, and/or location information for **bench marks** shown on this map, please contact the Information Services Branch of the National Geodetic Survey at (301) 713-3242, or visit its website at <http://www.ngs.noaa.gov>.

Base map information shown on this FIRM was provided in digital format by the Miami-Dade County Information Technology Department. These data were compiled at a scale of 1:3,600 from digital orthophotography dated 2001. Additional base map information was provided by the Cities of Aventura, Coral Gables, and Homestead, the Town of Cutler Bay, and Miami-Dade County.

This map reflects more detailed and up-to-date **stream channel configurations** than those shown on the previous FIRM for this jurisdiction. The floodplains and floodways that were transferred from the previous FIRM may have been adjusted to conform to these new stream channel configurations. As a result, the Flood Profiles and Floodway Data tables in the Flood Insurance Study Report (which contains authoritative hydraulic data) may reflect stream channel distances that differ from what is shown on this map.

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LEGEND

SPECIAL FLOOD HAZARD AREAS SUBJECT TO INUNDATION BY THE 1% ANNUAL CHANCE FLOOD

The 1% annual Flood (100-year flood), also known as the base flood, is the flood that has a 1% chance of being equaled or exceeded in any given year. The Special Flood Hazard Area is the area subject to flooding by the 1% annual chance flood. Areas of Special Flood Hazard include Zones A, AE, AH, AO, AR, AV, V, and VE. The Base Flood Elevation is the water-surface elevation of the 1% annual chance flood.

- ZONE A** No Base Flood Elevations determined. Base Flood Elevations determined.
- ZONE AE** Flood depths of 1 to 3 feet (usually areas of ponding). Base Flood Elevations determined.
- ZONE AH** Flood depths of 1 to 3 feet (usually sheet flow on sloping terrain). Average depths determined. For areas of shallow but flooding, velocities (as determined).
- ZONE AO** Flood depths of 1 to 3 feet (usually sheet flow on sloping terrain). Average depths determined. For areas of shallow but flooding, velocities (as determined).
- ZONE AR** Special Flood Hazard Area formerly protected from the 1% annual chance flood by a flood control system that was subsequently deactivated. Zone AR indicates that the former flood control system is being retained to provide protection from the 1% annual chance or greater flood.
- ZONE AV** Area to be protected from 1% annual chance flood by a Federal flood protection system under construction; no Base Flood Elevations determined.
- ZONE V** Coastal flood zone with velocity hazard (wave action); no Base Flood Elevation determined.
- ZONE VE** Coastal flood zone with velocity hazard (wave action); Base Flood Elevation determined.

FLOODWAY AREAS IN ZONE AE
The floodway is the channel of a stream plus any adjacent floodplain areas that must be kept free of encroachment so that the 1% annual chance flood can be carried without substantial increases in flood heights.

OTHER FLOOD AREAS
ZONE X Areas of 0.2% annual chance flood areas of 1% annual chance flood with average depths of less than 1 foot with average water level from 1 square mile and areas protected by levees from 2% annual chance flood.

OTHER AREAS
ZONE D Areas in which flood hazards are undetermined, but possible.

COASTAL BARRIER RESOURCES SYSTEM (CBRS) AREAS
OTHERWISE PROTECTED AREAS (OPAs)
CBRS areas and OPAs are normally located within or adjacent to Special Flood Hazard Areas.

- Floodway boundary
- Floodway boundary
- - - Zone D boundary
- - - CBRS and OPA boundary
- Boundary dividing Special Flood Hazard Area zones and boundary dividing Special Flood Hazard Areas of different Base Flood Elevations, flood depths or flood velocities.
- Base Flood Elevation line and velocity direction in feet* (EL 99')
- Base Flood Elevation value where within zone elevation in feet*

* Referenced to the National Geodetic Vertical Datum of 1929

— Cross-section line
— Tract line
87°07'45" 32°22'30"

76°N
600000 FT
DX5510 x

300-foot grid ticks: Florida State Plane coordinate system. Base line (FP52046 010). Transverse Mercator projection

Bench mark (see description in Notes to Users section of this FIRM panel)

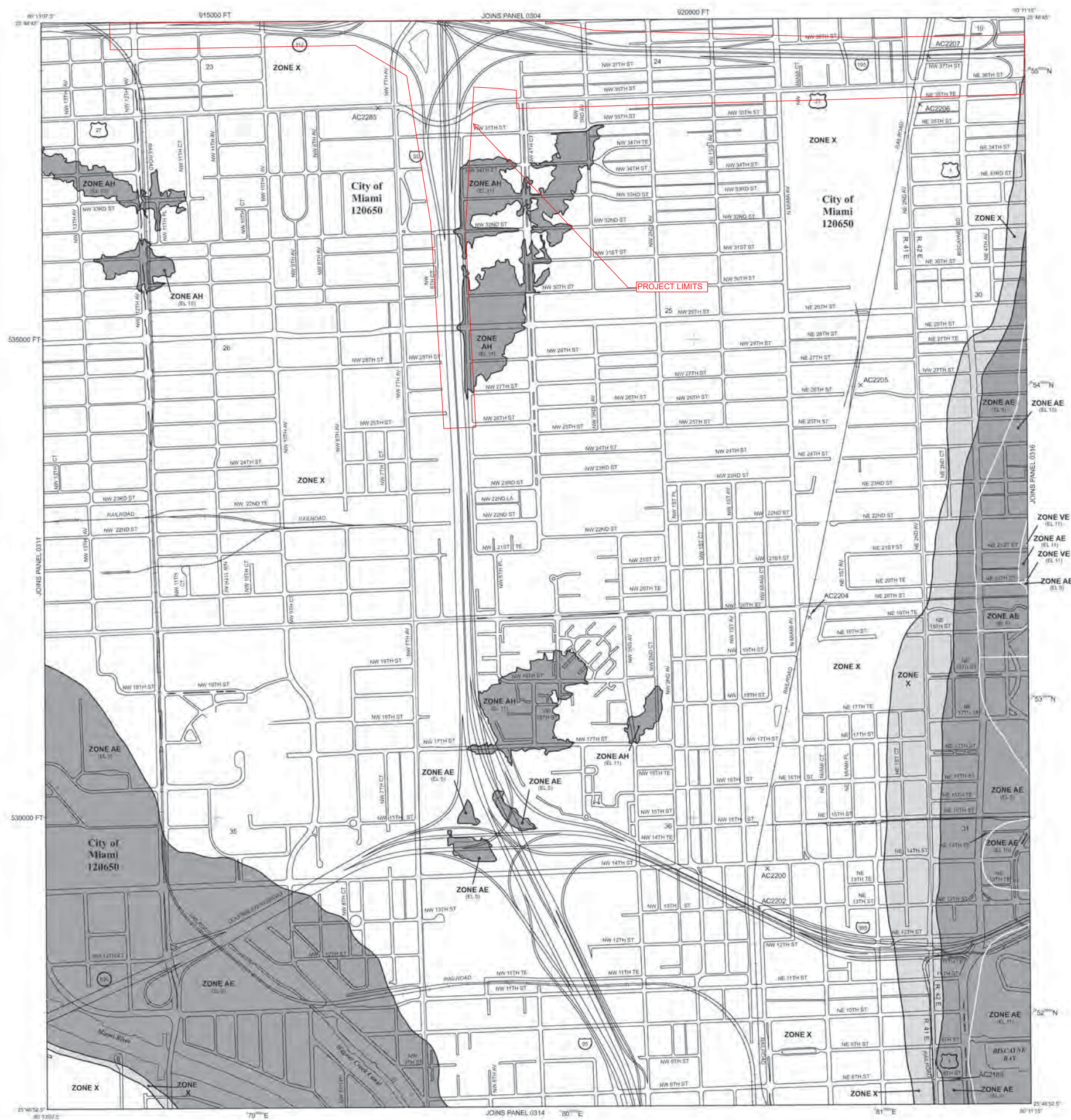
MAP POSITIONING
Rate of Map Improvements on Map Index

EFFECTIVE DATE OF COUNTYWIDE FLOOD INSURANCE RATE MAP
January 20, 1991

EFFECTIVE DATES OF REVISIONS TO THIS PANEL
March 2, 1984 - May 18, 1984 - July 17, 1985 - the description of revisions, see Notes to Users page in the Flood Insurance Study report.
September 11, 2009 - to reflect revised procedure, to incorporate previously issued Letters of Map Revision, to update corporate limits, to add and change Base Flood Elevations, to change zone designations, to add roads and road names, to add and change Special Flood Hazard Areas, and to reflect updated hydrographic information.

For community map revision history prior to countywide mapping, refer to the Community Map History table located in the Flood Insurance Study report for this jurisdiction.
To determine if flood insurance is available in this community, contact your insurance agent or call the National Flood Insurance Program at 1-800-338-6620.

MAP SCALE 1" = 500'
250 0 500 1000 FEET
150 0 150 300 METERS



NATIONAL FLOOD INSURANCE PROGRAM

PANEL 0312L

FIRM
FLOOD INSURANCE RATE MAP

MIAMI-DADE COUNTY,
FLORIDA
AND INCORPORATED AREAS

PANEL 312 OF 1031
(SEE MAP INDEX FOR FIRM PANEL LAYOUT)

CONTAINS	COMMUNITY	NUMBER	PANEL	SHEET
MIAMI CITY OF		10000	0307	L

Refer to User: The Map Number shown below should be used when filing map orders. The Community Number shown above should be used on insurance applications for the subject community.

MAP NUMBER
12086C0312L

MAP REVISED
SEPTEMBER 11, 2009

Federal Emergency Management Agency

NOTES TO USERS

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The projection used in the preparation of this map was Florida State Plane east zone (FIPSZONE 0501). The horizontal datum was NAD 83, GRS80 spheroid. Differences in datum, spheroid, projection or State Plane zones used in the production of FIRMs for adjacent jurisdictions may result in slight positional differences in map features across jurisdiction boundaries. These differences do not affect the accuracy of this FIRM.

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NGS Information Services
NOAA, N/NGS12
National Geodetic Survey
SSM-C-3, #6202
1315 East-West Highway
Silver Spring, Maryland 20910-3282
(301) 713-3242

To obtain current elevation, description, and/or location information for bench marks shown on this map, please contact the Information Services Branch of the National Geodetic Survey at (301) 713-3242, or visit its website at <http://www.ngs.noaa.gov>.

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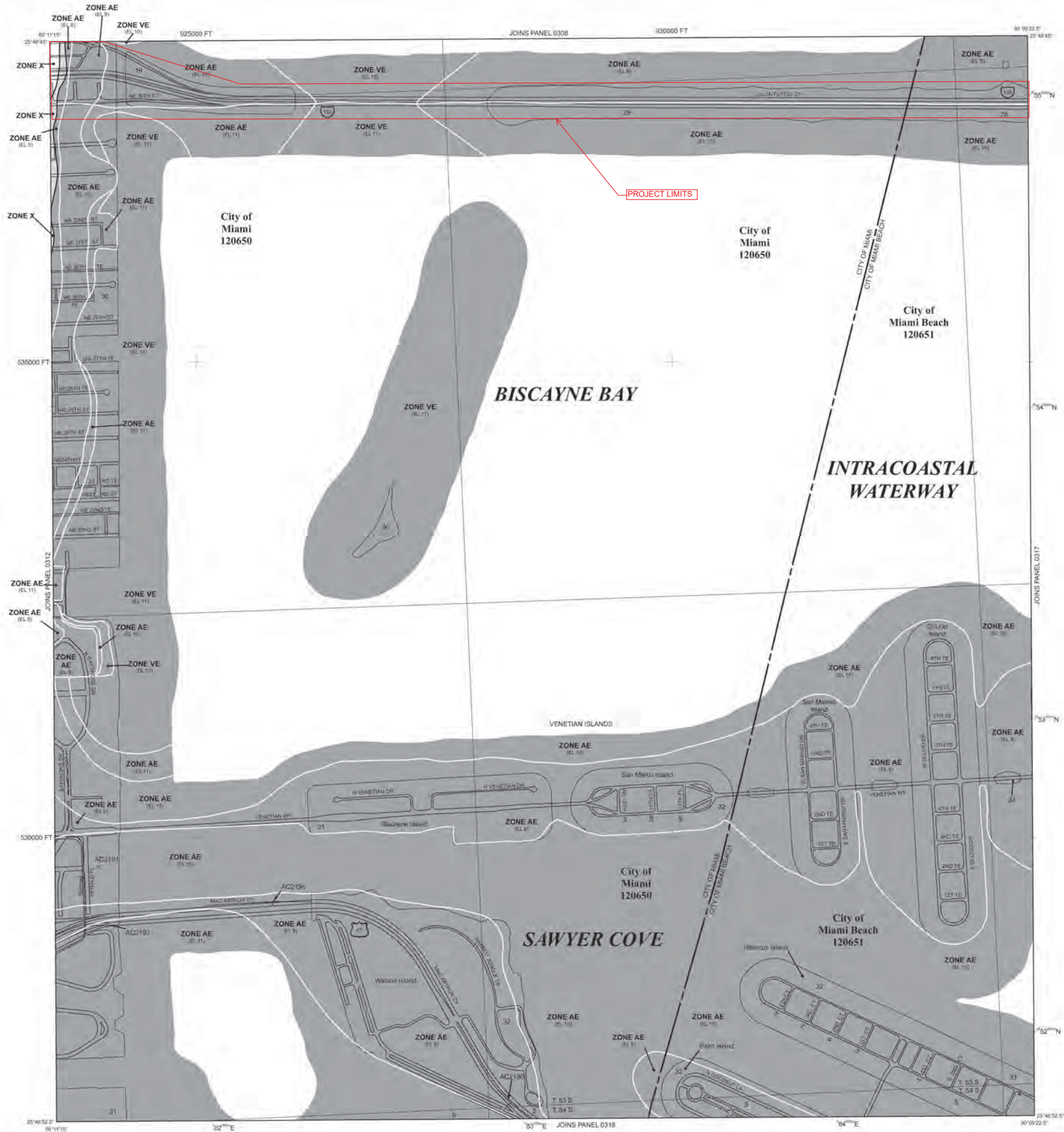
This map reflects more detailed and up-to-date stream channel configurations than those shown on the previous FIRM for this jurisdiction. The floodplains and floodways that were transferred from the previous FIRM may have been adjusted to conform to these new stream channel configurations. As a result, the Flood Profiles and Floodway Data tables in the Flood Insurance Study Report (which contain authoritative hydraulic data) may reflect stream channel distances that differ from what is shown on this map.

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If you have questions about this map or questions concerning the National Flood Insurance Program in general, please call 1-877-FEMA-MAP (1-877-336-2627) or visit the FEMA website at <http://www.fema.gov>.



LEGEND

SPECIAL FLOOD HAZARD AREAS SUBJECT TO INUNDATION BY THE 1% ANNUAL CHANCE FLOOD

The 1% annual flood (100-year flood), also known as the base flood, is the flood that has a 1% chance of being equaled or exceeded in any given year. The Special Flood Hazard Area is the area subject to flooding by the 1% annual chance flood. Areas of Special Flood Hazard include Zones A, AE, AH, AO, AR, AV, V, and VE. The Base Flood Elevation is the water-surface elevation of the 1% annual chance flood.

ZONE A No Base Flood Elevations determined.

ZONE AE Base Flood Elevations determined.

ZONE AH Flood depths of 1 to 3 feet (usually areas of ponding); Base Flood Elevations determined.

ZONE AD Flood depths of 1 to 3 feet (usually sheet flow on sloping terrain); average depths determined. For areas of shallow fast flooding, velocities also determined.

ZONE AR Special Flood Hazard Area formerly protected from the 1% annual chance flood by a flood control system that was subsequently derelict. Zone AR indicates that the former flood control system is being restored to provide protection from the 1% annual chance or greater flood.

ZONE AV Area to be protected from 1% annual chance flood by a Federal flood protection system under construction; no Base Flood Elevations determined.

ZONE V Coastal flood zone with velocity hazard (wave action); no Base Flood Elevation determined.

ZONE VE Coastal flood zone with velocity hazard (wave action); Base Flood Elevations determined.

FLOODWAY AREAS IN ZONE AE

The floodway is the channel of a stream plus any adjacent floodplain areas that must be kept free of encroachment so that the 1% annual chance flood can be carried without substantial increases in flood heights.

OTHER FLOOD AREAS

ZONE X Areas of 0.2% annual chance flood; areas of 1% annual chance flood with average depths of less than 1 foot or with drainage angles less than 1 square mile; and areas protected by levees from 1% annual chance flood.

OTHER AREAS

ZONE X Areas determined to be outside the 0.2% annual chance floodplain.

ZONE D Areas in which flood hazards are undetermined, but possible.

COASTAL BARRIER RESOURCES SYSTEM (CBRS) AREAS

OTHERWISE PROTECTED AREAS (OPAs)

CBRS areas and OPAs are normally located within or adjacent to Special Flood Hazard Areas.

— Floodplain boundary
— Floodway boundary
- - - Zone D boundary
- - - - - CBRS and OPA boundary
- - - - - Boundary dividing Special Flood Hazard Area zones and boundary dividing Special Flood Hazard Areas of different Base Flood Elevations, flood depths or flood velocities.
— 513' Base Flood Elevation line and water elevation in feet*
— 513' Base Flood Elevation value where uniform within zone; elevation in feet*

* Referenced to the National Geodetic Vertical Datum of 1929

— Cross-section line
— Truncate line
87°07'45" 32"22'30"

Geographic coordinates referenced to the North American Datum of 1983 (NAD 83), Western hemisphere
176°N
3000-meter Universal Transverse Mercator grid values, zone 17
600000 FT
DX5510 x
Bench mark (see elevation in Notes to Users section of the FIRM report)
M1.5
River Mile

MAP REPOSITORY
Refer to listing of Map Repositories on Map Index

EFFECTIVE DATE OF COUNTYWIDE FLOOD INSURANCE RATE MAP
January 20, 1993

EFFECTIVE DATES (OF REVISIONS) TO THIS PANEL
March 2, 1994 - May 16, 1994 - July 17, 1995 - the description of revision, see Notes to Users page in the Flood Insurance Study report.
September 11, 2009 - to reflect revised shoreline, to incorporate previously issued Letters of Map Revision, to update corporate limits, to add and change Base Flood Elevations, to change zone designations, to add roads and road names, to add and change Special Flood Hazard Areas, and to reflect updated hydrographic information.

For community map revision history prior to countywide mapping, refer to the Community Map History table located in the Flood Insurance Study report for this jurisdiction.

To determine if flood insurance is available in this community, contact your insurance agent or call the National Flood Insurance Program at 1-800-438-9620.

MAP SCALE 1" = 500'

250 0 500 1000 FEET
150 0 150 300 METERS

NATIONAL FLOOD INSURANCE PROGRAM

PANEL 0316L

FIRM
FLOOD INSURANCE RATE MAP

MIAMI-DADE COUNTY,
FLORIDA
AND INCORPORATED AREAS

PANEL 316 OF 1031
(SEE MAP INDEX FOR FIRM PANEL LAYOUT)

CONTAINS

COMMUNITY	NUMBER	PANEL	SUFFIX
MIAMI BEACH CITY OF	12061	0316	L
MIAMI CITY OF	12065	0316	L

Notice to User: The Map Number shown below should be used when placing map orders. The Community Number shown above should be used on insurance applications for the subject community.

MAP NUMBER
12086C0316L

MAP REVISED
SEPTEMBER 11, 2009

Federal Emergency Management Agency

NOTES TO USERS

This map is for use in administering the National Flood Insurance Program. It does not necessarily identify all areas subject to flooding, particularly from local drainage sources of small size. The community map repository should be consulted for possible updated or additional flood hazard information.

To obtain more detailed information in areas where **Base Flood Elevations (BFEs)** and/or **floodways** have been determined, users are encouraged to consult the Flood Profiles and Floodway Data and/or Summary of Stillwater Elevations tables contained within the Flood Insurance Study (FIS) report that accompanies this FIRM. Users should be aware that BFEs shown on the FIRM represent rounded whole-foot elevations. These BFEs are intended for flood insurance rating purposes only and should not be used as the sole source of flood elevation information. Accordingly, flood elevation data presented in the FIS report should be utilized in conjunction with the FIRM for purposes of construction and/or floodplain management.

Coastal Base Flood Elevations shown on this map apply only landward of 0' National Geodetic Vertical Datum of 1929 (NGVD 29). Users of this FIRM should be aware that coastal flood elevations are also provided in the Summary of Stillwater Elevations tables in the Flood Insurance Study report for this jurisdiction. Elevations shown in the Summary of Stillwater Elevations tables should be used for construction and/or floodplain management purposes when they are higher than the elevations shown on this FIRM.

Boundaries of the **floodways** were computed at cross sections and interpolated between cross sections. The floodways were based on hydraulic considerations with regard to requirements of the National Flood Insurance Program. Floodway widths and other pertinent floodway data are provided in the Flood Insurance Study report for this jurisdiction.

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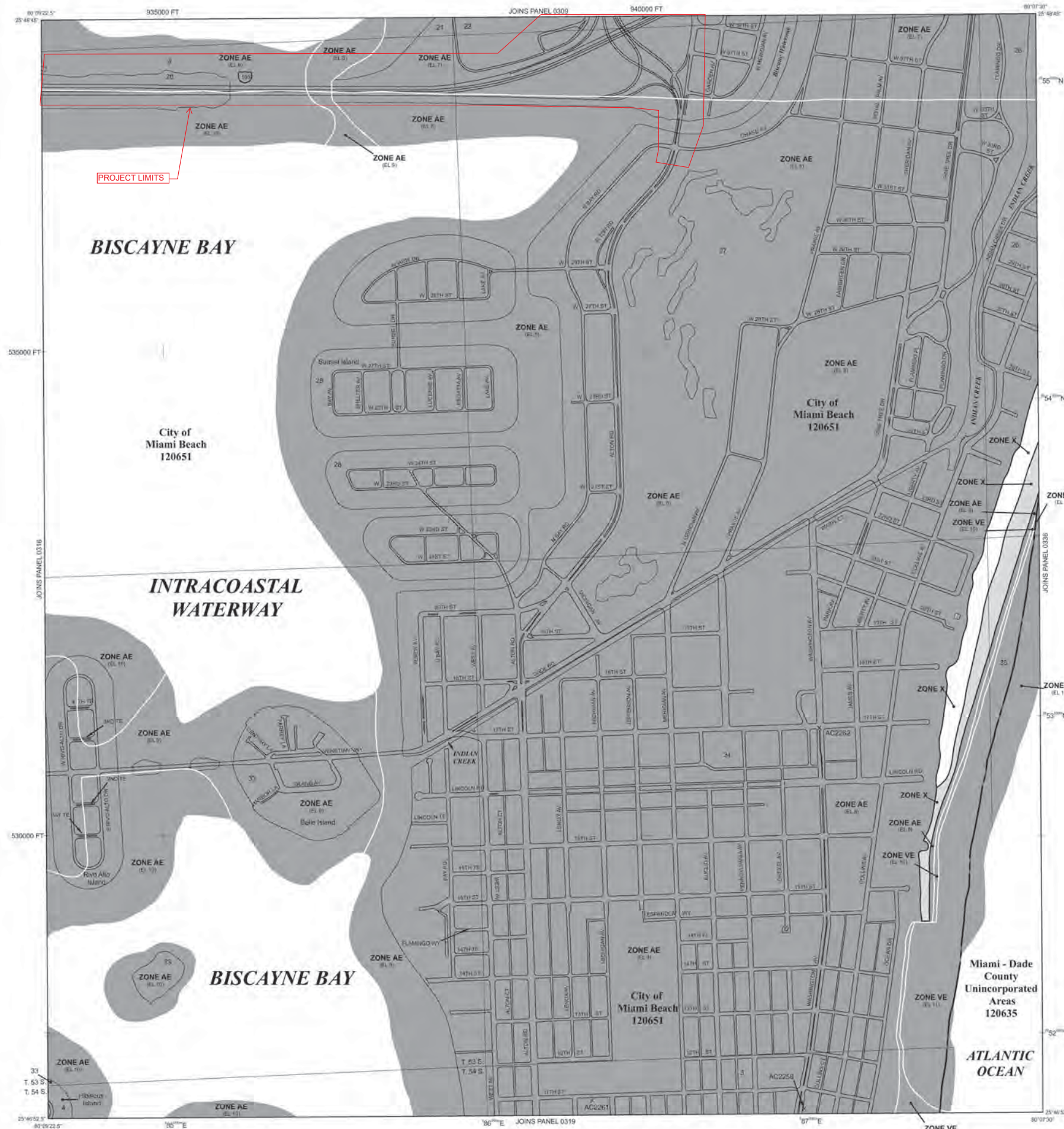
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APPENDIX C – EXISTING TRANSIT SUPPORTING INFORMATION

- MIAMI DADE TRANSIT ROUTE MAPS
- MDT RIDERSHIP TECHNICAL REPORT

MIAMI DADE TRANSIT ROUTE MAPS

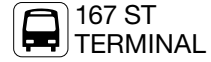


2

NE 167 St

163 St

12 Ave
NE 15 Ave



(weekdays only)

GOLDEN
GLADES

NORTH
MIAMI

N Miami Ave

MIAMI
SHORES



(see schedule)

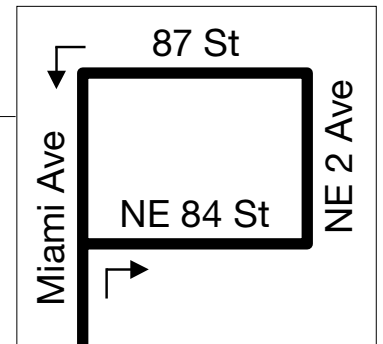
NW 2 Ave

95 St
91 St

HORACE MANN
MIDDLE SCHOOL

short turn

NW 79 St



LITTLE
HAITI

NW 2 Ave

WYNWOOD

NW 3 Ave

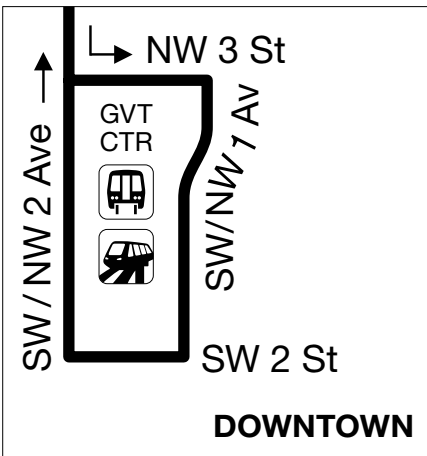
20 St

14 St
2 Ave

NW 1 PI

SW 2 St

DOWNTOWN



NORTH
06/2016

www.miamidade.gov/transit

311 (305.468.5900) TDD: 305.468.5402



DEPARTMENT OF TRANSPORTATION AND PUBLIC WORKS



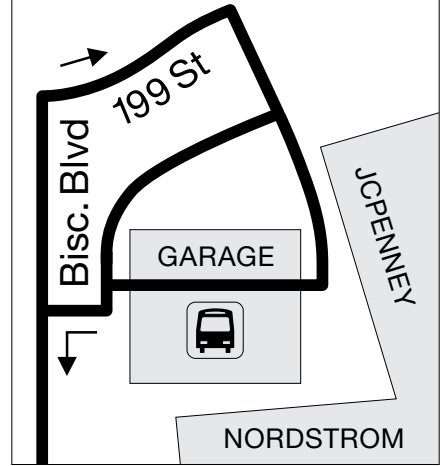
3

NORTH MIAMI BEACH

Aventura Blvd / NE 199 St

THE BUS TERMINAL AT AVENTURA MALL

167 St
12 Ave
15 Ave
171 / 172 St
163 St

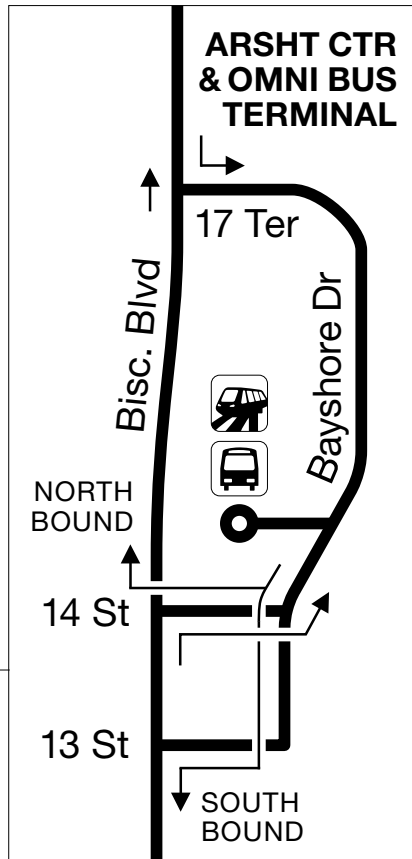


MIAMI SHORES

Biscayne Blvd

UPPER EAST SIDE

ARSHT CTR & OMNI BUS TERMINAL



DOWNTOWN BUS TERMINAL

GVT CTR



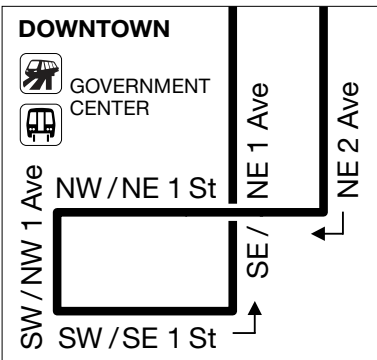
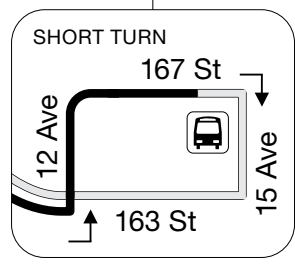
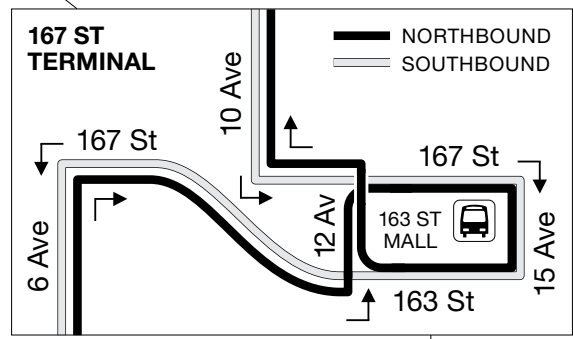
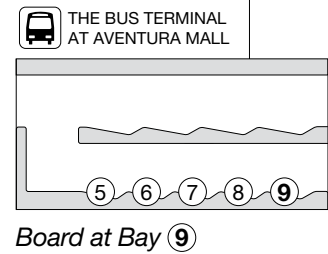
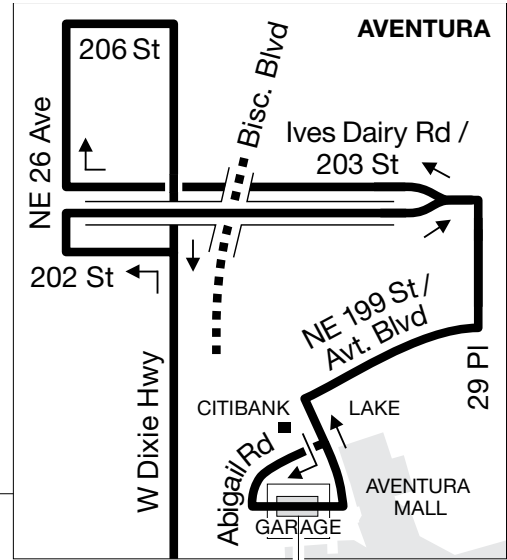
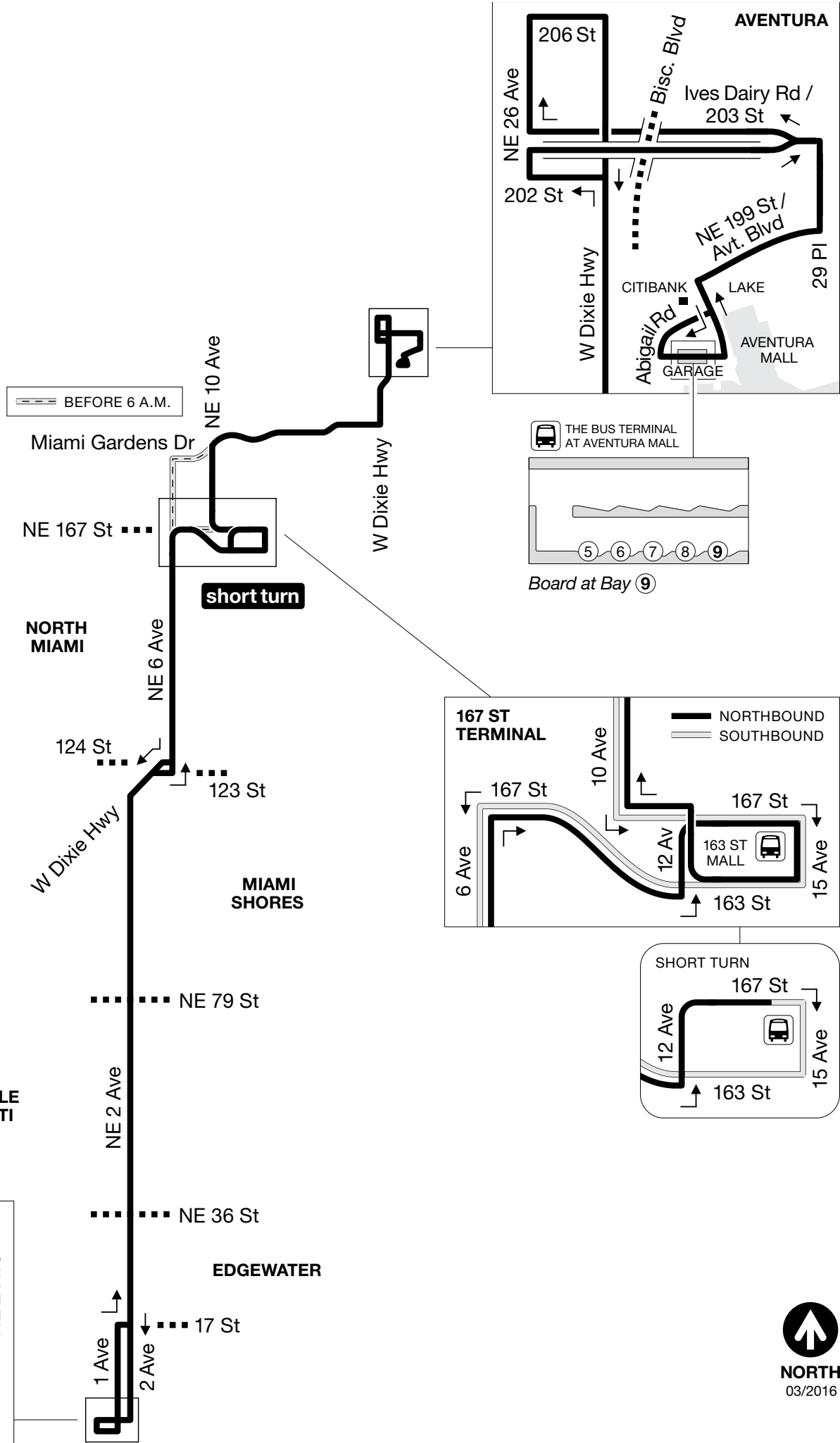
N Mia. Ct
NE 3 St
NW / NE 1 St
Flagler St
NE 2 Ave
SW / SE 1 St

Biscayne Blvd

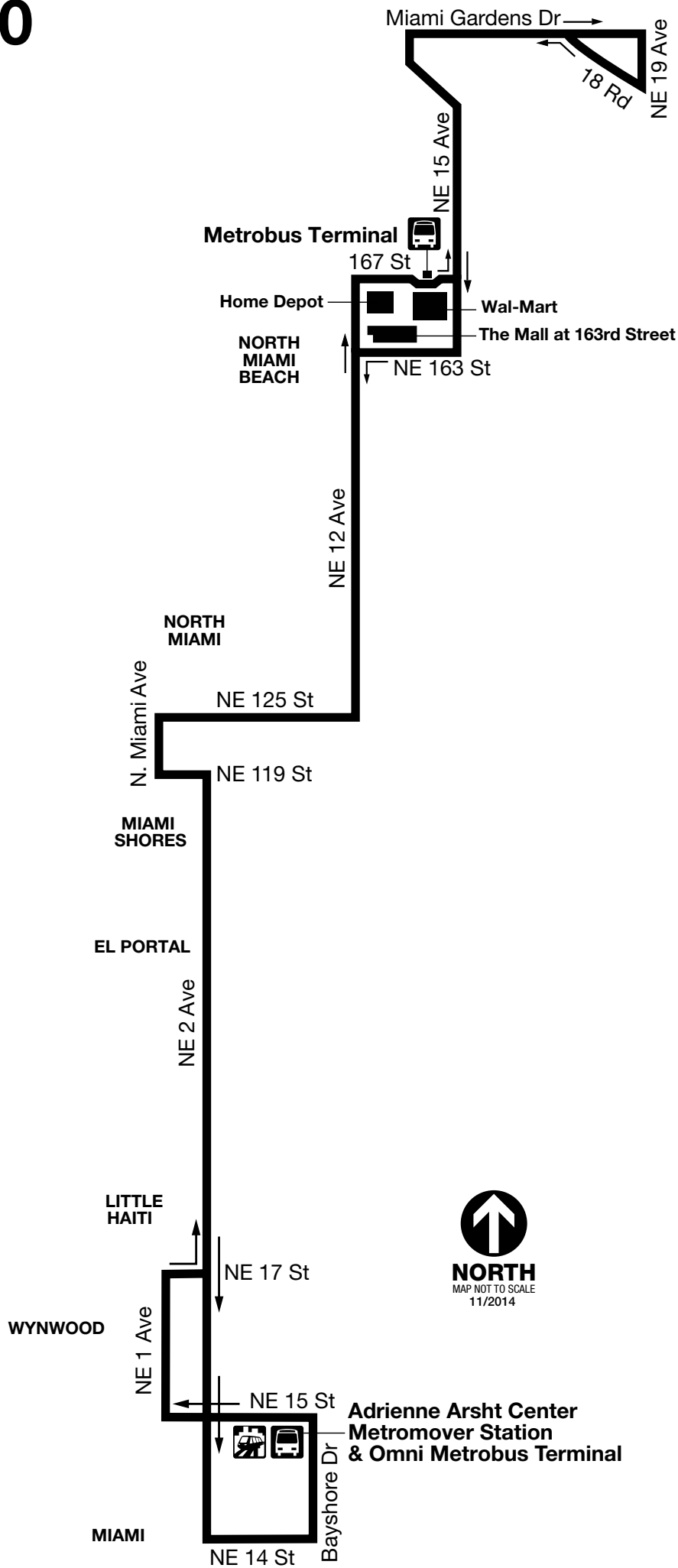


NORTH 06/2016

Route 9



Route 10



Route 12

Northside
Metrorail
Station



NORTHSIDE
SHOPPING CENTER

NW 79 St

NW 31 Ave

NW 22 Ave

NW 71 St

NW 71 St

NW 19 Ave

NW 14 Ave

NW 67 St

NW 62 St

NW 12 Ave

ALLAPATTAH
STATION

NW 12 Ave

NW 36 St

PARKING
LOT

NW 33 St

METRORAIL



Allapattah
Metrorail
Station

Santa Clara
Metrorail
Station



Civic
Center
Metrorail
Station



NW 12 Ave

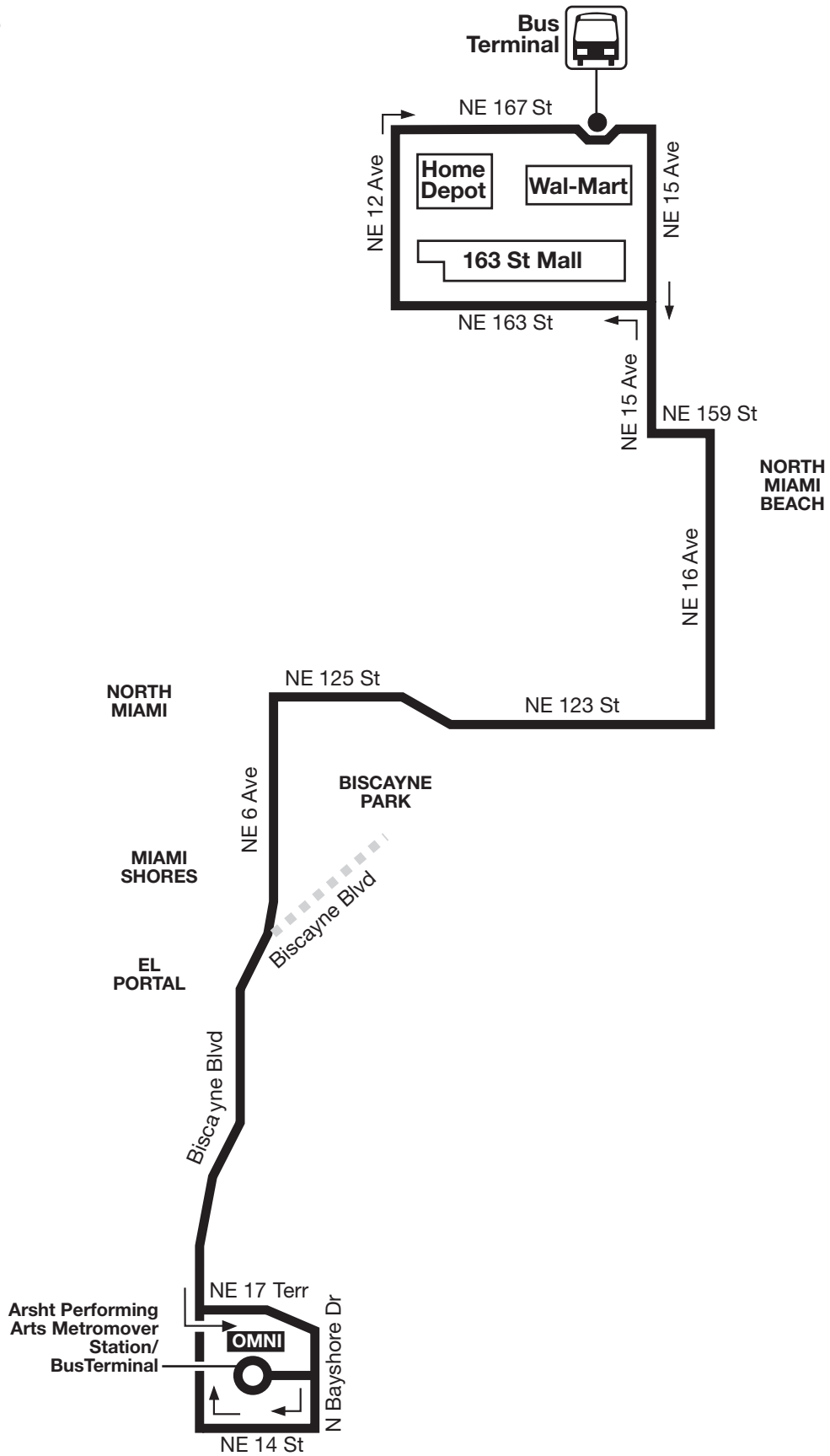


NORTH
MAP NOT TO SCALE
07/12

SW 13 Ave
SW 3 Ave
SW 1 Ave
SW 25 Rd
SW 26 Rd
Halissee St
S. Bayshore Dr.
Emergency
Mercy Way
Mercy Hospital
Vizcaya Metrorail Station

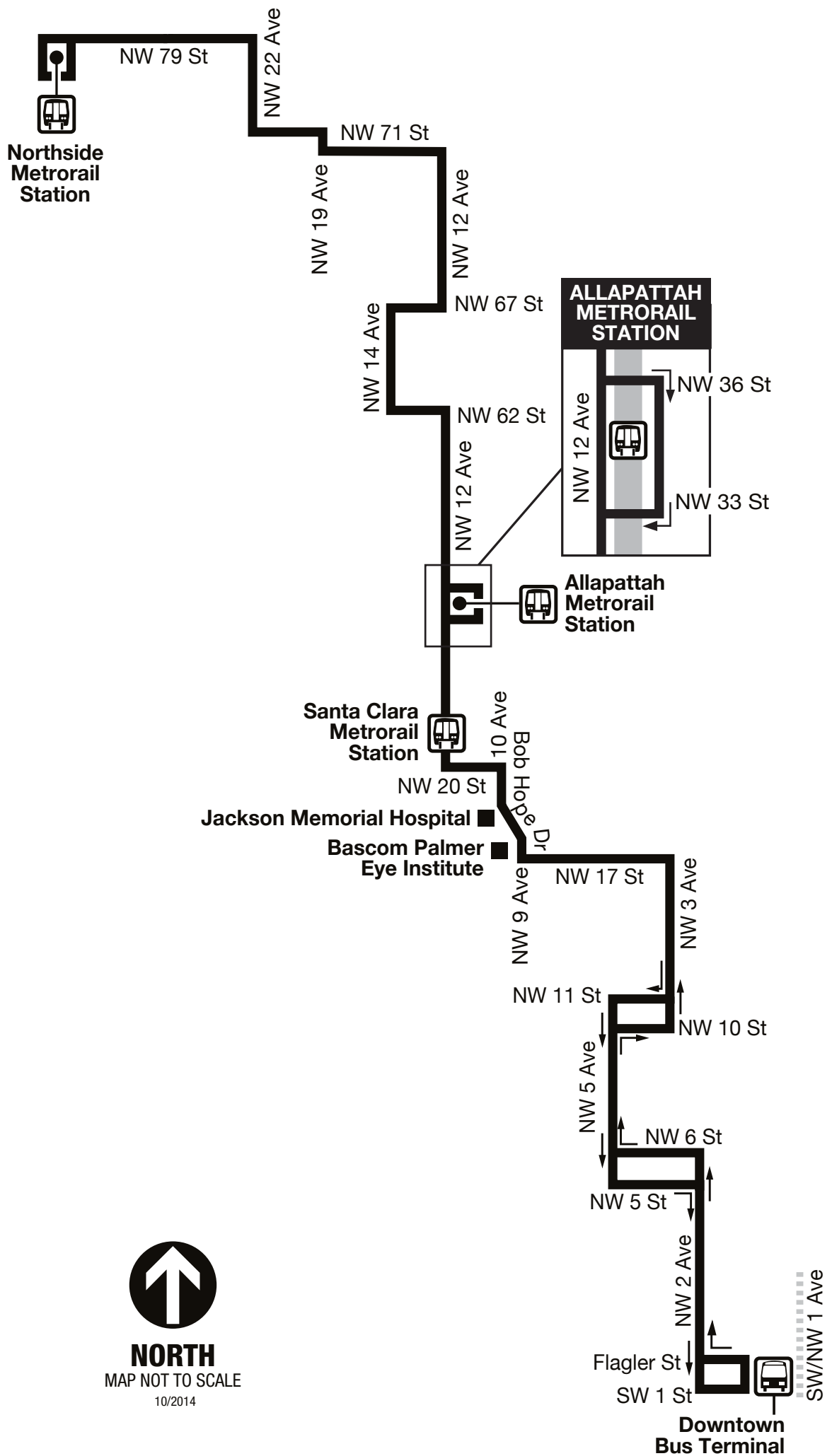
COCONUT
GROVE

Route 16



NORTH
MAP NOT TO SCALE
12/2009

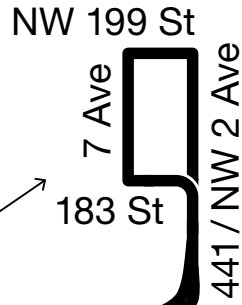
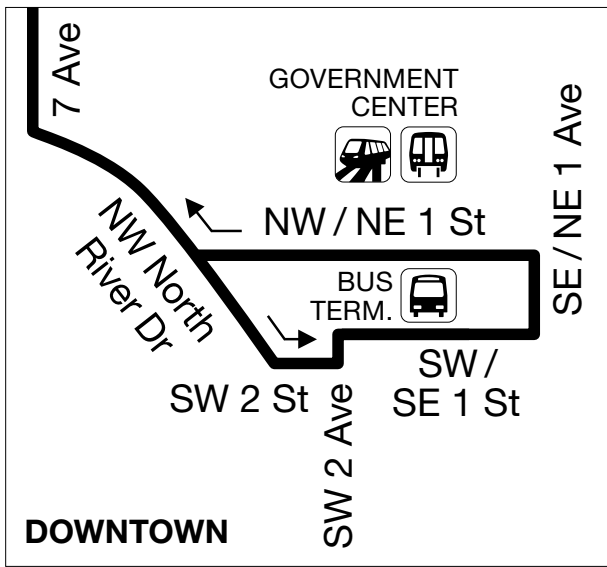
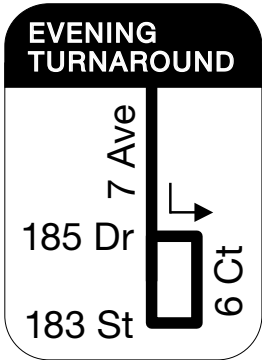
Route 21



NORTH
MAP NOT TO SCALE
10/2014



77



P
GOLDEN GLADES
PARK & RIDE
(WEST LOT)

WEST LITTLE RIVER

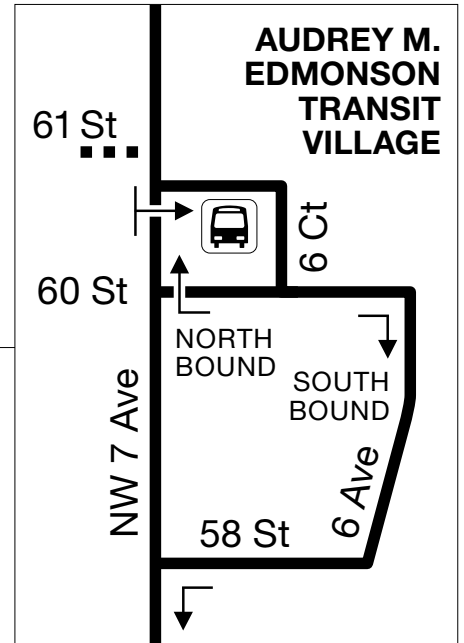
LIBERTY CITY

62 St

LITTLE HAITI

WYNWOOD

CULMER STATION



NORTH
06/2016





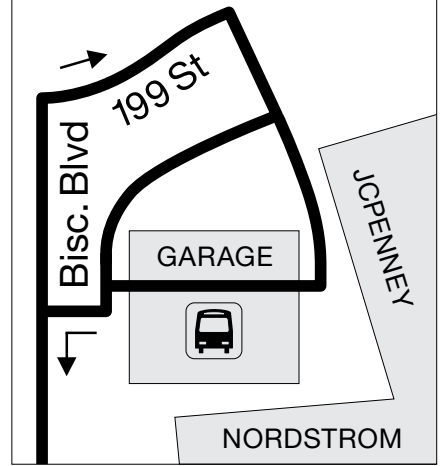
93

BISCAYNE MAX

Aventura Blvd / NE 199 St

MAKES ALL LOCAL STOPS NORTH OF 163 ST

THE BUS TERMINAL AT AVENTURA MALL



LIMITED STOPS
19 St to 163 St

Biscayne Blvd

163 ST

151 ST

146 ST

NORTH MIAMI

135 ST

123 ST

SANS SOUCI BLVD

MIAMI SHORES

107 ST

96 ST

91 ST

UPPER EAST SIDE

79 ST

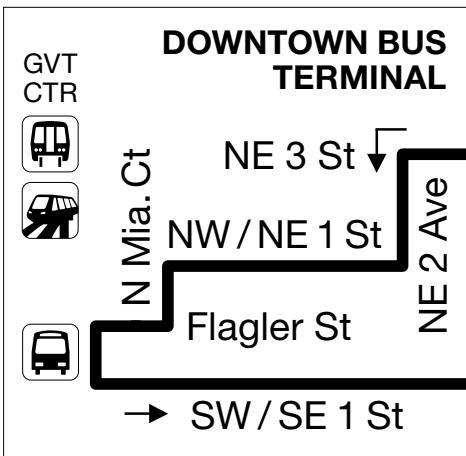
70 ST (NORTHBOUND)

71 ST (SOUTHBOUND)

62 ST

54 ST

DOWNTOWN BUS TERMINAL



36 ST

EDGEWATER

29 ST

19 ST

MAKES ALL LOCAL STOPS SOUTH OF 19 ST



NORTH
06/2016

www.miamidade.gov/transit 311 (305.468.5900) TDD: 305.468.5402

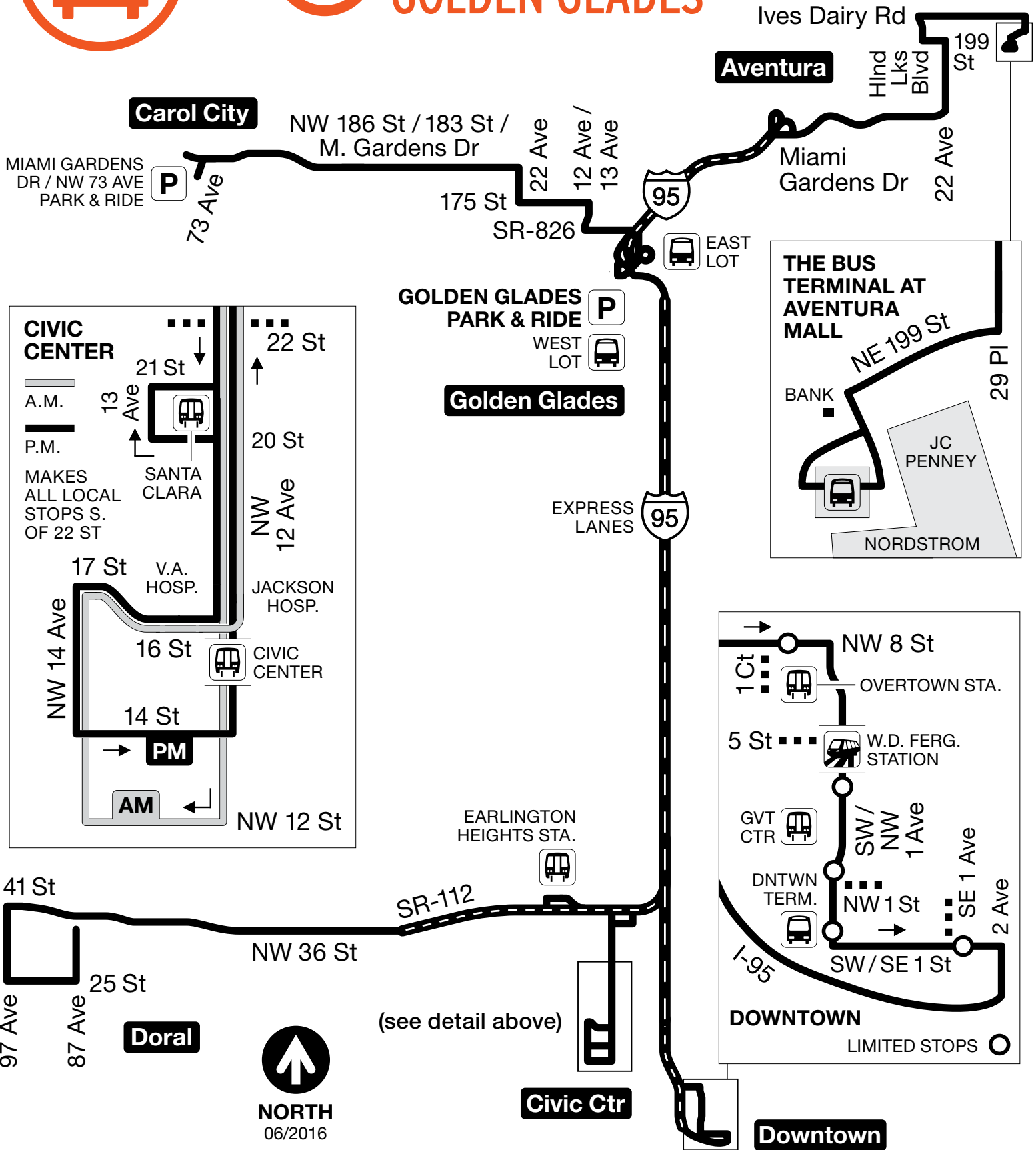
MiamiDadeTransit @IRideMDT @IRideMDT MDT TRACKER / MDT TRANSIT WATCH



DEPARTMENT OF TRANSPORTATION AND PUBLIC WORKS

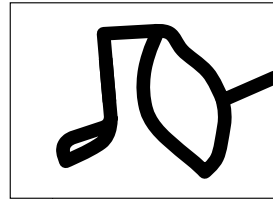
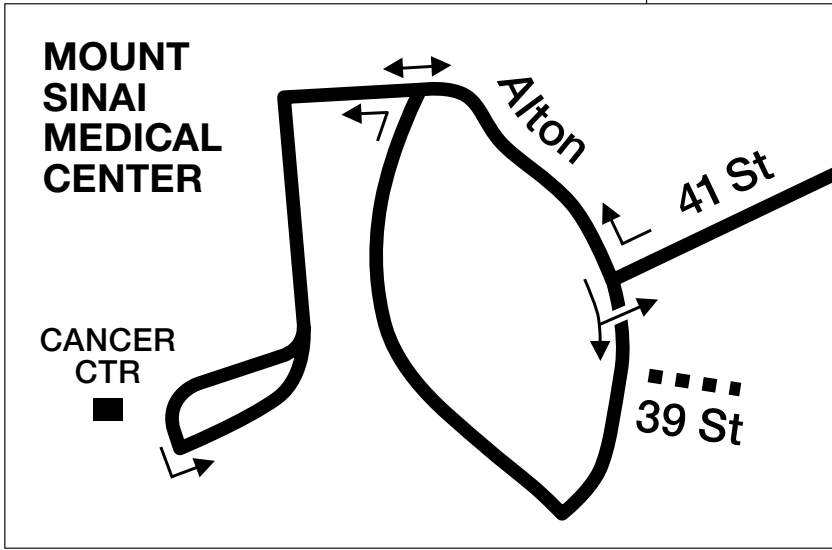


95 EXPRESS GOLDEN GLADES

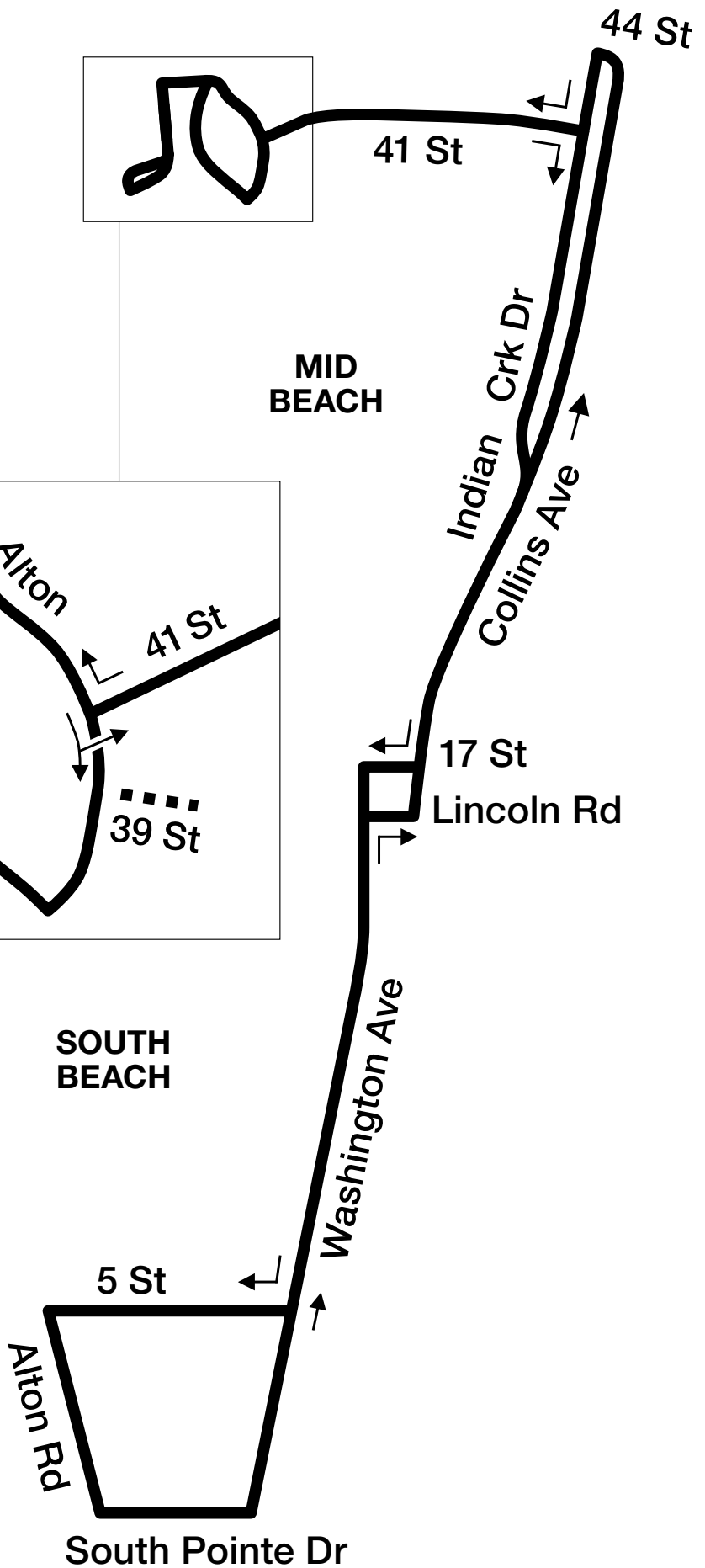




C



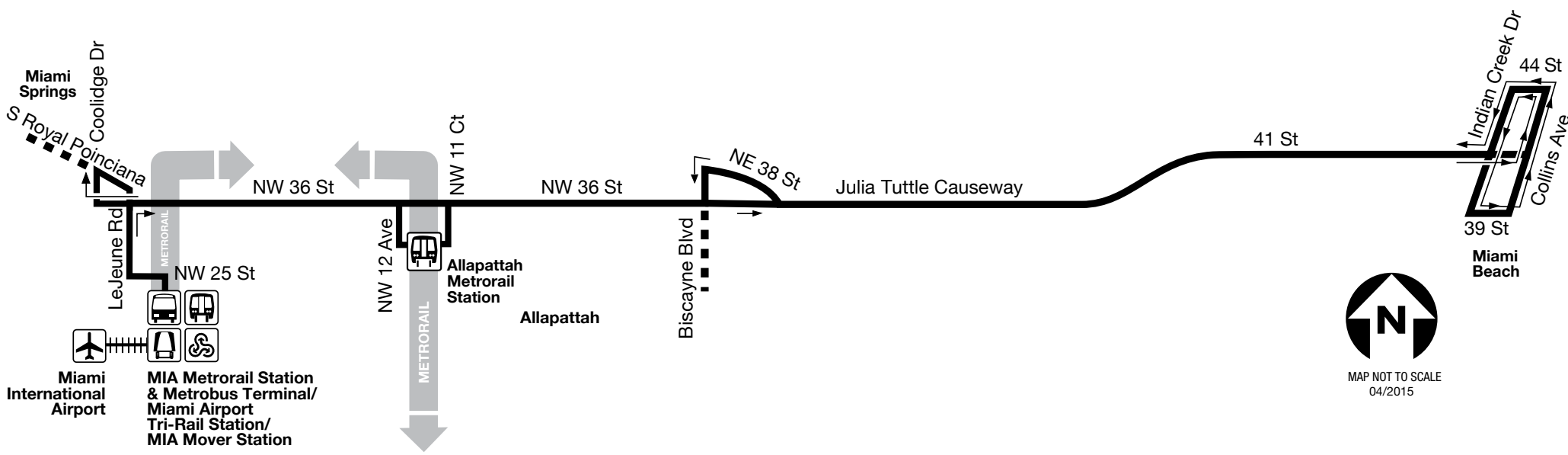
MID BEACH



NORTH
08/2017



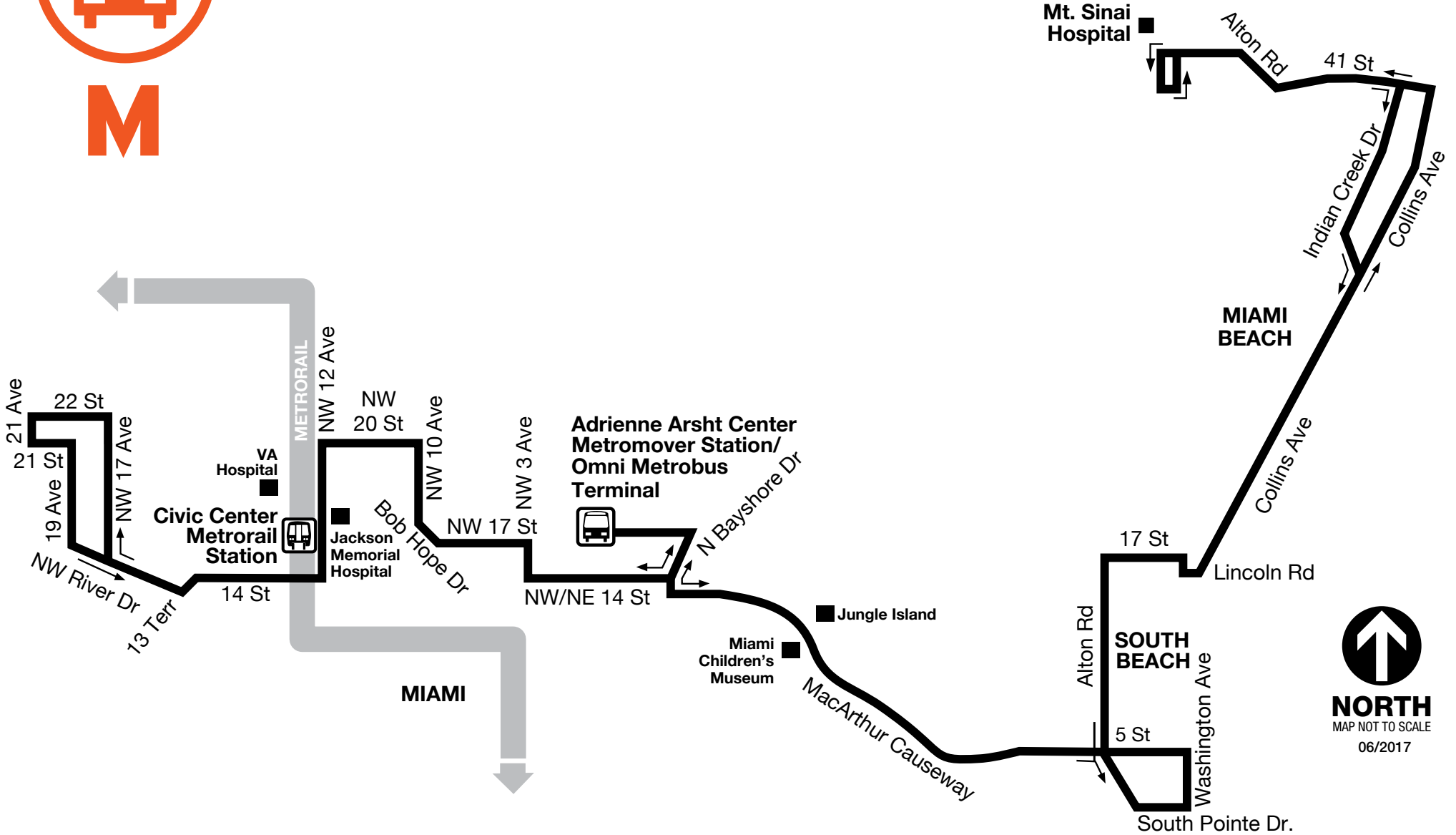
Route J



MAP NOT TO SCALE
04/2015



M



NORTH
MAP NOT TO SCALE
06/2017

www.miamidade.gov/transit DEPARTMENT OF TRANSPORTATION AND PUBLIC WORKS



@GoMiamiDade



MDT TRACKER | EASY PAY MIAMI | MDT TRANSIT WATCH



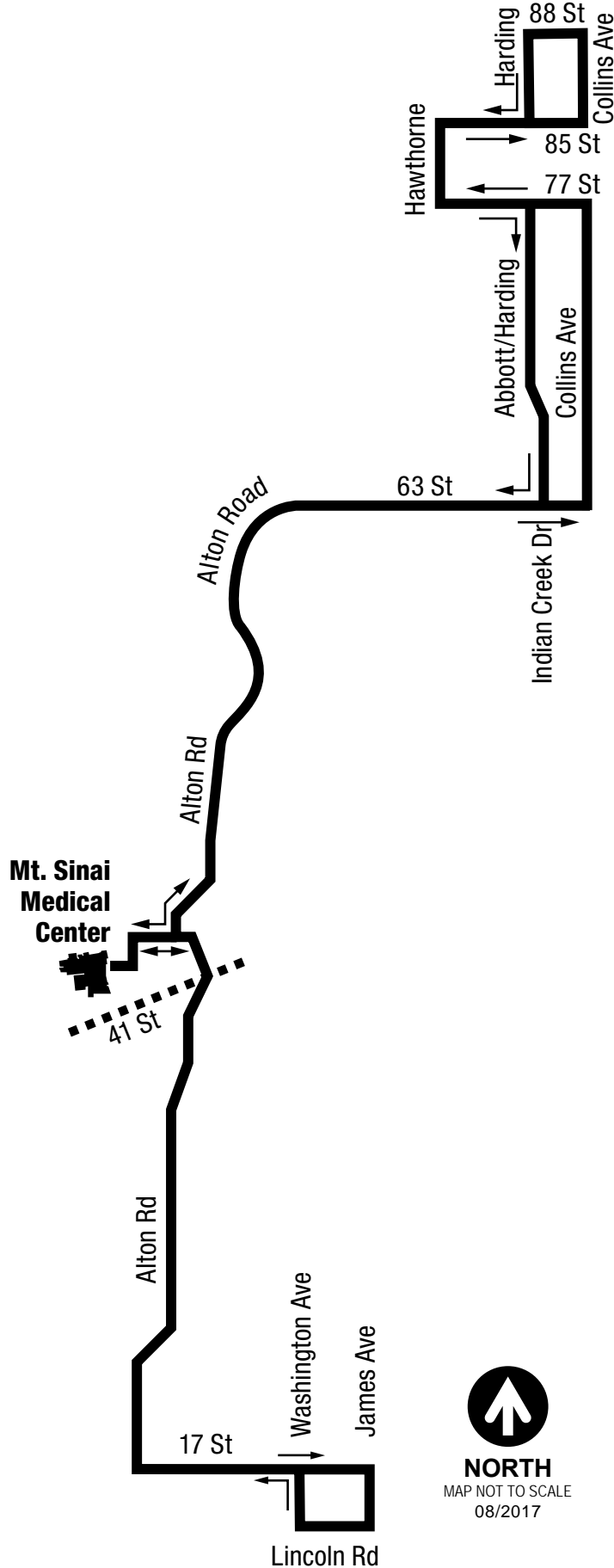
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311 OR 305.468.5900 (TDD: 305.468.5402)



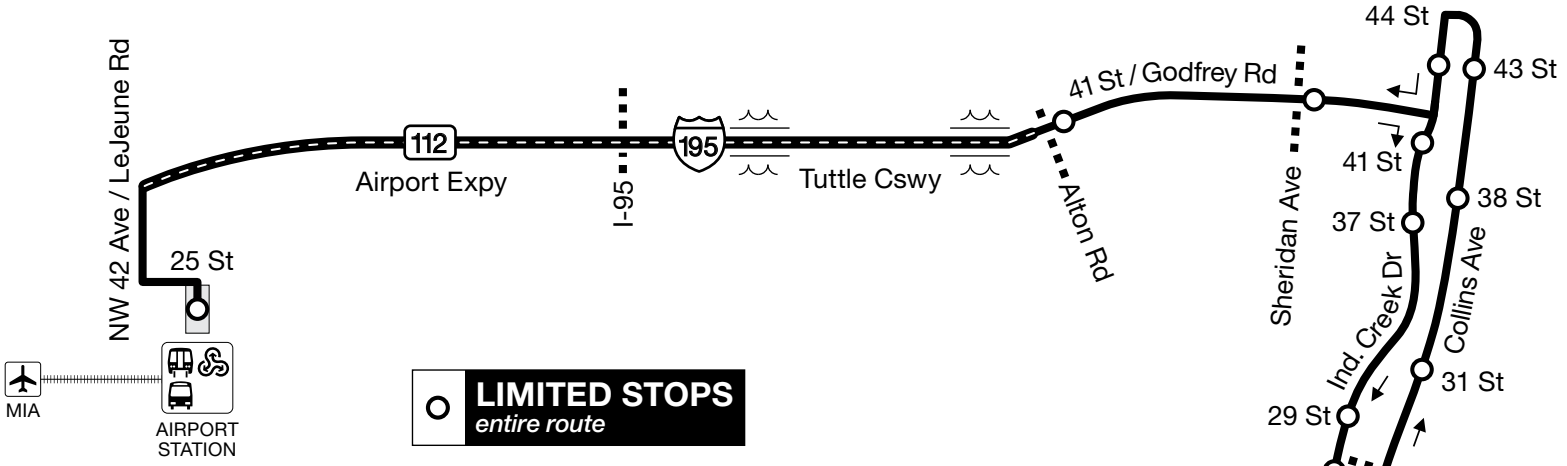


115

MID-NORTH
BEACH CONNECTION



NORTH
MAP NOT TO SCALE
08/2017

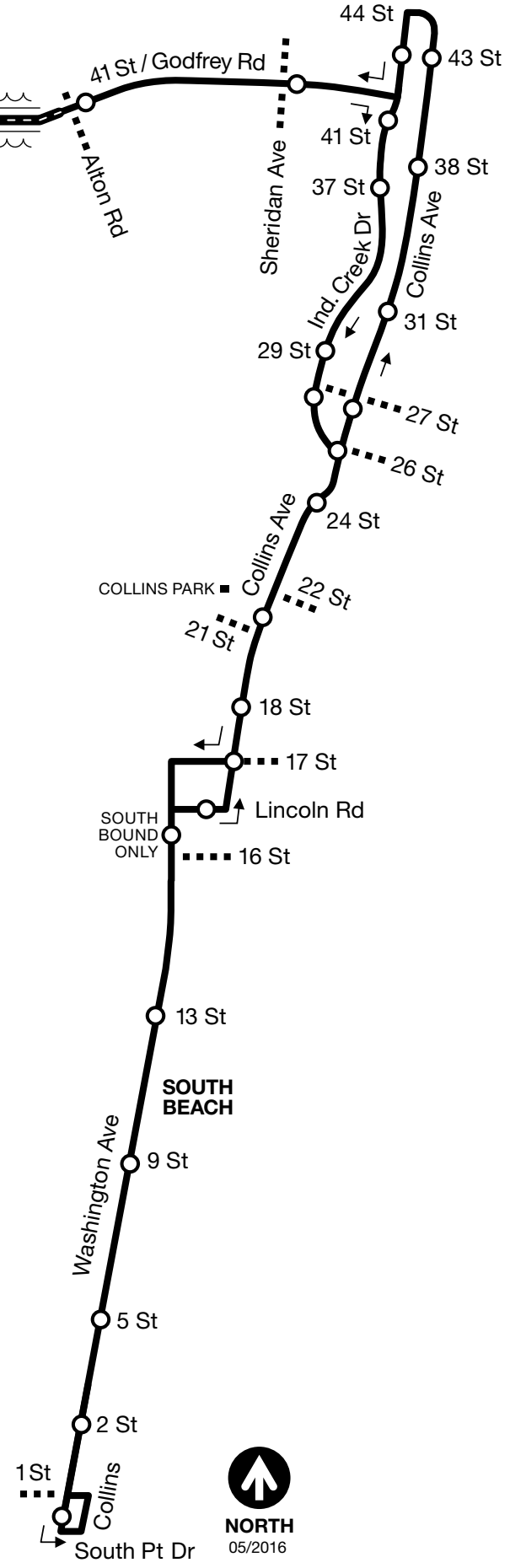


LIMITED STOPS
entire route

SEVEN DAYS A WEEK LOS SIETE DIAS SET JOU YON SEMEN	EVERY/CADA/CHAK 20m	
EASTBOUND RUMBO ESTE/DIREKSYON IS	FROM DESDE • DE	UNTIL* HASTA • A
MIA METRORAIL STATION	6:00 a.m.	11:40 p.m.
41 ST & ALTON RD	6:14 a.m.	11:52 p.m.
41 ST & INDIAN CREEK	6:20 a.m.	11:57 p.m.
LINCOLN RD & WASHINGTON AVE	6:29 a.m.	12:06 a.m.
SOUTH POINTE DR & WASHINGTON AVE	6:39 a.m.	12:16 a.m.
WESTBOUND RUMBO OESTE/DIREKSYON WES	FROM DESDE • DE	UNTIL* HASTA • A
SOUTH POINTE DR & WASHINGTON AVE	5:10 a.m.	10:55 p.m.
LINCOLN RD & WASHINGTON AVE	5:20 a.m.	11:05 p.m.
41 ST & INDIAN CREEK	5:29 a.m.	11:14 p.m.
41 ST & ALTON RD	5:33 a.m.	11:18 p.m.
MIA METRORAIL STATION	5:45 a.m.	11:30 p.m.

*LAST FOUR TRIPS 30 MINUTES APART/ULTIMOS CUATRO VIAJES 30 MINUTOS APARTE/DENYE KAT SOTI 30 MINIT APA

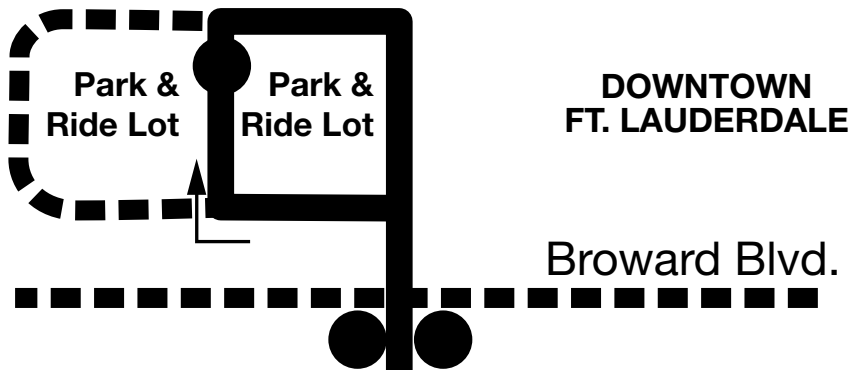
Frequencies are approximate and may vary depending on traffic and road conditions/
Frecuencias son aproximadas, pues dependen del trafico y otras condiciones de las vias/Asosye yo apwoksimatif epi yo ka varye selon kondisyon sikilasyon sou wout yo




150
MIAMI BEACH
AIRPORT EXPRESS



Route 95 Express Dade/Broward Broward Boulevard



 **Ft. Lauderdale
Tri-Rail Station**

Flyover

BROWARD COUNTY
MIAMI-DADE COUNTY



 LIMITED STOP

NW 8 St

**Historic Overtown/
Lyric Theatre
Metrorail Station** 

**Arena/State Plaza
Metromover Station** 

**Government Center
Metrorail/Metromover
Station** 

NW 1 St

**Downtown
Bus Terminal** 

NW 5 St

NW 1 Ave

**DOWNTOWN
MIAMI**

SW/SE 1 St



MAP NOT TO SCALE
10/2015



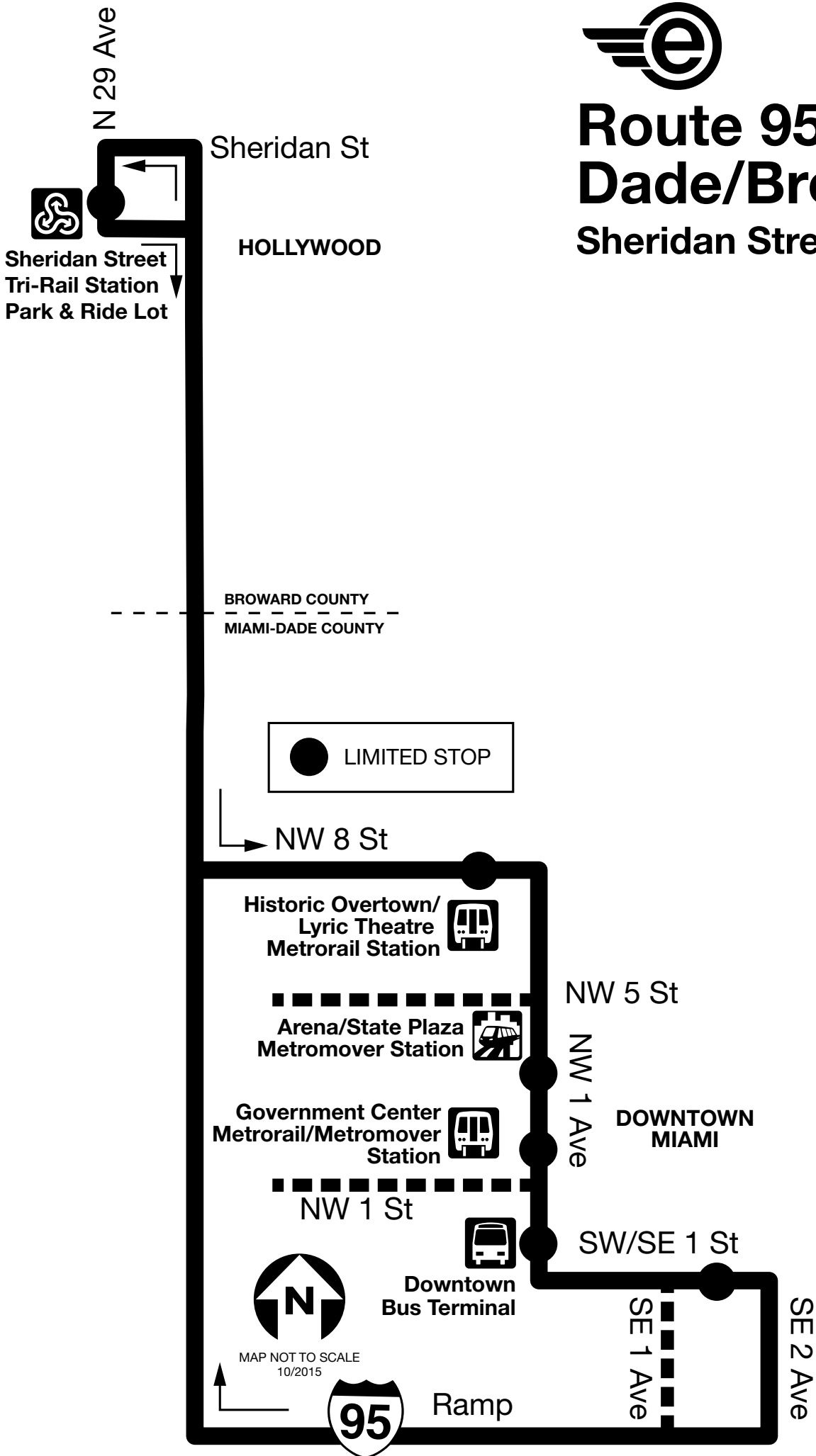
Ramp

SE 1 Ave

SE 2 Ave

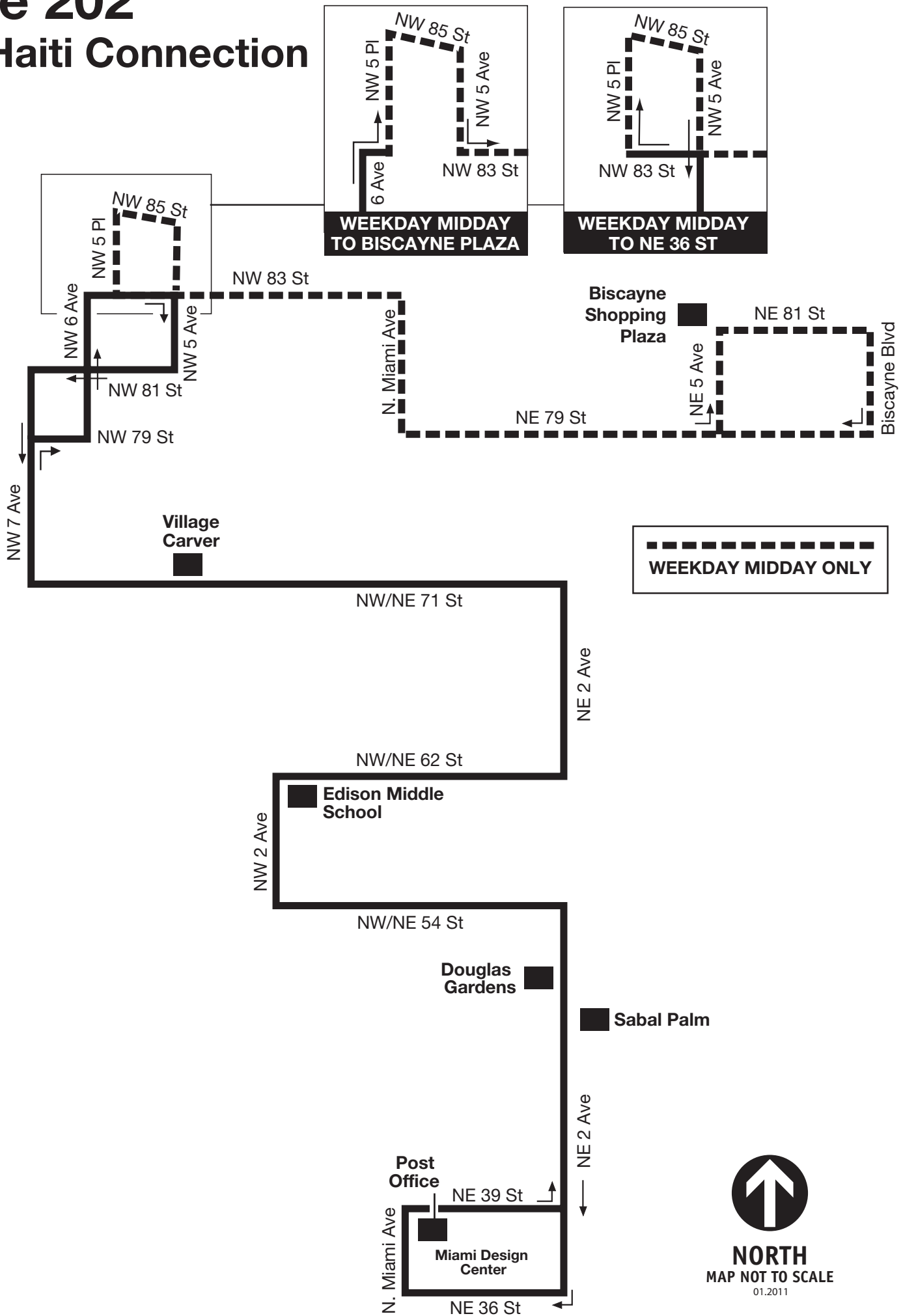


Route 95 Express Dade/Broward Sheridan Street



Route 202

Little Haiti Connection

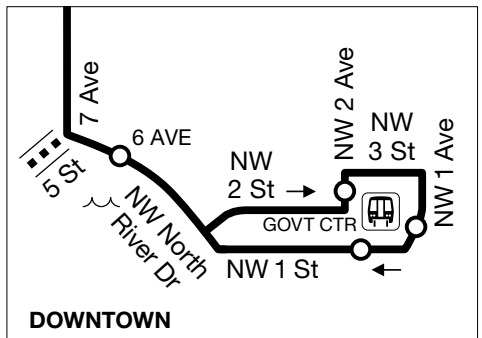
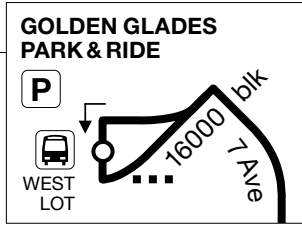
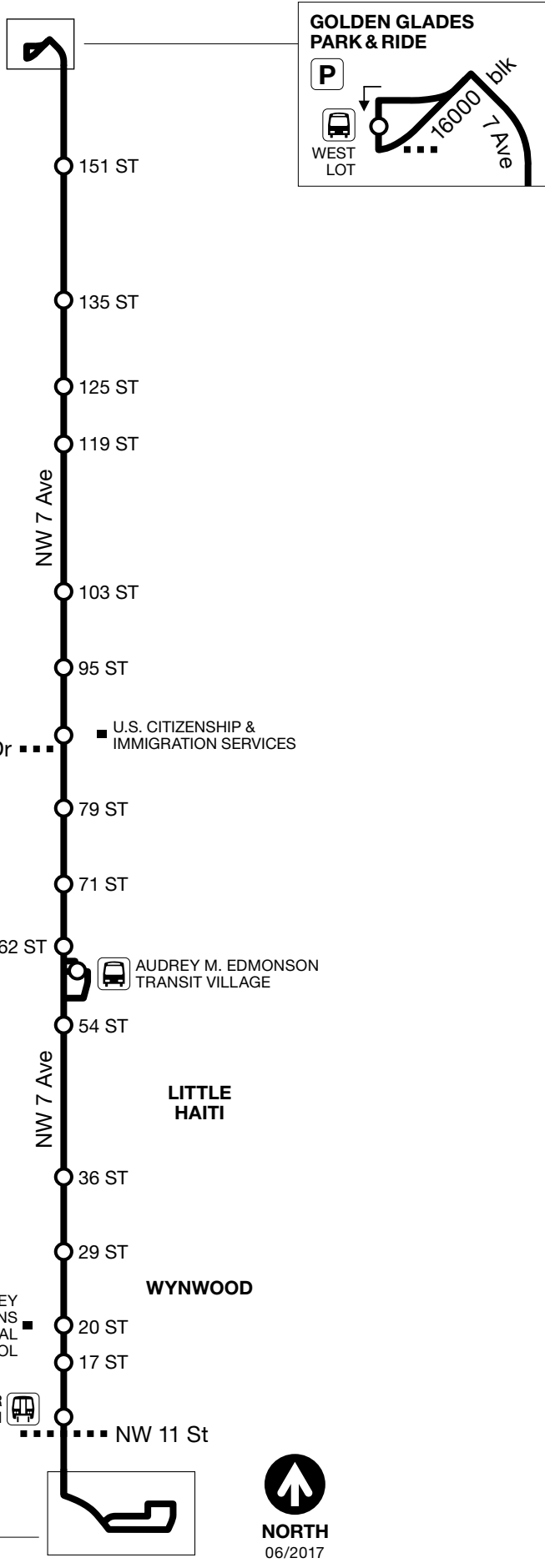




277

NW 7TH AVENUE MAX

LIMITED STOPS
entire route



NORTH
06/2017

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MDT TRACKER | EASY PAY MIAMI | MDT TRANSIT WATCH

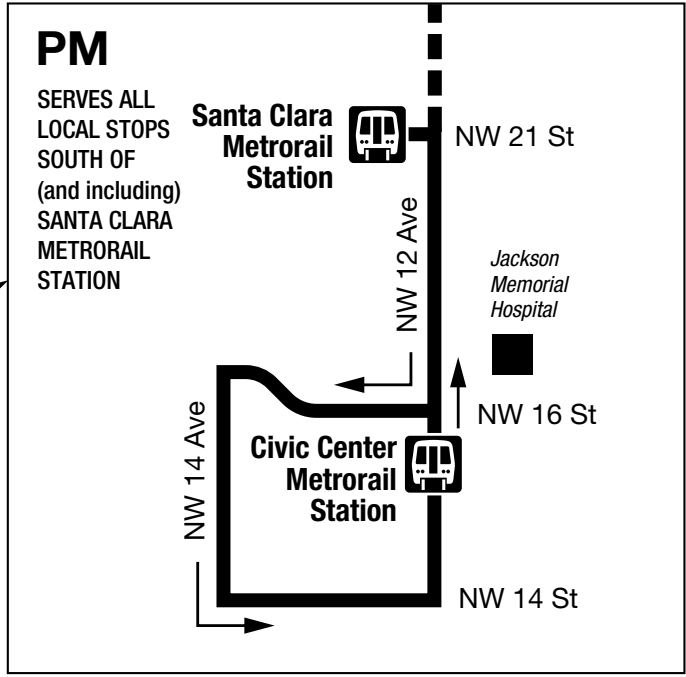
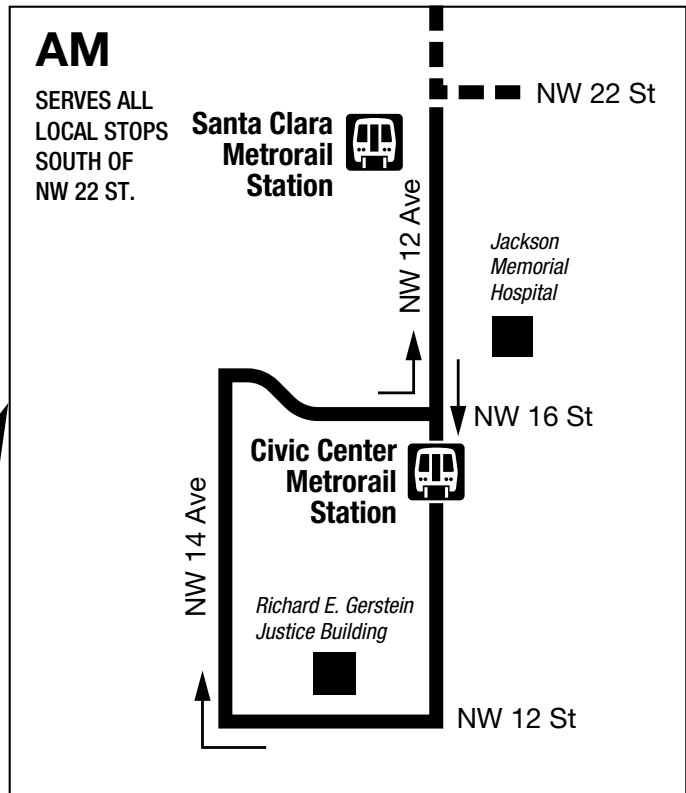
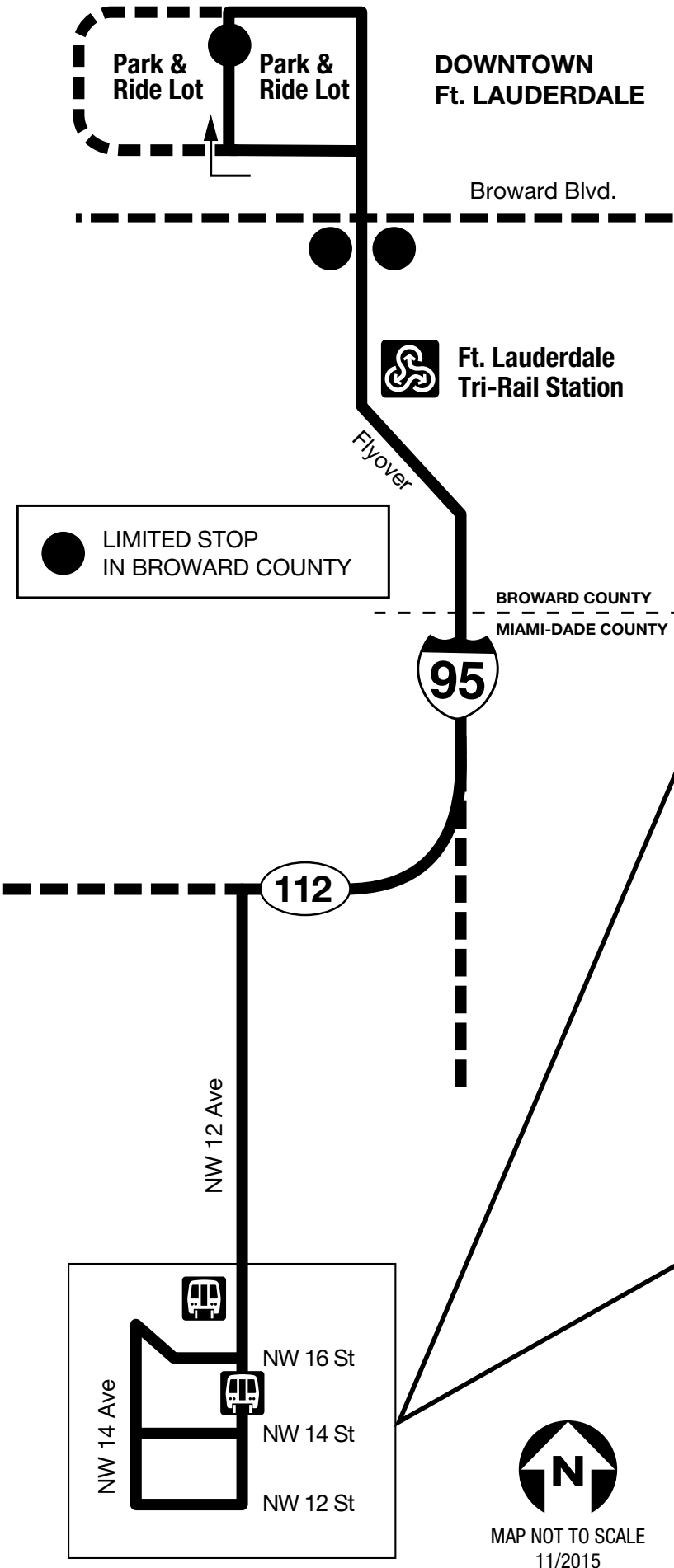
INFORMATION : INFORMACION : ENFOMASYON
311 OR 305.468.5900 (TDD: 305.468.5402)





Route 95 Express Dade/Broward

Broward Boulevard to Civic Center

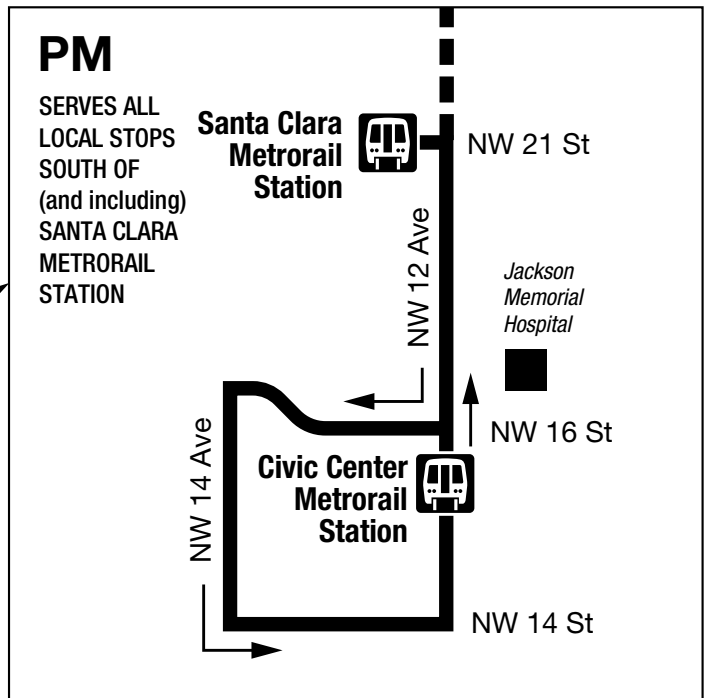
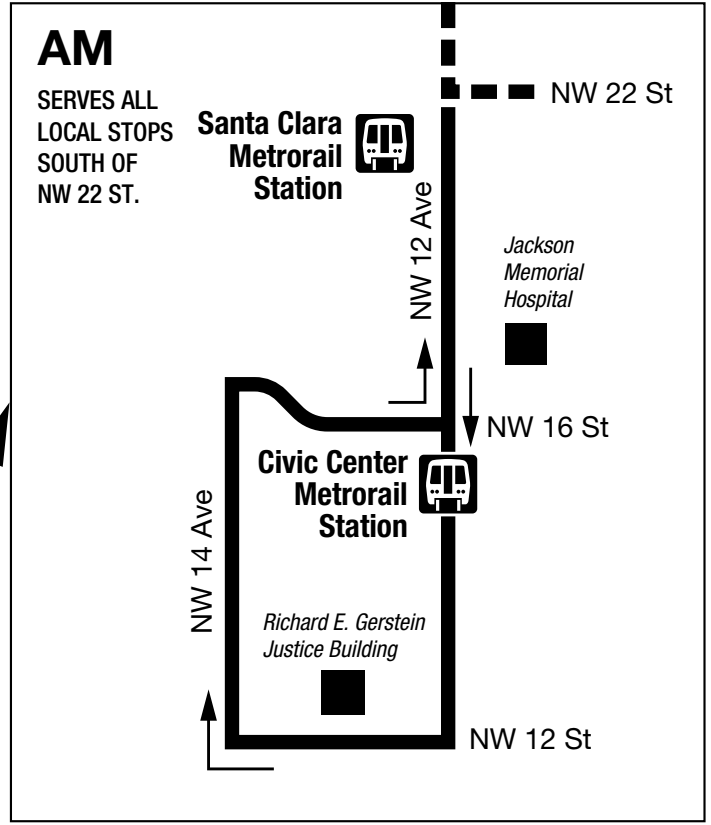
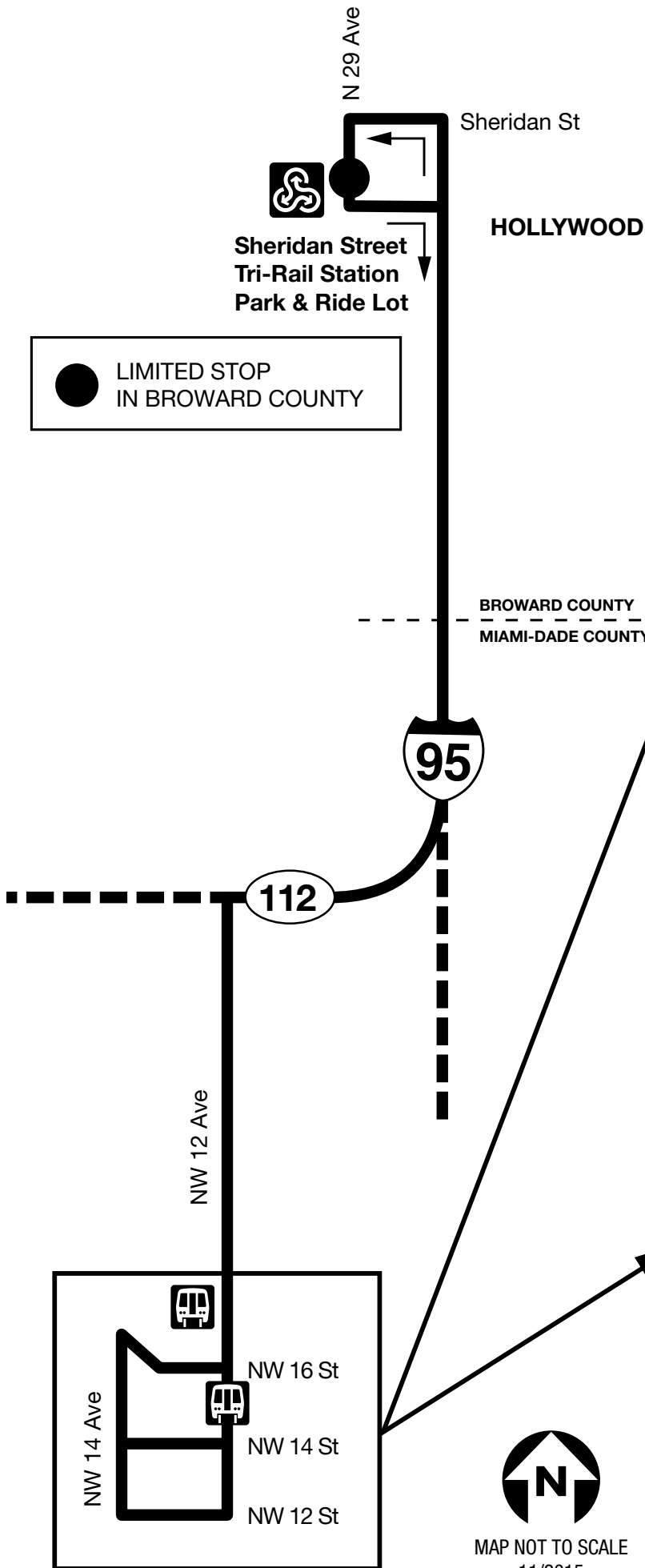


MAP NOT TO SCALE
11/2015



Route 95 Express Dade/Broward

Sheridan Street to Civic Center



MAP NOT TO SCALE
11/2015

MDT RIDERSHIP TECHNICAL REPORT

Metrobus



March 2018

METROBUS MONTHLY AND AVERAGE DAILY BOARDINGS BY ROUTE

Route	Average Weekday	Average Weekday	Percent Change	Average Saturday	Average Saturday	Percent Change	Average Sunday	Average Sunday	Percent Change	Total Monthly	Total Monthly	Percent Change
	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17
1 (*)	806	548	47.1%	720	416	73.0%	563	309	82.3%	23,589	15,504	52.1%
2	2,312	2,622	-11.8%	1,470	1,520	-3.2%	1,064	1,099	-3.2%	62,477	70,774	-11.7%
3	5,380	6,423	-16.2%	6,149	6,646	-7.5%	4,545	5,096	-10.8%	167,288	194,698	-14.1%
7	3,377	3,982	-15.2%	2,135	2,504	-14.7%	1,373	1,686	-18.6%	90,470	108,342	-16.5%
8	4,403	5,987	-26.5%	2,315	3,493	-33.7%	1,541	1,793	-14.1%	114,610	158,854	-27.9%
9	5,385	6,168	-12.7%	3,365	3,719	-9.5%	2,579	2,491	3.5%	145,622	166,707	-12.6%
10	2,211	2,745	-19.4%	1,916	2,071	-7.5%	1,410	1,432	-1.5%	63,869	77,152	-17.2%
11	8,157	10,018	-18.6%	6,190	7,486	-17.3%	4,389	5,643	-22.2%	227,959	282,938	-19.4%
12	2,546	2,642	-3.6%							56,015	60,755	-7.8%
16	1,904	2,640	-27.9%	1,651	2,049	-19.4%	1,003	1,155	-13.2%	54,147	73,525	-26.4%
17	3,815	4,701	-18.8%	2,396	3,012	-20.5%	1,699	2,008	-15.4%	102,697	128,194	-19.9%
19	1,872	2,180	-14.1%							41,177	50,143	-17.9%
21	1,756	1,816	-3.3%							38,640	41,758	-7.5%
22	3,867	4,524	-14.5%	2,286	2,699	-15.3%	1,732	1,836	-5.6%	103,424	122,192	-15.4%
24	2,060	2,101	-1.9%	1,342	1,333	0.7%	1,080	1,024	5.4%	56,352	57,741	-2.4%
27	7,350	8,531	-13.8%	5,005	5,624	-11.0%	3,378	3,692	-8.5%	200,231	233,477	-14.2%
29 (*)	612	833	-26.5%							13,463	19,159	-29.7%
31 - BUSWAY LOCAL	1,338	1,607	-16.7%	935	954	-2.0%	626	860	-27.2%	36,623	44,220	-17.2%
32	2,355	2,591	-9.1%	1,079	970	11.3%	816	806	1.1%	60,465	66,705	-9.4%
33	1,592	1,751	-9.1%	997	873	14.3%	583	658	-11.4%	42,330	46,390	-8.8%
34-EXPRESS	2,538	2,701	-6.0%							55,838	62,134	-10.1%
35	2,212	2,463	-10.2%	949	1,086	-12.6%	597	587	1.7%	55,803	63,345	-11.9%
36	2,110	2,337	-9.7%	1,380	965	43.1%	1,060	1,027	3.2%	57,555	61,720	-6.7%
37	3,351	3,193	4.9%	2,225	1,843	20.7%	1,711	1,904	-10.1%	91,687	88,434	3.7%
38	6,879	7,247	-5.1%	5,367	5,824	-7.9%	4,312	4,448	-3.1%	195,414	207,776	-5.9%

METROBUS MONTHLY AND AVERAGE DAILY BOARDINGS BY ROUTE

Route	Average Weekday	Average Weekday	Percent Change	Average Saturday	Average Saturday	Percent Change	Average Sunday	Average Sunday	Percent Change	Total Monthly	Total Monthly	Percent Change
	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17
40	1,864	2,078	-10.3%	753	907	-17.0%	506	590	-14.2%	46,803	53,788	-13.0%
42	887	1,322	-33.0%	646	553	16.8%	413	413	-0.1%	24,382	34,276	-28.9%
46 - LIBERTY CITY CONN. (*)	22	52	-57.1%							488	1,189	-59.0%
51 - FLAGLER MAX	2,975	3,139	-5.2%							65,446	72,201	-9.4%
52	1,348	1,483	-9.1%	693	698	-0.8%	525	556	-5.5%	35,220	39,133	-10.0%
54	2,810	3,000	-6.4%	1,792	1,486	20.6%	1,159	1,271	-8.8%	75,406	80,034	-5.8%
55 MARLINS SHUTTLE	120			11						250		
56	404	609	-33.8%							8,877	14,013	-36.7%
57	440	519	-15.2%							9,684	11,940	-18.9%
62	1,889	2,752	-31.4%	1,346	1,437	-6.3%	837	1,149	-27.2%	51,640	73,647	-29.9%
70	389	1,334	-70.8%	300	726	-58.7%	182	693	-73.8%	10,789	36,356	-70.3%
71	832	944	-11.9%	310	342	-9.3%	180	231	-21.8%	20,583	24,007	-14.3%
72	861	926	-7.0%	486	568	-14.4%	303	283	7.1%	22,580	24,695	-8.6%
73	2,125	2,502	-15.0%	952	1,140	-16.5%	466	543	-14.3%	53,383	64,274	-16.9%
75	1,236	1,685	-26.6%	480	616	-22.1%	232	276	-16.1%	30,518	42,325	-27.9%
77	7,707	9,252	-16.7%	4,572	5,067	-9.8%	3,054	3,234	-5.6%	204,625	245,997	-16.8%
79	181	243	-25.5%							3,984	5,593	-28.8%
82 - WESTCHESTER CIRC. (*)	58	69	-16.4%							1,266	1,583	-20.0%
87	1,547	1,741	-11.2%	539	582	-7.4%	345	363	-5.1%	38,097	43,832	-13.1%
88	2,131	2,506	-15.0%	1,793	2,049	-12.5%	1,182	1,359	-13.0%	60,576	71,275	-15.0%
93 - BISCAYNE MAX	3,577	3,887	-8.0%							78,704	89,409	-12.0%
95 EXPRESS	2,020	2,000	1.0%							44,448	46,010	-3.4%
99	1,641	1,957	-16.2%	1,047	1,124	-6.8%	624	673	-7.3%	43,823	52,197	-16.0%
101 - A (*)	134	117	14.6%	57	54	4.8%	25	32	-22.5%	3,334	3,037	9.8%
102 - B	1,641	1,729	-5.0%	874	912	-4.2%	864	968	-10.7%	43,935	47,277	-7.1%
103 - C	429	2,441	-82.4%	289	1,791	-83.9%	200	1,628	-87.7%	11,674	69,813	-83.3%

METROBUS MONTHLY AND AVERAGE DAILY BOARDINGS BY ROUTE

Route	Average Weekday	Average Weekday	Percent Change	Average Saturday	Average Saturday	Percent Change	Average Sunday	Average Sunday	Percent Change	Total Monthly	Total Monthly	Percent Change
	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17
104	994	1,191	-16.5%	512	589	-13.1%	278	376	-26.2%	25,542	31,246	-18.3%
105 - E	1,238	1,467	-15.6%	720	743	-3.0%	569	597	-4.8%	33,104	39,105	-15.3%
107 - G	1,673	1,993	-16.1%	1,301	1,421	-8.4%	858	1,017	-15.6%	46,742	55,597	-15.9%
108 - H	459	1,576	-70.9%	414	1,230	-66.3%	347	989	-64.9%	13,556	45,133	-70.0%
110 - J	2,409	2,139	12.6%	1,818	1,468	23.9%	1,363	1,381	-1.2%	67,535	60,594	11.5%
112 - L	7,353	7,528	-2.3%	6,150	4,698	30.9%	4,560	4,397	3.7%	210,756	209,523	0.6%
113 - M	773	663	16.6%	499	294	69.4%	362	372	-2.6%	20,954	17,913	17.0%
115	122	413	-70.6%	93	196	-52.5%	68	212	-68.2%	3,409	11,123	-69.4%
119 - S	8,863	8,615	2.9%	7,312	5,733	27.5%	7,343	6,977	5.2%	260,908	248,992	4.8%
120 - BEACH MAX	6,256	5,630	11.1%	4,883	3,366	45.1%	3,285	3,350	-1.9%	175,188	156,362	12.0%
132 TRI-RAIL (DORAL)	16	3	545.3%							358	58	517.2%
135	1,264	1,650	-23.4%	528	642	-17.9%	443	405	9.5%	32,210	42,138	-23.6%
136	169	314	-46.3%							3,713	7,227	-48.6%
137 - WEST DADE CONN.	1,672	1,912	-12.6%	1,194	1,322	-9.7%	858	958	-10.4%	46,184	53,100	-13.0%
150 - MIAMI BEACH AIRPORT FLYER	1,810	1,719	5.3%	1,588	1,274	24.6%	1,580	1,717	-8.0%	54,082	51,496	5.0%
183	3,476	3,832	-9.3%	2,202	2,272	-3.1%	1,559	1,533	1.7%	93,725	103,357	-9.3%
195 EXPRESS - BROWARD BLVD	533	561	-5.0%							11,721	12,896	-9.1%
196 EXPRESS - SHERIDAN ST	511	484	5.5%							11,235	11,130	0.9%
200 - CUTLER BAY LOCAL	172	196	-12.2%	141	138	2.0%				4,488	5,060	-11.3%
202 - LITTLE HAITI CONN. (*)	156	198	-21.3%	53	60	-11.0%	27	70	-61.5%	3,808	5,077	-25.0%
204 KILLIAN KAT	1,076	1,316	-18.2%							23,675	30,262	-21.8%
207 (7st) - LITTLE HAVANA CIRC	968	1,089	-11.2%	1,111	905	22.8%	900	1,055	-14.7%	30,443	32,893	-7.4%
208 (8st) - LITTLE HAVANA CIRC	751	1,060	-29.2%	1,035	881	17.4%	804	789	2.0%	24,903	31,052	-19.8%
210 - SKYLAKE CIRC	288			210			172			8,064		
211 - OVERTOWN CIRC. (*)	58	91	-36.0%							1,284	2,096	-38.7%
212 - SWEETWATER CIRC. (*)	41	49	-16.1%							900	1,121	-19.7%

METROBUS MONTHLY AND AVERAGE DAILY BOARDINGS BY ROUTE

Route	Average Weekday	Average Weekday	Percent Change	Average Saturday	Average Saturday	Percent Change	Average Sunday	Average Sunday	Percent Change	Total Monthly	Total Monthly	Percent Change
	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17
217 - BUNCHE PARK CIRC	57									1,262		
238 EAST/WEST CONN.	402	17	2209.7%							8,837	400	2109.3%
246 - NIGHT OWL (*)	15	55	-72.5%	18	66	-73.1%	12	63	-81.6%	468	1,779	-73.7%
252 - CORAL REEF MAX	907	1,016	-10.7%	347	391	-11.1%	267	255	4.6%	22,756	25,952	-12.3%
254 - BROWNSVILLE CIRC. (*)	28	73	-62.2%							610	1,688	-63.9%
267 - LUDLAM LIMITED (*)	83	106	-21.9%							1,827	2,446	-25.3%
272 SUNSET KAT (*)	445	551	-19.3%							9,784	12,680	-22.8%
277 - 7th AVENUE MAX	622	688	-9.7%							13,682	15,833	-13.6%
286 - NORTH POINTE CIRC. (*)	75	106	-29.0%	51	54	-5.4%				1,905	2,646	-28.0%
287 - SAGA BAY MAX	356	393	-9.5%							7,823	9,034	-13.4%
288 - KENDALL CRUISER	827	867	-4.7%							18,189	19,951	-8.8%
295 EXPRESS - BROWARD BLVD - CIVIC CENTER	236	207	13.9%							5,191	4,763	9.0%
296 EXPRESS - SHERIDAN ST - CIVIC CENTER	184	169	8.7%							4,042	3,888	4.0%
297-27th AVENUE ORANGE MAX	1,391	1,554	-10.5%							30,595	35,745	-14.4%
301 DADE-MONROE EXPRESS (*)	757	936	-19.1%	686	942	-27.2%	616	858	-28.2%	22,555	28,725	-21.5%
302 CARD SOUND EXPRESS (*)	61	97	-36.8%	60	66	-8.1%	49	66	-25.5%	1,843	2,752	-33.0%
338 - WEEKEND EXPRESS				211	156	35.2%	210	188	11.6%	1,895	1,377	37.6%
344 (*)	106	124	-14.2%							2,332	2,841	-17.9%
500	41	49	-17.2%	36	38	-5.3%	37	37	0.7%	1,226	1,433	-14.4%
12/21 WEEKEND INTERL.				2,532	2,118	19.6%	1,972	2,085	-5.4%	20,549	16,809	22.2%
GREEN HILLS SHUTTLE	7	6	30.9%							36	22	63.6%
KINGS CREEK SHUTTLE	9	11	-21.1%							36	57	-36.8%
SIERRA LAKES SHUTTLE	20	17	18.8%							101	85	18.8%
ROBERT SHARP SHUTTLE	12	16	-25.6%							58	78	-25.6%
AHEPA SHUTTLE	24	46	-47.8%							119	228	-47.8%
FEDERATION GARDENS SHUTTLE				19	20	-4.0%				96	80	20.0%
Total	169,219	193,436	-12.5%	102,533	106,215	-3.5%	75,688	83,553	-9.4%	4,534,564	5,206,381	-12.9%

Excludes shuttles and those routes which were not run both years.

(*) March 2018 is the seventh full month during which the following routes are operated under contract: 1, 29, 46, 82, 101-A, 202, 211, 212, 246, 254, 267, 272, 286 and 344. Routes 301 and 302 are also under contracted operation. Routes 6, 48, 117, 123 and 249 were eliminated; new routes 210 and 217 were added.

March 2018

18

BUS RIDERSHIP BY ROUTE SERVICE TYPE

Boardings by Service Type and Route	Average Weekday	Average Weekday	Percent Change	Average Saturday	Average Saturday	Percent Change	Average Sunday	Average Sunday	Percent Change	Total Monthly	Total Monthly	Percent Change
	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17
Express Routes												
34-EXPRESS	2,538	2,701	-6.0%							55,838	62,134	-10.1%
95-EXPRESS	2,020	2,000	1.0%							44,448	46,010	-3.4%
150-MIAMI BEACH AIRPORT FLYER	1,810	1,719	5.3%	1,588	1,274	24.6%	1,580	1,717	-8.0%	54,082	51,496	5.0%
195 EXPRESS - BROWARD BLVD	533	561	-5.0%							11,721	12,896	-9.1%
196 EXPRESS - SHERIDAN ST	511	484	5.5%							11,235	11,130	0.9%
295 EXPRESS - BROWARD BLVD - CIVIC CENTER	236	207	13.9%							5,191	4,763	9.0%
296 EXPRESS - SHERIDAN ST - CIVIC CENTER	184	169	8.7%							4,042	3,888	4.0%
204 KAT-KILLIAN	1,076	1,316	-18.2%							23,675	30,262	-21.8%
288 KENDALL CRUISER	827	867	-4.7%							18,189	19,951	-8.8%
338 - WEEKEND EXPRESS				211	156	35.2%	210	188	11.6%	1,895	1,377	37.6%
Subtotal Express Routes	9,735	10,025	-2.9%	1,799	1,430	25.8%	1,790	1,905	-6.0%	230,316	243,907	-5.6%
Percentage of All Bus Routes	5.8%	5.2%		1.8%	1.3%		2.4%	2.3%		5.1%	4.7%	
Contracted Express Routes												
301-DADE MONROE EXPRESS	757	936	-19.1%	686	942	-27.2%	616	858	-28.2%	22,555	28,725	-21.5%
302-CARD SOUND EXPRESS	61	97	-36.8%	60	66	-8.1%	49	66	-25.5%	1,843	2,752	-33.0%
Subtotal Contracted Express Routes	819	1,033	-20.7%	746	1,007	-26.0%	665	924	-28.0%	24,398	31,477	-22.5%
Percentage of All Bus Routes	0.5%	0.5%		0.7%	0.9%		0.9%	1.1%		0.5%	0.6%	
Other Contracted Routes												
1	806	548	47.1%	720	416	73.0%	563	309	82.3%	23,589	15,504	52.1%
29	612	833	-26.5%							13,463	19,159	-29.7%
46 - LIBERTY CITY CONN.	22	52	-57.1%							488	1,189	-59.0%
82 - WESTCHESTER CIRC	58	69	-16.4%							1,266	1,583	-20.0%
101 - A	134	117	14.6%	57	54	4.8%	25	32	-22.5%	3,334	3,037	9.8%
202 - LITTLE HAITI CONN.	156	198	-21.3%	53	60	-11.0%	27	70		3,808	5,077	-25.0%
211-OVERTOWN CIRC	58	91	-36.0%							1,284	2,096	-38.7%
212-SWEETWATER CIRC	41	49	-16.1%							900	1,121	-19.7%
246 - NIGHT OWL	15	55	-72.5%	18	66	-73.1%	12	63	-81.6%	468	1,779	-73.7%
254-BROWNSVILLE CIRC	28	73	-62.2%							610	1,688	-63.9%
267-LUDLAM LIMITED	83	106	-21.9%							1,827	2,446	-25.3%
272 KAT-SUNSET	445	551	-19.3%							9,784	12,680	-22.8%
286-NORTH POINTE CIRC	75	106	-29.0%	51	54	-5.4%				1,905	2,646	-28.0%
344	106	124	-14.2%							2,332	2,841	-17.9%
Subtotal Other Contracted Routes	2,639	2,972	-11.2%	898	650	38.2%	626	473	32.4%	65,058	72,846	-10.7%
Percentage of All Bus Routes	1.6%	1.5%		0.9%	0.6%		0.8%	0.6%		1.4%	1.4%	

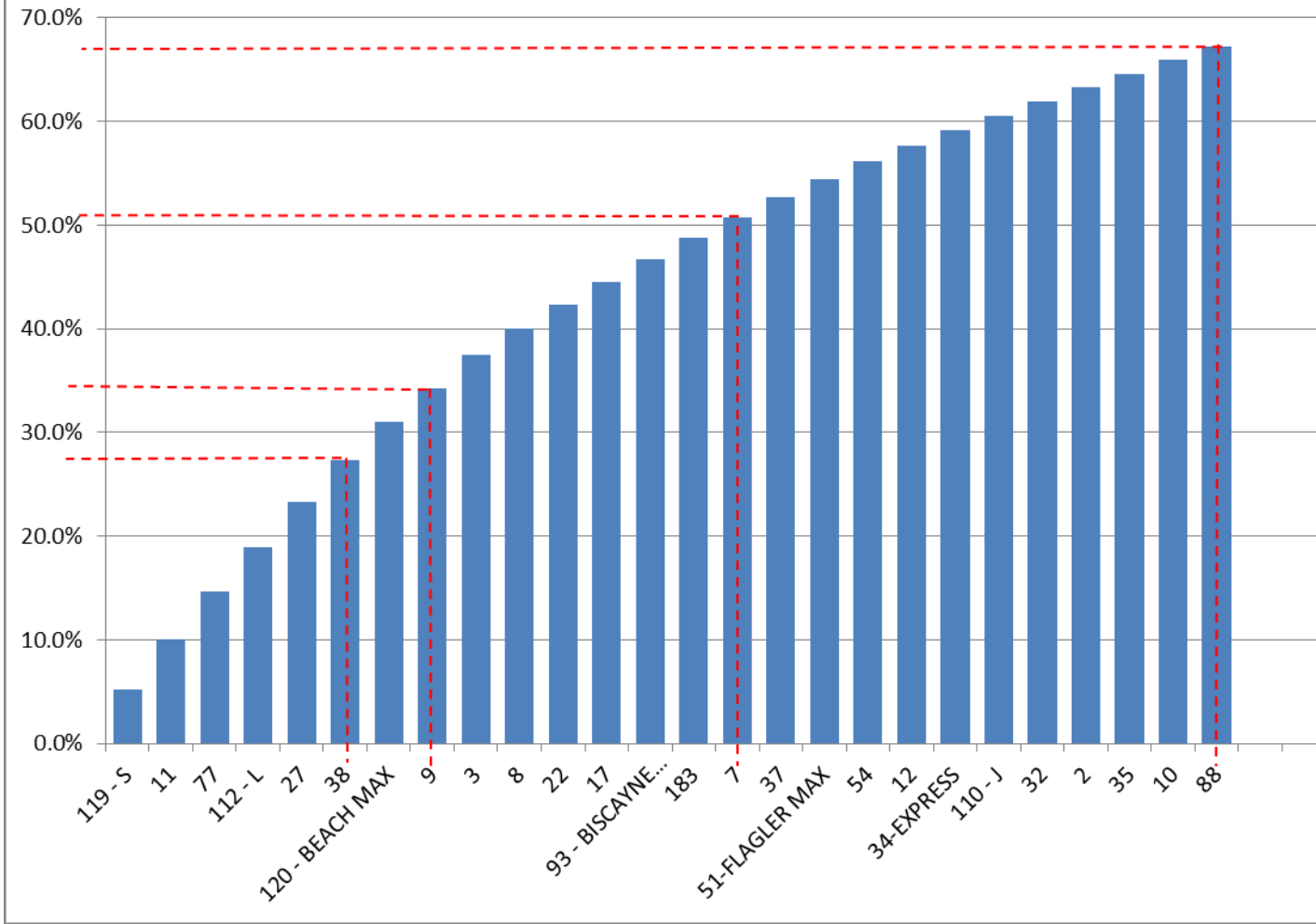
BUS RIDERSHIP BY ROUTE SERVICE TYPE

Boardings by Service Type and Route	Average Weekday	Average Weekday	Percent Change	Average Saturday	Average Saturday	Percent Change	Average Sunday	Average Sunday	Percent Change	Total Monthly	Total Monthly	Percent Change
	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17	Mar-18	Mar-17	18 vs 17
Limited Stop Routes												
51-FLAGLER MAX	2,975	3,139	-5.2%							65,446	72,201	-9.4%
93 - BISCAYNE MAX	3,577	3,887	-8.0%							78,704	89,409	-12.0%
120 - BEACH MAX	6,256	5,630	11.1%	4,883	3,366	45.1%	3,285	3,350	-1.9%	175,188	156,362	12.0%
252-CORAL REEF MAX	907	1,016	-10.7%	347	391	-11.1%	267	255	4.6%	22,756	25,952	-12.3%
277-7TH AVENUE MAX	622	688	-9.7%							13,682	15,833	-13.6%
287-SAGA BAY MAX	356	393	-9.5%							7,823	9,034	-13.4%
297-27th AVENUE ORANGE MAX	1,391	1,554	-10.5%							30,595	35,745	-14.4%
Subtotal Limited Stop Routes	16,083	16,308	-1.4%	5,230	3,757	39.2%	3,552	3,605	-1.5%	394,194	404,536	-2.6%
Percentage of All Bus Routes	9.5%	8.4%		5.1%	3.5%		4.7%	4.3%		8.7%	7.8%	
Free Route												
132 -TRI-RAIL DORAL	16	3	545.3%							358	58	517.2%
Percentage of All Bus Routes	0.0%	0.0%								0.0%	0.0%	
Lifelines & Shuttles												
55-MARLINS SHUTTLE	120			11						250		
GREEN HILLS SHUTTLE	7	6	30.9%							36	22	63.6%
KINGS CREEK SHUTTLE	9	11	-21.1%							36	57	-36.8%
SIERRA LAKES SHUTTLE	20	17	18.8%							101	85	18.8%
ROBERT SHARP SHUTTLE	12	16	-25.6%							58	78	-25.6%
AHEPA SHUTTLE	24	46	-47.8%							119	228	-47.8%
FEDERATION GARDENS SHUTTLE				19	20	-4.0%				96	80	20.0%
Subtotal Lifeline & Shuttle Routes	191	95	101.2%	30	20	51.0%				696	550	26.5%
Percentage of All Bus Routes	0.1%	0.0%		0.0%	0.0%					0.0%	0.0%	
Other Circulator Routes												
200-CUTLER BAY LOCAL	172	196	-12.2%	141	138	2.0%				4,488	5,060	-11.3%
210 - SKYLAKE CIRC	288			210			172			8,064		
217 - BUNCHE PARK CIRC	57									1,262		
Subtotal Circulators	517	196	163.7%	351	138	154.5%	172			13,814	5,060	173.0%
Percentage of All Bus Routes	0.3%	0.1%		0.3%	0.1%		0.2%			0.3%	0.1%	
All Other (Regular) Routes	139,218	162,805	-14.5%	93,479	99,213	-5.8%	68,884	76,647	-10.1%	3,805,730	4,447,947	-14.4%
Percentage of All Bus Routes	82.3%	84.2%		91.2%	93.4%		91.0%	91.7%		83.9%	85.4%	
Total Bus System (1)	169,219	193,436	-12.5%	102,533	106,215	-3.5%	75,688	83,553	-9.4%	4,534,564	5,206,381	-12.9%

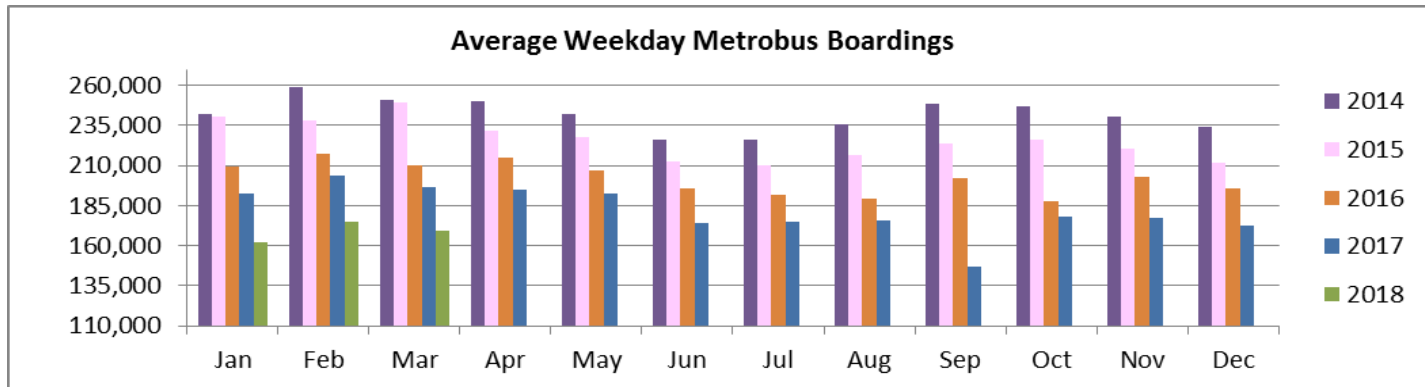
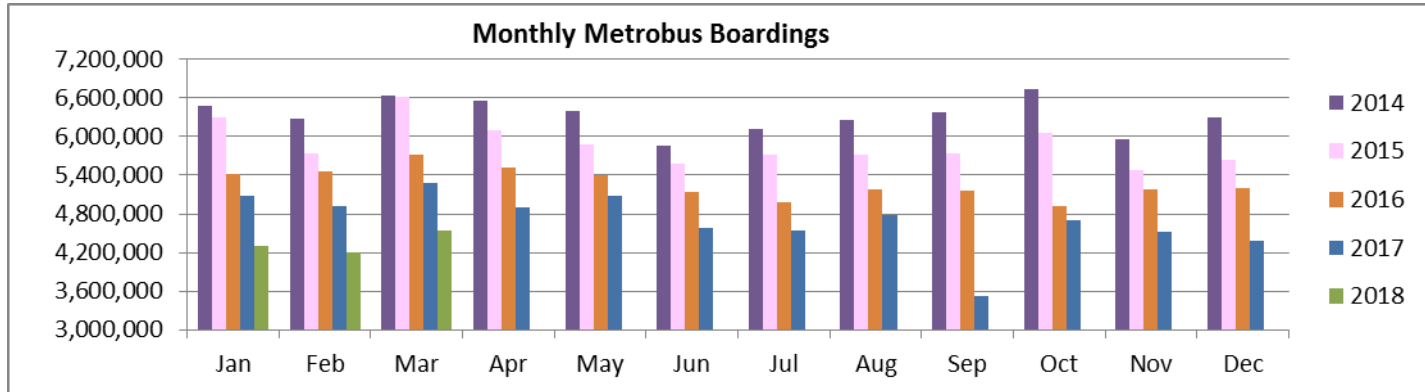
Excludes shuttles and those routes which were not run both years.

(* March 2018 is the seventh full month during which the following routes are operated under contract: 1, 29, 46, 82, 101-A, 202, 211, 212, 246, 254, 267, 272, 286 and 344. Routes 301 and 302 are also under contracted operation. Routes 6, 48, 117, 123 and 249 were eliminated; new routes 210 and 217 were added.

**Bus - Cumulative Percentage Average Weekday Ridership Share
Top 1/4, 1/3, 1/2 and 2/3 of Routes**



TOTAL MONTHLY AND AVERAGE WEEKDAY RIDERSHIP YEAR OVER YEAR COMPARISON CHARTS – METROBUS



**“TOP 5 / BOTTOM 5” BUS ROUTES AVERAGE WEEKDAY RIDERSHIP PERFORMANCE
YEAR OVER YEAR COMPARISONS**

Route	Mar-18	Mar-17	Absolute Delta (1)	Percent Delta
120 - BEACH MAX	6,256	5,630	626	11.1%
110 - J	2,409	2,139	269	12.6%
1	806	548	258	47.1%
119 - S	8,863	8,615	247	2.9%
37	3,351	3,193	158	4.9%
103 - C	429	2,441	-2,012	-82.4%
11	8,157	10,018	-1,861	-18.6%
8	4,403	5,987	-1,584	-26.5%
77	7,707	9,252	-1,545	-16.7%
27	7,350	8,531	-1,181	-13.8%

(1) Averages rounded to the nearest boarding

(*) Contracted route



APPENDIX D – TRAFFIC DATA SUPPORTING INFORMATION

- TRAFFIC DATA COLLECTION REPORT
 - PEAK SPREADING REVIEW
 - TRAFFIC SIGNAL TIMING PLANS

TRAFFIC DATA COLLECTION REPORT

Interstate 195 Corridor Planning Study from I-95/N.W. 12th Avenue to Alton Road (FM No. 440228-1-22-01)



Florida Department of Transportation
District 6
Adam Leigh Cann Building
1000 NW 111th Avenue
Miami, FL 33172

TABLE OF CONTENTS

TABLE OF CONTENTS	i
LIST OF TABLES	i
LIST OF FIGURES	i
LIST OF Appendices.....	ii
1. Introduction	3
2. Traffic Data Collection	5
3. Origin-Destination Data Collection	7
4. Bluetooth Data Collection	21
4.1. Bluetooth Technology.....	21
4.2. Bluetooth Data Adjustment.....	22
5. Origin-Destination Matrices.....	23

LIST OF TABLES

Table 1: I-195 O-D (Bluetooth Data Collection between 10/24/2017 and 10/26/2017) – AM Peak Period	24
Table 2: I-195 O-D (Bluetooth Data Collection between 10/24/2017 and 10/26/2017) – PM Peak Period.....	25
Table 3: I-195 Average Travel Time (seconds) - Bluetooth Data Collection between 10/24/2017 and 10/26/2017 – AM Peak Period	33
Table 4: I-195 Average Travel Time (seconds) - Bluetooth Data Collection between 10/24/2017 and 10/26/2017 – PM Peak Period	34

LIST OF FIGURES

Figure 1. Project Location Map.....	4
Figure 2.A. Inset D , Volume Count Locations	8
Figure 2.B. Inset C , Volume Count Locations.....	9
Figure 2.C. Inset B , Volume Count Locations.....	10
Figure 2.D. Inset A , Volume Count Locations	11
Figure 3.A. Bluetooth and Vehicle Classification Data Collection.....	12
Figure 3.B. Inset A, Volume Counts and TMCs (AM Peak)	13
Figure 3.C. Inset A, Volume Counts and TMCs (PM Peak).....	14
Figure 3.D. Inset B, Volume Counts and TMCs (AM Peak)	15
Figure 3.E. Inset B, Volume Counts and TMCs (PM Peak).....	16
Figure 3.F. Inset C, Volume Counts and TMCs (AM Peak).....	17
Figure 3.G. Inset C, Volume Counts and TMCs (PM Peak).....	18
Figure 3.H. Inset D, Volume Counts and TMCs (AM Peak).....	19
Figure 3.I. Inset D, Volume Counts and TMCs (PM Peak).....	20
Figure 4. How Bluetooth Technology Works with BlueTOAD’s.....	21
Figure 5. O-D and Travel Time Data Collection B5 and B12 – AM Peak Hour.....	26
Figure 6. O-D and Travel Time Data Collection B5 and B12 – PM Peak Hour	27

Figure 7. O-D and Travel Time Data Collection B2 and B9 – AM Peak Hour28
Figure 8. O-D and Travel Time Data Collection B2 and B9 – PM Peak Hour29
Figure 9. O-D and Travel Time Data Collection B3 and B8 – AM Peak Hour30
Figure 10. O-D and Travel Time Data Collection B3 and B8 – PM Peak Hour.....31

LIST OF Appendices

Appendix A – Raw Data

Appendix B – Raw Bluetooth Counts

Appendix C – Bluetooth Matrix Adjustment Procedure

1. Introduction

Interstate 195 (I-195) is an important limited access facility in Miami-Dade County area which connects significant trip generation sources such as Miami International Airport (MIA) to Miami Beach area and crosses Interstate 95 (I-95) which is a major interstate facility in South Florida area. I-195 is one of the two limited access facilities in Miami-Dade County which connects the mainland to barrier Island. Several nearby neighborhoods such as the Design District, Midtown and Wynwood experienced significant growth which will result in the growth in travel demand in near future. Geometric expansions opportunities are limited along the corridor considering the existing Right-of-Way (ROW). The study area along I-195 starts from the interchange of I-195 and N.W. 12th Avenue and ends at the interchange of I-195 and SR-907/Alton Road. Project location map can be seen in **Figure 1**.

Florida Department of Transportation (FDOT) is evaluating the existing condition and deficiencies of this corridor in order to develop and evaluate alternatives concepts based on that. The study includes the evaluation of study interchanges, interchange influence areas, and ramp junctions to identify deficiencies focusing on reoccurring bottlenecks and development of a series of proposed improvements to address short-term and long-term demands of the corridor. Alternatives including option to enhance mainline operation will be considered. In addition, the feasibility of providing bicycle/pedestrian facilities for providing a connection from City of Miami Beach to the City of Miami will be explored. To analyze the existing condition and also setup a base for future condition, traffic data have been collected in the study area. This memorandum describes data collection efforts undertaken to quantify existing conditions within the study limit along the I-195 corridor. This memorandum will cover the effort to collect following data:

- Origin-Destination data using 72-hour Bluetooth Counts (12 Stations)
- 72-Hour traffic station volumes (at 32 locations)
- 5-Hour Turning Movement Counts (TMC at 28 locations)
- 72-Hour classification stations (at 5 locations)

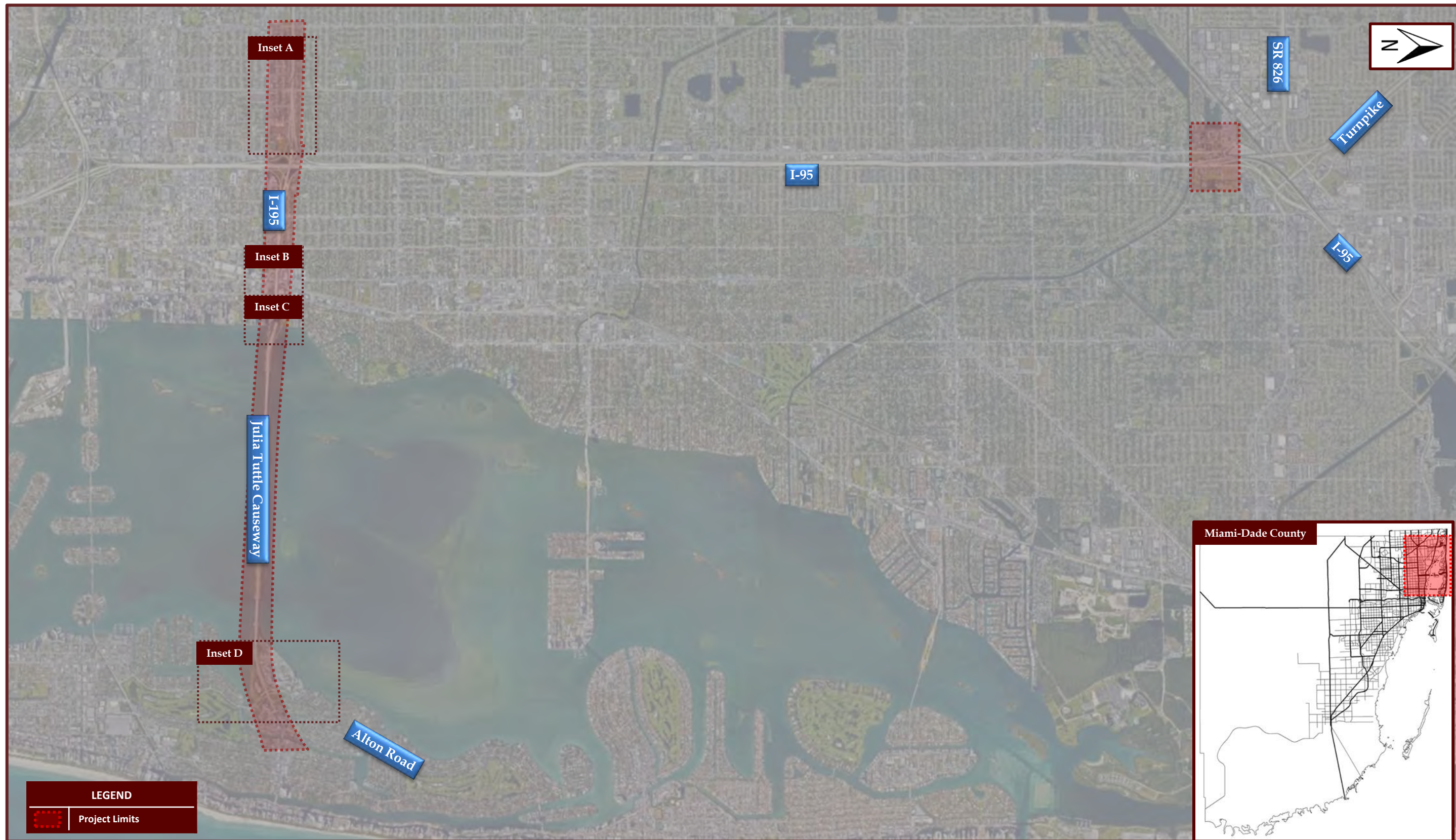


Figure 1. Project Location Map

2. Traffic Data Collection

72-hour traffic station volumes were collected at 32 following locations (**Figure 2.A to Figure 2.D** show location map of the traffic count stations). Insets in the figure caption refer to **Figure 1**.

1. Alton Road, North of North Bay Road and South of W 44th Street
2. Alton Road, North of Nautilus Road
3. 41st Street, East of Alton Road
4. Ramp 87037202
5. Southbound Alton Road to Westbound I-195
6. Ramp from Mount Sinai Medical Center to Westbound I-195
7. Northbound Alton Road, 300' South of I-195 and about 1400' North of Chase Ave
8. Southbound Alton Road, South of I-195
9. Ramp 87037201: Ramp 87004024 to Northbound Alton Road
10. Ramp 87004024: Eastbound I-195 to Alton Road
11. Alton Road, North of Chase Avenue
12. Ramp 87004023: Northeast 36th Street to Eastbound I-195
13. Ramp 87004022: Westbound I-195 to Northeast 38th Street
14. Ramp 87004021: Eastbound I-195 to Westbound Northeast 36th Street
15. Ramp 87004020: Northeast 38th Street to Westbound I-195
16. US-1, North of Northeast 38th Street
17. US-1, South of Northeast 36th Street (Under Construction)
18. Northeast 36th Street, East of Federal Highway
19. Northeast 2nd Avenue, North of Northeast 39th Street
20. Northeast 39th Street West of Northwest 2nd Avenue
21. Northeast 39th Street East of Northeast 2nd Avenue
22. Northeast 2nd Avenue South of Northeast 39th Street
23. Northeast 36th Street, East of Northeast 1st Avenue
24. Northeast 1st Avenue, North of Northeast 38th Street
25. Northeast 38th Street, West of Northeast 1st Avenue
26. North Miami Avenue, North of Northeast 38th Street
27. North Miami Avenue, North of Northeast 36th Street
28. Northeast 36th Street, East of North Miami Avenue
29. Northeast 36th Street, West of North Miami Avenue
30. Ramp 87004018: North Miami Avenue to Westbound I-195
31. Ramp 87004019: Eastbound SR-112 to North Miami Avenue
32. North Miami Avenue, North of Northeast 34th Street

72-hour classification counts were performed at five following stations:

1. SR-112, East of Northwest 2nd Avenue
2. SR-112, East of US-1
3. SR-112, East of Intercoastal Waterway Bridge
4. SR-112, East of Biscayne Bay Bridge
5. SR-112, West of Alton Road

5-hour Turning Movement Counts (TMCs) were conducted at following 28 locations (**Figure 2.A to Figure 2.D** show location map of the TMC location). Insets in the figure caption refer to **Figure 1**.

1. Northwest 12nd Avenue & Northwest 40th Street
2. Northwest 12nd Avenue & Northwest 39th Street
3. Northwest 10th Avenue & Northwest 39th Street
4. North Miami Avenue & Northwest 36th Street
5. North Miami Avenue & SR-112 Eastbound Off-Ramp
6. North Miami Avenue & Northwest 38th Street
7. Northeast 1st Avenue & Northeast 36th Street
8. Northeast 1st Avenue & Northeast 38th Street
9. North Federal Highway & Northeast 36th Street
10. Northeast 2nd Avenue & Northeast 38th Street
11. Northeast 2nd Avenue & Northeast 39th Street
12. North Federal Highway & Northeast 39th / 38th Street
13. US-1 Biscayne Blvd & Northeast 36th Street
14. US-1 Biscayne Blvd & Northeast 38th Street
15. Northeast 5th Avenue & Northeast 36th Street
16. Northeast 6th Avenue & Northeast 38th Street
17. Alton Road and Chase Avenue
18. Alton Road and West 34th Street
19. SR-907 and SR-112
20. Alton Road and Barry Street
21. Alton Road and West 39th Street
22. Alton Road and 41st Street
23. Alton Road and Nautilus Road
24. SR-907 and SR-112 Ramps
25. North Bay Road
26. Alton Road and 43rd Street

27. Mount Sinai Emergency Entrance and Sullivan Drive
28. Alton Road and North Bay Road

Raw TMCs and traffic station counts and classification counts can be found in **Appendix A**. Also the summary of collected data can be seen in **Figure 3**. Insets in the figures captions refer to **Figure 1**.

3. Origin-Destination Data Collection

The Origin-Destination survey is intended to capture empirically, the route choice of vehicles between origins and destinations within or outside the study area. Bluetooth Origin-Destination method of data collection is used in this study to capture the travel pattern along the I-195.

CALTRAN worked with Florida Department of Transportation (FDOT) to identify placement locations for the BlueTOAD devices (Bluetooth Travel-time Origin and Destination - the Bluetooth device of TrafficCast International©) in order to capture trips entering/exiting study interchange. The Origin-Destination data collection stations are shown in **Figure 2**. Two devices were installed along I-95 close to Golden Glades Interchange in order to capture the traffic which takes I-195 in order to move between Miami Beach and North Miami Dade toward Broward County. 72-hour traffic station volumes were collected at locations where BlueTOADs were installed. The count data will be used in order to expand and adjust raw O-D data.

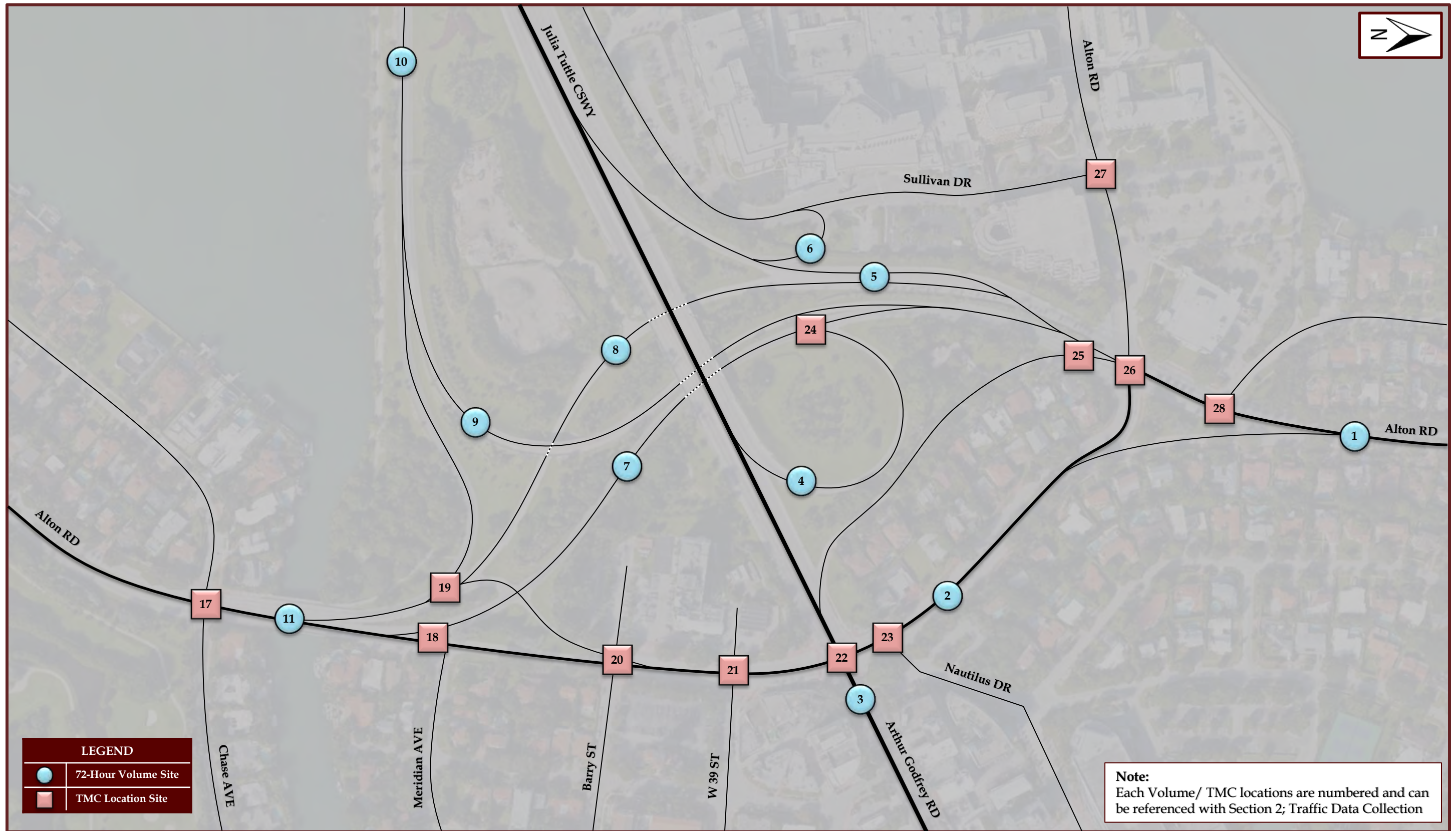


Figure 2.A. Inset D , Volume Count Locations



Figure 2.B. Inset C , Volume Count Locations



Figure 2.C. Inset B , Volume Count Locations



Figure 2.D. Inset A , Volume Count Locations

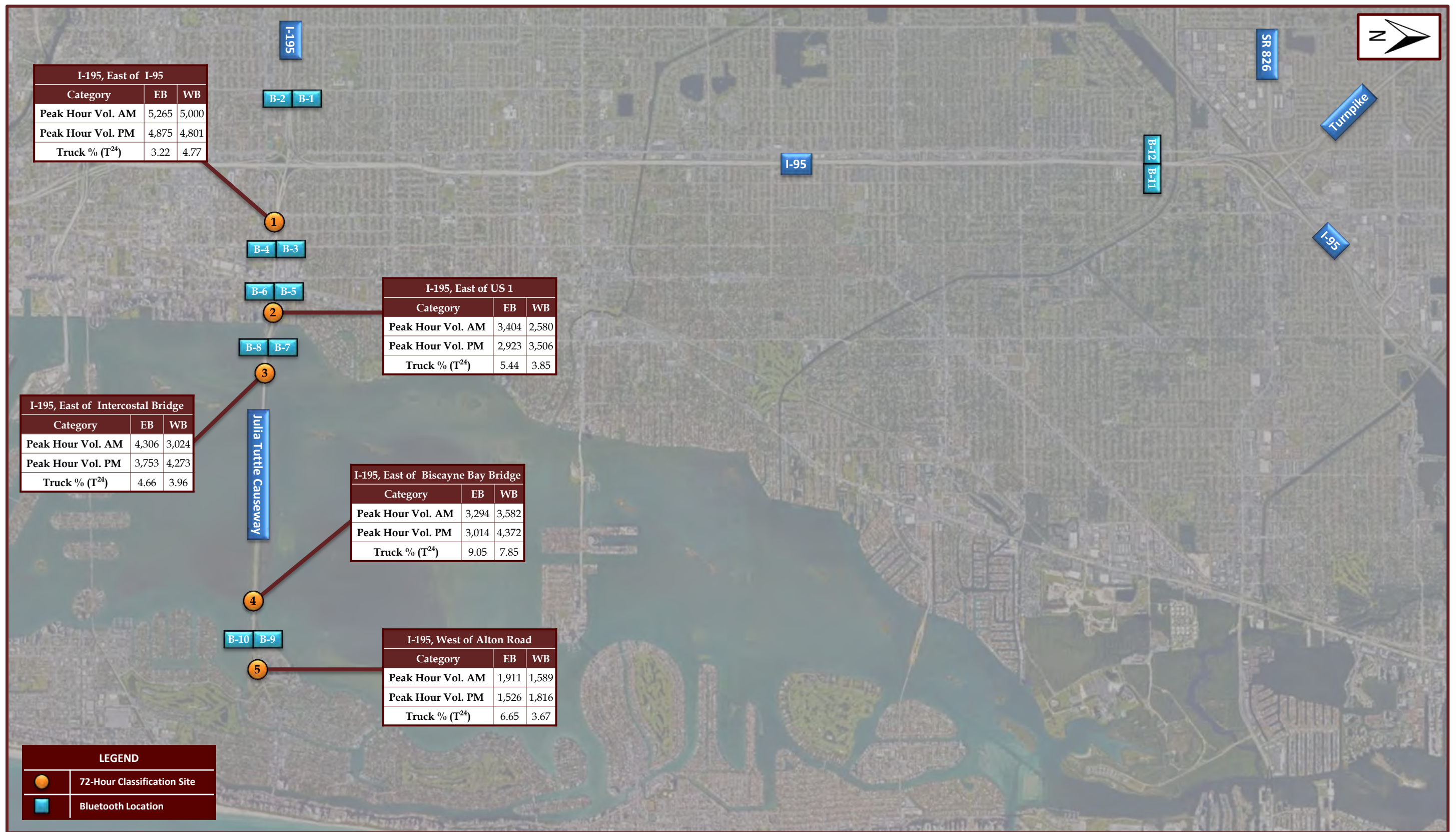


Figure 3.A. Bluetooth and Vehicle Classification Data Collection

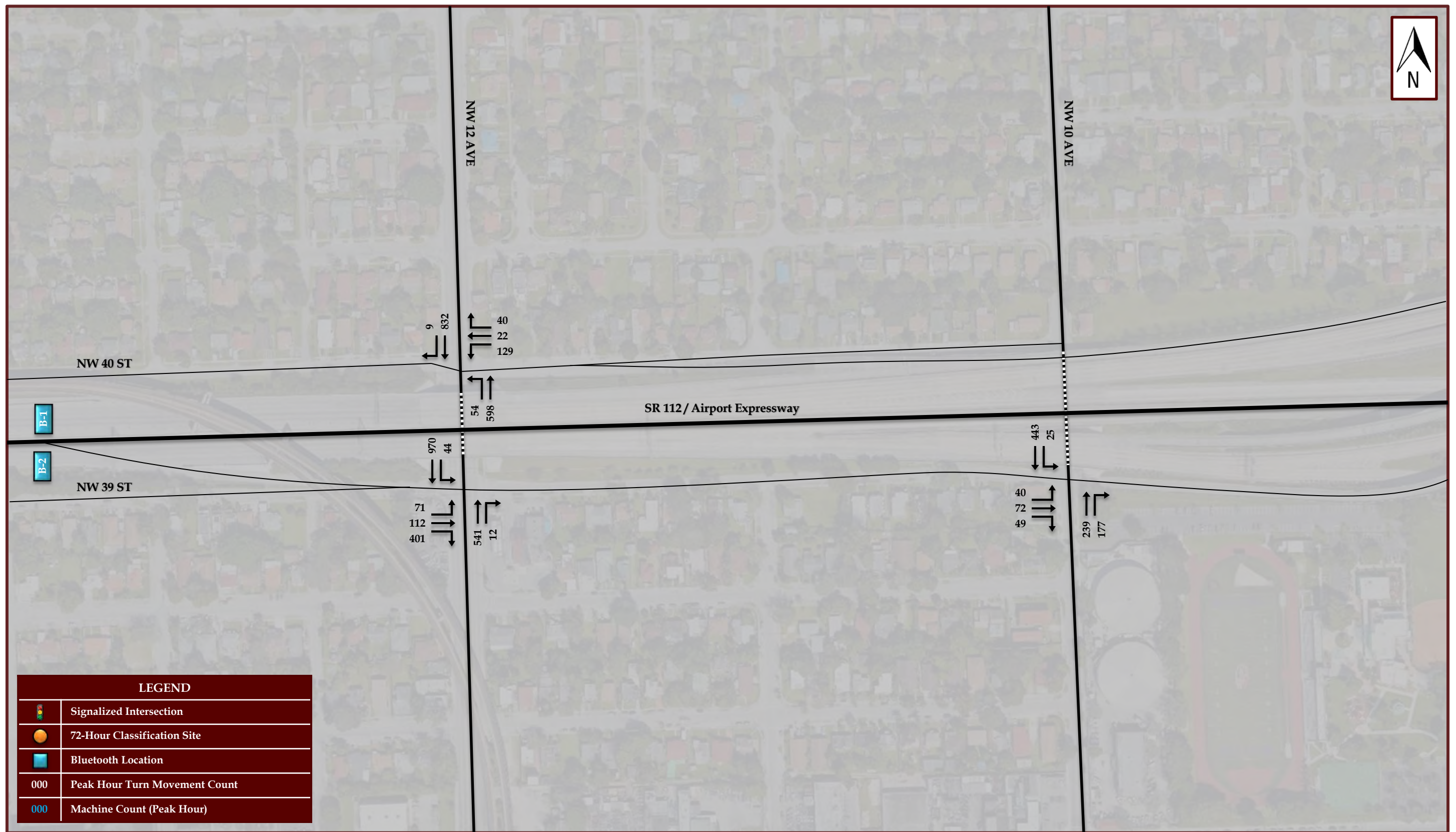


Figure 3.B. Inset A , Volume Counts and TMCs (AM Peak)

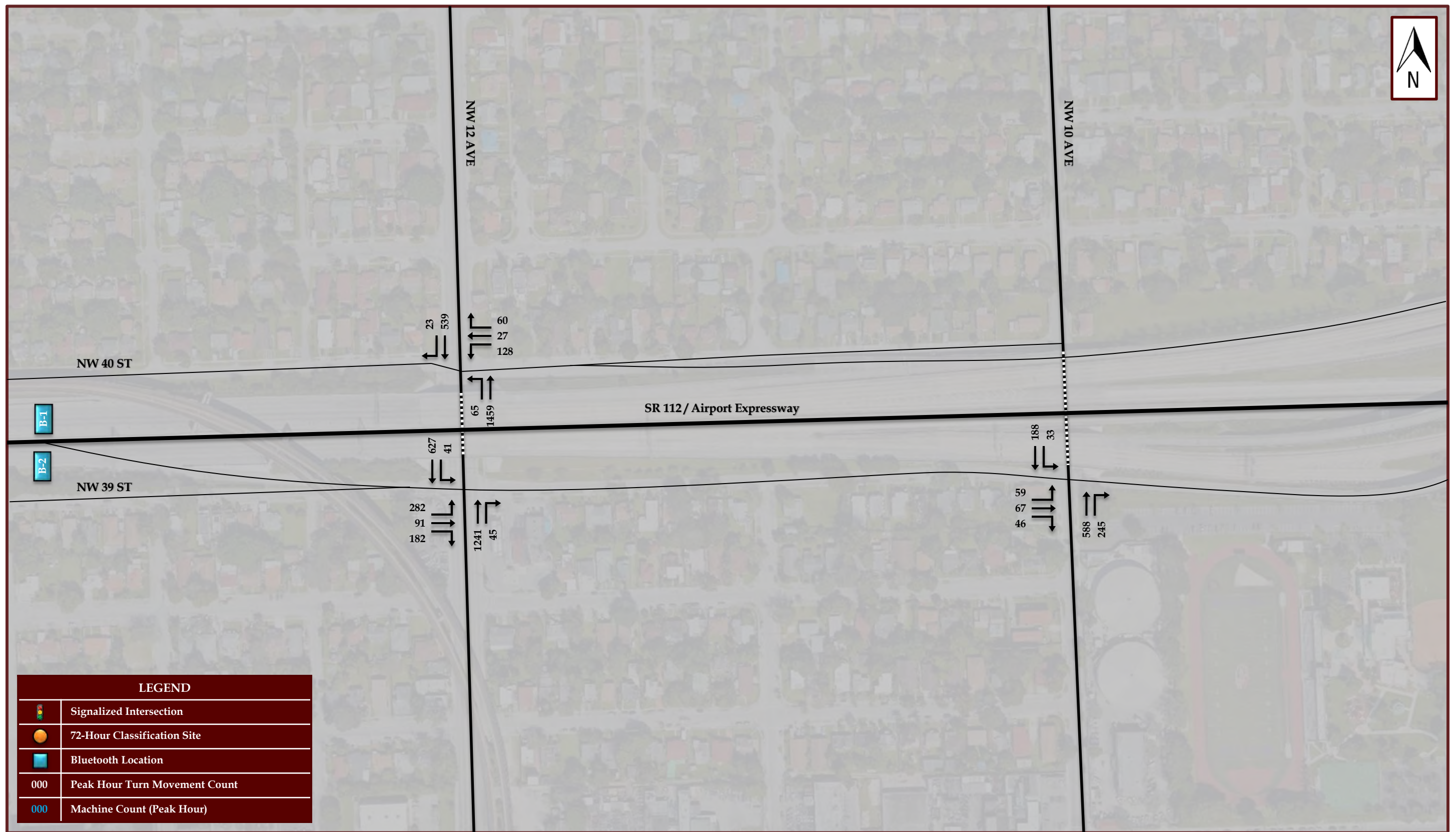


Figure 3.C. Inset A , Volume Counts and TMCs (PM Peak)

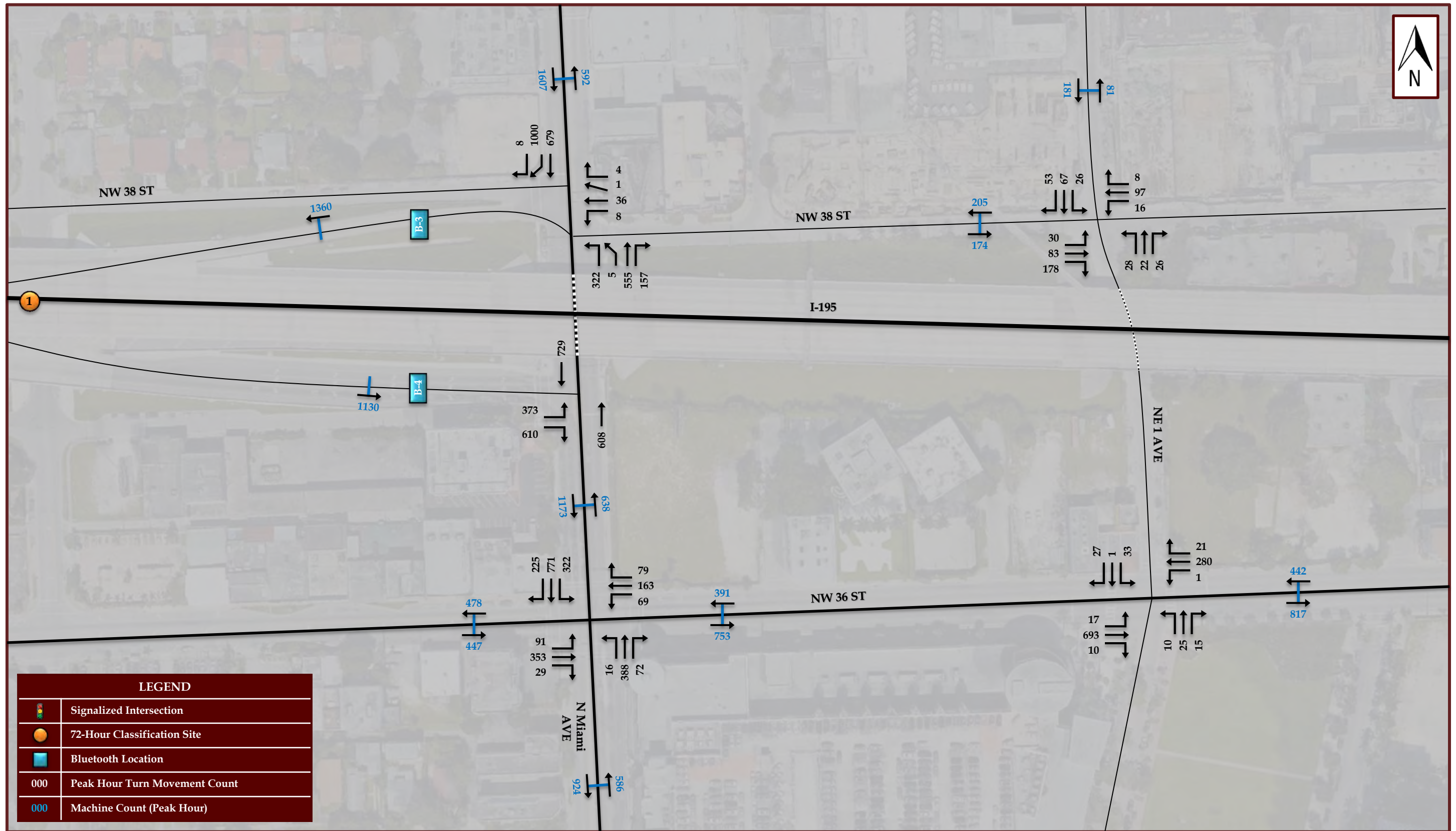


Figure 3.D. Inset B , Volume Counts and TMCs (AM Peak)



Figure 3.E. Inset B , Volume Counts and TMCs (PM Peak)



Figure 3.F. Inset C , Volume Counts and TMCs (AM Peak)



Figure 3.G. Inset C , Volume Counts and TMCs (PM Peak)



Figure 3.I. Inset D, Volume Counts and TMCs (PM Peak)

4. Bluetooth Data Collection

4.1. Bluetooth Technology

Bluetooth is an open wireless technology standard for exchanging data over short distances from fixed and mobile devices (car radio systems, smartphones, smartwatches, etc.). Each Bluetooth device is identified with a unique MAC (Media Access Control) Address that is anonymous. Bluetooth devices by TrafficCast International© were used in this project. TrafficCast International© is a private company which works on developing technology, applications, and content based on advanced digital traffic data that provide travel time forecasting, road speed monitoring, and other traffic-related information. BlueTOAD – which is the proprietary Bluetooth device from TrafficCast International© – detects discoverable and non-discoverable devices within a radius of approximately 300 feet. Sensors in the field collect data which are then transmitted wirelessly to the BlueARGUS server that stores the data. The server and data can be accessed via a web-based software suite (BlueARGUS applications). **Figure 4** illustrates how Bluetooth technology was used for traffic data collection as well as all the components used in this method.

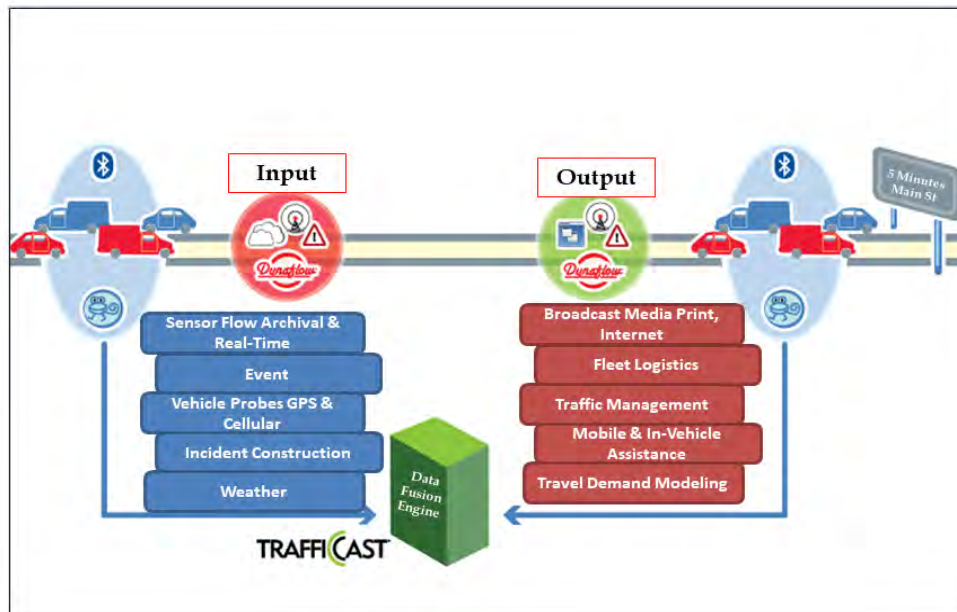


Figure 4. How Bluetooth Technology Works with BlueTOAD's

The web-based software suite shows real time traffic detection by the installed devices. The following steps were performed in order to use data collected by BlueTOAD:

1. Add devices into the software. The addition of devices is based on the exact location (latitude and longitude coordinates). Any issues with the software that affected the data collection efforts were reported. This reporting allowed these issues to be quickly resolved so that interruptions in the data collection effort were minimized.
2. After the devices were added, study pairs were defined. The study pairs are segments consisting of two devices and a known length between them which were used to create matches and calculate speed and travel-time.

Using this methodology, speed, travel time, and O-D data were extracted. The devices were deployed on Saturday, October 21st, 2017, and data were collected for one week. The survey locations were defined in the TrafficCast International© dashboard and quality control of the data collection process were performed. The data collection and sensor detection were monitored via the dashboard in real time which allowed issues with the devices to be identified and resolved in a timely manner. A 3-day data collection between Tuesday, October 24th, 2017 and Thursday, October 26th, 2017 was extracted from the online TrafficCast software.

To simplify the presentation of the O-D network used in this study, each station is shown with an alphanumeric symbol, as presented in **Figure 3.A** through **Figure 3.H**. For adjustment purposes, 72-hour traffic station volumes were collected at the exact locations at which Bluetooth sensors were deployed.

4.2. Bluetooth Data Adjustment

Data collected using Bluetooth and Wi-Fi methods require adjustment since these methods do not capture a significant portion of the traffic that pass by the detection devices. Bluetooth data collection efforts in various locations throughout the United States have typically shown a capture rate of between 2.0% to 10.0% of traffic volume.

Because the detector detects every five seconds, if a Bluetooth device is within range of a detector for more than five seconds, it can result in multiple recorded detections. To correct this problem, TrafficCast group redundant detections into one cluster and then choose the middle detection of each cluster to represent that cluster.

72-hour traffic station volumes were performed at previously mentioned 12 locations, starting on Tuesday (12:00 AM) to Thursday (11:59 PM). Raw data can be found in **Appendix A**. Average Daily Traffic (ADT) values were obtained through the 72-hour

volumes collected by averaging each daily total traffic observed (24-hour period) over the 3-days of data collection.

The volumes detected by BlueTOADs at each station were factored up by the AADT counts of the similar period (AM or PM) to extrapolate the sample data to full count. Raw Bluetooth counts can be seen in **Appendix B** and the detailed procedure of matrix estimation from raw data can be seen in **Appendix C**. Also, the data which were used for adjustment is presented in **Appendix A**.

5. Origin-Destination Matrices

The Origin-Destination data collection summary can be seen in **Table 1** and **Table 2**. The share of traffic (percentages) passing through each station to other stations is shown in the tables for AM and PM peak periods. AM peak period is selected to be 6:00 AM to 9:59 AM and the PM peak period is selected to be 3:00 PM to 7:59 PM. The numbers are representing 72 hour traffic pattern during data collection period. O-D maps can be seen in **Figures 5** through **Figure 10**. O-D summary is provided by percentage of vehicles moving between each station.

Table 1: I-195 O-D (Bluetooth Data Collection between 10/24/2017 and 10/26/2017) – AM Peak Period

Origin	Destination	Origin											
		B1	B2	B3	B4	B5	B6	B7	B8	B9	B10	B11	B12
		WB I-195 Mainline @ NW 12th Ave	EB I-195 Mainline @ NW 12th Ave	WB I-195 On-Ramp from N Miami Ave	EB I-195 Off-Ramp to N Miami Ave	WB I-195 On-Ramp from US-1	EB I-195 Off-Ramp to US-1	WB I-195 Off-Ramp to US-1	EB I-195 On-Ramp From US-1	WB I-195 Mainline @ West of On-Ramp from Alton Road	EB I-195 Mainline @ West of Off-Ramp to Alton Road	NB I-95 Mainline near GGI	SB I-95 Mainline near GGI
WB I-195 Mainline @ NW 12th Ave	B1		n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 Mainline @ NW 12th Ave	B2	n/a		n/a	1.78%	n/a	1.28%	n/a	n/a	n/a	5.64%	13.74%	n/a
WB I-195 On-Ramp from N Miami Ave	B3	15.60%	n/a		n/a	n/a	n/a	n/a	n/a	n/a	n/a	2.50%	n/a
EB I-195 Off-Ramp to N Miami Ave	B4	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 On-Ramp from US-1	B5	9.55%	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	1.88%	n/a
EB I-195 Off-Ramp to US-1	B6	n/a	n/a	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 Off-Ramp to US-1	B7	n/a	n/a	n/a	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a
EB I-195 On-Ramp From US-1	B8	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a	100.00%	n/a	n/a
WB I-195 Mainline @ West of On-Ramp from Alton Road	B9	4.70%	n/a	n/a	n/a	n/a	n/a	6.47%	n/a		n/a	0.86%	n/a
EB I-195 Mainline @ West of Off-Ramp to Alton Road	B10	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a	n/a
NB I-95 Mainline near GGI	B11	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a
SB I-95 Mainline near GGI	B12	21.16%	n/a	n/a	7.16%	n/a	2.27%	n/a	n/a	n/a	0.97%	n/a	

Table 2: I-195 O-D (Bluetooth Data Collection between 10/24/2017 and 10/26/2017) – PM Peak Period

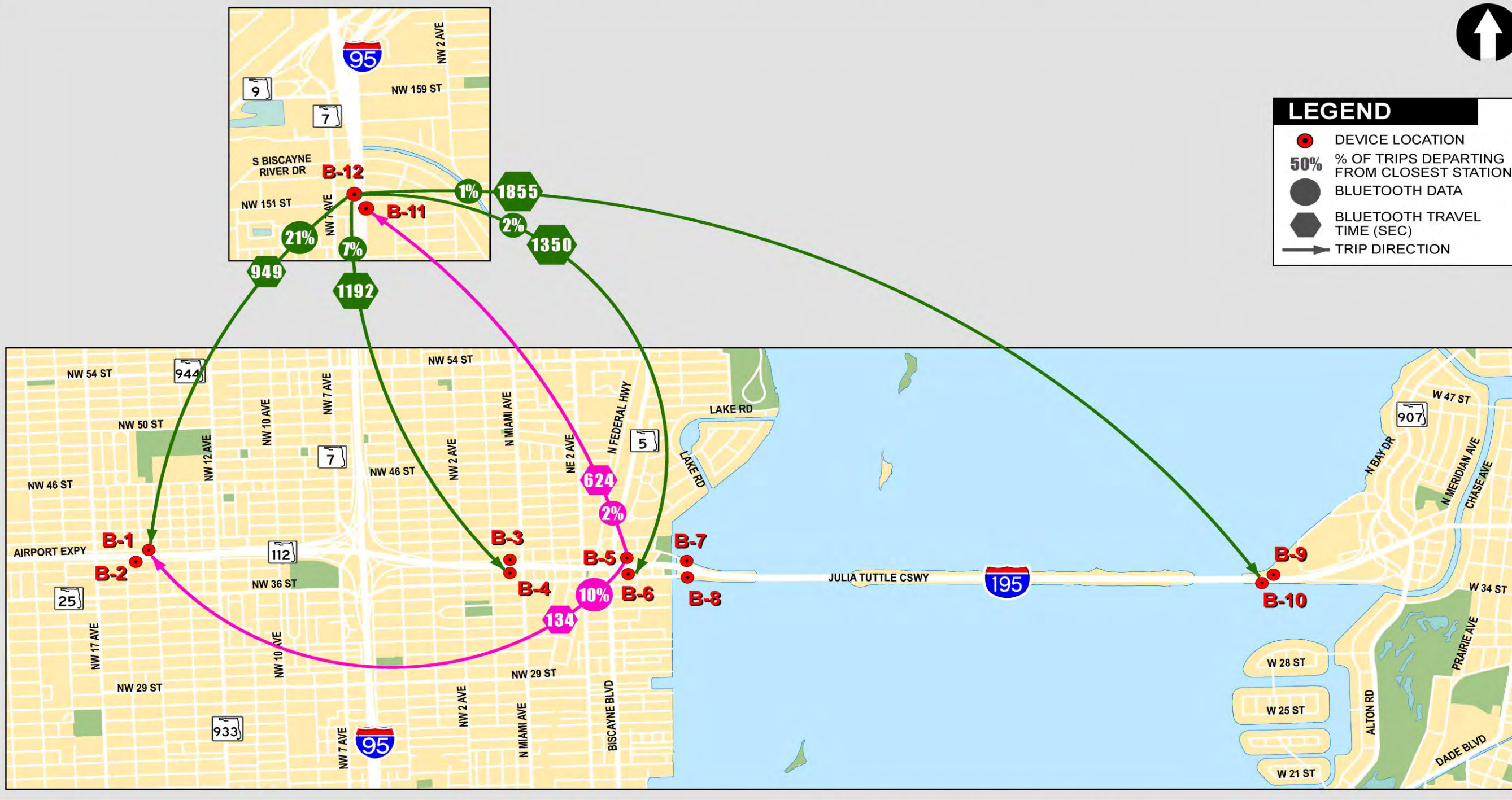
Destination	Origin											
	WB I-195 Mainline @ NW 12th Ave	EB I-195 Mainline @ NW 12th Ave	WB I-195 On-Ramp from N Miami Ave	EB I-195 Off-Ramp to N Miami Ave	WB I-195 On-Ramp from US-1	EB I-195 Off-Ramp to US-1	WB I-195 Off-Ramp to US-1	EB I-195 On-Ramp From US-1	WB I-195 Mainline @ West of On-Ramp from Alton Road	EB I-195 Mainline @ West of Off-Ramp to Alton Road	NB I-95 Mainline near GGI	SB I-95 Mainline near GGI
Origin	B1	B2	B3	B4	B5	B6	B7	B8	B9	B10	B11	B12
WB I-195 Mainline @ NW 12th Ave	B1	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 Mainline @ NW 12th Ave	B2	n/a	n/a	3.30%	n/a	2.51%	n/a	n/a	n/a	8.31%	18.14%	n/a
WB I-195 On-Ramp from N Miami Ave	B3	9.01%	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	9.61%	n/a
EB I-195 Off-Ramp to N Miami Ave	B4	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 On-Ramp from US-1	B5	1.82%	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	0.88%	n/a
EB I-195 Off-Ramp to US-1	B6	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 Off-Ramp to US-1	B7	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 On-Ramp From US-1	B8	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	100.00%	n/a	n/a
WB I-195 Mainline @ West of On-Ramp from Alton Road	B9	11.63%	n/a	n/a	n/a	n/a	12.65%	n/a	n/a	n/a	6.39%	n/a
EB I-195 Mainline @ West of Off-Ramp to Alton Road	B10	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
NB I-95 Mainline near GGI	B11	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
SB I-95 Mainline near GGI	B12	34.09%	n/a	n/a	11.76%	n/a	4.34%	n/a	n/a	4.09%	n/a	n/a

INTERSTATE 195 CORRIDOR PLANNING STUDY FROM I-95 TO ALTON ROAD (FM No. 440228-1-22-01)



LEGEND

- DEVICE LOCATION
- 50% % OF TRIPS DEPARTING FROM CLOSEST STATION
- BLUETOOTH DATA
- ⬠ BLUETOOTH TRAVEL TIME (SEC)
- TRIP DIRECTION

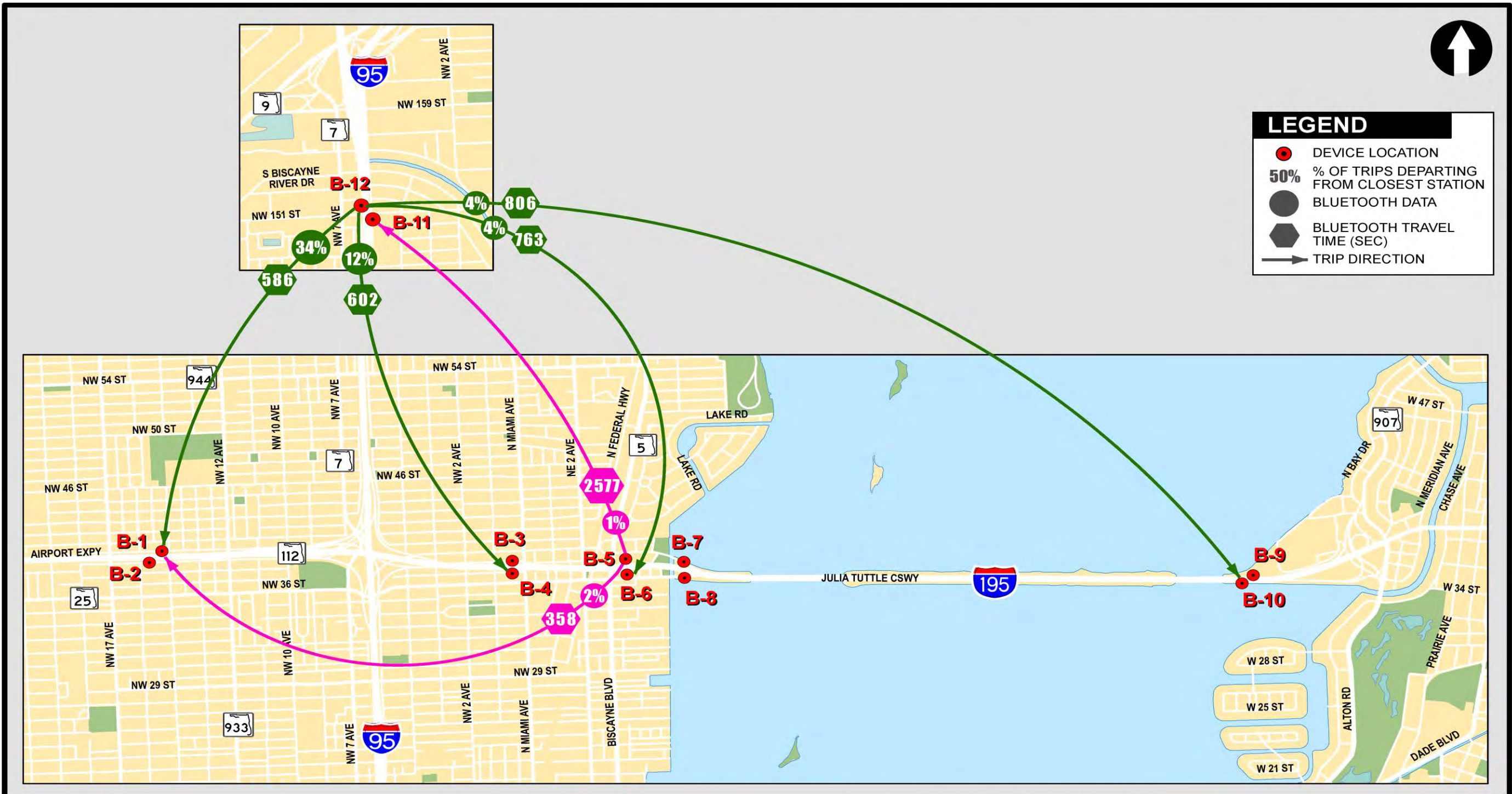


Departing Trips (% of total trips passing through each station) from Stations B5 and B12
AM PEAK PERIOD

MAP 1 OF 6

Figure 5. O-D and Travel Time Data Collection B5 and B12 - AM Peak Hour

INTERSTATE 195 CORRIDOR PLANNING STUDY FROM I-95 TO ALTON ROAD (FM No. 440228-1-22-01)



Departing Trips (% of total trips passing through each station) from Stations B5 and B12
PM PEAK PERIOD

MAP 2 OF 6



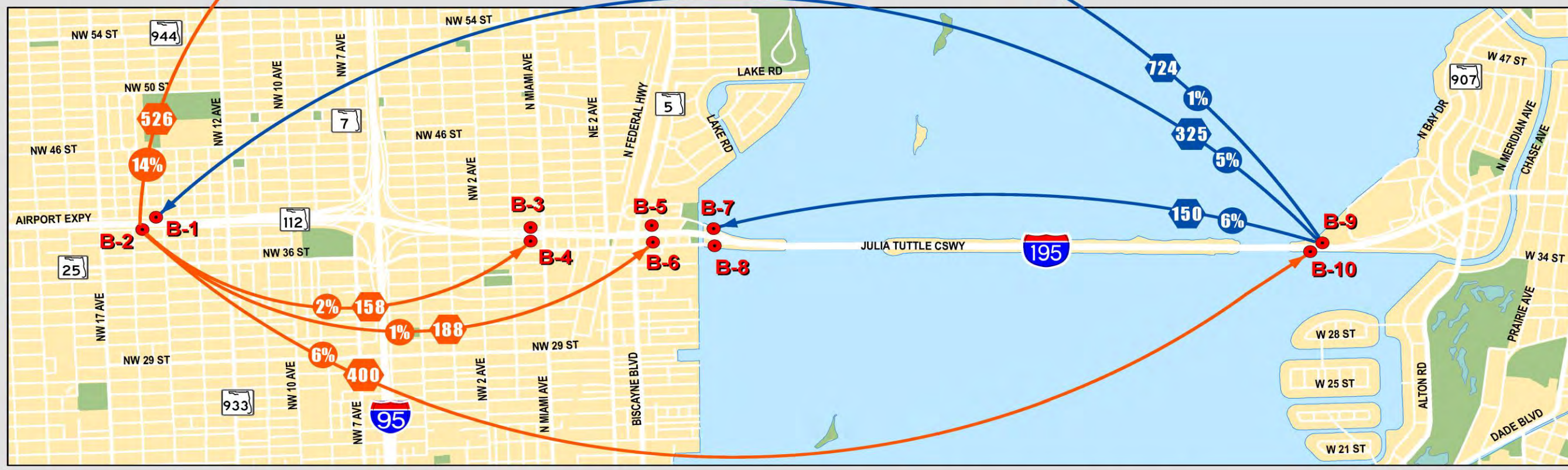
Figure 6. O-D and Travel Time Data Collection B5 and B12 - PM Peak Hour

INTERSTATE 195 CORRIDOR PLANNING STUDY FROM I-95 TO ALTON ROAD (FM No. 440228-1-22-01)



LEGEND

- DEVICE LOCATION
- 50% % OF TRIPS DEPARTING FROM CLOSEST STATION
- BLUETOOTH DATA
- ⬡ BLUETOOTH TRAVEL TIME (SEC)
- ➔ TRIP DIRECTION



Departing Trips (% of total trips passing through each station) from Stations B2 and B9
AM PEAK PERIOD

MAP 3 OF 6

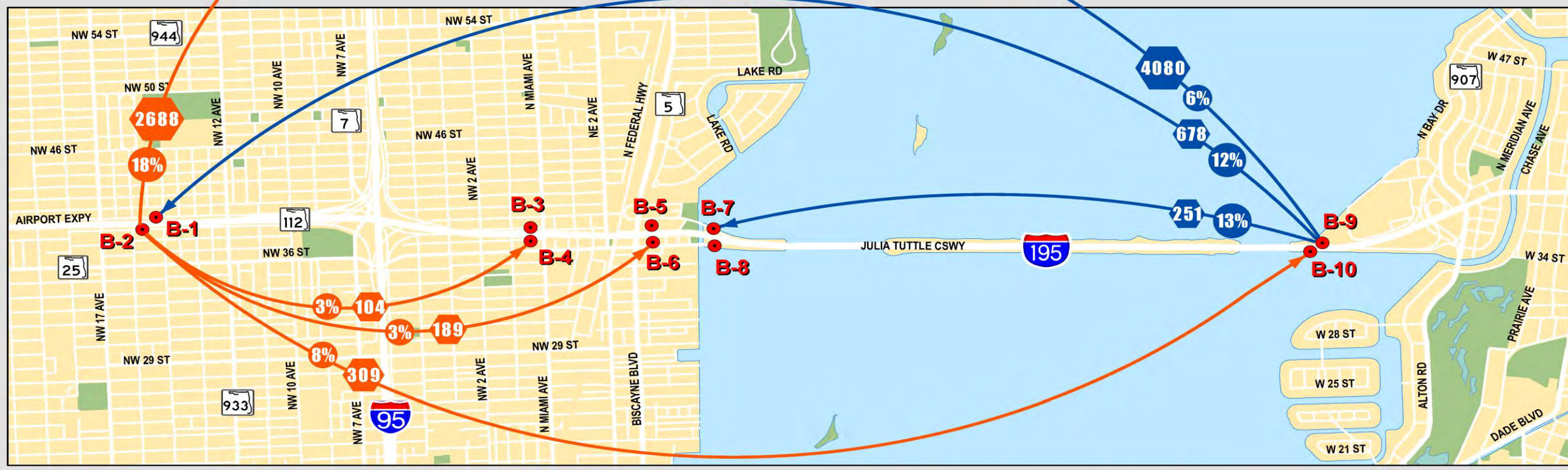
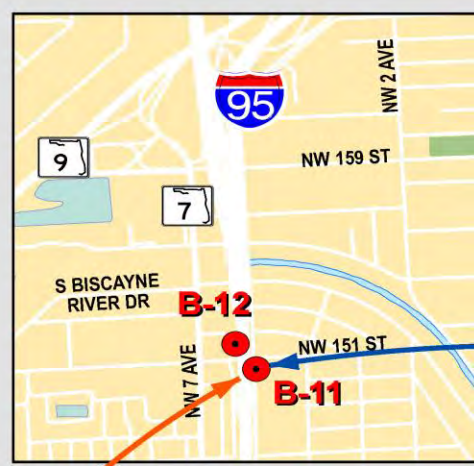
Figure 7. O-D and Travel Time Data Collection B2 and B9 - AM Peak Hour

INTERSTATE 195 CORRIDOR PLANNING STUDY FROM I-95 TO ALTON ROAD (FM No. 440228-1-22-01)



LEGEND

- DEVICE LOCATION
- 50% % OF TRIPS DEPARTING FROM CLOSEST STATION
- BLUETOOTH DATA
- ⬡ BLUETOOTH TRAVEL TIME (SEC)
- ➔ TRIP DIRECTION



Departing Trips (% of total trips passing through each station) from Stations B2 and B9
PM PEAK PERIOD

MAP 4 OF 6

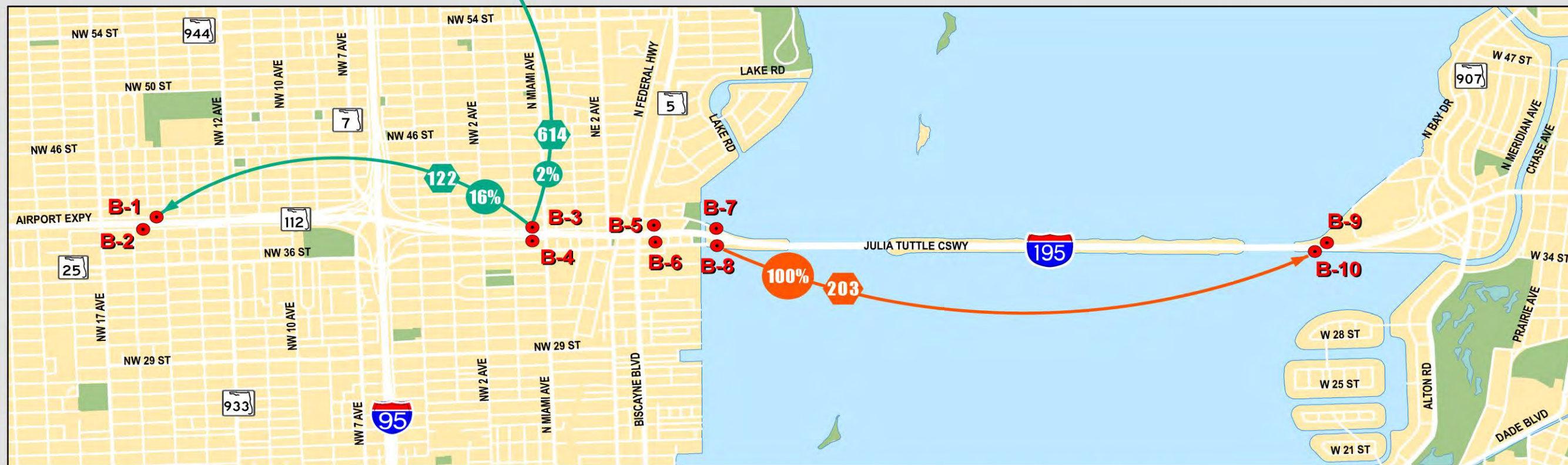
Figure 8. O-D and Travel Time Data Collection B2 and B9 - PM Peak Hour

INTERSTATE 195 CORRIDOR PLANNING STUDY FROM I-95 TO ALTON ROAD (FM No. 440228-1-22-01)



LEGEND

- DEVICE LOCATION
- 50% % OF TRIPS DEPARTING FROM CLOSEST STATION
- BLUETOOTH DATA
- ⬡ BLUETOOTH TRAVEL TIME (SEC)
- ➔ TRIP DIRECTION



Departing Trips (% of total trips passing through each station) from Stations B3 and B8
AM PEAK PERIOD

MAP 5 OF 6

Figure 9. O-D and Travel Time Data Collection B3 and B8 - AM Peak Hour

INTERSTATE 195 CORRIDOR PLANNING STUDY FROM I-95 TO ALTON ROAD (FM No. 440228-1-22-01)



LEGEND

- DEVICE LOCATION
- 50% % OF TRIPS DEPARTING FROM CLOSEST STATION
- BLUETOOTH DATA
- ⬡ BLUETOOTH TRAVEL TIME (SEC)
- ➔ TRIP DIRECTION



Departing Trips (% of total trips passing through each station) from Stations B3 and B8
PM PEAK PERIOD

MAP 6 OF 6

Figure 10. O-D and Travel Time Data Collection B3 and B8 - PM Peak Hour

Travel time between each of the stations will be recorded when the devices are activated. The average 60 minutes travel time for AM and PM peak periods for the data collection days (October 24th, 2017 to October 26th, 2017) is reported in **Table 3** and **Table 4** for AM and PM peak periods, respectively. Average travel time between two stations is recorded if unique MAC address is reported to both. This study considers the logical routes between two stations and reported the average weight travel time based on the number of counts per route.

Table 3: I-195 Average Travel Time (seconds) - Bluetooth Data Collection between 10/24/2017 and 10/26/2017 - AM Peak Period

Destination	Origin											
	WB I-195 Mainline @ NW 12th Ave	EB I-195 Mainline @ NW 12th Ave	WB I-195 On-Ramp from N Miami Ave	EB I-195 Off-Ramp to N Miami Ave	WB I-195 On-Ramp from US-1	EB I-195 Off-Ramp to US-1	WB I-195 Off-Ramp to US-1	EB I-195 On-Ramp From US-1	WB I-195 Mainline @ West of On-Ramp from Alton Road	EB I-195 Mainline @ West of Off-Ramp to Alton Road	NB I-95 Mainline near GGI	SB I-95 Mainline near GGI
Origin	B1	B2	B3	B4	B5	B6	B7	B8	B9	B10	B11	B12
WB I-195 Mainline @ NW 12th Ave	B1	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 Mainline @ NW 12th Ave	B2	n/a	n/a	158	n/a	188	n/a	n/a	n/a	400	526	n/a
WB I-195 On-Ramp from N Miami Ave	B3	122	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	614	n/a
EB I-195 Off-Ramp to N Miami Ave	B4	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 On-Ramp from US-1	B5	134	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	624	n/a
EB I-195 Off-Ramp to US-1	B6	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 Off-Ramp to US-1	B7	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 On-Ramp From US-1	B8	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	203	n/a	n/a
WB I-195 Mainline @ West of On-Ramp from Alton Road	B9	325	n/a	n/a	n/a	n/a	150	n/a	n/a	n/a	724	n/a
EB I-195 Mainline @ West of Off-Ramp to Alton Road	B10	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
NB I-95 Mainline near GGI	B11	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
SB I-95 Mainline near GGI	B12	949	n/a	n/a	1,192	n/a	1,350	n/a	n/a	1,855	n/a	n/a

Table 4: I-195 Average Travel Time (seconds) - Bluetooth Data Collection between 10/24/2017 and 10/26/2017 - PM Peak Period

Destination	Origin												
	B1	B2	B3	B4	B5	B6	B7	B8	B9	B10	B11	B12	
WB I-195 Mainline @ NW 12th Ave	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 Mainline @ NW 12th Ave	n/a	104	n/a	n/a	n/a	189	n/a	n/a	n/a	309	2,688	n/a	n/a
WB I-195 On-Ramp from N Miami Ave	259	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	1,792	n/a	n/a
EB I-195 Off-Ramp to N Miami Ave	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 On-Ramp from US-1	358	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	2,577	n/a	n/a
EB I-195 Off-Ramp to US-1	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 Off-Ramp to US-1	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 On-Ramp From US-1	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	161	n/a	n/a	n/a
WB I-195 Mainline @ West of On-Ramp from Alton Road	678	n/a	n/a	n/a	n/a	n/a	251	n/a	n/a	n/a	4,080	n/a	n/a
EB I-195 Mainline @ West of Off-Ramp to Alton Road	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
NB I-95 Mainline near GGI	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
SB I-95 Mainline near GGI	586	n/a	n/a	602	n/a	763	n/a	n/a	n/a	806	n/a	n/a	n/a

Appendix A

Raw Data



72-Hour Traffic Station Volumes

County: 87
 Station: 0001
 Description: ALTON RD, N. OF N BAY RD
 Start Date: 10/17/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	66	74	60	62	262	59	42	34	25	160	422	
0100	33	29	21	18	101	31	31	16	21	99	200	
0200	15	15	15	15	60	16	6	12	11	45	105	
0300	9	15	16	10	50	9	13	7	10	39	89	
0400	7	13	19	20	59	13	18	32	28	91	150	
0500	20	39	64	64	187	34	63	96	105	298	485	
0600	113	138	127	176	554	105	193	284	333	915	1469	
0700	211	276	251	292	1030	331	337	343	344	1355	2385	
0800	269	271	320	307	1167	405	418	380	390	1593	2760	
0900	283	251	278	229	1041	397	371	328	345	1441	2482	
1000	260	254	287	236	1037	378	368	216	318	1280	2317	
1100	245	244	242	254	985	414	350	202	266	1232	2217	
1200	277	313	283	298	1171	331	304	285	272	1192	2363	
1300	286	299	294	266	1145	317	292	211	317	1137	2282	
1400	329	333	372	363	1397	299	268	282	298	1147	2544	
1500	328	326	368	366	1388	254	277	350	346	1227	2615	
1600	356	408	431	417	1612	315	278	266	239	1098	2710	
1700	460	427	455	445	1787	249	357	297	292	1195	2982	
1800	470	485	441	424	1820	276	284	262	289	1111	2931	
1900	391	331	309	285	1316	250	242	195	174	861	2177	
2000	258	236	246	245	985	158	164	144	133	599	1584	
2100	197	190	189	200	776	131	103	118	130	482	1258	
2200	178	155	173	156	662	86	84	78	69	317	979	
2300	143	112	115	88	458	78	83	48	60	269	727	
24-Hour Totals:					21050						19183	40233

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	815	1181	800	1593	815	2766
P.M.	1730	1855	1530	1289	1715	3019
Daily	1730	1855	800	1593	1715	3019

County: 87
 Station: 0001
 Description: ALTON RD, N. OF N BAY RD
 Start Date: 10/18/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	73	62	59	72	266	43	50	40	37	170	436		
0100	42	36	24	30	132	22	23	15	11	71	203		
0200	19	24	17	16	76	21	14	16	9	60	136		
0300	8	7	19	16	50	9	9	7	13	38	88		
0400	9	16	21	17	63	16	20	31	36	103	166		
0500	25	33	64	69	191	33	50	101	87	271	462		
0600	100	160	152	191	603	137	193	288	324	942	1545		
0700	230	246	251	312	1039	343	314	327	321	1305	2344		
0800	296	251	307	308	1162	416	408	384	380	1588	2750		
0900	281	248	253	252	1034	399	389	364	358	1510	2544		
1000	255	224	258	252	989	375	376	336	356	1443	2432		
1100	252	228	286	284	1050	316	349	383	320	1368	2418		
1200	271	283	291	267	1112	260	281	270	257	1068	2180		
1300	242	264	301	244	1051	291	315	307	281	1194	2245		
1400	259	285	361	335	1240	205	297	310	257	1069	2309		
1500	331	310	353	339	1333	306	275	279	231	1091	2424		
1600	349	355	396	420	1520	247	280	285	276	1088	2608		
1700	417	375	446	464	1702	260	264	263	254	1041	2743		
1800	437	453	411	387	1688	281	263	275	266	1085	2773		
1900	360	353	305	301	1319	188	241	250	212	891	2210		
2000	297	265	271	218	1051	184	122	157	169	632	1683		
2100	228	212	172	156	768	119	132	101	96	448	1216		
2200	169	154	181	145	649	87	90	88	87	352	1001		
2300	154	135	147	142	578	80	105	72	58	315	893		
24-Hour Totals:						20666						19143	39809

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	745	1166	800	1588	800	2750
P.M.	1730	1800	1300	1194	1730	2861
Daily	1730	1800	800	1588	1730	2861

County: 87
 Station: 0001
 Description: ALTON RD, N. OF N BAY RD
 Start Date: 10/19/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	134	87	68	63	352	67	43	48	30	188	540		
0100	54	34	30	23	141	26	33	20	16	95	236		
0200	21	12	17	18	68	22	16	15	13	66	134		
0300	13	9	15	21	58	14	18	10	12	54	112		
0400	20	12	16	23	71	14	16	27	23	80	151		
0500	20	33	60	72	185	42	52	98	97	289	474		
0600	98	143	162	172	575	122	186	258	308	874	1449		
0700	230	252	301	316	1099	338	288	318	300	1244	2343		
0800	267	279	284	297	1127	425	391	400	384	1600	2727		
0900	310	250	255	238	1053	382	368	372	358	1480	2533		
1000	256	241	245	229	971	399	336	287	352	1374	2345		
1100	229	253	273	259	1014	366	352	223	102	1043	2057		
1200	204	263	283	256	1006	294	319	282	249	1144	2150		
1300	251	250	282	249	1032	287	313	236	300	1136	2168		
1400	258	276	313	291	1138	348	312	308	315	1283	2421		
1500	293	297	331	345	1266	302	341	313	271	1227	2493		
1600	360	358	383	406	1507	318	283	370	341	1312	2819		
1700	414	445	432	501	1792	298	297	320	287	1202	2994		
1800	428	406	436	412	1682	268	265	292	285	1110	2792		
1900	479	408	343	261	1491	288	167	335	352	1142	2633		
2000	307	246	235	192	980	318	262	233	227	1040	2020		
2100	175	212	193	157	737	149	182	103	124	558	1295		
2200	154	157	145	120	576	102	90	65	106	363	939		
2300	127	123	99	88	437	92	56	60	53	261	698		
24-Hour Totals:						20358						20165	40523

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	815	1170	800	1600	800	2727
P.M.	1715	1806	1600	1312	1700	2994
Daily	1715	1806	800	1600	1700	2994

County: 87
 Station: 0002
 Description: ALTON RD, N. OF NAUTILUS RD
 Start Date: 10/17/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	11	14	10	9	44	4	2	3	1	10	54
0100	8	4	2	8	22	2	1	4	5	12	34
0200	2	2	5	2	11	0	2	2	0	4	15
0300	3	5	2	3	13	3	0	2	1	6	19
0400	3	6	2	5	16	0	2	0	2	4	20
0500	5	28	29	49	111	1	3	1	7	12	123
0600	68	82	126	150	426	2	12	8	16	38	464
0700	111	93	86	84	374	25	20	28	27	100	474
0800	122	125	102	87	436	29	30	31	34	124	560
0900	83	95	70	65	313	36	42	37	35	150	463
1000	73	66	69	72	280	49	50	35	44	178	458
1100	76	61	71	70	278	36	57	39	55	187	465
1200	63	65	68	88	284	49	54	31	39	173	457
1300	71	69	67	87	294	38	24	51	38	151	445
1400	80	85	75	81	321	46	38	33	46	163	484
1500	71	87	73	68	299	45	41	40	45	171	470
1600	69	60	66	96	291	38	43	54	56	191	482
1700	61	70	85	85	301	66	50	50	36	202	503
1800	85	87	78	75	325	38	29	39	21	127	452
1900	70	47	40	35	192	27	24	19	21	91	283
2000	43	32	43	46	164	19	18	11	14	62	226
2100	36	37	28	29	130	16	17	16	18	67	197
2200	25	28	22	18	93	8	7	12	11	38	131
2300	20	12	10	13	55	8	9	7	4	28	83
24-Hour Totals:	5073					2289					7362

Peak Volume Information

	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	645	440	845	149	800	560
P.M.	1730	342	1630	226	1645	534
Daily	630	480	1630	226	800	560

County: 87
 Station: 0002
 Description: ALTON RD, N. OF NAUTILUS RD
 Start Date: 10/18/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	7	16	5	9	37	6	4	2	6	18	55
0100	4	6	5	6	21	2	2	3	3	10	31
0200	0	5	5	4	14	2	4	0	1	7	21
0300	1	2	2	3	8	0	1	1	1	3	11
0400	2	0	9	5	16	3	2	1	1	7	23
0500	10	25	33	41	109	0	2	3	5	10	119
0600	56	98	129	144	427	7	11	9	17	44	471
0700	87	93	88	96	364	24	18	23	28	93	457
0800	101	116	87	77	381	25	31	41	33	130	511
0900	92	77	66	68	303	47	42	39	37	165	468
1000	63	72	55	66	256	41	38	54	48	181	437
1100	74	72	71	68	285	32	38	43	56	169	454
1200	74	82	87	84	327	45	55	40	36	176	503
1300	77	69	85	77	308	42	31	44	34	151	459
1400	50	79	73	76	278	38	42	52	42	174	452
1500	80	90	92	57	319	51	57	51	55	214	533
1600	60	72	67	79	278	42	44	41	51	178	456
1700	65	65	98	94	322	65	54	43	44	206	528
1800	57	77	66	56	256	23	20	22	26	91	347
1900	47	50	47	50	194	22	31	25	18	96	290
2000	38	41	46	33	158	16	20	19	14	69	227
2100	36	30	24	23	113	19	15	13	16	63	176
2200	25	25	30	26	106	11	20	7	10	48	154
2300	20	10	27	24	81	11	5	4	11	31	112
24-Hour Totals:	4961					2334					7295

Peak Volume Information

	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	645	412	830	163	745	525
P.M.	1445	338	1500	214	1445	539
Daily	615	458	1500	214	1445	539

County: 87
 Station: 0002
 Description: ALTON RD, N. OF NAUTILUS RD
 Start Date: 10/19/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	6	13	9	8	36	5	2	2	3	12	48	
0100	6	0	5	3	14	2	6	2	0	10	24	
0200	5	4	2	4	15	1	3	0	1	5	20	
0300	1	2	5	5	13	0	0	1	0	1	14	
0400	2	4	2	7	15	1	1	0	4	6	21	
0500	9	13	37	45	104	2	2	4	4	12	116	
0600	60	93	113	104	370	8	10	8	21	47	417	
0700	88	84	82	93	347	19	24	23	28	94	441	
0800	110	130	99	98	437	18	34	35	32	119	556	
0900	100	88	80	65	333	46	38	19	43	146	479	
1000	74	69	72	61	276	37	40	41	48	166	442	
1100	57	72	64	60	253	35	49	50	66	200	453	
1200	61	67	56	72	256	51	38	46	41	176	432	
1300	52	75	84	65	276	47	39	38	35	159	435	
1400	71	55	65	65	256	38	41	60	46	185	441	
1500	86	70	71	62	289	65	33	34	62	194	483	
1600	72	66	62	52	252	52	44	61	46	203	455	
1700	64	79	78	66	287	60	51	42	31	184	471	
1800	78	62	65	70	275	24	45	31	35	135	410	
1900	94	55	47	44	240	25	31	32	27	115	355	
2000	48	44	29	32	153	25	28	20	30	103	256	
2100	30	39	28	30	127	26	15	11	15	67	194	
2200	21	31	14	16	82	11	19	11	4	45	127	
2300	22	12	8	14	56	8	4	4	8	24	80	
24-Hour Totals:					4762						2408	7170

Peak Volume Information

	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	800	437	830	151	815	574
P.M.	1715	301	1545	219	1430	490
Daily	800	437	1545	219	815	574

County: 87
 Station: 0003
 Description: 41 ST, E OF ALTON RD
 Start Date: 10/17/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	137	114	98	78	427	111	93	81	57	342	769		
0100	74	54	50	51	229	52	27	45	29	153	382		
0200	40	37	25	23	125	34	31	28	22	115	240		
0300	30	43	33	35	141	30	20	32	20	102	243		
0400	32	49	57	45	183	29	29	29	58	145	328		
0500	58	64	99	119	340	44	62	64	73	243	583		
0600	122	159	182	228	691	103	126	164	184	577	1268		
0700	397	344	298	343	1382	243	276	294	331	1144	2526		
0800	388	374	315	294	1371	309	308	343	340	1300	2671		
0900	200	171	228	221	820	293	321	274	245	1133	1953		
1000	238	227	231	199	895	252	249	255	295	1051	1946		
1100	252	220	244	234	950	278	291	264	274	1107	2057		
1200	258	232	236	252	978	274	316	314	303	1207	2185		
1300	257	249	252	233	991	260	275	268	285	1088	2079		
1400	275	230	268	281	1054	277	290	295	325	1187	2241		
1500	279	274	298	291	1142	346	371	393	351	1461	2603		
1600	289	291	282	270	1132	398	328	369	369	1464	2596		
1700	244	280	338	266	1128	317	367	338	344	1366	2494		
1800	294	244	248	272	1058	296	270	277	233	1076	2134		
1900	253	215	250	210	928	278	283	226	185	972	1900		
2000	225	225	221	227	898	220	166	159	172	717	1615		
2100	224	223	201	233	881	150	157	156	120	583	1464		
2200	220	264	248	256	988	150	149	131	115	545	1533		
2300	222	208	200	158	788	176	188	159	127	650	1438		
24-Hour Totals:						19520						19728	39248

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	745	1420	800	1300	745	2711
P.M.	1715	1178	1515	1513	1515	2665
Daily	745	1420	1515	1513	745	2711

County: 87
 Station: 0003
 Description: 41 ST, E OF ALTON RD
 Start Date: 10/18/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	148	106	84	83	421	135	86	98	66	385	806		
0100	48	59	54	35	196	73	39	53	37	202	398		
0200	44	37	35	32	148	29	24	28	33	114	262		
0300	28	18	30	25	101	17	19	30	12	78	179		
0400	32	29	42	67	170	24	36	45	32	137	307		
0500	62	79	101	123	365	38	41	61	84	224	589		
0600	109	136	182	235	662	100	118	173	165	556	1218		
0700	288	290	341	318	1237	242	272	313	329	1156	2393		
0800	302	294	317	325	1238	302	317	311	308	1238	2476		
0900	228	205	231	222	886	307	310	298	253	1168	2054		
1000	206	226	290	282	1004	275	293	315	258	1141	2145		
1100	228	212	233	219	892	307	316	277	307	1207	2099		
1200	220	234	220	203	877	330	317	324	328	1299	2176		
1300	279	235	250	262	1026	306	329	264	268	1167	2193		
1400	236	281	277	283	1077	331	309	349	317	1306	2383		
1500	284	261	287	266	1098	357	432	403	358	1550	2648		
1600	284	271	292	253	1100	398	401	432	339	1570	2670		
1700	284	267	265	282	1098	381	382	334	290	1387	2485		
1800	268	262	258	263	1051	292	280	293	268	1133	2184		
1900	237	233	210	235	915	282	230	206	220	938	1853		
2000	213	243	239	228	923	240	210	176	162	788	1711		
2100	220	195	221	246	882	153	155	157	171	636	1518		
2200	221	257	206	244	928	132	142	156	147	577	1505		
2300	185	173	194	224	776	181	191	162	127	661	1437		
24-Hour Totals:						19071						20618	39689

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	730	1255	730	1261	730	2516
P.M.	1415	1125	1515	1591	1545	2702
Daily	730	1255	1515	1591	1545	2702

County: 87
 Station: 0003
 Description: 41 ST, E OF ALTON RD
 Start Date: 10/19/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	211	179	161	113	664	155	122	99	89	465	1129		
0100	93	61	65	47	266	71	46	51	39	207	473		
0200	45	25	30	39	139	43	48	51	34	176	315		
0300	33	27	23	27	110	35	32	54	30	151	261		
0400	30	37	36	62	165	46	65	60	55	226	391		
0500	54	76	90	126	346	57	52	73	88	270	616		
0600	118	138	179	241	676	107	103	182	179	571	1247		
0700	209	187	207	224	827	236	246	286	339	1107	1934		
0800	214	237	202	247	900	286	295	349	312	1242	2142		
0900	234	217	159	249	859	302	327	301	272	1202	2061		
1000	249	252	238	220	959	278	277	283	267	1105	2064		
1100	270	247	263	258	1038	296	285	309	305	1195	2233		
1200	277	277	284	319	1157	314	319	291	277	1201	2358		
1300	303	247	266	240	1056	276	310	266	273	1125	2181		
1400	280	280	347	285	1192	276	282	290	326	1174	2366		
1500	336	304	357	369	1366	305	384	413	287	1389	2755		
1600	315	313	323	335	1286	327	335	305	251	1218	2504		
1700	343	352	350	335	1380	292	268	281	265	1106	2486		
1800	273	347	369	374	1363	234	189	227	202	852	2215		
1900	304	274	218	278	1074	173	201	179	152	705	1779		
2000	256	237	227	224	944	148	127	139	112	526	1470		
2100	217	215	198	237	867	145	147	116	140	548	1415		
2200	231	237	252	237	957	144	121	127	143	535	1492		
2300	218	195	161	151	725	149	187	162	108	606	1331		
24-Hour Totals:						20316						18902	39218

Peak Volume Information

	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	815	920	830	1290	830	2190
P.M.	1645	1380	1445	1428	1515	2756
Daily	1815	1394	1445	1428	1515	2756

County: 87
 Station: 0004
 Description: RAMP 87037202
 Start Date: 10/17/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	105	127	67	69	368
0100	54	40	30	32	156
0200	32	25	27	16	100
0300	15	30	20	28	93
0400	26	19	21	26	92
0500	27	36	39	43	145
0600	56	60	82	113	311
0700	160	211	160	159	690
0800	181	189	185	180	735
0900	194	195	164	169	722
1000	173	163	180	196	712
1100	175	198	233	229	835
1200	243	204	209	234	890
1300	247	251	239	251	988
1400	297	330	389	319	1335
1500	377	402	432	326	1537
1600	437	408	352	338	1535
1700	346	362	334	356	1398
1800	334	322	294	272	1222
1900	266	253	225	193	937
2000	179	147	138	132	596
2100	140	150	138	138	566
2200	140	130	130	138	538
2300	189	212	204	213	818

24-Hour Totals: 17319

Peak Volume Information

	Hour	Volume
A.M.	830	754
P.M.	1530	1603
Daily	1530	1603

County: 87
 Station: 0004
 Description: RAMP 87037202
 Start Date: 10/18/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	163	118	96	89	466
0100	54	47	44	39	184
0200	29	26	25	25	105
0300	23	23	18	20	84
0400	15	31	31	29	106
0500	27	29	27	51	134
0600	59	67	77	99	302
0700	191	175	167	212	745
0800	191	200	202	194	787
0900	162	177	170	173	682
1000	150	159	175	185	669
1100	189	226	219	204	838
1200	203	221	185	221	830
1300	244	270	250	251	1015
1400	326	316	416	344	1402
1500	388	437	403	387	1615
1600	405	376	428	374	1583
1700	344	417	354	292	1407
1800	292	318	260	278	1148
1900	237	271	223	159	890
2000	169	170	143	145	627
2100	169	157	130	122	578
2200	149	127	137	107	520
2300	163	184	133	144	624

24-Hour Totals: 17341

Peak Volume Information

	Hour	Volume
A.M.	745	805
P.M.	1515	1632
Daily	1515	1632

County: 87
 Station: 0004
 Description: RAMP 87037202
 Start Date: 10/19/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	138	132	88	71	429
0100	54	60	43	39	196
0200	29	35	35	31	130
0300	24	26	15	25	90
0400	30	26	28	26	110
0500	29	35	37	48	149
0600	46	80	94	121	341
0700	165	206	170	173	714
0800	177	179	208	185	749
0900	149	190	165	182	686
1000	179	191	201	181	752
1100	208	194	211	211	824
1200	204	227	198	225	854
1300	220	239	258	264	981
1400	290	313	392	318	1313
1500	343	389	429	388	1549
1600	430	422	456	411	1719
1700	421	451	447	354	1673
1800	392	355	326	294	1367
1900	259	249	223	154	885
2000	161	158	127	122	568
2100	133	136	124	97	490
2200	123	113	146	101	483
2300	167	146	131	108	552

24-Hour Totals: 17604

Peak Volume Information

	Hour	Volume
A.M.	800	749
P.M.	1630	1739
Daily	1630	1739

County: 87
 Station: 0005
 Description: SB ALTON RD TO WB I-195
 Start Date: 10/17/2017
 Start Time: 0000

Direction: S

Time	1st	2nd	3rd	4th	Total
0000	48	34	23	24	129
0100	26	19	17	18	80
0200	11	7	13	9	40
0300	9	10	8	9	36
0400	12	14	26	21	73
0500	27	51	75	77	230
0600	87	159	201	219	666
0700	270	315	307	306	1198
0800	357	364	350	307	1378
0900	310	295	271	299	1175
1000	337	324	249	275	1185
1100	372	300	201	202	1075
1200	342	294	257	272	1165
1300	271	263	220	262	1016
1400	266	285	304	246	1101
1500	284	286	314	376	1260
1600	317	206	311	161	995
1700	300	374	260	220	1154
1800	207	219	204	192	822
1900	171	176	134	142	623
2000	130	108	101	89	428
2100	89	79	82	117	367
2200	74	69	80	55	278
2300	64	68	48	57	237

24-Hour Totals: 16711

Peak Volume Information

	Hour	Volume
A.M.	800	1378
P.M.	1515	1293
Daily	800	1378

County: 87
 Station: 0005
 Description: SB ALTON RD TO WB I-195
 Start Date: 10/18/2017
 Start Time: 0000

Direction: S

Time	1st	2nd	3rd	4th	Total
0000	45	37	32	31	145
0100	15	18	13	11	57
0200	14	14	11	6	45
0300	6	7	8	10	31
0400	14	20	29	28	91
0500	25	45	79	66	215
0600	107	160	229	235	731
0700	267	290	320	311	1188
0800	327	333	338	316	1314
0900	278	296	293	257	1124
1000	317	268	240	285	1110
1100	243	276	303	261	1083
1200	234	245	259	208	946
1300	210	227	236	253	926
1400	175	280	235	256	946
1500	282	312	199	304	1097
1600	272	290	313	236	1111
1700	302	134	312	230	978
1800	198	202	216	182	798
1900	145	211	185	162	703
2000	149	88	131	118	486
2100	76	109	83	81	349
2200	70	59	76	71	276
2300	63	86	67	45	261

24-Hour Totals: 16011

Peak Volume Information

	Hour	Volume
A.M.	800	1314
P.M.	1545	1179
Daily	800	1314

County: 87
 Station: 0005
 Description: SB ALTON RD TO WB I-195
 Start Date: 10/19/2017
 Start Time: 0000

Direction: S

Time	1st	2nd	3rd	4th	Total
0000	52	35	30	22	139
0100	19	23	20	12	74
0200	16	14	19	11	60
0300	12	14	8	10	44
0400	9	13	25	15	62
0500	35	43	78	77	233
0600	97	157	192	236	682
0700	260	266	319	272	1117
0800	355	367	353	316	1391
0900	282	305	282	292	1161
1000	286	293	239	268	1086
1100	274	280	221	120	895
1200	259	263	278	225	1025
1300	229	256	243	288	1016
1400	299	292	269	298	1158
1500	295	317	344	285	1241
1600	312	311	340	305	1268
1700	290	286	264	269	1109
1800	204	211	221	202	838
1900	198	131	230	203	762
2000	203	162	148	127	640
2100	100	127	75	97	399
2200	71	64	51	68	254
2300	81	52	50	47	230

24-Hour Totals: 16884

Peak Volume Information

	Hour	Volume
A.M.	800	1391
P.M.	1600	1268
Daily	800	1391

County: 87
 Station: 0006
 Description: RAMP FROM MOUNT SINAI MEDICAL CENTER TO WB I-195
 Start Date: 10/17/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	10	2	7	3	22
0100	4	4	0	0	8
0200	4	1	0	2	7
0300	2	2	1	1	6
0400	1	0	1	1	3
0500	1	2	9	5	17
0600	9	19	10	15	53
0700	29	16	33	27	105
0800	48	36	25	22	131
0900	19	37	27	29	112
1000	35	25	40	41	141
1100	19	47	39	26	131
1200	14	60	37	51	162
1300	32	41	51	44	168
1400	54	68	86	88	296
1500	168	111	235	134	648
1600	137	90	129	73	429
1700	169	108	96	78	451
1800	77	50	50	54	231
1900	36	45	109	70	260
2000	40	27	34	27	128
2100	15	12	14	8	49
2200	14	9	20	10	53
2300	21	15	57	11	104

24-Hour Totals: 3715

Peak Volume Information

	Hour	Volume
A.M.	730	144
P.M.	1500	648
Daily	1500	648

County: 87
 Station: 0006
 Description: RAMP FROM MOUNT SINAI MEDICAL CENTER TO WB I-195
 Start Date: 10/18/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	10	5	2	0	17
0100	2	4	2	0	8
0200	0	0	6	0	6
0300	1	5	1	0	7
0400	0	4	0	2	6
0500	0	6	3	13	22
0600	8	11	14	17	50
0700	19	14	22	26	81
0800	45	36	19	23	123
0900	18	12	27	16	73
1000	21	20	32	31	104
1100	30	31	30	39	130
1200	42	46	45	43	176
1300	60	28	61	50	199
1400	94	66	92	96	348
1500	139	135	162	133	569
1600	119	89	130	97	435
1700	140	103	109	81	433
1800	69	71	56	64	260
1900	29	30	102	47	208
2000	33	52	30	19	134
2100	11	10	14	16	51
2200	16	11	19	12	58
2300	17	16	48	16	97

24-Hour Totals: 3595

Peak Volume Information

	Hour	Volume
A.M.	730	129
P.M.	1500	569
Daily	1500	569

County: 87
 Station: 0006
 Description: RAMP FROM MOUNT SINAI MEDICAL CENTER TO WB I-195
 Start Date: 10/19/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	11	4	4	1	20
0100	1	2	1	1	5
0200	2	0	2	1	5
0300	1	1	1	0	3
0400	0	1	0	0	1
0500	2	5	3	8	18
0600	7	14	18	17	56
0700	16	20	29	24	89
0800	38	60	31	21	150
0900	17	21	27	32	97
1000	31	28	30	34	123
1100	44	35	44	31	154
1200	53	33	40	28	154
1300	36	31	49	46	162
1400	51	55	80	60	246
1500	165	101	242	115	623
1600	122	91	138	96	447
1700	196	161	109	84	550
1800	70	61	50	38	219
1900	29	26	104	46	205
2000	27	31	29	20	107
2100	15	10	7	13	45
2200	11	14	20	9	54
2300	12	6	64	25	107

24-Hour Totals: 3640

Peak Volume Information

	Hour	Volume
A.M.	745	153
P.M.	1500	623
Daily	1500	623

County: 87
 Station: 0007
 Description: NB ALTON RD, S OF I-195
 Start Date: 10/17/2017
 Start Time: 0000

Direction: N

Time	1st	2nd	3rd	4th	Total
0000	121	153	85	87	446
0100	65	42	31	33	171
0200	37	31	34	23	125
0300	20	36	29	34	119
0400	29	22	28	30	109
0500	31	45	46	55	177
0600	63	71	105	144	383
0700	186	265	179	225	855
0800	244	270	290	256	1060
0900	270	281	237	246	1034
1000	242	246	276	281	1045
1100	281	280	331	341	1233
1200	329	313	319	356	1317
1300	354	353	353	362	1422
1400	427	455	530	436	1848
1500	465	515	546	430	1956
1600	560	536	480	462	2038
1700	480	484	452	457	1873
1800	450	436	404	363	1653
1900	352	363	302	268	1285
2000	252	210	210	198	870
2100	191	201	190	190	772
2200	183	167	171	192	713
2300	239	260	239	238	976

24-Hour Totals: 23480

Peak Volume Information

	Hour	Volume
A.M.	830	1097
P.M.	1530	2072
Daily	1530	2072

County: 87
 Station: 0007
 Description: NB ALTON RD, S OF I-195
 Start Date: 10/18/2017
 Start Time: 0000

Direction: N

Time	1st	2nd	3rd	4th	Total
0000	198	136	113	103	550
0100	67	63	55	50	235
0200	35	34	31	30	130
0300	26	23	25	25	99
0400	18	41	37	31	127
0500	33	31	35	55	154
0600	73	79	93	122	367
0700	232	230	190	282	934
0800	259	261	262	293	1075
0900	248	260	255	251	1014
1000	217	234	272	276	999
1100	246	293	310	320	1169
1200	312	338	275	334	1259
1300	347	366	354	345	1412
1400	420	424	553	450	1847
1500	493	539	502	500	2034
1600	523	496	552	485	2056
1700	458	547	470	411	1886
1800	401	421	374	367	1563
1900	316	359	272	242	1189
2000	249	232	196	203	880
2100	239	208	168	165	780
2200	201	172	182	117	672
2300	217	225	164	166	772

24-Hour Totals: 23203

Peak Volume Information

	Hour	Volume
A.M.	800	1075
P.M.	1545	2071
Daily	1545	2071

County: 87
 Station: 0007
 Description: NB ALTON RD, S OF I-195
 Start Date: 10/19/2017
 Start Time: 0000

Direction: N

Time	1st	2nd	3rd	4th	Total
0000	173	159	111	86	529
0100	69	67	43	40	219
0200	29	35	36	35	135
0300	29	28	22	32	111
0400	36	30	34	29	129
0500	41	44	46	62	193
0600	59	94	109	147	409
0700	203	270	222	243	938
0800	237	243	284	281	1045
0900	200	259	242	262	963
1000	238	282	287	270	1077
1100	289	282	321	320	1212
1200	279	331	308	317	1235
1300	318	341	368	353	1380
1400	391	432	516	440	1779
1500	440	490	541	508	1979
1600	543	534	570	539	2186
1700	542	546	580	474	2142
1800	466	463	461	404	1794
1900	373	338	300	231	1242
2000	249	228	199	177	853
2100	182	199	183	121	685
2200	168	164	187	134	653
2300	195	194	160	122	671

24-Hour Totals: 23559

Peak Volume Information

	Hour	Volume
A.M.	800	1045
P.M.	1645	2207
Daily	1645	2207

County: 87
 Station: 0008
 Description: SB ALTON RD, S OF I-195
 Start Date: 10/17/2017
 Start Time: 0000

Direction: S

Time	1st	2nd	3rd	4th	Total
0000	19	16	16	12	63
0100	5	14	8	3	30
0200	6	1	2	2	11
0300	4	6	2	2	14
0400	3	5	7	5	20
0500	8	5	16	27	56
0600	22	28	42	69	161
0700	55	80	52	75	262
0800	79	76	89	100	344
0900	106	116	104	130	456
1000	121	87	69	104	381
1100	136	116	89	93	434
1200	126	128	96	94	444
1300	99	115	80	94	388
1400	105	99	99	91	394
1500	77	100	95	114	386
1600	93	109	136	90	428
1700	110	118	101	111	440
1800	98	97	103	117	415
1900	100	91	81	78	350
2000	47	73	59	51	230
2100	54	38	42	35	169
2200	31	33	18	19	101
2300	24	25	11	12	72

24-Hour Totals: 6049

Peak Volume Information

	Hour	Volume
A.M.	845	426
P.M.	1630	454
Daily	915	471

County: 87
 Station: 0008
 Description: SB ALTON RD, S OF I-195
 Start Date: 10/18/2017
 Start Time: 0000

Direction: S

Time	1st	2nd	3rd	4th	Total
0000	11	13	16	12	52
0100	10	8	7	5	30
0200	9	3	6	4	22
0300	4	1	3	3	11
0400	1	5	5	8	19
0500	7	11	25	22	65
0600	29	15	43	71	158
0700	55	62	61	76	254
0800	100	96	86	107	389
0900	97	100	89	100	386
1000	117	123	106	117	463
1100	114	139	135	118	506
1200	109	96	110	96	411
1300	103	110	104	110	427
1400	78	105	86	89	358
1500	93	87	74	105	359
1600	88	82	105	113	388
1700	112	114	134	115	475
1800	103	94	100	100	397
1900	62	78	83	86	309
2000	47	48	50	61	206
2100	49	44	34	33	160
2200	34	33	27	30	124
2300	21	32	28	20	101

24-Hour Totals: 6070

Peak Volume Information

	Hour	Volume
A.M.	845	393
P.M.	1700	475
Daily	1100	506

County: 87
 Station: 0008
 Description: SB ALTON RD, S OF I-195
 Start Date: 10/19/2017
 Start Time: 0000

Direction: S

Time	1st	2nd	3rd	4th	Total
0000	21	14	21	10	66
0100	8	15	4	7	34
0200	8	6	2	5	21
0300	6	6	6	3	21
0400	4	3	3	4	14
0500	8	10	15	18	51
0600	22	24	47	56	149
0700	54	57	52	75	238
0800	86	86	90	104	366
0900	119	89	109	124	441
1000	112	116	110	120	458
1100	123	102	102	51	378
1200	104	119	117	80	420
1300	95	101	90	91	377
1400	99	90	124	92	405
1500	90	90	86	100	366
1600	91	97	126	131	445
1700	134	122	131	84	471
1800	102	86	113	116	417
1900	112	58	126	140	436
2000	142	118	101	99	460
2100	63	76	48	41	228
2200	34	36	24	42	136
2300	32	10	23	12	77

24-Hour Totals: 6475

Peak Volume Information

	Hour	Volume
A.M.	845	421
P.M.	1645	518
Daily	1930	526

County: 87
 Station: 0009
 Description: RAMP 87037201: RAMP 87004024 TO NB ALTON RD
 Start Date: 10/17/2017
 Start Time: 0000

Direction: N

Time	1st	2nd	3rd	4th	Total
0000	42	38	44	30	154
0100	19	18	13	5	55
0200	11	7	7	8	33
0300	7	5	11	4	27
0400	8	20	27	38	93
0500	37	60	111	147	355
0600	199	276	244	222	941
0700	285	318	340	315	1258
0800	338	275	325	308	1246
0900	291	295	276	257	1119
1000	216	262	246	212	936
1100	193	178	188	175	734
1200	196	226	226	216	864
1300	207	237	192	205	841
1400	218	261	266	279	1024
1500	236	239	227	226	928
1600	235	256	241	254	986
1700	275	296	310	234	1115
1800	316	356	351	377	1400
1900	288	207	212	187	894
2000	180	159	156	147	642
2100	142	110	128	139	519
2200	131	96	129	107	463
2300	95	60	69	56	280

24-Hour Totals: 16907

Peak Volume Information

	Hour	Volume
A.M.	715	1311
P.M.	1800	1400
Daily	1800	1400

County: 87
 Station: 0009
 Description: RAMP 87037201: RAMP 87004024 TO NB ALTON RD
 Start Date: 10/18/2017
 Start Time: 0000

Direction: N

Time	1st	2nd	3rd	4th	Total
0000	41	36	37	37	151
0100	25	20	13	13	71
0200	15	14	11	10	50
0300	7	8	11	13	39
0400	9	18	26	33	86
0500	37	63	97	173	370
0600	207	283	296	285	1071
0700	303	263	313	352	1231
0800	340	321	335	325	1321
0900	286	224	281	238	1029
1000	225	182	219	226	852
1100	195	196	197	189	777
1200	209	194	190	201	794
1300	176	214	227	208	825
1400	210	234	269	257	970
1500	246	211	217	221	895
1600	226	219	235	256	936
1700	233	212	293	300	1038
1800	302	317	304	322	1245
1900	281	245	228	208	962
2000	203	192	179	136	710
2100	161	140	120	123	544
2200	115	106	111	114	446
2300	103	84	91	108	386

24-Hour Totals: 16799

Peak Volume Information

	Hour	Volume
A.M.	745	1348
P.M.	1800	1245
Daily	745	1348

County: 87
 Station: 0009
 Description: RAMP 87037201: RAMP 87004024 TO NB ALTON RD
 Start Date: 10/19/2017
 Start Time: 0000

Direction: N

Time	1st	2nd	3rd	4th	Total
0000	89	57	42	39	227
0100	35	31	17	14	97
0200	11	10	8	12	41
0300	7	8	8	18	41
0400	13	16	19	39	87
0500	38	64	114	140	356
0600	194	269	297	270	1030
0700	288	278	348	357	1271
0800	343	300	283	295	1221
0900	309	252	261	241	1063
1000	237	235	241	220	933
1100	202	197	168	188	755
1200	134	180	220	173	707
1300	210	197	232	208	847
1400	190	223	190	233	836
1500	205	193	210	245	853
1600	207	239	212	253	911
1700	287	295	288	292	1162
1800	308	302	312	301	1223
1900	361	281	257	201	1100
2000	175	173	154	134	636
2100	122	129	118	110	479
2200	102	101	114	95	412
2300	77	67	60	64	268

24-Hour Totals: 16556

Peak Volume Information

	Hour	Volume
A.M.	730	1348
P.M.	1800	1223
Daily	730	1348

County: 87
 Station: 0010
 Description: RAMP 87004024: EB I-195 TO ALTON RD
 Start Date: 10/17/2017
 Start Time: 0000

Direction: E

Time	1st	2nd	3rd	4th	Total
0000	116	101	94	85	396
0100	70	55	43	35	203
0200	39	30	19	25	113
0300	30	33	30	32	125
0400	33	65	134	185	417
0500	120	139	258	312	829
0600	376	492	525	510	1903
0700	601	605	610	670	2486
0800	614	598	679	660	2551
0900	601	576	602	526	2305
1000	492	516	524	481	2013
1100	396	405	440	434	1675
1200	406	453	474	412	1745
1300	460	452	401	432	1745
1400	424	524	525	544	2017
1500	453	484	469	438	1844
1600	450	465	469	474	1858
1700	453	474	471	429	1827
1800	570	561	545	577	2253
1900	503	472	451	399	1825
2000	357	391	372	321	1441
2100	299	218	282	278	1077
2200	263	236	274	253	1026
2300	190	171	144	142	647

24-Hour Totals: 34321

Peak Volume Information

	Hour	Volume
A.M.	745	2561
P.M.	1800	2253
Daily	745	2561

County: 87
 Station: 0010
 Description: RAMP 87004024: EB I-195 TO ALTON RD
 Start Date: 10/18/2017
 Start Time: 0000

Direction: E

Time	1st	2nd	3rd	4th	Total
0000	109	92	97	82	380
0100	60	46	46	44	196
0200	38	34	36	28	136
0300	27	34	42	43	146
0400	33	54	121	179	387
0500	143	127	248	319	837
0600	375	496	561	580	2012
0700	633	556	614	648	2451
0800	674	575	670	694	2613
0900	602	492	554	534	2182
1000	467	422	504	457	1850
1100	392	441	437	432	1702
1200	426	417	433	447	1723
1300	444	443	476	456	1819
1400	480	497	541	507	2025
1500	492	469	439	449	1849
1600	447	444	453	472	1816
1700	422	430	520	488	1860
1800	501	547	560	573	2181
1900	526	444	439	398	1807
2000	418	383	333	302	1436
2100	316	307	264	268	1155
2200	260	242	294	267	1063
2300	210	171	208	192	781

24-Hour Totals: 34407

Peak Volume Information

	Hour	Volume
A.M.	800	2613
P.M.	1800	2181
Daily	800	2613

County: 87
 Station: 0010
 Description: RAMP 87004024: EB I-195 TO ALTON RD
 Start Date: 10/19/2017
 Start Time: 0000

Direction: E

Time	1st	2nd	3rd	4th	Total
0000	166	118	102	72	458
0100	86	72	45	57	260
0200	48	30	30	36	144
0300	27	30	41	42	140
0400	43	56	122	191	412
0500	125	148	253	308	834
0600	344	481	600	565	1990
0700	594	544	677	671	2486
0800	624	599	603	651	2477
0900	686	581	593	582	2442
1000	520	505	534	500	2059
1100	446	407	384	411	1648
1200	346	412	445	405	1608
1300	458	408	484	471	1821
1400	388	471	455	476	1790
1500	469	413	431	500	1813
1600	401	422	435	468	1726
1700	451	482	477	475	1885
1800	525	509	489	544	2067
1900	559	486	422	382	1849
2000	331	319	295	279	1224
2100	250	275	261	242	1028
2200	229	240	244	234	947
2300	160	152	134	146	592

24-Hour Totals: 33700

Peak Volume Information

	Hour	Volume
A.M.	730	2571
P.M.	1800	2067
Daily	730	2571

County: 87
 Station: 0011
 Description: ALTON RD, N. OF CHASE AVE
 Start Date: 10/17/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	148	168	97	93	506	95	76	71	68	310	816	
0100	85	48	49	48	230	60	49	42	32	183	413	
0200	41	34	38	29	142	38	25	12	24	99	241	
0300	24	36	36	39	135	28	36	22	31	117	252	
0400	34	25	34	35	128	29	49	109	161	348	476	
0500	35	45	57	60	197	85	89	162	199	535	732	
0600	85	79	129	167	460	189	239	349	388	1165	1625	
0700	254	290	255	247	1046	404	423	382	465	1674	2720	
0800	302	317	357	309	1285	420	429	476	513	1838	3123	
0900	302	313	278	289	1182	490	466	461	451	1868	3050	
1000	300	290	337	315	1242	436	412	398	415	1661	2903	
1100	336	331	402	389	1458	391	363	391	392	1537	2995	
1200	392	372	392	399	1555	398	394	402	360	1554	3109	
1300	412	401	432	447	1692	383	373	331	350	1437	3129	
1400	472	531	591	518	2112	349	391	419	409	1568	3680	
1500	533	585	606	515	2239	342	387	397	346	1472	3711	
1600	619	558	549	521	2247	361	349	430	363	1503	3750	
1700	517	547	508	511	2083	330	358	361	357	1406	3489	
1800	485	481	439	428	1833	389	352	369	394	1504	3337	
1900	393	382	344	300	1419	376	392	363	321	1452	2871	
2000	291	251	240	240	1022	263	318	305	250	1136	2158	
2100	227	248	213	206	894	240	167	209	187	803	1697	
2200	230	213	193	207	843	179	182	179	172	712	1555	
2300	260	275	239	267	1041	130	137	103	103	473	1514	
24-Hour Totals:					26991						26355	53346

Peak Volume Information

	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	800	1285	830	1945	830	3226
P.M.	1515	2325	1400	1568	1515	3816
Daily	1515	2325	830	1945	1515	3816

County: 87
 Station: 0011
 Description: ALTON RD, N. OF CHASE AVE
 Start Date: 10/18/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	215	149	131	132	627	91	72	81	63	307	934
0100	77	69	60	56	262	51	36	42	34	163	425
0200	43	39	33	35	150	36	33	27	25	121	271
0300	27	27	33	31	118	26	29	35	35	125	243
0400	22	42	41	43	148	25	44	92	160	321	469
0500	39	36	45	63	183	110	90	168	183	551	734
0600	81	93	115	144	433	182	248	316	402	1148	1581
0700	261	272	235	337	1105	428	380	427	445	1680	2785
0800	318	333	362	350	1363	466	430	458	531	1885	3248
0900	290	299	296	283	1168	516	413	428	410	1767	2935
1000	265	287	304	340	1196	417	407	393	399	1616	2812
1100	307	351	353	368	1379	366	423	398	391	1578	2957
1200	368	410	315	402	1495	359	353	395	389	1496	2991
1300	410	419	420	410	1659	397	408	395	374	1574	3233
1400	468	485	632	504	2089	377	418	382	418	1595	3684
1500	544	610	560	557	2271	380	360	364	387	1491	3762
1600	579	559	598	518	2254	354	351	382	364	1451	3705
1700	528	621	524	450	2123	352	335	408	375	1470	3593
1800	452	450	414	408	1724	366	369	372	387	1494	3218
1900	363	387	320	283	1353	332	321	337	313	1303	2656
2000	297	277	238	244	1056	277	272	230	244	1023	2079
2100	276	243	203	183	905	233	217	208	198	856	1761
2200	230	205	204	167	806	189	195	215	198	797	1603
2300	236	258	189	192	875	143	128	153	114	538	1413
24-Hour Totals:	26742					26350					53092

	Peak Volume Information					
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	800	1363	815	1935	815	3270
P.M.	1515	2306	1415	1598	1430	3830
Daily	1515	2306	815	1935	1430	3830

County: 87
 Station: 0011
 Description: ALTON RD, N. OF CHASE AVE
 Start Date: 10/19/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	188	172	133	92	585	101	80	89	50	320	905	
0100	80	70	60	55	265	62	57	38	52	209	474	
0200	40	47	44	41	172	43	29	26	31	129	301	
0300	32	31	25	41	129	30	29	38	32	129	258	
0400	41	34	37	36	148	36	44	108	160	348	496	
0500	45	51	53	72	221	99	90	162	178	529	750	
0600	82	87	141	166	476	184	246	341	412	1183	1659	
0700	267	309	283	279	1138	404	360	411	471	1646	2784	
0800	292	318	355	319	1284	405	435	457	535	1832	3116	
0900	292	316	274	300	1182	521	447	474	492	1934	3116	
1000	294	327	340	321	1282	470	425	430	439	1764	3046	
1100	356	400	367	375	1498	396	375	368	319	1458	2956	
1200	328	386	362	371	1447	372	451	450	371	1644	3091	
1300	380	396	421	392	1589	437	402	472	459	1770	3359	
1400	464	491	554	503	2012	398	421	431	382	1632	3644	
1500	502	565	595	560	2222	435	385	348	382	1550	3772	
1600	617	602	593	596	2408	348	343	372	387	1450	3858	
1700	568	617	631	532	2348	337	373	363	323	1396	3744	
1800	524	518	512	444	1998	374	317	344	341	1376	3374	
1900	429	375	337	275	1416	380	310	320	324	1334	2750	
2000	272	267	218	208	965	331	281	260	272	1144	2109	
2100	210	232	207	162	811	209	239	205	202	855	1666	
2200	192	183	216	148	739	179	193	159	194	725	1464	
2300	225	205	167	143	740	143	99	101	106	449	1189	
24-Hour Totals:					27075						26806	53881

Peak Volume Information

	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	800	1284	845	1977	830	3242
P.M.	1645	2412	1300	1770	1645	3872
Daily	1645	2412	845	1977	1645	3872

County: 87
 Station: 0012
 Description: RAMP 87004023: NE 36 ST TO EB I-195
 Start Date: 10/24/2017
 Start Time: 0000

Direction: E

Time	1st	2nd	3rd	4th	Total
0000	69	61	53	41	224
0100	43	48	32	28	151
0200	28	30	23	28	109
0300	20	15	20	14	69
0400	18	23	25	33	99
0500	34	40	58	77	209
0600	76	113	147	196	532
0700	180	220	223	231	854
0800	235	243	246	238	962
0900	189	190	240	213	832
1000	185	216	229	199	829
1100	181	184	203	221	789
1200	215	225	224	218	882
1300	222	210	234	235	901
1400	208	242	256	248	954
1500	218	224	245	214	901
1600	225	210	201	233	869
1700	226	224	208	231	889
1800	209	246	247	206	908
1900	235	206	218	220	879
2000	241	216	178	179	814
2100	172	187	219	153	731
2200	171	163	163	134	631
2300	150	124	111	111	496

24-Hour Totals: 15514

Peak Volume Information

	Hour	Volume
A.M.	800	962
P.M.	1415	964
Daily	1415	964

County: 87
 Station: 0012
 Description: RAMP 87004023: NE 36 ST TO EB I-195
 Start Date: 10/25/2017
 Start Time: 0000

Direction: E

Time	1st	2nd	3rd	4th	Total
0000	78	65	73	63	279
0100	56	57	49	32	194
0200	36	28	26	36	126
0300	25	20	36	27	108
0400	20	20	28	36	104
0500	24	41	61	60	186
0600	76	128	171	159	534
0700	163	199	222	210	794
0800	190	216	214	208	828
0900	181	189	212	189	771
1000	181	166	176	200	723
1100	179	184	200	213	776
1200	206	217	201	204	828
1300	206	207	238	224	875
1400	208	245	254	229	936
1500	242	211	233	197	883
1600	230	227	210	229	896
1700	228	215	257	247	947
1800	204	237	206	237	884
1900	212	224	204	211	851
2000	199	212	166	152	729
2100	164	182	170	161	677
2200	155	144	157	132	588
2300	140	127	112	115	494

24-Hour Totals: 15011

Peak Volume Information

	Hour	Volume
A.M.	730	838
P.M.	1415	970
Daily	1415	970

County: 87
 Station: 0012
 Description: RAMP 87004023: NE 36 ST TO EB I-195
 Start Date: 10/26/2017
 Start Time: 0000

Direction: E

Time	1st	2nd	3rd	4th	Total
0000	99	73	89	70	331
0100	62	66	48	62	238
0200	48	26	38	44	156
0300	34	37	44	27	142
0400	33	31	25	40	129
0500	32	60	67	71	230
0600	84	114	157	163	518
0700	157	172	238	170	737
0800	201	213	212	208	834
0900	182	185	210	184	761
1000	181	176	176	201	734
1100	179	189	202	215	785
1200	158	184	203	189	734
1300	241	206	218	217	882
1400	214	233	250	224	921
1500	234	209	214	213	870
1600	216	221	235	230	902
1700	224	207	216	242	889
1800	221	223	244	225	913
1900	253	212	202	211	878
2000	187	178	172	189	726
2100	149	150	148	151	598
2200	148	153	176	117	594
2300	101	126	99	93	419

24-Hour Totals: 14921

Peak Volume Information

	Hour	Volume
A.M.	800	834
P.M.	1415	941
Daily	1815	945

County: 87
 Station: 0013
 Description: RAMP 87004022: WB I-195 TO NE 38 ST
 Start Date: 10/24/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	80	81	50	61	272
0100	43	31	23	27	124
0200	28	20	18	18	84
0300	13	14	17	9	53
0400	9	15	20	19	63
0500	19	18	25	38	100
0600	29	43	50	82	204
0700	114	176	197	246	733
0800	164	165	171	180	680
0900	195	168	190	173	726
1000	179	164	140	155	638
1100	178	170	167	179	694
1200	185	173	175	168	701
1300	171	147	169	170	657
1400	152	213	229	213	807
1500	231	279	242	231	983
1600	274	283	275	290	1122
1700	280	293	315	307	1195
1800	262	268	224	181	935
1900	231	180	157	132	700
2000	117	144	96	107	464
2100	89	102	99	78	368
2200	93	80	85	86	344
2300	96	125	88	101	410

24-Hour Totals: 13057

Peak Volume Information

	Hour	Volume
A.M.	715	783
P.M.	1700	1195
Daily	1700	1195

County: 87
 Station: 0013
 Description: RAMP 87004022: WB I-195 TO NE 38 ST
 Start Date: 10/25/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	79	73	56	39	247
0100	34	27	21	17	99
0200	13	22	11	12	58
0300	11	12	11	12	46
0400	12	10	12	11	45
0500	12	18	18	30	78
0600	29	41	73	83	226
0700	109	162	183	221	675
0800	190	193	180	217	780
0900	201	157	193	181	732
1000	166	178	163	193	700
1100	161	166	187	176	690
1200	161	179	171	164	675
1300	167	140	192	174	673
1400	189	177	247	228	841
1500	221	290	248	232	991
1600	276	268	296	315	1155
1700	286	316	362	386	1350
1800	283	301	247	201	1032
1900	187	190	148	143	668
2000	110	131	112	88	441
2100	101	79	77	70	327
2200	90	98	87	76	351
2300	124	123	121	85	453
24-Hour Totals:					13333

Peak Volume Information

	Hour	Volume
A.M.	815	791
P.M.	1700	1350
Daily	1700	1350

County: 87
 Station: 0013
 Description: RAMP 87004022: WB I-195 TO NE 38 ST
 Start Date: 10/26/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	78	56	66	47	247
0100	39	21	37	19	116
0200	25	21	13	12	71
0300	14	8	6	15	43
0400	12	10	10	13	45
0500	16	19	18	27	80
0600	35	47	58	77	217
0700	105	153	174	220	652
0800	237	197	219	212	865
0900	188	200	179	181	748
1000	160	157	180	201	698
1100	148	176	173	210	707
1200	193	202	193	181	769
1300	173	184	154	170	681
1400	198	202	207	213	820
1500	239	246	259	288	1032
1600	296	266	305	277	1144
1700	251	306	291	297	1145
1800	249	274	243	219	985
1900	204	221	178	167	770
2000	115	141	115	129	500
2100	94	126	88	83	391
2200	99	94	86	99	378
2300	85	115	101	96	397
24-Hour Totals:					13501

Peak Volume Information

	Hour	Volume
A.M.	745	873
P.M.	1545	1155
Daily	1545	1155

County: 87
 Station: 0014
 Description: RAMP 87004021: EB I-195 TO WB NE 36 ST
 Start Date: 10/24/2017
 Start Time: 0000

Direction: E

Time	1st	2nd	3rd	4th	Total
0000	35	36	23	23	117
0100	27	12	24	12	75
0200	18	10	9	10	47
0300	5	6	8	14	33
0400	18	16	18	26	78
0500	36	46	93	84	259
0600	94	147	169	159	569
0700	158	164	190	209	721
0800	178	170	194	191	733
0900	183	217	206	207	813
1000	200	189	165	145	699
1100	166	159	125	171	621
1200	160	162	185	205	712
1300	153	127	150	133	563
1400	141	206	195	160	702
1500	175	131	174	167	647
1600	134	143	173	149	599
1700	157	163	165	127	612
1800	173	216	237	178	804
1900	194	173	187	139	693
2000	125	125	121	106	477
2100	110	107	107	97	421
2200	91	117	90	96	394
2300	53	64	50	44	211

24-Hour Totals: 11600

Peak Volume Information

	Hour	Volume
A.M.	845	797
P.M.	1800	804
Daily	915	830

County: 87
 Station: 0014
 Description: RAMP 87004021: EB I-195 TO WB NE 36 ST
 Start Date: 10/25/2017
 Start Time: 0000

Direction: E

Time	1st	2nd	3rd	4th	Total
0000	42	37	34	25	138
0100	15	20	22	14	71
0200	11	13	11	13	48
0300	7	9	11	11	38
0400	17	15	14	18	64
0500	36	60	71	107	274
0600	121	154	155	192	622
0700	196	182	221	260	859
0800	208	174	188	190	760
0900	216	214	208	215	853
1000	235	175	158	174	742
1100	124	113	167	157	561
1200	146	145	156	171	618
1300	148	148	148	145	589
1400	134	168	181	235	718
1500	245	162	177	176	760
1600	167	156	176	164	663
1700	188	191	183	192	754
1800	219	173	181	174	747
1900	223	227	200	197	847
2000	194	146	134	101	575
2100	108	106	106	114	434
2200	114	83	81	88	366
2300	77	72	67	55	271

24-Hour Totals: 12372

Peak Volume Information

	Hour	Volume
A.M.	715	871
P.M.	1415	829
Daily	915	872

County: 87
 Station: 0014
 Description: RAMP 87004021: EB I-195 TO WB NE 36 ST
 Start Date: 10/26/2017
 Start Time: 0000

Direction: E

Time	1st	2nd	3rd	4th	Total
0000	63	36	36	27	162
0100	19	12	10	12	53
0200	11	13	9	14	47
0300	5	11	6	12	34
0400	11	15	13	18	57
0500	28	37	67	101	233
0600	116	148	148	190	602
0700	169	107	39	200	515
0800	201	169	194	205	769
0900	193	207	206	198	804
1000	189	189	165	155	698
1100	156	159	145	171	631
1200	160	172	185	197	714
1300	153	127	150	133	563
1400	141	206	195	160	702
1500	183	151	174	167	675
1600	182	173	173	149	677
1700	157	163	185	227	732
1800	213	206	237	178	834
1900	184	173	187	149	693
2000	128	112	129	120	489
2100	117	123	103	84	427
2200	80	120	79	85	364
2300	60	47	49	55	211

24-Hour Totals: 11686

Peak Volume Information

	Hour	Volume
A.M.	845	811
P.M.	1745	883
Daily	1745	883

County: 87
 Station: 0015
 Description: RAMP 87004020: NE 38 ST TO WB I-195
 Start Date: 10/24/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	42	35	27	17	121
0100	27	24	26	19	96
0200	16	19	16	13	64
0300	8	12	16	15	51
0400	17	22	19	19	77
0500	26	36	41	67	170
0600	62	95	160	176	493
0700	258	238	240	241	977
0800	219	243	240	214	916
0900	185	196	168	160	709
1000	140	152	154	119	565
1100	140	124	122	127	513
1200	122	109	120	93	444
1300	121	112	120	109	462
1400	126	143	125	146	540
1500	182	173	167	198	720
1600	181	187	168	123	659
1700	147	158	146	125	576
1800	96	101	89	84	370
1900	114	117	88	102	421
2000	88	84	70	62	304
2100	69	63	57	69	258
2200	78	67	56	61	262
2300	45	42	39	35	161

24-Hour Totals: 9929

Peak Volume Information

	Hour	Volume
A.M.	700	977
P.M.	1545	734
Daily	700	977

County: 87
 Station: 0015
 Description: RAMP 87004020: NE 38 ST TO WB I-195
 Start Date: 10/25/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	45	29	17	25	116
0100	15	20	19	14	68
0200	16	14	26	13	69
0300	11	10	11	18	50
0400	10	14	13	23	60
0500	28	31	47	52	158
0600	76	83	123	143	425
0700	264	231	235	235	965
0800	209	240	256	218	923
0900	188	142	138	155	623
1000	131	120	133	137	521
1100	112	112	101	118	443
1200	104	122	124	104	454
1300	113	101	118	129	461
1400	115	125	117	127	484
1500	179	164	166	183	692
1600	196	155	140	142	633
1700	154	147	130	152	583
1800	82	90	101	91	364
1900	105	119	97	99	420
2000	79	86	74	67	306
2100	56	67	61	69	253
2200	70	61	51	53	235
2300	39	39	58	46	182

24-Hour Totals: 9488

Peak Volume Information

	Hour	Volume
A.M.	700	965
P.M.	1515	709
Daily	700	965

County: 87
 Station: 0015
 Description: RAMP 87004020: NE 38 ST TO WB I-195
 Start Date: 10/26/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	39	22	17	17	95
0100	16	9	8	4	37
0200	12	3	9	7	31
0300	8	8	6	8	30
0400	5	11	6	7	29
0500	11	16	23	52	102
0600	91	110	102	129	432
0700	190	191	208	224	813
0800	233	211	246	200	890
0900	169	180	190	162	701
1000	168	164	150	147	629
1100	114	112	121	112	459
1200	105	116	121	129	471
1300	115	110	124	132	481
1400	130	145	127	135	537
1500	169	177	156	178	680
1600	162	147	162	134	605
1700	143	140	147	134	564
1800	95	105	88	74	362
1900	93	97	73	99	362
2000	72	66	56	54	248
2100	70	66	56	76	268
2200	58	62	51	45	216
2300	43	29	31	25	128

24-Hour Totals: 9170

Peak Volume Information

	Hour	Volume
A.M.	745	914
P.M.	1500	680
Daily	745	914

County: 87
 Station: 0016
 Description: US-1, N OF NE 38 ST
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	114	98	81	74	367	66	60	56	45	227	594
0100	67	62	41	29	199	48	39	31	31	149	348
0200	31	20	32	18	101	30	21	19	20	90	191
0300	19	25	14	25	83	9	22	30	17	78	161
0400	19	18	23	23	83	22	22	29	43	116	199
0500	28	25	42	50	145	48	76	127	120	371	516
0600	64	68	94	120	346	151	205	354	455	1165	1511
0700	162	188	228	206	784	427	429	469	467	1792	2576
0800	182	184	159	195	720	460	492	428	409	1789	2509
0900	207	199	213	253	872	391	417	382	342	1532	2404
1000	219	239	237	227	922	295	326	300	251	1172	2094
1100	234	234	231	249	948	264	235	292	180	971	1919
1200	254	245	227	232	958	218	290	255	243	1006	1964
1300	223	265	213	247	948	297	295	324	305	1221	2169
1400	242	266	293	290	1091	343	301	317	325	1286	2377
1500	270	322	321	259	1172	336	329	269	278	1212	2384
1600	284	316	320	346	1266	353	329	329	293	1304	2570
1700	353	350	364	392	1459	315	334	325	294	1268	2727
1800	358	341	319	300	1318	335	315	296	283	1229	2547
1900	267	254	271	230	1022	303	281	290	287	1161	2183
2000	221	200	214	170	805	294	282	279	262	1117	1922
2100	151	156	166	171	644	284	257	227	213	981	1625
2200	155	167	152	169	643	197	191	170	185	743	1386
2300	153	155	172	165	645	165	156	143	131	595	1240
24-Hour Totals:	17541					22575					40116

	Peak Volume Information					
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	845	814	730	1888	730	2688
P.M.	1715	1464	1430	1307	1715	2752
Daily	1715	1464	730	1888	1715	2752

County: 87
 Station: 0016
 Description: US-1, N OF NE 38 ST
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	164	151	149	151	615	112	100	120	84	416	1031		
0100	133	117	97	77	424	97	69	71	65	302	726		
0200	85	65	78	72	300	54	44	47	49	194	494		
0300	80	67	75	62	284	58	38	53	52	201	485		
0400	60	36	39	34	169	49	37	50	52	188	357		
0500	32	39	45	64	180	59	90	117	137	403	583		
0600	74	67	105	99	345	168	216	346	427	1157	1502		
0700	134	181	198	205	718	435	468	400	347	1650	2368		
0800	219	245	235	240	939	352	333	368	349	1402	2341		
0900	206	187	201	230	824	348	354	342	326	1370	2194		
1000	236	225	227	223	911	281	312	287	252	1132	2043		
1100	236	224	205	275	940	282	295	272	281	1130	2070		
1200	223	245	249	242	959	201	291	258	256	1006	1965		
1300	237	254	210	256	957	302	300	298	291	1191	2148		
1400	256	252	336	319	1163	288	293	273	293	1147	2310		
1500	316	286	303	305	1210	336	302	386	341	1365	2575		
1600	309	304	286	300	1199	260	296	328	302	1186	2385		
1700	311	322	361	326	1320	297	280	307	316	1200	2520		
1800	312	303	282	292	1189	274	310	322	332	1238	2427		
1900	256	229	237	259	981	311	305	284	274	1174	2155		
2000	241	205	197	180	823	239	260	225	190	914	1737		
2100	235	183	180	153	751	195	152	184	147	678	1429		
2200	165	198	163	174	700	144	144	144	133	565	1265		
2300	156	175	176	157	664	113	91	113	86	403	1067		
24-Hour Totals:						18565						21612	40177

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	800	939	645	1730	715	2370
P.M.	1715	1321	1500	1365	1500	2575
Daily	1715	1321	645	1730	1500	2575

County: 87
 Station: 0016
 Description: US-1, N OF NE 38 ST
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	139	119	85	73	416	77	62	55	54	248	664		
0100	69	46	54	45	214	36	42	38	35	151	365		
0200	40	31	30	26	127	52	32	23	21	128	255		
0300	28	16	20	23	87	22	12	31	27	92	179		
0400	19	27	16	23	85	28	24	35	43	130	215		
0500	28	39	44	48	159	57	101	99	139	396	555		
0600	70	81	113	110	374	140	238	370	436	1184	1558		
0700	157	183	229	227	796	444	441	357	375	1617	2413		
0800	196	225	253	183	857	309	378	351	361	1399	2256		
0900	206	238	196	227	867	362	342	310	353	1367	2234		
1000	239	203	224	216	882	355	306	347	331	1339	2221		
1100	228	200	239	233	900	300	249	228	179	956	1856		
1200	228	260	242	243	973	244	234	233	188	899	1872		
1300	238	269	243	287	1037	278	371	268	296	1213	2250		
1400	234	253	300	293	1080	304	288	341	334	1267	2347		
1500	303	327	287	298	1215	333	325	340	286	1284	2499		
1600	286	318	339	327	1270	309	315	334	290	1248	2518		
1700	332	316	333	342	1323	334	354	322	348	1358	2681		
1800	339	308	317	340	1304	342	320	348	301	1311	2615		
1900	271	294	267	265	1097	308	339	313	293	1253	2350		
2000	263	242	204	216	925	270	250	226	207	953	1878		
2100	178	153	138	139	608	214	199	157	161	731	1339		
2200	117	146	137	120	520	158	145	131	147	581	1101		
2300	131	172	161	133	597	110	83	88	95	376	973		
24-Hour Totals:						17713						21481	39194

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	745	901	645	1678	700	2413
P.M.	1715	1330	1715	1366	1715	2696
Daily	1715	1330	630	1691	1715	2696

County: 87
 Station: 0018
 Description: NE 36 ST, E OF FERERAL HWY
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	65	44	37	34	180	31	40	33	22	126	306
0100	29	28	20	13	90	34	28	15	11	88	178
0200	13	19	17	16	65	17	13	12	11	53	118
0300	24	13	13	9	59	6	12	9	10	37	96
0400	18	11	13	26	68	11	20	8	6	45	113
0500	25	21	42	55	143	22	14	22	25	83	226
0600	67	64	98	110	339	45	52	49	51	197	536
0700	117	118	143	133	511	58	45	61	56	220	731
0800	142	153	134	126	555	57	53	54	62	226	781
0900	138	118	108	128	492	75	92	93	101	361	853
1000	152	136	141	111	540	104	95	94	78	371	911
1100	140	130	108	128	506	74	78	89	81	322	828
1200	150	150	123	158	581	82	85	80	81	328	909
1300	136	147	170	160	613	95	106	107	90	398	1011
1400	153	158	140	161	612	102	102	98	109	411	1023
1500	123	108	138	144	513	82	96	101	117	396	909
1600	167	109	122	144	542	118	110	117	129	474	1016
1700	128	146	130	124	528	107	117	108	111	443	971
1800	113	113	108	111	445	117	103	89	92	401	846
1900	120	118	121	129	488	115	109	124	102	450	938
2000	127	110	131	106	474	109	99	103	90	401	875
2100	164	141	130	145	580	44	55	47	56	202	782
2200	147	121	119	128	515	62	66	57	51	236	751
2300	151	113	142	109	515	71	60	63	60	254	769
24-Hour Totals:	9954					6523					16477

	Peak Volume Information					
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	730	571	845	322	845	812
P.M.	1330	641	1600	474	1330	1042
Daily	1330	641	1600	474	1330	1042

County: 87
 Station: 0018
 Description: NE 36 ST, E OF FERERAL HWY
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	121	107	128	125	481	60	66	55	44	225	706		
0100	105	88	76	73	342	57	46	57	58	218	560		
0200	55	71	70	61	257	42	46	32	25	145	402		
0300	68	84	79	54	285	30	51	50	40	171	456		
0400	45	29	29	33	136	30	32	20	20	102	238		
0500	30	34	50	54	168	24	35	22	50	131	299		
0600	70	77	111	108	366	48	45	51	80	224	590		
0700	97	130	137	105	469	73	68	58	51	250	719		
0800	114	129	126	128	497	50	59	66	66	241	738		
0900	142	127	123	122	514	76	82	82	105	345	859		
1000	132	119	155	135	541	100	82	73	100	355	896		
1100	147	144	124	143	558	81	84	74	94	333	891		
1200	143	167	160	140	610	88	91	94	86	359	969		
1300	182	151	176	148	657	86	84	111	73	354	1011		
1400	153	152	137	190	632	83	98	85	97	363	995		
1500	132	111	133	99	475	80	96	81	83	340	815		
1600	99	96	101	109	405	83	90	98	125	396	801		
1700	102	104	97	62	365	125	87	118	122	452	817		
1800	98	111	111	115	435	87	116	106	118	427	862		
1900	122	114	121	121	478	107	129	115	108	459	937		
2000	126	103	145	122	496	76	87	75	85	323	819		
2100	128	112	121	103	464	67	70	91	60	288	752		
2200	112	92	103	105	412	51	61	69	69	250	662		
2300	108	76	80	62	326	55	60	65	56	236	562		
24-Hour Totals:						10369						6987	17356

Peak Volume Information

	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	815	525	845	306	845	826
P.M.	1300	657	1645	455	1245	1016
Daily	1300	657	1845	469	1245	1016

County: 87
 Station: 0018
 Description: NE 36 ST, E OF FERERAL HWY
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	69	55	49	45	218	51	52	34	40	177	395		
0100	31	31	22	28	112	30	23	21	20	94	206		
0200	27	24	13	15	79	25	14	16	13	68	147		
0300	14	16	20	12	62	13	13	19	11	56	118		
0400	17	12	29	26	84	7	10	7	10	34	118		
0500	24	33	34	60	151	14	24	24	32	94	245		
0600	70	89	93	98	350	42	43	61	61	207	557		
0700	96	125	134	114	469	68	50	69	52	239	708		
0800	109	136	124	124	493	66	60	47	73	246	739		
0900	119	123	115	128	485	93	64	83	78	318	803		
1000	127	120	122	131	500	112	96	71	81	360	860		
1100	154	138	140	147	579	66	87	74	68	295	874		
1200	153	164	147	174	638	75	73	79	84	311	949		
1300	128	148	169	156	601	91	96	100	79	366	967		
1400	145	157	149	167	618	96	101	86	104	387	1005		
1500	117	117	111	86	431	108	104	121	107	440	871		
1600	106	96	111	94	407	114	122	100	127	463	870		
1700	114	106	106	113	439	134	121	141	116	512	951		
1800	110	118	125	92	445	132	137	93	108	470	915		
1900	108	106	108	107	429	117	113	124	119	473	902		
2000	144	134	127	111	516	115	91	103	75	384	900		
2100	123	111	121	125	480	76	54	38	49	217	697		
2200	111	92	106	105	414	65	52	48	29	194	608		
2300	106	72	102	61	341	47	51	38	44	180	521		
24-Hour Totals:						9341						6585	15926

	Peak Volume Information					
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	815	503	845	313	845	794
P.M.	1200	638	1730	526	1400	1005
Daily	1200	638	1730	526	1400	1005

County: 87
 Station: 0019
 Description: NE 2 AVE, N OF NE 39 ST
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	24	15	20	7	66	22	14	12	7	55	121
0100	6	15	9	2	32	4	8	9	5	26	58
0200	5	5	8	4	22	1	4	12	5	22	44
0300	5	2	2	3	12	4	3	3	2	12	24
0400	3	0	2	1	6	4	6	5	9	24	30
0500	6	3	4	9	22	9	10	19	21	59	81
0600	11	20	11	18	60	34	43	74	94	245	305
0700	56	87	68	42	253	79	115	132	140	466	719
0800	29	48	51	68	196	156	121	153	185	615	811
0900	61	58	51	48	218	157	115	118	147	537	755
1000	67	52	45	62	226	118	110	111	99	438	664
1100	71	67	53	56	247	102	92	66	101	361	608
1200	60	72	53	70	255	85	116	109	121	431	686
1300	72	74	61	61	268	111	109	137	123	480	748
1400	64	83	57	89	293	126	118	104	100	448	741
1500	74	101	97	117	389	101	92	99	128	420	809
1600	116	102	110	126	454	79	101	72	96	348	802
1700	129	122	99	115	465	85	81	80	83	329	794
1800	112	87	119	61	379	116	70	92	70	348	727
1900	63	63	63	62	251	81	63	62	65	271	522
2000	37	58	51	45	191	50	51	60	61	222	413
2100	44	57	42	39	182	52	59	46	42	199	381
2200	39	52	32	36	159	44	39	50	53	186	345
2300	39	47	43	40	169	50	42	44	29	165	334
24-Hour Totals:	4815					6707					11522

Peak Volume Information

	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	700	253	815	616	830	848
P.M.	1630	487	1330	504	1530	839
Daily	1630	487	815	616	830	848

County: 87
 Station: 0019
 Description: NE 2 AVE, N OF NE 39 ST
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	33	31	24	21	109	31	20	27	26	104	213
0100	28	25	18	19	90	18	20	17	24	79	169
0200	10	8	11	10	39	18	21	12	16	67	106
0300	7	16	5	6	34	10	11	6	14	41	75
0400	3	6	9	5	23	5	7	10	16	38	61
0500	10	7	11	15	43	13	9	26	20	68	111
0600	16	17	16	21	70	22	41	70	77	210	280
0700	30	50	58	27	165	92	141	168	147	548	713
0800	49	43	45	41	178	160	123	125	140	548	726
0900	64	56	56	47	223	126	118	142	114	500	723
1000	67	74	64	67	272	121	132	117	133	503	775
1100	80	63	66	60	269	91	104	102	83	380	649
1200	63	58	74	89	284	113	118	107	121	459	743
1300	76	56	80	71	283	122	114	133	144	513	796
1400	99	87	80	72	338	118	111	104	97	430	768
1500	79	123	108	96	406	116	113	138	117	484	890
1600	103	109	116	116	444	105	85	111	85	386	830
1700	112	87	108	94	401	104	81	109	101	395	796
1800	113	111	88	92	404	96	92	99	80	367	771
1900	92	68	78	73	311	107	66	73	77	323	634
2000	62	46	62	55	225	73	69	59	51	252	477
2100	55	56	44	45	200	54	52	50	47	203	403
2200	40	41	61	41	183	43	55	40	44	182	365
2300	41	35	23	27	126	37	26	23	21	107	233
24-Hour Totals:	5120					7187					12307

	Peak Volume Information					
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	845	217	715	616	715	800
P.M.	1615	453	1300	513	1515	903
Daily	1615	453	715	616	1515	903

County: 87
 Station: 0019
 Description: NE 2 AVE, N OF NE 39 ST
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	15	24	29	9	77	18	20	9	13	60	137
0100	11	5	8	6	30	12	6	10	7	35	65
0200	9	4	12	6	31	6	7	1	9	23	54
0300	4	4	5	7	20	3	4	4	2	13	33
0400	5	1	4	1	11	3	3	5	7	18	29
0500	8	7	14	10	39	10	9	21	27	67	106
0600	14	18	25	20	77	31	44	70	84	229	306
0700	39	49	62	36	186	96	140	180	132	548	734
0800	43	45	50	55	193	162	151	142	140	595	788
0900	55	57	61	60	233	146	101	131	141	519	752
1000	52	53	67	73	245	122	106	99	84	411	656
1100	56	68	58	61	243	134	91	100	73	398	641
1200	70	72	60	65	267	96	96	100	129	421	688
1300	64	67	82	51	264	109	94	106	92	401	665
1400	68	76	55	70	269	109	123	126	106	464	733
1500	81	67	64	91	303	123	109	157	120	509	812
1600	118	110	108	108	444	110	70	93	107	380	824
1700	121	120	125	98	464	107	79	103	97	386	850
1800	122	102	78	77	379	116	95	84	81	376	755
1900	84	67	71	46	268	91	89	76	73	329	597
2000	62	46	48	54	210	46	54	60	53	213	423
2100	44	41	31	40	156	52	41	35	42	170	326
2200	36	30	16	21	103	60	32	38	27	157	260
2300	45	32	35	19	131	22	26	26	9	83	214
24-Hour Totals:	4643					6805					11448

Peak Volume Information

	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	845	228	730	625	730	811
P.M.	1645	474	1500	509	1645	870
Daily	1645	474	730	625	1645	870

County: 87
 Station: 0020
 Description: NE 39TH STREET W OF NE 2ND AVENUE
 Start Date: 10/31/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	4	5	5	1	15	4	6	8	0	18	33	
0100	0	2	2	1	5	0	1	0	0	1	6	
0200	4	0	0	0	4	3	0	0	0	3	7	
0300	0	0	0	0	0	3	0	0	0	3	3	
0400	3	0	0	1	4	1	0	2	1	4	8	
0500	0	0	0	0	0	0	0	0	2	2	2	
0600	5	1	4	17	27	7	6	7	18	38	65	
0700	16	28	28	22	94	18	32	34	35	119	213	
0800	20	26	32	36	114	42	39	45	52	178	292	
0900	31	38	43	39	151	50	51	40	41	182	333	
1000	38	36	40	40	154	44	47	53	63	207	361	
1100	30	24	32	32	118	49	61	53	70	233	351	
1200	40	25	45	33	143	51	83	54	98	286	429	
1300	24	59	49	38	170	96	59	51	55	261	431	
1400	41	23	41	27	132	46	51	64	35	196	328	
1500	28	36	31	32	127	38	32	35	44	149	276	
1600	34	32	19	23	108	61	42	39	45	187	295	
1700	29	23	30	27	109	43	44	38	26	151	260	
1800	45	33	43	41	162	42	33	45	49	169	331	
1900	31	36	25	17	109	21	45	20	27	113	222	
2000	34	25	26	10	95	25	21	23	14	83	178	
2100	12	10	10	9	41	14	9	10	16	49	90	
2200	2	6	1	8	17	16	7	12	5	40	57	
2300	7	6	2	1	16	6	10	8	7	31	47	
24-Hour Totals:					1915						2703	4618

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	845	148	830	198	845	341
P.M.	1315	187	1215	331	1245	469
Daily	1315	187	1215	331	1245	469

County: 87
 Station: 0020
 Description: NE 39TH STREET W OF NE 2ND AVENUE
 Start Date: 11/01/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	2	1	1	0	4	6	1	3	2	12	16	
0100	2	1	2	0	5	2	4	2	2	10	15	
0200	2	3	0	0	5	2	2	0	0	4	9	
0300	0	1	4	2	7	0	0	1	0	1	8	
0400	0	1	0	1	2	1	1	0	2	4	6	
0500	1	3	4	1	9	2	0	3	3	8	17	
0600	6	7	17	8	38	2	14	14	12	42	80	
0700	29	29	27	24	109	23	36	41	50	150	259	
0800	35	34	31	37	137	47	55	38	61	201	338	
0900	26	31	44	26	127	48	31	57	44	180	307	
1000	25	36	28	28	117	57	48	56	59	220	337	
1100	57	36	41	49	183	42	42	37	64	185	368	
1200	55	36	43	33	167	71	52	67	48	238	405	
1300	23	34	31	35	123	49	59	46	63	217	340	
1400	21	28	39	30	118	56	54	34	54	198	316	
1500	50	46	59	26	181	43	55	63	43	204	385	
1600	43	52	52	42	189	38	50	56	40	184	373	
1700	32	45	17	27	121	48	49	60	71	228	349	
1800	58	30	38	37	163	57	57	57	52	223	386	
1900	32	34	32	23	121	46	38	42	17	143	264	
2000	18	13	14	22	67	29	30	33	22	114	181	
2100	29	34	15	23	101	11	32	14	32	89	190	
2200	18	15	17	10	60	15	14	12	9	50	110	
2300	8	13	6	3	30	7	10	7	0	24	54	
24-Hour Totals:					2184						2929	5113

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	845	138	815	202	800	338
P.M.	1600	189	1730	245	1200	405
Daily	1600	189	1145	254	1145	437

County: 87
 Station: 0020
 Description: NE 39TH STREET W OF NE 2ND AVENUE
 Start Date: 11/02/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	3	3	7	0	13	5	3	5	0	13	26		
0100	4	6	0	0	10	2	1	0	0	3	13		
0200	1	1	3	0	5	1	0	3	0	4	9		
0300	0	2	1	0	3	0	0	1	0	1	4		
0400	0	0	3	1	4	0	0	2	1	3	7		
0500	0	0	0	0	0	0	0	1	1	2	2		
0600	8	9	22	10	49	0	6	14	14	34	83		
0700	22	28	29	21	100	19	28	35	51	133	233		
0800	17	26	34	33	110	46	45	35	51	177	287		
0900	25	29	49	54	157	55	51	38	43	187	344		
1000	35	32	37	37	141	51	62	53	48	214	355		
1100	27	34	33	46	140	63	56	67	70	256	396		
1200	47	31	60	67	205	45	64	49	66	224	429		
1300	37	30	44	31	142	56	47	50	48	201	343		
1400	38	42	30	33	143	71	58	54	52	235	378		
1500	45	35	48	46	174	47	63	53	50	213	387		
1600	37	58	51	37	183	44	52	50	49	195	378		
1700	34	38	31	41	144	44	44	66	54	208	352		
1800	39	52	43	41	175	68	63	43	28	202	377		
1900	30	33	29	14	106	45	30	30	23	128	234		
2000	22	7	3	3	35	42	21	16	16	95	130		
2100	1	9	10	5	25	10	23	13	12	58	83		
2200	6	9	5	9	29	8	14	9	12	43	72		
2300	2	3	8	7	20	5	10	8	7	30	50		
24-Hour Totals:						2113						2859	4972

Peak Volume Information

	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	845	136	845	195	845	331
P.M.	1200	205	1730	251	1215	430
Daily	1200	205	1100	256	1215	430

County: 87
 Station: 0021
 Description: NE 39th Street E of NE 2nd Avenue
 Start Date: 10/31/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	4	2	0	2	8	12	11	8	10	41	49	
0100	2	3	1	2	8	3	9	8	3	23	31	
0200	3	1	2	11	17	6	5	1	7	19	36	
0300	5	0	0	1	6	4	1	4	4	13	19	
0400	0	0	3	0	3	1	5	1	2	9	12	
0500	2	3	7	11	23	4	4	8	11	27	50	
0600	14	26	18	31	89	18	17	18	34	87	176	
0700	21	36	45	35	137	41	59	54	56	210	347	
0800	36	45	62	71	214	64	53	71	69	257	471	
0900	61	61	46	32	200	68	74	69	63	274	474	
1000	35	28	37	41	141	61	32	68	64	225	366	
1100	27	23	26	27	103	50	60	53	67	230	333	
1200	34	18	20	28	100	50	67	77	63	257	357	
1300	22	27	37	37	123	56	76	45	58	235	358	
1400	24	27	34	24	109	37	45	44	45	171	280	
1500	30	35	38	41	144	61	53	50	61	225	369	
1600	32	44	40	34	150	72	53	52	63	240	390	
1700	41	39	46	37	163	55	49	56	41	201	364	
1800	43	55	54	45	197	56	37	28	39	160	357	
1900	37	35	28	22	122	32	30	29	18	109	231	
2000	23	12	16	11	62	20	16	20	25	81	143	
2100	6	15	10	16	47	11	24	23	24	82	129	
2200	9	10	2	7	28	20	18	7	15	60	88	
2300	6	6	7	2	21	7	10	14	13	44	65	
24-Hour Totals:					2215						3280	5495

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	830	255	830	282	830	537
P.M.	1800	197	1230	272	1545	395
Daily	830	255	830	282	830	537

County: 87
 Station: 0021
 Description: NE 39th Street E of NE 2nd Avenue
 Start Date: 11/01/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	5	3	2	0	10	4	6	2	4	16	26	
0100	2	1	2	1	6	3	5	3	3	14	20	
0200	1	3	0	2	6	0	1	0	4	5	11	
0300	0	0	3	2	5	1	2	4	2	9	14	
0400	4	0	1	0	5	2	0	0	3	5	10	
0500	4	4	8	12	28	3	2	4	3	12	40	
0600	20	19	33	38	110	4	15	16	28	63	173	
0700	33	33	43	49	158	25	35	52	70	182	340	
0800	39	40	51	65	195	64	70	72	76	282	477	
0900	40	45	48	51	184	73	48	53	75	249	433	
1000	32	27	20	36	115	64	40	16	19	139	254	
1100	28	28	31	34	121	14	12	15	23	64	185	
1200	46	31	31	32	140	41	16	30	50	137	277	
1300	28	33	35	32	128	39	68	57	67	231	359	
1400	27	29	32	33	121	60	50	64	66	240	361	
1500	47	69	50	49	215	61	61	63	45	230	445	
1600	53	50	42	44	189	53	56	56	70	235	424	
1700	40	49	37	40	166	61	74	63	71	269	435	
1800	53	51	42	34	180	65	53	55	48	221	401	
1900	34	41	22	26	123	42	47	45	32	166	289	
2000	23	16	10	21	70	33	13	17	20	83	153	
2100	12	16	10	8	46	13	9	9	4	35	81	
2200	7	3	3	3	16	13	8	12	12	45	61	
2300	6	1	3	1	11	17	14	10	2	43	54	
24-Hour Totals:					2348						2975	5323

Peak Volume Information

	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	830	201	815	291	815	487
P.M.	1515	221	1715	273	1715	452
Daily	1515	221	815	291	815	487

County: 87
 Station: 0021
 Description: NE 39th Street E of NE 2nd Avenue
 Start Date: 11/02/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	2	2	3	1	8	7	1	3	4	15	23	
0100	3	2	0	1	6	2	3	1	4	10	16	
0200	0	1	5	1	7	3	5	1	4	13	20	
0300	0	0	1	1	2	2	1	1	2	6	8	
0400	0	0	0	0	0	4	3	1	3	11	11	
0500	2	6	15	18	41	1	3	4	3	11	52	
0600	14	20	20	18	72	4	12	16	25	57	129	
0700	31	36	43	52	162	25	27	53	52	157	319	
0800	40	36	43	55	174	61	65	73	51	250	424	
0900	39	36	48	44	167	51	48	55	68	222	389	
1000	37	27	27	27	118	65	21	48	36	170	288	
1100	35	26	22	48	131	40	31	43	50	164	295	
1200	40	33	39	46	158	86	99	76	84	345	503	
1300	44	27	32	38	141	74	59	73	76	282	423	
1400	31	44	27	38	140	75	83	79	68	305	445	
1500	32	37	41	40	150	60	66	79	83	288	438	
1600	34	44	44	56	178	77	61	57	64	259	437	
1700	57	46	53	55	211	64	64	73	62	263	474	
1800	59	61	57	40	217	54	77	50	45	226	443	
1900	42	25	26	13	106	37	47	34	24	142	248	
2000	20	12	21	11	64	39	29	28	27	123	187	
2100	5	14	11	11	41	21	27	20	16	84	125	
2200	12	5	5	10	32	23	18	17	17	75	107	
2300	6	4	3	4	17	14	19	8	9	50	67	
24-Hour Totals:					2343						3528	5871

Peak Volume Information

	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	845	178	745	251	800	424
P.M.	1745	232	1200	345	1200	503
Daily	1745	232	1200	345	1200	503

County: 87
 Station: 0022
 Description: NE 2ND AVENUE S OF NE 39TH STREET
 Start Date: 10/31/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	9	5	11	4	29	14	22	11	12	59	88		
0100	1	2	0	0	3	5	12	8	2	27	30		
0200	1	1	0	0	2	7	8	3	6	24	26		
0300	2	1	1	5	9	2	3	4	6	15	24		
0400	0	0	0	1	1	3	5	8	12	28	29		
0500	2	3	5	7	17	9	4	18	21	52	69		
0600	8	11	18	17	54	23	35	77	72	207	261		
0700	38	53	51	41	183	77	117	121	134	449	632		
0800	40	51	58	58	207	160	114	140	122	536	743		
0900	55	46	54	43	198	128	133	110	114	485	683		
1000	60	50	45	69	224	97	83	82	91	353	577		
1100	48	48	58	63	217	113	79	72	97	361	578		
1200	49	62	63	79	253	90	106	96	97	389	642		
1300	65	59	58	59	241	117	98	122	98	435	676		
1400	46	66	63	74	249	11	88	103	86	288	537		
1500	66	94	95	107	362	88	88	89	99	364	726		
1600	86	89	106	101	382	71	85	75	72	303	685		
1700	114	121	108	107	450	79	72	70	73	294	744		
1800	107	101	81	95	384	78	84	86	83	331	715		
1900	58	65	40	53	216	61	56	60	44	221	437		
2000	34	44	33	30	141	62	71	53	54	240	381		
2100	35	18	16	20	89	48	35	41	52	176	265		
2200	13	14	14	10	51	30	24	17	26	97	148		
2300	6	17	9	3	35	24	25	21	20	90	125		
24-Hour Totals:						3997						5824	9821

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	815	222	745	548	800	743
P.M.	1700	450	1300	435	1700	744
Daily	1700	450	745	548	1700	744

County: 87
 Station: 0022
 Description: NE 2ND AVENUE S OF NE 39TH STREET
 Start Date: 11/01/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	8	2	3	0	13	14	16	8	7	45	58	
0100	2	2	4	3	11	5	13	8	5	31	42	
0200	0	1	0	1	2	6	8	7	6	27	29	
0300	0	1	0	0	1	5	4	3	2	14	15	
0400	1	0	0	2	3	3	4	3	10	20	23	
0500	0	3	6	8	17	8	10	15	15	48	65	
0600	8	15	18	25	66	20	32	59	82	193	259	
0700	36	58	47	32	173	100	112	160	145	517	690	
0800	27	44	27	53	151	169	129	112	131	541	692	
0900	50	44	46	55	195	144	149	127	124	544	739	
1000	75	43	45	51	214	100	86	92	100	378	592	
1100	49	39	47	66	201	104	88	106	96	394	595	
1200	64	67	44	52	227	94	94	97	90	375	602	
1300	61	67	63	61	252	116	93	97	100	406	658	
1400	69	65	56	62	252	98	100	113	92	403	655	
1500	61	115	88	85	349	98	87	109	89	383	732	
1600	109	90	88	100	387	105	95	78	87	365	752	
1700	82	105	102	99	388	89	79	73	97	338	726	
1800	104	100	84	91	379	89	70	86	70	315	694	
1900	65	59	41	59	224	114	96	65	68	343	567	
2000	54	36	35	28	153	48	45	60	52	205	358	
2100	22	18	17	19	76	45	46	48	38	177	253	
2200	10	23	15	16	64	38	47	37	23	145	209	
2300	15	12	11	9	47	36	33	10	17	96	143	
24-Hour Totals:					3845						6303	10148

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	845	193	730	603	730	753
P.M.	1715	410	1345	411	1515	787
Daily	1715	410	730	603	1515	787

County: 87
 Station: 0022
 Description: NE 2ND AVENUE S OF NE 39TH STREET
 Start Date: 11/02/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	5	6	5	5	21	15	13	8	17	53	74	
0100	3	3	2	3	11	12	8	9	3	32	43	
0200	1	1	5	0	7	0	2	1	6	9	16	
0300	0	1	0	2	3	6	3	3	1	13	16	
0400	0	1	0	5	6	1	2	9	6	18	24	
0500	5	10	20	24	59	8	5	15	21	49	108	
0600	22	18	15	18	73	26	36	76	78	216	289	
0700	35	65	42	41	183	111	111	122	147	491	674	
0800	42	46	41	50	179	148	126	142	147	563	742	
0900	44	33	51	48	176	152	113	100	117	482	658	
1000	72	72	30	74	248	91	85	94	87	357	605	
1100	56	41	65	72	234	93	99	118	102	412	646	
1200	68	64	80	79	291	111	138	127	133	509	800	
1300	68	61	74	65	268	117	100	94	103	414	682	
1400	69	61	74	63	267	102	106	127	115	450	717	
1500	97	86	89	93	365	113	90	95	103	401	766	
1600	117	92	99	83	391	99	84	82	89	354	745	
1700	90	115	106	101	412	101	93	99	100	393	805	
1800	100	81	84	71	336	96	88	73	69	326	662	
1900	54	46	49	31	180	76	71	52	50	249	429	
2000	49	26	28	17	120	51	44	42	41	178	298	
2100	30	26	14	15	85	30	35	31	26	122	207	
2200	6	17	10	9	42	31	22	27	34	114	156	
2300	11	10	8	8	37	25	18	24	19	86	123	
24-Hour Totals:					3994						6291	10285

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	715	190	815	567	815	748
P.M.	1715	422	1215	515	1715	810
Daily	1715	422	815	567	1715	810

County: 87
 Station: 0023
 Description: NE 36 ST, E OF NE 1 AVE
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	46	42	31	33	152	38	46	37	31	152	304
0100	24	25	16	17	82	32	33	21	17	103	185
0200	14	19	12	11	56	15	14	10	6	45	101
0300	21	15	17	13	66	10	9	10	12	41	107
0400	24	11	18	23	76	14	12	8	5	39	115
0500	32	37	65	92	226	20	15	24	23	82	308
0600	108	114	170	174	566	38	48	52	72	210	776
0700	186	159	193	199	737	70	49	88	90	297	1034
0800	189	207	212	203	811	67	84	74	96	321	1132
0900	199	168	234	183	784	91	119	122	122	454	1238
1000	220	209	177	156	762	163	134	142	122	561	1323
1100	168	192	170	176	706	140	141	132	136	549	1255
1200	183	174	203	204	764	148	152	151	148	599	1363
1300	185	190	207	195	777	148	151	150	140	589	1366
1400	170	198	182	220	770	151	137	144	149	581	1351
1500	142	128	141	167	578	113	124	154	164	555	1133
1600	162	147	145	157	611	168	170	132	166	636	1247
1700	152	157	127	140	576	130	163	134	153	580	1156
1800	144	167	162	151	624	136	144	144	147	571	1195
1900	163	179	165	184	691	143	128	129	139	539	1230
2000	150	144	143	134	571	149	121	127	107	504	1075
2100	164	146	142	151	603	100	96	98	109	403	1006
2200	132	155	122	137	546	131	111	117	89	448	994
2300	121	106	150	114	491	134	100	95	83	412	903
24-Hour Totals:	12626					9271					21897

Peak Volume Information

	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	815	821	845	428	845	1232
P.M.	1245	786	1530	656	1245	1383
Daily	930	846	1530	656	930	1387

County: 87
 Station: 0023
 Description: NE 36 ST, E OF NE 1 AVE
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	112	95	119	104	430	86	84	72	57	299	729		
0100	94	80	74	55	303	55	64	67	76	262	565		
0200	59	70	73	58	260	46	59	50	34	189	449		
0300	74	74	70	57	275	45	56	47	28	176	451		
0400	37	30	25	38	130	39	31	20	23	113	243		
0500	32	44	68	91	235	24	28	26	39	117	352		
0600	94	139	177	189	599	36	37	51	84	208	807		
0700	148	184	190	197	719	78	76	84	80	318	1037		
0800	199	155	188	194	736	85	99	83	107	374	1110		
0900	206	172	187	178	743	88	131	123	138	480	1223		
1000	154	192	182	174	702	139	157	133	161	590	1292		
1100	157	150	154	177	638	136	149	161	124	570	1208		
1200	175	174	207	200	756	160	169	166	152	647	1403		
1300	197	175	168	175	715	142	150	152	146	590	1305		
1400	181	177	176	214	748	161	161	132	144	598	1346		
1500	171	166	210	180	727	143	151	159	164	617	1344		
1600	184	144	145	158	631	138	194	185	197	714	1345		
1700	200	169	172	167	708	195	164	193	189	741	1449		
1800	149	166	161	181	657	145	180	143	138	606	1263		
1900	174	158	184	159	675	152	170	147	129	598	1273		
2000	153	173	146	143	615	131	100	131	114	476	1091		
2100	140	125	142	120	527	92	104	101	83	380	907		
2200	102	96	92	103	393	61	78	76	86	301	694		
2300	86	79	71	74	310	82	71	79	62	294	604		
24-Hour Totals:						13232						10258	23490

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	715	770	845	449	845	1208
P.M.	1230	779	1615	771	1700	1449
Daily	1230	779	1615	771	1700	1449

County: 87
 Station: 0023
 Description: NE 36 ST, E OF NE 1 AVE
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	66	53	41	34	194	57	66	38	48	209	403
0100	26	27	18	27	98	31	25	19	21	96	194
0200	23	23	16	13	75	23	19	11	11	64	139
0300	16	15	16	13	60	13	14	13	6	46	106
0400	21	15	21	30	87	12	6	10	10	38	125
0500	37	42	47	93	219	12	29	23	26	90	309
0600	85	134	138	167	524	37	35	52	68	192	716
0700	169	172	177	198	716	75	65	107	82	329	1045
0800	212	209	204	228	853	84	88	82	102	356	1209
0900	203	220	210	217	850	116	110	120	123	469	1319
1000	199	148	160	162	669	170	124	131	122	547	1216
1100	153	160	175	203	691	124	112	134	124	494	1185
1200	183	209	196	215	803	139	135	150	152	576	1379
1300	155	199	218	193	765	147	153	157	150	607	1372
1400	232	181	231	239	883	164	160	159	140	623	1506
1500	170	166	190	171	697	154	174	167	223	718	1415
1600	173	156	181	153	663	218	210	196	200	824	1487
1700	194	170	177	195	736	191	178	180	162	711	1447
1800	209	167	188	188	752	177	160	141	160	638	1390
1900	170	186	186	204	746	184	175	152	165	676	1422
2000	182	179	164	142	667	138	124	106	103	471	1138
2100	145	139	141	134	559	108	83	72	81	344	903
2200	108	107	112	102	429	90	79	55	74	298	727
2300	88	71	71	65	295	62	76	66	70	274	569
24-Hour Totals:	13031					9690					22721

	Peak Volume Information					
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	845	861	845	448	845	1309
P.M.	1400	883	1545	847	1545	1528
Daily	1400	883	1545	847	1545	1528

County: 87
 Station: 0024
 Description: NE 1 AVE, N OF NE 38 ST
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	3	4	0	0	7	1	0	4	2	7	14
0100	1	1	1	0	3	1	2	3	0	6	9
0200	0	1	0	0	1	0	4	1	0	5	6
0300	0	0	0	0	0	3	0	2	1	6	6
0400	0	0	2	1	3	0	1	0	2	3	6
0500	1	0	0	3	4	4	3	4	2	13	17
0600	6	3	8	2	19	3	5	9	4	21	40
0700	10	7	13	10	40	11	23	25	26	85	125
0800	11	14	19	18	62	30	32	36	62	160	222
0900	16	17	27	11	71	56	69	27	39	191	262
1000	31	41	16	17	105	29	53	30	26	138	243
1100	31	14	25	56	126	50	40	28	20	138	264
1200	35	25	31	31	122	25	29	42	44	140	262
1300	38	31	41	36	146	32	31	48	37	148	294
1400	27	41	45	33	146	30	20	12	30	92	238
1500	40	38	47	36	161	35	29	26	31	121	282
1600	43	30	39	22	134	25	26	23	31	105	239
1700	32	38	28	29	127	40	13	11	12	76	203
1800	34	21	19	19	93	21	17	12	18	68	161
1900	24	27	17	11	79	15	8	19	8	50	129
2000	8	16	10	15	49	8	18	7	14	47	96
2100	8	7	7	5	27	8	5	5	9	27	54
2200	4	6	4	4	18	10	9	7	9	35	53
2300	5	5	4	5	19	4	3	2	4	13	32
24-Hour Totals:	1562					1695					3257

	Peak Volume Information					
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	845	78	830	223	830	293
P.M.	1515	164	1245	155	1245	296
Daily	1515	164	830	223	1245	296

County: 87
 Station: 0024
 Description: NE 1 AVE, N OF NE 38 ST
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	4	1	0	2	7	5	5	1	3	14	21		
0100	2	1	3	1	7	1	1	1	4	7	14		
0200	1	0	0	0	1	4	3	0	5	12	13		
0300	1	0	0	1	2	3	0	0	4	7	9		
0400	0	0	1	1	2	0	0	1	0	1	3		
0500	3	3	3	10	19	2	7	0	1	10	29		
0600	3	4	2	6	15	3	7	7	13	30	45		
0700	9	4	8	4	25	14	18	32	18	82	107		
0800	8	15	23	26	72	30	28	31	38	127	199		
0900	14	21	15	19	69	37	43	23	41	144	213		
1000	20	45	45	26	136	35	11	31	33	110	246		
1100	23	39	41	22	125	24	29	31	24	108	233		
1200	32	28	31	38	129	28	38	34	39	139	268		
1300	31	23	33	36	123	36	28	28	35	127	250		
1400	34	25	32	21	112	23	29	34	29	115	227		
1500	42	38	43	43	166	23	31	32	31	117	283		
1600	36	37	33	34	140	18	36	26	27	107	247		
1700	45	27	24	26	122	26	44	16	12	98	220		
1800	37	33	29	25	124	41	14	19	21	95	219		
1900	17	17	20	12	66	28	34	20	15	97	163		
2000	19	13	12	18	62	17	14	0	7	38	100		
2100	8	7	13	2	30	10	11	6	3	30	60		
2200	8	5	7	1	21	6	7	7	4	24	45		
2300	2	7	5	2	16	8	7	6	5	26	42		
24-Hour Totals:						1591						1665	3256

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	830	84	830	149	830	233
P.M.	1500	166	1215	147	1500	283
Daily	1500	166	830	149	1500	283

County: 87
 Station: 0024
 Description: NE 1 AVE, N OF NE 38 ST
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	2	3	3	0	8	2	2	1	1	6	14	
0100	0	0	0	0	0	0	1	0	1	2	2	
0200	0	0	0	0	0	0	1	1	0	2	2	
0300	0	0	2	1	3	0	1	2	1	4	7	
0400	0	0	0	1	1	3	1	1	1	6	7	
0500	1	5	3	5	14	3	5	1	1	10	24	
0600	2	6	9	7	24	3	2	8	8	21	45	
0700	7	16	12	11	46	11	18	18	28	75	121	
0800	7	20	13	20	60	31	25	38	49	143	203	
0900	15	18	27	14	74	48	33	40	56	177	251	
1000	27	74	33	12	146	48	10	46	35	139	285	
1100	57	64	17	10	148	24	15	24	20	83	231	
1200	13	32	20	24	89	17	26	32	29	104	193	
1300	23	21	42	32	118	33	39	23	23	118	236	
1400	21	19	26	26	92	30	23	38	26	117	209	
1500	39	44	36	40	159	24	38	21	35	118	277	
1600	38	38	27	28	131	15	32	21	33	101	232	
1700	24	22	31	27	104	49	30	19	20	118	222	
1800	29	27	23	16	95	33	24	12	21	90	185	
1900	24	29	15	7	75	15	15	14	17	61	136	
2000	8	10	8	6	32	14	9	8	5	36	68	
2100	4	5	6	3	18	15	11	6	3	35	53	
2200	6	3	2	5	16	3	9	4	1	17	33	
2300	3	2	3	8	16	2	3	5	6	16	32	
24-Hour Totals:					1469						1599	3068

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	845	80	845	170	845	250
P.M.	1500	159	1615	135	1500	277
Daily	1015	176	900	177	945	308

County: 87
 Station: 0025
 Description: NE 38TH STREET W OF NE 1ST AVENUE
 Start Date: 10/31/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	2	5	1	0	8	9	13	8	1	31	39		
0100	0	1	1	1	3	4	5	2	0	11	14		
0200	0	1	0	0	1	1	1	2	2	6	7		
0300	0	1	1	4	6	2	3	0	3	8	14		
0400	1	1	2	1	5	1	0	2	7	10	15		
0500	3	5	11	16	35	4	2	5	3	14	49		
0600	10	19	16	29	74	5	10	22	12	49	123		
0700	39	42	31	29	141	20	39	46	33	138	279		
0800	32	42	37	59	170	45	36	41	39	161	331		
0900	53	36	35	36	160	45	48	49	62	204	364		
1000	32	23	22	27	104	51	36	35	43	165	269		
1100	23	16	25	27	91	46	34	34	46	160	251		
1200	20	32	32	31	115	47	57	47	36	187	302		
1300	25	22	31	19	97	49	47	49	41	186	283		
1400	29	25	23	18	95	39	50	49	46	184	279		
1500	22	23	29	20	94	55	54	69	49	227	321		
1600	18	14	15	17	64	57	83	40	42	222	286		
1700	14	26	14	17	71	52	83	56	37	228	299		
1800	38	33	27	34	132	56	39	29	40	164	296		
1900	33	19	17	32	101	29	33	25	35	122	223		
2000	36	39	15	7	97	20	14	25	22	81	178		
2100	11	10	8	11	40	19	15	16	17	67	107		
2200	9	7	6	8	30	11	7	14	7	39	69		
2300	0	5	3	9	17	17	8	6	9	40	57		
24-Hour Totals:						1751						2704	4455

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	815	191	845	181	845	364
P.M.	1800	132	1530	258	1530	339
Daily	815	191	1530	258	845	364

County: 87
 Station: 0025
 Description: NE 38TH STREET W OF NE 1ST AVENUE
 Start Date: 11/01/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	3	2	1	2	8	9	8	7	5	29	37	
0100	0	2	0	1	3	2	6	2	4	14	17	
0200	0	1	0	0	1	3	4	0	2	9	10	
0300	0	0	2	0	2	2	0	2	1	5	7	
0400	0	0	2	2	4	1	2	4	0	7	11	
0500	5	4	7	17	33	3	3	3	6	15	48	
0600	15	13	23	24	75	8	12	20	14	54	129	
0700	31	41	32	28	132	38	29	61	51	179	311	
0800	20	24	39	53	136	48	58	53	55	214	350	
0900	40	48	43	43	174	43	60	52	60	215	389	
1000	27	25	32	31	115	48	47	51	42	188	303	
1100	43	61	47	33	184	36	42	46	46	170	354	
1200	32	21	23	31	107	47	52	50	41	190	297	
1300	31	29	20	20	100	42	46	43	50	181	281	
1400	21	19	24	26	90	51	51	43	56	201	291	
1500	22	27	31	17	97	52	72	68	75	267	364	
1600	27	16	34	23	100	56	36	46	42	180	280	
1700	26	30	30	27	113	52	40	50	42	184	297	
1800	34	35	42	17	128	58	42	49	35	184	312	
1900	31	18	25	21	95	58	47	42	29	176	271	
2000	26	27	20	17	90	38	25	32	34	129	219	
2100	11	8	6	8	33	30	21	18	18	87	120	
2200	6	5	1	4	16	20	29	16	16	81	97	
2300	6	1	2	3	12	10	13	10	9	42	54	
24-Hour Totals:					1848						3001	4849

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	845	184	730	218	845	394
P.M.	1745	138	1515	271	1515	373
Daily	845	184	1515	271	845	394

County: 87
 Station: 0025
 Description: NE 38TH STREET W OF NE 1ST AVENUE
 Start Date: 11/02/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	4	1	1	0	6	11	7	4	3	25	31	
0100	1	0	1	2	4	9	2	2	2	15	19	
0200	1	2	1	1	5	0	1	3	1	5	10	
0300	0	0	0	1	1	5	2	1	2	10	11	
0400	0	0	2	3	5	1	2	1	2	6	11	
0500	1	5	8	14	28	1	4	2	3	10	38	
0600	7	21	20	27	75	2	20	23	25	70	145	
0700	23	34	35	23	115	44	38	57	49	188	303	
0800	28	29	38	43	138	55	56	47	38	196	334	
0900	34	33	33	47	147	57	49	49	53	208	355	
1000	34	27	26	41	128	54	36	53	45	188	316	
1100	32	24	27	19	102	41	51	43	36	171	273	
1200	27	32	26	29	114	57	58	52	42	209	323	
1300	26	29	24	19	98	43	31	45	54	173	271	
1400	25	28	24	23	100	48	39	49	53	189	289	
1500	22	27	36	23	108	63	61	74	65	263	371	
1600	24	23	20	28	95	46	48	43	43	180	275	
1700	24	28	26	25	103	58	49	48	60	215	318	
1800	36	42	37	28	143	65	51	38	33	187	330	
1900	28	21	22	42	113	39	37	28	30	134	247	
2000	27	20	6	15	68	32	21	22	14	89	157	
2100	5	9	3	11	28	19	14	16	8	57	85	
2200	5	7	6	11	29	14	16	17	15	62	91	
2300	2	2	5	1	10	22	6	10	6	44	54	
24-Hour Totals:					1763						2894	4657

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	830	148	730	217	815	342
P.M.	1800	143	1500	263	1500	371
Daily	830	148	1500	263	1500	371

County: 87
 Station: 0026
 Description: N MIAMI AVE, N OF NE 38 ST
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	67	52	33	36	188	29	37	31	21	118	306
0100	42	30	22	13	107	17	16	11	21	65	172
0200	12	18	12	9	51	19	21	28	11	79	130
0300	12	10	6	9	37	10	11	9	8	38	75
0400	2	10	11	18	41	11	8	18	21	58	99
0500	11	15	22	29	77	32	28	56	73	189	266
0600	58	47	57	95	257	87	146	193	240	666	923
0700	92	117	149	161	519	274	318	379	393	1364	1883
0800	181	166	153	143	643	413	407	427	436	1683	2326
0900	143	145	161	143	592	390	395	353	294	1432	2024
1000	155	137	143	176	611	236	245	180	186	847	1458
1100	100	147	147	165	559	180	191	181	182	734	1293
1200	162	180	168	210	720	168	191	179	181	719	1439
1300	194	180	183	186	743	166	186	195	212	759	1502
1400	183	181	199	239	802	189	242	215	192	838	1640
1500	225	265	236	296	1022	186	212	245	206	849	1871
1600	277	328	388	405	1398	215	197	155	163	730	2128
1700	334	347	342	368	1391	178	173	162	180	693	2084
1800	336	335	305	290	1266	170	155	157	138	620	1886
1900	235	171	191	150	747	152	133	122	146	553	1300
2000	168	136	131	125	560	128	115	104	97	444	1004
2100	113	108	94	91	406	81	66	81	67	295	701
2200	106	80	87	90	363	85	67	91	61	304	667
2300	78	61	76	55	270	64	62	55	45	226	496
24-Hour Totals:	13370					14303					27673

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	745	661	800	1683	800	2326
P.M.	1630	1474	1515	878	1615	2148
Daily	1630	1474	800	1683	800	2326

County: 87
 Station: 0026
 Description: N MIAMI AVE, N OF NE 38 ST
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	54	54	37	20	165	56	41	27	27	151	316
0100	28	13	31	21	93	22	21	15	26	84	177
0200	14	18	10	9	51	18	14	13	8	53	104
0300	14	8	6	7	35	13	11	13	6	43	78
0400	7	11	8	7	33	18	14	22	17	71	104
0500	14	13	26	30	83	29	38	52	84	203	286
0600	48	37	52	66	203	92	137	249	258	736	939
0700	115	96	117	136	464	286	333	391	407	1417	1881
0800	143	147	120	121	531	409	419	382	363	1573	2104
0900	128	131	106	159	524	365	363	339	331	1398	1922
1000	134	143	117	181	575	260	217	187	190	854	1429
1100	153	136	156	184	629	173	185	169	192	719	1348
1200	179	195	186	188	748	193	232	201	193	819	1567
1300	197	168	161	153	679	166	186	195	212	759	1438
1400	176	191	213	192	772	189	242	215	229	875	1647
1500	228	219	236	258	941	244	226	243	207	920	1861
1600	258	267	347	320	1192	184	176	175	169	704	1896
1700	374	327	375	338	1414	180	153	121	151	605	2019
1800	345	333	298	267	1243	143	162	165	161	631	1874
1900	228	217	190	191	826	167	178	143	146	634	1460
2000	158	148	121	143	570	126	118	115	92	451	1021
2100	136	103	126	103	468	98	89	100	87	374	842
2200	98	121	85	94	398	112	87	96	90	385	783
2300	96	86	72	59	313	80	77	59	72	288	601
24-Hour Totals:	12950					14747					27697

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	745	546	730	1626	730	2169
P.M.	1700	1414	1445	942	1630	2045
Daily	1700	1414	730	1626	730	2169

County: 87
 Station: 0026
 Description: N MIAMI AVE, N OF NE 38 ST
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	69	71	57	53	250	49	36	37	50	172	422
0100	53	45	38	31	167	38	44	36	34	152	319
0200	35	39	23	35	132	25	23	28	25	101	233
0300	25	19	20	14	78	33	31	24	29	117	195
0400	21	13	17	13	64	35	24	26	36	121	185
0500	29	20	21	44	114	46	62	61	74	243	357
0600	41	46	35	76	198	78	134	232	232	676	874
0700	101	125	118	152	496	265	367	405	375	1412	1908
0800	142	145	130	115	532	366	358	378	335	1437	1969
0900	134	124	120	137	515	304	310	272	243	1129	1644
1000	104	122	135	129	490	216	201	213	216	846	1336
1100	148	140	149	129	566	192	185	178	196	751	1317
1200	177	193	181	188	739	193	232	201	189	815	1554
1300	146	171	196	195	708	179	193	198	190	760	1468
1400	182	202	192	209	785	213	257	214	245	929	1714
1500	231	255	257	281	1024	245	206	252	208	911	1935
1600	323	357	335	336	1351	198	176	188	196	758	2109
1700	310	363	398	393	1464	198	166	160	161	685	2149
1800	376	356	326	285	1343	162	165	159	151	637	1980
1900	246	212	197	178	833	158	130	126	106	520	1353
2000	140	151	103	129	523	107	92	105	97	401	924
2100	123	103	104	91	421	76	67	70	77	290	711
2200	96	111	73	126	406	68	56	68	52	244	650
2300	113	148	79	58	398	64	53	39	42	198	596
24-Hour Totals:	13597					14305					27902

	Peak Volume Information					
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	745	569	715	1513	730	2061
P.M.	1715	1530	1415	961	1715	2179
Daily	1715	1530	715	1513	1715	2179

County: 87
 Station: 0027
 Description: N MIAMI AVE, N OF NE 36 ST
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	81	74	60	51	266	65	57	40	33	195	461
0100	76	54	41	20	191	29	27	13	16	85	276
0200	30	29	23	20	102	14	20	10	11	55	157
0300	22	19	24	9	74	15	9	9	15	48	122
0400	11	22	21	18	72	24	30	20	34	108	180
0500	24	22	28	45	119	43	55	89	116	303	422
0600	76	74	99	83	332	134	148	134	221	637	969
0700	122	123	136	163	544	249	242	308	346	1145	1689
0800	154	167	159	136	616	292	308	289	333	1222	1838
0900	142	163	147	145	597	291	309	297	323	1220	1817
1000	189	195	158	215	757	258	234	190	249	931	1688
1100	217	241	257	225	940	213	187	238	236	874	1814
1200	234	243	239	269	985	258	262	249	241	1010	1995
1300	276	282	263	262	1083	259	238	196	222	915	1998
1400	257	271	290	291	1109	204	208	240	259	911	2020
1500	302	277	233	287	1099	240	235	200	239	914	2013
1600	275	362	375	335	1347	200	232	207	225	864	2211
1700	319	363	358	348	1388	207	211	194	218	830	2218
1800	352	323	273	274	1222	188	214	205	203	810	2032
1900	256	246	225	227	954	193	200	204	202	799	1753
2000	264	198	194	200	856	186	206	224	226	842	1698
2100	198	181	180	132	691	228	243	233	221	925	1616
2200	140	162	129	182	613	215	232	217	200	864	1477
2300	151	102	133	114	500	118	55	52	54	279	779
24-Hour Totals:	16457					16786					33243

	Peak Volume Information					
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	745	643	730	1254	745	1878
P.M.	1715	1421	1215	1011	1615	2262
Daily	1715	1421	730	1254	1615	2262

County: 87
 Station: 0027
 Description: N MIAMI AVE, N OF NE 36 ST
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	116	91	69	51	327	65	43	38	59	205	532
0100	64	55	49	48	216	37	33	31	18	119	335
0200	36	37	39	46	158	27	11	24	16	78	236
0300	59	27	17	8	111	24	14	8	12	58	169
0400	16	20	20	17	73	18	20	18	22	78	151
0500	18	26	42	43	129	30	38	71	121	260	389
0600	61	77	76	91	305	117	133	173	221	644	949
0700	136	126	136	162	560	205	172	248	253	878	1438
0800	176	160	155	136	627	219	254	209	278	960	1587
0900	150	132	148	153	583	219	373	388	388	1368	1951
1000	194	184	218	207	803	361	323	301	316	1301	2104
1100	206	215	288	268	977	308	321	290	298	1217	2194
1200	275	289	258	295	1117	291	283	288	286	1148	2265
1300	281	277	280	243	1081	278	278	313	282	1151	2232
1400	259	270	290	272	1091	293	234	265	277	1069	2160
1500	322	282	279	288	1171	185	168	208	196	757	1928
1600	323	318	367	329	1337	189	209	177	172	747	2084
1700	374	373	362	328	1437	205	187	206	203	801	2238
1800	358	320	306	284	1268	187	221	238	197	843	2111
1900	275	269	262	244	1050	189	210	188	189	776	1826
2000	272	204	208	241	925	195	173	165	149	682	1607
2100	234	198	216	215	863	171	123	192	163	649	1512
2200	199	201	170	142	712	171	176	155	153	655	1367
2300	154	129	124	122	529	105	109	84	94	392	921
24-Hour Totals:	17450					16836					34286

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	745	653	845	1258	845	1824
P.M.	1630	1443	1315	1166	1245	2288
Daily	1630	1443	915	1510	1245	2288

County: 87
 Station: 0027
 Description: N MIAMI AVE, N OF NE 36 ST
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	112	120	103	78	413	99	81	52	51	283	696
0100	82	91	76	64	313	67	39	53	60	219	532
0200	56	66	53	73	248	27	29	24	27	107	355
0300	80	34	46	30	190	26	20	19	32	97	287
0400	31	31	26	24	112	48	19	27	28	122	234
0500	35	32	37	52	156	41	71	61	135	308	464
0600	51	74	71	106	302	127	170	183	194	674	976
0700	134	133	132	168	567	157	197	215	219	788	1355
0800	159	160	128	135	582	228	198	221	238	885	1467
0900	132	161	134	156	583	262	259	248	274	1043	1626
1000	201	180	240	205	826	238	247	256	245	986	1812
1100	222	227	213	268	930	267	276	303	282	1128	2058
1200	275	289	258	295	1117	281	282	278	276	1117	2234
1300	281	245	311	296	1133	268	247	247	236	998	2131
1400	265	282	284	266	1097	231	192	221	229	873	1970
1500	295	344	300	325	1264	256	250	238	248	992	2256
1600	347	398	362	370	1477	214	181	159	170	724	2201
1700	329	366	394	350	1439	166	163	144	162	635	2074
1800	347	302	311	288	1248	134	148	162	150	594	1842
1900	274	265	236	245	1020	171	164	175	155	665	1685
2000	218	255	156	205	834	147	131	170	149	597	1431
2100	193	174	199	150	716	101	90	105	106	402	1118
2200	162	167	137	172	638	102	96	92	104	394	1032
2300	153	141	112	85	491	93	65	54	64	276	767
24-Hour Totals:	17696					14907					32603

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	730	619	845	1007	845	1569
P.M.	1600	1477	1200	1117	1515	2266
Daily	1600	1477	1130	1148	1515	2266

County: 87
 Station: 0028
 Description: NE 36 ST, E OF N MIAMI AVE
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	52	40	28	33	153	35	48	35	27	145	298		
0100	23	26	15	16	80	28	30	22	16	96	176		
0200	15	16	13	12	56	14	13	8	5	40	96		
0300	21	15	18	18	72	10	9	12	10	41	113		
0400	26	10	17	23	76	11	11	11	6	39	115		
0500	32	32	66	96	226	19	12	22	19	72	298		
0600	110	116	159	178	563	34	41	50	61	186	749		
0700	176	153	167	184	680	64	48	86	69	267	947		
0800	187	210	192	171	760	60	68	79	81	288	1048		
0900	174	177	192	166	709	86	102	110	95	393	1102		
1000	189	187	151	156	683	115	100	108	95	418	1101		
1100	168	180	157	171	676	100	114	105	95	414	1090		
1200	168	171	185	209	733	114	120	117	122	473	1206		
1300	176	197	196	191	760	127	111	116	123	477	1237		
1400	179	209	184	194	766	117	107	120	132	476	1242		
1500	157	162	146	151	616	109	133	147	155	544	1160		
1600	164	166	133	169	632	137	147	128	154	566	1198		
1700	145	142	145	157	589	152	152	155	113	572	1161		
1800	178	178	180	149	685	140	140	119	123	522	1207		
1900	181	174	157	188	700	122	110	112	112	456	1156		
2000	173	142	139	129	583	126	118	112	84	440	1023		
2100	162	138	137	140	577	89	87	85	94	355	932		
2200	119	152	121	113	505	125	103	104	82	414	919		
2300	115	112	142	121	490	118	92	90	80	380	870		
24-Hour Totals:						12370						8074	20444

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	745	773	845	379	845	1093
P.M.	1245	778	1645	613	1245	1254
Daily	1245	778	1645	613	1245	1254

County: 87
 Station: 0028
 Description: NE 36 ST, E OF N MIAMI AVE
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	114	93	122	102	431	84	83	65	53	285	716		
0100	86	76	72	59	293	54	60	66	71	251	544		
0200	55	64	67	65	251	41	56	45	32	174	425		
0300	64	67	68	55	254	39	52	45	23	159	413		
0400	40	32	21	40	133	37	29	16	22	104	237		
0500	28	53	65	99	245	21	25	23	37	106	351		
0600	115	127	165	185	592	31	35	47	62	175	767		
0700	159	166	177	196	698	80	72	86	79	317	1015		
0800	170	162	180	190	702	80	98	75	89	342	1044		
0900	186	172	189	163	710	61	112	93	108	374	1084		
1000	171	174	145	164	654	95	124	89	109	417	1071		
1100	155	149	147	170	621	119	113	140	90	462	1083		
1200	167	162	192	200	721	156	147	132	141	576	1297		
1300	184	155	168	167	674	114	149	137	118	518	1192		
1400	172	170	179	169	690	163	135	133	134	565	1255		
1500	163	164	162	186	675	113	144	135	158	550	1225		
1600	159	149	158	163	629	138	151	145	151	585	1214		
1700	193	155	177	163	688	158	124	157	158	597	1285		
1800	163	182	143	184	672	129	130	125	123	507	1179		
1900	171	155	175	161	662	130	135	139	117	521	1183		
2000	170	176	153	151	650	103	84	113	113	413	1063		
2100	152	113	140	114	519	84	85	100	72	341	860		
2200	102	97	94	102	395	55	73	66	72	266	661		
2300	79	82	71	75	307	76	72	67	65	280	587		
24-Hour Totals:						12866						8885	21751

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	845	737	845	355	845	1092
P.M.	1215	738	1615	605	1200	1297
Daily	1215	738	1615	605	1200	1297

County: 87
 Station: 0028
 Description: NE 36 ST, E OF N MIAMI AVE
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	62	47	45	33	187	57	61	34	42	194	381	
0100	23	23	19	24	89	29	25	18	17	89	178	
0200	26	23	17	8	74	21	18	11	11	61	135	
0300	19	12	17	12	60	12	12	12	5	41	101	
0400	23	14	20	29	86	11	7	11	10	39	125	
0500	34	48	49	89	220	12	25	22	25	84	304	
0600	107	134	147	167	555	32	26	46	55	159	714	
0700	171	166	178	176	691	67	54	96	69	286	977	
0800	186	182	187	195	750	82	90	84	103	359	1109	
0900	173	188	185	182	728	105	109	123	107	444	1172	
1000	177	152	158	145	632	116	108	103	112	439	1071	
1100	150	162	168	191	671	99	92	100	101	392	1063	
1200	169	193	200	209	771	116	99	118	124	457	1228	
1300	148	187	199	186	720	116	130	116	109	471	1191	
1400	217	162	219	215	813	123	123	122	131	499	1312	
1500	161	187	184	176	708	126	136	114	187	563	1271	
1600	158	176	169	173	676	167	158	161	147	633	1309	
1700	193	167	183	193	736	162	169	166	137	634	1370	
1800	183	171	197	203	754	135	145	124	134	538	1292	
1900	179	204	183	210	776	156	160	125	136	577	1353	
2000	177	184	167	139	667	116	119	93	89	417	1084	
2100	150	142	147	134	573	90	80	66	67	303	876	
2200	130	99	106	102	437	80	73	58	69	280	717	
2300	86	73	66	66	291	57	68	65	64	254	545	
24-Hour Totals:					12665						8213	20878

Peak Volume Information

	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	800	750	845	440	845	1181
P.M.	1400	813	1545	673	1700	1370
Daily	1400	813	1545	673	1700	1370

County: 87
 Station: 0029
 Description: NE 36 ST, W OF N MIAMI AVE
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	43	43	33	36	155	43	43	45	50	181	336		
0100	26	32	21	12	91	26	36	17	23	102	193		
0200	14	19	11	16	60	18	22	12	8	60	120		
0300	19	18	19	18	74	15	13	12	9	49	123		
0400	18	12	23	21	74	21	22	14	16	73	147		
0500	24	23	49	61	157	27	33	45	42	147	304		
0600	87	103	122	99	411	55	93	92	103	343	754		
0700	117	114	98	103	432	108	86	103	119	416	848		
0800	112	117	105	109	443	109	112	95	102	418	861		
0900	89	89	104	109	391	104	121	121	121	467	858		
1000	141	128	117	117	503	126	136	120	147	529	1032		
1100	131	136	127	135	529	133	146	124	136	539	1068		
1200	123	122	150	165	560	143	138	151	143	575	1135		
1300	136	141	153	132	562	160	150	152	156	618	1180		
1400	128	145	150	159	582	149	136	132	170	587	1169		
1500	102	98	108	127	435	146	143	161	186	636	1071		
1600	128	126	116	110	480	197	185	167	186	735	1215		
1700	150	135	131	117	533	191	190	170	196	747	1280		
1800	111	128	106	126	471	176	185	160	141	662	1133		
1900	141	136	127	148	552	150	169	152	159	630	1182		
2000	117	108	103	95	423	184	156	164	146	650	1073		
2100	136	100	116	133	485	132	126	161	150	569	1054		
2200	130	154	142	142	568	176	152	151	129	608	1176		
2300	140	138	145	148	571	151	159	162	159	631	1202		
24-Hour Totals:						9542						10972	20514

Peak Volume Information

	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	800	443	845	448	730	873
P.M.	1245	595	1700	747	1700	1280
Daily	1245	595	1700	747	1700	1280

County: 87
 Station: 0029
 Description: NE 36 ST, W OF N MIAMI AVE
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	141	117	141	145	544	135	129	107	105	476	1020		
0100	85	61	82	92	320	90	75	78	94	337	657		
0200	66	81	64	69	280	71	71	60	70	272	552		
0300	48	47	56	90	241	55	59	66	37	217	458		
0400	53	56	34	31	174	40	46	20	27	133	307		
0500	28	47	60	75	210	26	34	36	56	152	362		
0600	92	116	114	116	438	71	78	89	103	341	779		
0700	119	109	110	111	449	107	115	120	134	476	925		
0800	98	105	92	103	398	121	105	124	112	462	860		
0900	110	105	110	119	444	98	131	134	120	483	927		
1000	113	111	118	112	454	115	151	110	140	516	970		
1100	101	136	112	130	479	158	147	148	131	584	1063		
1200	116	138	128	140	522	138	153	123	152	566	1088		
1300	140	103	103	118	464	142	145	121	114	522	986		
1400	128	131	124	113	496	138	127	145	136	546	1042		
1500	95	76	102	122	395	106	147	140	169	562	957		
1600	124	117	109	123	473	172	206	172	155	705	1178		
1700	137	126	123	124	510	199	190	163	173	725	1235		
1800	144	111	124	124	503	158	152	145	153	608	1111		
1900	139	123	141	124	527	143	145	130	136	554	1081		
2000	113	116	103	107	439	116	104	113	111	444	883		
2100	106	79	98	91	374	107	111	111	91	420	794		
2200	79	98	73	106	356	80	99	70	69	318	674		
2300	85	63	72	75	295	71	88	75	69	303	598		
24-Hour Totals:						9785						10722	20507

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	645	454	715	490	700	925
P.M.	1215	546	1615	732	1700	1235
Daily	1215	546	1615	732	1700	1235

County: 87
 Station: 0029
 Description: NE 36 ST, W OF N MIAMI AVE
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	58	55	40	28	181	71	54	44	61	230	411		
0100	35	27	29	28	119	35	35	31	20	121	240		
0200	20	19	27	23	89	33	17	21	17	88	177		
0300	30	18	18	11	77	25	17	16	8	66	143		
0400	21	12	17	31	81	20	20	21	18	79	160		
0500	26	35	55	68	184	24	34	50	64	172	356		
0600	79	119	122	104	424	69	69	85	93	316	740		
0700	116	126	98	90	430	128	93	123	126	470	900		
0800	111	111	100	113	435	119	129	119	114	481	916		
0900	83	101	99	95	378	121	91	130	134	476	854		
1000	140	110	116	129	495	113	143	110	127	493	988		
1100	120	118	151	155	544	134	111	122	146	513	1057		
1200	129	145	148	138	560	147	139	136	137	559	1119		
1300	133	152	149	151	585	120	130	148	160	558	1143		
1400	138	144	156	125	563	168	135	139	120	562	1125		
1500	108	119	119	95	441	147	169	153	174	643	1084		
1600	118	134	118	113	483	196	171	176	181	724	1207		
1700	123	145	141	134	543	203	160	178	161	702	1245		
1800	124	150	136	140	550	165	162	148	141	616	1166		
1900	140	141	117	157	555	157	154	149	153	613	1168		
2000	128	130	94	100	452	135	130	131	108	504	956		
2100	102	96	106	78	382	114	95	76	87	372	754		
2200	95	92	86	87	360	93	88	78	86	345	705		
2300	61	78	60	56	255	71	80	64	61	276	531		
24-Hour Totals:						9166						9979	19145

Peak Volume Information

	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	645	444	730	497	800	916
P.M.	1315	590	1615	731	1700	1245
Daily	1315	590	1615	731	1700	1245

County: 87
 Station: 0030
 Description: RAMP 87004018:N MIAMI AVE TO WB I-195
 Start Date: 10/31/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	107	90	73	97	367
0100	87	95	67	49	298
0200	48	58	65	48	219
0300	57	35	22	22	136
0400	22	24	33	46	125
0500	46	42	65	116	269
0600	128	136	180	214	658
0700	292	326	346	335	1299
0800	300	323	372	354	1349
0900	271	237	209	207	924
1000	219	232	219	238	908
1100	265	258	258	261	1042
1200	247	286	285	261	1079
1300	260	240	281	290	1071
1400	278	281	324	296	1179
1500	315	282	318	297	1212
1600	292	242	222	247	1003
1700	279	284	232	240	1035
1800	262	290	277	241	1070
1900	249	227	216	215	907
2000	224	217	189	176	806
2100	169	144	136	127	576
2200	124	126	151	115	516
2300	112	93	76	88	369

24-Hour Totals: 18417

Peak Volume Information

	Hour	Volume
A.M.	800	1349
P.M.	1430	1217
Daily	800	1349

County: 87
 Station: 0030
 Description: RAMP 87004018:N MIAMI AVE TO WB I-195
 Start Date: 11/01/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	66	62	58	46	232
0100	46	47	24	22	139
0200	26	33	22	21	102
0300	28	17	18	19	82
0400	17	31	36	49	133
0500	39	44	95	105	283
0600	110	151	204	197	662
0700	300	326	371	336	1333
0800	355	313	307	256	1231
0900	277	268	249	223	1017
1000	244	214	242	241	941
1100	231	265	244	273	1013
1200	284	282	267	291	1124
1300	282	290	265	280	1117
1400	290	300	302	312	1204
1500	300	284	314	276	1174
1600	272	256	212	218	958
1700	263	237	240	221	961
1800	220	213	230	232	895
1900	234	258	201	217	910
2000	254	250	227	204	935
2100	179	201	177	174	731
2200	181	145	156	103	585
2300	142	122	112	89	465

24-Hour Totals: 18227

Peak Volume Information

	Hour	Volume
A.M.	715	1388
P.M.	1415	1214
Daily	715	1388

County: 87
 Station: 0030
 Description: RAMP 87004018:N MIAMI AVE TO WB I-195
 Start Date: 11/02/2017
 Start Time: 0000

Direction: W

Time	1st	2nd	3rd	4th	Total
0000	85	94	72	52	303
0100	61	46	38	35	180
0200	43	32	28	27	130
0300	29	15	30	17	91
0400	19	27	28	27	101
0500	45	49	72	97	263
0600	126	164	189	251	730
0700	278	316	360	318	1272
0800	302	362	318	307	1289
0900	217	207	245	213	882
1000	264	230	275	218	987
1100	232	253	258	243	986
1200	269	295	277	278	1119
1300	280	286	325	302	1193
1400	310	297	312	274	1193
1500	312	318	304	252	1186
1600	286	259	221	210	976
1700	223	229	253	214	919
1800	236	271	215	244	966
1900	274	234	242	233	983
2000	244	220	220	213	897
2100	268	224	195	170	857
2200	165	174	147	151	637
2300	147	114	132	97	490

24-Hour Totals: 18630

Peak Volume Information

	Hour	Volume
A.M.	730	1342
P.M.	1330	1234
Daily	730	1342

County: 87
 Station: 0031
 Description: RAMP 87004019: EB SR 112 TO N MIAMI AVE
 Start Date: 10/24/2017
 Start Time: 0000

Direction: E

Time	1st	2nd	3rd	4th	Total
0000	91	67	66	62	286
0100	45	37	38	37	157
0200	31	24	18	25	98
0300	21	19	14	23	77
0400	23	22	16	39	100
0500	35	59	92	127	313
0600	111	156	157	229	653
0700	228	218	238	237	921
0800	252	216	236	284	988
0900	275	305	293	312	1185
1000	298	300	266	263	1127
1100	232	232	242	261	967
1200	300	270	267	259	1096
1300	245	219	241	233	938
1400	258	213	244	273	988
1500	278	257	266	238	1039
1600	221	226	218	230	895
1700	287	285	272	282	1126
1800	295	298	340	257	1190
1900	254	267	268	227	1016
2000	193	174	172	178	717
2100	151	158	128	125	562
2200	145	115	127	118	505
2300	110	92	81	60	343

24-Hour Totals: 17287

Peak Volume Information

	Hour	Volume
A.M.	845	1157
P.M.	1745	1215
Daily	1745	1215

County: 87
 Station: 0031
 Description: RAMP 87004019: EB SR 112 TO N MIAMI AVE
 Start Date: 10/25/2017
 Start Time: 0000

Direction: E

Time	1st	2nd	3rd	4th	Total
0000	63	46	37	40	186
0100	31	43	19	18	111
0200	21	20	10	16	67
0300	14	16	14	15	59
0400	10	16	15	30	71
0500	47	39	73	154	313
0600	158	168	176	212	714
0700	217	240	277	287	1021
0800	262	236	234	275	1007
0900	271	325	279	305	1180
1000	286	287	275	301	1149
1100	258	211	251	248	968
1200	250	258	257	259	1024
1300	267	227	238	211	943
1400	206	215	248	244	913
1500	248	280	287	268	1083
1600	256	221	282	273	1032
1700	245	240	252	264	1001
1800	273	265	256	272	1066
1900	292	308	254	242	1096
2000	235	215	197	186	833
2100	164	145	146	135	590
2200	125	119	109	98	451
2300	101	92	101	89	383
24-Hour Totals:					17261

Peak Volume Information

	Hour	Volume
A.M.	845	1150
P.M.	1515	1091
Daily	915	1195

County: 87
 Station: 0031
 Description: RAMP 87004019: EB SR 112 TO N MIAMI AVE
 Start Date: 10/26/2017
 Start Time: 0000

Direction: E

Time	1st	2nd	3rd	4th	Total
0000	91	75	57	38	261
0100	45	32	32	22	131
0200	35	27	17	23	102
0300	24	19	11	20	74
0400	15	22	17	31	85
0500	46	54	86	125	311
0600	145	178	188	206	717
0700	220	218	201	169	808
0800	191	210	253	278	932
0900	294	250	260	274	1078
1000	265	240	287	267	1059
1100	237	270	289	299	1095
1200	275	281	297	286	1139
1300	252	291	223	261	1027
1400	254	305	283	309	1151
1500	280	270	272	298	1120
1600	284	239	259	240	1022
1700	285	277	273	318	1153
1800	322	330	362	293	1307
1900	303	256	299	241	1099
2000	267	257	266	178	968
2100	176	175	156	163	670
2200	167	193	155	130	645
2300	121	111	106	97	435

24-Hour Totals: 18389

Peak Volume Information

	Hour	Volume
A.M.	845	1082
P.M.	1745	1332
Daily	1745	1332

County: 87
 Station: 0032
 Description: N MIAMI AVENUE N OF NE 34TH STREET
 Start Date: 10/31/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	61	41	43	40	185	13	22	21	12	68	253		
0100	54	30	27	21	132	17	6	13	8	44	176		
0200	20	17	19	15	71	8	10	1	7	26	97		
0300	12	14	13	6	45	4	4	2	7	17	62		
0400	5	15	8	15	43	6	5	5	6	22	65		
0500	17	12	15	31	75	11	26	25	38	100	175		
0600	41	36	56	57	190	62	83	115	142	402	592		
0700	85	81	109	136	411	144	170	182	193	689	1100		
0800	140	138	139	97	514	212	215	246	285	958	1472		
0900	123	146	107	123	499	229	235	200	203	867	1366		
1000	159	124	120	167	570	193	156	158	150	657	1227		
1100	198	227	193	223	841	145	127	149	160	581	1422		
1200	197	224	222	266	909	165	156	171	159	651	1560		
1300	267	331	220	249	1067	166	155	160	136	617	1684		
1400	278	286	275	293	1132	161	139	148	158	606	1738		
1500	256	207	229	231	923	145	151	140	139	575	1498		
1600	232	283	360	260	1135	118	134	124	134	510	1645		
1700	302	322	347	313	1284	146	144	115	160	565	1849		
1800	354	324	273	215	1166	135	148	117	128	528	1694		
1900	251	198	171	176	796	152	134	113	95	494	1290		
2000	172	149	157	133	611	118	97	83	65	363	974		
2100	121	136	152	108	517	65	62	63	58	248	765		
2200	138	125	112	121	496	54	55	50	56	215	711		
2300	101	110	85	68	364	34	37	32	33	136	500		
24-Hour Totals:						13976						9939	23915

Peak Volume Information

	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	745	553	830	995	830	1500
P.M.	1730	1338	1215	652	1730	1896
Daily	1730	1338	830	995	1730	1896

County: 87
 Station: 0032
 Description: N MIAMI AVENUE N OF NE 34TH STREET
 Start Date: 11/01/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	51	31	53	30	165	32	22	25	9	88	253		
0100	44	20	17	31	112	16	21	12	11	60	172		
0200	22	16	17	14	69	6	7	7	2	22	91		
0300	10	15	10	16	51	5	5	9	5	24	75		
0400	9	10	18	25	62	4	5	4	13	26	88		
0500	19	16	25	33	93	7	27	33	62	129	222		
0600	32	40	48	51	171	51	79	116	127	373	544		
0700	90	101	109	136	436	141	162	215	217	735	1171		
0800	170	168	169	127	634	199	188	200	207	794	1428		
0900	103	136	117	133	489	198	195	209	194	796	1285		
1000	179	144	130	187	640	177	158	148	155	638	1278		
1100	168	207	213	233	821	165	136	161	174	636	1457		
1200	217	224	222	256	919	190	152	122	160	624	1543		
1300	277	301	234	267	1079	124	168	142	169	603	1682		
1400	265	286	264	240	1055	153	138	150	147	588	1643		
1500	280	241	214	291	1026	146	134	151	159	590	1616		
1600	219	269	249	246	983	134	135	124	108	501	1484		
1700	309	290	329	334	1262	107	158	151	127	543	1805		
1800	339	315	283	282	1219	125	140	144	134	543	1762		
1900	279	239	235	207	960	129	135	99	134	497	1457		
2000	192	228	151	189	760	99	109	90	95	393	1153		
2100	191	146	172	118	627	79	72	78	58	287	914		
2200	138	115	102	131	486	76	63	53	56	248	734		
2300	111	111	75	58	355	45	57	33	36	171	526		
24-Hour Totals:						14474						9909	24383

Peak Volume Information						
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	745	643	730	819	745	1447
P.M.	1730	1317	1315	632	1730	1860
Daily	1730	1317	730	819	1730	1860

County: 87
 Station: 0032
 Description: N MIAMI AVENUE N OF NE 34TH STREET
 Start Date: 11/02/2017
 Start Time: 0000

Time	Direction: N					Direction: S					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	31	21	13	20	85	32	21	30	21	104	189
0100	34	40	17	11	102	17	25	10	10	62	164
0200	30	27	29	15	101	5	7	12	5	29	130
0300	18	24	12	16	70	4	8	13	4	29	99
0400	12	10	18	24	64	3	6	12	15	36	100
0500	21	33	37	41	132	13	21	27	44	105	237
0600	45	38	66	77	226	77	80	118	119	394	620
0700	85	91	119	116	411	142	145	175	220	682	1093
0800	120	139	147	117	523	215	187	236	215	853	1376
0900	133	166	117	113	529	262	245	200	188	895	1424
1000	129	104	132	177	542	199	151	165	164	679	1221
1100	188	170	193	148	699	177	174	177	162	690	1389
1200	190	162	193	219	764	173	169	191	215	748	1512
1300	205	178	202	195	780	183	140	166	160	649	1429
1400	219	258	279	262	1018	156	142	154	153	605	1623
1500	306	287	293	300	1186	158	160	151	118	587	1773
1600	315	296	297	316	1224	111	164	156	140	571	1795
1700	323	329	319	312	1283	154	116	137	129	536	1819
1800	261	241	238	189	929	138	153	146	110	547	1476
1900	143	130	135	187	595	135	115	92	94	436	1031
2000	172	208	171	169	720	91	92	80	73	336	1056
2100	151	126	132	148	557	59	60	53	42	214	771
2200	188	125	102	121	536	49	35	44	46	174	710
2300	101	95	65	48	309	46	24	21	19	110	419
24-Hour Totals:	13385					10071					23456

	Peak Volume Information					
	Direction: N		Direction: S		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	830	563	830	958	830	1521
P.M.	1645	1287	1215	758	1615	1846
Daily	1645	1287	830	958	1615	1846

TMC



NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Peds & Bikes

Start Time	NW 12nd Avenue Southbound			NW 12nd Avenue Northbound			NW 40th Street Westbound			NW 40th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	0	0	0	1	1	2	2	2	4	0	0	0	6
07:15 AM	0	0	0	1	0	1	2	1	3	0	0	0	4
07:30 AM	0	0	0	1	0	1	3	2	5	0	0	0	6
*** BREAK ***													
Total	0	0	0	3	1	4	7	5	12	0	0	0	16
08:00 AM	0	0	0	0	0	0	4	0	4	0	0	0	4
08:15 AM	0	0	0	0	0	0	4	1	5	0	0	0	5
08:30 AM	0	0	0	3	0	3	6	1	7	0	0	0	10
08:45 AM	1	0	1	0	0	0	6	1	7	0	0	0	8
Total	1	0	1	3	0	3	20	3	23	0	0	0	27
*** BREAK ***													
03:00 PM	0	0	0	0	0	0	2	1	3	0	0	0	3
03:15 PM	0	0	0	0	0	0	4	1	5	0	0	0	5
03:30 PM	0	0	0	0	0	0	6	1	7	0	0	0	7
03:45 PM	1	0	1	0	0	0	7	0	7	0	0	0	8
Total	1	0	1	0	0	0	19	3	22	0	0	0	23
04:00 PM	0	1	1	2	0	2	6	1	7	0	0	0	10
04:15 PM	0	0	0	0	0	0	2	0	2	0	0	0	2
04:30 PM	0	0	0	0	0	0	6	1	7	0	0	0	7
04:45 PM	0	0	0	1	0	1	7	1	8	0	0	0	9
Total	0	1	1	3	0	3	21	3	24	0	0	0	28
05:00 PM	0	0	0	0	0	0	3	1	4	0	0	0	4
05:15 PM	1	1	2	0	0	0	11	1	12	0	0	0	14
05:30 PM	0	0	0	2	0	2	5	4	9	0	0	0	11
05:45 PM	1	0	1	2	0	2	6	3	9	0	0	0	12
Total	2	1	3	4	0	4	25	9	34	0	0	0	41
Grand Total	4	2	6	13	1	14	92	23	115	0	0	0	135
Apprch %	66.7	33.3		92.9	7.1		80	20		0	0	0	
Total %	3	1.5	4.4	9.6	0.7	10.4	68.1	17	85.2	0	0	0	

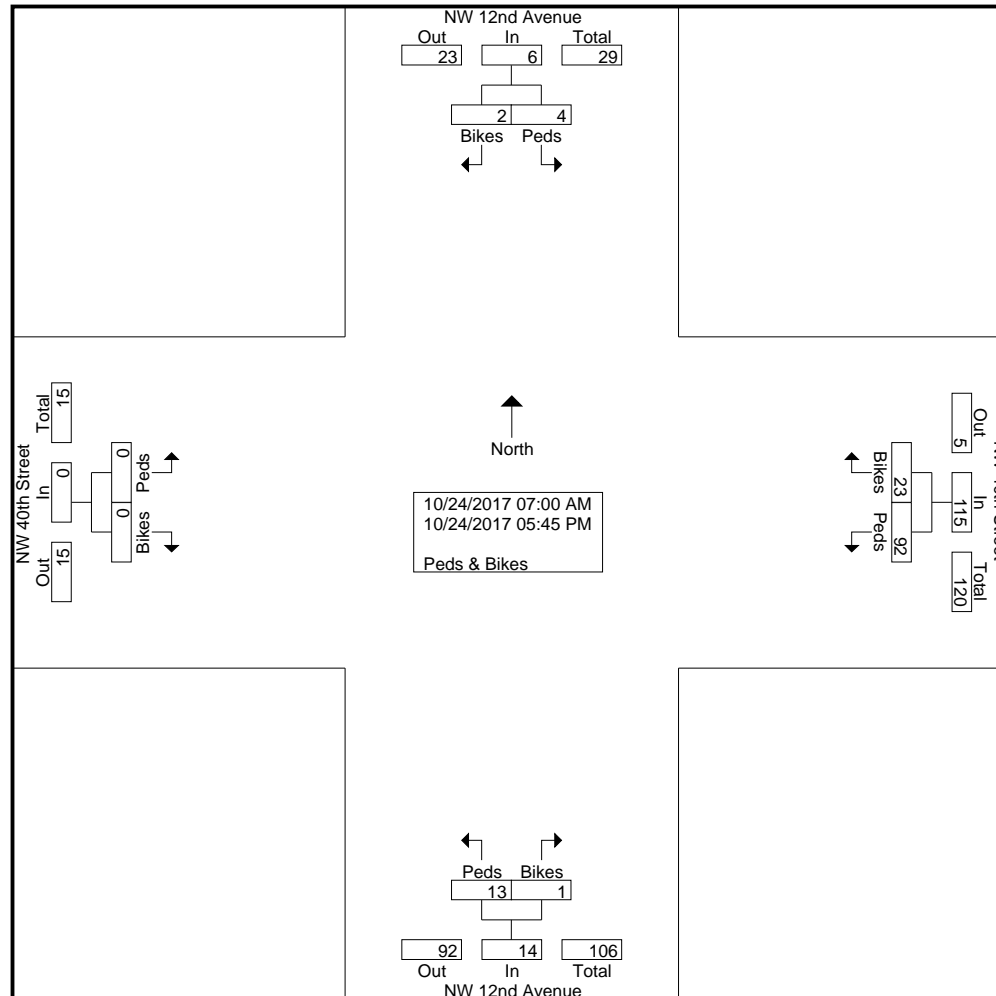
NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NW 12nd Avenue Southbound			NW 12nd Avenue Northbound			NW 40th Street Westbound			NW 40th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 08:00 AM													
08:00 AM	0	0	0	0	0	0	4	0	4	0	0	0	4
08:15 AM	0	0	0	0	0	0	4	1	5	0	0	0	5
08:30 AM	0	0	0	3	0	3	6	1	7	0	0	0	10
08:45 AM	1	0	1	0	0	0	6	1	7	0	0	0	8
Total Volume	1	0	1	3	0	3	20	3	23	0	0	0	27
% App. Total	100	0		100	0		87	13		0	0		
PHF	.250	.000	.250	.250	.000	.250	.833	.750	.821	.000	.000	.000	.675

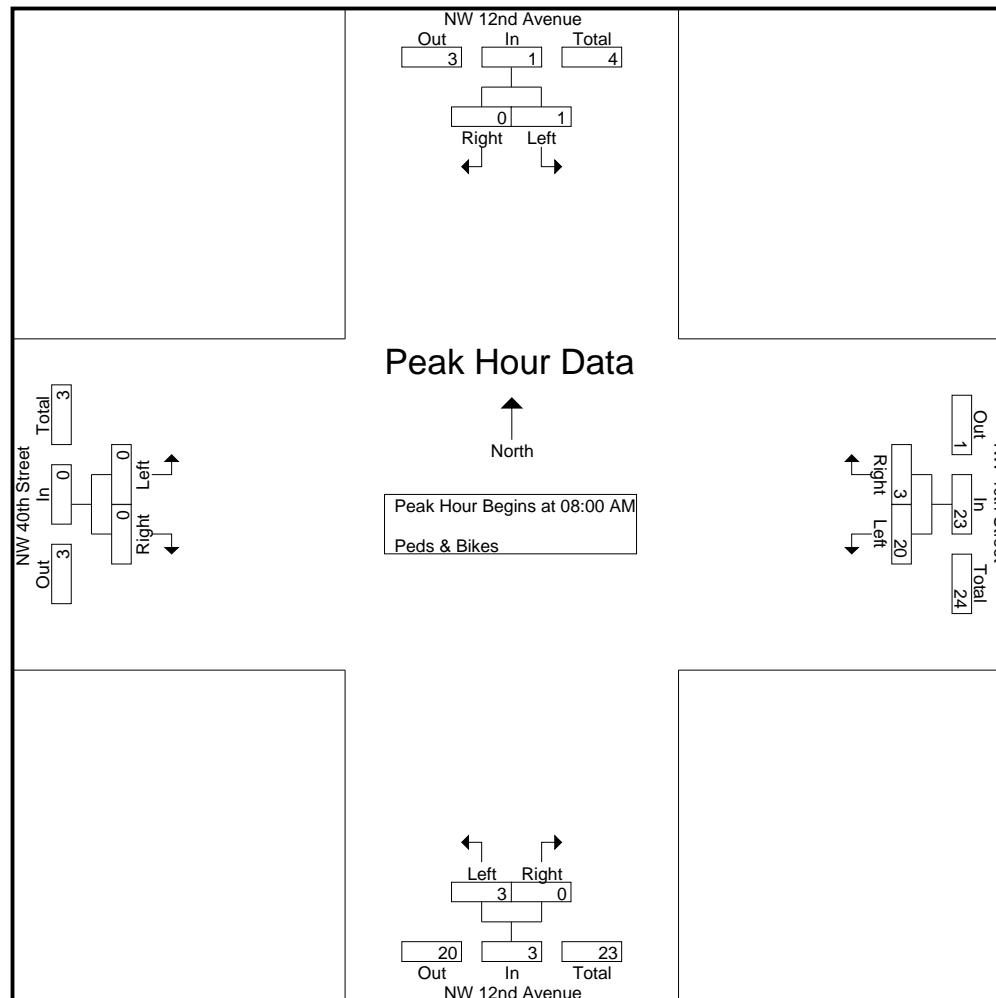
NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5

Start Time	NW 12nd Avenue Southbound			NW 12nd Avenue Northbound			NW 40th Street Westbound			NW 40th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 05:00 PM													
05:00 PM	0	0	0	0	0	0	3	1	4	0	0	0	4
05:15 PM	1	1	2	0	0	0	11	1	12	0	0	0	14
05:30 PM	0	0	0	2	0	2	5	4	9	0	0	0	11
05:45 PM	1	0	1	2	0	2	6	3	9	0	0	0	12
Total Volume	2	1	3	4	0	4	25	9	34	0	0	0	41
% App. Total	66.7	33.3		100	0		73.5	26.5		0	0		
PHF	.500	.250	.375	.500	.000	.500	.568	.563	.708	.000	.000	.000	.732

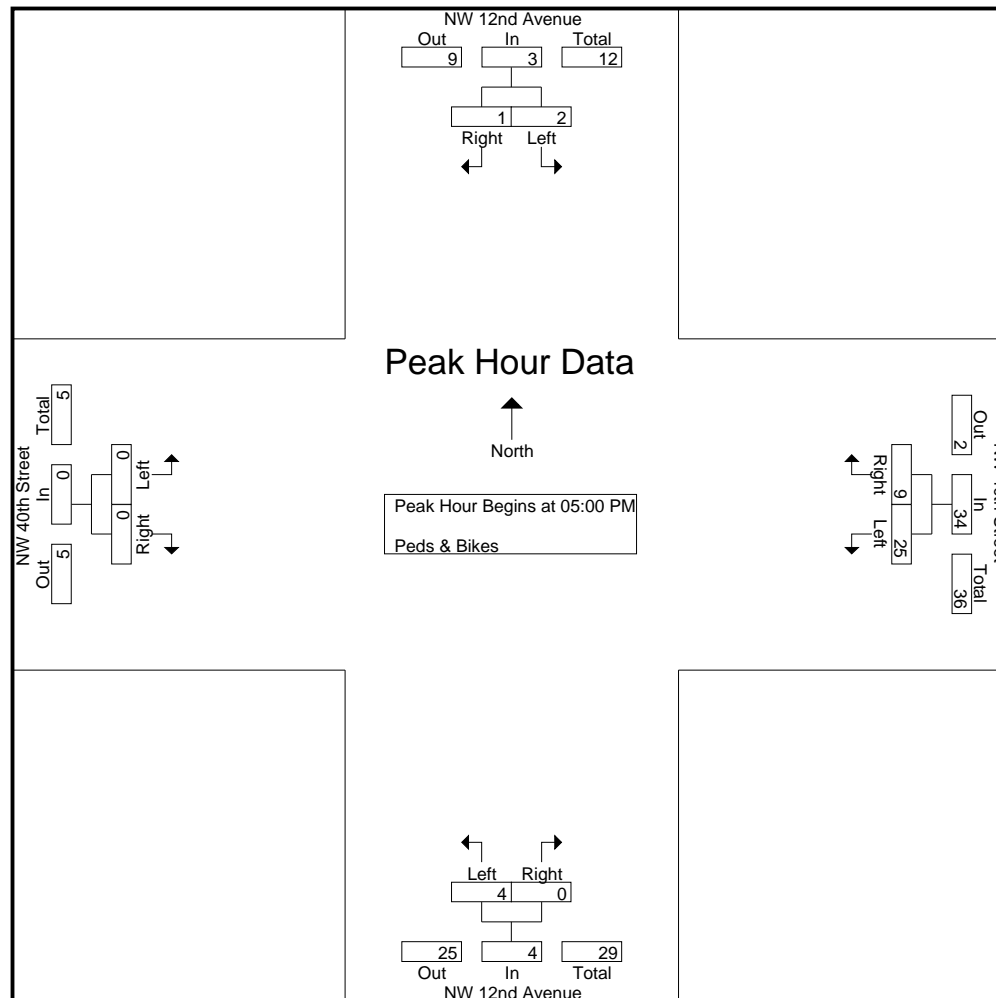
NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Trucks

Start Time	NW 12nd Avenue Southbound					NW 12nd Avenue Northbound					NW 40th Street Westbound					NW 40th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	8	0	8	0	0	13	0	13	0	8	0	0	8	0	0	0	0	0	29
07:15 AM	0	0	7	0	7	0	1	7	0	8	0	8	0	2	10	0	0	0	0	0	25
07:30 AM	0	0	11	0	11	0	1	13	0	14	0	8	0	1	9	0	0	0	0	0	34
07:45 AM	0	0	5	0	5	0	1	10	0	11	0	2	0	0	2	0	0	0	0	0	18
Total	0	0	31	0	31	0	3	43	0	46	0	26	0	3	29	0	0	0	0	0	106
08:00 AM	0	0	4	0	4	0	2	17	0	19	0	4	0	0	4	0	0	0	0	0	27
08:15 AM	0	0	3	0	3	0	1	16	0	17	0	5	0	1	6	0	0	0	0	0	26
08:30 AM	0	0	9	0	9	0	0	11	0	11	0	5	0	0	5	0	0	0	0	0	25
08:45 AM	0	0	8	0	8	0	2	9	0	11	0	5	0	0	5	0	0	0	0	0	24
Total	0	0	24	0	24	0	5	53	0	58	0	19	0	1	20	0	0	0	0	0	102
*** BREAK ***																					
03:00 PM	0	0	6	0	6	0	1	12	0	13	0	0	0	0	0	0	0	0	0	0	19
03:15 PM	0	0	9	0	9	0	1	13	0	14	0	2	0	0	2	0	0	0	0	0	25
03:30 PM	0	0	6	0	6	0	2	10	0	12	0	4	1	1	6	0	0	0	0	0	24
03:45 PM	0	0	8	0	8	0	1	7	0	8	0	0	0	0	0	0	0	0	0	0	16
Total	0	0	29	0	29	0	5	42	0	47	0	6	1	1	8	0	0	0	0	0	84
04:00 PM	0	0	3	0	3	0	2	10	0	12	0	2	1	0	3	0	0	0	0	0	18
04:15 PM	0	0	6	1	7	0	2	9	0	11	0	2	2	0	4	0	0	0	0	0	22
04:30 PM	0	0	3	0	3	0	3	5	0	8	0	3	0	0	3	0	0	0	0	0	14
04:45 PM	0	0	3	0	3	0	3	5	0	8	0	1	1	0	2	0	0	0	0	0	13
Total	0	0	15	1	16	0	10	29	0	39	0	8	4	0	12	0	0	0	0	0	67
05:00 PM	0	0	4	0	4	0	0	5	0	5	0	3	1	0	4	0	0	0	0	0	13
05:15 PM	0	0	9	0	9	0	2	4	0	6	0	5	0	1	6	0	0	0	0	0	21
05:30 PM	0	0	4	0	4	0	2	7	0	9	0	0	0	0	0	0	0	0	0	0	13
05:45 PM	0	0	6	0	6	0	0	2	0	2	0	4	0	1	5	0	0	0	0	0	13
Total	0	0	23	0	23	0	4	18	0	22	0	12	1	2	15	0	0	0	0	0	60
Grand Total	0	0	122	1	123	0	27	185	0	212	0	71	6	7	84	0	0	0	0	0	419
Apprch %	0	0	99.2	0.8		0	12.7	87.3	0		0	84.5	7.1	8.3		0	0	0	0		
Total %	0	0	29.1	0.2	29.4	0	6.4	44.2	0	50.6	0	16.9	1.4	1.7	20	0	0	0	0	0	

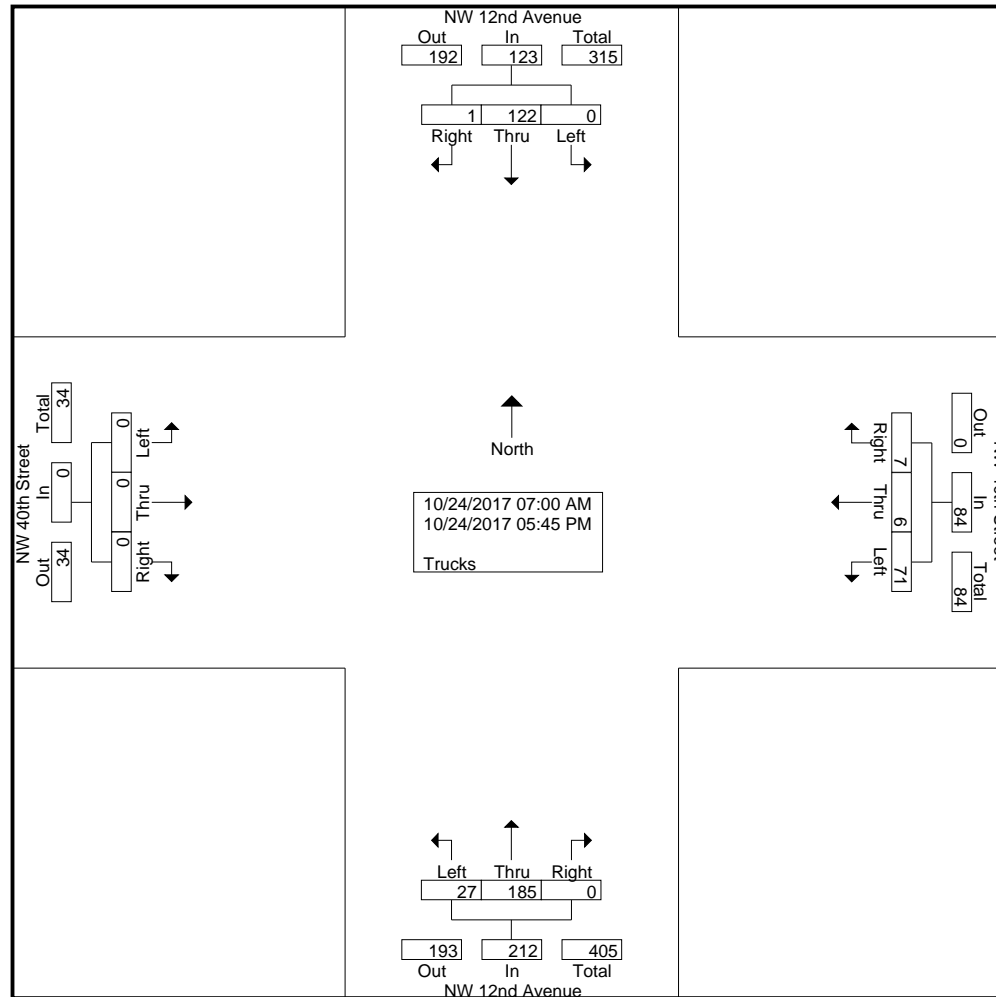
NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



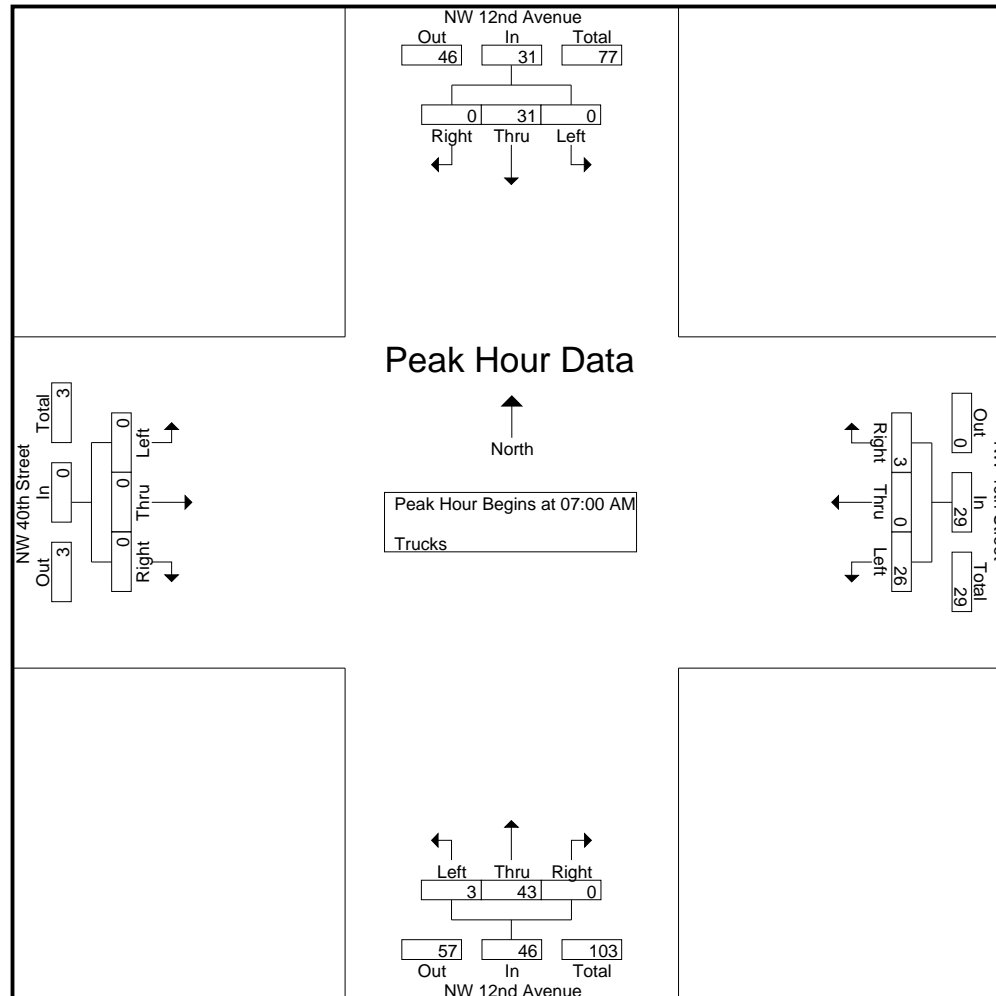
NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



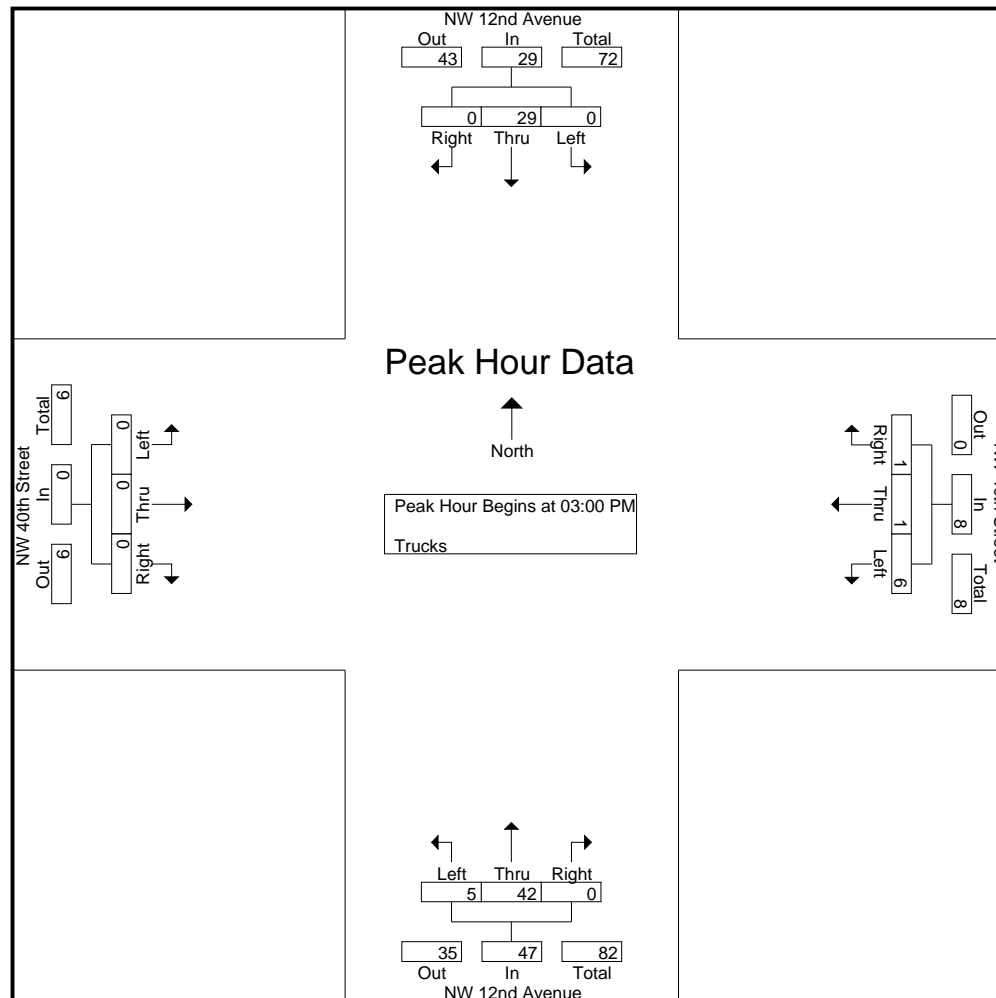
NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	NW 12nd Avenue Southbound					NW 12nd Avenue Northbound					NW 40th Street Westbound					NW 40th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	204	1	205	0	6	126	0	132	0	55	3	1	59	0	0	0	0	0	396
07:15 AM	0	0	234	2	236	0	9	139	0	148	0	40	4	8	52	0	0	0	0	0	436
07:30 AM	0	0	211	3	214	0	15	157	0	172	0	38	6	13	57	0	0	0	0	0	443
07:45 AM	0	0	194	2	196	0	13	160	0	173	0	22	4	8	34	0	0	0	0	0	403
Total	0	0	843	8	851	0	43	582	0	625	0	155	17	30	202	0	0	0	0	0	1678
08:00 AM	0	0	193	2	195	0	17	142	0	159	0	29	8	11	48	0	0	0	0	0	402
08:15 AM	0	0	174	4	178	0	11	172	0	183	0	35	1	9	45	0	0	0	0	0	406
08:30 AM	0	0	176	1	177	0	14	131	0	145	0	39	3	11	53	0	0	0	0	0	375
08:45 AM	0	0	183	0	183	0	7	148	0	155	0	35	0	10	45	0	0	0	0	0	383
Total	0	0	726	7	733	0	49	593	0	642	0	138	12	41	191	0	0	0	0	0	1566
*** BREAK ***																					
03:00 PM	0	0	122	0	122	1	18	235	0	254	0	22	4	6	32	0	0	0	0	0	408
03:15 PM	0	0	131	2	133	0	21	293	0	314	0	14	3	6	23	0	0	0	0	0	470
03:30 PM	0	0	113	4	117	0	18	289	0	307	0	18	12	9	39	0	0	0	0	0	463
03:45 PM	0	0	131	6	137	0	20	305	0	325	0	18	7	10	35	0	0	0	0	0	497
Total	0	0	497	12	509	1	77	1122	0	1200	0	72	26	31	129	0	0	0	0	0	1838
04:00 PM	0	0	135	2	137	0	24	302	0	326	0	31	11	9	51	0	0	0	0	0	514
04:15 PM	0	0	143	5	148	0	23	327	0	350	0	19	10	12	41	0	0	0	0	0	539
04:30 PM	0	0	138	5	143	0	25	331	0	356	0	26	5	11	42	0	0	0	0	0	541
04:45 PM	0	0	142	4	146	0	18	381	0	399	0	28	10	25	63	0	0	0	0	0	608
Total	0	0	558	16	574	0	90	1341	0	1431	0	104	36	57	197	0	0	0	0	0	2202
05:00 PM	0	0	121	5	126	0	16	337	0	353	0	27	5	17	49	0	0	0	0	0	528
05:15 PM	0	0	131	5	136	0	17	396	0	413	0	28	8	12	48	0	0	0	0	0	597
05:30 PM	0	0	141	6	147	0	19	350	0	369	0	30	5	9	44	0	0	0	0	0	560
05:45 PM	0	0	146	7	153	0	13	376	0	389	0	43	9	22	74	0	0	0	0	0	616
Total	0	0	539	23	562	0	65	1459	0	1524	0	128	27	60	215	0	0	0	0	0	2301
Grand Total	0	0	3163	66	3229	1	324	5097	0	5422	0	597	118	219	934	0	0	0	0	0	9585
Apprch %	0	0	98	2		0	6	94	0		0	63.9	12.6	23.4		0	0	0	0		
Total %	0	0	33	0.7	33.7	0	3.4	53.2	0	56.6	0	6.2	1.2	2.3	9.7	0	0	0	0	0	
Vehicle	0	0	3041	65	3106	1	297	4912	0	5210	0	526	112	212	850	0	0	0	0	0	9166
% Vehicle	0	0	96.1	98.5	96.2	100	91.7	96.4	0	96.1	0	88.1	94.9	96.8	91	0	0	0	0	0	95.6

NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	NW 12nd Avenue Southbound					NW 12nd Avenue Northbound					NW 40th Street Westbound					NW 40th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	0	122	1	123	0	27	185	0	212	0	71	6	7	84	0	0	0	0	0	419
% Trucks	0	0	3.9	1.5	3.8	0	8.3	3.6	0	3.9	0	11.9	5.1	3.2	9	0	0	0	0	0	4.4

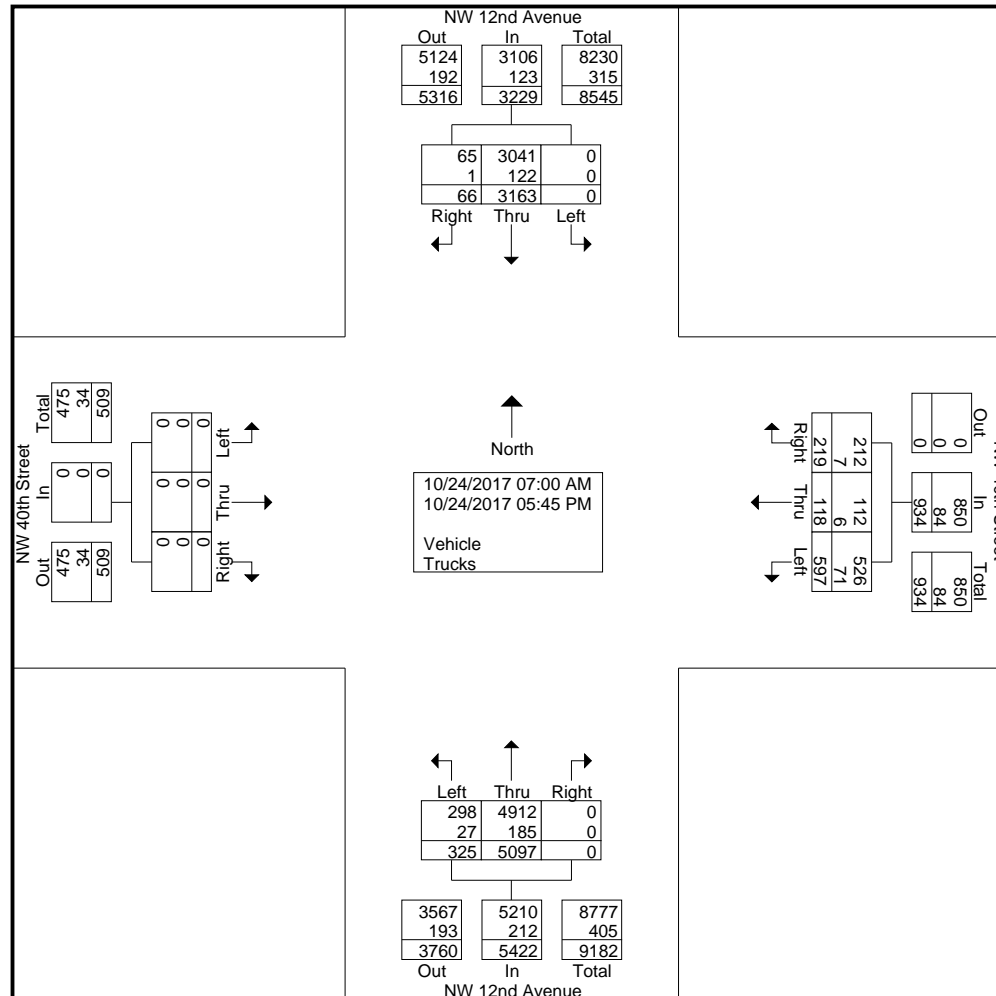
NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



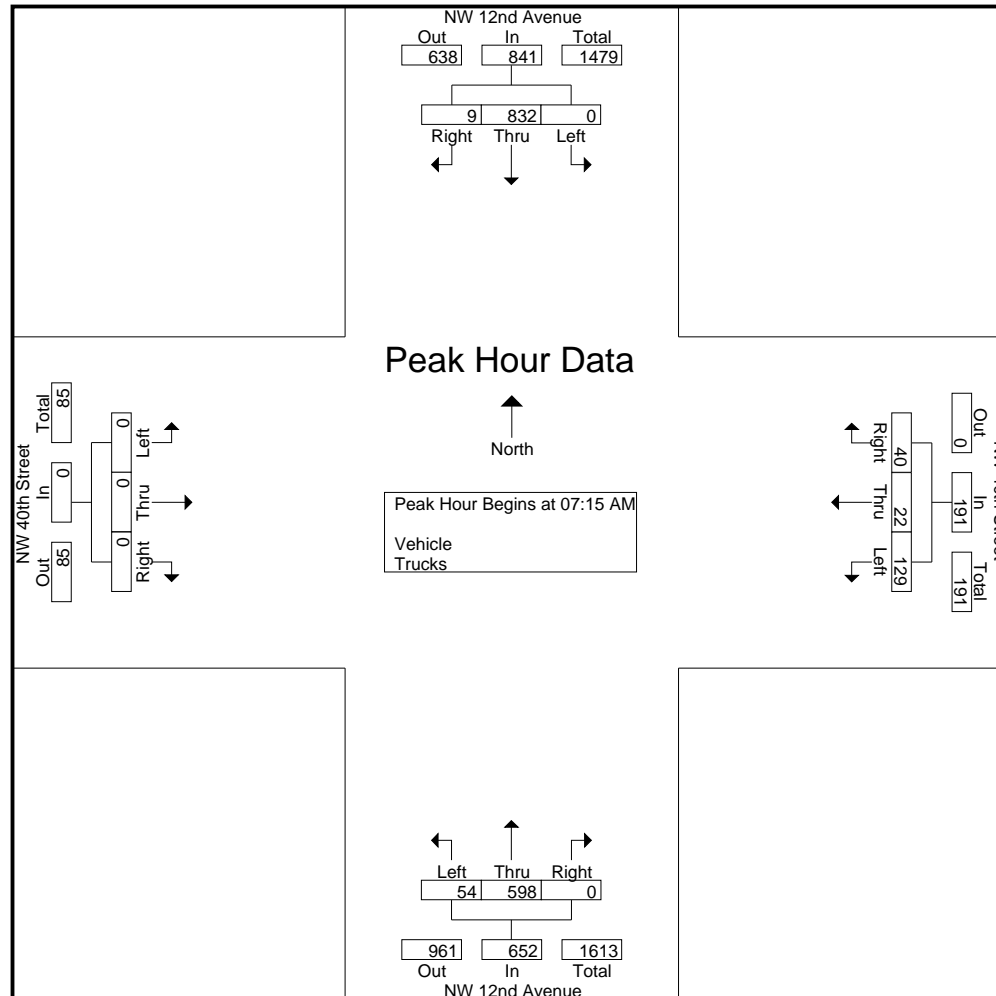
NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



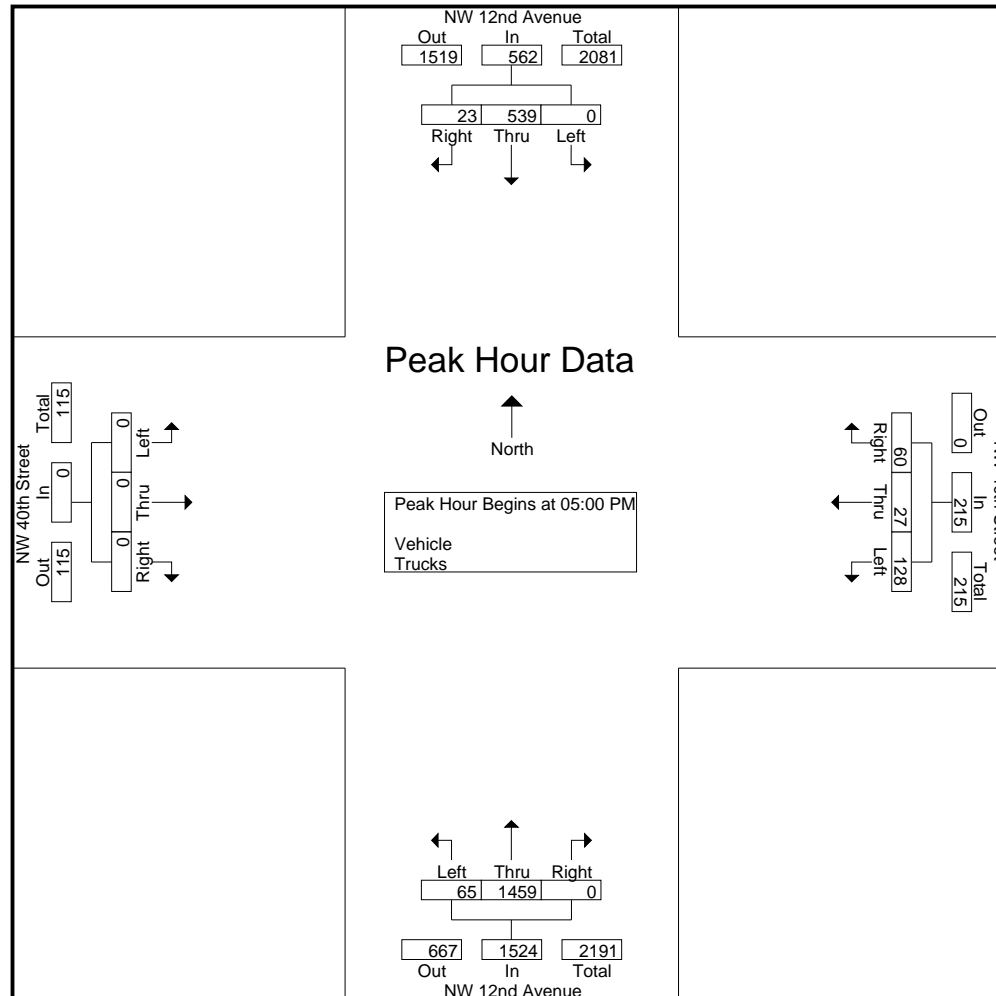
NW 12nd Avenue & NW 40th Street

File Name : TMC-1 NW 12nd Avenue & NW 40th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	NW 12nd Avenue Southbound			NW 12nd Avenue Northbound			Westbound			NW 39th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	0	0	0	0	0	0	7	3	10	3	2	5	15
07:15 AM	0	0	0	0	0	0	6	1	7	3	0	3	10
07:30 AM	0	0	0	0	0	0	4	1	5	5	2	7	12
07:45 AM	1	0	1	0	0	0	0	0	0	1	2	3	4
Total	1	0	1	0	0	0	17	5	22	12	6	18	41
08:00 AM	0	0	0	0	0	0	4	0	4	1	0	1	5
08:15 AM	0	0	0	0	0	0	7	1	8	11	1	12	20
08:30 AM	0	0	0	0	1	1	7	1	8	3	0	3	12
08:45 AM	0	0	0	0	0	0	9	2	11	1	0	1	12
Total	0	0	0	0	1	1	27	4	31	16	1	17	49
*** BREAK ***													
03:00 PM	0	0	0	0	0	0	4	0	4	2	1	3	7
03:15 PM	0	0	0	0	0	0	5	0	5	5	3	8	13
03:30 PM	1	0	1	0	0	0	14	0	14	1	1	2	17
03:45 PM	0	1	1	0	0	0	9	1	10	9	3	12	23
Total	1	1	2	0	0	0	32	1	33	17	8	25	60
04:00 PM	5	1	6	0	0	0	14	3	17	7	0	7	30
04:15 PM	2	0	2	0	0	0	3	1	4	1	0	1	7
04:30 PM	1	0	1	1	0	1	8	0	8	3	0	3	13
04:45 PM	1	0	1	0	0	0	12	1	13	1	0	1	15
Total	9	1	10	1	0	1	37	5	42	12	0	12	65
05:00 PM	0	0	0	0	0	0	9	1	10	4	4	8	18
05:15 PM	0	0	0	0	0	0	8	4	12	4	2	6	18
05:30 PM	0	0	0	0	0	0	8	6	14	2	0	2	16
05:45 PM	4	0	4	0	1	1	14	5	19	3	1	4	28
Total	4	0	4	0	1	1	39	16	55	13	7	20	80
Grand Total	15	2	17	1	2	3	152	31	183	70	22	92	295
Apprch %	88.2	11.8		33.3	66.7		83.1	16.9		76.1	23.9		
Total %	5.1	0.7	5.8	0.3	0.7	1	51.5	10.5	62	23.7	7.5	31.2	

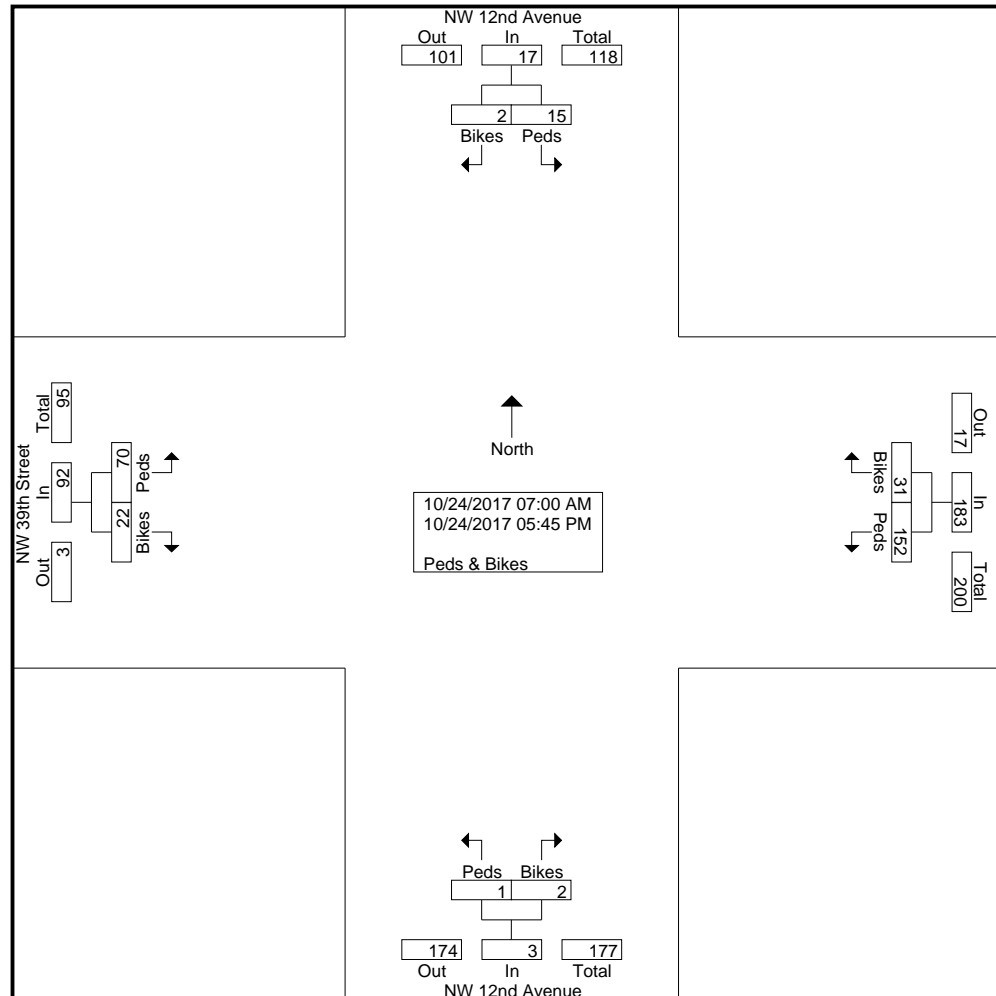
NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NW 12nd Avenue Southbound			NW 12nd Avenue Northbound			Westbound			NW 39th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 08:00 AM													
08:00 AM	0	0	0	0	0	0	4	0	4	1	0	1	5
08:15 AM	0	0	0	0	0	0	7	1	8	11	1	12	20
08:30 AM	0	0	0	0	1	1	7	1	8	3	0	3	12
08:45 AM	0	0	0	0	0	0	9	2	11	1	0	1	12
Total Volume	0	0	0	0	1	1	27	4	31	16	1	17	49
% App. Total	0	0		0	100		87.1	12.9		94.1	5.9		
PHF	.000	.000	.000	.000	.250	.250	.750	.500	.705	.364	.250	.354	.613

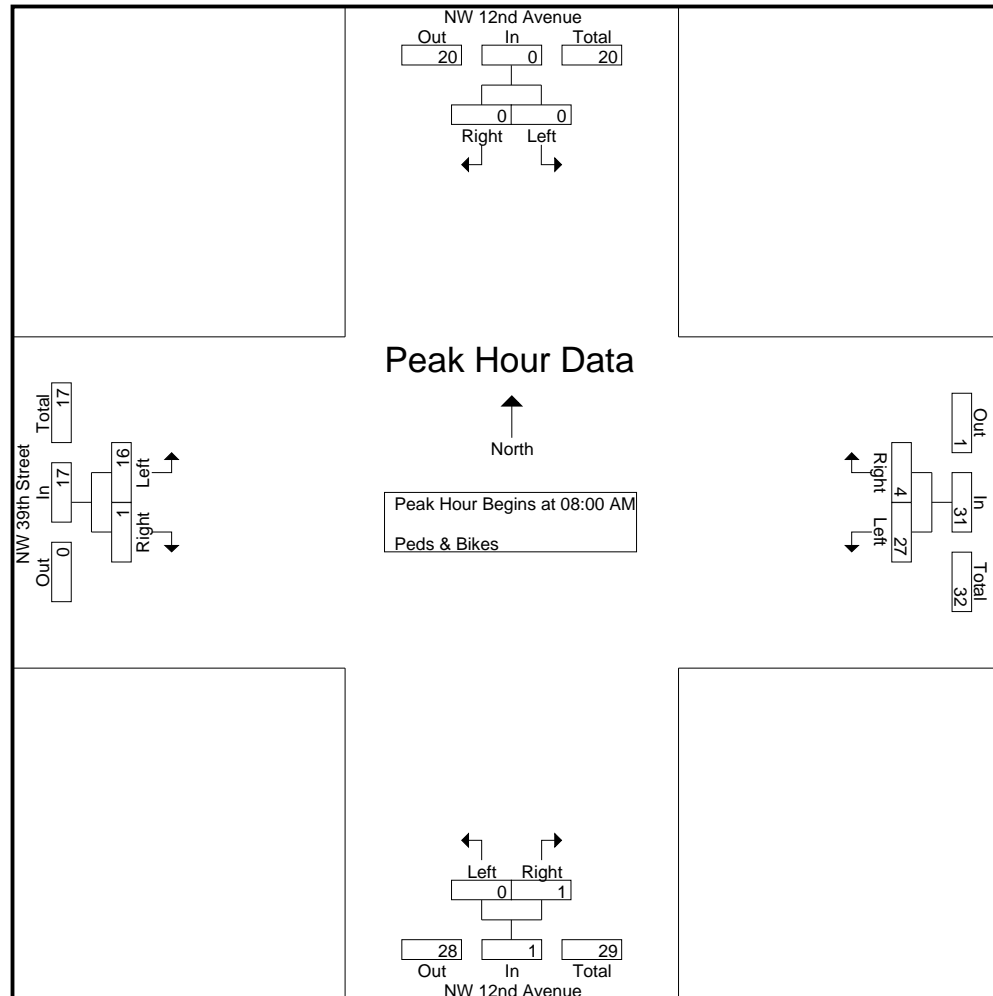
NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	NW 12nd Avenue Southbound			NW 12nd Avenue Northbound			Westbound			NW 39th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 03:15 PM													
03:15 PM	0	0	0	0	0	0	5	0	5	5	3	8	13
03:30 PM	1	0	1	0	0	0	14	0	14	1	1	2	17
03:45 PM	0	1	1	0	0	0	9	1	10	9	3	12	23
04:00 PM	5	1	6	0	0	0	14	3	17	7	0	7	30
Total Volume	6	2	8	0	0	0	42	4	46	22	7	29	83
% App. Total	75	25		0	0		91.3	8.7		75.9	24.1		
PHF	.300	.500	.333	.000	.000	.000	.750	.333	.676	.611	.583	.604	.692

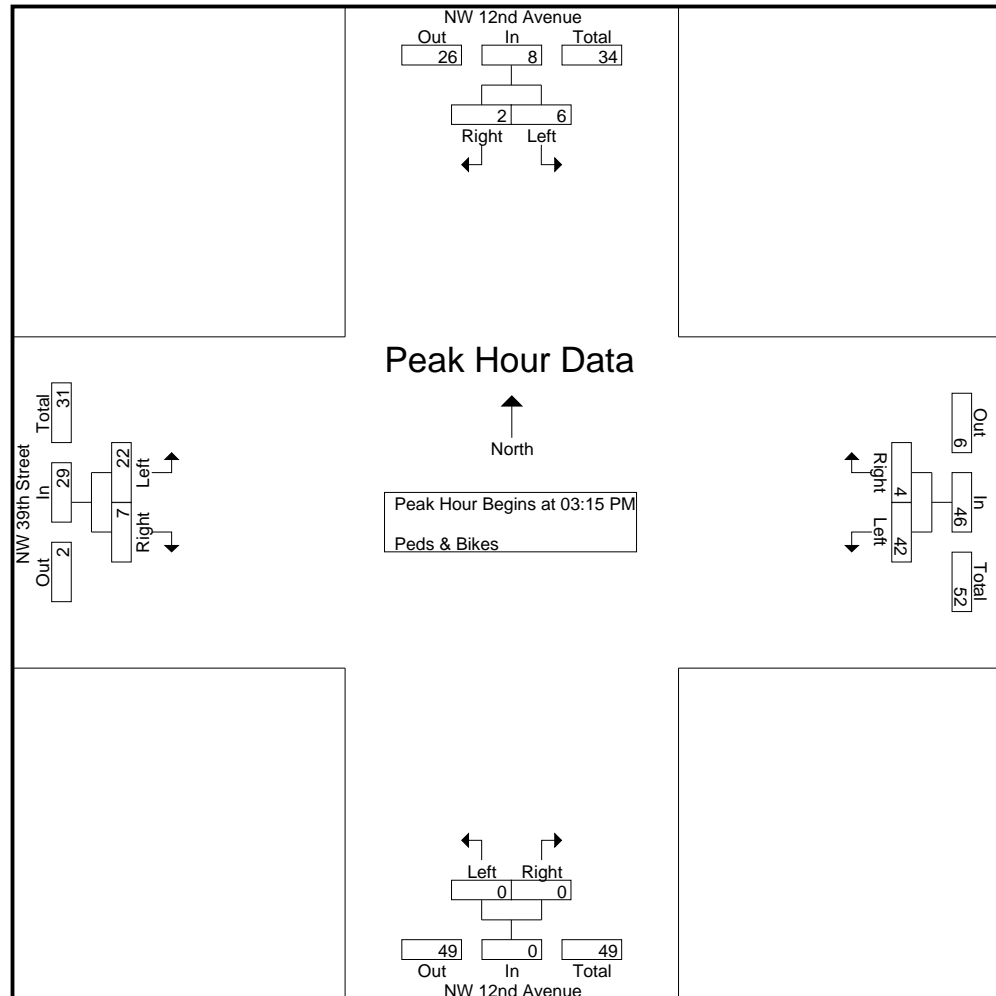
NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Trucks

Start Time	NW 12nd Avenue Southbound					NW 12nd Avenue Northbound					Westbound					NW 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	3	11	0	14	0	0	9	0	9	0	0	0	0	0	0	3	1	5	9	32
07:15 AM	0	1	11	0	12	0	0	7	0	7	0	0	0	0	0	0	2	2	3	7	26
07:30 AM	0	0	19	0	19	0	0	12	0	12	0	0	0	0	0	0	1	0	3	4	35
07:45 AM	0	0	7	0	7	0	0	5	0	5	0	0	0	0	0	0	3	1	2	6	18
Total	0	4	48	0	52	0	0	33	0	33	0	0	0	0	0	0	9	4	13	26	111
08:00 AM	0	0	8	0	8	0	0	13	0	13	0	0	0	0	0	0	5	1	7	13	34
08:15 AM	0	1	7	0	8	0	0	12	0	12	0	0	0	0	0	0	3	0	8	11	31
08:30 AM	0	1	13	0	14	0	0	10	0	10	0	0	0	0	0	0	1	0	8	9	33
08:45 AM	0	0	13	0	13	0	0	7	0	7	0	0	0	0	0	0	2	0	4	6	26
Total	0	2	41	0	43	0	0	42	0	42	0	0	0	0	0	0	11	1	27	39	124
*** BREAK ***																					
03:00 PM	0	0	4	0	4	0	0	10	0	10	0	0	0	0	0	0	3	1	6	10	24
03:15 PM	0	0	11	0	11	0	0	14	0	14	0	0	0	0	0	0	2	1	4	7	32
03:30 PM	0	0	9	0	9	0	0	9	0	9	0	0	0	0	0	0	3	0	5	8	26
03:45 PM	0	0	8	0	8	0	0	1	0	1	0	0	0	0	0	0	4	1	1	6	15
Total	0	0	32	0	32	0	0	34	0	34	0	0	0	0	0	0	12	3	16	31	97
04:00 PM	0	0	5	0	5	0	0	7	0	7	0	0	0	0	0	0	1	0	3	4	16
04:15 PM	0	0	9	0	9	0	0	8	2	10	0	0	0	0	0	0	2	1	5	8	27
04:30 PM	0	1	6	0	7	0	0	4	0	4	0	0	0	0	0	0	2	0	6	8	19
04:45 PM	0	0	4	0	4	0	0	5	0	5	0	0	0	0	0	0	2	0	1	3	12
Total	0	1	24	0	25	0	0	24	2	26	0	0	0	0	0	0	7	1	15	23	74
05:00 PM	0	0	5	0	5	0	0	3	1	4	0	0	0	0	0	0	0	0	5	5	14
05:15 PM	0	0	14	0	14	0	0	5	0	5	0	0	0	0	0	0	1	0	1	2	21
05:30 PM	0	1	6	0	7	0	0	11	0	11	0	0	0	0	0	0	0	2	2	4	22
05:45 PM	0	0	9	0	9	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	11
Total	0	1	34	0	35	0	0	21	1	22	0	0	0	0	0	0	1	2	8	11	68
Grand Total	0	8	179	0	187	0	0	154	3	157	0	0	0	0	0	0	40	11	79	130	474
Apprch %	0	4.3	95.7	0		0	0	98.1	1.9		0	0	0	0		0	30.8	8.5	60.8		
Total %	0	1.7	37.8	0	39.5	0	0	32.5	0.6	33.1	0	0	0	0	0	0	8.4	2.3	16.7	27.4	

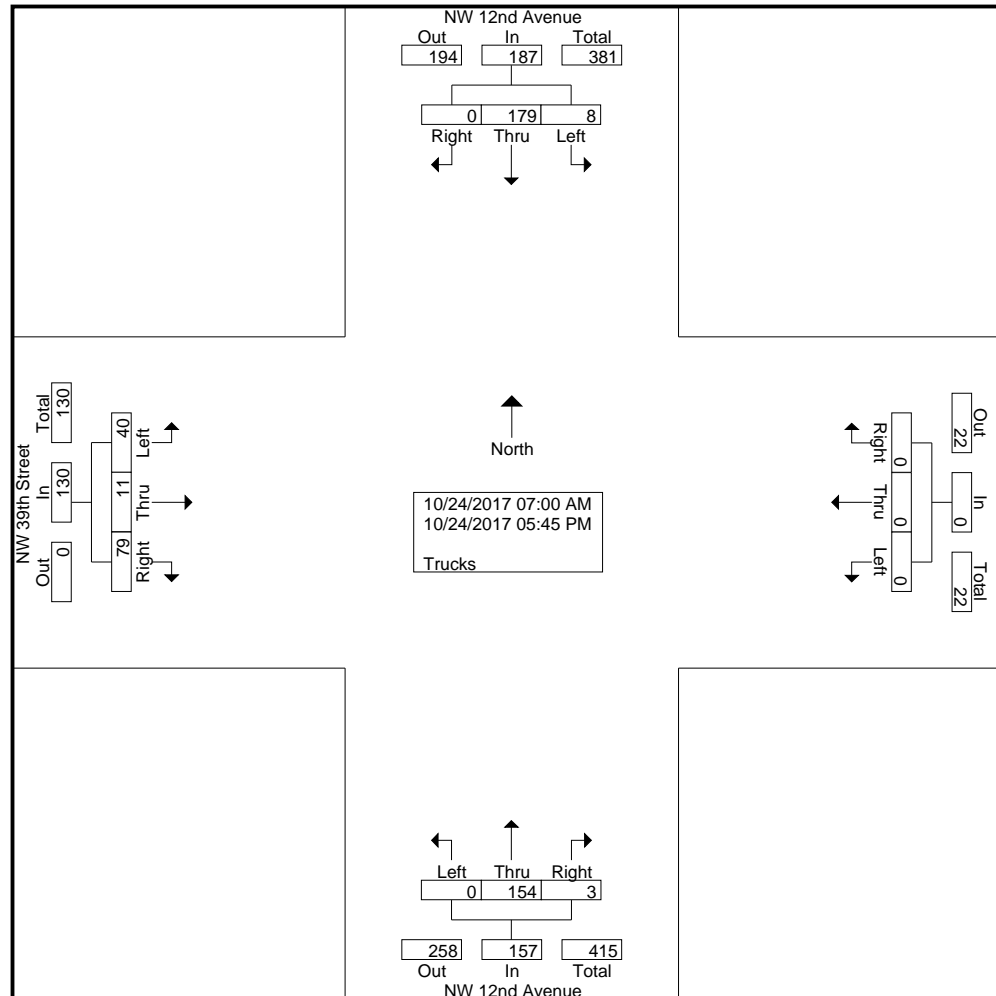
NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NW 12nd Avenue Southbound					NW 12nd Avenue Northbound					Westbound					NW 39th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	0	8	0	8	0	0	13	0	13	0	0	0	0	0	0	5	1	7	13	34	
08:15 AM	0	1	7	0	8	0	0	12	0	12	0	0	0	0	0	0	3	0	8	11	31	
08:30 AM	0	1	13	0	14	0	0	10	0	10	0	0	0	0	0	0	1	0	8	9	33	
08:45 AM	0	0	13	0	13	0	0	7	0	7	0	0	0	0	0	0	2	0	4	6	26	
Total Volume	0	2	41	0	43	0	0	42	0	42	0	0	0	0	0	0	11	1	27	39	124	
% App. Total	0	4.7	95.3	0		0	0	100	0		0	0	0	0		0	28.2	2.6	69.2			
PHF	.000	.500	.788	.000	.768	.000	.000	.808	.000	.808	.000	.000	.000	.000	.000	.000	.550	.250	.844	.750	.912	

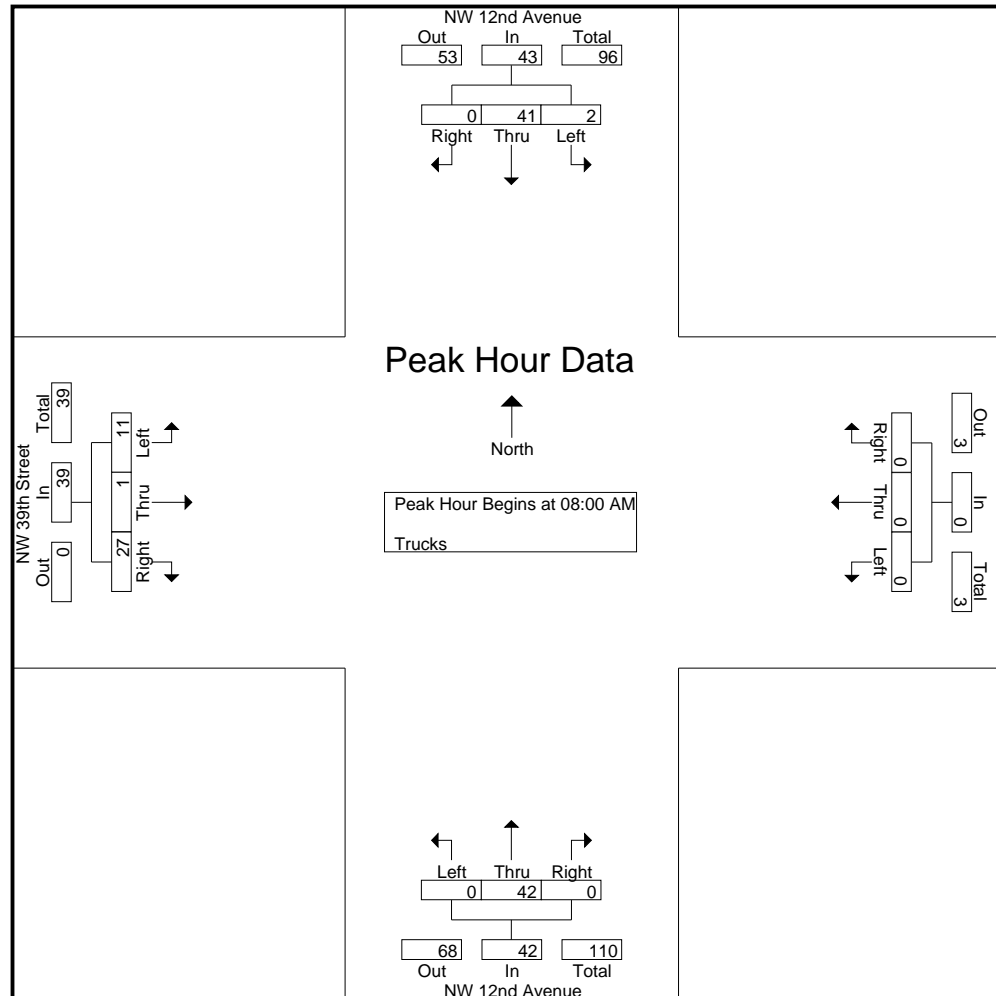
NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	NW 12nd Avenue Southbound					NW 12nd Avenue Northbound					Westbound					NW 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:00 PM																					
03:00 PM	0	0	4	0	4	0	0	10	0	10	0	0	0	0	0	0	3	1	6	10	24
03:15 PM	0	0	11	0	11	0	0	14	0	14	0	0	0	0	0	0	2	1	4	7	32
03:30 PM	0	0	9	0	9	0	0	9	0	9	0	0	0	0	0	0	3	0	5	8	26
03:45 PM	0	0	8	0	8	0	0	1	0	1	0	0	0	0	0	0	4	1	1	6	15
Total Volume	0	0	32	0	32	0	0	34	0	34	0	0	0	0	0	0	12	3	16	31	97
% App. Total	0	0	100	0		0	0	100	0		0	0	0	0		0	38.7	9.7	51.6		
PHF	.000	.000	.727	.000	.727	.000	.000	.607	.000	.607	.000	.000	.000	.000	.000	.000	.750	.750	.667	.775	.758

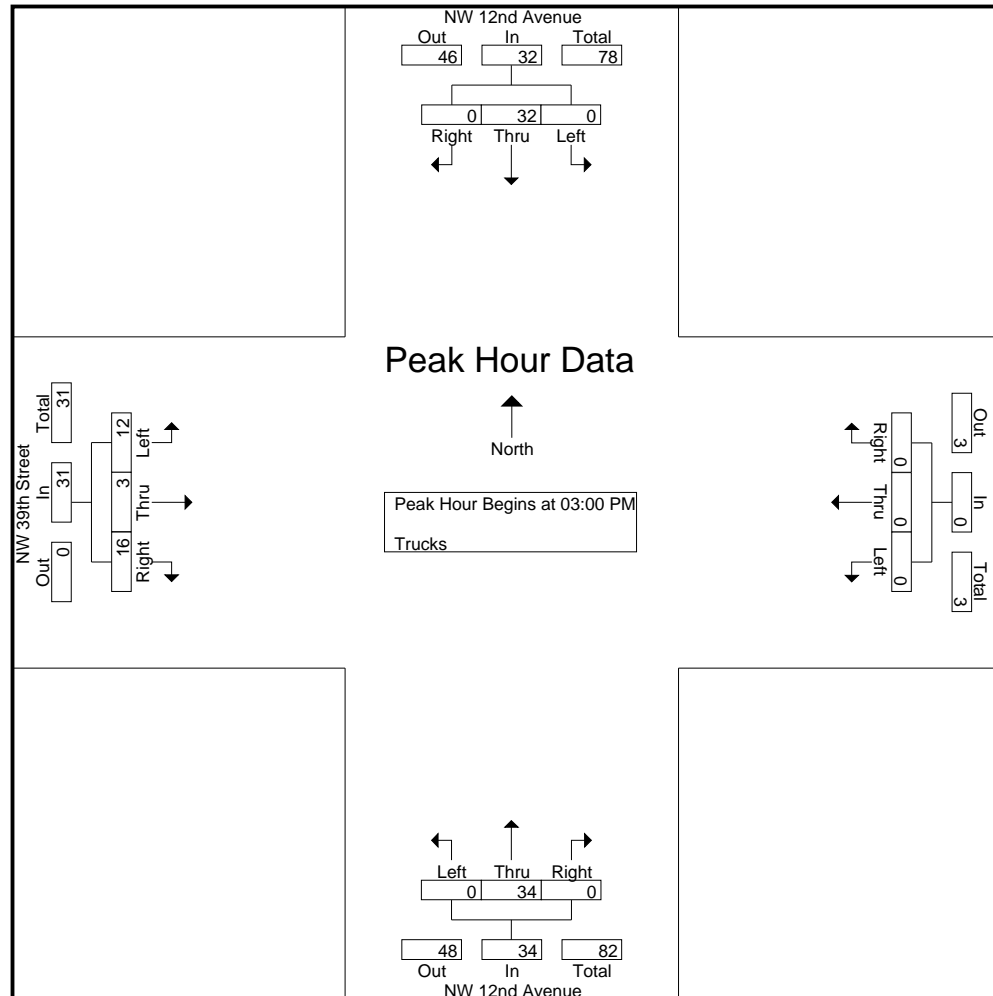
NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	NW 12nd Avenue Southbound					NW 12nd Avenue Northbound					Westbound					NW 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	14	250	0	264	0	0	115	1	116	0	0	0	0	0	0	19	27	90	136	516
07:15 AM	0	12	268	0	280	0	0	126	3	129	0	0	0	0	0	0	23	26	108	157	566
07:30 AM	0	9	240	0	249	0	0	155	2	157	0	0	0	0	0	0	12	23	93	128	534
07:45 AM	0	9	212	0	221	0	0	145	6	151	0	0	0	0	0	0	17	36	110	163	535
Total	0	44	970	0	1014	0	0	541	12	553	0	0	0	0	0	0	71	112	401	584	2151
08:00 AM	0	11	215	0	226	0	0	127	8	135	0	0	0	0	0	0	22	25	101	148	509
08:15 AM	0	9	200	0	209	0	0	145	10	155	0	0	0	0	0	0	34	24	98	156	520
08:30 AM	0	7	209	0	216	0	0	125	12	137	0	0	0	0	0	0	23	30	102	155	508
08:45 AM	0	7	207	0	214	0	0	131	7	138	0	0	0	0	0	0	24	39	121	184	536
Total	0	34	831	0	865	0	0	528	37	565	0	0	0	0	0	0	103	118	422	643	2073
*** BREAK ***																					
03:00 PM	0	12	137	0	149	0	0	233	16	249	0	0	0	0	0	0	21	15	45	81	479
03:15 PM	0	8	130	0	138	0	0	282	18	300	0	0	0	0	0	0	35	11	48	94	532
03:30 PM	0	14	114	0	128	0	0	275	12	287	0	0	0	0	0	0	40	21	57	118	533
03:45 PM	0	10	132	0	142	0	0	276	11	287	0	0	0	0	0	0	51	17	40	108	537
Total	0	44	513	0	557	0	0	1066	57	1123	0	0	0	0	0	0	147	64	190	401	2081
04:00 PM	0	10	156	0	166	0	0	282	17	299	0	0	0	0	0	0	43	19	45	107	572
04:15 PM	0	13	150	0	163	0	0	313	12	325	0	0	0	0	0	0	41	21	59	121	609
04:30 PM	0	9	151	0	160	0	0	278	6	284	0	0	0	0	0	0	75	21	34	130	574
04:45 PM	0	6	161	0	167	0	0	325	7	332	0	0	0	0	0	0	71	13	51	135	634
Total	0	38	618	0	656	0	0	1198	42	1240	0	0	0	0	0	0	230	74	189	493	2389
05:00 PM	0	4	140	0	144	0	0	299	8	307	0	0	0	0	0	0	61	15	48	124	575
05:15 PM	0	5	157	0	162	0	0	326	7	333	0	0	0	0	0	0	81	23	40	144	639
05:30 PM	0	16	157	0	173	0	0	302	11	313	0	0	0	0	0	0	68	26	50	144	630
05:45 PM	0	16	173	0	189	0	0	314	19	333	0	0	0	0	0	0	72	27	44	143	665
Total	0	41	627	0	668	0	0	1241	45	1286	0	0	0	0	0	0	282	91	182	555	2509
Grand Total	0	201	3559	0	3760	0	0	4574	193	4767	0	0	0	0	0	0	833	459	1384	2676	11203
Apprch %	0	5.3	94.7	0		0	0	96	4		0	0	0	0		0	31.1	17.2	51.7		
Total %	0	1.8	31.8	0	33.6	0	0	40.8	1.7	42.6	0	0	0	0	0	0	7.4	4.1	12.4	23.9	
Vehicle	0	193	3380	0	3573	0	0	4420	190	4610	0	0	0	0	0	0	793	448	1305	2546	10729
% Vehicle	0	96	95	0	95	0	0	96.6	98.4	96.7	0	0	0	0	0	0	95.2	97.6	94.3	95.1	95.8

NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	NW 12nd Avenue Southbound					NW 12nd Avenue Northbound					Westbound					NW 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	8	179	0	187	0	0	154	3	157	0	0	0	0	0	0	40	11	79	130	474
% Trucks	0	4	5	0	5	0	0	3.4	1.6	3.3	0	0	0	0	0	0	4.8	2.4	5.7	4.9	4.2

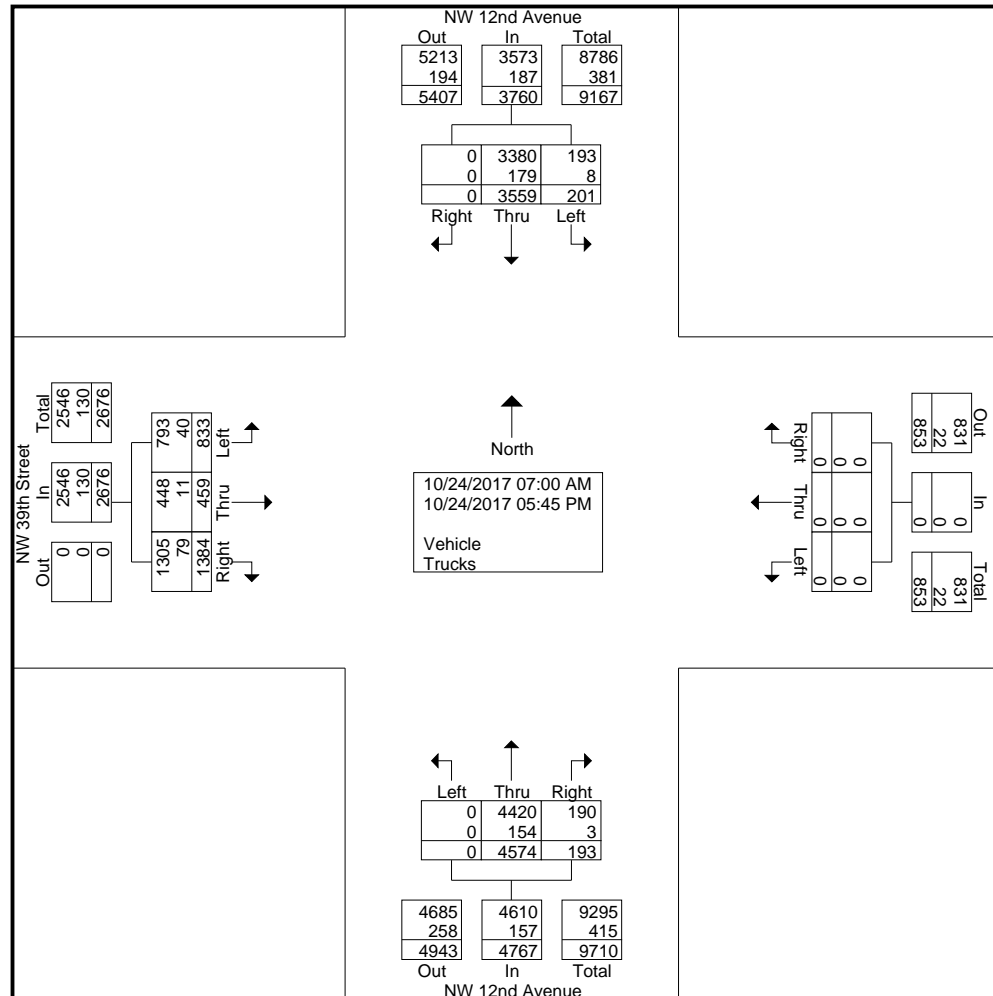
NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4

Start Time	NW 12nd Avenue Southbound					NW 12nd Avenue Northbound					Westbound					NW 39th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 07:00 AM																						
07:00 AM	0	14	250	0	264	0	0	115	1	116	0	0	0	0	0	0	19	27	90	136	516	
07:15 AM	0	12	268	0	280	0	0	126	3	129	0	0	0	0	0	0	23	26	108	157	566	
07:30 AM	0	9	240	0	249	0	0	155	2	157	0	0	0	0	0	0	12	23	93	128	534	
07:45 AM	0	9	212	0	221	0	0	145	6	151	0	0	0	0	0	0	17	36	110	163	535	
Total Volume	0	44	970	0	1014	0	0	541	12	553	0	0	0	0	0	0	71	112	401	584	2151	
% App. Total	0	4.3	95.7	0		0	0	97.8	2.2		0	0	0	0	0	0	12.2	19.2	68.7			
PHF	.000	.786	.905	.000	.905	.000	.000	.873	.500	.881	.000	.000	.000	.000	.000	.000	.772	.778	.911	.896	.950	

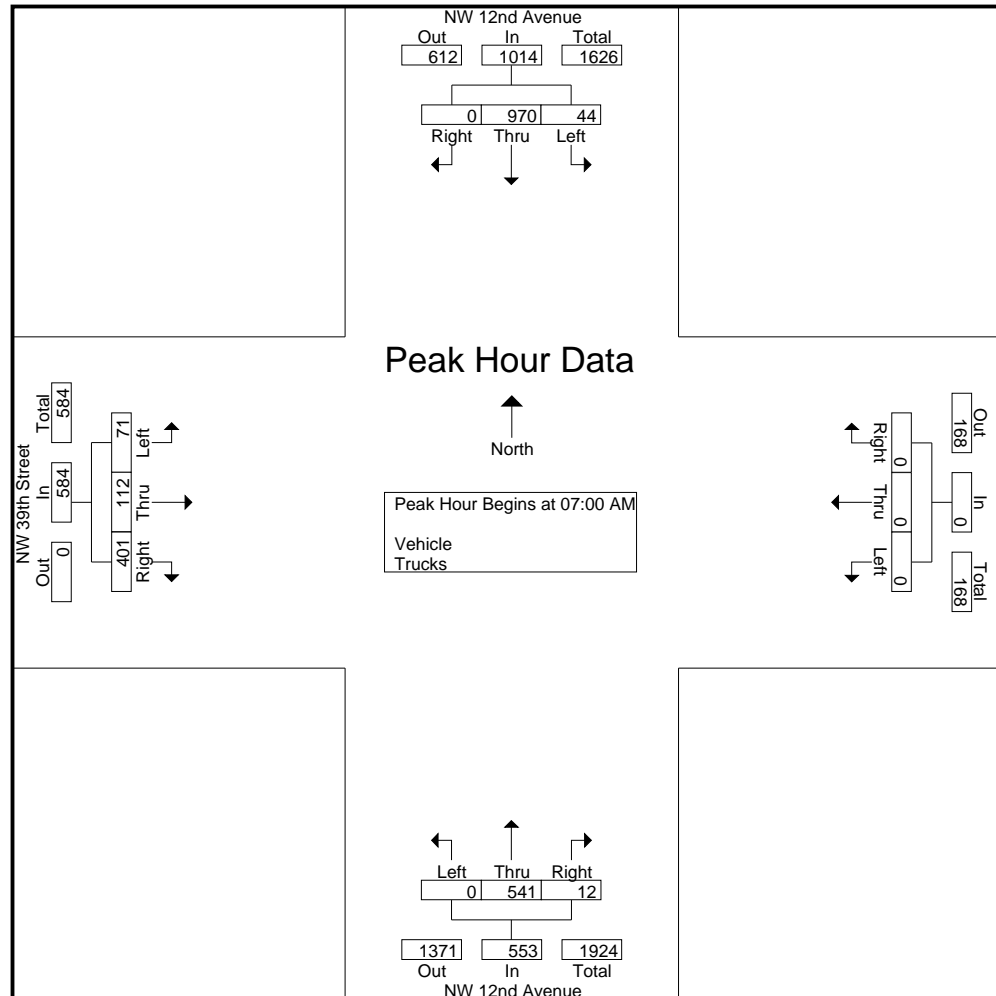
NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 6

Start Time	NW 12nd Avenue Southbound					NW 12nd Avenue Northbound					Westbound					NW 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 05:00 PM																					
05:00 PM	0	4	140	0	144	0	0	299	8	307	0	0	0	0	0	0	61	15	48	124	575
05:15 PM	0	5	157	0	162	0	0	326	7	333	0	0	0	0	0	0	81	23	40	144	639
05:30 PM	0	16	157	0	173	0	0	302	11	313	0	0	0	0	0	0	68	26	50	144	630
05:45 PM	0	16	173	0	189	0	0	314	19	333	0	0	0	0	0	0	72	27	44	143	665
Total Volume	0	41	627	0	668	0	0	1241	45	1286	0	0	0	0	0	0	282	91	182	555	2509
% App. Total	0	6.1	93.9	0		0	0	96.5	3.5		0	0	0	0	0	0	50.8	16.4	32.8		
PHF	.000	.641	.906	.000	.884	.000	.000	.952	.592	.965	.000	.000	.000	.000	.000	.000	.870	.843	.910	.964	.943

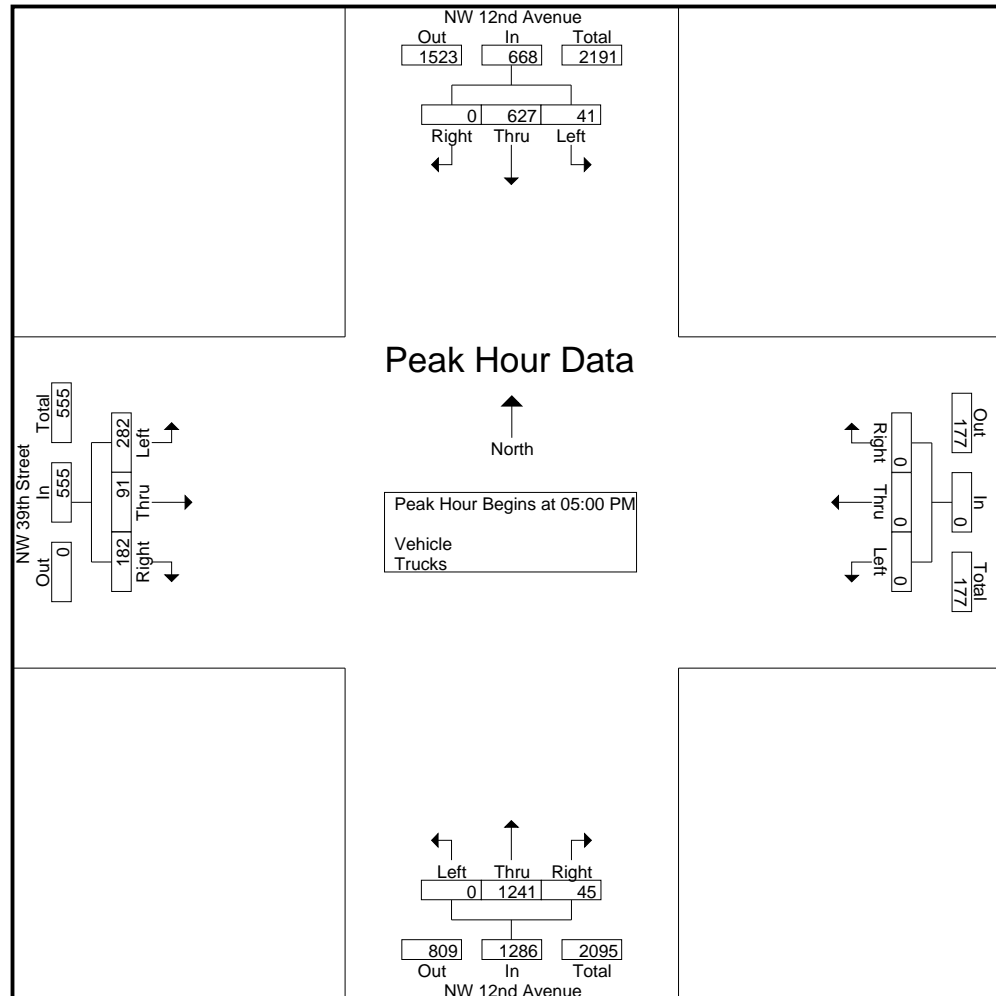
NW 12nd Avenue & NW 39th Street

File Name : TMC-2 NW 12nd Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	NW 10th Avenue Southbound			NW 10th Avenue Northbound			Westbound			NW 39th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	0	0	0	0	0	0	3	1	4	1	1	2	6
07:15 AM	0	0	0	0	0	0	0	0	0	2	2	4	4
07:30 AM	0	0	0	0	0	0	3	0	3	1	2	3	6
07:45 AM	0	0	0	0	0	0	3	0	3	1	1	2	5
Total	0	0	0	0	0	0	9	1	10	5	6	11	21
08:00 AM	0	0	0	0	0	0	3	1	4	1	0	1	5
08:15 AM	0	0	0	0	0	0	2	1	3	1	3	4	7
08:30 AM	0	0	0	0	0	0	2	1	3	0	2	2	5
08:45 AM	0	0	0	0	0	0	2	1	3	0	3	3	6
Total	0	0	0	0	0	0	9	4	13	2	8	10	23
*** BREAK ***													
03:00 PM	0	0	0	0	0	0	2	2	4	2	3	5	9
03:15 PM	0	0	0	0	0	0	0	6	6	5	1	6	12
03:30 PM	0	0	0	0	0	0	5	1	6	0	1	1	7
03:45 PM	0	0	0	0	0	0	0	1	1	4	2	6	7
Total	0	0	0	0	0	0	7	10	17	11	7	18	35
04:00 PM	0	0	0	0	0	0	1	2	3	0	1	1	4
04:15 PM	0	0	0	0	0	0	1	1	2	1	2	3	5
04:30 PM	0	0	0	0	0	0	4	1	5	0	0	0	5
04:45 PM	0	0	0	0	0	0	2	3	5	2	1	3	8
Total	0	0	0	0	0	0	8	7	15	3	4	7	22
05:00 PM	0	0	0	0	0	0	1	0	1	0	0	0	1
05:15 PM	0	0	0	0	0	0	2	1	3	2	3	5	8
05:30 PM	0	0	0	0	0	0	5	2	7	0	3	3	10
05:45 PM	0	0	0	0	0	0	3	1	4	3	3	6	10
Total	0	0	0	0	0	0	11	4	15	5	9	14	29
Grand Total	0	0	0	0	0	0	44	26	70	26	34	60	130
Apprch %	0	0	0	0	0	0	62.9	37.1	70	43.3	56.7	60	130
Total %	0	0	0	0	0	0	33.8	20	53.8	20	26.2	46.2	130

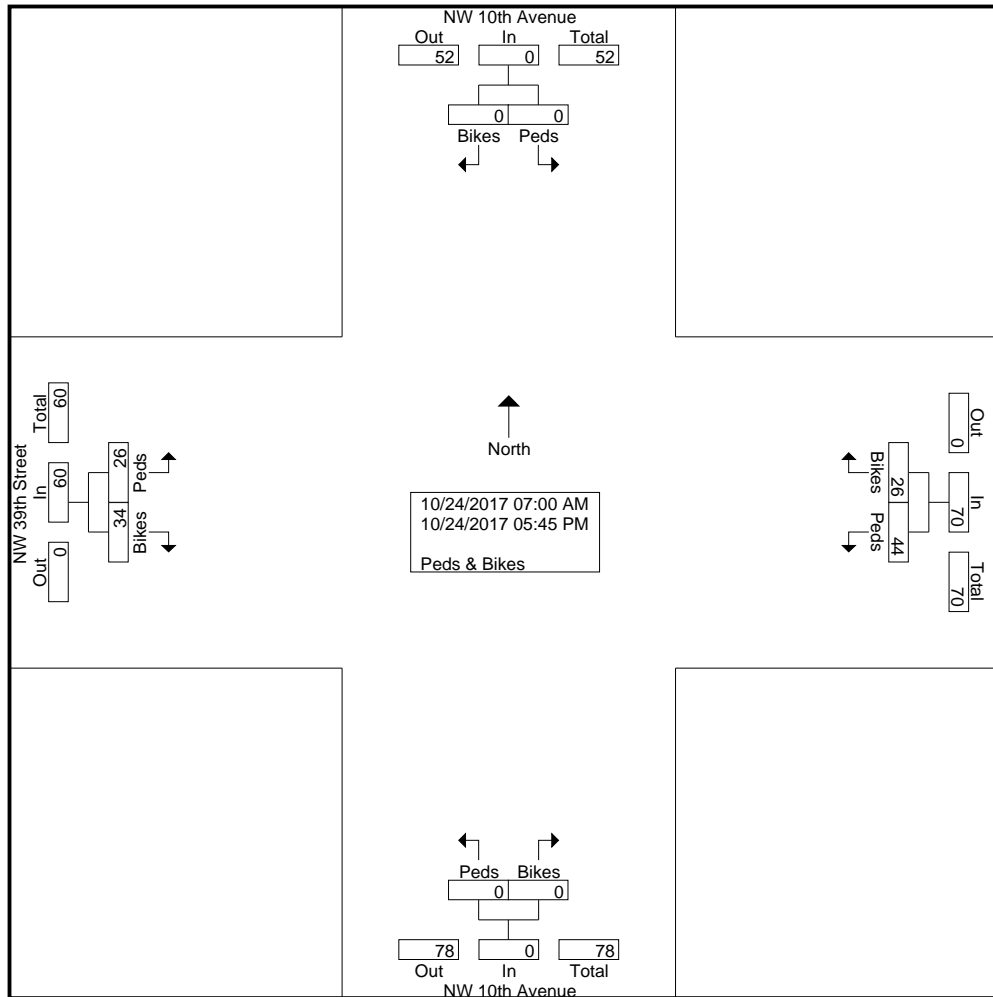
NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



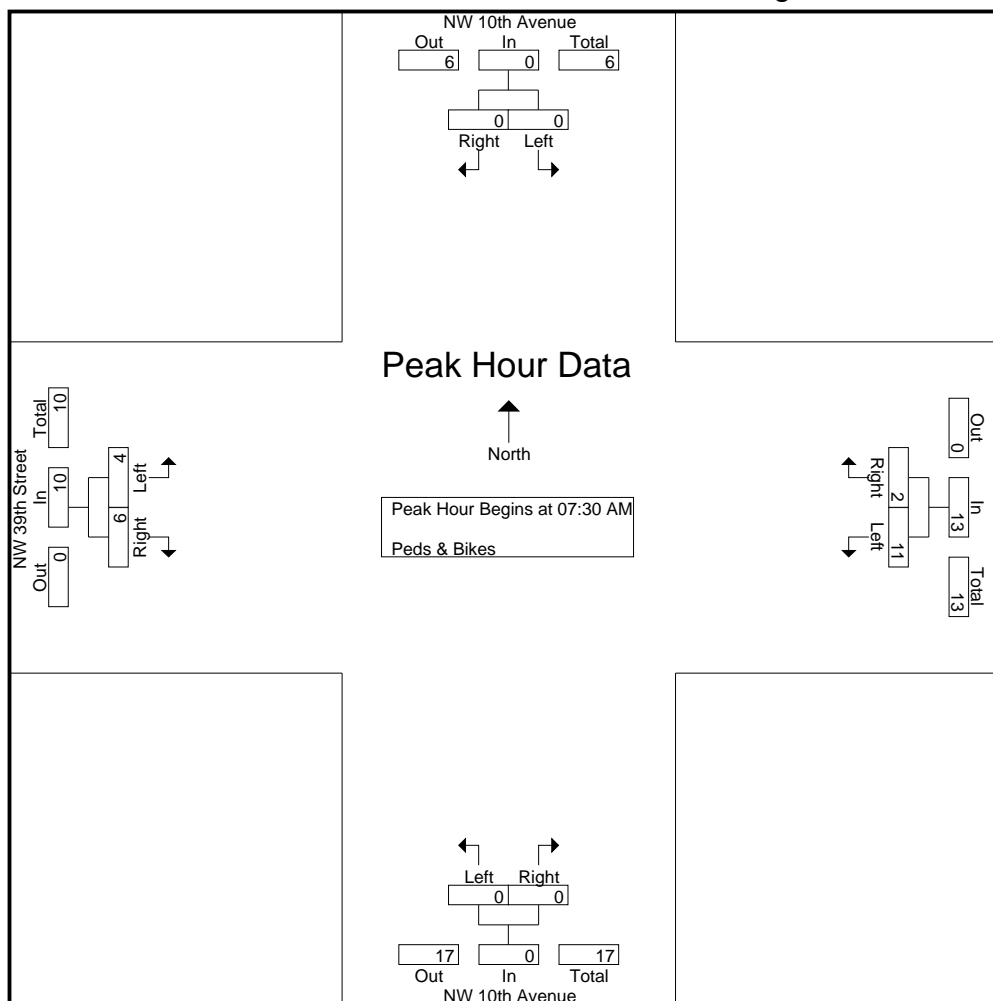
NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NW 10th Avenue Southbound			NW 10th Avenue Northbound			Westbound			NW 39th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:30 AM													
07:30 AM	0	0	0	0	0	0	3	0	3	1	2	3	6
07:45 AM	0	0	0	0	0	0	3	0	3	1	1	2	5
08:00 AM	0	0	0	0	0	0	3	1	4	1	0	1	5
08:15 AM	0	0	0	0	0	0	2	1	3	1	3	4	7
Total Volume	0	0	0	0	0	0	11	2	13	4	6	10	23
% App. Total	0	0		0	0		84.6	15.4		40	60		
PHF	.000	.000	.000	.000	.000	.000	.917	.500	.813	1.00	.500	.625	.821

NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4



NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	NW 10th Avenue Southbound			NW 10th Avenue Northbound			Westbound			NW 39th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 03:00 PM													
03:00 PM	0	0	0	0	0	0	2	2	4	2	3	5	9
03:15 PM	0	0	0	0	0	0	0	6	6	5	1	6	12
03:30 PM	0	0	0	0	0	0	5	1	6	0	1	1	7
03:45 PM	0	0	0	0	0	0	0	1	1	4	2	6	7
Total Volume	0	0	0	0	0	0	7	10	17	11	7	18	35
% App. Total	0	0	0	0	0	0	41.2	58.8		61.1	38.9		
PHF	.000	.000	.000	.000	.000	.000	.350	.417	.708	.550	.583	.750	.729

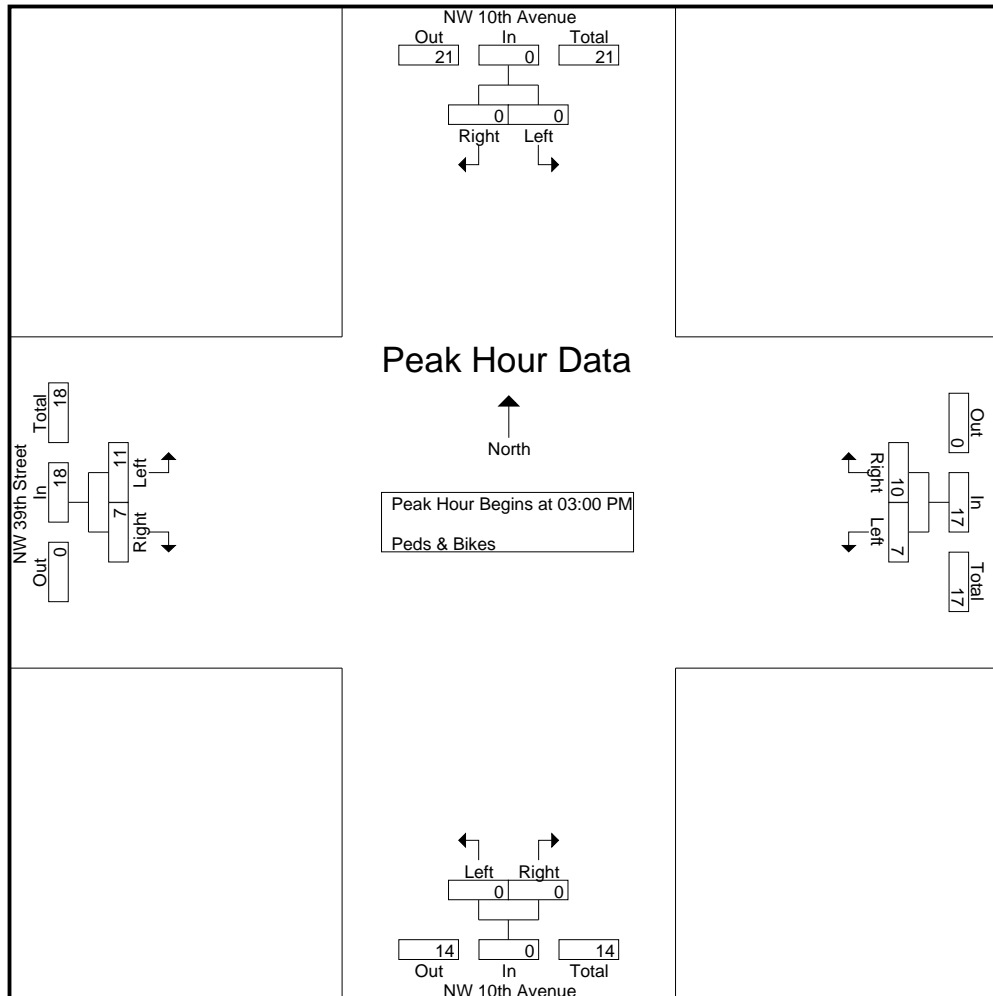
NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Trucks

Start Time	NW 10th Avenue Southbound					NW 10th Avenue Northbound					Westbound					NW 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	0	0	2	2	4	0	0	0	0	0	0	0	0	2	2	6
07:15 AM	0	1	0	0	1	0	0	1	5	6	0	0	0	0	0	0	0	2	1	3	10
07:30 AM	0	0	2	0	2	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	5
07:45 AM	0	0	2	0	2	0	0	1	5	6	0	0	0	0	0	0	0	0	1	1	9
Total	0	1	4	0	5	0	0	5	14	19	0	0	0	0	0	0	0	2	4	6	30
08:00 AM	0	0	1	0	1	0	0	3	1	4	0	0	0	0	0	0	2	1	0	3	8
08:15 AM	0	0	1	0	1	0	0	2	1	3	0	0	0	0	0	0	0	1	2	3	7
08:30 AM	0	0	1	0	1	0	0	3	3	6	0	0	0	0	0	0	0	1	0	1	8
08:45 AM	0	0	1	0	1	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	3
Total	0	0	4	0	4	0	0	9	6	15	0	0	0	0	0	0	2	3	2	7	26
*** BREAK ***																					
03:00 PM	0	0	1	0	1	0	0	4	0	4	0	0	0	0	0	0	0	0	0	0	5
03:15 PM	0	0	2	0	2	0	0	4	3	7	0	0	0	0	0	0	0	0	3	3	12
03:30 PM	0	1	2	0	3	0	0	1	4	5	0	0	0	0	0	0	0	0	0	0	8
03:45 PM	0	0	0	0	0	0	0	2	4	6	0	0	0	0	0	0	0	0	0	0	6
Total	0	1	5	0	6	0	0	11	11	22	0	0	0	0	0	0	0	0	3	3	31
04:00 PM	0	0	1	0	1	0	0	2	4	6	0	0	0	0	0	0	0	0	0	0	7
04:15 PM	0	0	0	0	0	0	0	3	3	6	0	0	0	0	0	0	0	1	2	3	9
04:30 PM	0	0	2	0	2	0	0	11	0	11	0	0	0	0	0	0	1	0	1	2	15
04:45 PM	0	0	0	0	0	0	0	8	0	8	0	0	0	0	0	0	0	0	0	0	8
Total	0	0	3	0	3	0	0	24	7	31	0	0	0	0	0	0	1	1	3	5	39
05:00 PM	0	0	0	0	0	0	0	8	0	8	0	0	0	0	0	0	0	0	0	0	8
05:15 PM	0	0	1	0	1	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	3
05:30 PM	0	0	1	0	1	0	0	3	0	3	0	0	0	0	0	0	0	0	2	2	6
05:45 PM	0	0	0	0	0	0	0	5	3	8	0	0	0	0	0	0	0	0	0	0	8
Total	0	0	2	0	2	0	0	18	3	21	0	0	0	0	0	0	0	0	2	2	25
Grand Total	0	2	18	0	20	0	0	67	41	108	0	0	0	0	0	0	3	6	14	23	151
Apprch %	0	10	90	0		0	0	62	38		0	0	0	0		0	13	26.1	60.9		
Total %	0	1.3	11.9	0	13.2	0	0	44.4	27.2	71.5	0	0	0	0	0	0	2	4	9.3	15.2	

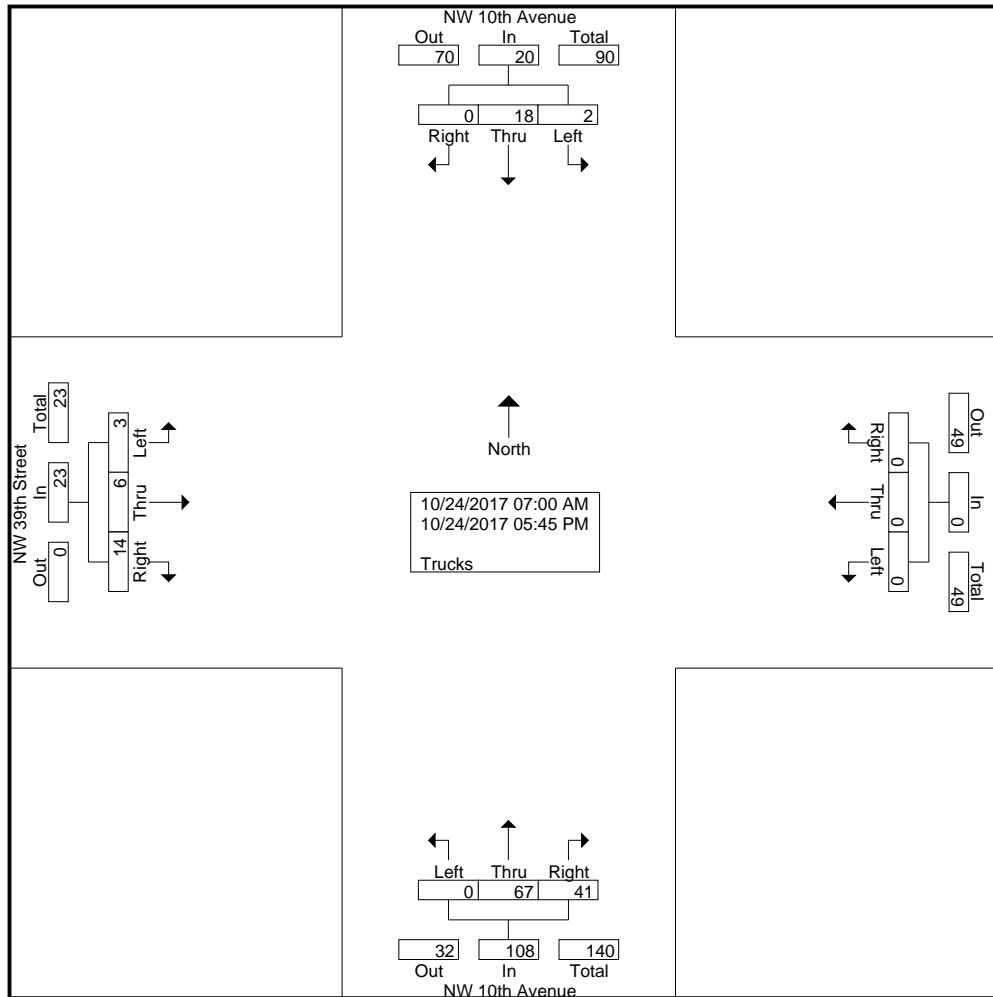
NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NW 10th Avenue Southbound					NW 10th Avenue Northbound					Westbound					NW 39th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 07:15 AM																						
07:15 AM	0	1	0	0	1	0	0	1	5	6	0	0	0	0	0	0	0	2	1	3	10	
07:30 AM	0	0	2	0	2	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	5	
07:45 AM	0	0	2	0	2	0	0	1	5	6	0	0	0	0	0	0	0	0	1	1	9	
08:00 AM	0	0	1	0	1	0	0	3	1	4	0	0	0	0	0	0	2	1	0	3	8	
Total Volume	0	1	5	0	6	0	0	6	13	19	0	0	0	0	0	0	2	3	2	7	32	
% App. Total	0	16.7	83.3	0		0	0	31.6	68.4		0	0	0	0		0	28.6	42.9	28.6			
PHF	.000	.250	.625	.000	.750	.000	.000	.500	.650	.792	.000	.000	.000	.000	.000	.000	.250	.375	.500	.583	.800	

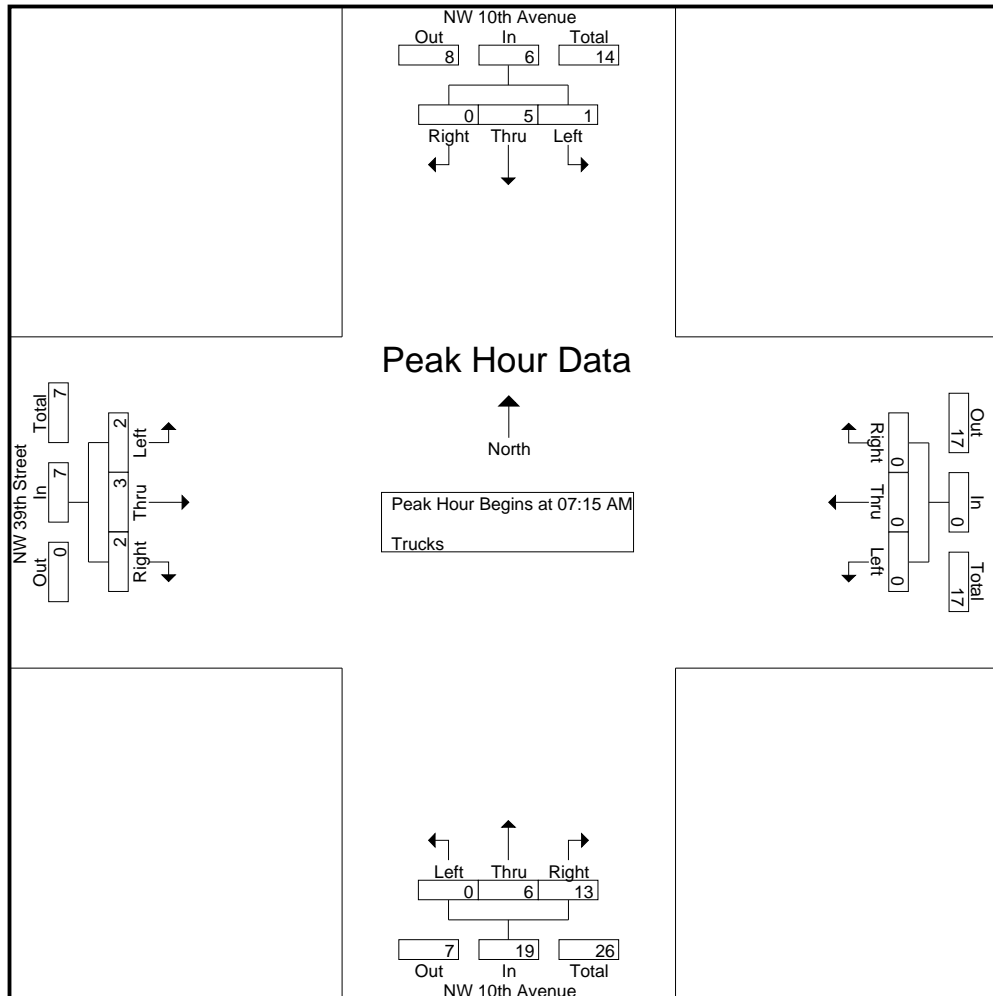
NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	NW 10th Avenue Southbound					NW 10th Avenue Northbound					Westbound					NW 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:15 PM																					
04:15 PM	0	0	0	0	0	0	0	3	3	6	0	0	0	0	0	0	0	1	2	3	9
04:30 PM	0	0	2	0	2	0	0	11	0	11	0	0	0	0	0	0	1	0	1	2	15
04:45 PM	0	0	0	0	0	0	0	8	0	8	0	0	0	0	0	0	0	0	0	0	8
05:00 PM	0	0	0	0	0	0	0	8	0	8	0	0	0	0	0	0	0	0	0	0	8
Total Volume	0	0	2	0	2	0	0	30	3	33	0	0	0	0	0	0	1	1	3	5	40
% App. Total	0	0	100	0		0	0	90.9	9.1		0	0	0	0		0	20	20	60		
PHF	.000	.000	.250	.000	.250	.000	.000	.682	.250	.750	.000	.000	.000	.000	.000	.000	.250	.250	.375	.417	.667

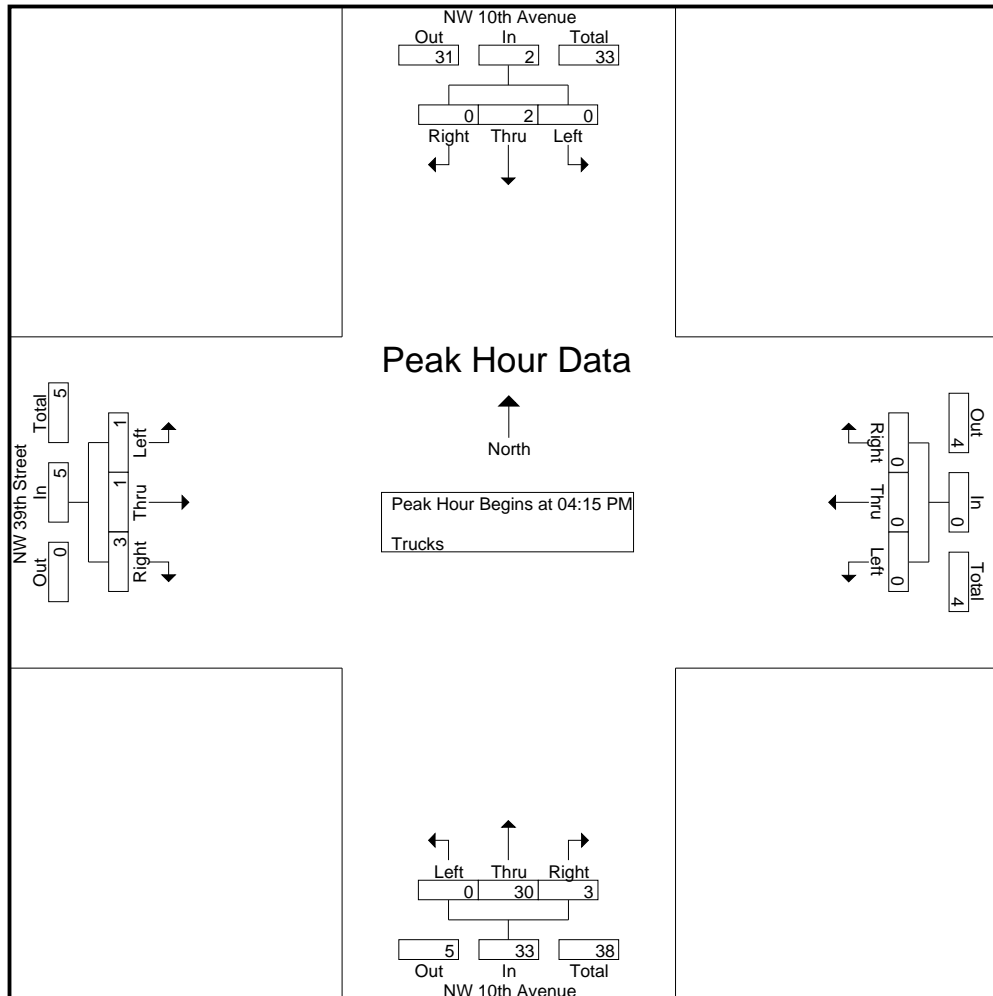
NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	NW 10th Avenue Southbound					NW 10th Avenue Northbound					Westbound					NW 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	4	81	0	85	0	0	40	28	68	0	0	0	0	0	0	8	21	20	49	202
07:15 AM	0	4	119	0	123	0	0	42	36	78	0	0	0	0	0	0	7	18	14	39	240
07:30 AM	0	7	112	0	119	0	0	58	49	107	0	0	0	0	0	0	5	13	10	28	254
07:45 AM	0	6	109	0	115	0	0	75	45	120	0	0	0	0	0	0	16	20	14	50	285
Total	0	21	421	0	442	0	0	215	158	373	0	0	0	0	0	0	36	72	58	166	981
08:00 AM	0	8	103	0	111	0	0	64	47	111	0	0	0	0	0	0	12	21	11	44	266
08:15 AM	0	7	70	0	77	0	0	52	30	82	0	0	0	0	0	0	15	21	12	48	207
08:30 AM	0	4	106	0	110	0	0	48	32	80	0	0	0	0	0	0	16	16	13	45	235
08:45 AM	0	3	83	0	86	0	0	43	23	66	0	0	0	0	0	0	17	18	14	49	201
Total	0	22	362	0	384	0	0	207	132	339	0	0	0	0	0	0	60	76	50	186	909
*** BREAK ***																					
03:00 PM	0	1	41	0	42	0	0	130	12	142	0	0	0	0	0	0	26	6	11	43	227
03:15 PM	0	2	51	0	53	0	0	140	20	160	0	0	0	0	0	0	20	5	12	37	250
03:30 PM	0	7	49	0	56	0	0	89	70	159	0	0	0	0	0	0	6	25	16	47	262
03:45 PM	0	12	49	0	61	0	0	124	99	223	0	0	0	0	0	0	10	24	6	40	324
Total	0	22	190	0	212	0	0	483	201	684	0	0	0	0	0	0	62	60	45	167	1063
04:00 PM	0	12	42	0	54	0	0	130	82	212	0	0	0	0	0	0	12	20	12	44	310
04:15 PM	0	9	43	0	52	0	0	139	64	203	0	0	0	0	0	0	14	22	14	50	305
04:30 PM	0	0	54	0	54	0	0	195	0	195	0	0	0	0	0	0	23	1	14	38	287
04:45 PM	0	0	42	0	42	0	0	206	0	206	0	0	0	0	0	0	18	2	8	28	276
Total	0	21	181	0	202	0	0	670	146	816	0	0	0	0	0	0	67	45	48	160	1178
05:00 PM	0	2	47	0	49	0	0	165	3	168	0	0	0	0	0	0	11	1	14	26	243
05:15 PM	0	0	45	0	45	0	0	176	0	176	0	0	0	0	0	0	24	3	9	36	257
05:30 PM	0	1	59	0	60	0	0	190	0	190	0	0	0	0	0	0	22	1	25	48	298
05:45 PM	0	3	57	0	60	0	0	166	24	190	0	0	0	0	0	0	28	11	19	58	308
Total	0	6	208	0	214	0	0	697	27	724	0	0	0	0	0	0	85	16	67	168	1106
Grand Total	0	92	1362	0	1454	0	0	2272	664	2936	0	0	0	0	0	0	310	269	268	847	5237
Apprch %	0	6.3	93.7	0		0	0	77.4	22.6		0	0	0	0		0	36.6	31.8	31.6		
Total %	0	1.8	26	0	27.8	0	0	43.4	12.7	56.1	0	0	0	0	0	0	5.9	5.1	5.1	16.2	
Vehicle	0	90	1344	0	1434	0	0	2205	623	2828	0	0	0	0	0	0	307	263	254	824	5086
% Vehicle	0	97.8	98.7	0	98.6	0	0	97.1	93.8	96.3	0	0	0	0	0	0	99	97.8	94.8	97.3	97.1

NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	NW 10th Avenue Southbound					NW 10th Avenue Northbound					Westbound					NW 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	2	18	0	20	0	0	67	41	108	0	0	0	0	0	0	3	6	14	23	151
% Trucks	0	2.2	1.3	0	1.4	0	0	2.9	6.2	3.7	0	0	0	0	0	0	1	2.2	5.2	2.7	2.9

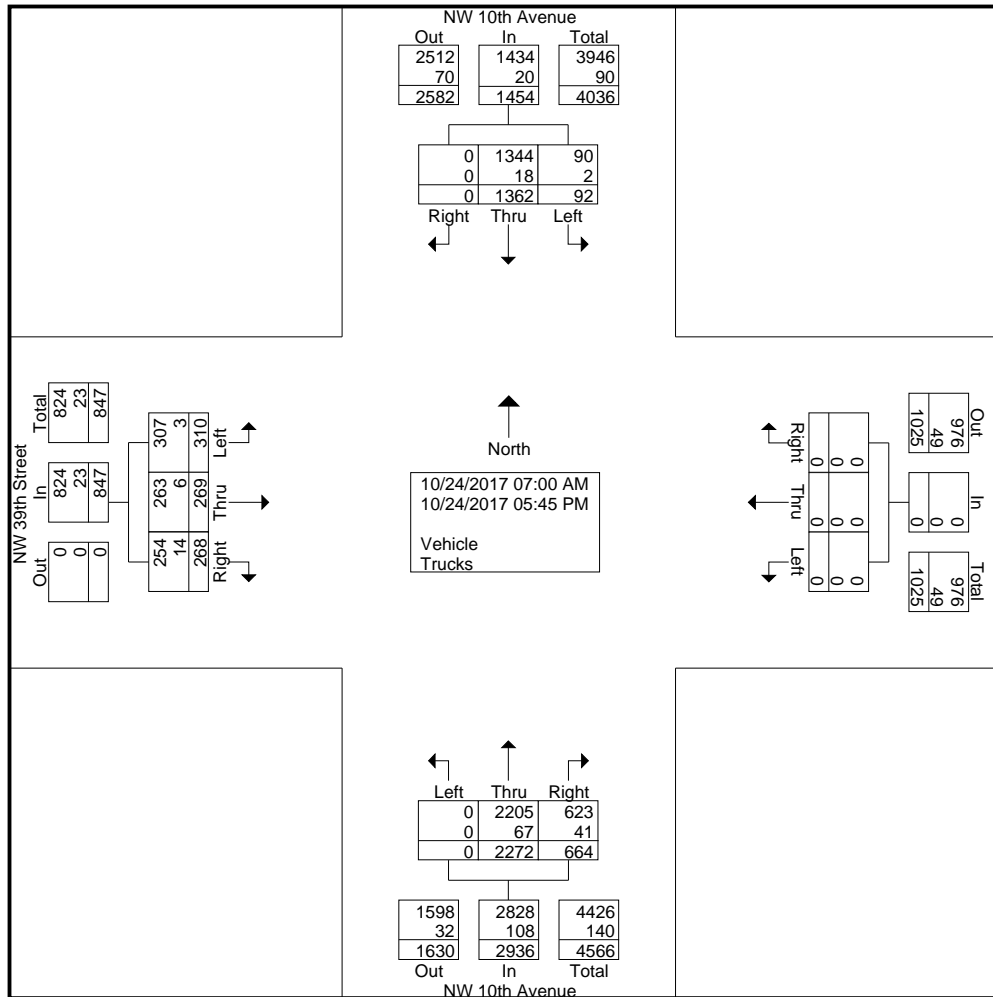
NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4

Start Time	NW 10th Avenue Southbound					NW 10th Avenue Northbound					Westbound					NW 39th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 07:15 AM																						
07:15 AM	0	4	119	0	123	0	0	42	36	78	0	0	0	0	0	0	7	18	14	39	240	
07:30 AM	0	7	112	0	119	0	0	58	49	107	0	0	0	0	0	0	5	13	10	28	254	
07:45 AM	0	6	109	0	115	0	0	75	45	120	0	0	0	0	0	0	16	20	14	50	285	
08:00 AM	0	8	103	0	111	0	0	64	47	111	0	0	0	0	0	0	12	21	11	44	266	
Total Volume	0	25	443	0	468	0	0	239	177	416	0	0	0	0	0	0	40	72	49	161	1045	
% App. Total	0	5.3	94.7	0		0	0	57.5	42.5		0	0	0	0	0	0	24.8	44.7	30.4			
PHF	.000	.781	.931	.000	.951	.000	.000	.797	.903	.867	.000	.000	.000	.000	.000	.000	.625	.857	.875	.805	.917	

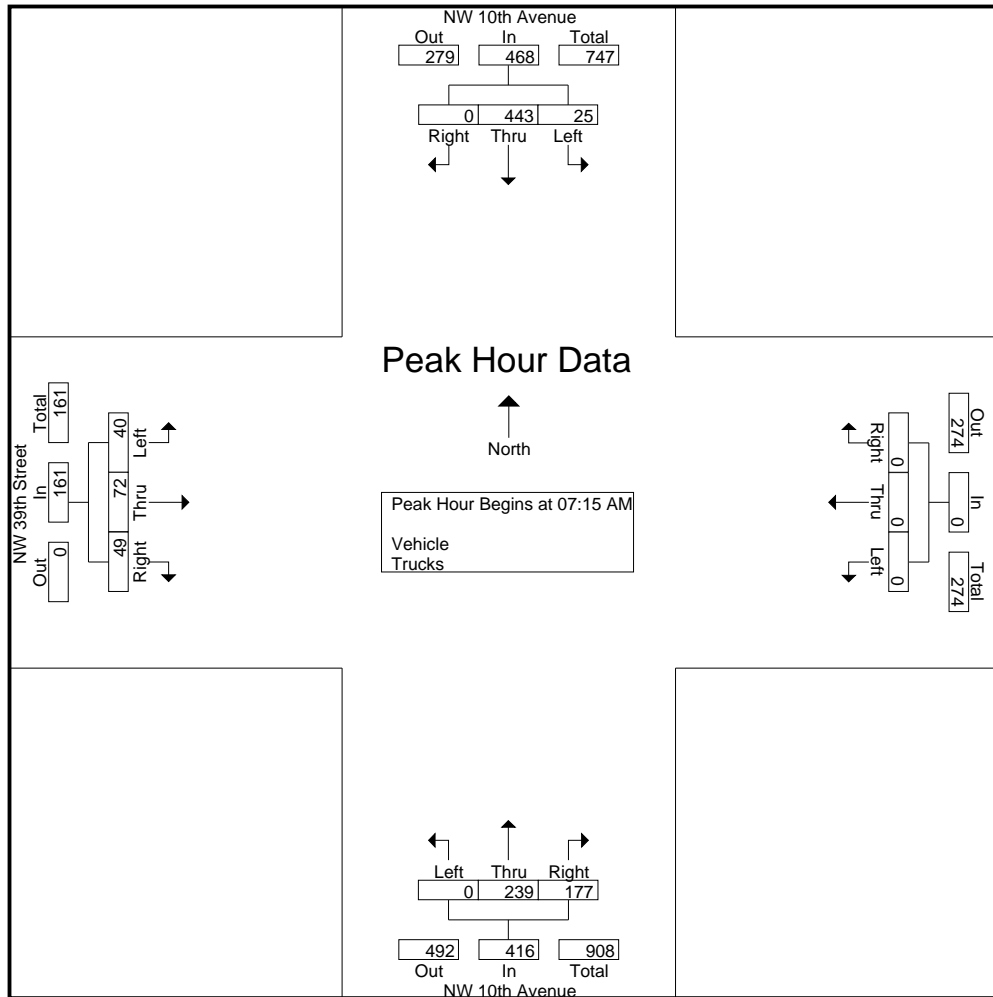
NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 6

Start Time	NW 10th Avenue Southbound					NW 10th Avenue Northbound					Westbound					NW 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:45 PM																					
03:45 PM	0	12	49	0	61	0	0	124	99	223	0	0	0	0	0	0	10	24	6	40	324
04:00 PM	0	12	42	0	54	0	0	130	82	212	0	0	0	0	0	0	12	20	12	44	310
04:15 PM	0	9	43	0	52	0	0	139	64	203	0	0	0	0	0	0	14	22	14	50	305
04:30 PM	0	0	54	0	54	0	0	195	0	195	0	0	0	0	0	0	23	1	14	38	287
Total Volume	0	33	188	0	221	0	0	588	245	833	0	0	0	0	0	0	59	67	46	172	1226
% App. Total	0	14.9	85.1	0		0	0	70.6	29.4		0	0	0	0		0	34.3	39	26.7		
PHF	.000	.688	.870	.000	.906	.000	.000	.754	.619	.934	.000	.000	.000	.000	.000	.000	.641	.698	.821	.860	.946

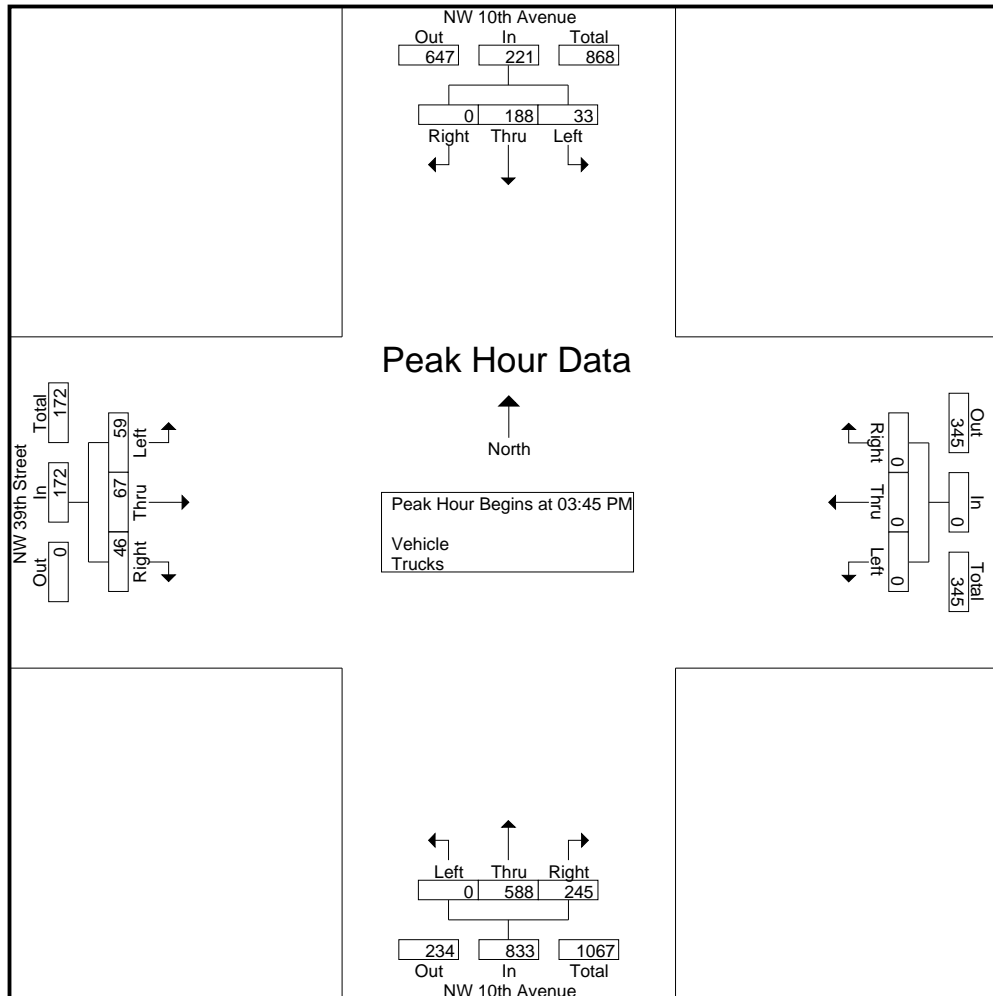
NW 10th Avenue & NW 39th Street

File Name : TMC-3 NW 10th Avenue & NW 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



N Miami Avenue & NE 36th Street

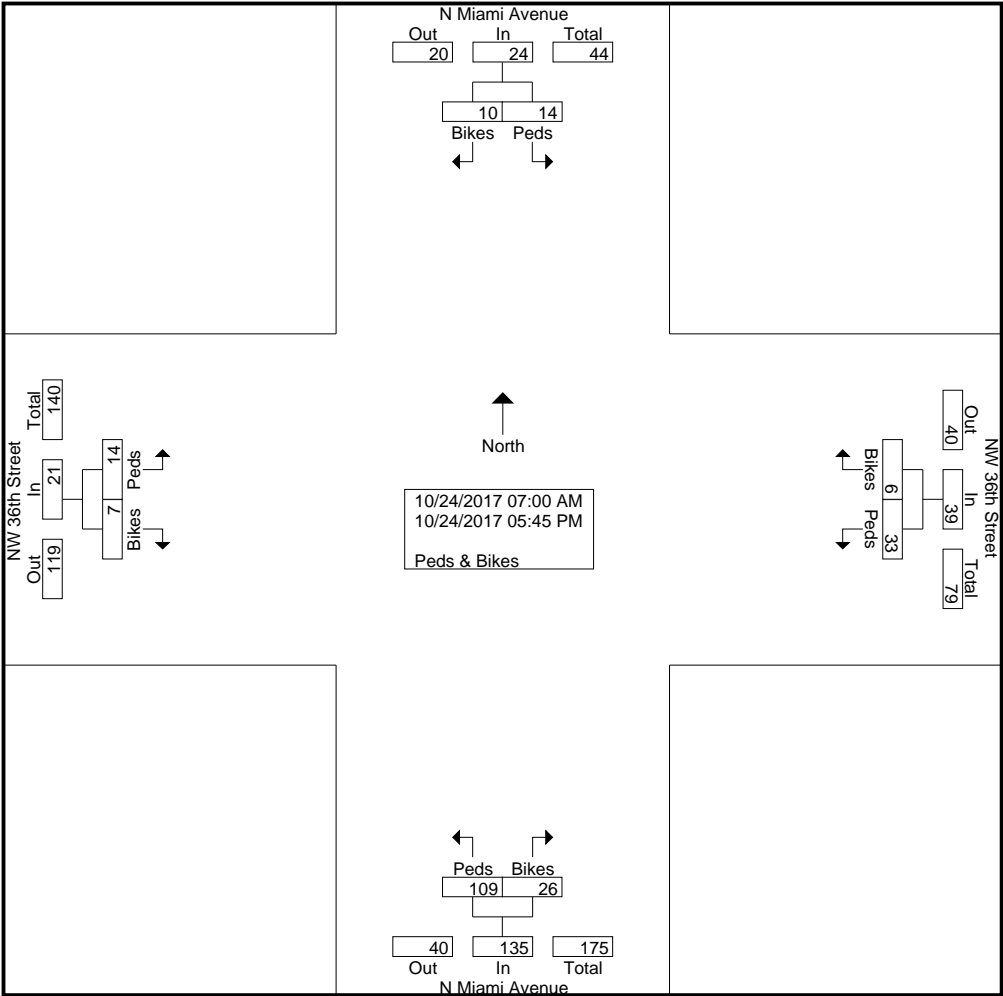
File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	N Miami Avenue Southbound			N Miami Avenue Northbound			NW 36th Street Westbound			NW 36th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	1
07:15 AM	0	0	0	1	0	1	0	0	0	0	0	0	1
07:30 AM	0	2	2	1	0	1	0	0	0	0	0	0	3
07:45 AM	1	1	2	0	1	1	1	0	1	0	0	0	4
Total	1	4	5	2	1	3	1	0	1	0	0	0	9
08:00 AM	0	1	1	2	1	3	0	0	0	0	0	0	4
08:15 AM	0	0	0	5	1	6	0	0	0	0	1	1	7
08:30 AM	1	1	2	3	1	4	2	1	3	2	0	2	11
08:45 AM	0	0	0	1	0	1	3	0	3	0	0	0	4
Total	1	2	3	11	3	14	5	1	6	2	1	3	26
*** BREAK ***													
03:00 PM	2	0	2	5	3	8	0	0	0	1	0	1	11
03:15 PM	0	0	0	3	1	4	0	1	1	0	2	2	7
03:30 PM	0	1	1	6	3	9	4	3	7	0	2	2	19
03:45 PM	1	0	1	8	1	9	3	0	3	0	0	0	13
Total	3	1	4	22	8	30	7	4	11	1	4	5	50
04:00 PM	1	0	1	8	0	8	3	0	3	4	0	4	16
04:15 PM	2	0	2	5	2	7	6	0	6	1	0	1	16
04:30 PM	2	0	2	10	1	11	0	0	0	0	0	0	13
04:45 PM	0	0	0	11	1	12	2	0	2	1	0	1	15
Total	5	0	5	34	4	38	11	0	11	6	0	6	60
05:00 PM	0	0	0	9	2	11	0	0	0	0	0	0	11
05:15 PM	1	1	2	14	5	19	2	0	2	1	0	1	24
05:30 PM	1	1	2	6	1	7	1	1	2	4	1	5	16
05:45 PM	2	1	3	11	2	13	6	0	6	0	1	1	23
Total	4	3	7	40	10	50	9	1	10	5	2	7	74
Grand Total	14	10	24	109	26	135	33	6	39	14	7	21	219
Apprch %	58.3	41.7		80.7	19.3		84.6	15.4		66.7	33.3		
Total %	6.4	4.6	11	49.8	11.9	61.6	15.1	2.7	17.8	6.4	3.2	9.6	

N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2



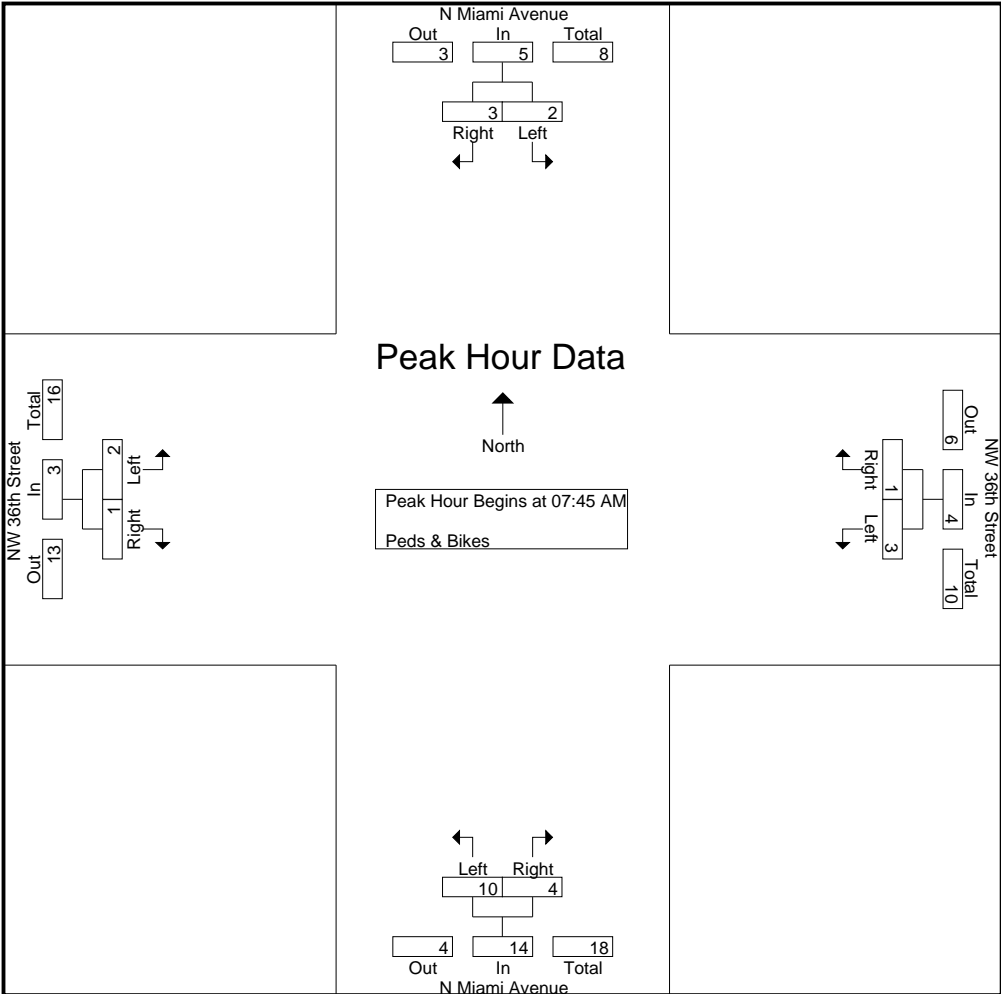
N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	N Miami Avenue Southbound			N Miami Avenue Northbound			NW 36th Street Westbound			NW 36th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:45 AM													
07:45 AM	1	1	2	0	1	1	1	0	1	0	0	0	4
08:00 AM	0	1	1	2	1	3	0	0	0	0	0	0	4
08:15 AM	0	0	0	5	1	6	0	0	0	0	1	1	7
08:30 AM	1	1	2	3	1	4	2	1	3	2	0	2	11
Total Volume	2	3	5	10	4	14	3	1	4	2	1	3	26
% App. Total	40	60		71.4	28.6		75	25		66.7	33.3		
PHF	.500	.750	.625	.500	1.00	.583	.375	.250	.333	.250	.250	.375	.591

N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4



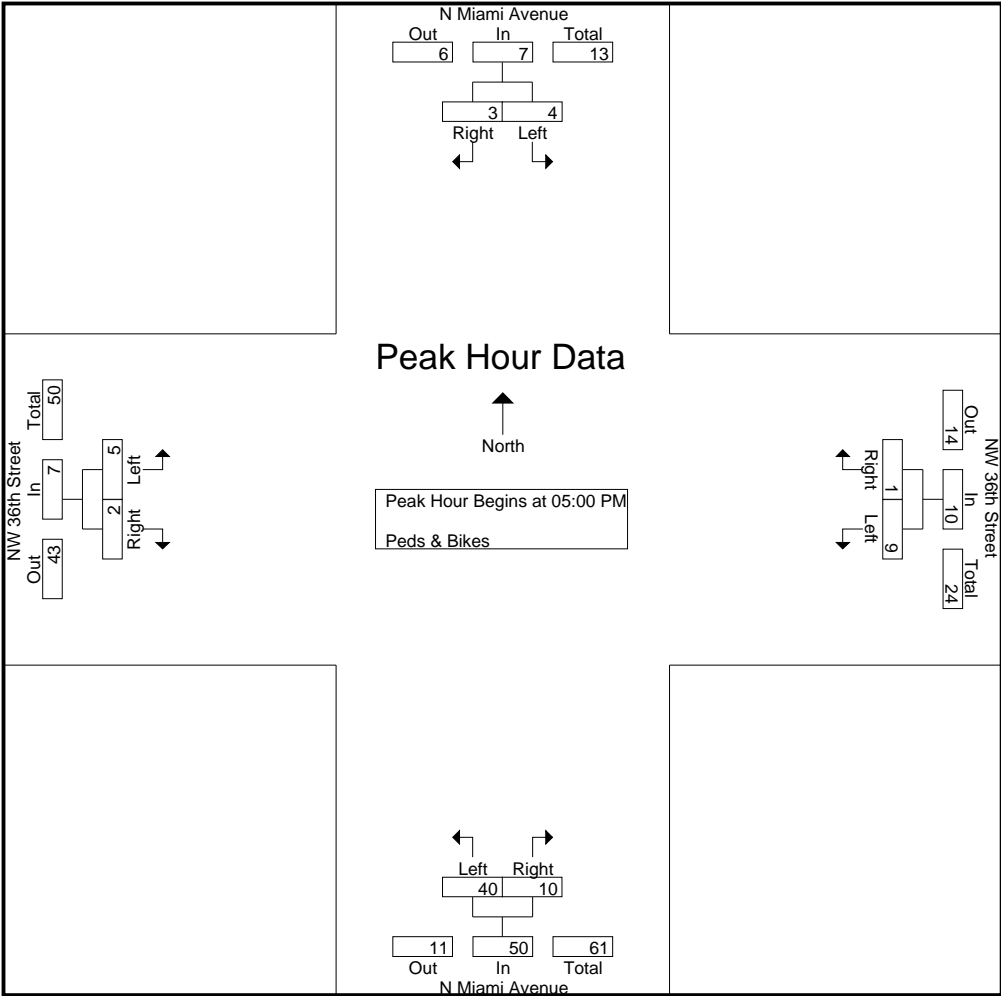
N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	N Miami Avenue Southbound			N Miami Avenue Northbound			NW 36th Street Westbound			NW 36th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 05:00 PM													
05:00 PM	0	0	0	9	2	11	0	0	0	0	0	0	11
05:15 PM	1	1	2	14	5	19	2	0	2	1	0	1	24
05:30 PM	1	1	2	6	1	7	1	1	2	4	1	5	16
05:45 PM	2	1	3	11	2	13	6	0	6	0	1	1	23
Total Volume	4	3	7	40	10	50	9	1	10	5	2	7	74
% App. Total	57.1	42.9		80	20		90	10		71.4	28.6		
PHF	.500	.750	.583	.714	.500	.658	.375	.250	.417	.313	.500	.350	.771

N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 6



N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street

Site Code : 00000000

Start Date : 10/24/2017

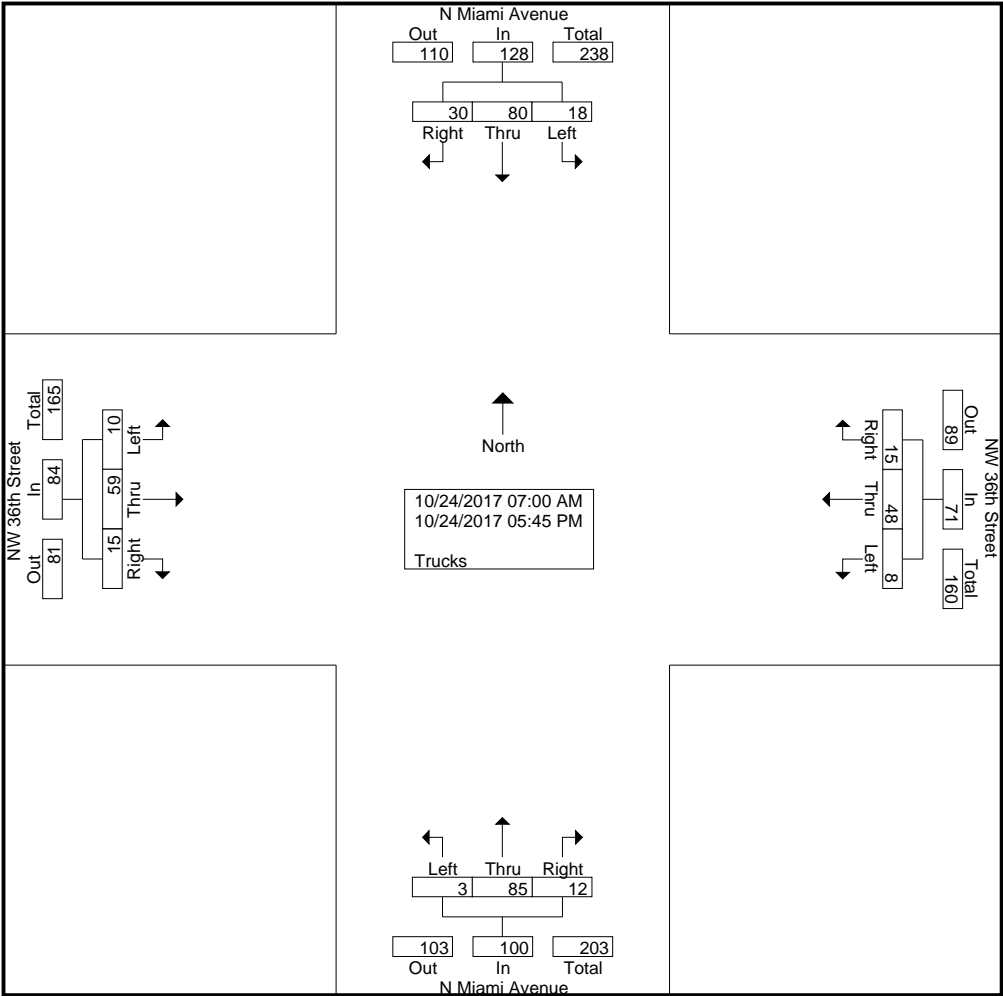
Page No : 1

Groups Printed- Trucks

Start Time	N Miami Avenue Southbound					N Miami Avenue Northbound					NW 36th Street Westbound					NW 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	4	2	6	0	0	2	0	2	0	0	4	0	4	0	2	4	1	7	19
07:15 AM	0	2	9	2	13	0	0	6	0	6	0	1	0	1	2	0	2	3	0	5	26
07:30 AM	0	0	3	0	3	0	0	5	1	6	0	1	3	1	5	0	3	3	1	7	21
07:45 AM	0	1	3	1	5	0	0	3	1	4	0	0	3	0	3	0	0	6	1	7	19
Total	0	3	19	5	27	0	0	16	2	18	0	2	10	2	14	0	7	16	3	26	85
08:00 AM	0	4	5	2	11	0	0	3	0	3	0	2	2	0	4	0	0	4	1	5	23
08:15 AM	0	2	2	2	6	0	0	1	0	1	0	0	1	1	2	0	0	5	2	7	16
08:30 AM	0	5	6	3	14	0	1	5	2	8	0	0	3	0	3	0	0	3	0	3	28
08:45 AM	0	2	13	2	17	0	0	3	2	5	0	1	2	3	6	0	0	3	0	3	31
Total	0	13	26	9	48	0	1	12	4	17	0	3	8	4	15	0	0	15	3	18	98
*** BREAK ***																					
03:00 PM	0	0	4	1	5	0	0	8	0	8	0	1	3	0	4	0	0	0	0	0	17
03:15 PM	0	1	5	1	7	0	0	8	2	10	0	1	0	1	2	0	0	3	1	4	23
03:30 PM	0	0	4	0	4	0	1	4	0	5	0	0	3	0	3	0	0	0	0	0	12
03:45 PM	0	0	4	3	7	0	0	4	0	4	0	0	4	0	4	0	0	8	1	9	24
Total	0	1	17	5	23	0	1	24	2	27	0	2	10	1	13	0	0	11	2	13	76
04:00 PM	0	0	5	0	5	0	0	5	0	5	0	1	3	2	6	0	0	3	1	4	20
04:15 PM	0	0	2	2	4	0	0	5	2	7	0	0	4	0	4	0	1	1	0	2	17
04:30 PM	0	0	3	1	4	0	0	4	1	5	0	0	3	1	4	0	0	3	0	3	16
04:45 PM	0	0	3	1	4	0	1	2	0	3	0	0	1	2	3	0	1	1	1	3	13
Total	0	0	13	4	17	0	1	16	3	20	0	1	11	5	17	0	2	8	2	12	66
05:00 PM	0	0	4	2	6	0	0	0	0	0	0	0	0	1	1	0	0	1	2	3	10
05:15 PM	0	1	0	1	2	0	0	8	1	9	0	0	2	2	4	0	1	2	3	6	21
05:30 PM	0	0	0	4	4	0	0	7	0	7	0	0	3	0	3	0	0	2	0	2	16
05:45 PM	0	0	1	0	1	0	0	2	0	2	0	0	4	0	4	0	0	4	0	4	11
Total	0	1	5	7	13	0	0	17	1	18	0	0	9	3	12	0	1	9	5	15	58
Grand Total	0	18	80	30	128	0	3	85	12	100	0	8	48	15	71	0	10	59	15	84	383
Apprch %	0	14.1	62.5	23.4		0	3	85	12		0	11.3	67.6	21.1		0	11.9	70.2	17.9		
Total %	0	4.7	20.9	7.8	33.4	0	0.8	22.2	3.1	26.1	0	2.1	12.5	3.9	18.5	0	2.6	15.4	3.9	21.9	

N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2



N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	N Miami Avenue Southbound					N Miami Avenue Northbound					NW 36th Street Westbound					NW 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 08:00 AM																					
08:00 AM	0	4	5	2	11	0	0	3	0	3	0	2	2	0	4	0	0	4	1	5	23
08:15 AM	0	2	2	2	6	0	0	1	0	1	0	0	1	1	2	0	0	5	2	7	16
08:30 AM	0	5	6	3	14	0	1	5	2	8	0	0	3	0	3	0	0	3	0	3	28
08:45 AM	0	2	13	2	17	0	0	3	2	5	0	1	2	3	6	0	0	3	0	3	31
Total Volume	0	13	26	9	48	0	1	12	4	17	0	3	8	4	15	0	0	15	3	18	98
% App. Total	0	27.1	54.2	18.8		0	5.9	70.6	23.5		0	20	53.3	26.7		0	0	83.3	16.7		
PHF	.000	.650	.500	.750	.706	.000	.250	.600	.500	.531	.000	.375	.667	.333	.625	.000	.000	.750	.375	.643	.790

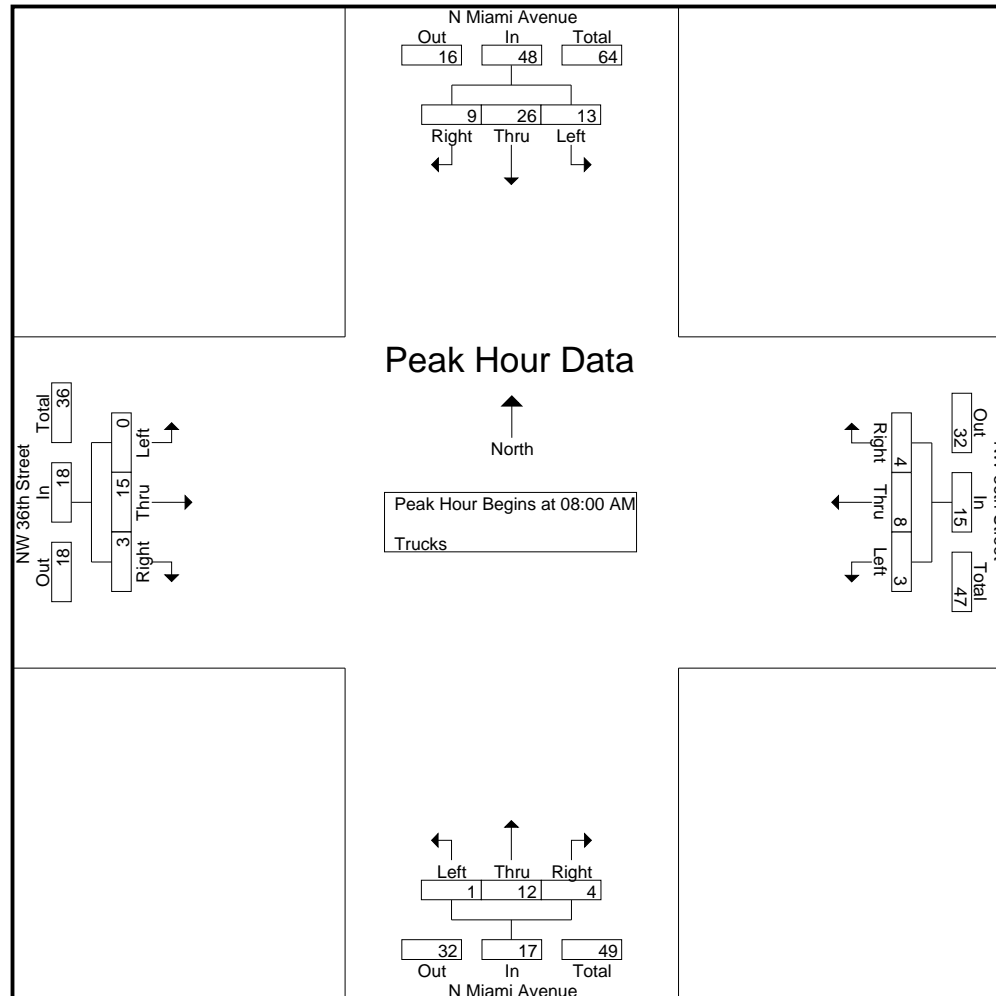
N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



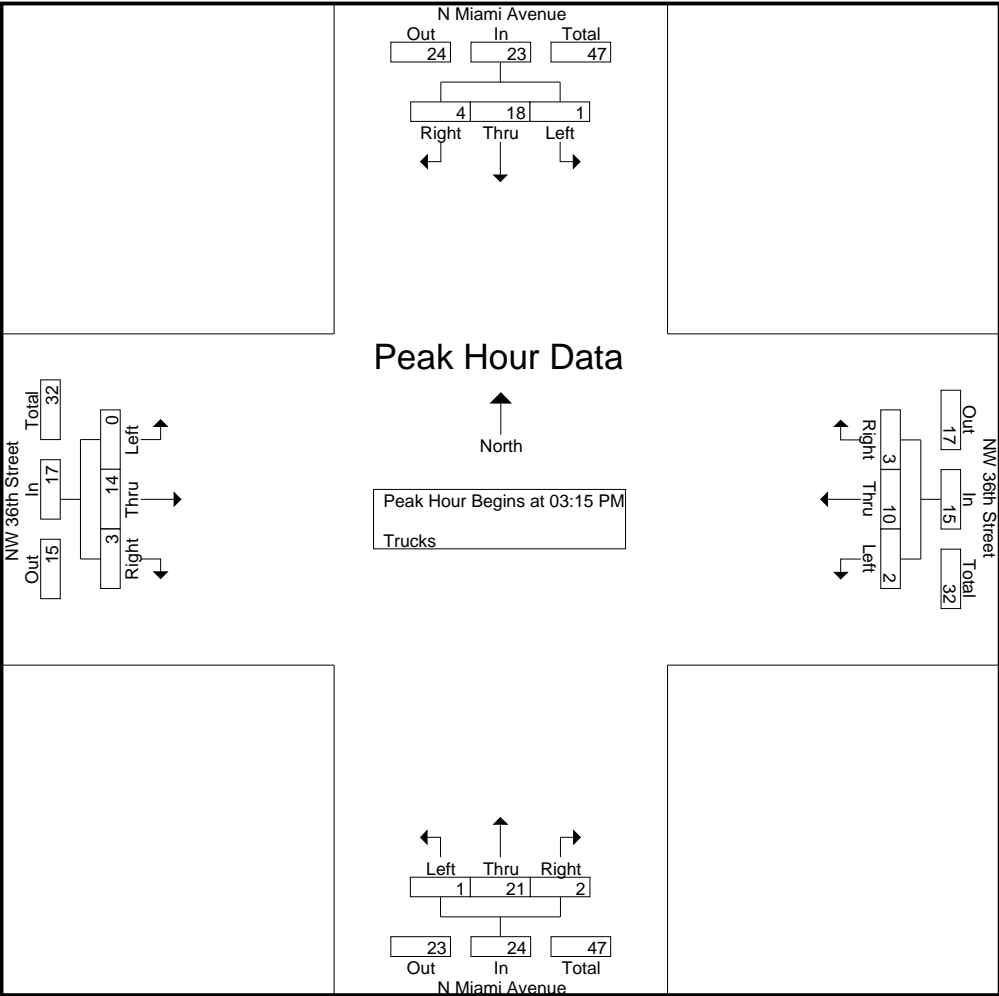
N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	N Miami Avenue Southbound					N Miami Avenue Northbound					NW 36th Street Westbound					NW 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:15 PM																					
03:15 PM	0	1	5	1	7	0	0	8	2	10	0	1	0	1	2	0	0	3	1	4	23
03:30 PM	0	0	4	0	4	0	1	4	0	5	0	0	3	0	3	0	0	0	0	0	12
03:45 PM	0	0	4	3	7	0	0	4	0	4	0	0	4	0	4	0	0	8	1	9	24
04:00 PM	0	0	5	0	5	0	0	5	0	5	0	1	3	2	6	0	0	3	1	4	20
Total Volume	0	1	18	4	23	0	1	21	2	24	0	2	10	3	15	0	0	14	3	17	79
% App. Total	0	4.3	78.3	17.4		0	4.2	87.5	8.3		0	13.3	66.7	20		0	0	82.4	17.6		
PHF	.000	.250	.900	.333	.821	.000	.250	.656	.250	.600	.000	.500	.625	.375	.625	.000	.000	.438	.750	.472	.823

N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 6



N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	N Miami Avenue Southbound					N Miami Avenue Northbound					NW 36th Street Westbound					NW 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	78	110	55	243	0	4	86	3	93	0	8	54	8	70	0	38	81	7	126	532
07:15 AM	0	70	133	50	253	0	5	64	1	70	0	6	17	18	41	0	30	89	4	123	487
07:30 AM	0	65	178	55	298	0	2	83	6	91	0	23	55	16	94	0	24	83	5	112	595
07:45 AM	0	77	204	62	343	0	3	109	17	129	0	12	43	10	65	0	15	76	5	96	633
Total	0	290	625	222	1137	0	14	342	27	383	0	49	169	52	270	0	107	329	21	457	2247
08:00 AM	0	69	179	64	312	0	2	118	23	143	0	17	40	12	69	0	19	85	4	108	632
08:15 AM	0	77	180	54	311	0	4	103	18	125	0	9	47	16	72	0	26	92	9	127	635
08:30 AM	0	83	190	57	330	0	7	95	18	120	0	26	42	16	84	0	24	85	6	115	649
08:45 AM	0	93	222	50	365	0	3	72	13	88	0	17	34	35	86	0	22	91	10	123	662
Total	0	322	771	225	1318	0	16	388	72	476	0	69	163	79	311	0	91	353	29	473	2578
*** BREAK ***																					
03:00 PM	0	61	119	50	230	0	8	223	17	248	0	20	70	23	113	0	10	76	15	101	692
03:15 PM	0	47	117	63	227	0	1	167	24	192	0	32	65	44	141	0	21	78	11	110	670
03:30 PM	0	43	111	62	216	0	10	183	22	215	0	32	99	22	153	0	16	69	10	95	679
03:45 PM	0	33	104	59	196	0	4	196	22	222	0	18	105	33	156	0	31	87	14	132	706
Total	0	184	451	234	869	0	23	769	85	877	0	102	339	122	563	0	78	310	50	438	2747
04:00 PM	0	31	88	71	190	0	11	224	16	251	0	8	84	34	126	0	28	96	13	137	704
04:15 PM	0	40	113	61	214	0	12	222	38	272	0	13	94	28	135	0	27	83	9	119	740
04:30 PM	0	34	99	59	192	0	16	298	18	332	0	12	76	31	119	0	39	82	6	127	770
04:45 PM	0	46	103	54	203	0	12	245	29	286	0	12	92	38	142	0	18	70	16	104	735
Total	0	151	403	245	799	0	51	989	101	1141	0	45	346	131	522	0	112	331	44	487	2949
05:00 PM	0	39	95	75	209	0	29	267	11	307	0	13	67	45	125	0	29	85	43	157	798
05:15 PM	0	36	104	72	212	0	21	268	23	312	0	20	67	50	137	0	23	89	36	148	809
05:30 PM	0	44	82	65	191	0	13	290	21	324	0	14	80	33	127	0	29	77	28	134	776
05:45 PM	0	46	113	86	245	0	20	255	29	304	0	26	52	30	108	0	22	59	26	107	764
Total	0	165	394	298	857	0	83	1080	84	1247	0	73	266	158	497	0	103	310	133	546	3147
Grand Total	0	1112	2644	1224	4980	0	187	3568	369	4124	0	338	1283	542	2163	0	491	1633	277	2401	13668
Apprch %	0	22.3	53.1	24.6		0	4.5	86.5	8.9		0	15.6	59.3	25.1		0	20.4	68	11.5		
Total %	0	8.1	19.3	9	36.4	0	1.4	26.1	2.7	30.2	0	2.5	9.4	4	15.8	0	3.6	11.9	2	17.6	
Vehicle	0	1094	2564	1194	4852	0	184	3483	357	4024	0	330	1235	527	2092	0	481	1574	262	2317	13285
% Vehicle	0	98.4	97	97.5	97.4	0	98.4	97.6	96.7	97.6	0	97.6	96.3	97.2	96.7	0	98	96.4	94.6	96.5	97.2

N Miami Avenue & NE 36th Street

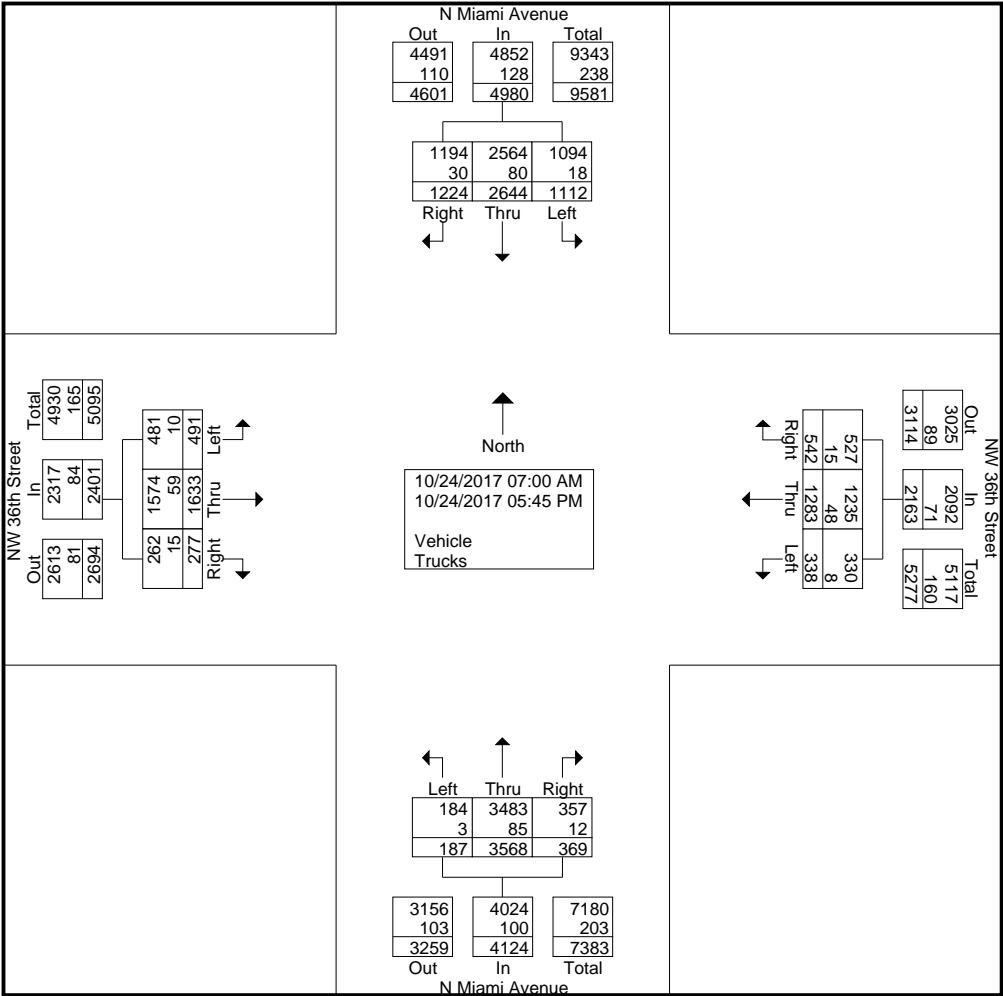
File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	N Miami Avenue Southbound					N Miami Avenue Northbound					NW 36th Street Westbound					NW 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	18	80	30	128	0	3	85	12	100	0	8	48	15	71	0	10	59	15	84	383
% Trucks	0	1.6	3	2.5	2.6	0	1.6	2.4	3.3	2.4	0	2.4	3.7	2.8	3.3	0	2	3.6	5.4	3.5	2.8

N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3



N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4

Start Time	N Miami Avenue Southbound					N Miami Avenue Northbound					NW 36th Street Westbound					NW 36th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	69	179	64	312	0	2	118	23	143	0	17	40	12	69	0	19	85	4	108	632	
08:15 AM	0	77	180	54	311	0	4	103	18	125	0	9	47	16	72	0	26	92	9	127	635	
08:30 AM	0	83	190	57	330	0	7	95	18	120	0	26	42	16	84	0	24	85	6	115	649	
08:45 AM	0	93	222	50	365	0	3	72	13	88	0	17	34	35	86	0	22	91	10	123	662	
Total Volume	0	322	771	225	1318	0	16	388	72	476	0	69	163	79	311	0	91	353	29	473	2578	
% App. Total	0	24.4	58.5	17.1		0	3.4	81.5	15.1		0	22.2	52.4	25.4		0	19.2	74.6	6.1			
PHF	.000	.866	.868	.879	.903	.000	.571	.822	.783	.832	.000	.663	.867	.564	.904	.000	.875	.959	.725	.931	.974	

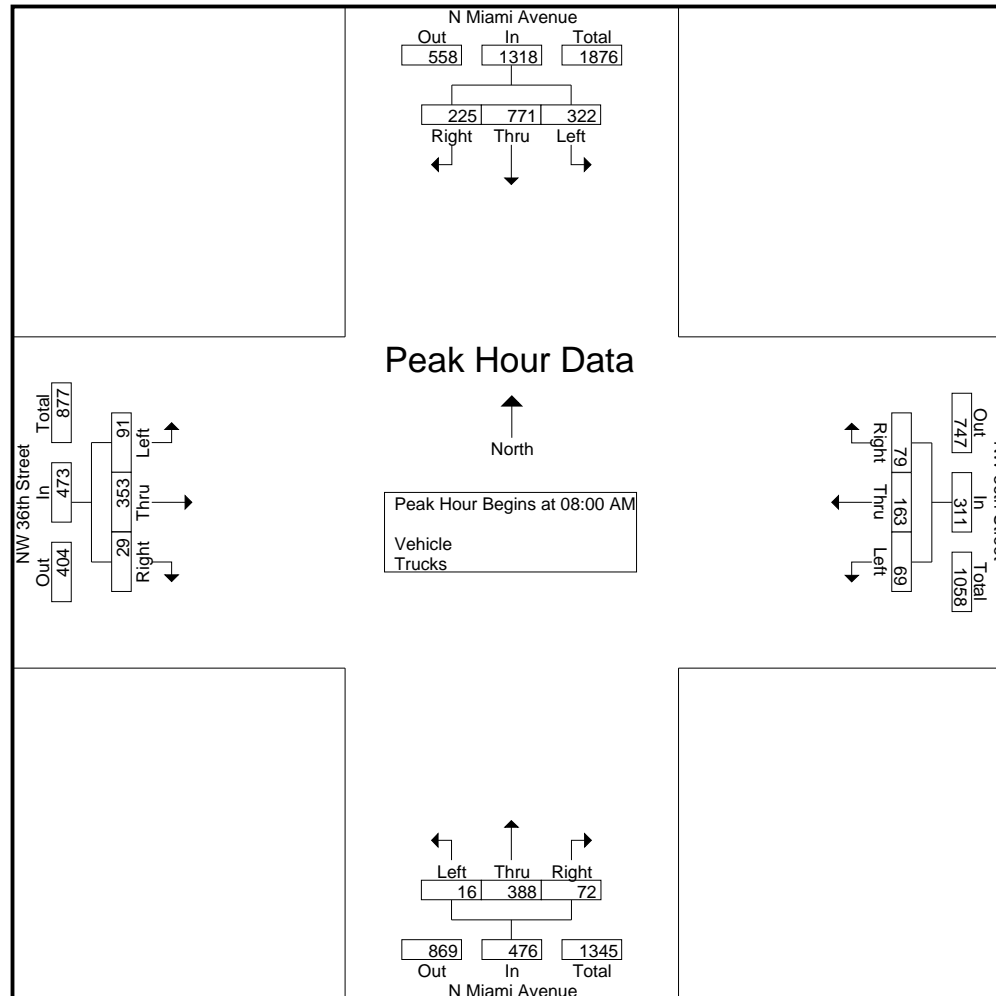
N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



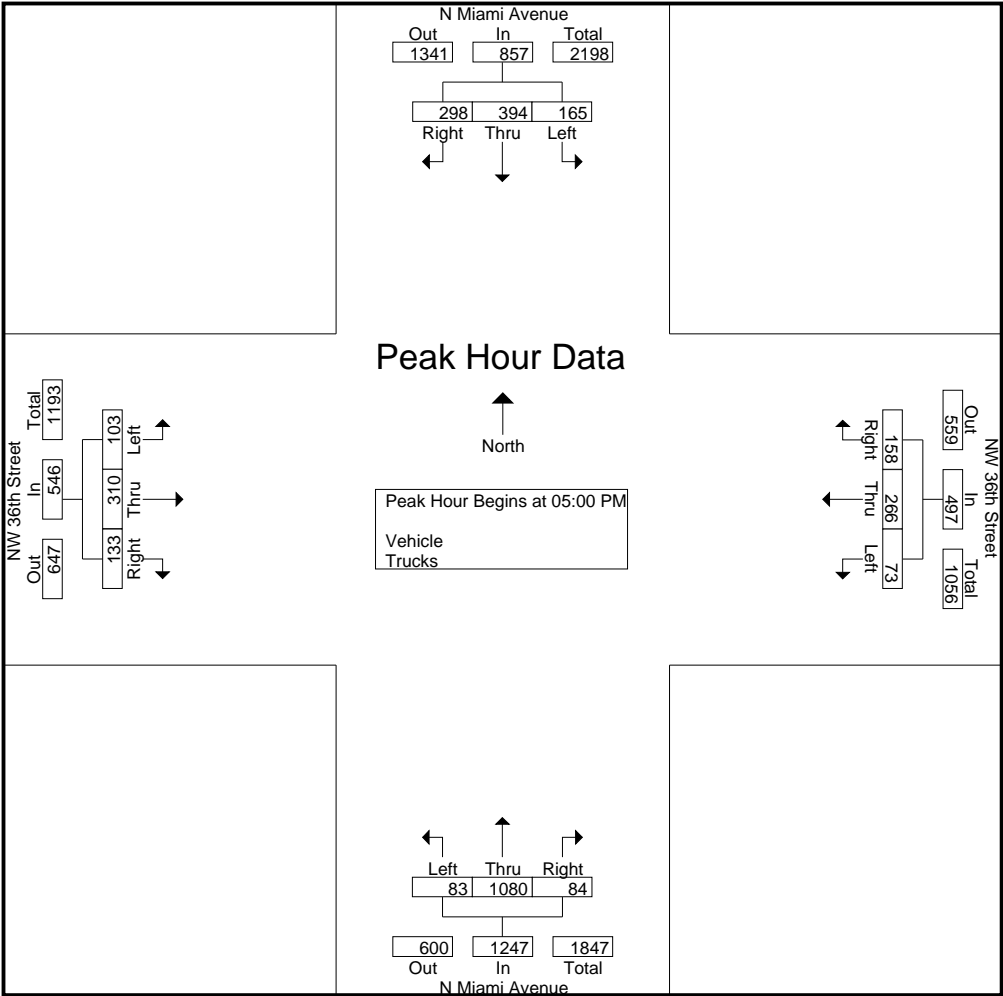
N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 6

Start Time	N Miami Avenue Southbound					N Miami Avenue Northbound					NW 36th Street Westbound					NW 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 05:00 PM																					
05:00 PM	0	39	95	75	209	0	29	267	11	307	0	13	67	45	125	0	29	85	43	157	798
05:15 PM	0	36	104	72	212	0	21	268	23	312	0	20	67	50	137	0	23	89	36	148	809
05:30 PM	0	44	82	65	191	0	13	290	21	324	0	14	80	33	127	0	29	77	28	134	776
05:45 PM	0	46	113	86	245	0	20	255	29	304	0	26	52	30	108	0	22	59	26	107	764
Total Volume	0	165	394	298	857	0	83	1080	84	1247	0	73	266	158	497	0	103	310	133	546	3147
% App. Total	0	19.3	46	34.8		0	6.7	86.6	6.7		0	14.7	53.5	31.8		0	18.9	56.8	24.4		
PHF	.000	.897	.872	.866	.874	.000	.716	.931	.724	.962	.000	.702	.831	.790	.907	.000	.888	.871	.773	.869	.972

N Miami Avenue & NE 36th Street

File Name : TMC-4 N Miami Avenue & NW 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 7



N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Peds & Bikes

	N Miami Avenue Southbound			N Miami Avenue Northbound			Westbound			SR 112 off Ramp Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	0	0	0	0	0	0	0	0	0	1	0	1	1
*** BREAK ***													
07:30 AM	0	0	0	0	0	0	0	0	0	2	1	3	3
07:45 AM	0	0	0	0	0	0	0	0	0	1	0	1	1
Total	0	0	0	0	0	0	0	0	0	4	1	5	5
08:00 AM	0	0	0	0	0	0	0	0	0	0	1	1	1
08:15 AM	0	0	0	0	0	0	0	0	0	1	2	3	3
08:30 AM	0	0	0	0	0	0	0	0	0	3	0	3	3
08:45 AM	0	0	0	0	0	0	0	0	0	2	1	3	3
Total	0	0	0	0	0	0	0	0	0	6	4	10	10
09:00 AM	0	0	0	0	0	0	0	0	0	1	0	1	1
*** BREAK ***													
Total	0	0	0	0	0	0	0	0	0	1	0	1	1
*** BREAK ***													
03:15 PM	0	0	0	0	0	0	0	0	0	0	2	2	2
03:30 PM	0	0	0	0	0	0	0	0	0	3	2	5	5
*** BREAK ***													
Total	0	0	0	0	0	0	0	0	0	3	4	7	7
04:00 PM	0	0	0	0	0	0	0	0	0	2	0	2	2
04:15 PM	0	0	0	0	0	0	0	0	0	4	1	5	5
04:30 PM	0	0	0	0	0	0	0	0	0	2	0	2	2
04:45 PM	0	0	0	0	0	0	0	0	0	2	0	2	2
Total	0	0	0	0	0	0	0	0	0	10	1	11	11
*** BREAK ***													
05:15 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
05:30 PM	0	0	0	0	0	0	0	0	0	2	1	3	3
05:45 PM	0	0	0	0	0	0	0	0	0	2	0	2	2
Total	0	0	0	0	0	0	0	0	0	5	1	6	6

N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2

Groups Printed- Peds & Bikes

	N Miami Avenue Southbound			N Miami Avenue Northbound			Westbound			SR 112 off Ramp Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Grand Total	0	0	0	0	0	0	0	0	0	29	11	40	40
Apprch %	0	0		0	0		0	0		72.5	27.5		
Total %	0	0	0	0	0	0	0	0	0	72.5	27.5	100	

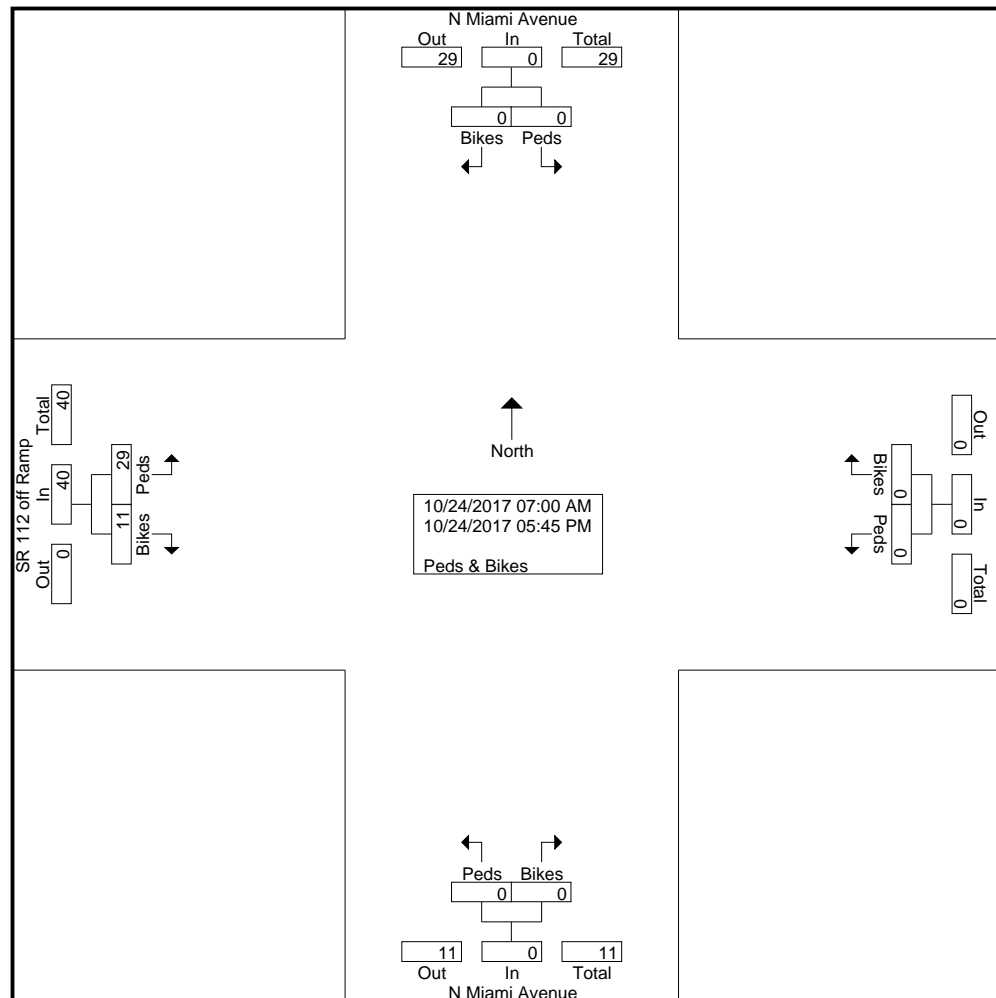
N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4

Start Time	N Miami Avenue Southbound			N Miami Avenue Northbound			Westbound			SR 112 off Ramp Eastbound			Int. Total	
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1														
Peak Hour for Entire Intersection Begins at 08:00 AM														
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	1	1	1
08:15 AM	0	0	0	0	0	0	0	0	0	0	1	2	3	3
08:30 AM	0	0	0	0	0	0	0	0	0	0	3	0	3	3
08:45 AM	0	0	0	0	0	0	0	0	0	0	2	1	3	3
Total Volume	0	0	0	0	0	0	0	0	0	0	6	4	10	10
% App. Total	0	0	0	0	0	0	0	0	0	0	60	40		
PHF	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.500	.500	.833	.833

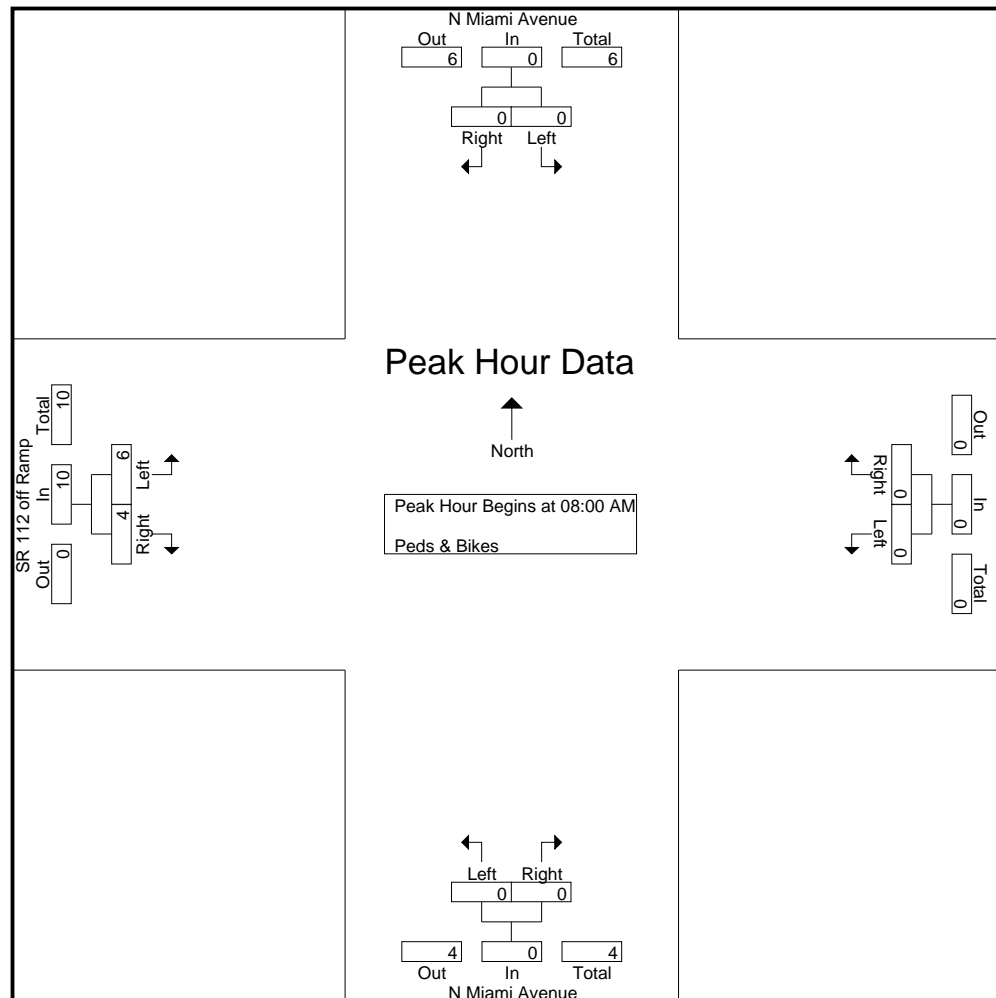
N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 6

Start Time	N Miami Avenue Southbound			N Miami Avenue Northbound			Westbound			SR 112 off Ramp Eastbound			Int. Total	
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total		
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1														
Peak Hour for Entire Intersection Begins at 03:30 PM														
03:30 PM	0	0	0	0	0	0	0	0	0	0	3	2	5	5
03:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	0	0	0	0	0	0	0	0	0	2	0	2	2
04:15 PM	0	0	0	0	0	0	0	0	0	0	4	1	5	5
Total Volume	0	0	0	0	0	0	0	0	0	0	9	3	12	12
% App. Total	0	0	0	0	0	0	0	0	0	0	75	25		
PHF	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.563	.375	.600	.600

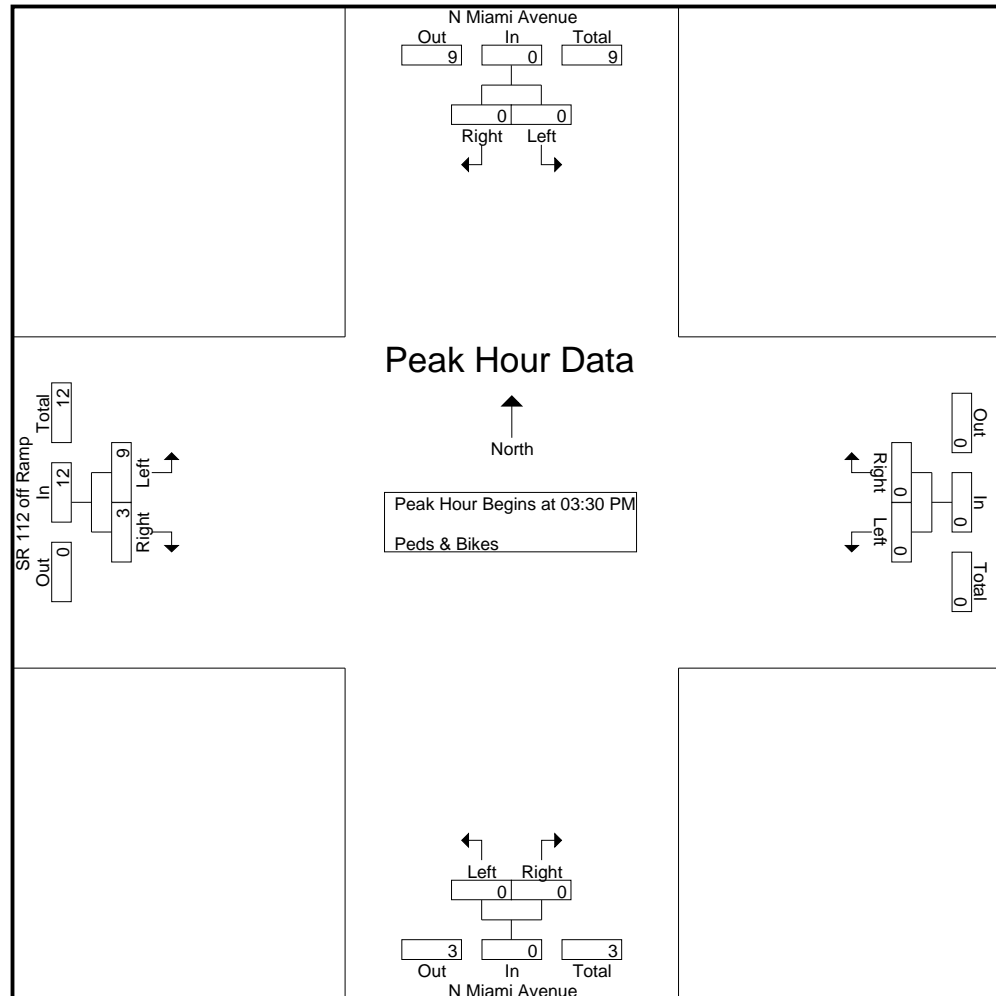
N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Trucks

Start Time	N Miami Avenue Southbound					N Miami Avenue Northbound					Westbound					SR 112 off Ramp Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	4	0	4	0	0	5	0	5	0	0	0	0	0	0	1	0	5	6	15
07:15 AM	0	0	6	0	6	0	0	6	0	6	0	0	0	0	0	0	1	0	7	8	20
07:30 AM	0	0	2	0	2	0	0	10	0	10	0	0	0	0	0	0	4	0	0	4	16
07:45 AM	0	0	2	0	2	0	0	3	0	3	0	0	0	0	0	0	1	0	5	6	11
Total	0	0	14	0	14	0	0	24	0	24	0	0	0	0	0	0	7	0	17	24	62
08:00 AM	0	0	5	0	5	0	0	4	0	4	0	0	0	0	0	0	0	0	6	6	15
08:15 AM	0	0	4	0	4	0	0	2	0	2	0	0	0	0	0	0	1	0	3	4	10
08:30 AM	0	0	4	0	4	0	0	7	0	7	0	0	0	0	0	0	2	0	7	9	20
08:45 AM	0	0	8	0	8	0	0	5	0	5	0	0	0	0	0	0	3	0	4	7	20
Total	0	0	21	0	21	0	0	18	0	18	0	0	0	0	0	0	6	0	20	26	65
*** BREAK ***																					
03:00 PM	0	0	2	0	2	0	0	8	0	8	0	0	0	0	0	0	3	0	3	6	16
03:15 PM	0	0	4	0	4	0	0	9	0	9	0	0	0	0	0	0	2	0	3	5	18
03:30 PM	0	0	2	0	2	0	0	4	0	4	0	0	0	0	0	0	2	0	2	4	10
03:45 PM	0	0	2	0	2	0	0	6	0	6	0	0	0	0	0	0	4	0	5	9	17
Total	0	0	10	0	10	0	0	27	0	27	0	0	0	0	0	0	11	0	13	24	61
04:00 PM	0	0	1	0	1	0	0	11	0	11	0	0	0	0	0	0	2	0	4	6	18
04:15 PM	0	0	2	0	2	0	0	7	0	7	0	0	0	0	0	0	1	0	2	3	12
04:30 PM	0	0	2	0	2	0	0	6	0	6	0	0	0	0	0	0	1	0	1	2	10
04:45 PM	0	0	3	0	3	0	0	6	0	6	0	0	0	0	0	0	2	0	1	3	12
Total	0	0	8	0	8	0	0	30	0	30	0	0	0	0	0	0	6	0	8	14	52
05:00 PM	0	0	3	0	3	0	0	3	0	3	0	0	0	0	0	0	2	0	3	5	11
05:15 PM	0	0	0	0	0	0	0	10	0	10	0	0	0	0	0	0	1	0	1	2	12
05:30 PM	0	0	2	0	2	0	0	6	0	6	0	0	0	0	0	0	0	0	2	2	10
05:45 PM	0	0	1	0	1	0	0	2	0	2	0	0	0	0	0	0	2	0	0	2	5
Total	0	0	6	0	6	0	0	21	0	21	0	0	0	0	0	0	5	0	6	11	38
Grand Total	0	0	59	0	59	0	0	120	0	120	0	0	0	0	0	0	35	0	64	99	278
Apprch %	0	0	100	0		0	0	100	0		0	0	0	0		0	35.4	0	64.6		
Total %	0	0	21.2	0	21.2	0	0	43.2	0	43.2	0	0	0	0	0	0	12.6	0	23	35.6	

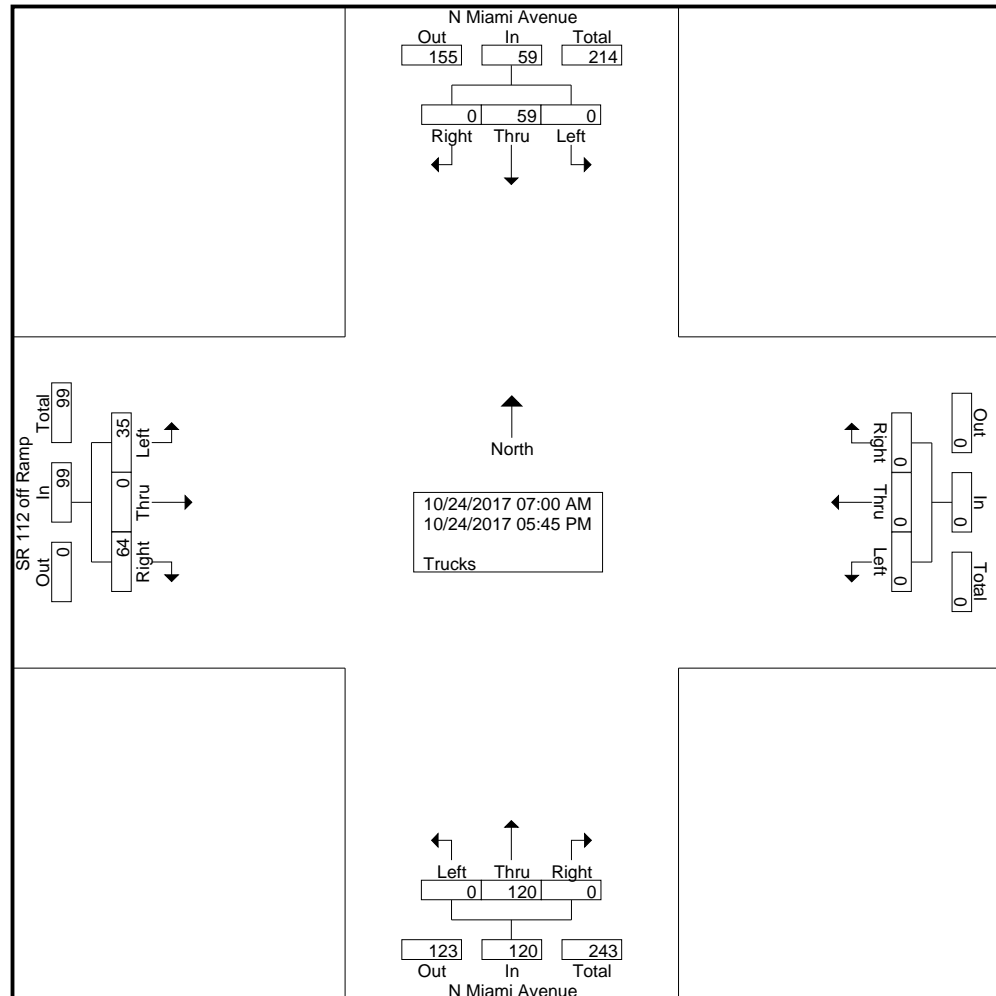
N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	N Miami Avenue Southbound					N Miami Avenue Northbound					Westbound					SR 112 off Ramp Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	0	5	0	5	0	0	4	0	4	0	0	0	0	0	0	0	0	6	6	15	
08:15 AM	0	0	4	0	4	0	0	2	0	2	0	0	0	0	0	0	1	0	3	4	10	
08:30 AM	0	0	4	0	4	0	0	7	0	7	0	0	0	0	0	0	2	0	7	9	20	
08:45 AM	0	0	8	0	8	0	0	5	0	5	0	0	0	0	0	0	3	0	4	7	20	
Total Volume	0	0	21	0	21	0	0	18	0	18	0	0	0	0	0	0	6	0	20	26	65	
% App. Total	0	0	100	0		0	0	100	0		0	0	0	0		0	23.1	0	76.9			
PHF	.000	.000	.656	.000	.656	.000	.000	.643	.000	.643	.000	.000	.000	.000	.000	.000	.500	.000	.714	.722	.813	

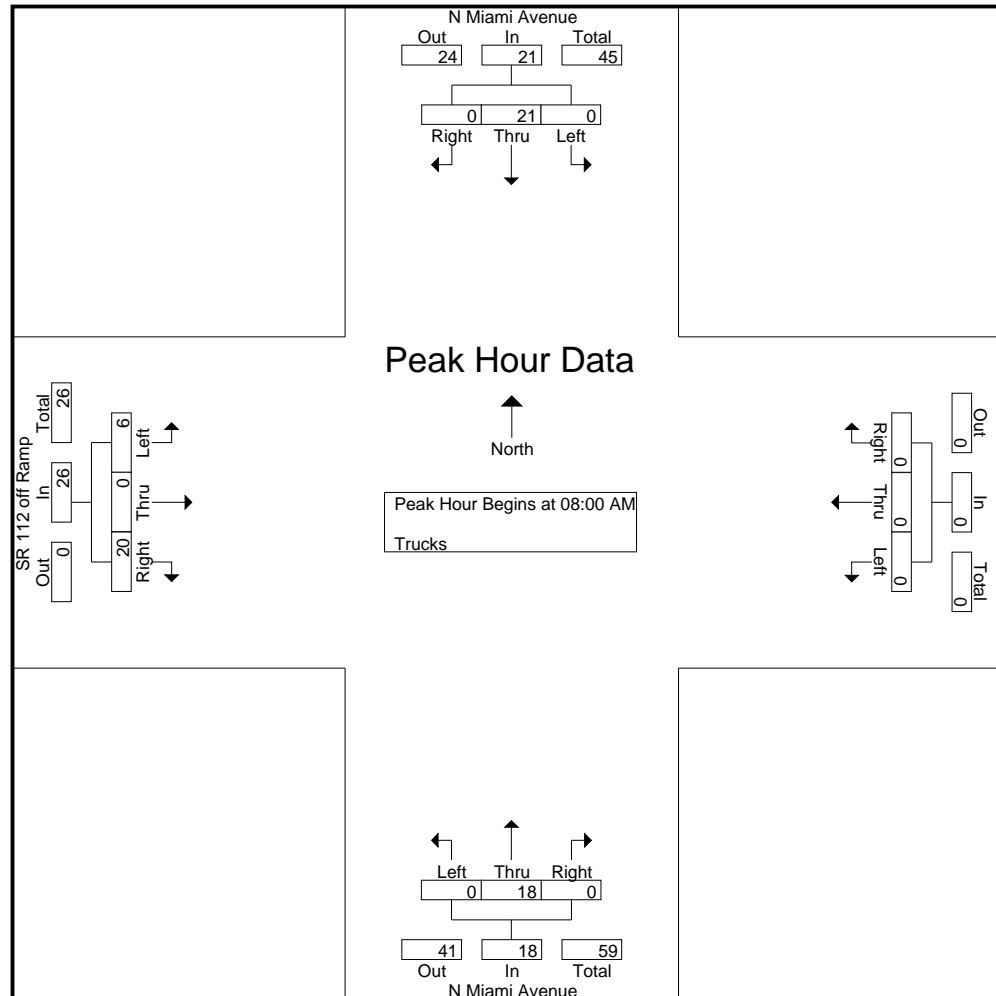
N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	N Miami Avenue Southbound					N Miami Avenue Northbound					Westbound					SR 112 off Ramp Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:15 PM																					
03:15 PM	0	0	4	0	4	0	0	9	0	9	0	0	0	0	0	0	2	0	3	5	18
03:30 PM	0	0	2	0	2	0	0	4	0	4	0	0	0	0	0	0	2	0	2	4	10
03:45 PM	0	0	2	0	2	0	0	6	0	6	0	0	0	0	0	0	4	0	5	9	17
04:00 PM	0	0	1	0	1	0	0	11	0	11	0	0	0	0	0	0	2	0	4	6	18
Total Volume	0	0	9	0	9	0	0	30	0	30	0	0	0	0	0	0	10	0	14	24	63
% App. Total	0	0	100	0		0	0	100	0		0	0	0	0		0	41.7	0	58.3		
PHF	.000	.000	.563	.000	.563	.000	.000	.682	.000	.682	.000	.000	.000	.000	.000	.000	.625	.000	.700	.667	.875

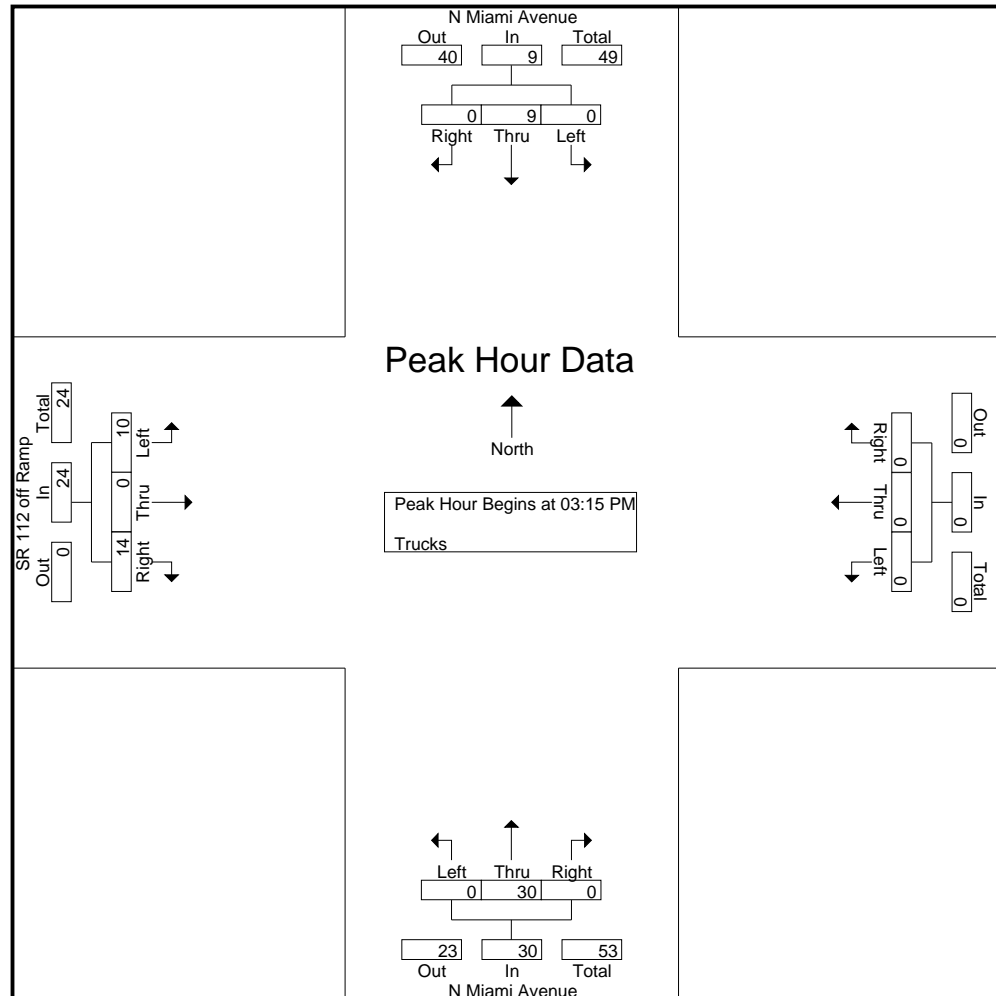
N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	N Miami Avenue Southbound					N Miami Avenue Northbound					Westbound					SR 112 off Ramp Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	143	0	143	0	0	120	0	120	0	0	0	0	0	0	89	0	120	209	472
07:15 AM	0	0	125	0	125	0	0	119	0	119	0	0	0	0	0	0	101	0	141	242	486
07:30 AM	0	0	192	0	192	0	0	133	0	133	0	0	0	0	0	0	109	0	143	252	577
07:45 AM	0	0	195	0	195	0	0	157	0	157	0	0	0	0	0	0	91	0	171	262	614
Total	0	0	655	0	655	0	0	529	0	529	0	0	0	0	0	0	390	0	575	965	2149
08:00 AM	0	0	155	0	155	0	0	166	0	166	0	0	0	0	0	0	68	0	158	226	547
08:15 AM	0	0	187	0	187	0	0	152	0	152	0	0	0	0	0	0	105	0	138	243	582
08:30 AM	0	0	185	0	185	0	0	136	0	136	0	0	0	0	0	0	93	0	131	224	545
08:45 AM	0	0	193	0	193	0	0	129	0	129	0	0	0	0	0	0	110	0	159	269	591
Total	0	0	720	0	720	0	0	583	0	583	0	0	0	0	0	0	376	0	586	962	2265
*** BREAK ***																					
03:00 PM	0	0	72	0	72	0	0	298	0	298	0	0	0	0	0	0	89	0	157	246	616
03:15 PM	0	0	68	0	68	0	0	256	0	256	0	0	0	0	0	0	104	0	154	258	582
03:30 PM	0	0	65	0	65	0	0	255	0	255	0	0	0	0	0	0	102	0	161	263	583
03:45 PM	0	0	73	0	73	0	0	273	0	273	0	0	0	0	0	0	104	0	141	245	591
Total	0	0	278	0	278	0	0	1082	0	1082	0	0	0	0	0	0	399	0	613	1012	2372
04:00 PM	0	0	68	0	68	0	0	299	0	299	0	0	0	0	0	0	93	0	119	212	579
04:15 PM	0	0	77	0	77	0	0	298	0	298	0	0	0	0	0	0	102	0	135	237	612
04:30 PM	0	0	78	0	78	0	0	361	0	361	0	0	0	0	0	0	118	0	123	241	680
04:45 PM	0	0	70	0	70	0	0	324	0	324	0	0	0	0	0	0	113	0	137	250	644
Total	0	0	293	0	293	0	0	1282	0	1282	0	0	0	0	0	0	426	0	514	940	2515
05:00 PM	0	0	65	0	65	0	0	345	0	345	0	0	0	0	0	0	155	0	138	293	703
05:15 PM	0	0	59	0	59	0	0	340	0	340	0	0	0	0	0	0	130	0	164	294	693
05:30 PM	0	0	62	0	62	0	0	361	0	361	0	0	0	0	0	0	151	0	156	307	730
05:45 PM	0	0	73	0	73	0	0	304	0	304	0	0	0	0	0	0	139	0	173	312	689
Total	0	0	259	0	259	0	0	1350	0	1350	0	0	0	0	0	0	575	0	631	1206	2815
Grand Total	0	0	2205	0	2205	0	0	4826	0	4826	0	0	0	0	0	0	2166	0	2919	5085	12116
Apprch %	0	0	100	0		0	0	100	0		0	0	0	0		0	42.6	0	57.4		
Total %	0	0	18.2	0	18.2	0	0	39.8	0	39.8	0	0	0	0	0	0	17.9	0	24.1	42	
Vehicle	0	0	2146	0	2146	0	0	4706	0	4706	0	0	0	0	0	0	2131	0	2855	4986	11838
% Vehicle	0	0	97.3	0	97.3	0	0	97.5	0	97.5	0	0	0	0	0	0	98.4	0	97.8	98.1	97.7

N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	N Miami Avenue Southbound					N Miami Avenue Northbound					Westbound					SR 112 off Ramp Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	0	59	0	59	0	0	120	0	120	0	0	0	0	0	0	35	0	64	99	278
% Trucks	0	0	2.7	0	2.7	0	0	2.5	0	2.5	0	0	0	0	0	0	1.6	0	2.2	1.9	2.3

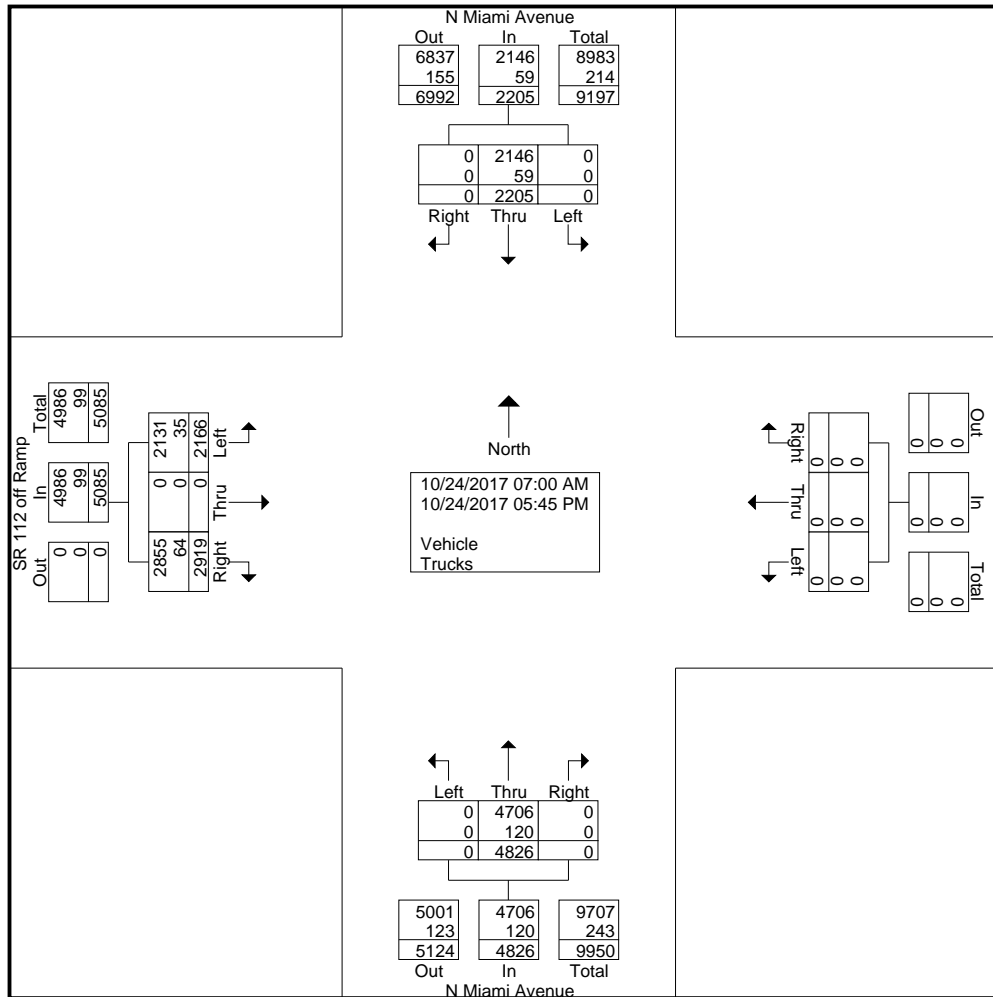
N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4

Start Time	N Miami Avenue Southbound					N Miami Avenue Northbound					Westbound					SR 112 off Ramp Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 07:30 AM																						
07:30 AM	0	0	192	0	192	0	0	133	0	133	0	0	0	0	0	0	109	0	143	252	577	
07:45 AM	0	0	195	0	195	0	0	157	0	157	0	0	0	0	0	0	91	0	171	262	614	
08:00 AM	0	0	155	0	155	0	0	166	0	166	0	0	0	0	0	0	68	0	158	226	547	
08:15 AM	0	0	187	0	187	0	0	152	0	152	0	0	0	0	0	0	105	0	138	243	582	
Total Volume	0	0	729	0	729	0	0	608	0	608	0	0	0	0	0	0	373	0	610	983	2320	
% App. Total	0	0	100	0		0	0	100	0		0	0	0	0		0	37.9	0	62.1			
PHF	.000	.000	.935	.000	.935	.000	.000	.916	.000	.916	.000	.000	.000	.000	.000	.000	.856	.000	.892	.938	.945	

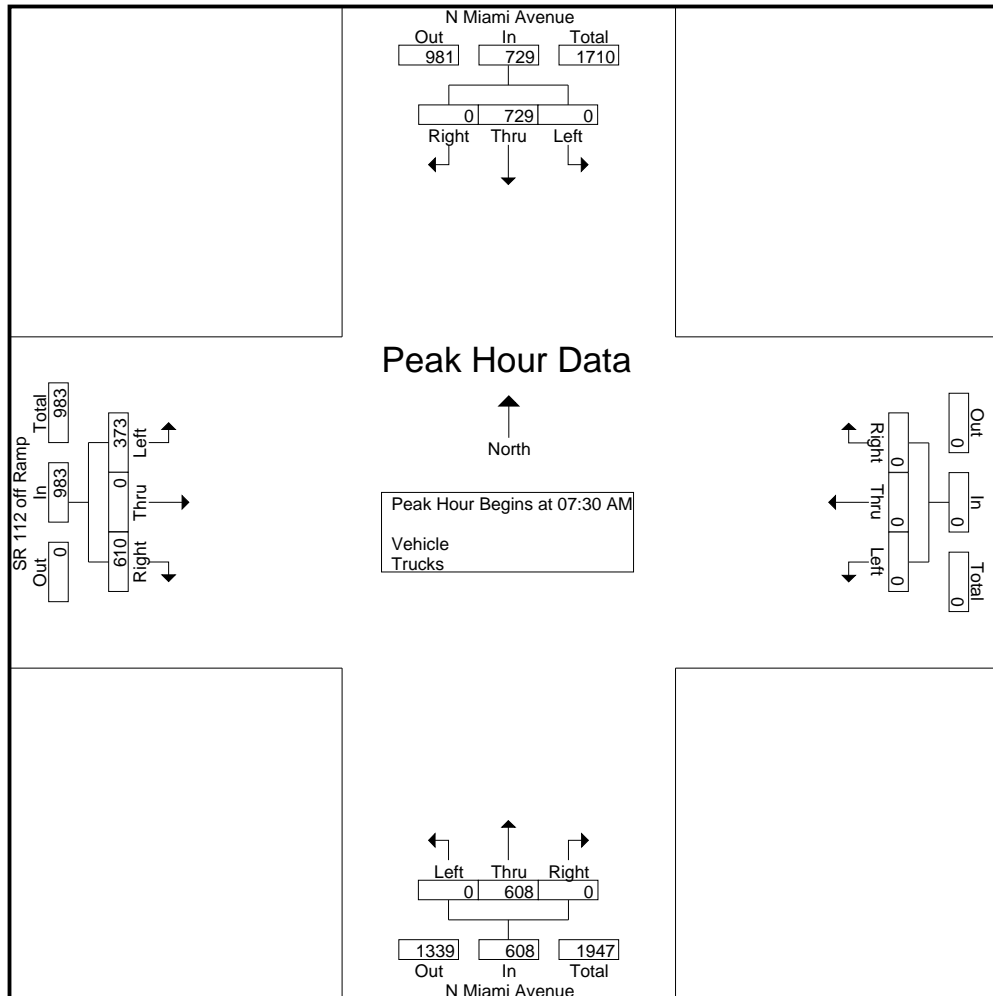
N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 6

Start Time	N Miami Avenue Southbound					N Miami Avenue Northbound					Westbound					SR 112 off Ramp Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 05:00 PM																					
05:00 PM	0	0	65	0	65	0	0	345	0	345	0	0	0	0	0	0	155	0	138	293	703
05:15 PM	0	0	59	0	59	0	0	340	0	340	0	0	0	0	0	0	130	0	164	294	693
05:30 PM	0	0	62	0	62	0	0	361	0	361	0	0	0	0	0	0	151	0	156	307	730
05:45 PM	0	0	73	0	73	0	0	304	0	304	0	0	0	0	0	0	139	0	173	312	689
Total Volume	0	0	259	0	259	0	0	1350	0	1350	0	0	0	0	0	0	575	0	631	1206	2815
% App. Total	0	0	100	0		0	0	100	0		0	0	0	0		0	47.7	0	52.3		
PHF	.000	.000	.887	.000	.887	.000	.000	.935	.000	.935	.000	.000	.000	.000	.000	.000	.927	.000	.912	.966	.964

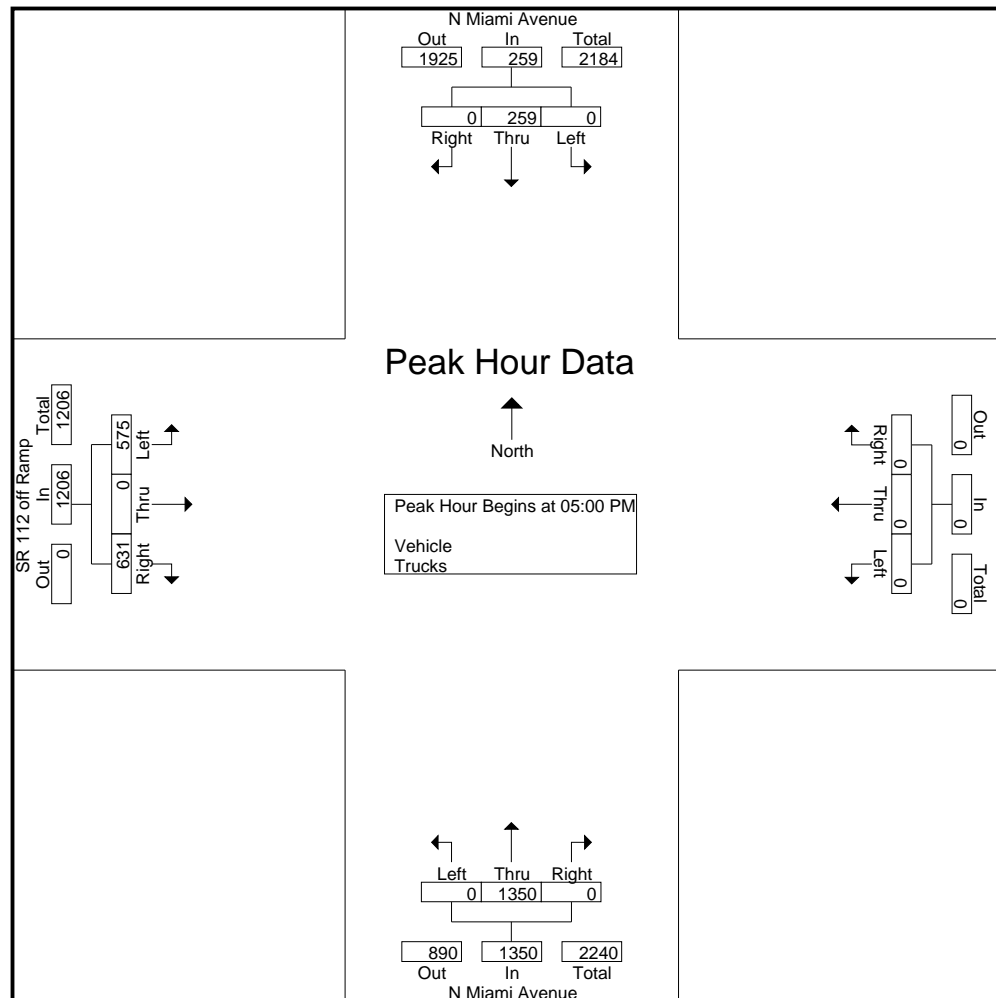
N Miami Avenue & SR 112 EB off Ramp

File Name : TMC-5 N Miami Avenue & SR 112 EB off Ramp

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 1

Groups Printed- Peds & Bikes

Start Time	N Miami Avenue Southbound			N Miami Avenue Northbound			NE 38th Street Westbound			Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	0	0	0	0	0	0	1	0	1	1	0	1	2
07:15 AM	1	0	1	0	0	0	1	0	1	0	0	0	2
07:30 AM	0	0	0	0	0	0	2	0	2	1	1	2	4
07:45 AM	0	0	0	0	0	0	3	0	3	1	0	1	4
Total	1	0	1	0	0	0	7	0	7	3	1	4	12
08:00 AM	0	0	0	0	0	0	0	0	0	0	1	1	1
08:15 AM	0	0	0	0	0	0	0	0	0	1	1	2	2
08:30 AM	1	0	1	0	0	0	0	2	2	2	0	2	5
08:45 AM	0	0	0	0	0	0	0	1	1	1	1	2	3
Total	1	0	1	0	0	0	0	3	3	4	3	7	11
*** BREAK ***													
03:00 PM	0	0	0	1	0	1	1	0	1	0	0	0	2
03:15 PM	0	0	0	0	0	0	5	0	5	0	2	2	7
03:30 PM	0	0	0	0	0	0	3	1	4	2	1	3	7
03:45 PM	0	0	0	0	0	0	0	1	1	1	0	1	2
Total	0	0	0	1	0	1	9	2	11	3	3	6	18
04:00 PM	1	0	1	1	0	1	0	0	0	3	0	3	5
04:15 PM	0	0	0	2	0	2	0	0	0	2	1	3	5
04:30 PM	1	0	1	0	0	0	2	0	2	1	0	1	4
04:45 PM	0	0	0	0	0	0	2	0	2	1	0	1	3
Total	2	0	2	3	0	3	4	0	4	7	1	8	17
*** BREAK ***													
05:15 PM	0	0	0	0	0	0	2	0	2	1	1	2	4
05:30 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
05:45 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
Total	0	0	0	0	0	0	2	0	2	3	1	4	6
Grand Total	4	0	4	4	0	4	22	5	27	20	9	29	64
Apprch %	100	0		100	0		81.5	18.5		69	31		
Total %	6.2	0	6.2	6.2	0	6.2	34.4	7.8	42.2	31.2	14.1	45.3	

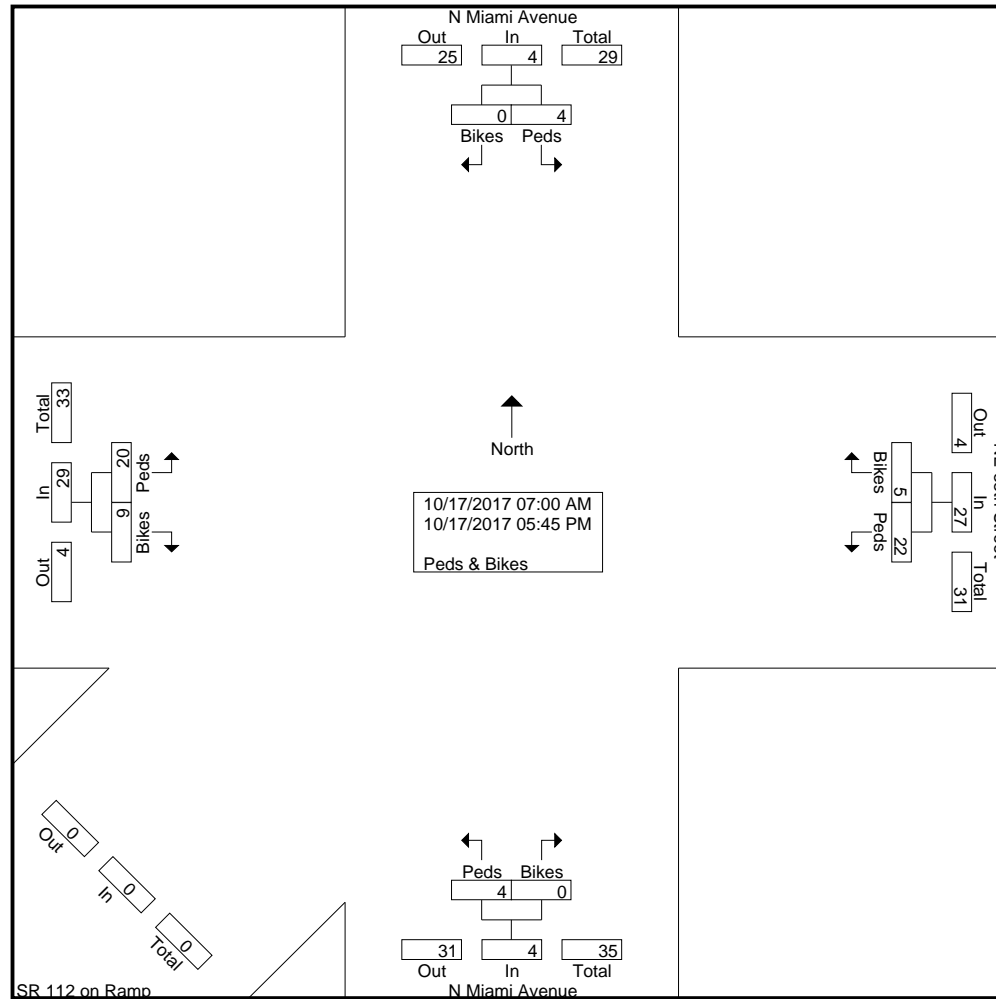
N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



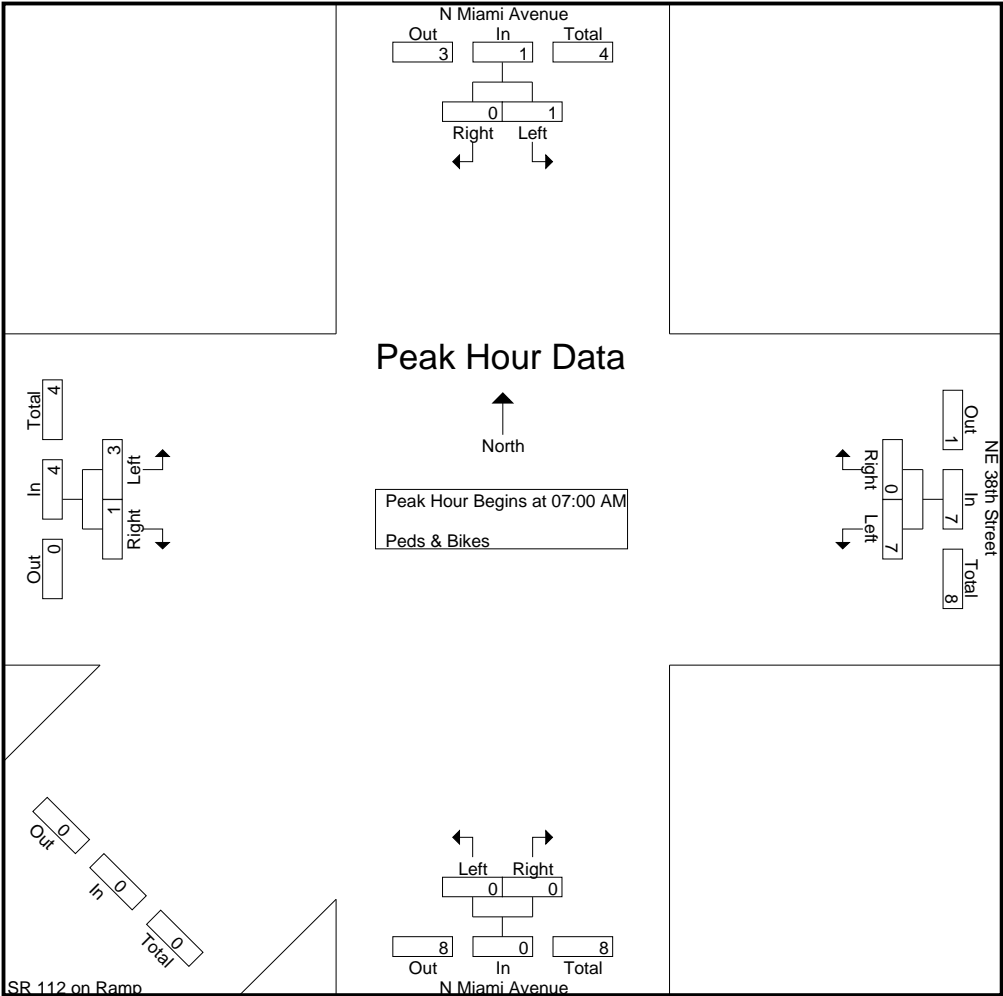
N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	N Miami Avenue Southbound			N Miami Avenue Northbound			NE 38th Street Westbound			Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:00 AM													
07:00 AM	0	0	0	0	0	0	1	0	1	1	0	1	2
07:15 AM	1	0	1	0	0	0	1	0	1	0	0	0	2
07:30 AM	0	0	0	0	0	0	2	0	2	1	1	2	4
07:45 AM	0	0	0	0	0	0	3	0	3	1	0	1	4
Total Volume	1	0	1	0	0	0	7	0	7	3	1	4	12
% App. Total	100	0		0	0		100	0		75	25		
PHF	.250	.000	.250	.000	.000	.000	.583	.000	.583	.750	.250	.500	.750

N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4



N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street

Site Code : 00000000

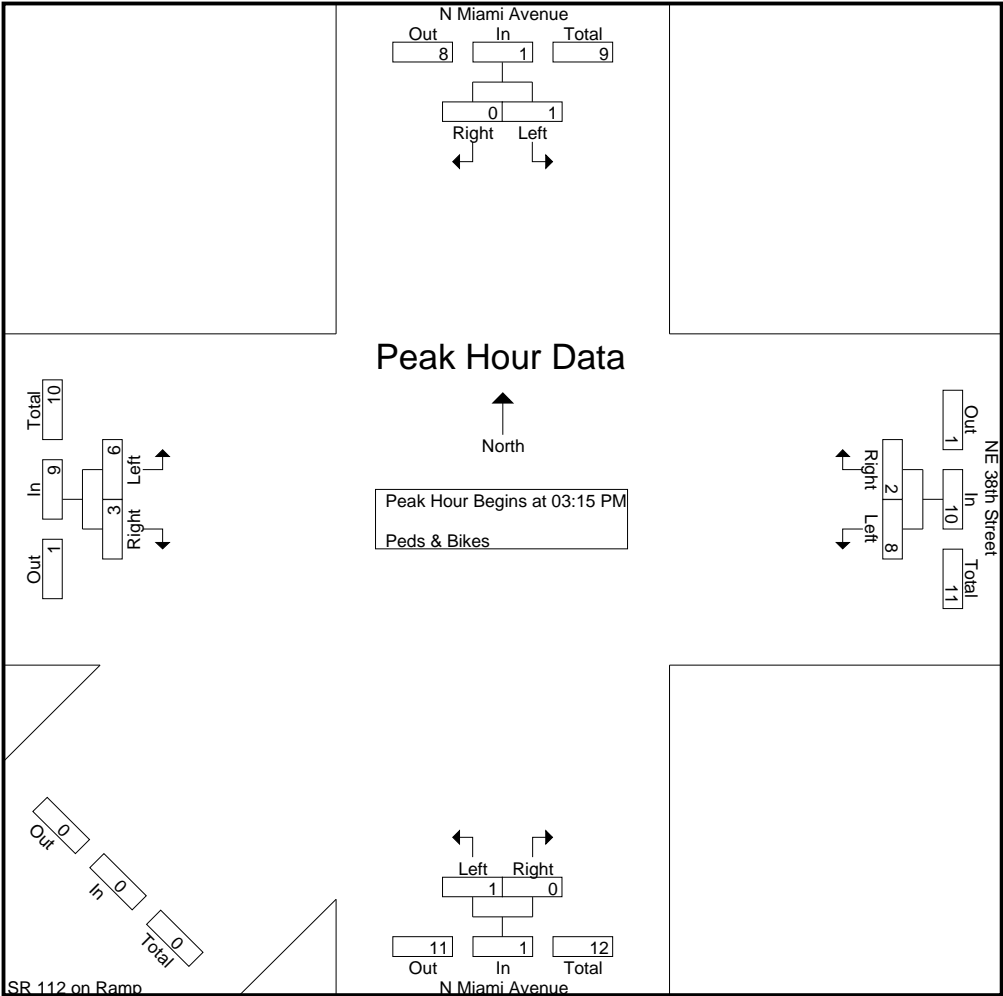
Start Date : 10/17/2017

Page No : 5

Start Time	N Miami Avenue Southbound			N Miami Avenue Northbound			NE 38th Street Westbound			Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 03:15 PM													
03:15 PM	0	0	0	0	0	0	5	0	5	0	2	2	7
03:30 PM	0	0	0	0	0	0	3	1	4	2	1	3	7
03:45 PM	0	0	0	0	0	0	0	1	1	1	0	1	2
04:00 PM	1	0	1	1	0	1	0	0	0	3	0	3	5
Total Volume	1	0	1	1	0	1	8	2	10	6	3	9	21
% App. Total	100	0		100	0		80	20		66.7	33.3		
PHF	.250	.000	.250	.250	.000	.250	.400	.500	.500	.500	.375	.750	.750

N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6



N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street

Site Code : 00000000

Start Date : 10/17/2017

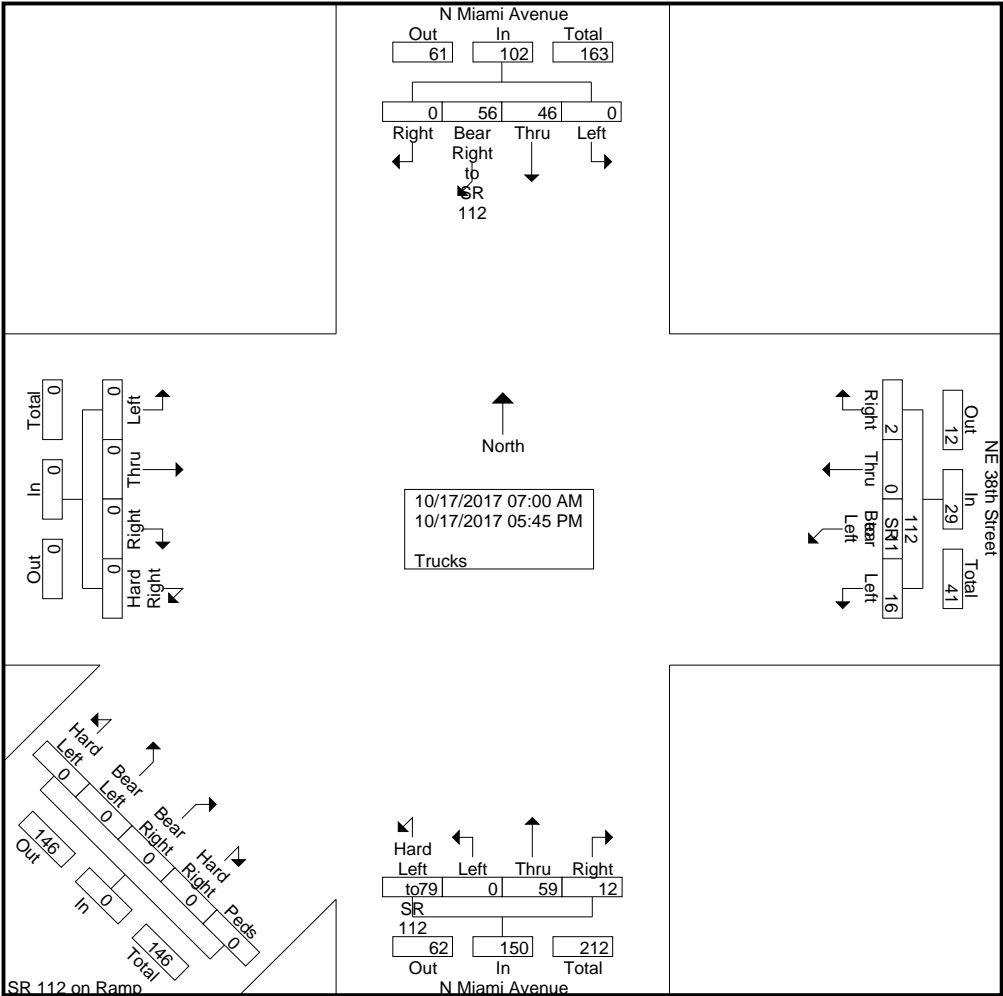
Page No : 1

Groups Printed- Trucks

Start Time	N Miami Avenue Southbound						N Miami Avenue Northbound						NE 38th Street Westbound						Eastbound						SR 112 on Ramp Northeast						Int. Total							
	U-Turns	Left	Thru	Bear Right to SR 112	Right	App. Total	Hard Left to SR 112	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Bear Left to SR 112	Thru	Right	App. Total	U-Turns	Left	Thru	Right	Hard Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	Peds	App. Total								
07:00 AM	0	0	4	4	0	8	2	0	0	3	0	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13
07:15 AM	0	0	2	4	0	6	6	0	0	4	0	10	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17
07:30 AM	0	0	1	8	0	9	2	0	0	5	2	9	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19
07:45 AM	0	0	3	3	0	6	9	0	0	2	0	11	0	2	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19
Total	0	0	10	19	0	29	19	0	0	14	2	35	0	3	1	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	68	
08:00 AM	0	0	2	7	0	9	3	0	0	4	0	7	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17
08:15 AM	0	0	3	4	0	7	3	0	0	1	0	4	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12
08:30 AM	0	0	2	2	0	4	3	0	0	4	0	7	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12
08:45 AM	0	0	4	3	0	7	2	0	0	1	0	3	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11
Total	0	0	11	16	0	27	11	0	0	10	0	21	0	4	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	52	
*** BREAK ***																																						
03:00 PM	0	0	4	5	0	9	8	0	0	2	1	11	0	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	22
03:15 PM	0	0	1	3	0	4	6	0	0	5	1	12	0	2	1	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19
03:30 PM	0	0	2	6	0	8	3	0	0	3	1	7	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16
03:45 PM	0	0	4	2	0	6	3	0	0	6	0	9	0	1	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17
Total	0	0	11	16	0	27	20	0	0	16	3	39	0	4	3	0	1	8	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	74	
04:00 PM	0	0	2	0	0	2	5	0	0	2	2	9	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12
04:15 PM	0	0	2	0	0	2	6	0	0	1	0	7	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10
04:30 PM	0	0	1	2	0	3	6	0	0	0	0	6	0	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11
04:45 PM	0	0	4	0	0	4	1	0	0	3	0	4	0	1	1	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11
Total	0	0	9	2	0	11	18	0	0	6	2	26	0	2	4	0	1	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	44	
05:00 PM	0	0	2	0	0	2	2	0	0	3	2	7	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10
05:15 PM	0	0	2	1	0	3	4	0	0	4	2	10	0	1	2	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16
05:30 PM	0	0	0	0	0	0	3	0	0	3	0	6	0	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8
05:45 PM	0	0	1	2	0	3	2	0	0	3	1	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9
Total	0	0	5	3	0	8	11	0	0	13	5	29	0	3	3	0	0	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	43	
Grand Total	0	0	46	56	0	102	79	0	0	59	12	150	0	16	11	0	2	29	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	281	
Apprch %	0	0	45.1	54.9	0		52.7	0	0	39.3	8		0	55.2	37.9	0	6.9		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
Total %	0	0	16.4	19.9	0	36.3	28.1	0	0	21	4.3	53.4	0	5.7	3.9	0	0.7	10.3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		

N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 2



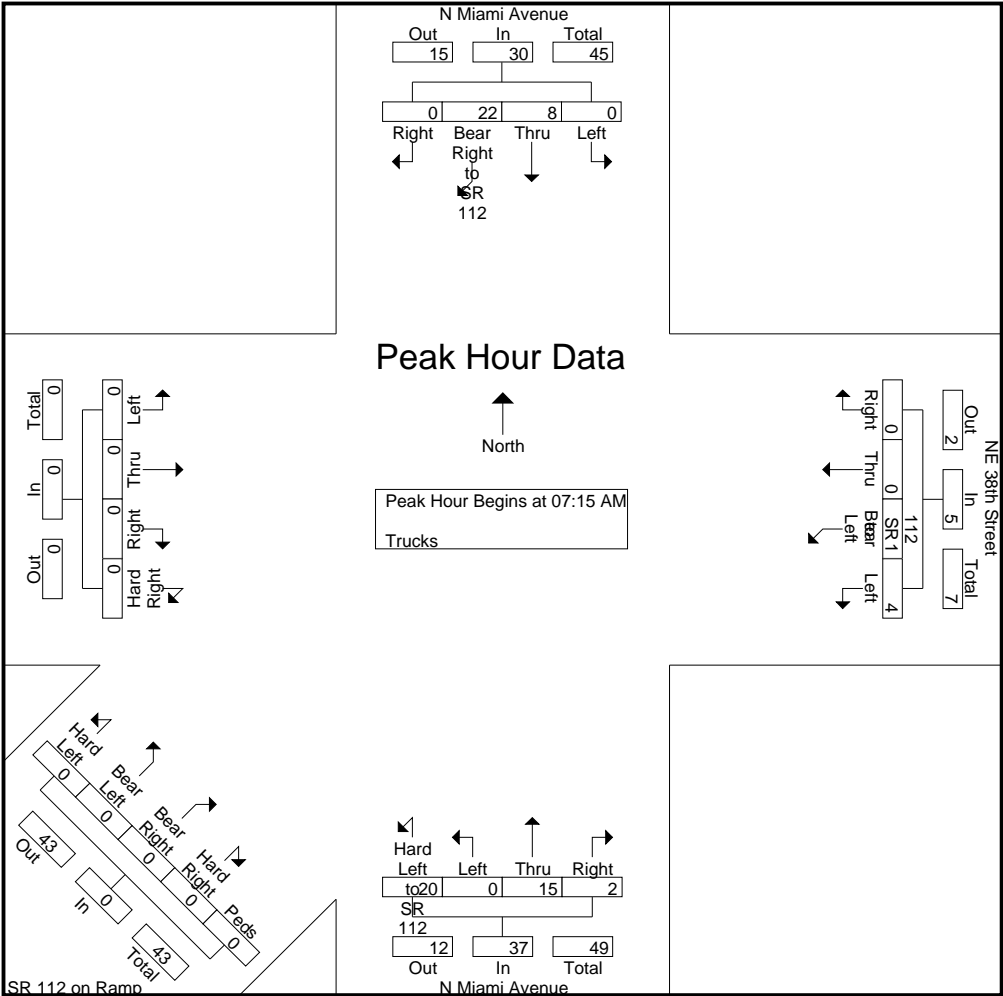
N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	N Miami Avenue Southbound						N Miami Avenue Northbound						NE 38th Street Westbound						Eastbound						SR 112 on Ramp Northeast						Int. Total						
	U-Turns	Left	Thru	Bear Right to SR.112	Right	App. Total	Hard Left to SR 112	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Bear Left to SR 112	Thru	Right	App. Total	U-Turns	Left	Thru	Right	Hard Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	Peds	App. Total							
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																																					
Peak Hour for Entire Intersection Begins at 07:15 AM																																					
07:15 AM	0	0	2	4	0	6	6	0	0	4	0	10	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17
07:30 AM	0	0	1	8	0	9	2	0	0	5	2	9	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19
07:45 AM	0	0	3	3	0	6	9	0	0	2	0	11	0	2	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19
08:00 AM	0	0	2	7	0	9	3	0	0	4	0	7	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17
Total Volume	0	0	8	22	0	30	20	0	0	15	2	37	0	4	1	0	0	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	72
% App. Total	0	0	26.7	73.3	0		54.1	0	0	40.5	5.4		0	80	20	0	0		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
PHF	.000	.000	.667	.688	.000	.833	.556	.000	.000	.750	.250	.841	.000	.500	.250	.000	.000	.625	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.947	

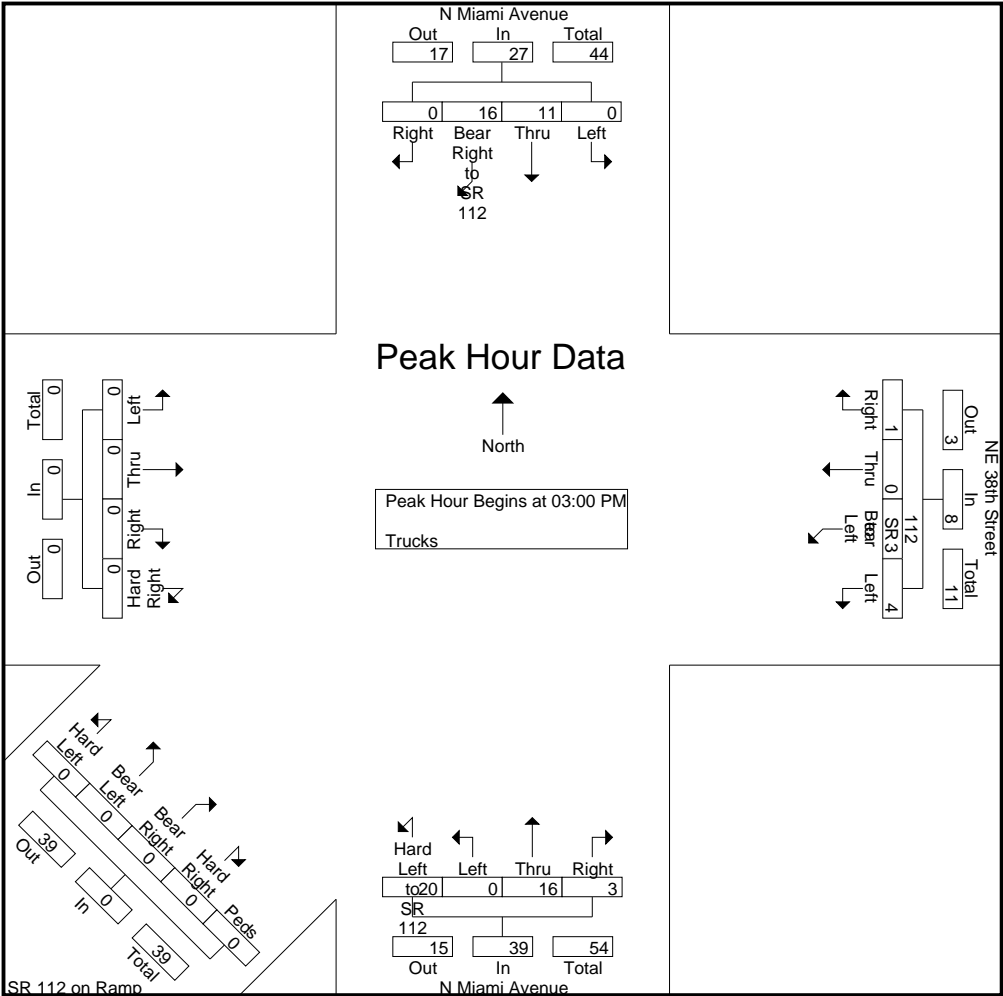
N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4



N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6



N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street

Site Code : 00000000

Start Date : 10/17/2017

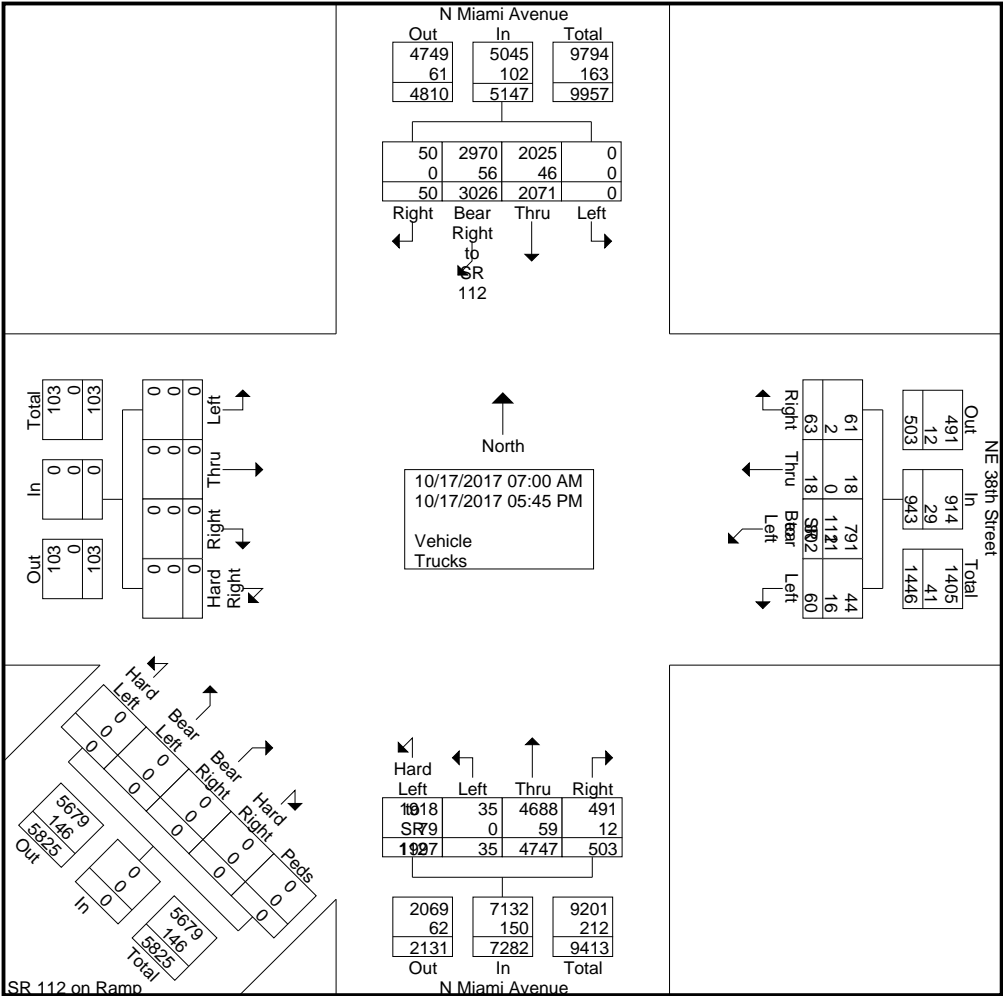
Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	N Miami Avenue Southbound						N Miami Avenue Northbound					NE 38th Street Westbound						Eastbound						SR 112 on Ramp Northeast						Int. Total							
	U-Turns	Left	Thru	Bear Right to SR 112	Right	App. Total	Hard Left to SR 112	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Bear Left to SR 112	Thru	Right	App. Total	U-Turns	Left	Thru	Right	Hard Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	Peds		App. Total						
07:00 AM	0	0	117	160	3	280	79	0	0	90	33	202	0	0	20	0	0	20	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	502
07:15 AM	0	0	126	181	2	309	67	0	3	104	50	224	0	3	29	1	0	33	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	566
07:30 AM	0	0	170	219	0	389	80	0	4	130	39	253	0	1	45	0	1	47	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	689
07:45 AM	0	0	165	214	3	382	83	0	1	138	23	245	0	3	26	0	0	29	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	656
Total	0	0	578	774	8	1360	309	0	8	462	145	924	0	7	120	1	1	129	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2413
08:00 AM	0	0	207	210	1	418	76	0	2	154	29	261	0	1	41	0	1	43	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	722
08:15 AM	0	0	172	236	3	411	80	0	1	143	36	260	0	1	27	1	2	31	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	702
08:30 AM	0	0	134	292	1	427	87	0	0	133	34	254	0	1	37	0	0	38	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	719
08:45 AM	0	0	166	262	3	431	79	0	2	125	58	264	0	5	31	0	1	37	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	732
Total	0	0	679	1000	8	1687	322	0	5	555	157	1039	0	8	136	1	4	149	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2875
*** BREAK ***																																					
03:00 PM	0	0	65	123	2	190	137	0	4	230	17	388	0	4	53	3	8	68	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	646
03:15 PM	0	0	64	135	2	201	97	0	1	230	19	347	0	6	43	1	2	52	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	600
03:30 PM	0	0	61	153	5	219	122	0	1	218	23	364	0	1	67	0	3	71	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	654
03:45 PM	0	0	75	139	0	214	122	0	0	270	15	407	0	3	49	0	7	59	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	680
Total	0	0	265	550	9	824	478	0	6	948	74	1506	0	14	212	4	20	250	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2580
04:00 PM	0	0	71	99	4	174	125	0	3	290	14	432	0	4	53	0	7	64	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	670
04:15 PM	0	0	69	85	0	154	122	0	3	306	10	441	0	4	47	1	4	56	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	651
04:30 PM	0	0	73	73	4	150	121	0	1	336	13	471	0	2	29	3	2	36	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	657
04:45 PM	0	0	69	79	1	149	103	0	2	344	15	464	0	3	29	0	5	37	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	650
Total	0	0	282	336	9	627	471	0	9	1276	52	1808	0	13	158	4	18	193	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2628
05:00 PM	0	0	65	107	6	178	103	0	1	350	17	471	0	3	45	3	3	54	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	703
05:15 PM	0	0	62	94	3	159	120	0	1	375	21	517	0	6	54	1	3	64	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	740
05:30 PM	0	0	72	87	2	161	99	0	2	385	16	502	0	5	43	1	11	60	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	723
05:45 PM	0	0	68	78	5	151	95	0	3	396	21	515	0	4	34	3	3	44	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	710
Total	0	0	267	366	16	649	417	0	7	1506	75	2005	0	18	176	8	20	222	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2876
Grand Total	0	0	2071	3026	50	5147	1997	0	35	4747	503	7282	0	60	802	18	63	943	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13372
Apprch %	0	0	40.2	58.8	1		27.4	0	0.5	65.2	6.9		0	6.4	85	1.9	6.7		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total %	0	0	15.5	22.6	0.4	38.5	14.9	0	0.3	35.5	3.8	54.5	0	0.4	6	0.1	0.5	7.1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Vehicle	0	0	2025	2970	50	5045	1918	0	35	4688	491	7132	0	44	791	18	61	914	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13091
% Vehicle	0	0	97.8	98.1	100	98	96	0	100	98.8	97.6	97.9	0	73.3	98.6	100	96.8	96.9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	97.9

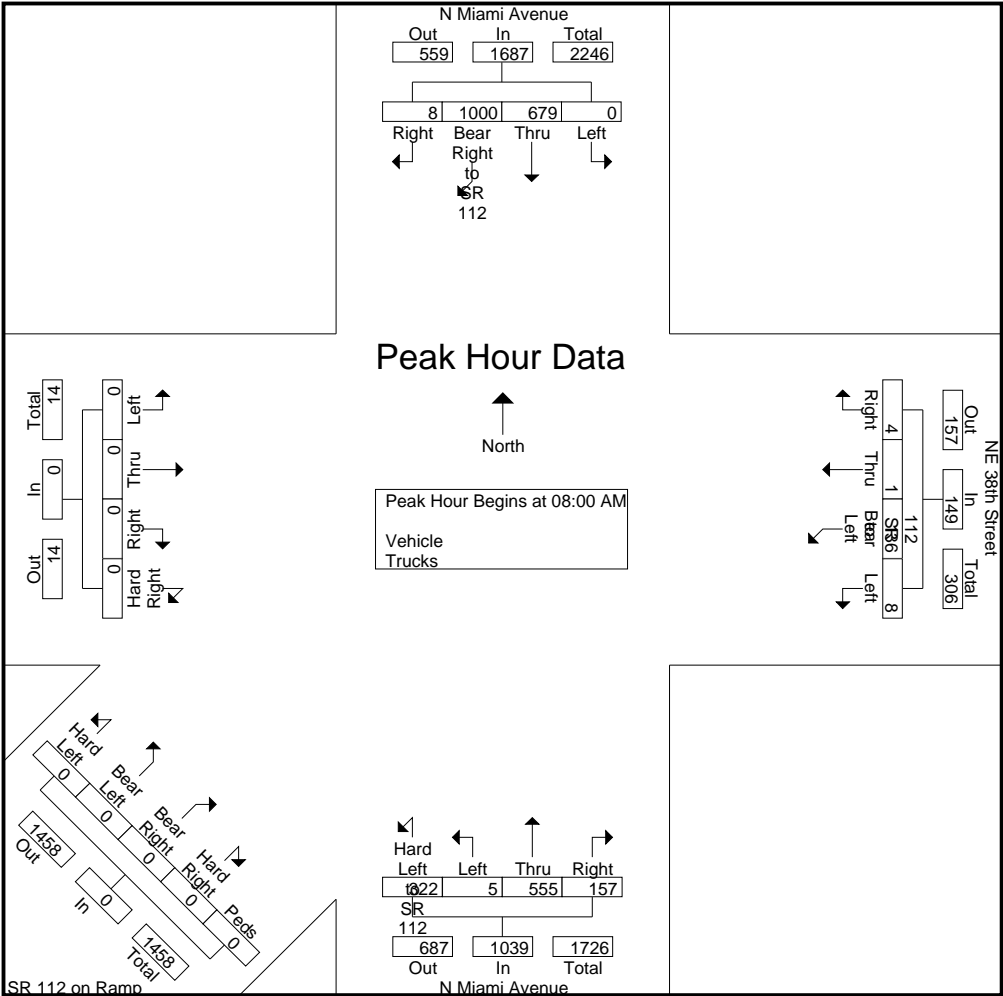
N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3



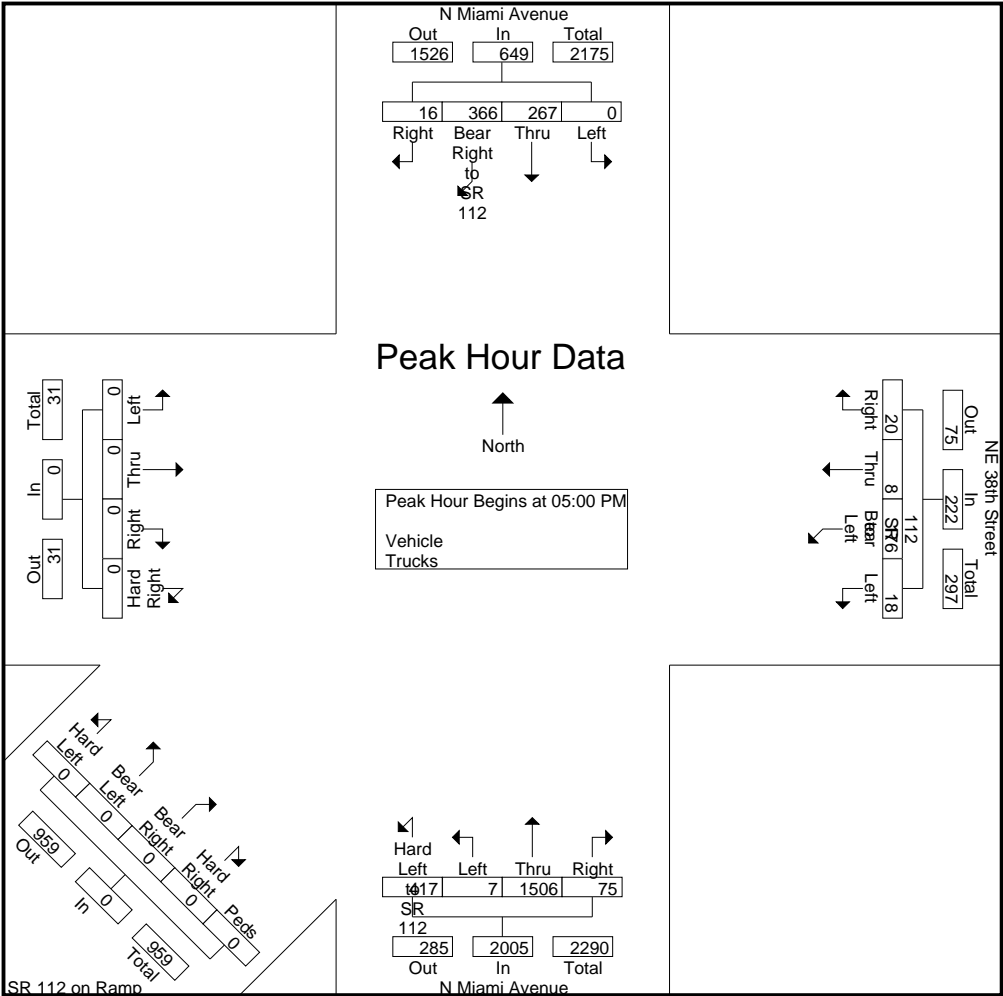
N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5



N Miami Avenue & NE 38th Street

File Name : TMC-6 N Miami Avenue & NE-NW 38th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 7



NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	NE 1st Avenue Southbound			NE 1st Avenue Northbound			NE 36th Street Westbound			NE 36th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	0	0	0	0	0	0	0	0	0	0	2	2	2
07:15 AM	3	0	3	1	1	2	0	0	0	2	0	2	7
07:30 AM	0	0	0	2	0	2	1	0	1	0	0	0	3
07:45 AM	3	3	6	3	0	3	0	0	0	3	0	3	12
Total	6	3	9	6	1	7	1	0	1	5	2	7	24
08:00 AM	3	1	4	1	1	2	0	1	1	5	1	6	13
08:15 AM	1	0	1	0	0	0	2	0	2	6	0	6	9
08:30 AM	2	1	3	1	0	1	1	0	1	6	0	6	11
08:45 AM	1	1	2	1	0	1	1	0	1	6	0	6	10
Total	7	3	10	3	1	4	4	1	5	23	1	24	43
*** BREAK ***													
03:00 PM	3	0	3	11	0	11	0	0	0	10	0	10	24
03:15 PM	2	1	3	3	0	3	3	0	3	7	0	7	16
03:30 PM	1	2	3	0	1	1	0	0	0	2	1	3	7
03:45 PM	2	1	3	4	0	4	2	0	2	7	0	7	16
Total	8	4	12	18	1	19	5	0	5	26	1	27	63
04:00 PM	5	0	5	12	2	14	0	0	0	15	1	16	35
04:15 PM	0	0	0	4	0	4	4	0	4	32	0	32	40
04:30 PM	16	0	16	13	0	13	2	0	2	26	1	27	58
04:45 PM	5	0	5	9	1	10	2	0	2	34	2	36	53
Total	26	0	26	38	3	41	8	0	8	107	4	111	186
05:00 PM	6	0	6	17	3	20	11	0	11	11	2	13	50
05:15 PM	4	0	4	9	2	11	7	0	7	15	1	16	38
05:30 PM	6	1	7	32	0	32	11	0	11	13	0	13	63
05:45 PM	2	1	3	12	2	14	11	0	11	15	0	15	43
Total	18	2	20	70	7	77	40	0	40	54	3	57	194
Grand Total	65	12	77	135	13	148	58	1	59	215	11	226	510
Apprch %	84.4	15.6		91.2	8.8		98.3	1.7		95.1	4.9		
Total %	12.7	2.4	15.1	26.5	2.5	29	11.4	0.2	11.6	42.2	2.2	44.3	

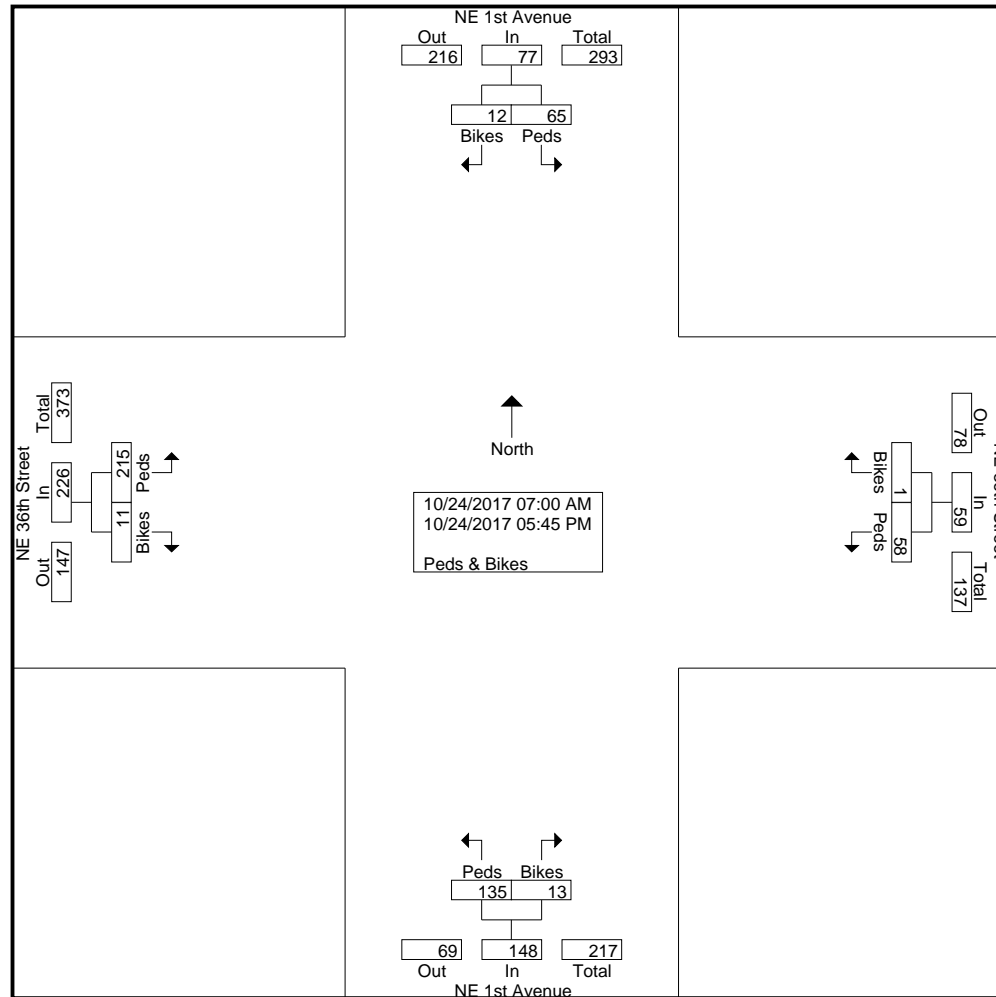
NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NE 1st Avenue Southbound			NE 1st Avenue Northbound			NE 36th Street Westbound			NE 36th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:45 AM													
07:45 AM	3	3	6	3	0	3	0	0	0	3	0	3	12
08:00 AM	3	1	4	1	1	2	0	1	1	5	1	6	13
08:15 AM	1	0	1	0	0	0	2	0	2	6	0	6	9
08:30 AM	2	1	3	1	0	1	1	0	1	6	0	6	11
Total Volume	9	5	14	5	1	6	3	1	4	20	1	21	45
% App. Total	64.3	35.7		83.3	16.7		75	25		95.2	4.8		
PHF	.750	.417	.583	.417	.250	.500	.375	.250	.500	.833	.250	.875	.865

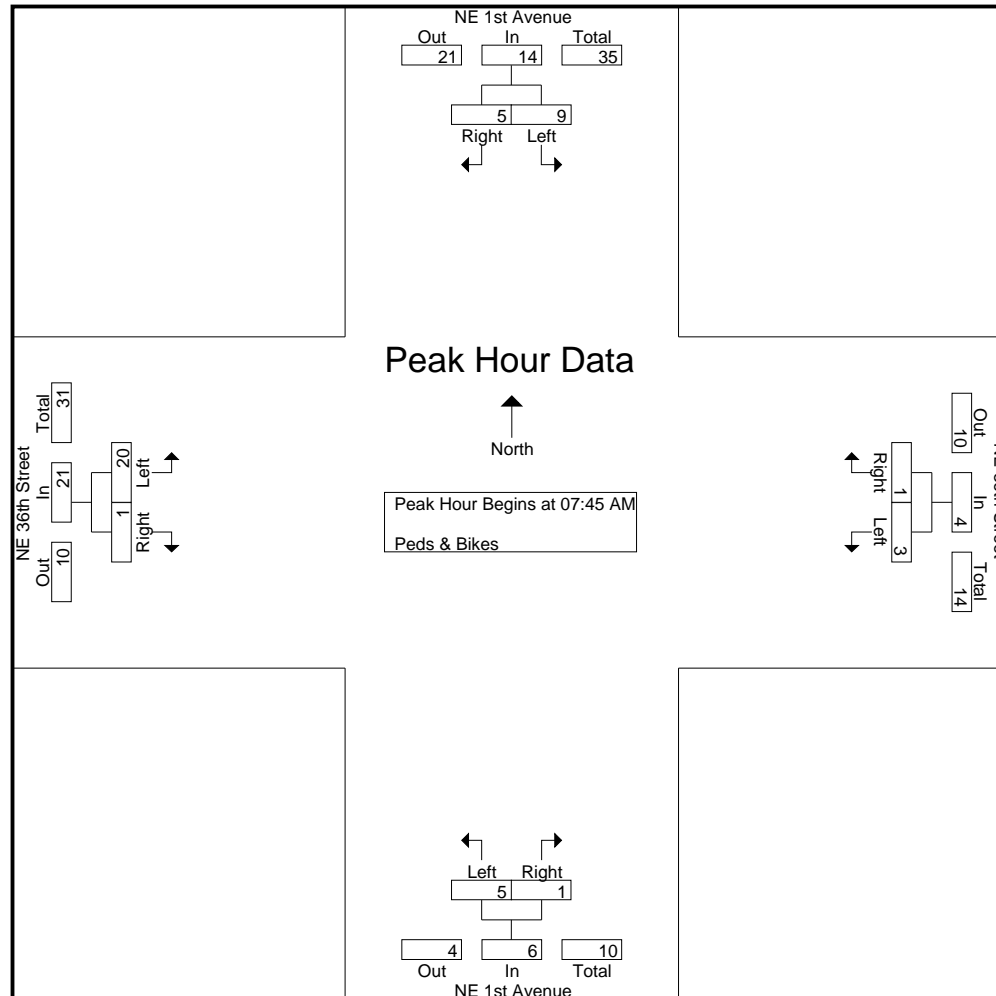
NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



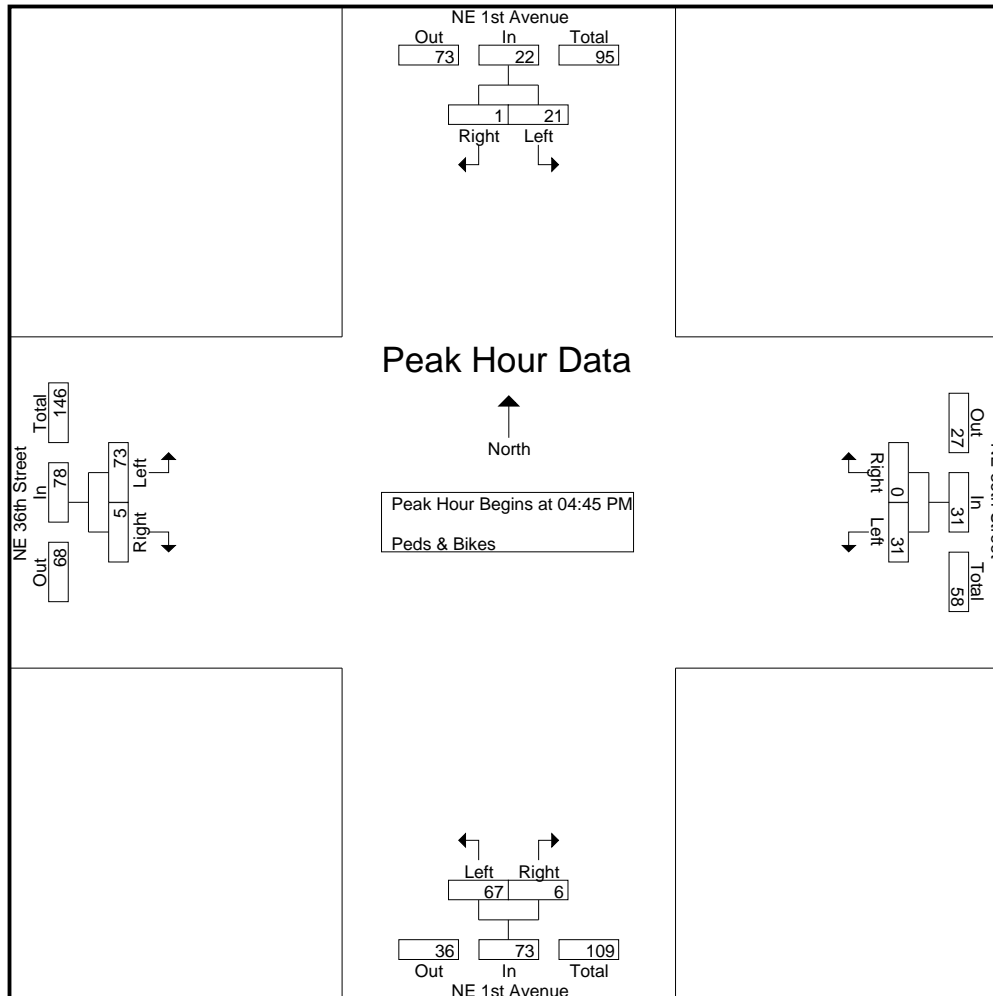
NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	NE 1st Avenue Southbound			NE 1st Avenue Northbound			NE 36th Street Westbound			NE 36th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 04:45 PM													
04:45 PM	5	0	5	9	1	10	2	0	2	34	2	36	53
05:00 PM	6	0	6	17	3	20	11	0	11	11	2	13	50
05:15 PM	4	0	4	9	2	11	7	0	7	15	1	16	38
05:30 PM	6	1	7	32	0	32	11	0	11	13	0	13	63
Total Volume	21	1	22	67	6	73	31	0	31	73	5	78	204
% App. Total	95.5	4.5		91.8	8.2		100	0		93.6	6.4		
PHF	.875	.250	.786	.523	.500	.570	.705	.000	.705	.537	.625	.542	.810

NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 6



NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Trucks

Start Time	NE 1st Avenue Southbound					NE 1st Avenue Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	4	0	4	7
07:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	4	0	4	5
07:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	5	0	5	7
07:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	6	0	6	8
Total	0	0	0	0	0	0	0	0	0	0	0	0	8	0	8	0	0	19	0	19	27
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	8	0	8	9
08:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	4	0	4	5
08:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	10	0	10	12
08:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	9	0	9	11
Total	0	0	0	0	0	0	0	0	0	0	0	0	6	0	6	0	0	31	0	31	37
*** BREAK ***																					
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	4	0	4	0	0	0	0	0	4
03:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	3	4	0	7	9
03:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	5	2	7	0	0	0	0	0	7
03:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	7	0	7	10
Total	0	0	0	0	0	0	0	0	0	0	0	0	14	2	16	0	3	11	0	14	30
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	6	0	6	0	0	1	0	1	7
04:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	4	0	4	0	0	2	1	3	7
04:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	1	2	0	3	5
04:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	1	0	0	1	3
Total	0	0	0	0	0	0	0	0	0	0	0	0	14	0	14	0	2	5	1	8	22
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	1	2	0	0	0	0	0	2
05:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	2
05:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	1	4	0	5	7
05:45 PM	0	1	0	0	1	0	0	0	0	0	0	0	4	0	4	0	0	1	0	1	6
Total	0	1	0	0	1	0	0	0	0	0	0	0	9	1	10	0	1	5	0	6	17
Grand Total	0	1	0	0	1	0	0	0	0	0	0	0	51	3	54	0	6	71	1	78	133
Apprch %	0	100	0	0		0	0	0	0		0	0	94.4	5.6		0	7.7	91	1.3		
Total %	0	0.8	0	0	0.8	0	0	0	0	0	0	0	38.3	2.3	40.6	0	4.5	53.4	0.8	58.6	

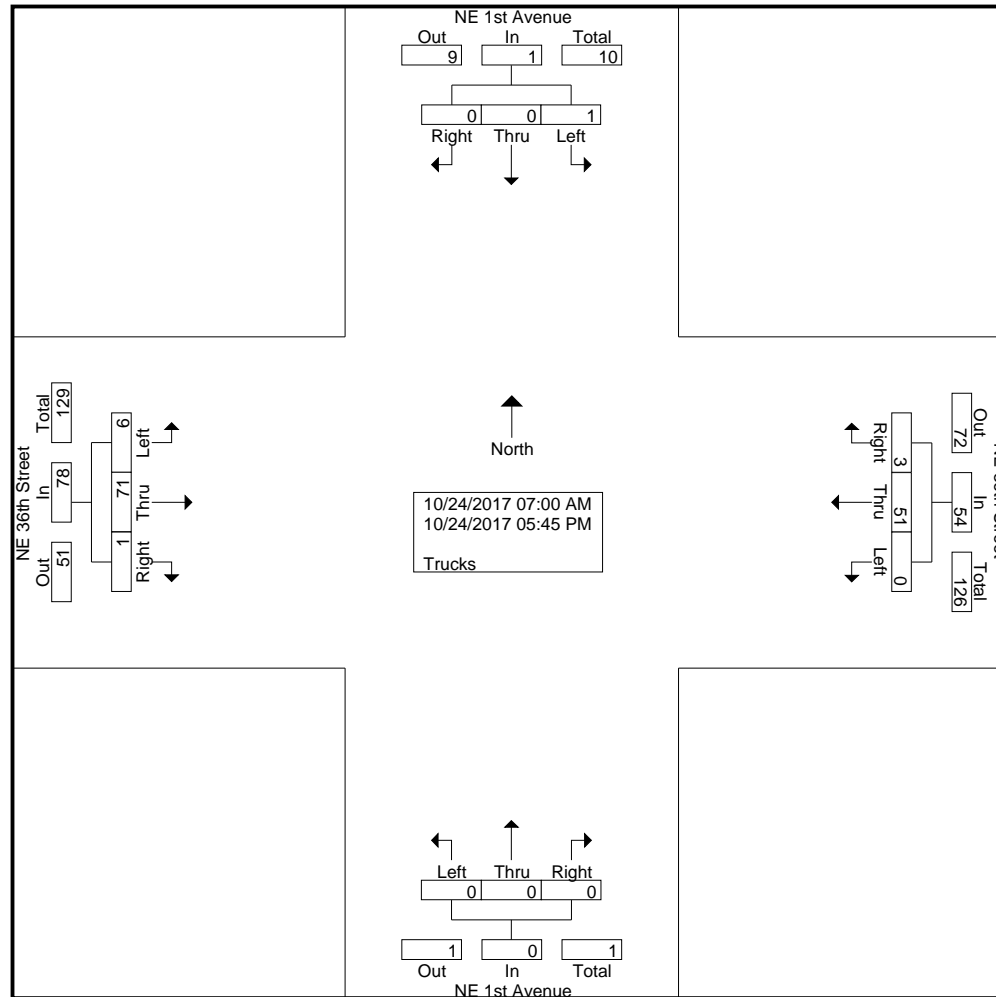
NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NE 1st Avenue Southbound					NE 1st Avenue Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	8	0	8	9	
08:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	4	0	4	5	
08:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	10	0	10	12	
08:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	9	0	9	11	
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	6	0	6	0	0	31	0	31	37	
% App. Total	0	0	0	0	0	0	0	0	0	0	0	0	100	0	6	0	0	100	0	31	37	
PHF	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.750	.000	.750	.000	.000	.775	.000	.775	.771	

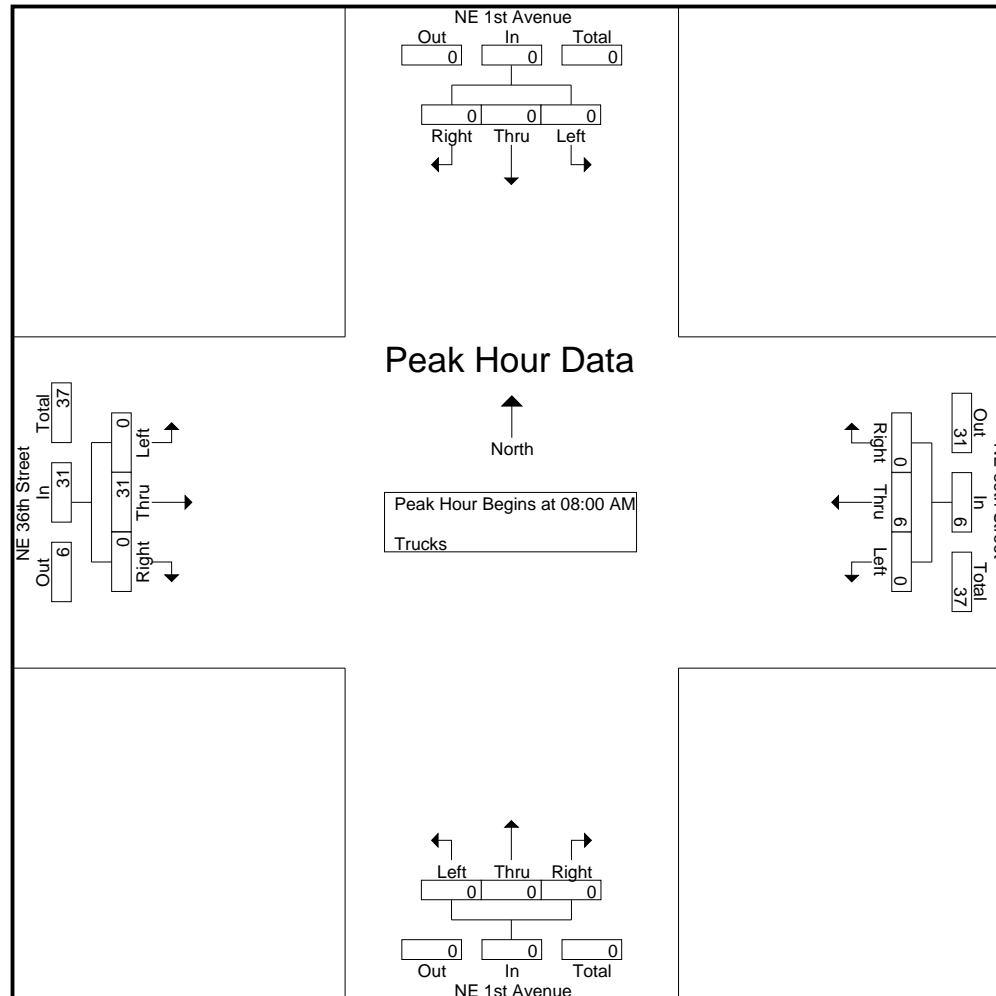
NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	NE 1st Avenue Southbound					NE 1st Avenue Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:15 PM																					
03:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	3	4	0	7	9
03:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	5	2	7	0	0	0	0	0	7
03:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	7	0	7	10
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	6	0	6	0	0	1	0	1	7
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	16	2	18	0	3	12	0	15	33
% App. Total	0	0	0	0	0	0	0	0	0	0	0	0	88.9	11.1		0	20	80	0		
PHF	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.667	.250	.643	.000	.250	.429	.000	.536	.825

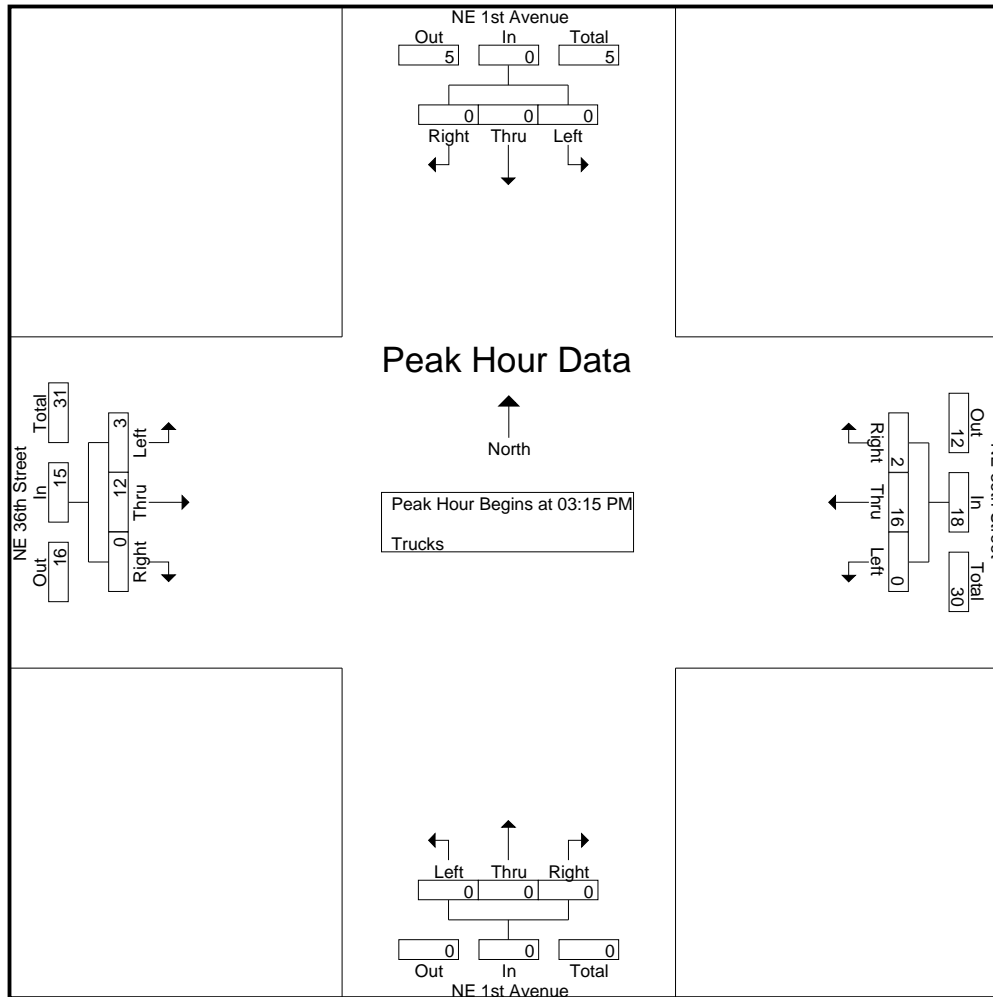
NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	NE 1st Avenue Southbound					NE 1st Avenue Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	2	1	4	7	0	0	5	2	7	0	4	64	3	71	0	4	152	4	160	245
07:15 AM	0	2	2	1	5	0	1	2	1	4	0	1	40	3	44	0	5	150	1	156	209
07:30 AM	0	1	3	0	4	0	0	2	1	3	0	5	86	5	96	0	4	167	1	172	275
07:45 AM	0	7	4	4	15	0	1	4	3	8	0	2	60	6	68	0	5	160	1	166	257
Total	0	12	10	9	31	0	2	13	7	22	0	12	250	17	279	0	18	629	7	654	986
08:00 AM	0	8	0	5	13	0	0	6	2	8	0	0	66	5	71	0	4	162	2	168	260
08:15 AM	0	7	0	5	12	0	3	5	4	12	0	0	61	4	65	0	3	179	5	187	276
08:30 AM	0	9	0	7	16	0	4	8	4	16	0	1	69	5	75	0	4	168	1	173	280
08:45 AM	0	9	1	10	20	0	3	6	5	14	0	0	84	7	91	0	6	184	2	192	317
Total	0	33	1	27	61	0	10	25	15	50	0	1	280	21	302	0	17	693	10	720	1133
*** BREAK ***																					
03:00 PM	0	3	3	1	7	0	17	17	18	52	0	15	102	9	126	0	14	113	11	138	323
03:15 PM	0	5	2	3	10	0	13	14	14	41	0	11	103	4	118	0	18	97	15	130	299
03:30 PM	0	0	0	4	4	0	7	32	19	58	0	10	131	10	151	0	14	112	12	138	351
03:45 PM	0	4	2	1	7	0	8	19	22	49	0	14	137	8	159	0	10	124	15	149	364
Total	0	12	7	9	28	0	45	82	73	200	0	50	473	31	554	0	56	446	53	555	1337
04:00 PM	0	4	7	4	15	0	4	15	13	32	0	12	136	10	158	0	8	116	19	143	348
04:15 PM	0	3	0	3	6	0	2	10	19	31	0	18	117	9	144	0	12	120	24	156	337
04:30 PM	0	3	2	2	7	0	3	17	26	46	0	13	104	8	125	0	10	108	10	128	306
04:45 PM	0	2	0	1	3	0	6	12	11	29	0	16	125	3	144	0	16	141	20	177	353
Total	0	12	9	10	31	0	15	54	69	138	0	59	482	30	571	0	46	485	73	604	1344
05:00 PM	0	2	6	5	13	0	2	18	17	37	0	14	127	10	151	0	9	137	15	161	362
05:15 PM	0	3	3	4	10	0	3	20	21	44	0	11	133	8	152	0	14	125	21	160	366
05:30 PM	0	4	2	4	10	0	1	16	21	38	0	22	127	6	155	0	12	96	17	125	328
05:45 PM	0	4	2	0	6	0	7	8	17	32	0	7	90	10	107	0	12	111	17	140	285
Total	0	13	13	13	39	0	13	62	76	151	0	54	477	34	565	0	47	469	70	586	1341
Grand Total	0	82	40	68	190	0	85	236	240	561	0	176	1962	133	2271	0	184	2722	213	3119	6141
Apprch %	0	43.2	21.1	35.8		0	15.2	42.1	42.8		0	7.7	86.4	5.9		0	5.9	87.3	6.8		
Total %	0	1.3	0.7	1.1	3.1	0	1.4	3.8	3.9	9.1	0	2.9	31.9	2.2	37	0	3	44.3	3.5	50.8	
Vehicle	0	81	40	68	189	0	85	236	240	561	0	176	1911	130	2217	0	178	2651	212	3041	6008
% Vehicle	0	98.8	100	100	99.5	0	100	100	100	100	0	100	97.4	97.7	97.6	0	96.7	97.4	99.5	97.5	97.8

NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	NE 1st Avenue Southbound					NE 1st Avenue Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	1	0	0	1	0	0	0	0	0	0	0	51	3	54	0	6	71	1	78	133
% Trucks	0	1.2	0	0	0.5	0	0	0	0	0	0	0	2.6	2.3	2.4	0	3.3	2.6	0.5	2.5	2.2

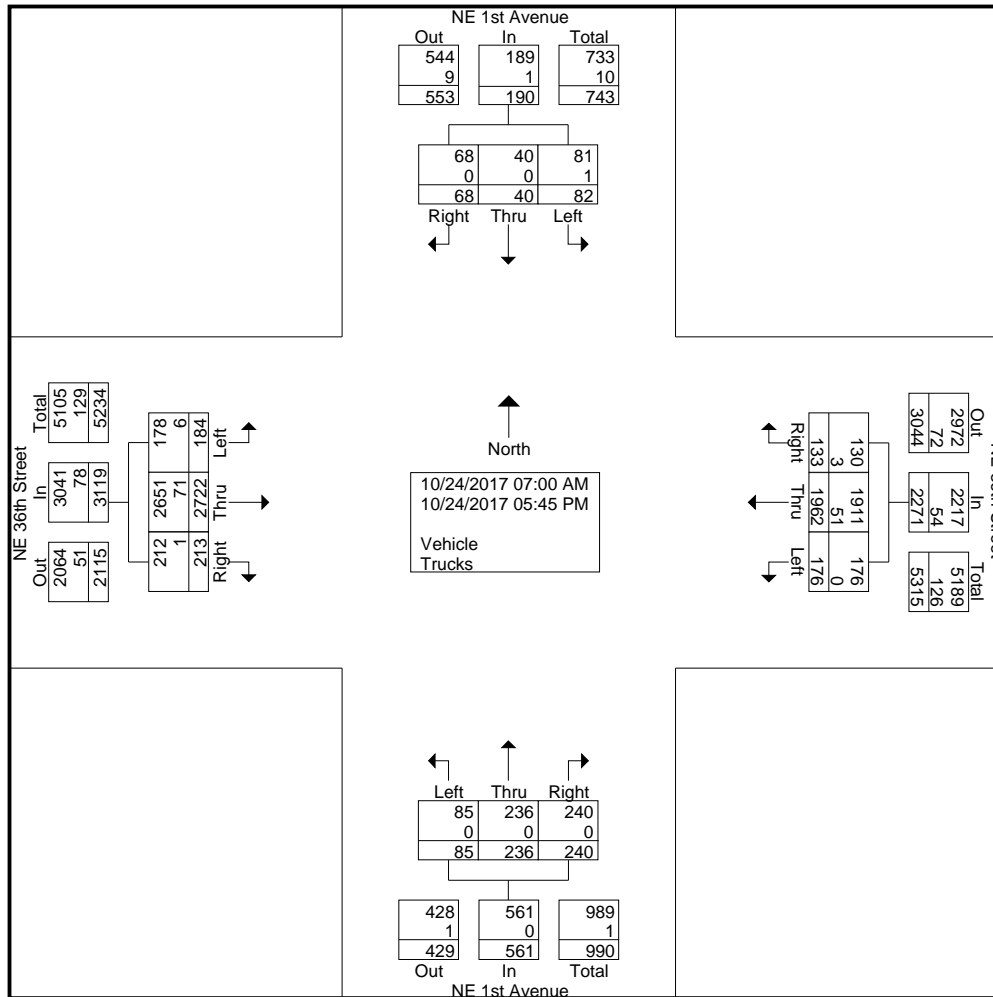
NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4

Start Time	NE 1st Avenue Southbound					NE 1st Avenue Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	8	0	5	13	0	0	6	2	8	0	0	66	5	71	0	4	162	2	168	260	
08:15 AM	0	7	0	5	12	0	3	5	4	12	0	0	61	4	65	0	3	179	5	187	276	
08:30 AM	0	9	0	7	16	0	4	8	4	16	0	1	69	5	75	0	4	168	1	173	280	
08:45 AM	0	9	1	10	20	0	3	6	5	14	0	0	84	7	91	0	6	184	2	192	317	
Total Volume	0	33	1	27	61	0	10	25	15	50	0	1	280	21	302	0	17	693	10	720	1133	
% App. Total	0	54.1	1.6	44.3		0	20	50	30		0	0.3	92.7	7		0	2.4	96.2	1.4			
PHF	.000	.917	.250	.675	.763	.000	.625	.781	.750	.781	.000	.250	.833	.750	.830	.000	.708	.942	.500	.938	.894	

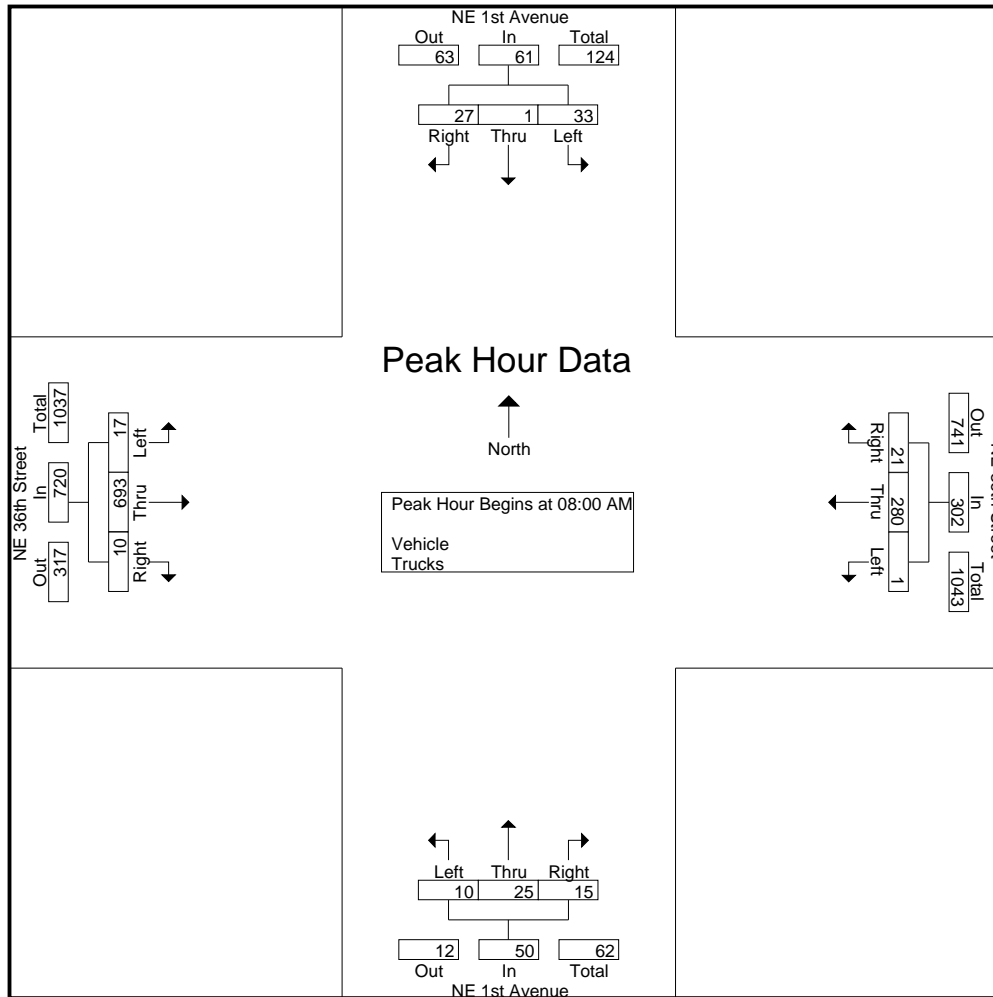
NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 6

Start Time	NE 1st Avenue Southbound					NE 1st Avenue Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:45 PM																					
04:45 PM	0	2	0	1	3	0	6	12	11	29	0	16	125	3	144	0	16	141	20	177	353
05:00 PM	0	2	6	5	13	0	2	18	17	37	0	14	127	10	151	0	9	137	15	161	362
05:15 PM	0	3	3	4	10	0	3	20	21	44	0	11	133	8	152	0	14	125	21	160	366
05:30 PM	0	4	2	4	10	0	1	16	21	38	0	22	127	6	155	0	12	96	17	125	328
Total Volume	0	11	11	14	36	0	12	66	70	148	0	63	512	27	602	0	51	499	73	623	1409
% App. Total	0	30.6	30.6	38.9		0	8.1	44.6	47.3		0	10.5	85	4.5		0	8.2	80.1	11.7		
PHF	.000	.688	.458	.700	.692	.000	.500	.825	.833	.841	.000	.716	.962	.675	.971	.000	.797	.885	.869	.880	.962

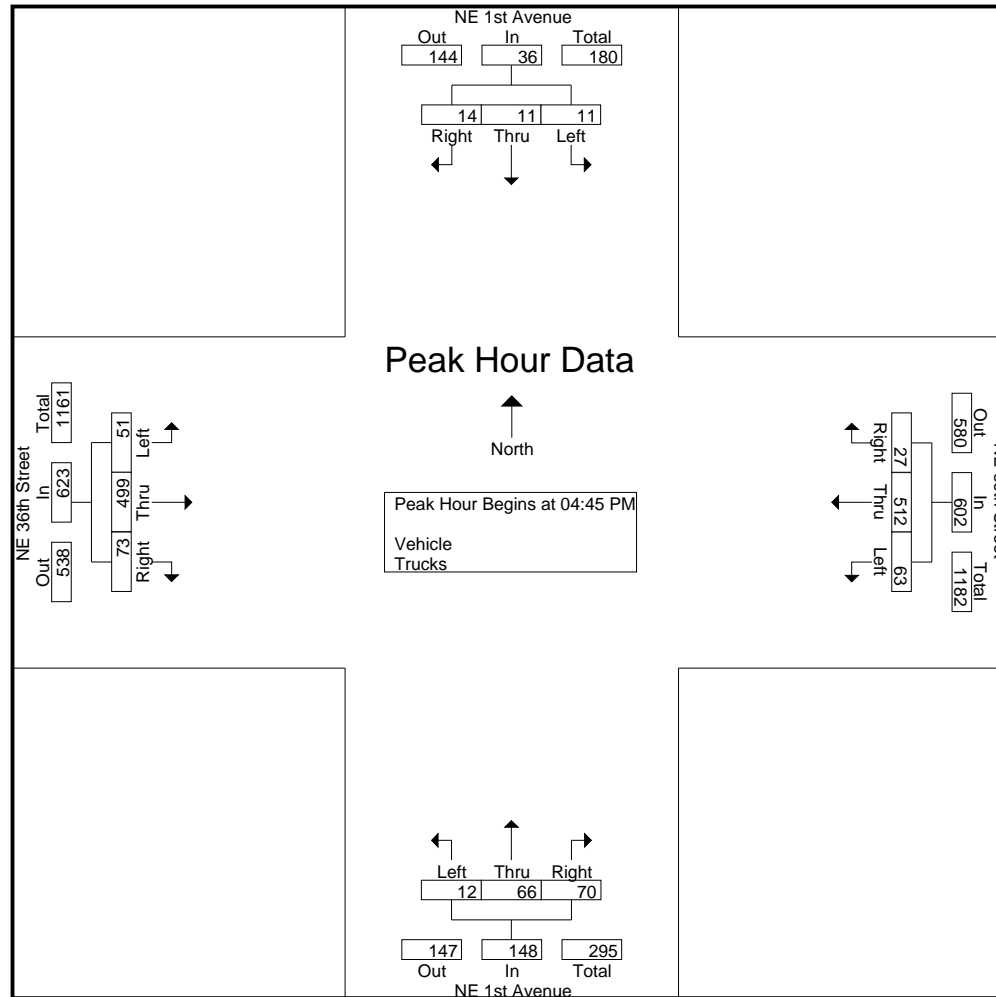
NE 1st Avenue & NE 36th Street

File Name : TMC-7 NE 1st Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



NE 1st Avenue & NE 38th Street

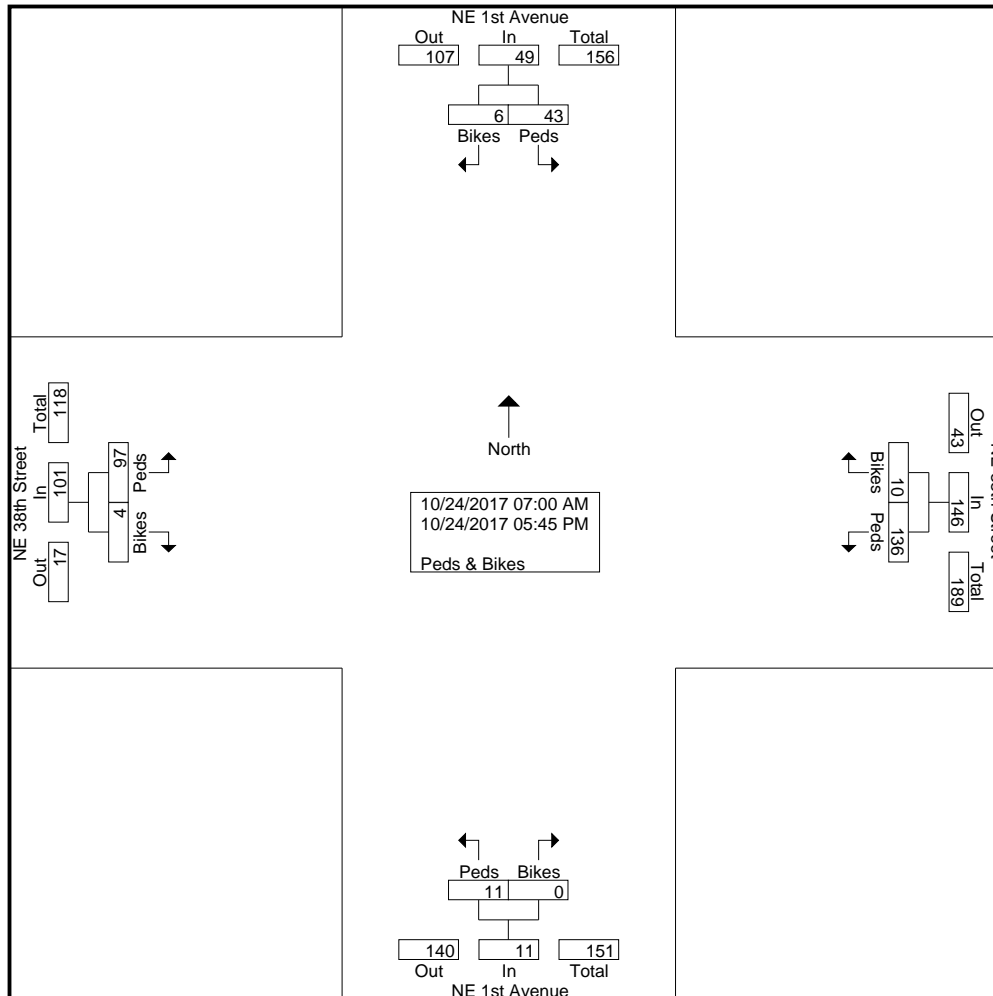
File Name : TMC-8 NE 1st Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	NE 1st Avenue Southbound			NE 1st Avenue Northbound			NE 38th Street Westbound			NE 38th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	2	0	2	0	0	0	1	0	1	4	0	4	7
07:15 AM	4	0	4	1	0	1	2	0	2	6	0	6	13
07:30 AM	3	0	3	1	0	1	7	0	7	2	0	2	13
07:45 AM	1	2	3	0	0	0	1	0	1	3	1	4	8
Total	10	2	12	2	0	2	11	0	11	15	1	16	41
08:00 AM	1	2	3	0	0	0	4	2	6	3	0	3	12
08:15 AM	0	0	0	0	0	0	6	0	6	2	0	2	8
08:30 AM	3	0	3	0	0	0	4	1	5	9	0	9	17
08:45 AM	2	0	2	2	0	2	5	0	5	11	0	11	20
Total	6	2	8	2	0	2	19	3	22	25	0	25	57
*** BREAK ***													
03:00 PM	3	0	3	0	0	0	9	1	10	3	0	3	16
03:15 PM	5	0	5	1	0	1	13	0	13	4	1	5	24
03:30 PM	5	0	5	0	0	0	13	1	14	6	0	6	25
03:45 PM	0	0	0	0	0	0	4	1	5	6	0	6	11
Total	13	0	13	1	0	1	39	3	42	19	1	20	76
04:00 PM	2	0	2	0	0	0	10	0	10	2	0	2	14
04:15 PM	4	0	4	0	0	0	14	1	15	6	1	7	26
04:30 PM	2	1	3	2	0	2	10	1	11	1	1	2	18
04:45 PM	1	0	1	0	0	0	2	0	2	2	0	2	5
Total	9	1	10	2	0	2	36	2	38	11	2	13	63
05:00 PM	2	1	3	1	0	1	5	2	7	5	0	5	16
05:15 PM	2	0	2	1	0	1	9	0	9	7	0	7	19
05:30 PM	1	0	1	2	0	2	7	0	7	13	0	13	23
05:45 PM	0	0	0	0	0	0	10	0	10	2	0	2	12
Total	5	1	6	4	0	4	31	2	33	27	0	27	70
Grand Total	43	6	49	11	0	11	136	10	146	97	4	101	307
Apprch %	87.8	12.2		100	0		93.2	6.8		96	4		
Total %	14	2	16	3.6	0	3.6	44.3	3.3	47.6	31.6	1.3	32.9	

NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2



NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NE 1st Avenue Southbound			NE 1st Avenue Northbound			NE 38th Street Westbound			NE 38th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 08:00 AM													
08:00 AM	1	2	3	0	0	0	4	2	6	3	0	3	12
08:15 AM	0	0	0	0	0	0	6	0	6	2	0	2	8
08:30 AM	3	0	3	0	0	0	4	1	5	9	0	9	17
08:45 AM	2	0	2	2	0	2	5	0	5	11	0	11	20
Total Volume	6	2	8	2	0	2	19	3	22	25	0	25	57
% App. Total	75	25		100	0		86.4	13.6		100	0		
PHF	.500	.250	.667	.250	.000	.250	.792	.375	.917	.568	.000	.568	.713

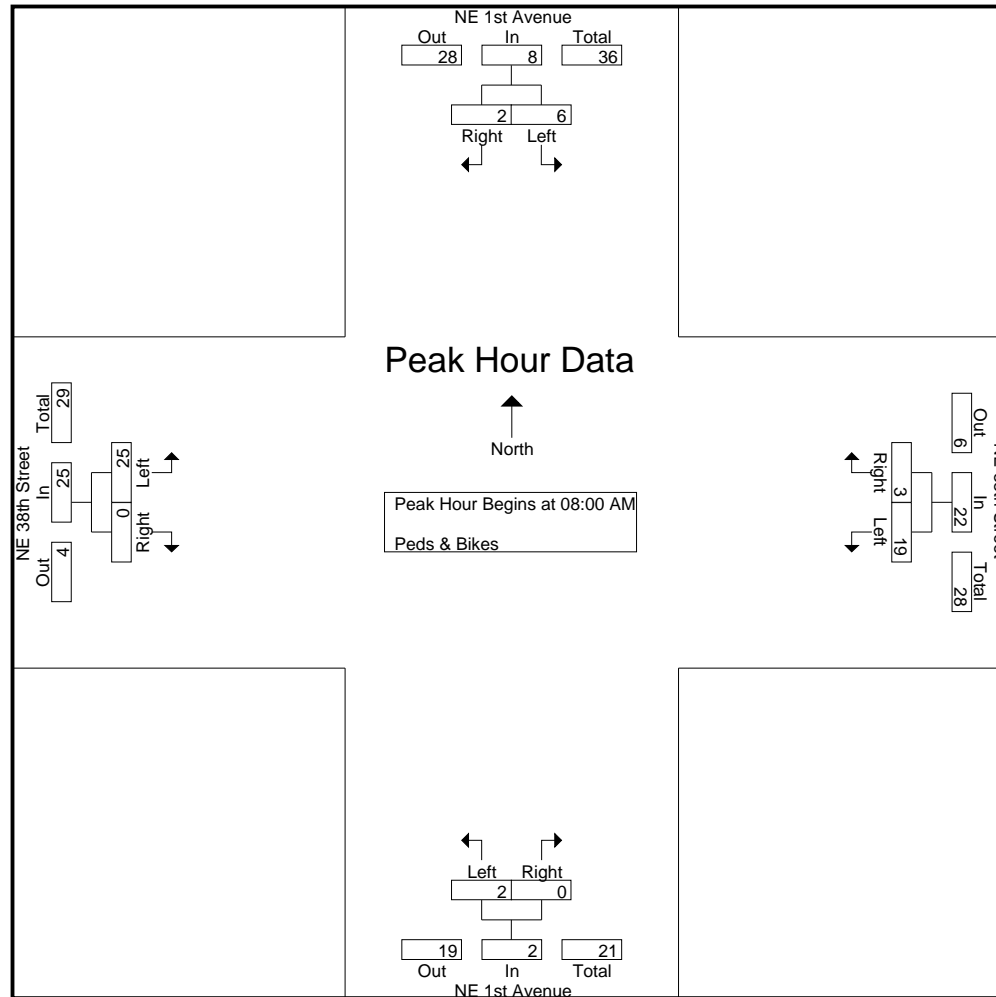
NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	NE 1st Avenue Southbound			NE 1st Avenue Northbound			NE 38th Street Westbound			NE 38th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 03:00 PM													
03:00 PM	3	0	3	0	0	0	9	1	10	3	0	3	16
03:15 PM	5	0	5	1	0	1	13	0	13	4	1	5	24
03:30 PM	5	0	5	0	0	0	13	1	14	6	0	6	25
03:45 PM	0	0	0	0	0	0	4	1	5	6	0	6	11
Total Volume	13	0	13	1	0	1	39	3	42	19	1	20	76
% App. Total	100	0		100	0		92.9	7.1		95	5		
PHF	.650	.000	.650	.250	.000	.250	.750	.750	.750	.792	.250	.833	.760

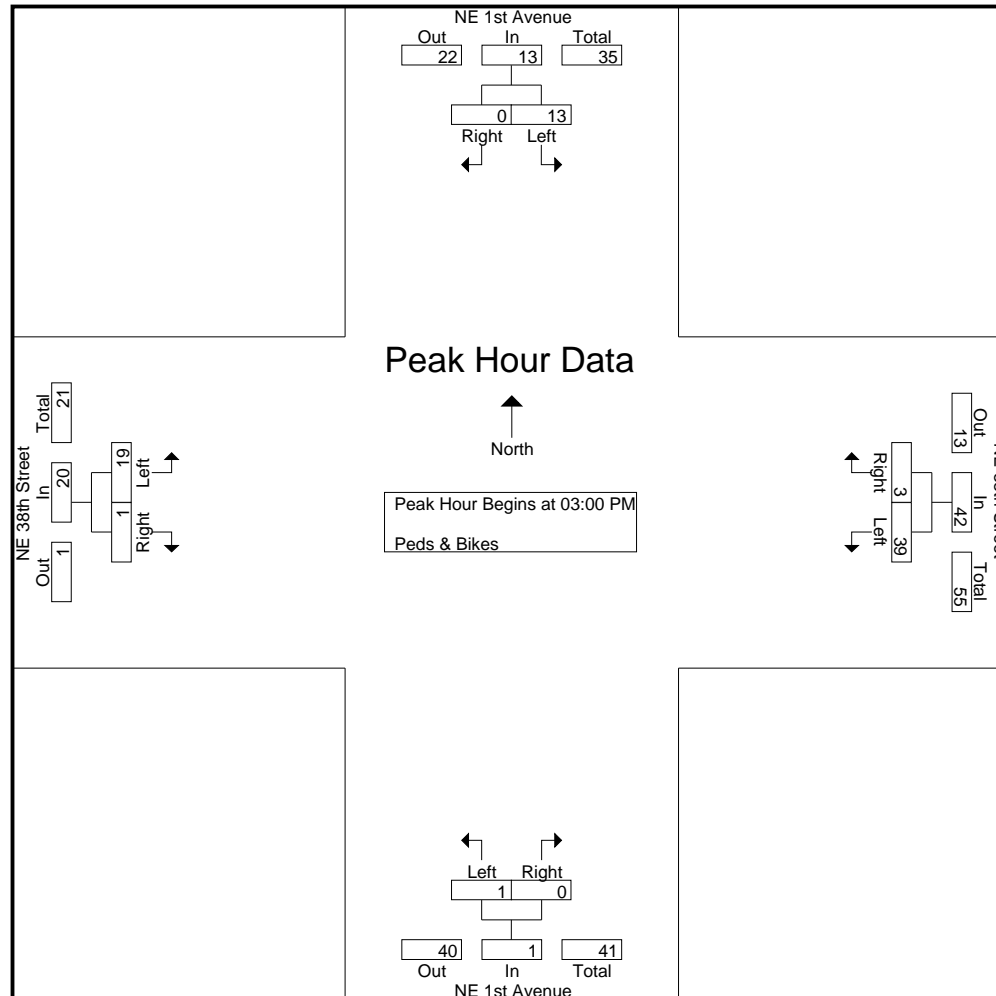
NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Trucks

Start Time	NE 1st Avenue Southbound					NE 1st Avenue Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
*** BREAK ***																					
07:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	3
07:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	2	0	2	3
07:45 AM	0	0	2	0	2	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	4
Total	0	0	2	0	2	0	0	0	0	0	0	0	6	0	6	0	0	2	0	2	10
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	1	2	0	3	0	0	0	0	0	3
08:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	1
08:30 AM	0	1	0	0	1	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	2
08:45 AM	0	0	0	0	0	0	0	0	0	0	0	1	3	0	4	0	0	0	0	0	4
Total	0	1	0	0	1	0	0	0	0	0	0	2	7	0	9	0	0	0	0	0	10
*** BREAK ***																					
03:00 PM	0	0	1	1	2	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	5
03:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	3	1	4	0	0	1	0	1	5
03:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	1
03:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	2
Total	0	0	1	1	2	0	0	0	0	0	0	0	8	1	9	0	0	2	0	2	13
04:00 PM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	2	0	2	3
04:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	1	2	0	0	0	0	0	2
04:30 PM	0	1	0	0	1	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	3
04:45 PM	0	0	0	0	0	0	0	1	0	1	0	0	2	1	3	0	0	0	0	0	4
Total	0	1	0	0	1	0	0	1	1	2	0	0	5	2	7	0	0	2	0	2	12
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	1
05:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	3	2	5	0	0	0	0	0	5
05:30 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	1	1	0	0	1	0	1	3
05:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	1	2	0	1	0	0	1	3
Total	0	1	0	0	1	0	0	0	0	0	0	0	5	4	9	0	1	1	0	2	12
Grand Total	0	3	3	1	7	0	0	1	1	2	0	2	31	7	40	0	1	7	0	8	57
Apprch %	0	42.9	42.9	14.3		0	0	50	50		0	5	77.5	17.5		0	12.5	87.5	0		
Total %	0	5.3	5.3	1.8	12.3	0	0	1.8	1.8	3.5	0	3.5	54.4	12.3	70.2	0	1.8	12.3	0	14	

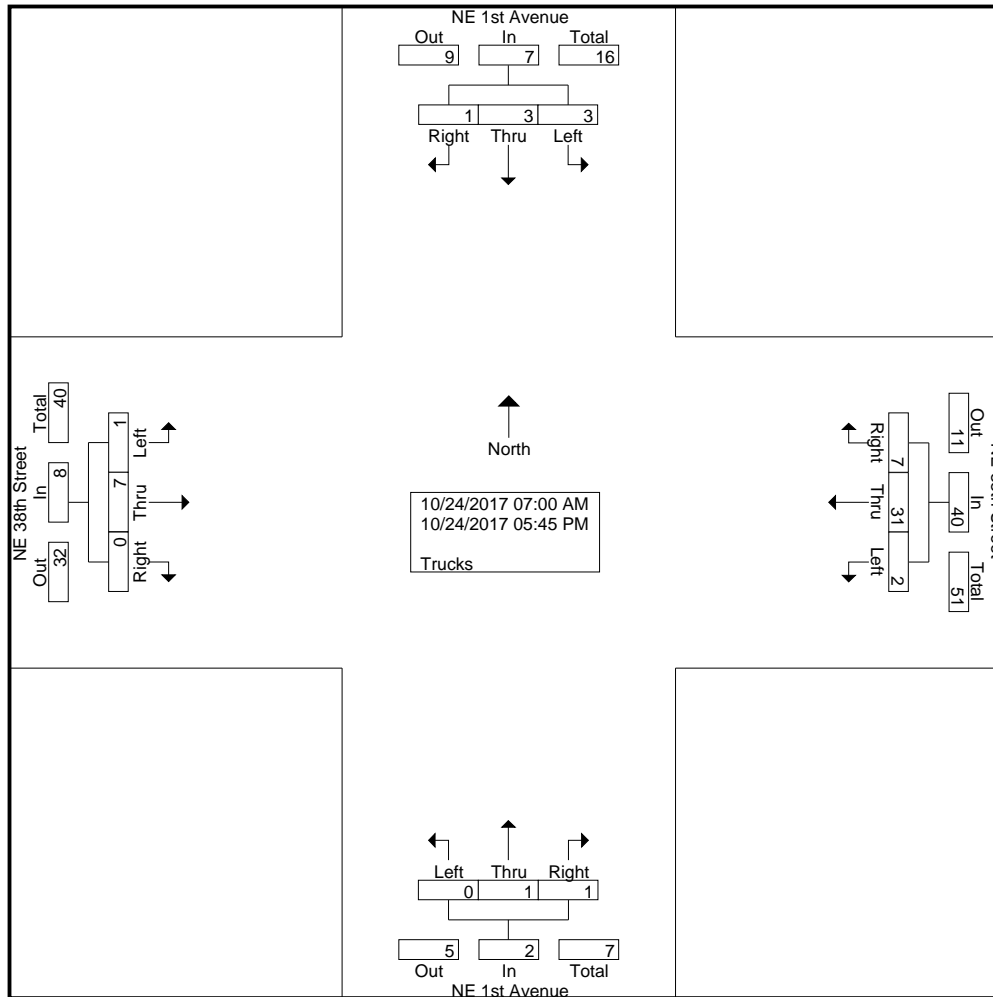
NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NE 1st Avenue Southbound					NE 1st Avenue Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 07:15 AM																						
07:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	0	3
07:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	2	0	0	2	3
07:45 AM	0	0	2	0	2	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	4
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	1	2	0	3	0	0	0	0	0	0	3
Total Volume	0	0	2	0	2	0	0	0	0	0	0	1	8	0	9	0	0	2	0	0	2	13
% App. Total	0	0	100	0		0	0	0	0		0	11.1	88.9	0		0	0	100	0			
PHF	.000	.000	.250	.000	.250	.000	.000	.000	.000	.000	.000	.250	.667	.000	.750	.000	.000	.250	.000	.250	.250	.813

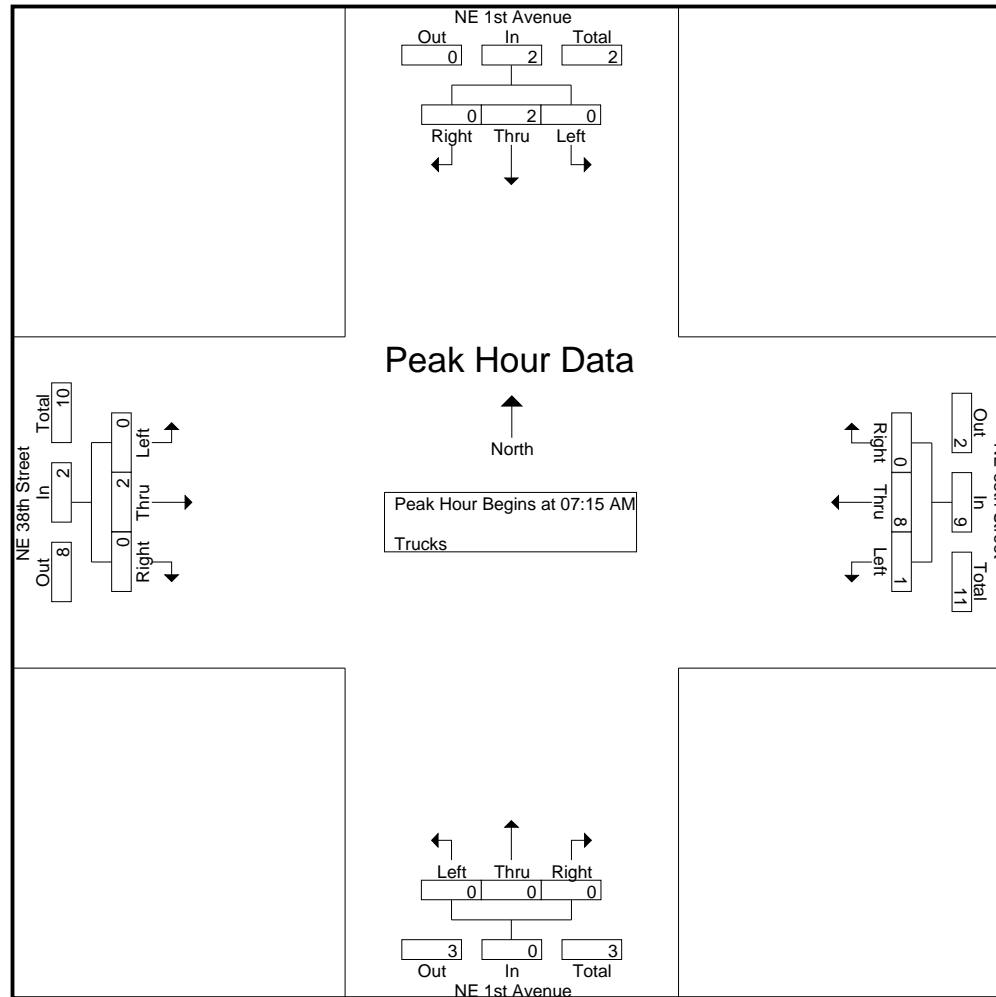
NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	NE 1st Avenue Southbound					NE 1st Avenue Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:00 PM																					
03:00 PM	0	0	1	1	2	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	5
03:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	3	1	4	0	0	1	0	1	5
03:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	1
03:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	2
Total Volume	0	0	1	1	2	0	0	0	0	0	0	0	8	1	9	0	0	2	0	2	13
% App. Total	0	0	50	50		0	0	0	0		0	0	88.9	11.1		0	0	100	0		
PHF	.000	.000	.250	.250	.250	.000	.000	.000	.000	.000	.000	.000	.667	.250	.563	.000	.000	.500	.000	.500	.650

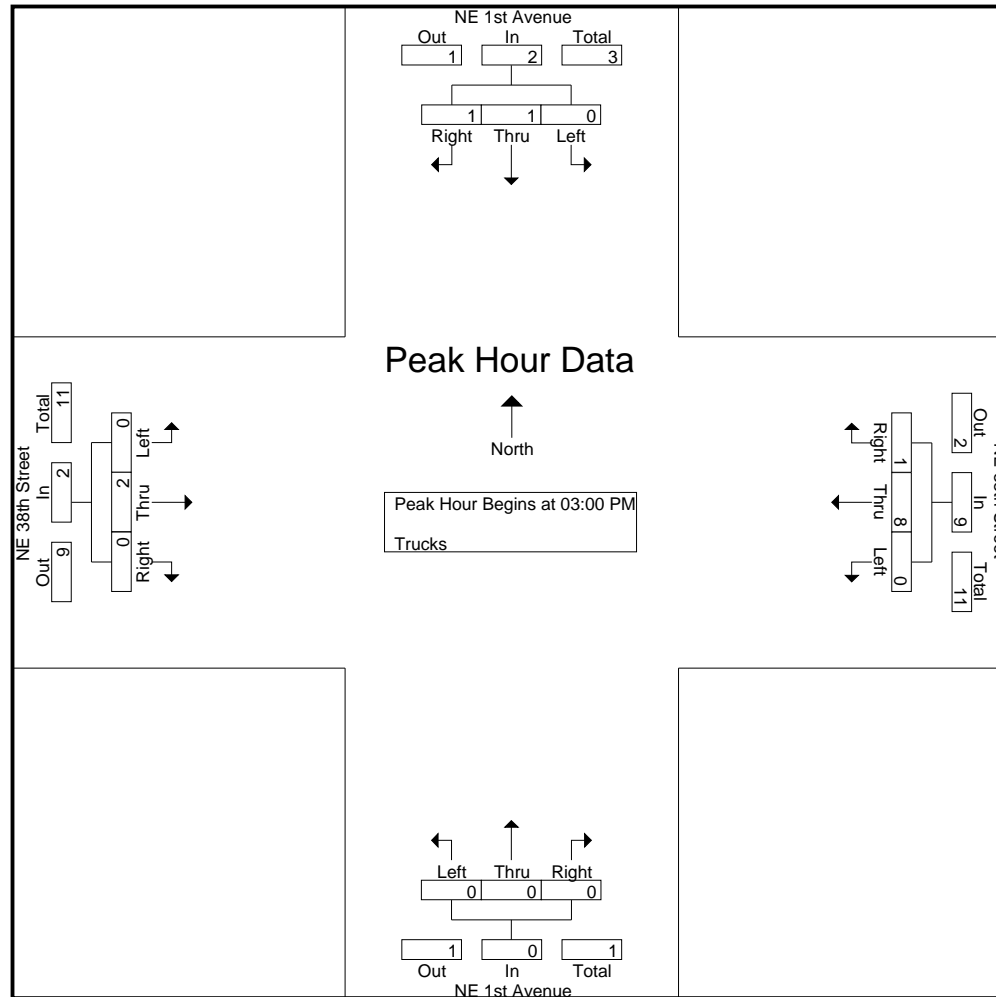
NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	NE 1st Avenue Southbound					NE 1st Avenue Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	1	7	1	9	0	10	3	1	14	0	0	12	0	12	0	8	20	1	29	64
07:15 AM	0	5	10	8	23	0	9	1	1	11	0	0	24	1	25	0	6	35	2	43	102
07:30 AM	0	2	9	15	26	0	5	2	5	12	0	1	26	2	29	0	6	30	2	38	105
07:45 AM	0	4	17	5	26	0	10	4	3	17	0	2	16	1	19	0	3	15	4	22	84
Total	0	12	43	29	84	0	34	10	10	54	0	3	78	4	85	0	23	100	9	132	355
08:00 AM	0	4	15	11	30	0	7	6	5	18	0	4	32	0	36	0	4	18	4	26	110
08:15 AM	0	4	19	10	33	0	6	3	4	13	0	1	19	1	21	0	8	22	3	33	100
08:30 AM	0	3	14	14	31	0	9	7	5	21	0	6	24	2	32	0	8	17	1	26	110
08:45 AM	0	15	19	18	52	0	6	6	12	24	0	5	22	5	32	0	10	26	8	44	152
Total	0	26	67	53	146	0	28	22	26	76	0	16	97	8	121	0	30	83	16	129	472
*** BREAK ***																					
03:00 PM	0	6	11	10	27	0	15	15	4	34	0	3	38	14	55	0	2	13	7	22	138
03:15 PM	0	7	8	11	26	1	17	11	4	33	0	3	22	17	42	0	6	17	1	24	125
03:30 PM	0	5	8	6	19	0	23	27	6	56	0	6	35	16	57	0	8	19	2	29	161
03:45 PM	0	3	19	5	27	0	18	18	6	42	0	2	34	15	51	0	3	12	2	17	137
Total	0	21	46	32	99	1	73	71	20	165	0	14	129	62	205	0	19	61	12	92	561
04:00 PM	0	2	8	9	19	0	16	17	6	39	0	3	25	17	45	0	6	12	3	21	124
04:15 PM	0	4	10	6	20	0	8	13	9	30	0	2	28	7	37	0	8	8	3	19	106
04:30 PM	0	5	9	8	22	1	8	21	11	41	0	0	32	10	42	0	9	13	3	25	130
04:45 PM	0	3	15	7	25	0	5	14	10	29	0	1	20	11	32	0	2	7	7	16	102
Total	0	14	42	30	86	1	37	65	36	139	0	6	105	45	156	0	25	40	16	81	462
05:00 PM	0	8	11	11	30	0	16	16	10	42	0	2	32	8	42	0	6	10	5	21	135
05:15 PM	0	2	7	2	11	0	12	22	16	50	0	7	33	13	53	0	8	15	5	28	142
05:30 PM	0	3	6	5	14	0	19	16	5	40	0	5	23	12	40	0	5	16	5	26	120
05:45 PM	0	3	4	3	10	1	12	15	4	32	0	2	20	7	29	0	7	13	5	25	96
Total	0	16	28	21	65	1	59	69	35	164	0	16	108	40	164	0	26	54	20	100	493
Grand Total	0	89	226	165	480	3	231	237	127	598	0	55	517	159	731	0	123	338	73	534	2343
Apprch %	0	18.5	47.1	34.4		0.5	38.6	39.6	21.2		0	7.5	70.7	21.8		0	23	63.3	13.7		
Total %	0	3.8	9.6	7	20.5	0.1	9.9	10.1	5.4	25.5	0	2.3	22.1	6.8	31.2	0	5.2	14.4	3.1	22.8	
Vehicle	0	86	223	164	473	3	231	236	126	596	0	53	486	152	691	0	122	331	73	526	2286
% Vehicle	0	96.6	98.7	99.4	98.5	100	100	99.6	99.2	99.7	0	96.4	94	95.6	94.5	0	99.2	97.9	100	98.5	97.6

NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	NE 1st Avenue Southbound					NE 1st Avenue Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	3	3	1	7	0	0	1	1	2	0	2	31	7	40	0	1	7	0	8	57
% Trucks	0	3.4	1.3	0.6	1.5	0	0	0.4	0.8	0.3	0	3.6	6	4.4	5.5	0	0.8	2.1	0	1.5	2.4

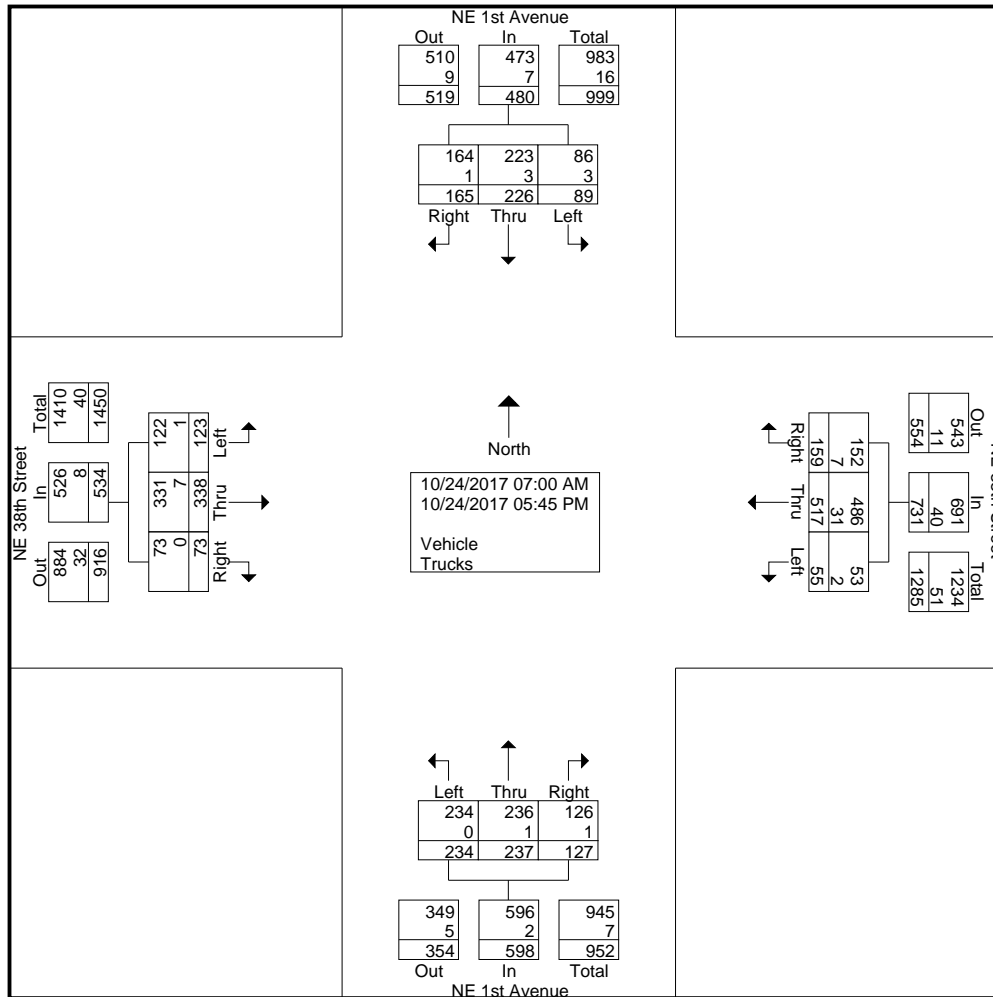
NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4

Start Time	NE 1st Avenue Southbound					NE 1st Avenue Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	4	15	11	30	0	7	6	5	18	0	4	32	0	36	0	4	18	4	26	110	
08:15 AM	0	4	19	10	33	0	6	3	4	13	0	1	19	1	21	0	8	22	3	33	100	
08:30 AM	0	3	14	14	31	0	9	7	5	21	0	6	24	2	32	0	8	17	1	26	110	
08:45 AM	0	15	19	18	52	0	6	6	12	24	0	5	22	5	32	0	10	26	8	44	152	
Total Volume	0	26	67	53	146	0	28	22	26	76	0	16	97	8	121	0	30	83	16	129	472	
% App. Total	0	17.8	45.9	36.3		0	36.8	28.9	34.2		0	13.2	80.2	6.6		0	23.3	64.3	12.4			
PHF	.000	.433	.882	.736	.702	.000	.778	.786	.542	.792	.000	.667	.758	.400	.840	.000	.750	.798	.500	.733	.776	

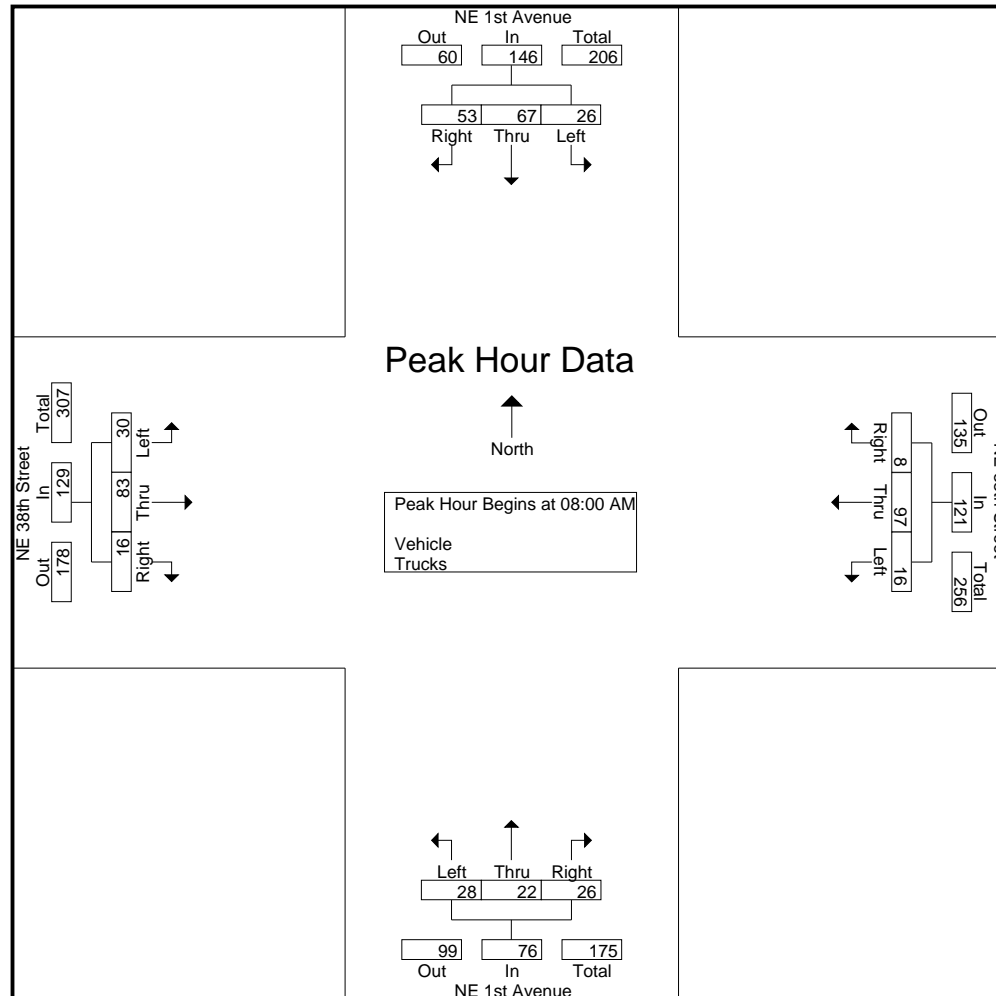
NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 6

Start Time	NE 1st Avenue Southbound					NE 1st Avenue Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:00 PM																					
03:00 PM	0	6	11	10	27	0	15	15	4	34	0	3	38	14	55	0	2	13	7	22	138
03:15 PM	0	7	8	11	26	1	17	11	4	33	0	3	22	17	42	0	6	17	1	24	125
03:30 PM	0	5	8	6	19	0	23	27	6	56	0	6	35	16	57	0	8	19	2	29	161
03:45 PM	0	3	19	5	27	0	18	18	6	42	0	2	34	15	51	0	3	12	2	17	137
Total Volume	0	21	46	32	99	1	73	71	20	165	0	14	129	62	205	0	19	61	12	92	561
% App. Total	0	21.2	46.5	32.3		0.6	44.2	43	12.1		0	6.8	62.9	30.2		0	20.7	66.3	13		
PHF	.000	.750	.605	.727	.917	.250	.793	.657	.833	.737	.000	.583	.849	.912	.899	.000	.594	.803	.429	.793	.871

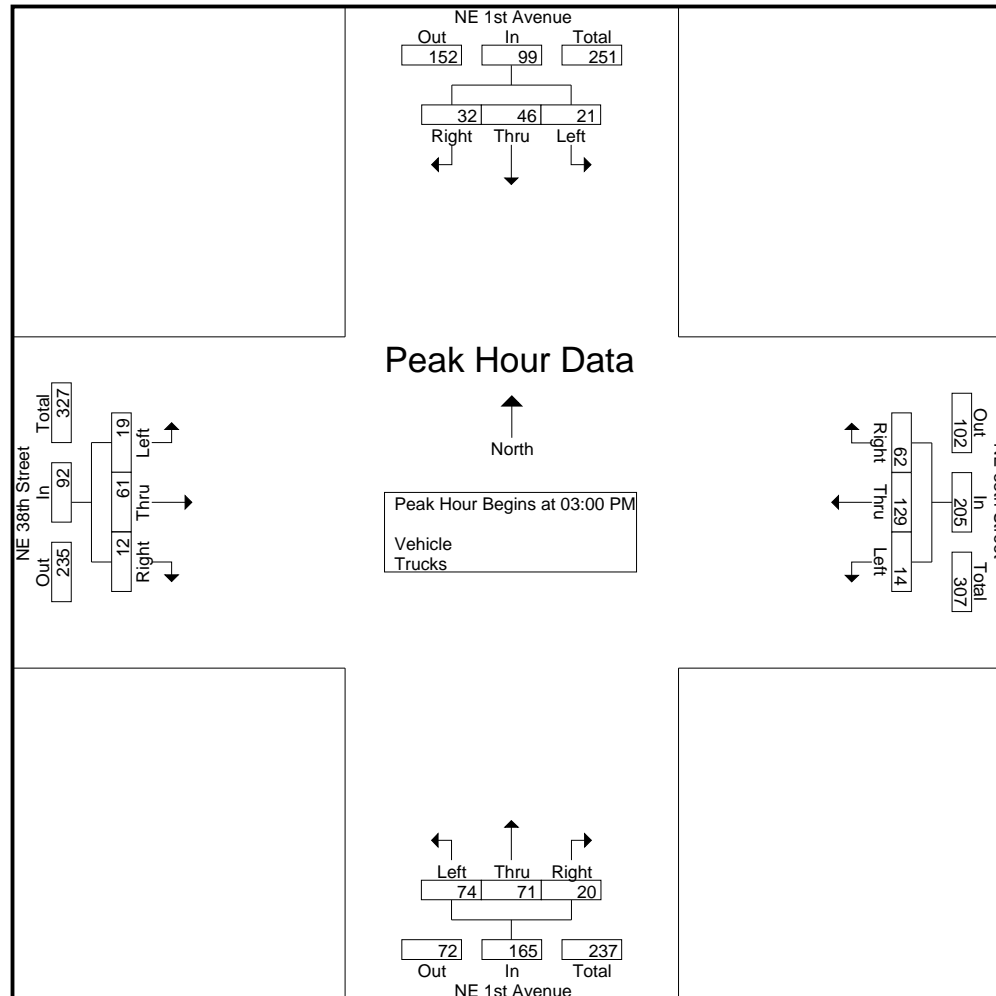
NE 1st Avenue & NE 38th Street

File Name : TMC-8 NE 1st Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Peds & Bikes

Start Time	N Federal Hwy Southbound			N Federal Hwy Northbound			NE 36th Street Westbound			NE 36th Street Eastbound			NE 2nd Avenue Southeast			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	2	1	3	1	0	1	3	1	4	1	0	1	1	0	1	10
07:15 AM	5	0	5	3	0	3	1	0	1	0	0	0	4	0	4	13
07:30 AM	3	0	3	1	1	2	4	0	4	1	0	1	1	0	1	11
07:45 AM	15	1	16	2	0	2	7	0	7	1	0	1	6	2	8	34
Total	25	2	27	7	1	8	15	1	16	3	0	3	12	2	14	68
08:00 AM	9	0	9	2	1	3	11	0	11	0	0	0	2	0	2	25
08:15 AM	1	0	1	2	2	4	0	0	0	1	0	1	0	0	0	6
08:30 AM	10	1	11	3	0	3	2	1	3	0	0	0	5	1	6	23
08:45 AM	6	0	6	2	0	2	2	0	2	2	0	2	3	0	3	15
Total	26	1	27	9	3	12	15	1	16	3	0	3	10	1	11	69
*** BREAK ***																
03:00 PM	13	0	13	10	0	10	6	0	6	1	0	1	1	0	1	31
03:15 PM	8	0	8	10	0	10	3	0	3	0	0	0	3	0	3	24
03:30 PM	13	0	13	6	0	6	3	0	3	1	0	1	1	0	1	24
03:45 PM	12	1	13	2	1	3	4	0	4	2	0	2	4	0	4	26
Total	46	1	47	28	1	29	16	0	16	4	0	4	9	0	9	105
04:00 PM	5	0	5	4	0	4	2	0	2	1	0	1	1	0	1	13
04:15 PM	8	0	8	6	0	6	3	0	3	0	0	0	3	0	3	20
04:30 PM	11	0	11	4	1	5	1	0	1	2	0	2	4	0	4	23
04:45 PM	12	0	12	6	0	6	4	1	5	0	0	0	2	1	3	26
Total	36	0	36	20	1	21	10	1	11	3	0	3	10	1	11	82
05:00 PM	0	0	0	7	0	7	0	0	0	1	0	1	1	0	1	9
05:15 PM	10	0	10	11	0	11	6	0	6	1	0	1	4	1	5	33
05:30 PM	11	0	11	11	0	11	4	0	4	0	0	0	4	1	5	31
05:45 PM	16	0	16	8	0	8	6	0	6	0	0	0	6	0	6	36
Total	37	0	37	37	0	37	16	0	16	2	0	2	15	2	17	109
Grand Total	170	4	174	101	6	107	72	3	75	15	0	15	56	6	62	433
Apprch %	97.7	2.3		94.4	5.6		96	4		100	0		90.3	9.7		
Total %	39.3	0.9	40.2	23.3	1.4	24.7	16.6	0.7	17.3	3.5	0	3.5	12.9	1.4	14.3	

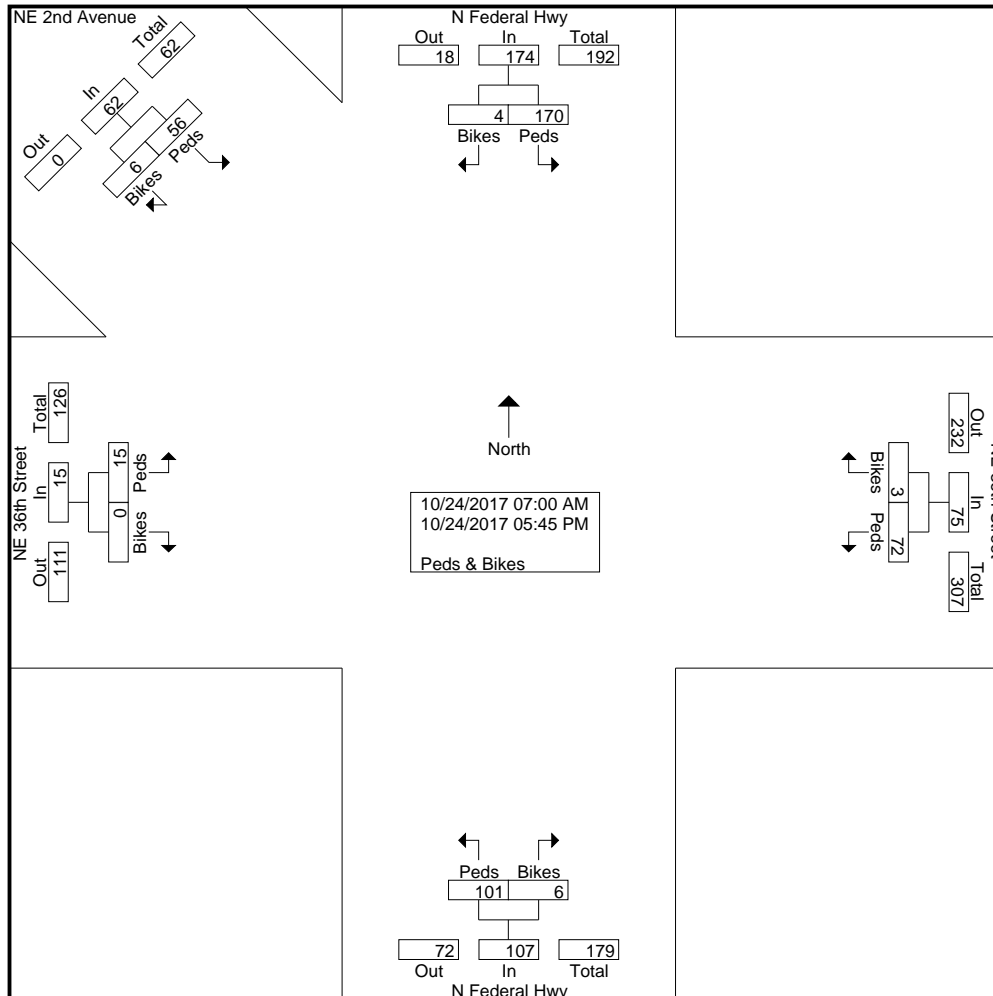
N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	N Federal Hwy Southbound			N Federal Hwy Northbound			NE 36th Street Westbound			NE 36th Street Eastbound			NE 2nd Avenue Southeast			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																
Peak Hour for Entire Intersection Begins at 07:45 AM																
07:45 AM	15	1	16	2	0	2	7	0	7	1	0	1	6	2	8	34
08:00 AM	9	0	9	2	1	3	11	0	11	0	0	0	2	0	2	25
08:15 AM	1	0	1	2	2	4	0	0	0	1	0	1	0	0	0	6
08:30 AM	10	1	11	3	0	3	2	1	3	0	0	0	5	1	6	23
Total Volume	35	2	37	9	3	12	20	1	21	2	0	2	13	3	16	88
% App. Total	94.6	5.4		75	25		95.2	4.8		100	0		81.2	18.8		
PHF	.583	.500	.578	.750	.375	.750	.455	.250	.477	.500	.000	.500	.542	.375	.500	.647

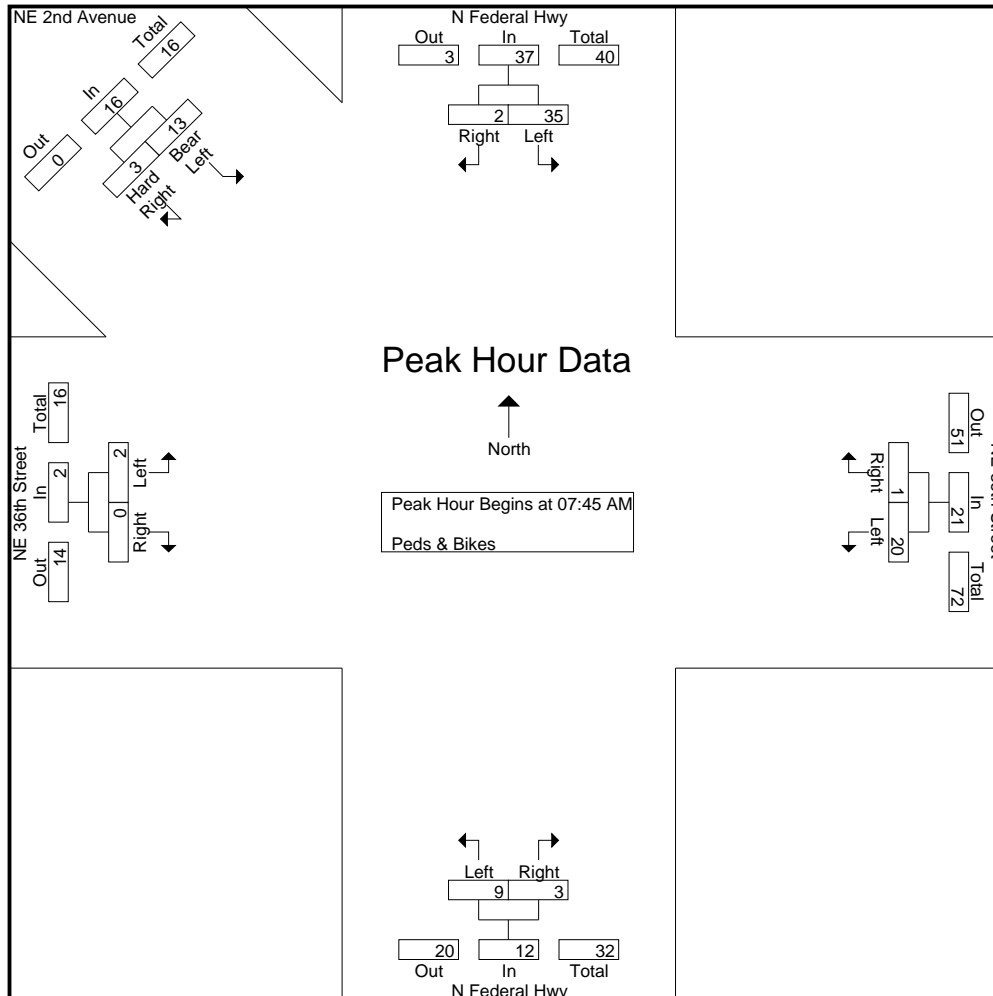
N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5

Start Time	N Federal Hwy Southbound			N Federal Hwy Northbound			NE 36th Street Westbound			NE 36th Street Eastbound			NE 2nd Avenue Southeast			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																
Peak Hour for Entire Intersection Begins at 05:00 PM																
05:00 PM	0	0	0	7	0	7	0	0	0	1	0	1	1	0	1	9
05:15 PM	10	0	10	11	0	11	6	0	6	1	0	1	4	1	5	33
05:30 PM	11	0	11	11	0	11	4	0	4	0	0	0	4	1	5	31
05:45 PM	16	0	16	8	0	8	6	0	6	0	0	0	6	0	6	36
Total Volume	37	0	37	37	0	37	16	0	16	2	0	2	15	2	17	109
% App. Total	100	0		100	0		100	0		100	0		88.2	11.8		
PHF	.578	.000	.578	.841	.000	.841	.667	.000	.667	.500	.000	.500	.625	.500	.708	.757

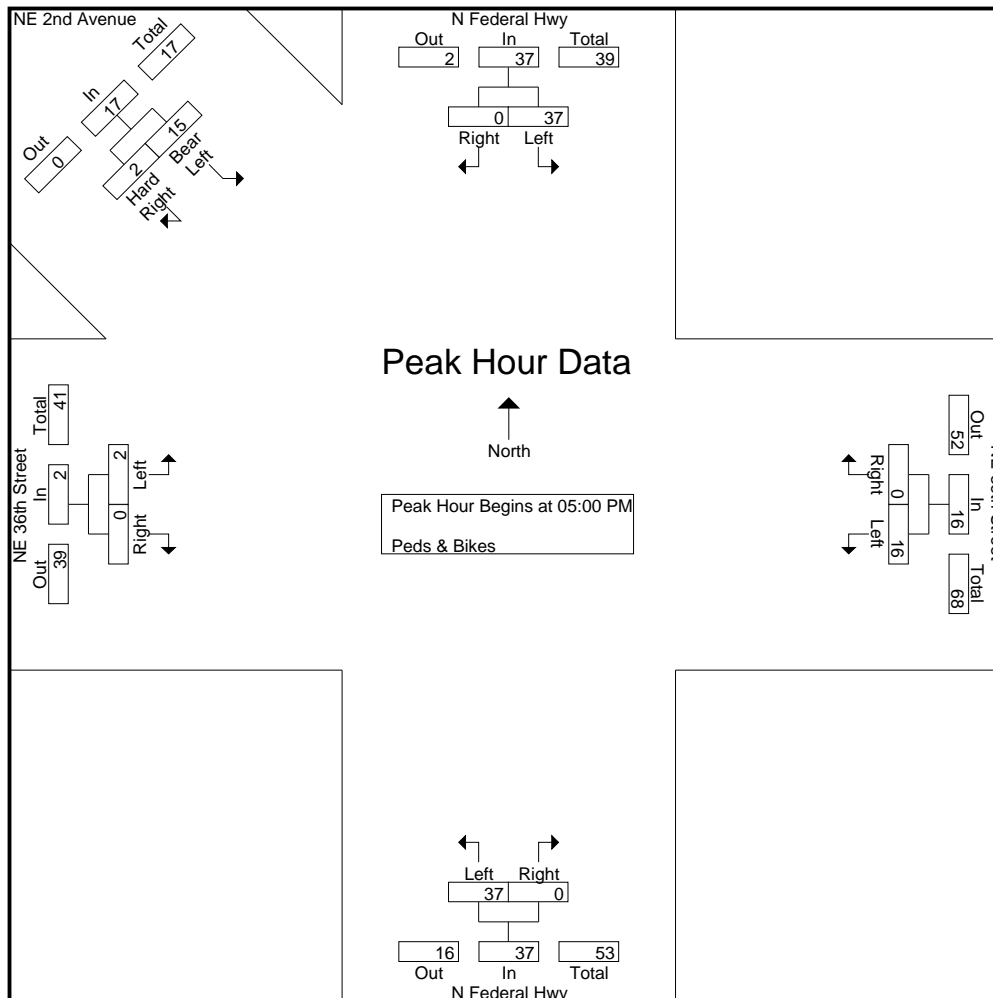
N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Trucks

Start Time	N Federal Hwy Southbound						N Federal Hwy Northbound						NE 36th Street Westbound						NE 36th Street Eastbound						NE 2nd Avenue Southeast						Int. Total
	U-Turns	Left	Thru	Right	Hard Right	App. Total	U-Turns	Left	Bear Left	Thru	Right	App. Total	U-Turns	Left	Thru	Bear Right	Right	App. Total	Hard Left	U-Turns	Left	Thru	Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	App. Total		
07:00 AM	0	0	3	0	0	3	0	0	1	0	1	2	0	0	3	0	1	4	0	0	0	3	0	3	0	0	2	0	2	14	
07:15 AM	0	0	0	0	0	0	0	0	3	1	0	4	0	0	3	0	0	3	0	0	0	3	1	4	0	0	8	0	8	19	
07:30 AM	0	0	0	0	0	0	0	1	3	1	1	6	0	0	3	1	0	4	0	0	1	4	1	6	0	2	5	0	7	23	
07:45 AM	0	0	0	0	0	0	0	0	3	0	1	4	0	0	2	0	0	2	0	0	0	5	1	6	0	1	5	0	6	18	
Total	0	0	3	0	0	3	0	1	10	2	3	16	0	0	11	1	1	13	0	0	1	15	3	19	0	3	20	0	23	74	
08:00 AM	0	2	0	0	0	2	0	0	3	0	0	3	0	0	2	1	0	3	0	0	0	5	1	6	0	0	5	1	6	20	
08:15 AM	0	0	1	0	0	1	0	0	2	0	0	2	0	0	2	0	0	2	0	0	0	4	0	4	0	1	6	2	9	18	
08:30 AM	0	0	0	0	0	0	0	0	6	1	0	7	0	0	4	0	0	4	0	0	1	6	1	8	0	0	5	2	7	26	
08:45 AM	0	0	0	0	0	0	0	0	5	0	0	5	0	0	4	0	0	4	0	0	0	4	2	6	0	1	6	0	7	22	
Total	0	2	1	0	0	3	0	0	16	1	0	17	0	0	12	1	0	13	0	0	1	19	4	24	0	2	22	5	29	86	
*** BREAK ***																															
03:00 PM	0	0	0	0	0	0	0	1	4	1	0	6	0	0	4	0	1	5	0	0	0	0	0	0	0	0	1	0	1	12	
03:15 PM	0	0	0	0	0	0	0	1	3	1	0	5	0	0	3	1	1	5	0	0	2	0	0	2	0	1	2	2	5	17	
03:30 PM	0	0	0	0	0	0	0	1	3	2	0	6	0	0	1	0	0	1	0	0	0	1	1	2	1	0	2	1	4	13	
03:45 PM	0	0	1	0	0	1	0	0	2	1	0	3	0	0	4	0	0	4	0	0	1	4	0	5	0	0	2	0	2	15	
Total	0	0	1	0	0	1	0	3	12	5	0	20	0	0	12	1	2	15	0	0	3	5	1	9	1	1	7	3	12	57	
04:00 PM	0	0	1	0	0	1	0	1	6	0	0	7	0	0	5	0	0	5	0	0	0	1	1	2	0	0	2	3	5	20	
04:15 PM	0	0	1	3	0	4	0	2	5	1	0	8	0	0	1	0	0	1	0	0	0	2	0	2	0	0	3	0	3	18	
04:30 PM	0	0	0	1	0	1	0	1	6	1	0	8	0	0	2	0	0	2	0	0	0	1	0	1	0	1	4	0	5	17	
04:45 PM	0	1	0	1	0	2	0	3	3	0	0	6	0	0	0	0	0	0	0	0	0	1	1	2	0	0	4	0	4	14	
Total	0	1	2	5	0	8	0	7	20	2	0	29	0	0	8	0	0	8	0	0	0	5	2	7	0	1	13	3	17	69	
05:00 PM	0	0	0	1	0	1	0	0	3	0	0	3	0	0	2	0	0	2	0	0	0	0	1	1	0	1	5	0	6	13	
05:15 PM	0	0	0	0	0	0	0	0	6	0	0	6	0	0	4	0	1	5	0	0	1	0	0	1	0	0	2	2	4	16	
05:30 PM	0	1	0	0	0	1	0	0	3	1	0	4	0	0	4	0	0	4	0	0	0	3	0	3	0	0	3	0	3	15	
05:45 PM	0	0	0	0	0	0	0	0	5	0	0	5	0	0	4	0	0	4	0	0	0	0	2	2	0	1	1	0	2	13	
Total	0	1	0	1	0	2	0	0	17	1	0	18	0	0	14	0	1	15	0	0	1	3	3	7	0	2	11	2	15	57	
Grand Total	0	4	7	6	0	17	0	11	75	11	3	100	0	0	57	3	4	64	0	0	6	47	13	66	1	9	73	13	96	343	
Apprch %	0	23.5	41.2	35.3	0		0	11	75	11	3		0	0	89.1	4.7	6.2		0	0	9.1	71.2	19.7		1	9.4	76	13.5			
Total %	0	1.2	2	1.7	0	5	0	3.2	21.9	3.2	0.9	29.2	0	0	16.6	0.9	1.2	18.7	0	0	1.7	13.7	3.8	19.2	0.3	2.6	21.3	3.8	28		

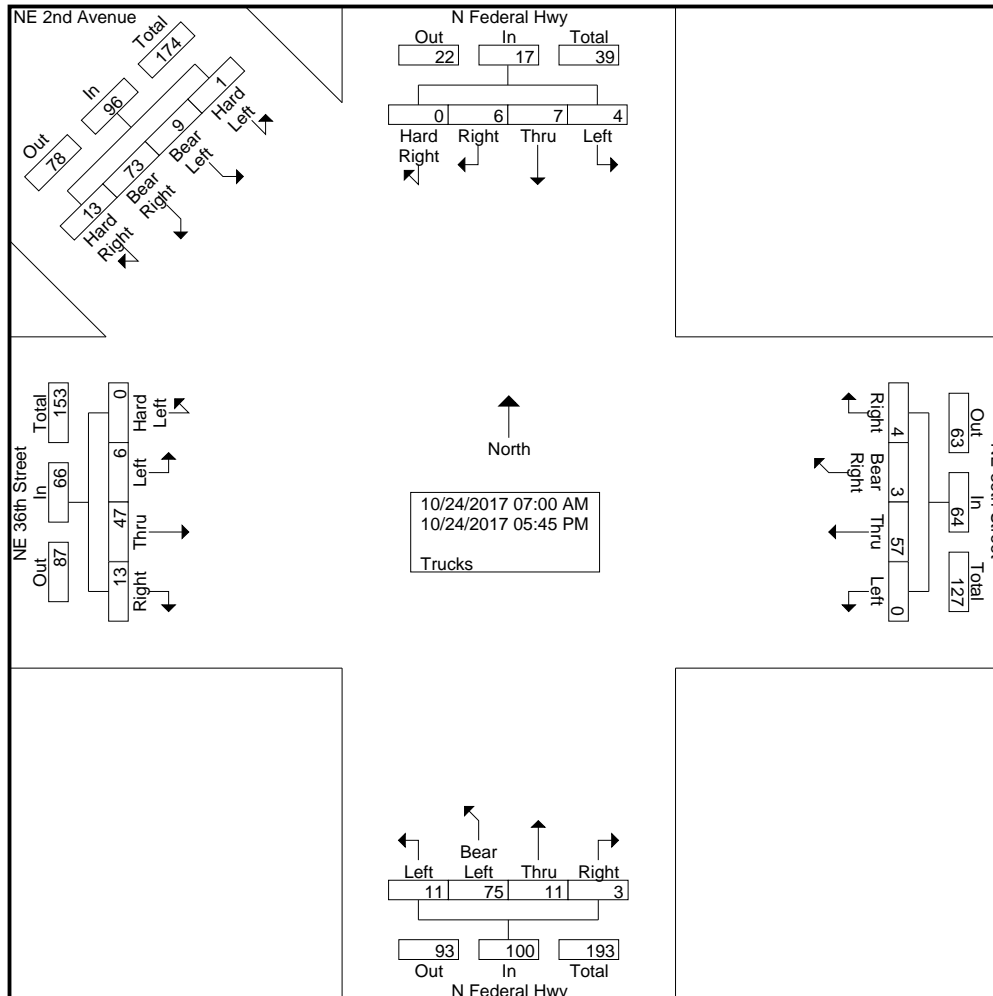
N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	N Federal Hwy Southbound						N Federal Hwy Northbound						NE 36th Street Westbound						NE 36th Street Eastbound						NE 2nd Avenue Southeast						Int. Total
	U-Turns	Left	Thru	Right	Hard Right	App. Total	U-Turns	Left	Bear Left	Thru	Right	App. Total	U-Turns	Left	Thru	Bear Right	Right	App. Total	Hard Left	U-Turns	Left	Thru	Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																															
Peak Hour for Entire Intersection Begins at 08:00 AM																															
08:00 AM	0	2	0	0	0	2	0	0	3	0	0	3	0	0	2	1	0	3	0	0	0	5	1	6	0	0	5	1	6	20	
08:15 AM	0	0	1	0	0	1	0	0	2	0	0	2	0	0	2	0	0	2	0	0	0	4	0	4	0	1	6	2	9	18	
08:30 AM	0	0	0	0	0	0	0	0	6	1	0	7	0	0	4	0	0	4	0	0	1	6	1	8	0	0	5	2	7	26	
08:45 AM	0	0	0	0	0	0	0	0	5	0	0	5	0	0	4	0	0	4	0	0	0	4	2	6	0	1	6	0	7	22	
Total Volume	0	2	1	0	0	3	0	0	16	1	0	17	0	0	12	1	0	13	0	0	1	19	4	24	0	2	22	5	29	86	
% App. Total	0	66.7	33.3	0	0		0	0	94.1	5.9	0		0	0	92.3	7.7	0		0	0	4.2	79.2	16.7		0	6.9	75.9	17.2			
PHF	.000	.250	.250	.000	.000	.375	.000	.000	.667	.250	.000	.607	.000	.000	.750	.250	.000	.813	.000	.000	.250	.792	.500	.750	.000	.500	.917	.625	.806	.827	

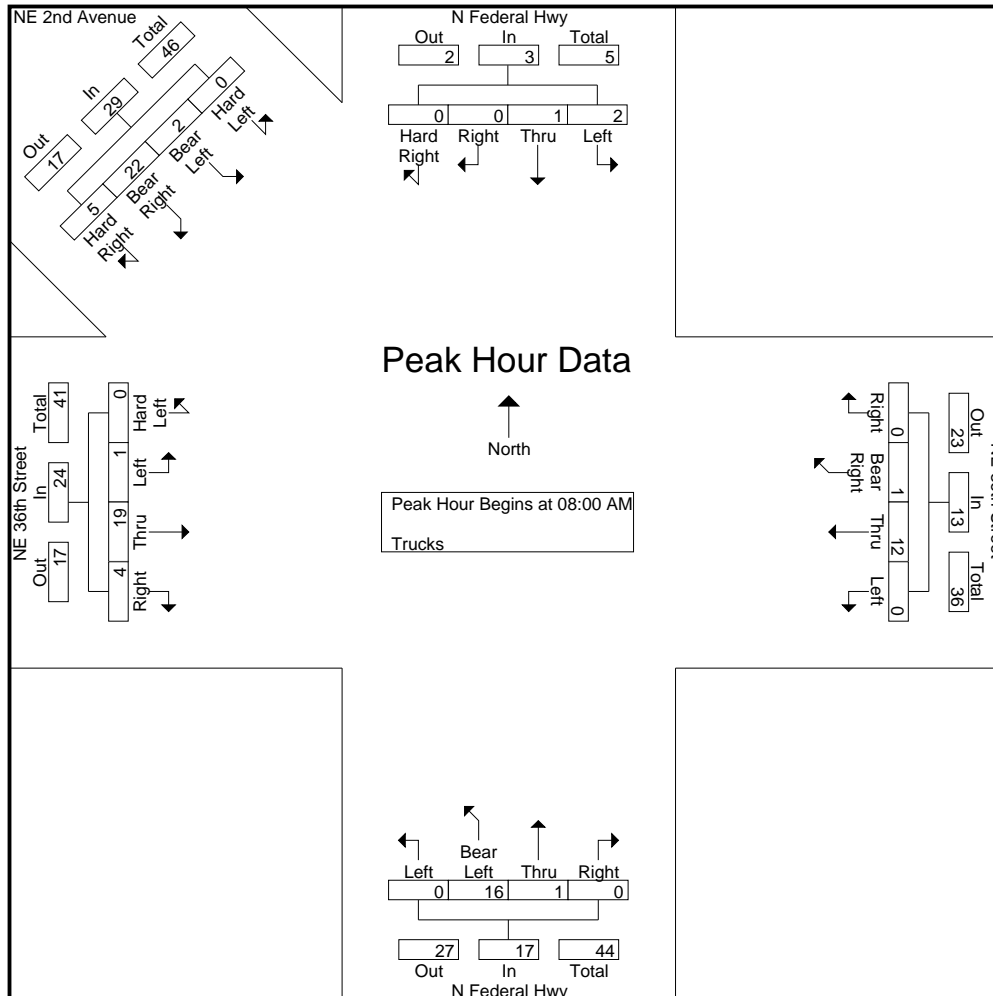
N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	N Federal Hwy Southbound						N Federal Hwy Northbound						NE 36th Street Westbound						NE 36th Street Eastbound						NE 2nd Avenue Southeast						Int. Total
	U-Turns	Left	Thru	Right	Hard Right	App. Total	U-Turns	Left	Bear Left	Thru	Right	App. Total	U-Turns	Left	Thru	Bear Right	Right	App. Total	Hard Left	U-Turns	Left	Thru	Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	App. Total		
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																															
Peak Hour for Entire Intersection Begins at 03:45 PM																															
03:45 PM	0	0	1	0	0	1	0	0	2	1	0	3	0	0	4	0	0	4	0	0	1	4	0	5	0	0	2	0	2	15	
04:00 PM	0	0	1	0	0	1	0	1	6	0	0	7	0	0	5	0	0	5	0	0	0	1	1	2	0	0	2	3	5	20	
04:15 PM	0	0	1	3	0	4	0	2	5	1	0	8	0	0	1	0	0	1	0	0	0	2	0	2	0	0	3	0	3	18	
04:30 PM	0	0	0	1	0	1	0	1	6	1	0	8	0	0	2	0	0	2	0	0	0	1	0	1	0	1	4	0	5	17	
Total Volume	0	0	3	4	0	7	0	4	19	3	0	26	0	0	12	0	0	12	0	0	1	8	1	10	0	1	11	3	15	70	
% App. Total	0	0	42.9	57.1	0		0	15.4	73.1	11.5	0		0	0	100	0	0		0	0	10	80	10		0	6.7	73.3	20			
PHF	.000	.000	.750	.333	.000	.438	.000	.500	.792	.750	.000	.813	.000	.000	.600	.000	.000	.600	.000	.000	.250	.500	.250	.500	.000	.250	.688	.250	.750	.875	

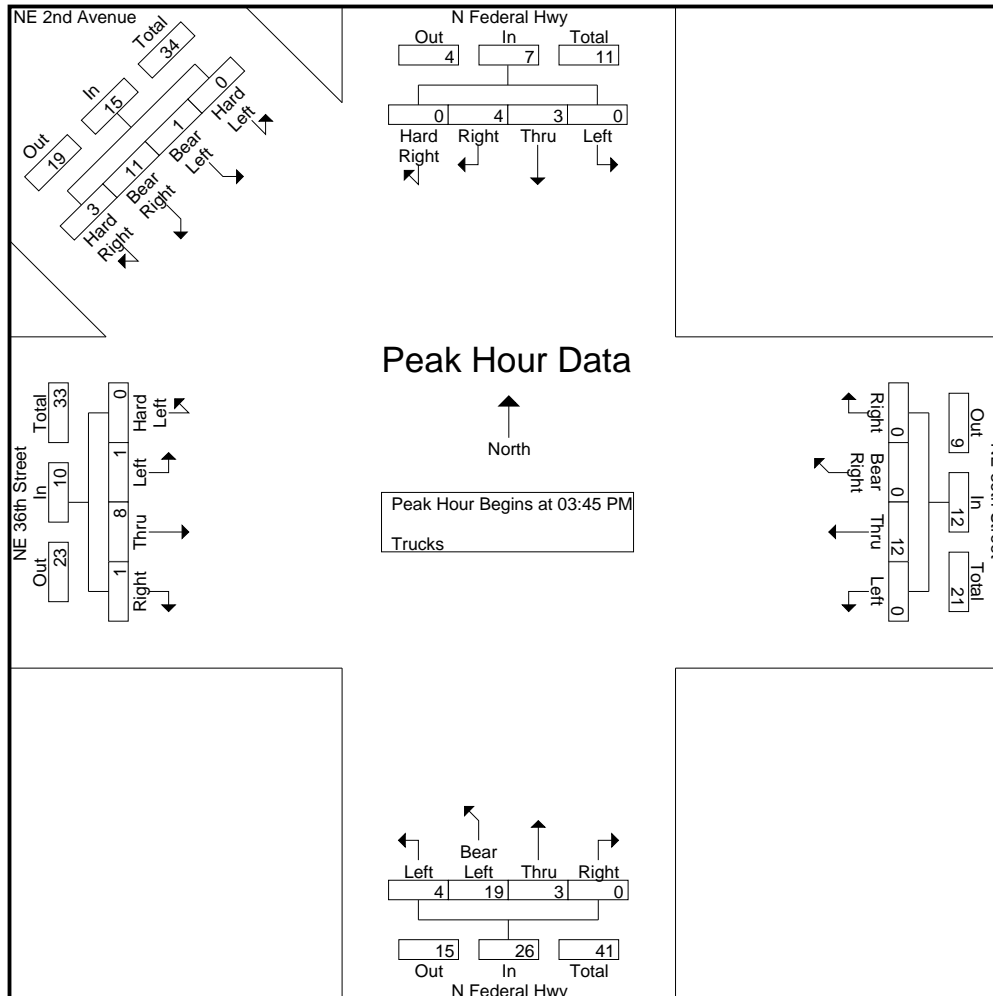
N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	N Federal Hwy Southbound						N Federal Hwy Northbound						NE 36th Street Westbound						NE 36th Street Eastbound						NE 2nd Avenue Southeast						Int. Total
	U-Turns	Left	Thru	Right	Hard Right	App. Total	U-Turns	Left	Bear Left	Thru	Right	App. Total	U-Turns	Left	Thru	Bear Right	Right	App. Total	Hard Left	U-Turns	Left	Thru	Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	App. Total		
07:00 AM	0	7	53	9	0	69	0	9	15	20	7	51	0	0	45	6	9	60	5	0	12	98	52	167	0	11	28	16	55	402	
07:15 AM	0	12	51	11	2	76	0	5	10	24	1	40	0	0	28	10	7	45	5	0	10	99	33	147	0	16	58	12	86	394	
07:30 AM	0	11	104	14	2	131	0	10	21	32	6	69	0	0	53	5	11	69	5	0	20	103	42	170	0	20	78	15	113	552	
07:45 AM	0	7	107	11	2	127	0	14	22	37	7	80	0	0	46	5	3	54	1	0	24	110	66	201	0	9	82	8	99	561	
Total	0	37	315	45	6	403	0	38	68	113	21	240	0	0	172	26	30	228	16	0	66	410	193	685	0	56	246	51	353	1909	
08:00 AM	0	7	113	13	0	133	0	18	18	45	8	89	0	1	34	10	10	55	2	0	17	98	65	182	0	13	102	12	127	586	
08:15 AM	0	8	115	8	1	132	0	12	34	25	2	73	0	1	42	2	11	56	5	0	29	117	52	203	2	14	79	11	106	570	
08:30 AM	0	10	75	19	0	104	0	14	31	36	7	88	0	0	35	6	10	51	3	0	26	109	59	197	0	14	70	13	97	537	
08:45 AM	0	10	122	16	2	150	0	18	27	44	5	94	0	0	44	5	12	61	3	0	21	85	67	176	2	15	110	15	142	623	
Total	0	35	425	56	3	519	0	62	110	150	22	344	0	2	155	23	43	223	13	0	93	409	243	758	4	56	361	51	472	2316	
*** BREAK ***																															
03:00 PM	0	8	27	22	4	61	0	15	49	48	8	120	0	0	73	4	9	86	4	0	17	73	26	120	0	20	40	19	79	466	
03:15 PM	0	5	26	28	3	62	0	14	45	65	7	131	0	0	80	18	12	110	6	0	17	59	31	113	1	18	34	20	73	489	
03:30 PM	0	12	37	32	14	95	0	29	54	98	9	190	0	0	92	9	11	112	10	0	23	78	38	149	1	8	27	20	56	602	
03:45 PM	0	10	24	23	3	60	0	34	57	149	9	249	0	3	113	12	1	129	14	0	15	89	32	150	2	16	28	13	59	647	
Total	0	35	114	105	24	278	0	92	205	360	33	690	0	3	358	43	33	437	34	0	72	299	127	532	4	62	129	72	267	2204	
04:00 PM	0	5	32	28	2	67	0	26	64	97	11	198	0	0	97	6	7	110	16	0	28	101	26	171	1	23	29	19	72	618	
04:15 PM	0	4	27	21	1	53	0	16	61	111	16	204	0	0	98	7	7	112	13	0	39	83	29	164	1	11	39	23	74	607	
04:30 PM	0	3	18	21	3	45	0	18	59	133	14	224	0	0	89	8	8	105	14	0	34	73	27	148	1	16	38	24	79	601	
04:45 PM	0	7	21	25	4	57	0	30	53	127	12	222	0	1	94	15	7	117	11	0	19	84	24	138	0	17	41	24	82	616	
Total	0	19	98	95	10	222	0	90	237	468	53	848	0	1	378	36	29	444	54	0	120	341	106	621	3	67	147	90	307	2442	
05:00 PM	0	8	25	22	0	55	0	21	68	85	8	182	0	2	93	9	7	111	13	0	36	69	37	155	3	25	50	17	95	598	
05:15 PM	0	3	33	31	1	68	0	27	67	168	10	272	0	0	103	7	11	121	9	0	24	89	15	137	0	8	42	28	78	676	
05:30 PM	0	8	22	21	3	54	0	21	57	103	19	200	0	0	90	7	11	108	13	0	37	72	21	143	0	19	45	20	84	589	
05:45 PM	0	2	30	20	4	56	0	19	70	95	12	196	0	0	89	6	9	104	13	0	26	72	39	150	0	11	41	18	70	576	
Total	0	21	110	94	8	233	0	88	262	451	49	850	0	2	375	29	38	444	48	0	123	302	112	585	3	63	178	83	327	2439	
Grand Total	0	147	1062	395	51	1655	0	370	882	1542	178	2972	0	8	1438	157	173	1776	165	0	474	1761	781	3181	14	304	1061	347	1726	11310	
Apprch %	0	8.9	64.2	23.9	3.1		0	12.4	29.7	51.9	6		0	0.5	81	8.8	9.7		5.2	0	14.9	55.4	24.6		0.8	17.6	61.5	20.1			
Total %	0	1.3	9.4	3.5	0.5	14.6	0	3.3	7.8	13.6	1.6	26.3	0	0.1	12.7	1.4	1.5	15.7	1.5	0	4.2	15.6	6.9	28.1	0.1	2.7	9.4	3.1	15.3		
Vehicle	0	143	1055	389	51	1638	0	359	807	1531	175	2872	0	8	1381	154	169	1712	165	0	468	1714	768	3115	13	295	988	334	1630	10967	
% Vehicle	0	97.3	99.3	98.5	100	99	0	97	91.5	99.3	98.3	96.6	0	100	96	98.1	97.7	96.4	100	0	98.7	97.3	98.3	97.9	92.9	97	93.1	96.3	94.4	97	

N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2

Groups Printed- Vehicle - Trucks

	N Federal Hwy Southbound						N Federal Hwy Northbound						NE 36th Street Westbound						NE 36th Street Eastbound						NE 2nd Avenue Southeast						Int. Total
	U-Turns	Left	Thru	Right	Hard Right	App. Total	U-Turns	Left	Bear Left	Thru	Right	App. Total	U-Turns	Left	Thru	Bear Right	Right	App. Total	Hard Left	U-Turns	Left	Thru	Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	App. Total		
Trucks	0	4	7	6	0	17	0	11	75	11	3	100	0	0	57	3	4	64	0	0	6	47	13	66	1	9	73	13	96	343	
% Trucks	0	2.7	0.7	1.5	0	1	0	3	8.5	0.7	1.7	3.4	0	0	4	1.9	2.3	3.6	0	0	1.3	2.7	1.7	2.1	7.1	3	6.9	3.7	5.6	3	

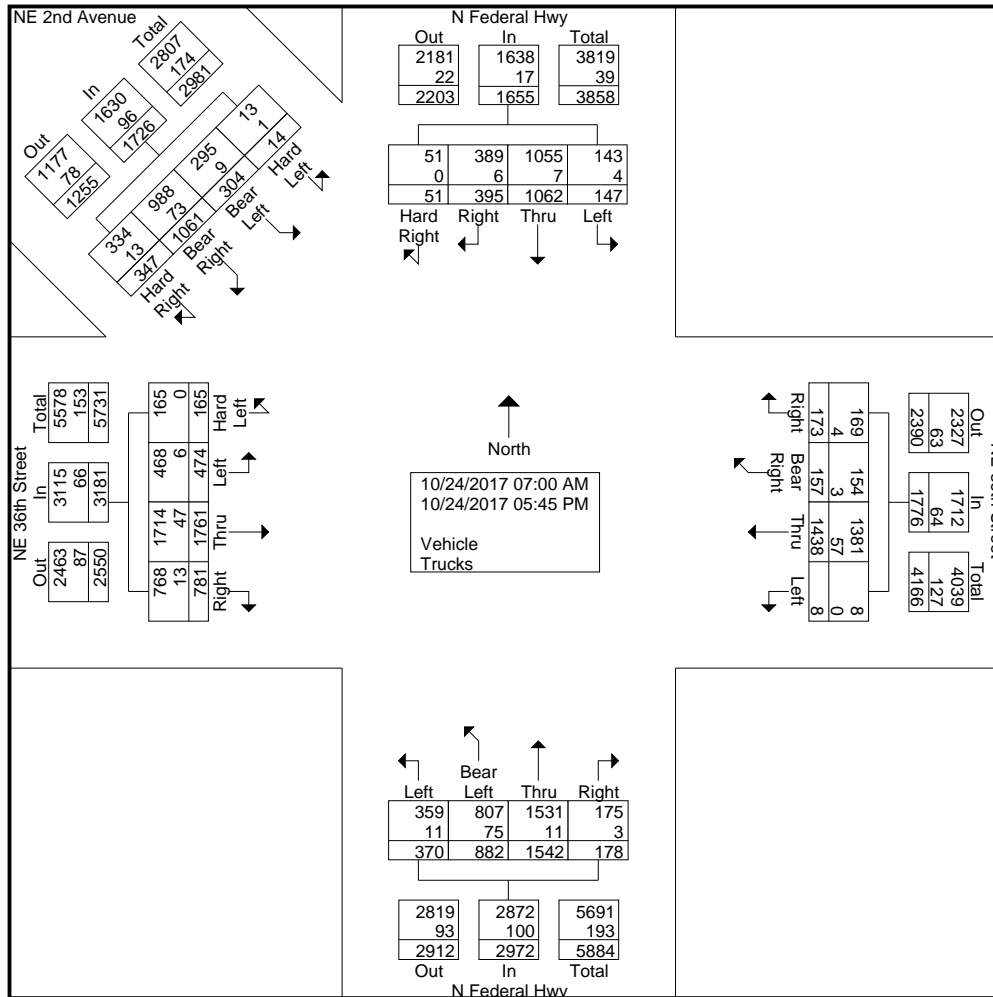
N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4

Start Time	N Federal Hwy Southbound						N Federal Hwy Northbound						NE 36th Street Westbound						NE 36th Street Eastbound						NE 2nd Avenue Southeast						Int. Total
	U-Turns	Left	Thru	Right	Hard Right	App. Total	U-Turns	Left	Bear Left	Thru	Right	App. Total	U-Turns	Left	Thru	Bear Right	Right	App. Total	Hard Left	U-Turns	Left	Thru	Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																															
Peak Hour for Entire Intersection Begins at 08:00 AM																															
08:00 AM	0	7	113	13	0	133	0	18	18	45	8	89	0	1	34	10	10	55	2	0	17	98	65	182	0	13	102	12	127	586	
08:15 AM	0	8	115	8	1	132	0	12	34	25	2	73	0	1	42	2	11	56	5	0	29	117	52	203	2	14	79	11	106	570	
08:30 AM	0	10	75	19	0	104	0	14	31	36	7	88	0	0	35	6	10	51	3	0	26	109	59	197	0	14	70	13	97	537	
08:45 AM	0	10	122	16	2	150	0	18	27	44	5	94	0	0	44	5	12	61	3	0	21	85	67	176	2	15	110	15	142	623	
Total Volume	0	35	425	56	3	519	0	62	110	150	22	344	0	2	155	23	43	223	13	0	93	409	243	758	4	56	361	51	472	2316	
% App. Total	0	6.7	81.9	10.8	0.6		0	18	32	43.6	6.4		0	0.9	69.5	10.3	19.3		1.7	0	12.3	54	32.1		0.8	11.9	76.5	10.8			
PHF	.000	.875	.871	.737	.375	.865	.000	.861	.809	.833	.688	.915	.000	.500	.881	.575	.896	.914	.650	.000	.802	.874	.907	.933	.500	.933	.820	.850	.831	.929	

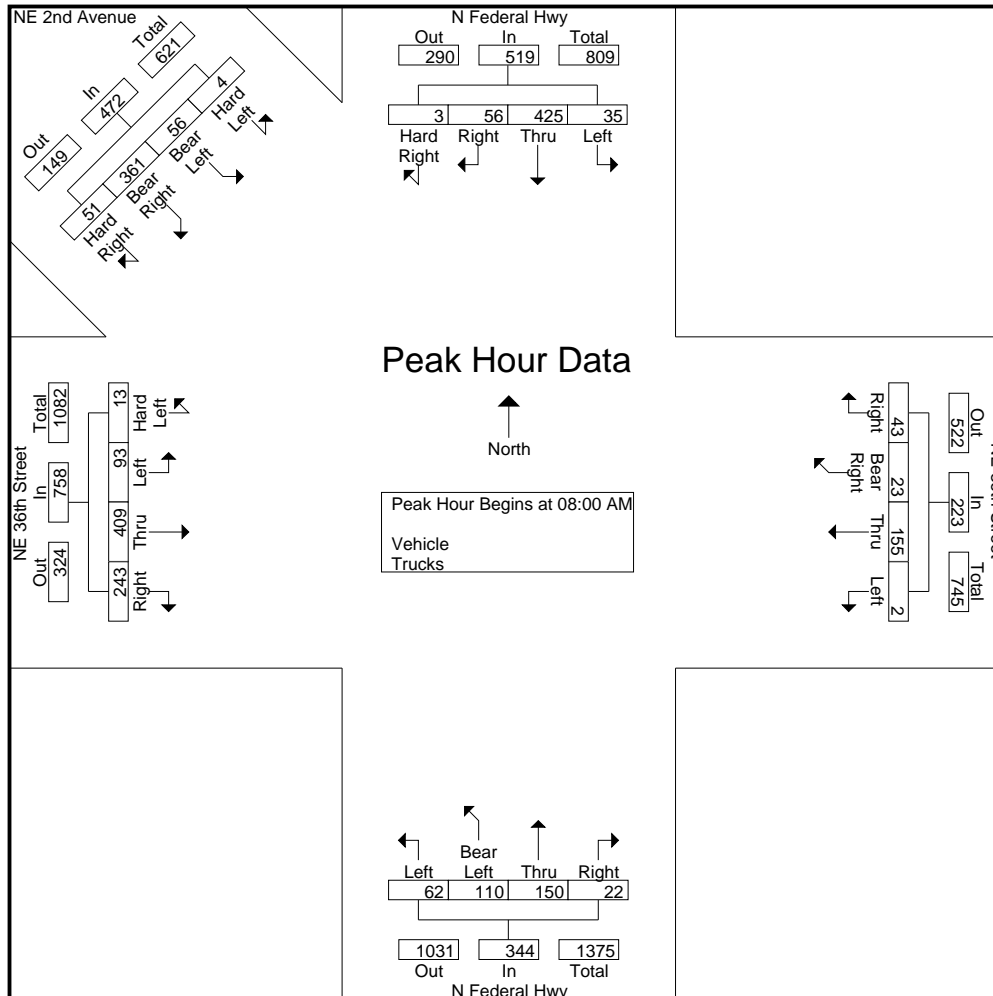
N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6

Start Time	N Federal Hwy Southbound						N Federal Hwy Northbound						NE 36th Street Westbound						NE 36th Street Eastbound						NE 2nd Avenue Southeast						Int. Total
	U-Turns	Left	Thru	Right	Hard Right	App. Total	U-Turns	Left	Bear Left	Thru	Right	App. Total	U-Turns	Left	Thru	Bear Right	Right	App. Total	Hard Left	U-Turns	Left	Thru	Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	App. Total		
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																															
Peak Hour for Entire Intersection Begins at 04:30 PM																															
04:30 PM	0	3	18	21	3	45	0	18	59	133	14	224	0	0	89	8	8	105	14	0	34	73	27	148	1	16	38	24	79	601	
04:45 PM	0	7	21	25	4	57	0	30	53	127	12	222	0	1	94	15	7	117	11	0	19	84	24	138	0	17	41	24	82	616	
05:00 PM	0	8	25	22	0	55	0	21	68	85	8	182	0	2	93	9	7	111	13	0	36	69	37	155	3	25	50	17	95	598	
05:15 PM	0	3	33	31	1	68	0	27	67	168	10	272	0	0	103	7	11	121	9	0	24	89	15	137	0	8	42	28	78	676	
Total Volume	0	21	97	99	8	225	0	96	247	513	44	900	0	3	379	39	33	454	47	0	113	315	103	578	4	66	171	93	334	2491	
% App. Total	0	9.3	43.1	44	3.6		0	10.7	27.4	57	4.9		0	0.7	83.5	8.6	7.3		8.1	0	19.6	54.5	17.8		1.2	19.8	51.2	27.8			
PHF	.000	.656	.735	.798	.500	.827	.000	.800	.908	.763	.786	.827	.000	.375	.920	.650	.750	.938	.839	.000	.785	.885	.696	.932	.333	.660	.855	.830	.879	.921	

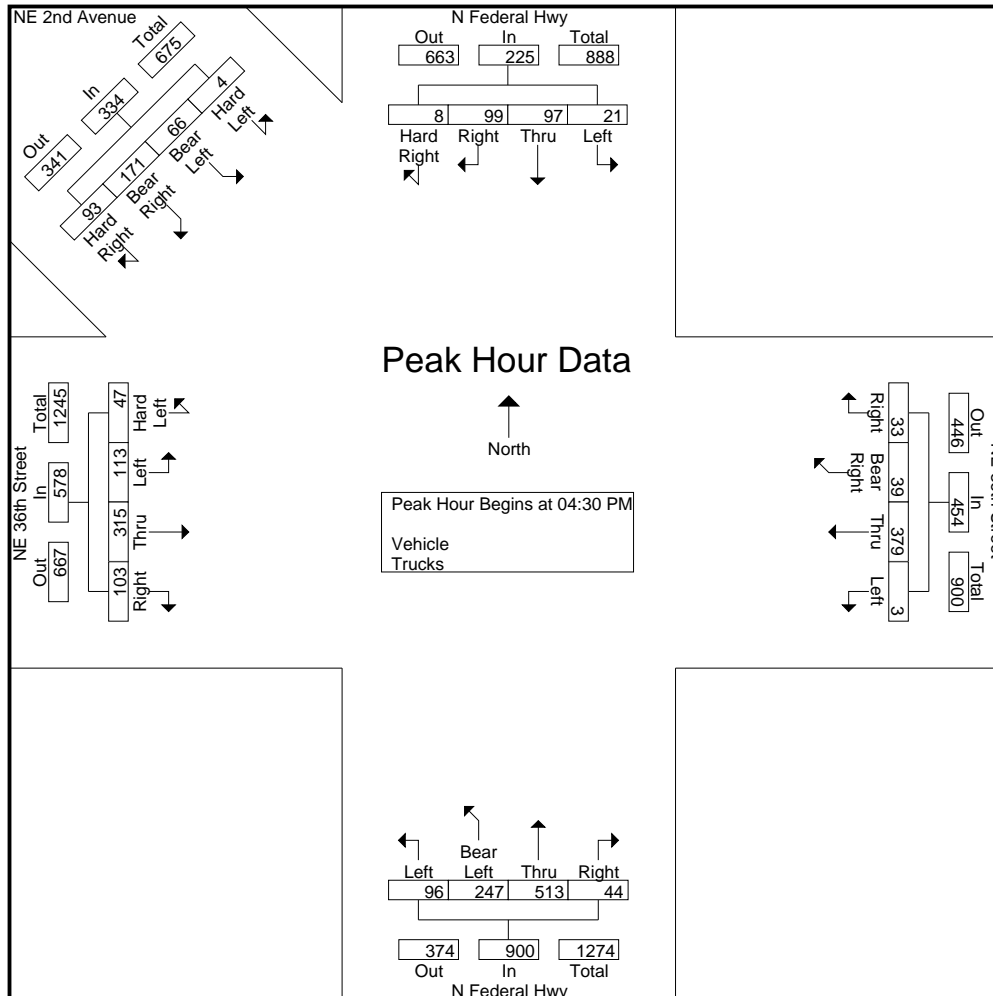
N Federal Hwy/NE 2nd Avenue & NE 36th Street

File Name : TMC-9 N Federal Hwy-NE 2nd Av & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	NE 2nd Avenue Southbound			NE 2nd Avenue Northbound			Driveway Westbound			NE 38th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	2	0	2	0	0	0	0	0	0	1	4	5	7
07:15 AM	0	0	0	0	0	0	0	0	0	4	0	4	4
07:30 AM	0	0	0	0	0	0	0	0	0	1	0	1	1
07:45 AM	0	0	0	0	0	0	0	0	0	15	1	16	16
Total	2	0	2	0	0	0	0	0	0	21	5	26	28
08:00 AM	1	0	1	1	0	1	0	0	0	3	0	3	5
08:15 AM	0	0	0	0	0	0	0	0	0	1	2	3	3
08:30 AM	2	0	2	0	0	0	0	0	0	0	0	0	2
*** BREAK ***													
Total	3	0	3	1	0	1	0	0	0	4	2	6	10
*** BREAK ***													
03:00 PM	0	0	0	1	0	1	0	0	0	9	1	10	11
03:15 PM	0	0	0	0	0	0	0	0	0	7	1	8	8
03:30 PM	1	0	1	1	0	1	0	0	0	7	2	9	11
03:45 PM	3	0	3	0	0	0	0	0	0	9	2	11	14
Total	4	0	4	2	0	2	0	0	0	32	6	38	44
04:00 PM	0	0	0	0	0	0	0	0	0	7	1	8	8
04:15 PM	4	0	4	0	0	0	0	5	5	7	0	7	16
04:30 PM	1	0	1	0	0	0	0	0	0	4	2	6	7
04:45 PM	0	0	0	1	0	1	0	0	0	10	2	12	13
Total	5	0	5	1	0	1	0	5	5	28	5	33	44
*** BREAK ***													
05:15 PM	0	0	0	0	1	1	0	0	0	0	1	1	2
*** BREAK ***													
05:45 PM	1	0	1	0	0	0	0	0	0	6	4	10	11
Total	1	0	1	0	1	1	0	0	0	6	5	11	13
Grand Total	15	0	15	4	1	5	0	5	5	91	23	114	139
Apprch %	100	0		80	20		0	100		79.8	20.2		
Total %	10.8	0	10.8	2.9	0.7	3.6	0	3.6	3.6	65.5	16.5	82	

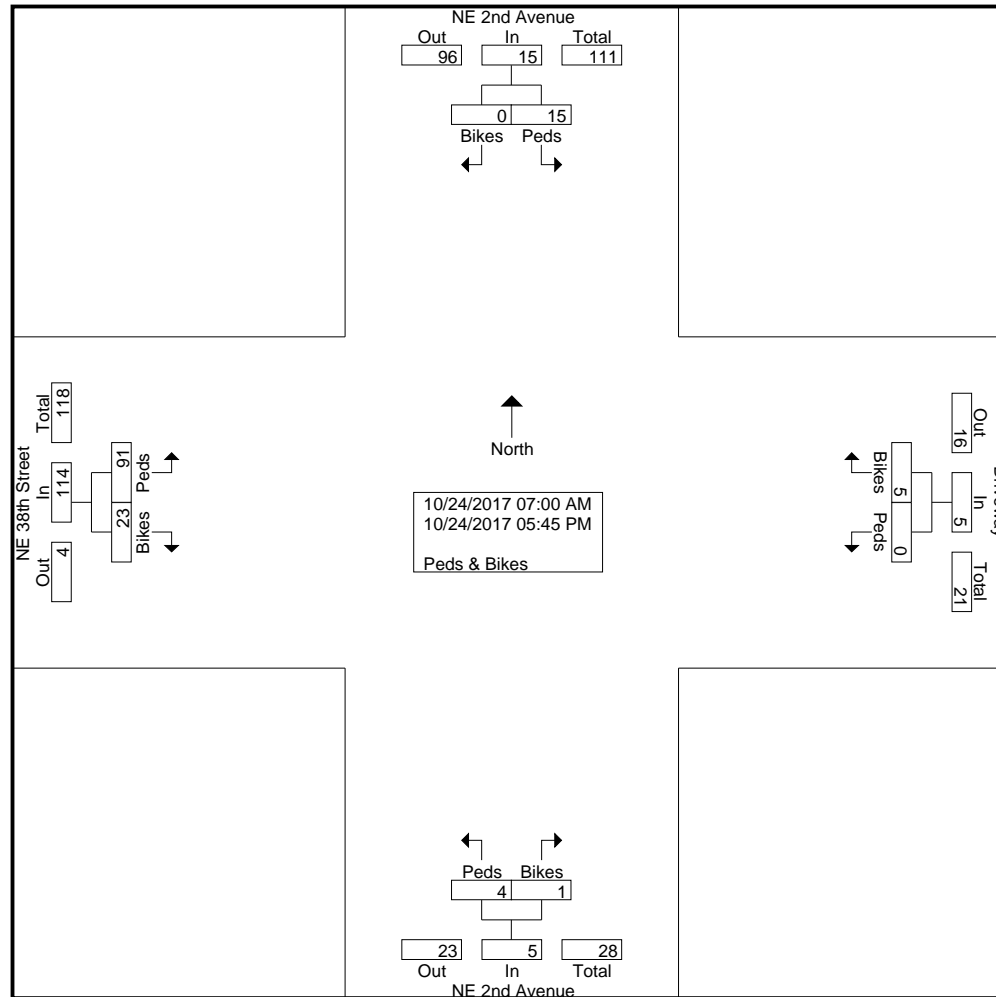
NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NE 2nd Avenue Southbound			NE 2nd Avenue Northbound			Driveway Westbound			NE 38th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:00 AM													
07:00 AM	2	0	2	0	0	0	0	0	0	1	4	5	7
07:15 AM	0	0	0	0	0	0	0	0	0	4	0	4	4
07:30 AM	0	0	0	0	0	0	0	0	0	1	0	1	1
07:45 AM	0	0	0	0	0	0	0	0	0	15	1	16	16
Total Volume	2	0	2	0	0	0	0	0	0	21	5	26	28
% App. Total	100	0		0	0		0	0		80.8	19.2		
PHF	.250	.000	.250	.000	.000	.000	.000	.000	.000	.350	.313	.406	.438

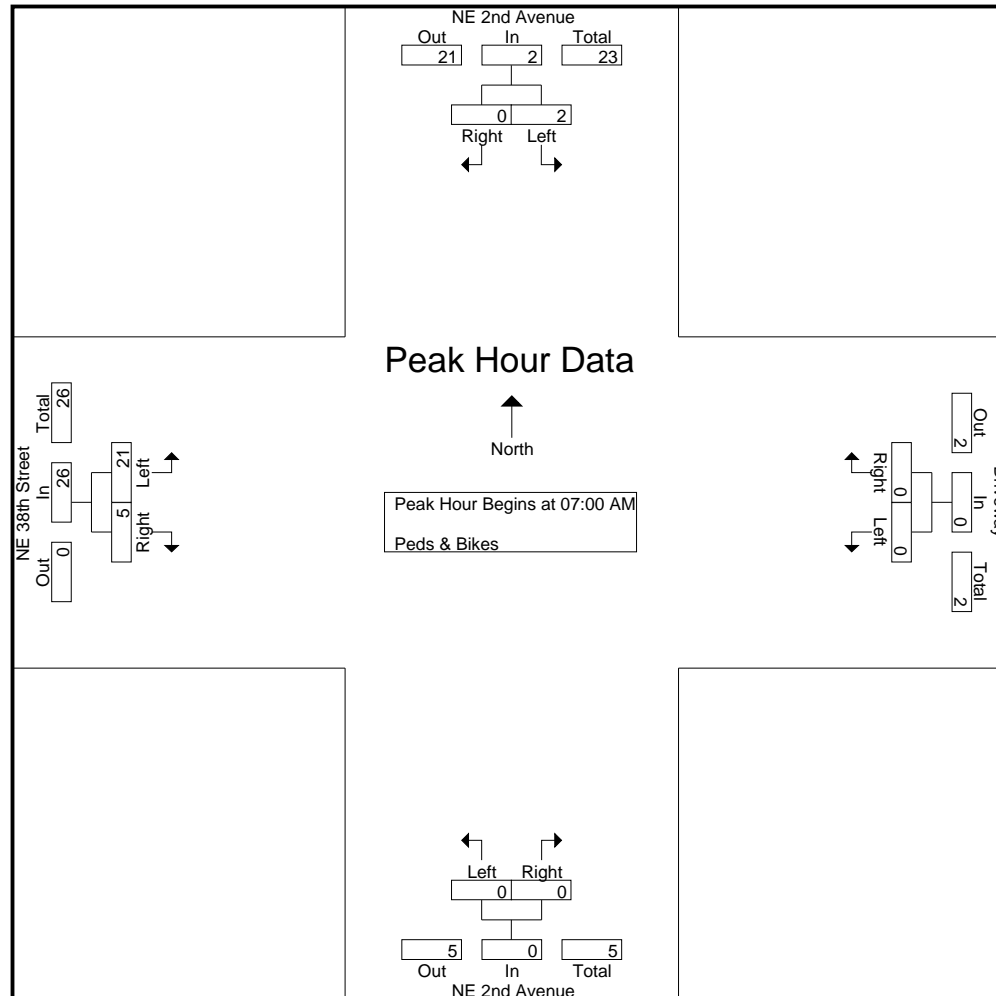
NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	NE 2nd Avenue Southbound			NE 2nd Avenue Northbound			Driveway Westbound			NE 38th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 03:30 PM													
03:30 PM	1	0	1	1	0	1	0	0	0	7	2	9	11
03:45 PM	3	0	3	0	0	0	0	0	0	9	2	11	14
04:00 PM	0	0	0	0	0	0	0	0	0	7	1	8	8
04:15 PM	4	0	4	0	0	0	0	5	5	7	0	7	16
Total Volume	8	0	8	1	0	1	0	5	5	30	5	35	49
% App. Total	100	0		100	0		0	100		85.7	14.3		
PHF	.500	.000	.500	.250	.000	.250	.000	.250	.250	.833	.625	.795	.766

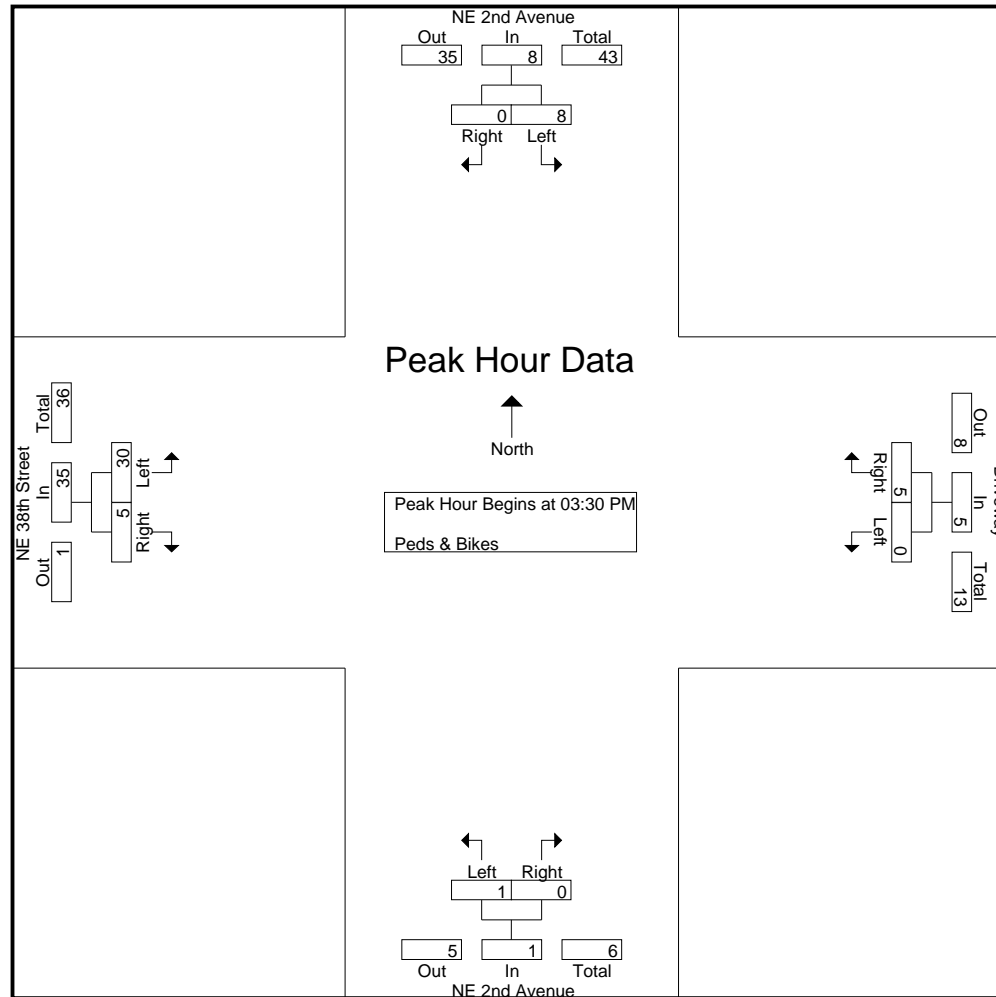
NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Trucks

Start Time	NE 2nd Avenue Southbound					NE 2nd Avenue Northbound					Driveway Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	3	0	3	0	0	5	0	5	0	0	0	0	0	0	0	0	0	0	8
07:15 AM	0	0	3	1	4	0	1	3	0	4	0	0	0	0	0	0	0	0	0	0	8
07:30 AM	0	0	5	0	5	0	2	5	0	7	0	0	0	0	0	0	2	0	0	2	14
07:45 AM	0	0	5	0	5	0	1	8	0	9	0	0	0	0	0	0	0	0	0	0	14
Total	0	0	16	1	17	0	4	21	0	25	0	0	0	0	0	0	2	0	0	2	44
08:00 AM	0	0	9	0	9	0	1	3	0	4	0	0	0	0	0	0	0	0	0	0	13
08:15 AM	0	0	5	0	5	0	3	6	0	9	0	0	0	0	0	0	0	0	1	1	15
08:30 AM	0	0	5	1	6	0	0	4	0	4	0	0	0	0	0	0	0	0	1	1	11
08:45 AM	0	0	4	1	5	0	1	4	0	5	0	0	0	0	0	0	0	0	0	0	10
Total	0	0	23	2	25	0	5	17	0	22	0	0	0	0	0	0	0	0	2	2	49
*** BREAK ***																					
03:00 PM	0	0	3	2	5	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	7
03:15 PM	0	0	3	0	3	0	2	7	0	9	0	0	0	0	0	0	0	0	0	0	12
03:30 PM	0	0	1	0	1	0	0	5	0	5	0	0	0	0	0	0	2	0	1	3	9
03:45 PM	0	0	4	1	5	0	1	4	0	5	0	0	0	0	0	0	0	0	0	0	10
Total	0	0	11	3	14	0	4	16	0	20	0	0	0	0	0	0	3	0	1	4	38
04:00 PM	0	0	2	0	2	0	0	4	0	4	0	0	0	0	0	0	2	0	0	2	8
04:15 PM	0	0	8	0	8	0	1	2	0	3	0	0	0	0	0	0	0	0	0	0	11
04:30 PM	0	0	1	1	2	0	1	2	0	3	0	0	0	0	0	0	1	0	0	1	6
04:45 PM	0	0	5	0	5	0	0	3	0	3	0	0	0	0	0	0	1	0	0	1	9
Total	0	0	16	1	17	0	2	11	0	13	0	0	0	0	0	0	4	0	0	4	34
05:00 PM	0	0	2	0	2	0	1	5	0	6	0	0	0	0	0	0	1	0	0	1	9
05:15 PM	0	0	0	2	2	0	2	4	0	6	0	0	0	0	0	0	0	0	0	0	8
05:30 PM	0	0	2	1	3	0	0	1	0	1	0	0	0	0	0	0	0	0	1	1	5
05:45 PM	0	0	1	1	2	0	0	4	0	4	0	0	0	0	0	0	0	0	0	0	6
Total	0	0	5	4	9	0	3	14	0	17	0	0	0	0	0	0	1	0	1	2	28
Grand Total	0	0	71	11	82	0	18	79	0	97	0	0	0	0	0	0	10	0	4	14	193
Apprch %	0	0	86.6	13.4		0	18.6	81.4	0		0	0	0	0		0	71.4	0	28.6		
Total %	0	0	36.8	5.7	42.5	0	9.3	40.9	0	50.3	0	0	0	0	0	0	5.2	0	2.1	7.3	

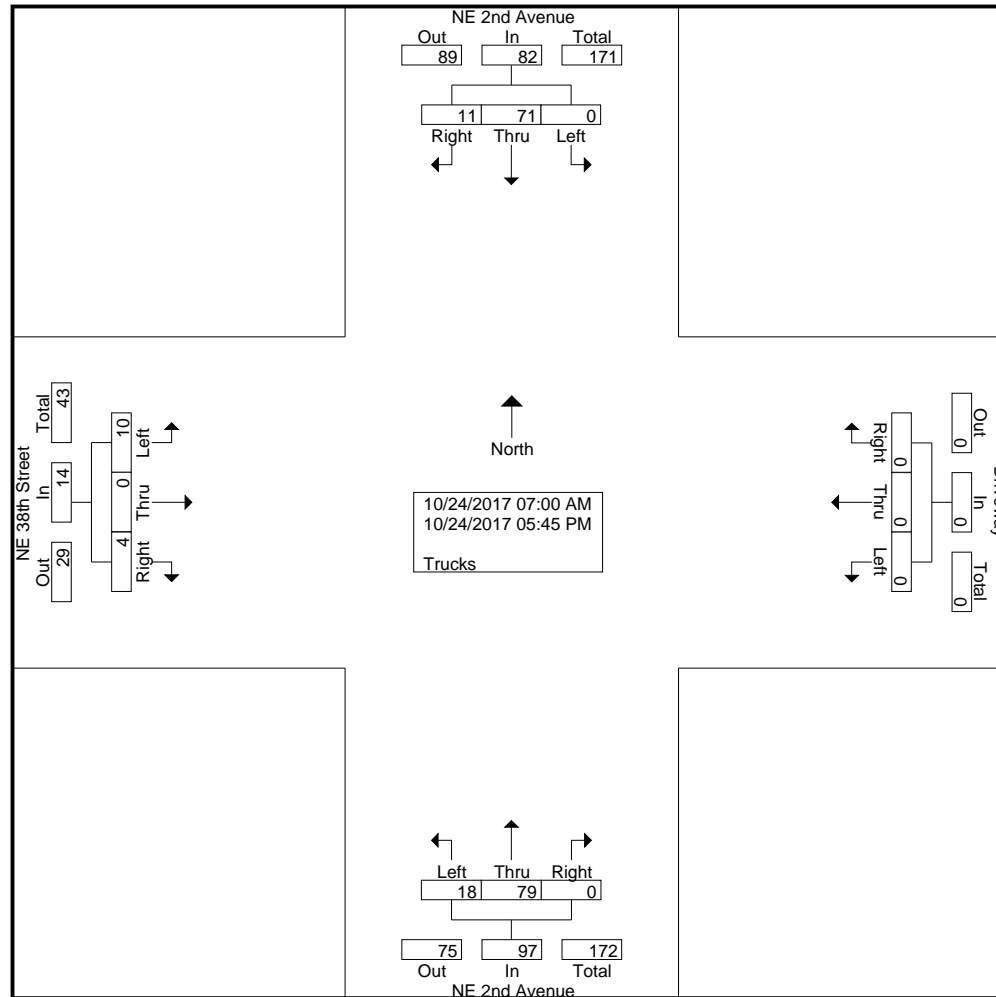
NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NE 2nd Avenue Southbound					NE 2nd Avenue Northbound					Driveway Westbound					NE 38th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 07:30 AM																						
07:30 AM	0	0	5	0	5	0	2	5	0	7	0	0	0	0	0	0	2	0	0	2	14	
07:45 AM	0	0	5	0	5	0	1	8	0	9	0	0	0	0	0	0	0	0	0	0	14	
08:00 AM	0	0	9	0	9	0	1	3	0	4	0	0	0	0	0	0	0	0	0	0	13	
08:15 AM	0	0	5	0	5	0	3	6	0	9	0	0	0	0	0	0	0	0	1	1	15	
Total Volume	0	0	24	0	24	0	7	22	0	29	0	0	0	0	0	0	2	0	1	3	56	
% App. Total	0	0	100	0		0	24.1	75.9	0		0	0	0	0		0	66.7	0	33.3			
PHF	.000	.000	.667	.000	.667	.000	.583	.688	.000	.806	.000	.000	.000	.000	.000	.000	.250	.000	.250	.375	.933	

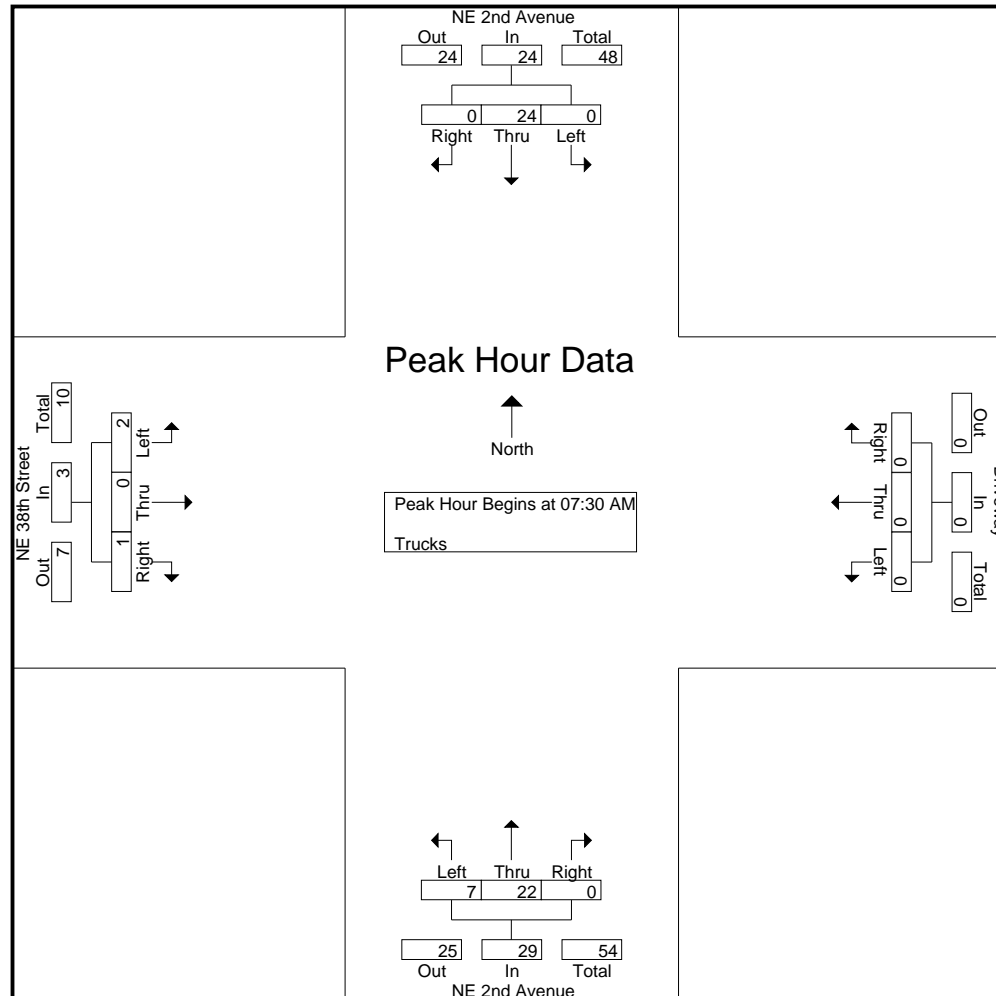
NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5

Start Time	NE 2nd Avenue Southbound					NE 2nd Avenue Northbound					Driveway Westbound					NE 38th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 03:15 PM																						
03:15 PM	0	0	3	0	3	0	2	7	0	9	0	0	0	0	0	0	0	0	0	0	0	12
03:30 PM	0	0	1	0	1	0	0	5	0	5	0	0	0	0	0	0	2	0	1	3	9	
03:45 PM	0	0	4	1	5	0	1	4	0	5	0	0	0	0	0	0	0	0	0	0	10	
04:00 PM	0	0	2	0	2	0	0	4	0	4	0	0	0	0	0	0	2	0	0	2	8	
Total Volume	0	0	10	1	11	0	3	20	0	23	0	0	0	0	0	0	4	0	1	5	39	
% App. Total	0	0	90.9	9.1		0	13	87	0		0	0	0	0		0	80	0	20			
PHF	.000	.000	.625	.250	.550	.000	.375	.714	.000	.639	.000	.000	.000	.000	.000	.000	.500	.000	.250	.417	.813	

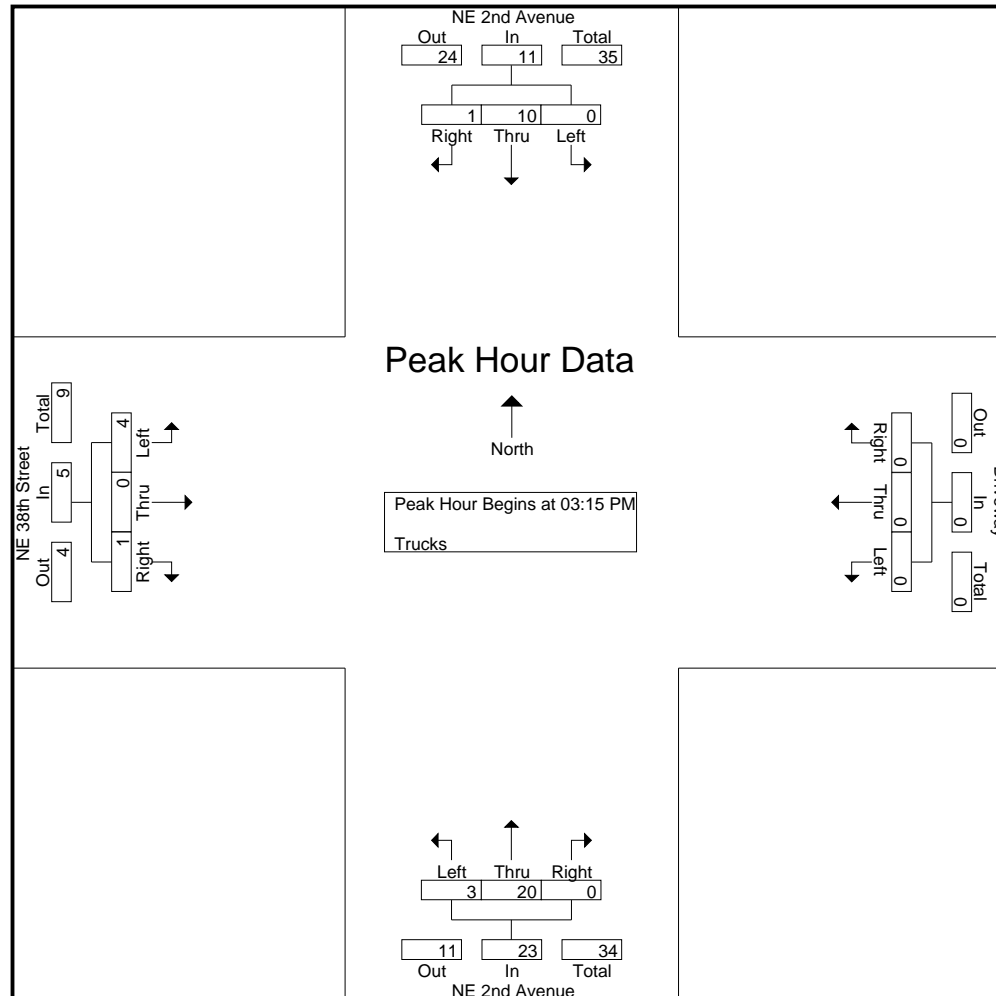
NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	NE 2nd Avenue Southbound					NE 2nd Avenue Northbound					Driveway Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	56	12	68	0	1	26	0	27	0	0	1	0	1	0	16	0	2	18	114
07:15 AM	0	0	77	15	92	0	3	22	0	25	0	0	1	0	1	0	33	2	4	39	157
07:30 AM	0	0	87	19	106	0	7	31	0	38	0	0	0	0	0	0	28	0	5	33	177
07:45 AM	0	0	113	23	136	0	3	31	0	34	0	0	0	0	0	0	17	0	1	18	188
Total	0	0	333	69	402	0	14	110	0	124	0	0	2	0	2	0	94	2	12	108	636
08:00 AM	0	0	109	33	142	0	8	21	0	29	0	0	0	0	0	0	9	0	5	14	185
08:15 AM	0	0	78	12	90	0	6	39	0	45	0	0	0	0	0	0	15	0	10	25	160
08:30 AM	0	0	103	36	139	0	5	32	0	37	0	0	0	0	0	0	16	0	11	27	203
08:45 AM	0	0	93	27	120	0	5	37	0	42	0	0	0	0	0	0	28	1	9	38	200
Total	0	0	383	108	491	0	24	129	0	153	0	0	0	0	0	0	68	1	35	104	748
*** BREAK ***																					
03:00 PM	0	0	57	32	89	0	8	52	0	60	0	0	0	0	0	0	25	0	6	31	180
03:15 PM	0	0	68	18	86	0	9	55	0	64	0	0	0	0	0	0	40	0	12	52	202
03:30 PM	0	2	64	22	88	0	13	66	0	79	0	0	0	0	0	0	32	1	14	47	214
03:45 PM	0	0	78	22	100	0	5	75	0	80	0	0	0	0	0	0	30	1	8	39	219
Total	0	2	267	94	363	0	35	248	0	283	0	0	0	0	0	0	127	2	40	169	815
04:00 PM	0	0	55	13	68	0	6	74	0	80	0	0	2	0	2	0	21	0	6	27	177
04:15 PM	0	0	67	14	81	0	4	68	0	72	0	0	0	0	0	0	21	0	11	32	185
04:30 PM	0	0	48	19	67	0	7	69	0	76	0	0	0	0	0	0	41	0	8	49	192
04:45 PM	0	0	46	7	53	0	5	84	0	89	0	0	0	0	0	0	23	6	12	41	183
Total	0	0	216	53	269	0	22	295	0	317	0	0	2	0	2	0	106	6	37	149	737
05:00 PM	0	0	57	14	71	0	7	80	0	87	0	0	0	0	0	0	40	0	14	54	212
05:15 PM	0	0	40	19	59	0	7	87	0	94	0	0	0	0	0	0	41	0	13	54	207
05:30 PM	0	0	51	20	71	0	5	68	0	73	0	0	0	0	0	0	33	0	2	35	179
05:45 PM	0	0	46	13	59	0	3	81	0	84	0	0	0	0	0	0	25	0	6	31	174
Total	0	0	194	66	260	0	22	316	0	338	0	0	0	0	0	0	139	0	35	174	772
Grand Total	0	2	1393	390	1785	0	117	1098	0	1215	0	0	4	0	4	0	534	11	159	704	3708
Apprch %	0	0.1	78	21.8		0	9.6	90.4	0		0	0	100	0		0	75.9	1.6	22.6		
Total %	0	0.1	37.6	10.5	48.1	0	3.2	29.6	0	32.8	0	0	0.1	0	0.1	0	14.4	0.3	4.3	19	
Vehicle	0	2	1322	379	1703	0	99	1019	0	1118	0	0	4	0	4	0	524	11	155	690	3515
% Vehicle	0	100	94.9	97.2	95.4	0	84.6	92.8	0	92	0	0	100	0	100	0	98.1	100	97.5	98	94.8

NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	NE 2nd Avenue Southbound					NE 2nd Avenue Northbound					Driveway Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	0	71	11	82	0	18	79	0	97	0	0	0	0	0	0	10	0	4	14	193
% Trucks	0	0	5.1	2.8	4.6	0	15.4	7.2	0	8	0	0	0	0	0	0	1.9	0	2.5	2	5.2

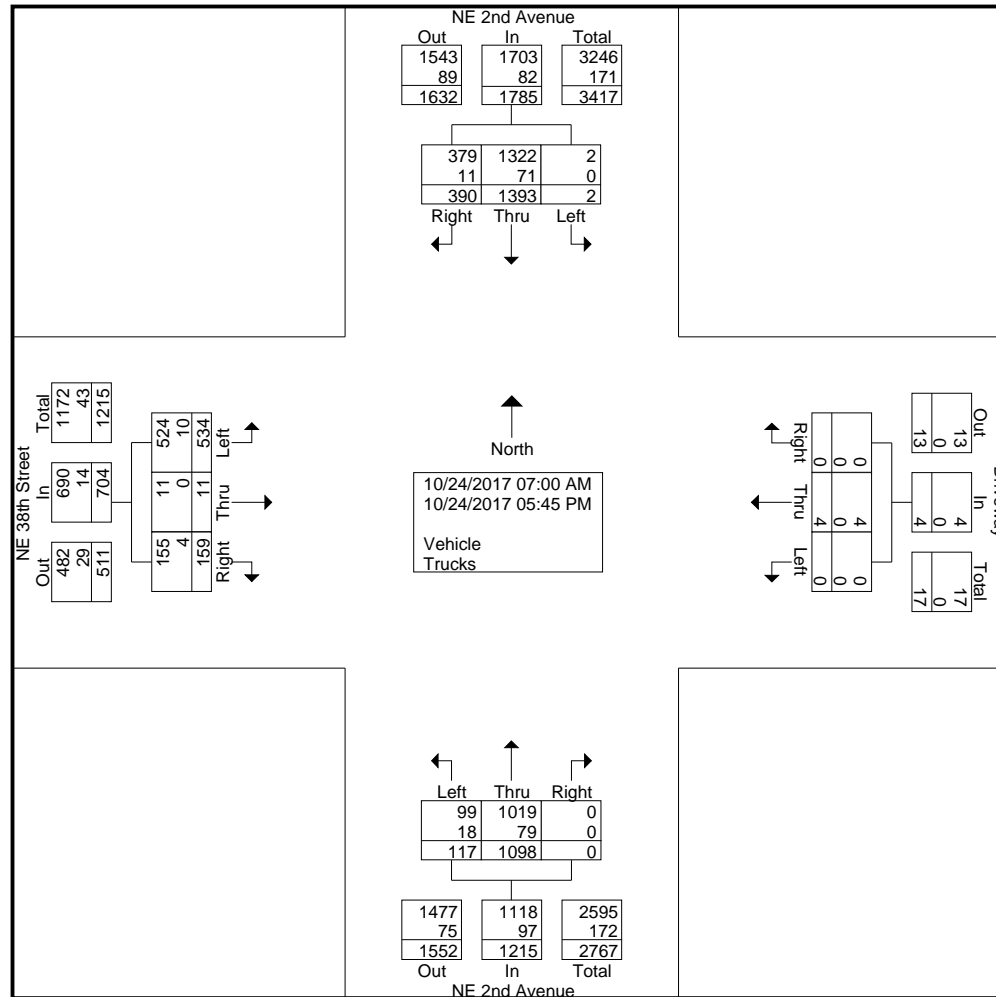
NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4

Start Time	NE 2nd Avenue Southbound					NE 2nd Avenue Northbound					Driveway Westbound					NE 38th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	0	109	33	142	0	8	21	0	29	0	0	0	0	0	0	9	0	5	14	185	
08:15 AM	0	0	78	12	90	0	6	39	0	45	0	0	0	0	0	0	15	0	10	25	160	
08:30 AM	0	0	103	36	139	0	5	32	0	37	0	0	0	0	0	0	16	0	11	27	203	
08:45 AM	0	0	93	27	120	0	5	37	0	42	0	0	0	0	0	0	28	1	9	38	200	
Total Volume	0	0	383	108	491	0	24	129	0	153	0	0	0	0	0	0	68	1	35	104	748	
% App. Total	0	0	78	22		0	15.7	84.3	0		0	0	0	0	0	0	65.4	1	33.7			
PHF	.000	.000	.878	.750	.864	.000	.750	.827	.000	.850	.000	.000	.000	.000	.000	.000	.607	.250	.795	.684	.921	

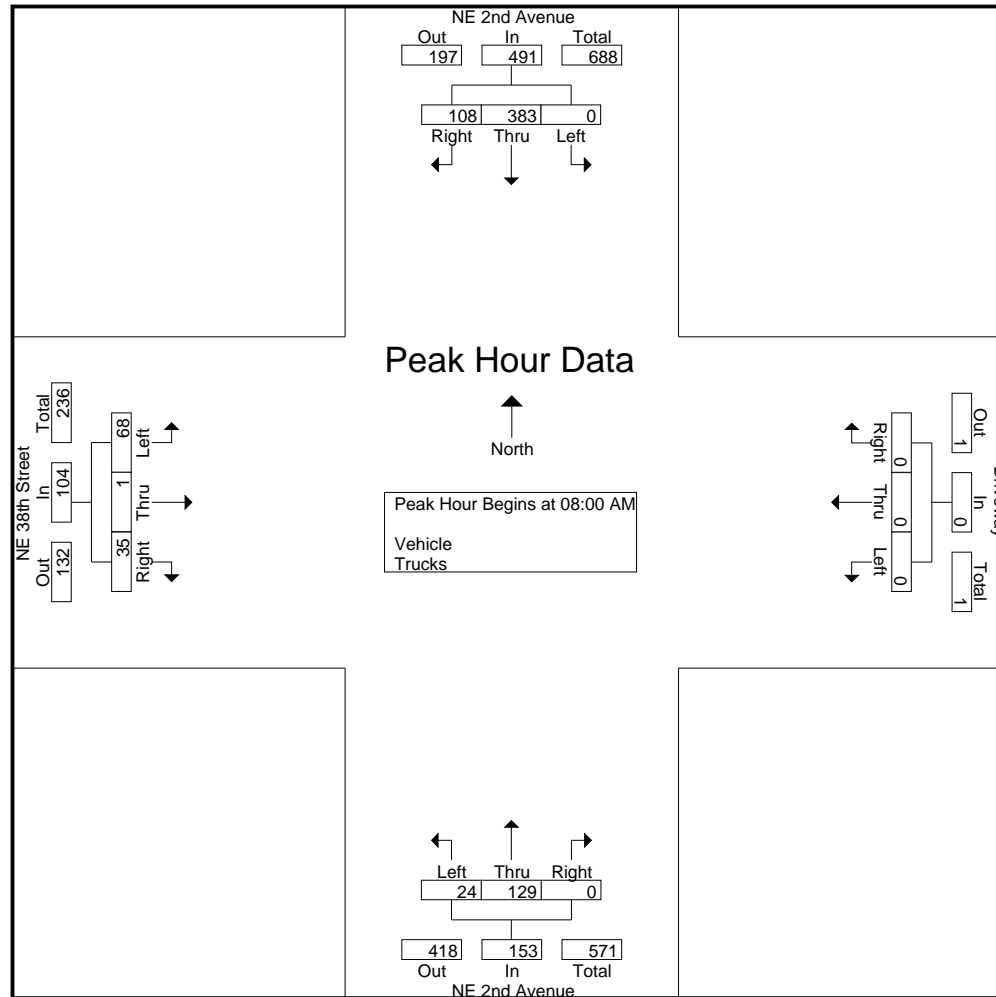
NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6

Start Time	NE 2nd Avenue Southbound					NE 2nd Avenue Northbound					Driveway Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:00 PM																					
03:00 PM	0	0	57	32	89	0	8	52	0	60	0	0	0	0	0	0	25	0	6	31	180
03:15 PM	0	0	68	18	86	0	9	55	0	64	0	0	0	0	0	0	40	0	12	52	202
03:30 PM	0	2	64	22	88	0	13	66	0	79	0	0	0	0	0	0	32	1	14	47	214
03:45 PM	0	0	78	22	100	0	5	75	0	80	0	0	0	0	0	0	30	1	8	39	219
Total Volume	0	2	267	94	363	0	35	248	0	283	0	0	0	0	0	0	127	2	40	169	815
% App. Total	0	0.6	73.6	25.9		0	12.4	87.6	0		0	0	0	0	0	0	75.1	1.2	23.7		
PHF	.000	.250	.856	.734	.908	.000	.673	.827	.000	.884	.000	.000	.000	.000	.000	.000	.794	.500	.714	.813	.930

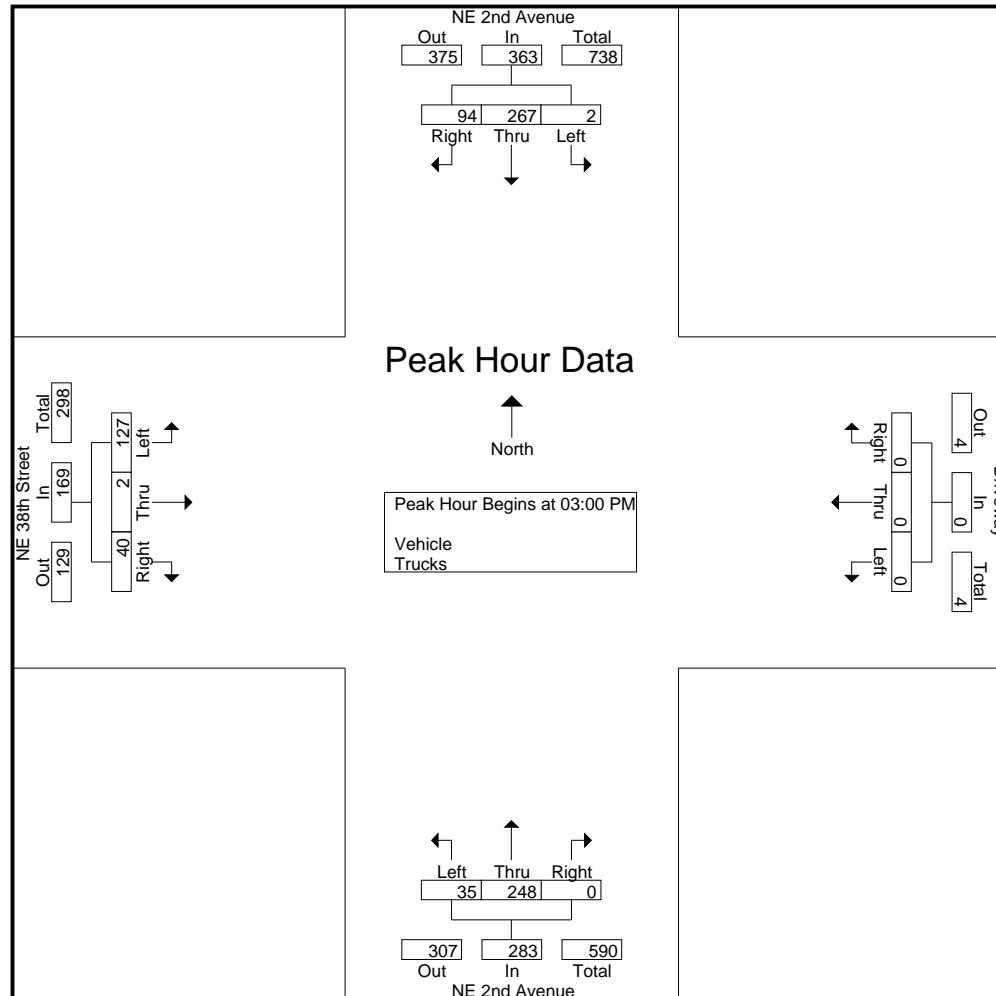
NE 2nd Avenue & NE 38th Street

File Name : TMC-10 NE 2nd Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	NE 2nd Avenue Southbound			NE 2nd Avenue Northbound			NE 39th Street Westbound			NE 39th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	13	0	13	2	0	2	1	0	1	4	3	7	23
07:15 AM	14	0	14	3	0	3	0	0	0	4	0	4	21
07:30 AM	11	0	11	0	0	0	2	0	2	6	0	6	19
07:45 AM	7	0	7	1	0	1	2	0	2	6	1	7	17
Total	45	0	45	6	0	6	5	0	5	20	4	24	80
08:00 AM	5	0	5	1	0	1	8	1	9	5	0	5	20
08:15 AM	1	2	3	1	0	1	8	0	8	1	2	3	15
08:30 AM	2	1	3	1	0	1	3	1	4	3	0	3	11
08:45 AM	5	1	6	0	0	0	4	5	9	6	1	7	22
Total	13	4	17	3	0	3	23	7	30	15	3	18	68
*** BREAK ***													
03:00 PM	6	1	7	0	0	0	16	0	16	16	1	17	40
03:15 PM	6	1	7	7	0	7	11	0	11	16	2	18	43
03:30 PM	42	2	44	11	0	11	32	0	32	21	2	23	110
03:45 PM	10	0	10	7	0	7	21	0	21	16	2	18	56
Total	64	4	68	25	0	25	80	0	80	69	7	76	249
04:00 PM	9	2	11	9	0	9	23	0	23	13	1	14	57
04:15 PM	11	0	11	4	0	4	5	0	5	9	0	9	29
04:30 PM	9	0	9	1	1	2	16	1	17	18	1	19	47
04:45 PM	1	0	1	24	2	26	2	3	5	13	0	13	45
Total	30	2	32	38	3	41	46	4	50	53	2	55	178
05:00 PM	10	0	10	5	0	5	11	0	11	5	0	5	31
05:15 PM	8	1	9	7	0	7	2	0	2	3	1	4	22
05:30 PM	4	0	4	12	0	12	4	0	4	11	2	13	33
05:45 PM	5	0	5	17	0	17	9	0	9	5	4	9	40
Total	27	1	28	41	0	41	26	0	26	24	7	31	126
Grand Total	179	11	190	113	3	116	180	11	191	181	23	204	701
Apprch %	94.2	5.8		97.4	2.6		94.2	5.8		88.7	11.3		
Total %	25.5	1.6	27.1	16.1	0.4	16.5	25.7	1.6	27.2	25.8	3.3	29.1	

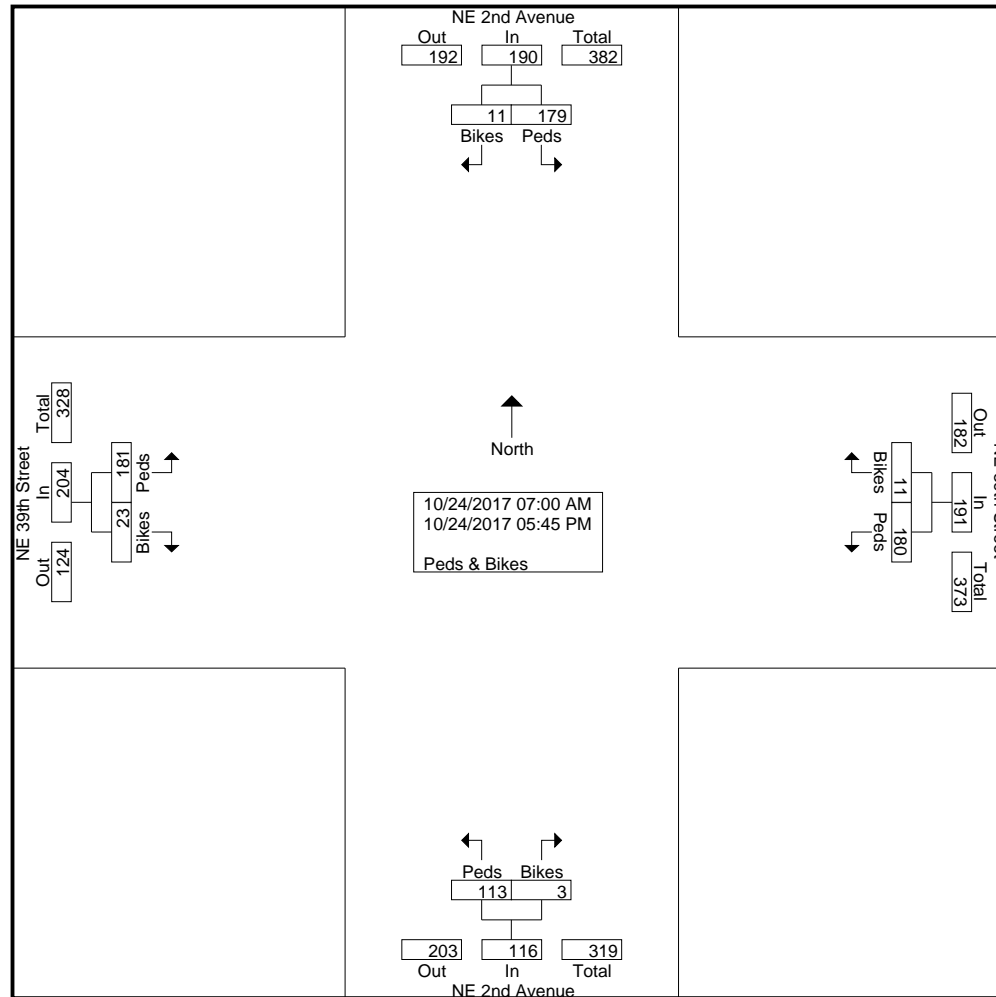
NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NE 2nd Avenue Southbound			NE 2nd Avenue Northbound			NE 39th Street Westbound			NE 39th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:00 AM													
07:00 AM	13	0	13	2	0	2	1	0	1	4	3	7	23
07:15 AM	14	0	14	3	0	3	0	0	0	4	0	4	21
07:30 AM	11	0	11	0	0	0	2	0	2	6	0	6	19
07:45 AM	7	0	7	1	0	1	2	0	2	6	1	7	17
Total Volume	45	0	45	6	0	6	5	0	5	20	4	24	80
% App. Total	100	0		100	0		100	0		83.3	16.7		
PHF	.804	.000	.804	.500	.000	.500	.625	.000	.625	.833	.333	.857	.870

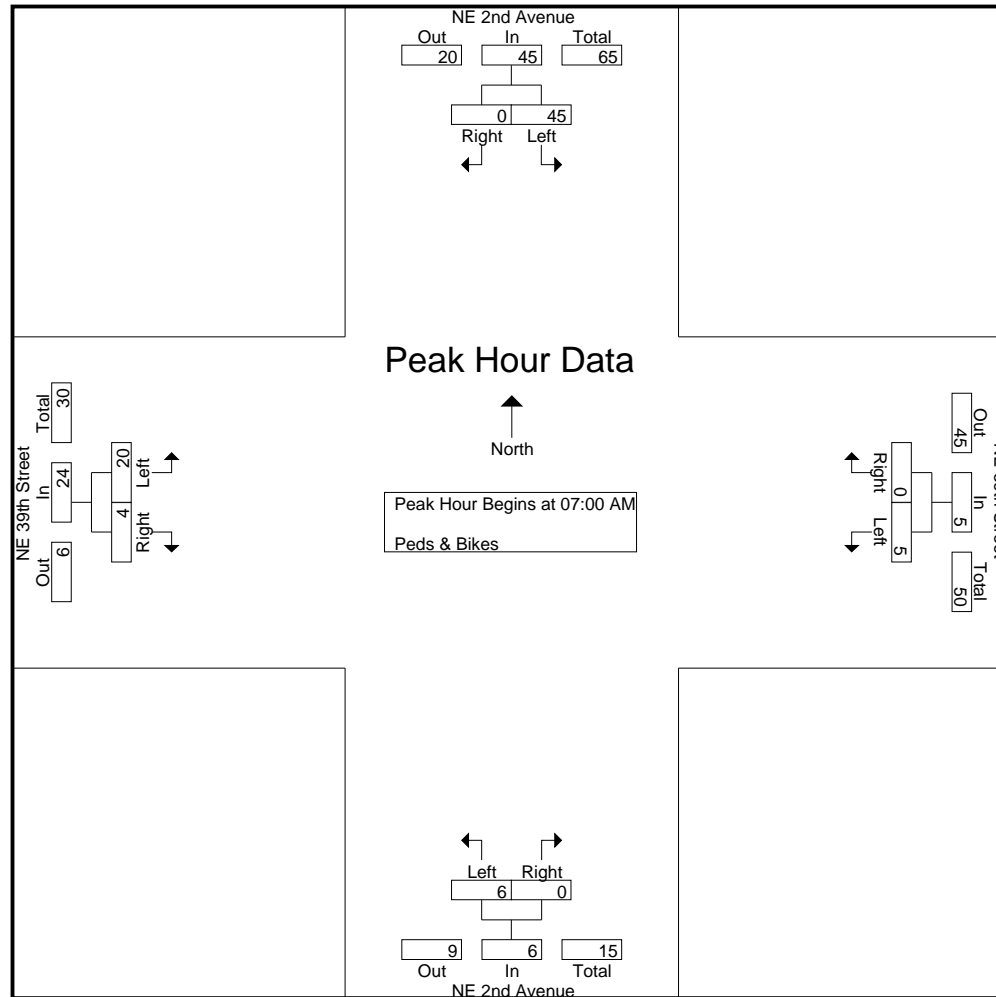
NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5

Start Time	NE 2nd Avenue Southbound			NE 2nd Avenue Northbound			NE 39th Street Westbound			NE 39th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 03:15 PM													
03:15 PM	6	1	7	7	0	7	11	0	11	16	2	18	43
03:30 PM	42	2	44	11	0	11	32	0	32	21	2	23	110
03:45 PM	10	0	10	7	0	7	21	0	21	16	2	18	56
04:00 PM	9	2	11	9	0	9	23	0	23	13	1	14	57
Total Volume	67	5	72	34	0	34	87	0	87	66	7	73	266
% App. Total	93.1	6.9		100	0		100	0		90.4	9.6		
PHF	.399	.625	.409	.773	.000	.773	.680	.000	.680	.786	.875	.793	.605

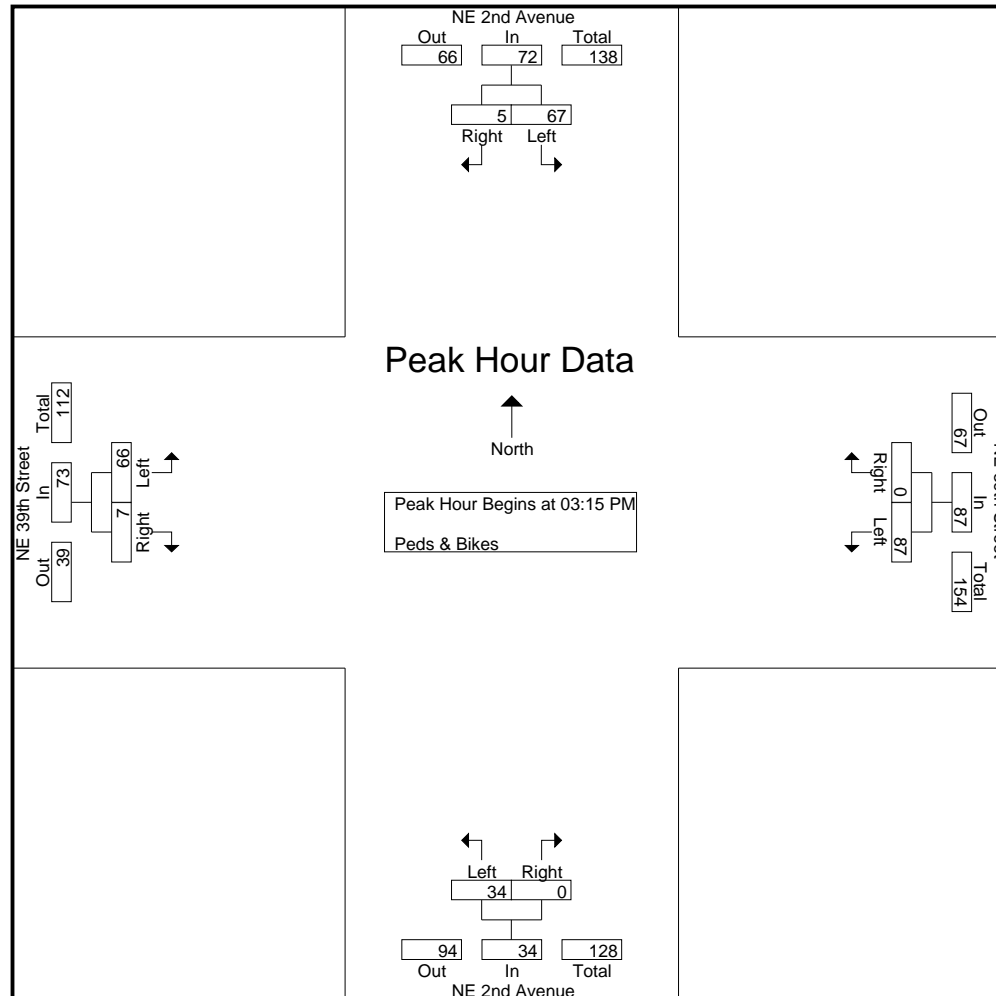
NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Trucks

Start Time	NE 2nd Avenue Southbound					NE 2nd Avenue Northbound					NE 39th Street Westbound					NE 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	4	0	4	0	0	4	0	4	0	0	1	3	4	0	1	0	0	1	13
07:15 AM	0	1	3	1	5	0	0	3	0	3	0	0	0	1	1	0	0	0	0	1	10
07:30 AM	0	0	6	0	6	0	1	6	0	7	0	0	0	1	1	0	0	0	0	0	14
07:45 AM	0	0	5	0	5	0	0	7	0	7	0	0	0	0	0	0	0	0	0	0	12
Total	0	1	18	1	20	0	1	20	0	21	0	0	1	5	6	0	1	0	1	2	49
08:00 AM	0	1	7	0	8	0	0	3	0	3	0	0	0	1	1	0	0	0	0	0	12
08:15 AM	0	3	5	0	8	0	0	6	0	6	0	0	1	1	2	0	0	0	0	0	16
08:30 AM	0	2	6	0	8	0	0	6	0	6	0	1	1	5	7	0	1	0	0	1	22
08:45 AM	0	0	4	0	4	0	0	3	0	3	0	1	1	1	3	0	0	1	1	2	12
Total	0	6	22	0	28	0	0	18	0	18	0	2	3	8	13	0	1	1	1	3	62
*** BREAK ***																					
03:00 PM	0	2	3	0	5	0	0	1	0	1	0	2	2	2	6	0	0	0	0	0	12
03:15 PM	0	0	2	0	2	0	0	7	0	7	0	0	0	2	2	0	0	0	0	0	11
03:30 PM	0	1	3	0	4	0	0	5	0	5	0	0	0	2	2	0	0	0	0	0	11
03:45 PM	0	1	5	0	6	0	0	4	0	4	0	0	2	1	3	0	1	0	0	1	14
Total	0	4	13	0	17	0	0	17	0	17	0	2	4	7	13	0	1	0	0	1	48
04:00 PM	0	0	2	0	2	0	0	5	1	6	0	0	0	0	0	0	0	0	0	0	8
04:15 PM	0	0	9	0	9	0	0	3	0	3	0	0	0	1	1	0	0	0	0	0	13
04:30 PM	0	0	1	0	1	0	1	2	0	3	0	0	1	1	2	0	0	0	0	0	6
04:45 PM	0	0	6	0	6	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	9
Total	0	0	18	0	18	0	1	13	1	15	0	0	1	2	3	0	0	0	0	0	36
05:00 PM	0	0	3	0	3	0	0	6	1	7	0	0	0	1	1	0	1	0	0	1	12
05:15 PM	0	0	2	0	2	0	0	5	0	5	0	0	0	1	1	0	0	0	0	0	8
05:30 PM	0	0	5	1	6	0	0	1	0	1	0	0	0	0	0	0	1	0	0	1	8
05:45 PM	0	0	1	0	1	0	0	4	0	4	0	0	0	1	1	0	0	1	1	2	8
Total	0	0	11	1	12	0	0	16	1	17	0	0	0	3	3	0	2	1	1	4	36
Grand Total	0	11	82	2	95	0	2	84	2	88	0	4	9	25	38	0	5	2	3	10	231
Apprch %	0	11.6	86.3	2.1		0	2.3	95.5	2.3		0	10.5	23.7	65.8		0	50	20	30		
Total %	0	4.8	35.5	0.9	41.1	0	0.9	36.4	0.9	38.1	0	1.7	3.9	10.8	16.5	0	2.2	0.9	1.3	4.3	

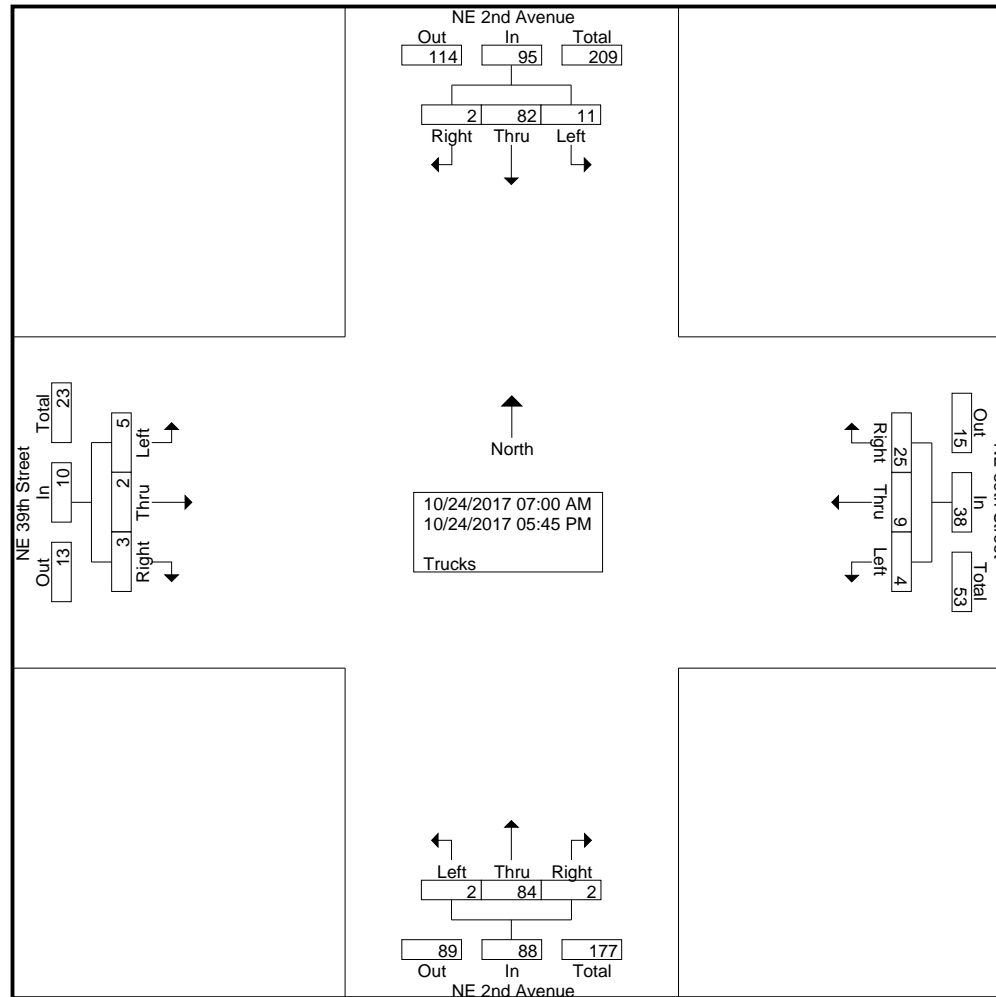
NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NE 2nd Avenue Southbound					NE 2nd Avenue Northbound					NE 39th Street Westbound					NE 39th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 07:45 AM																						
07:45 AM	0	0	5	0	5	0	0	7	0	7	0	0	0	0	0	0	0	0	0	0	0	12
08:00 AM	0	1	7	0	8	0	0	3	0	3	0	0	0	1	1	0	0	0	0	0	0	12
08:15 AM	0	3	5	0	8	0	0	6	0	6	0	0	1	1	2	0	0	0	0	0	0	16
08:30 AM	0	2	6	0	8	0	0	6	0	6	0	1	1	5	7	0	1	0	0	0	1	22
Total Volume	0	6	23	0	29	0	0	22	0	22	0	1	2	7	10	0	1	0	0	0	1	62
% App. Total	0	20.7	79.3	0		0	0	100	0		0	10	20	70		0	100	0	0			
PHF	.000	.500	.821	.000	.906	.000	.000	.786	.000	.786	.000	.250	.500	.350	.357	.000	.250	.000	.000	.250		.705

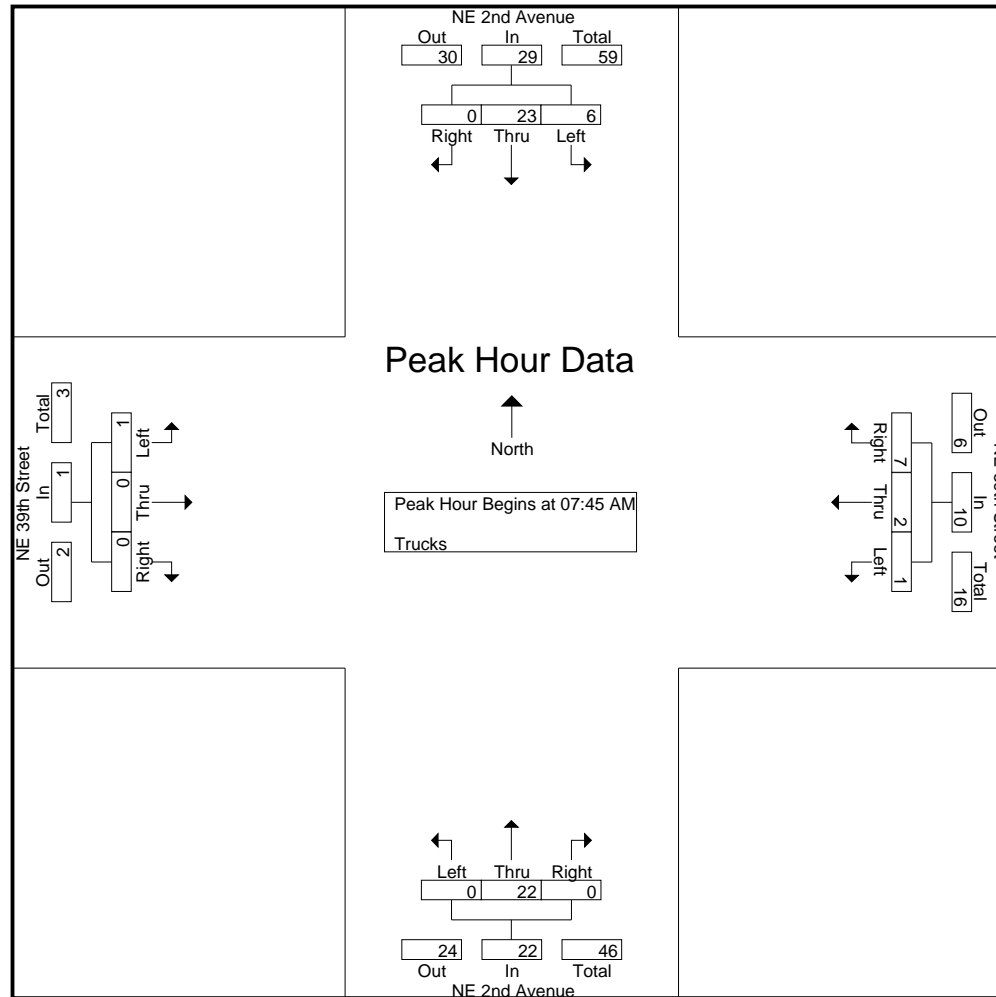
NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5

Start Time	NE 2nd Avenue Southbound					NE 2nd Avenue Northbound					NE 39th Street Westbound					NE 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:00 PM																					
03:00 PM	0	2	3	0	5	0	0	1	0	1	0	2	2	2	6	0	0	0	0	0	12
03:15 PM	0	0	2	0	2	0	0	7	0	7	0	0	0	2	2	0	0	0	0	0	11
03:30 PM	0	1	3	0	4	0	0	5	0	5	0	0	0	2	2	0	0	0	0	0	11
03:45 PM	0	1	5	0	6	0	0	4	0	4	0	0	2	1	3	0	1	0	0	1	14
Total Volume	0	4	13	0	17	0	0	17	0	17	0	2	4	7	13	0	1	0	0	1	48
% App. Total	0	23.5	76.5	0		0	0	100	0		0	15.4	30.8	53.8		0	100	0	0		
PHF	.000	.500	.650	.000	.708	.000	.000	.607	.000	.607	.000	.250	.500	.875	.542	.000	.250	.000	.000	.250	.857

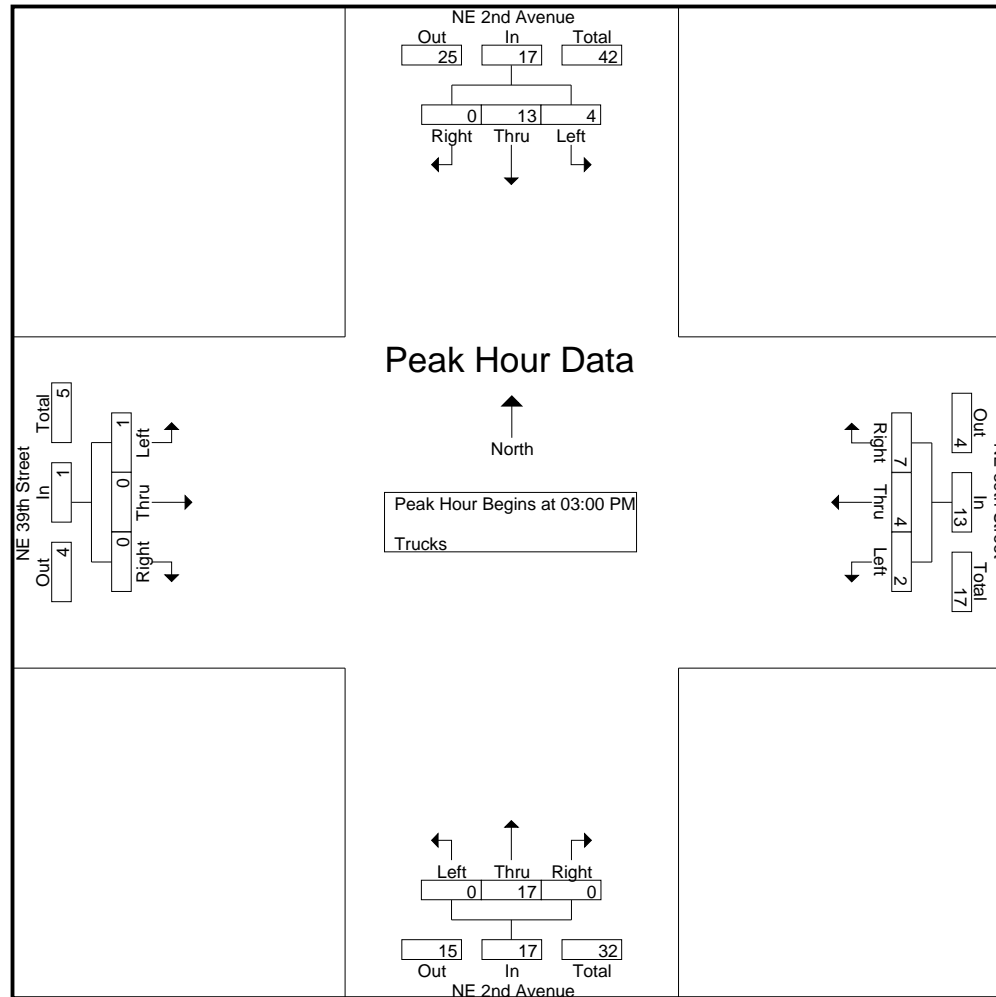
NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	NE 2nd Avenue Southbound					NE 2nd Avenue Northbound					NE 39th Street Westbound					NE 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	9	59	4	72	0	0	37	3	40	0	11	12	16	39	0	8	6	1	15	166
07:15 AM	0	17	88	6	111	0	1	50	8	59	0	17	20	28	65	0	15	7	3	25	260
07:30 AM	0	23	96	10	129	0	1	44	10	55	0	13	19	19	51	0	8	15	5	28	263
07:45 AM	0	12	119	4	135	0	0	33	10	43	0	13	27	12	52	0	0	10	9	19	249
Total	0	61	362	24	447	0	2	164	31	197	0	54	78	75	207	0	31	38	18	87	938
08:00 AM	0	14	115	7	136	0	3	21	12	36	0	21	29	9	59	0	1	8	7	16	247
08:15 AM	0	17	86	4	107	0	4	41	10	55	0	10	26	11	47	0	0	18	5	23	232
08:30 AM	0	24	116	8	148	0	2	37	14	53	0	20	31	14	65	0	4	18	6	28	294
08:45 AM	0	31	114	16	161	0	3	41	13	57	0	8	28	25	61	0	4	21	9	34	313
Total	0	86	431	35	552	0	12	140	49	201	0	59	114	59	232	0	9	65	27	101	1086
*** BREAK ***																					
03:00 PM	0	11	72	10	93	0	3	60	8	71	0	16	27	22	65	0	6	8	10	24	253
03:15 PM	0	10	71	7	88	0	2	75	17	94	0	7	21	23	51	0	10	11	11	32	265
03:30 PM	0	14	68	7	89	0	2	73	14	89	0	15	19	20	54	0	10	12	9	31	263
03:45 PM	0	14	86	15	115	0	5	96	14	115	0	11	23	21	55	0	8	9	10	27	312
Total	0	49	297	39	385	0	12	304	53	369	0	49	90	86	225	0	34	40	40	114	1093
04:00 PM	0	14	49	9	72	0	6	84	6	96	0	12	37	28	77	0	11	8	7	26	271
04:15 PM	0	13	70	9	92	0	3	86	9	98	0	11	26	21	58	0	6	19	9	34	282
04:30 PM	0	9	57	3	69	0	8	86	18	112	0	10	24	22	56	0	7	8	4	19	256
04:45 PM	0	19	56	11	86	0	3	96	6	105	0	7	27	33	67	0	4	9	6	19	277
Total	0	55	232	32	319	0	20	352	39	411	0	40	114	104	258	0	28	44	26	98	1086
05:00 PM	0	13	58	7	78	0	5	105	13	123	0	4	27	18	49	0	10	10	12	32	282
05:15 PM	0	8	54	8	70	0	4	111	19	134	0	6	27	19	52	0	5	9	8	22	278
05:30 PM	0	12	54	7	73	0	2	77	23	102	0	7	26	26	59	0	12	14	7	33	267
05:45 PM	0	10	53	9	72	0	2	92	12	106	0	4	13	20	37	0	6	11	8	25	240
Total	0	43	219	31	293	0	13	385	67	465	0	21	93	83	197	0	33	44	35	112	1067
Grand Total	0	294	1541	161	1996	0	59	1345	239	1643	0	223	489	407	1119	0	135	231	146	512	5270
Apprch %	0	14.7	77.2	8.1		0	3.6	81.9	14.5		0	19.9	43.7	36.4		0	26.4	45.1	28.5		
Total %	0	5.6	29.2	3.1	37.9	0	1.1	25.5	4.5	31.2	0	4.2	9.3	7.7	21.2	0	2.6	4.4	2.8	9.7	
Vehicle	0	283	1459	159	1901	0	57	1261	237	1555	0	219	480	382	1081	0	130	229	143	502	5039
% Vehicle	0	96.3	94.7	98.8	95.2	0	96.6	93.8	99.2	94.6	0	98.2	98.2	93.9	96.6	0	96.3	99.1	97.9	98	95.6

NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	NE 2nd Avenue Southbound					NE 2nd Avenue Northbound					NE 39th Street Westbound					NE 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	11	82	2	95	0	2	84	2	88	0	4	9	25	38	0	5	2	3	10	231
% Trucks	0	3.7	5.3	1.2	4.8	0	3.4	6.2	0.8	5.4	0	1.8	1.8	6.1	3.4	0	3.7	0.9	2.1	2	4.4

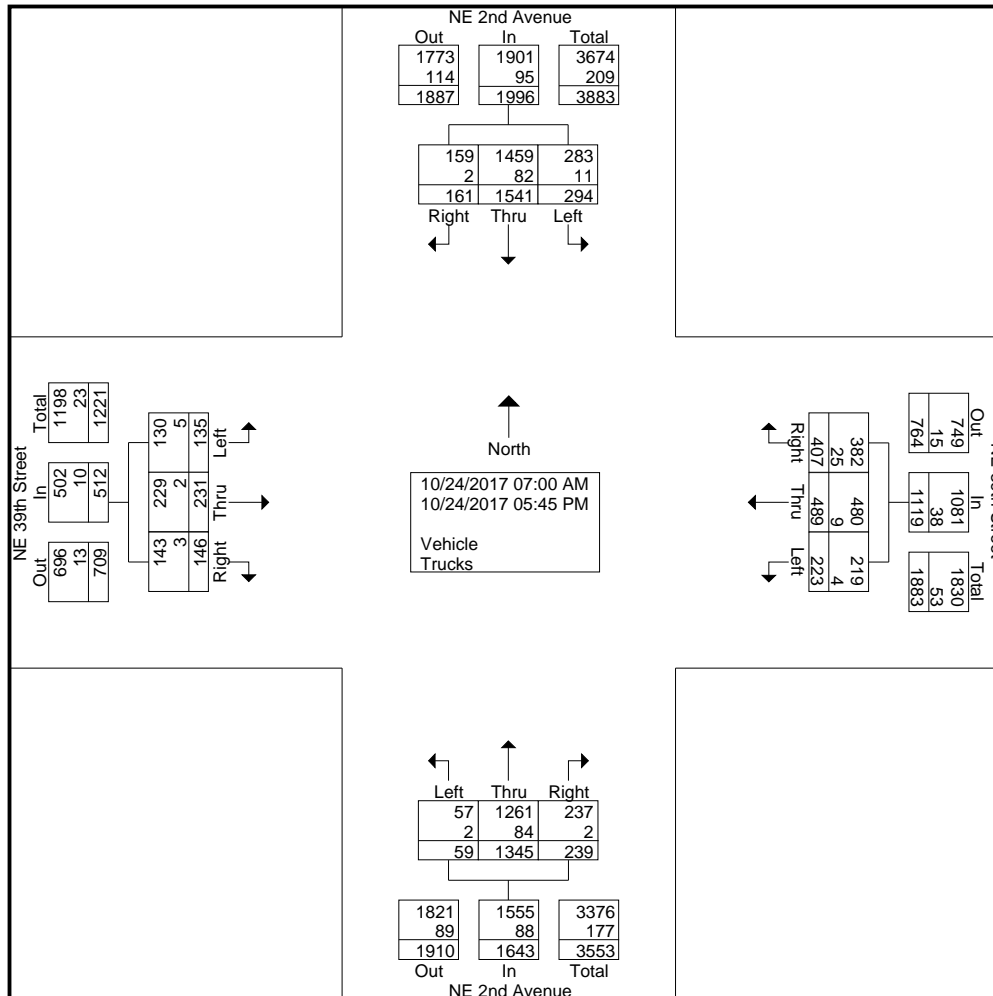
NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4

Start Time	NE 2nd Avenue Southbound					NE 2nd Avenue Northbound					NE 39th Street Westbound					NE 39th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	14	115	7	136	0	3	21	12	36	0	21	29	9	59	0	1	8	7	16	247	
08:15 AM	0	17	86	4	107	0	4	41	10	55	0	10	26	11	47	0	0	18	5	23	232	
08:30 AM	0	24	116	8	148	0	2	37	14	53	0	20	31	14	65	0	4	18	6	28	294	
08:45 AM	0	31	114	16	161	0	3	41	13	57	0	8	28	25	61	0	4	21	9	34	313	
Total Volume	0	86	431	35	552	0	12	140	49	201	0	59	114	59	232	0	9	65	27	101	1086	
% App. Total	0	15.6	78.1	6.3		0	6	69.7	24.4		0	25.4	49.1	25.4		0	8.9	64.4	26.7			
PHF	.000	.694	.929	.547	.857	.000	.750	.854	.875	.882	.000	.702	.919	.590	.892	.000	.563	.774	.750	.743	.867	

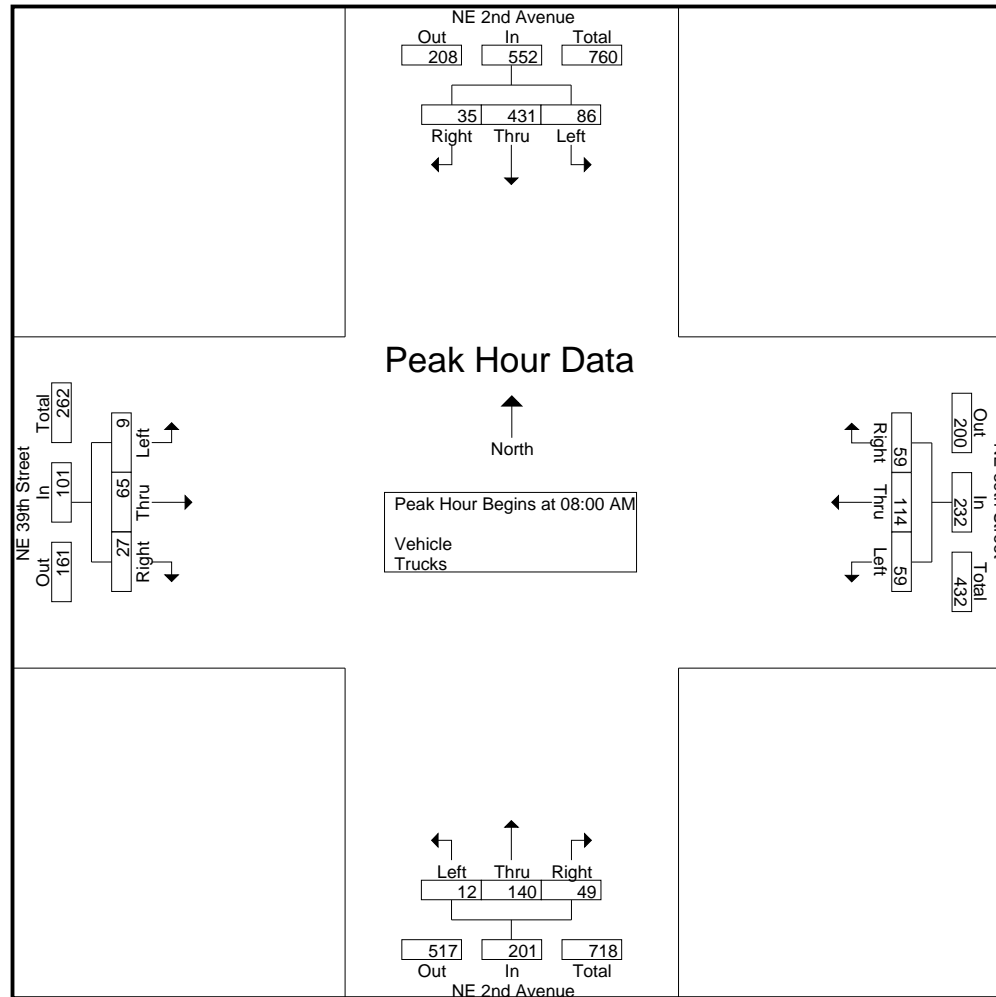
NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6

Start Time	NE 2nd Avenue Southbound					NE 2nd Avenue Northbound					NE 39th Street Westbound					NE 39th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:30 PM																					
03:30 PM	0	14	68	7	89	0	2	73	14	89	0	15	19	20	54	0	10	12	9	31	263
03:45 PM	0	14	86	15	115	0	5	96	14	115	0	11	23	21	55	0	8	9	10	27	312
04:00 PM	0	14	49	9	72	0	6	84	6	96	0	12	37	28	77	0	11	8	7	26	271
04:15 PM	0	13	70	9	92	0	3	86	9	98	0	11	26	21	58	0	6	19	9	34	282
Total Volume	0	55	273	40	368	0	16	339	43	398	0	49	105	90	244	0	35	48	35	118	1128
% App. Total	0	14.9	74.2	10.9		0	4	85.2	10.8		0	20.1	43	36.9		0	29.7	40.7	29.7		
PHF	.000	.982	.794	.667	.800	.000	.667	.883	.768	.865	.000	.817	.709	.804	.792	.000	.795	.632	.875	.868	.904

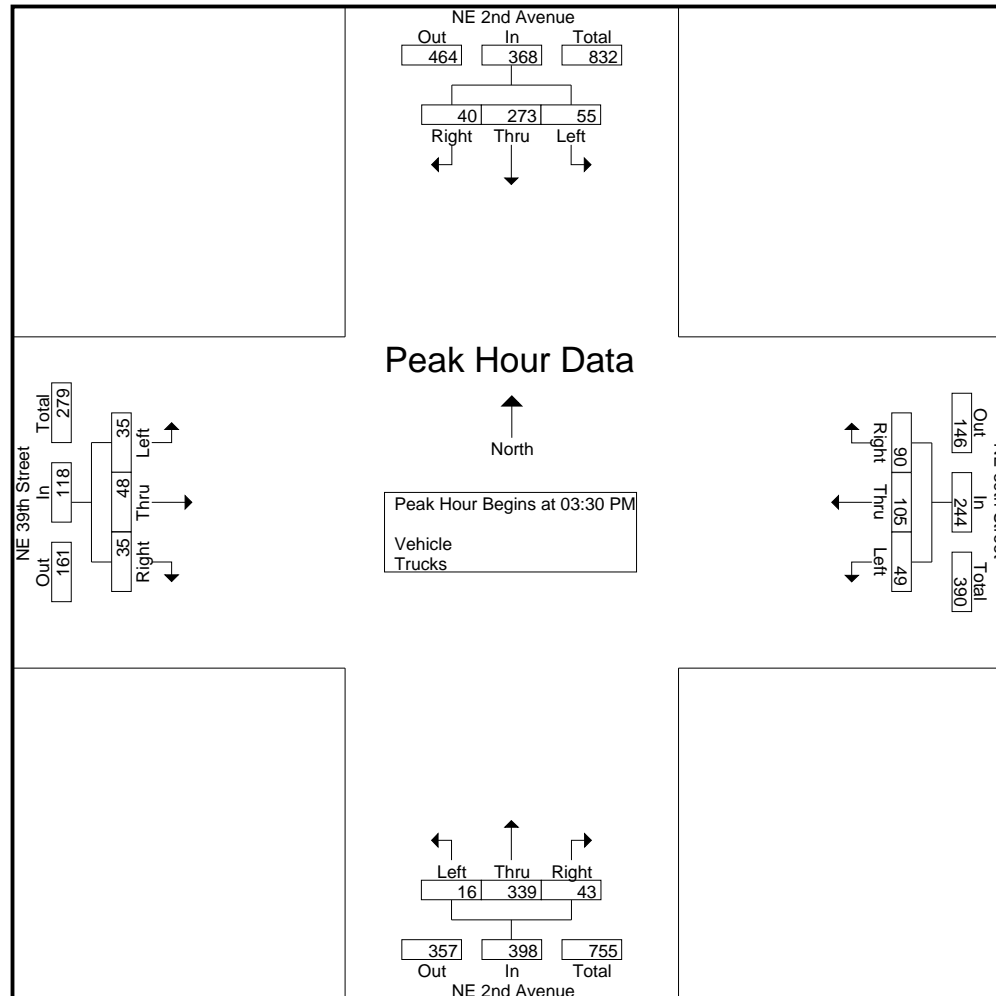
NE 2nd Avenue & NE 39th Street

File Name : TMC-11 NE 2nd Avenue & NE 39th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	N Federal Hwy Southbound			N Federal Hwy Northbound			Westbound			NE 39th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	7	0	7	10	0	10	1	0	1	0	0	0	18
07:15 AM	6	0	6	5	0	5	0	0	0	0	0	0	11
07:30 AM	2	0	2	24	0	24	1	0	1	3	0	3	30
07:45 AM	4	0	4	2	1	3	0	0	0	0	0	0	7
Total	19	0	19	41	1	42	2	0	2	3	0	3	66
08:00 AM	0	0	0	2	0	2	0	0	0	6	0	6	8
08:15 AM	2	0	2	2	2	4	0	0	0	0	0	0	6
08:30 AM	3	0	3	2	0	2	0	0	0	2	0	2	7
08:45 AM	7	0	7	2	2	4	0	0	0	1	0	1	12
Total	12	0	12	8	4	12	0	0	0	9	0	9	33
*** BREAK ***													
03:00 PM	7	0	7	7	0	7	0	0	0	0	0	0	14
03:15 PM	5	0	5	1	0	1	0	0	0	0	0	0	6
03:30 PM	17	1	18	35	1	36	1	0	1	0	1	1	56
03:45 PM	2	0	2	13	0	13	0	0	0	1	0	1	16
Total	31	1	32	56	1	57	1	0	1	1	1	2	92
04:00 PM	15	0	15	2	1	3	1	0	1	4	0	4	23
04:15 PM	0	0	0	1	1	2	0	0	0	0	1	1	3
04:30 PM	1	0	1	6	1	7	0	0	0	2	0	2	10
04:45 PM	6	0	6	8	0	8	0	0	0	1	0	1	15
Total	22	0	22	17	3	20	1	0	1	7	1	8	51
05:00 PM	7	2	9	6	1	7	0	0	0	2	0	2	18
05:15 PM	5	0	5	4	0	4	0	0	0	0	0	0	9
05:30 PM	8	0	8	3	0	3	0	0	0	0	0	0	11
05:45 PM	2	0	2	4	0	4	0	0	0	1	0	1	7
Total	22	2	24	17	1	18	0	0	0	3	0	3	45
Grand Total	106	3	109	139	10	149	4	0	4	23	2	25	287
Apprch %	97.2	2.8		93.3	6.7		100	0		92	8		
Total %	36.9	1	38	48.4	3.5	51.9	1.4	0	1.4	8	0.7	8.7	

N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2

N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	N Federal Hwy Southbound			N Federal Hwy Northbound			Westbound			NE 39th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:00 AM													
07:00 AM	7	0	7	10	0	10	1	0	1	0	0	0	18
07:15 AM	6	0	6	5	0	5	0	0	0	0	0	0	11
07:30 AM	2	0	2	24	0	24	1	0	1	3	0	3	30
07:45 AM	4	0	4	2	1	3	0	0	0	0	0	0	7
Total Volume	19	0	19	41	1	42	2	0	2	3	0	3	66
% App. Total	100	0		97.6	2.4		100	0		100	0		
PHF	.679	.000	.679	.427	.250	.438	.500	.000	.500	.250	.000	.250	.550

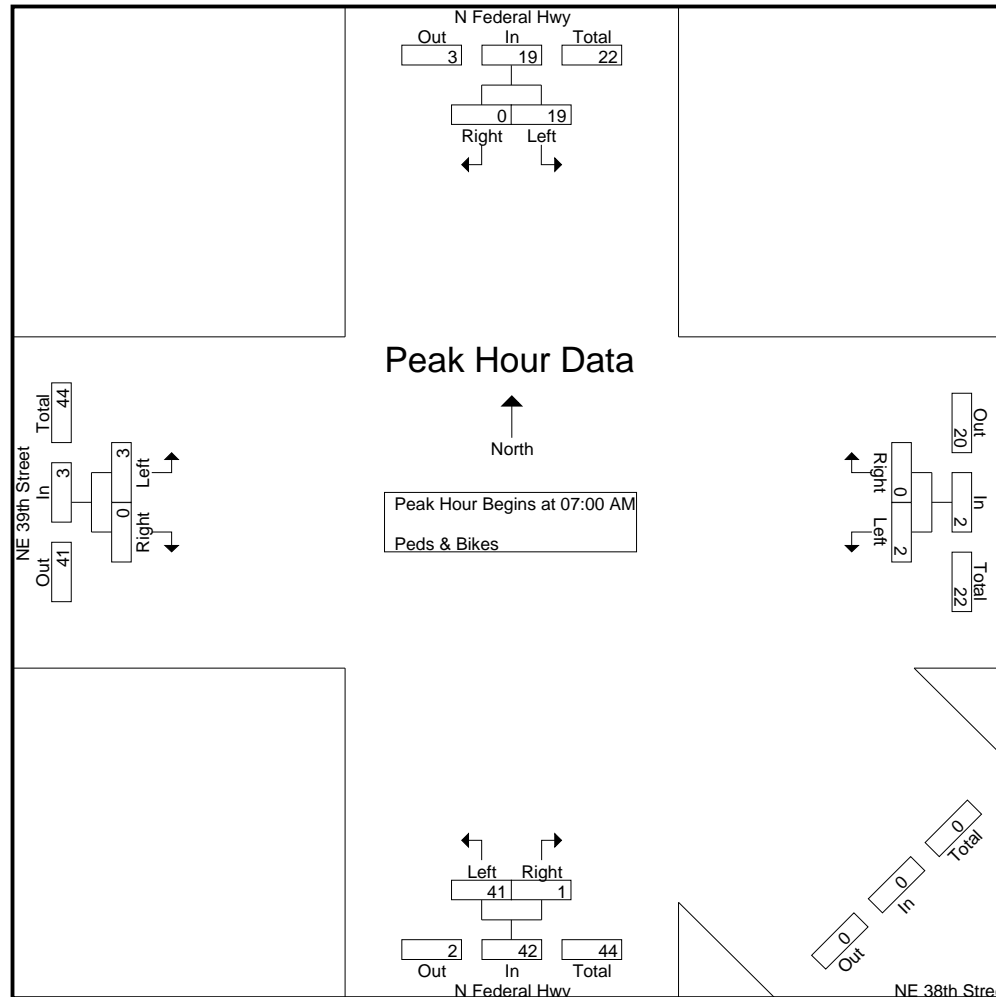
N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	N Federal Hwy Southbound			N Federal Hwy Northbound			Westbound			NE 39th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 03:15 PM													
03:15 PM	5	0	5	1	0	1	0	0	0	0	0	0	6
03:30 PM	17	1	18	35	1	36	1	0	1	0	1	1	56
03:45 PM	2	0	2	13	0	13	0	0	0	1	0	1	16
04:00 PM	15	0	15	2	1	3	1	0	1	4	0	4	23
Total Volume	39	1	40	51	2	53	2	0	2	5	1	6	101
% App. Total	97.5	2.5		96.2	3.8		100	0		83.3	16.7		
PHF	.574	.250	.556	.364	.500	.368	.500	.000	.500	.313	.250	.375	.451

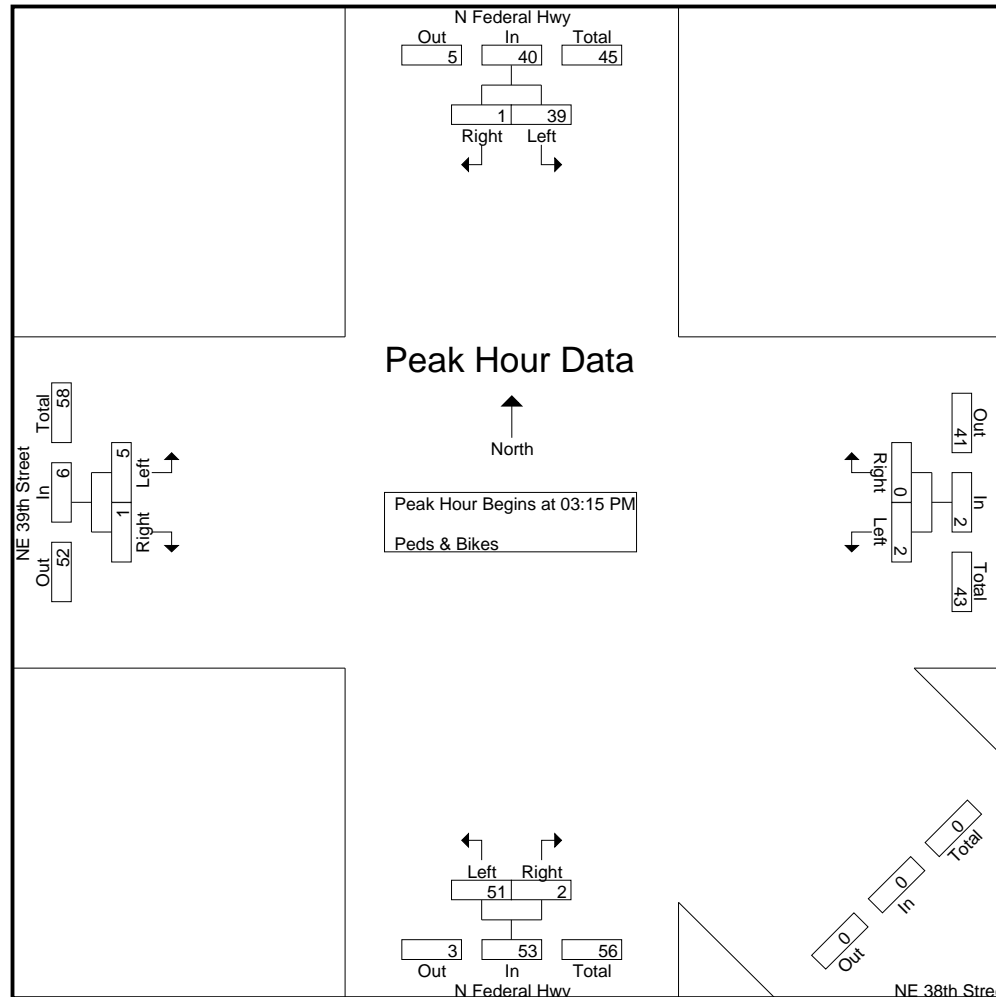
N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Trucks

Start Time	N Federal Hwy Southbound						N Federal Hwy Northbound						Westbound						NE 39th Street Eastbound						NE 38th Street						Int. Total
	U-Turns	Left	Bear Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	Hard Right	App. Total	Hard Left	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Bear Right	Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	App. Total		
07:00 AM	0	0	0	0	1	1	0	3	2	0	0	5	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	7		
07:15 AM	0	0	0	1	1	2	0	1	0	0	0	1	0	0	0	0	0	0	0	0	1	0	1	0	0	1	0	1	5		
07:30 AM	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	1	1	0	1	0	0	1	3		
07:45 AM	0	0	0	0	0	0	0	2	1	0	0	3	0	0	0	0	0	0	0	0	0	1	1	1	0	0	0	1	5		
Total	0	0	0	1	2	3	0	6	4	0	0	10	0	0	0	0	0	0	0	0	1	0	3	4	1	1	1	0	3	20	
08:00 AM	0	0	0	1	0	1	0	1	3	0	0	4	0	0	0	0	0	0	0	1	0	0	1	2	1	0	0	0	1	8	
08:15 AM	0	0	0	1	0	1	0	1	1	0	0	2	0	0	0	0	0	0	0	1	1	0	3	5	0	1	0	0	1	9	
08:30 AM	0	0	0	1	1	2	0	5	0	0	0	5	0	0	0	0	0	0	0	1	1	0	1	3	0	1	0	0	1	11	
08:45 AM	0	0	0	1	1	2	0	1	2	0	0	3	0	0	0	0	0	0	0	0	0	0	1	1	1	2	1	0	4	10	
Total	0	0	0	4	2	6	0	8	6	0	0	14	0	0	0	0	0	0	0	3	2	0	6	11	2	4	1	0	7	38	
*** BREAK ***																															
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	1	3	0	2	0	0	2	5	
03:15 PM	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2	
03:30 PM	0	0	0	0	3	3	0	0	4	0	0	4	0	0	0	0	0	0	0	1	0	0	0	1	1	0	0	0	1	9	
03:45 PM	0	0	0	1	1	2	0	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	1	1	1	0	0	0	1	6	
Total	0	0	0	1	4	5	0	0	7	0	0	7	0	0	0	0	0	0	0	1	3	0	2	6	2	2	0	0	4	22	
04:00 PM	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2	
04:15 PM	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
04:30 PM	0	0	0	0	1	1	0	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	
04:45 PM	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
Total	0	0	0	1	1	2	0	0	5	0	0	5	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	8		
05:00 PM	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	2	
05:15 PM	0	0	0	1	1	2	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	
05:30 PM	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
05:45 PM	0	0	0	1	1	2	0	0	1	0	0	1	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	4	
Total	0	0	0	3	2	5	0	0	3	0	0	3	0	0	0	0	0	0	0	2	0	0	0	2	0	0	0	0	10		
Grand Total	0	0	0	10	11	21	0	14	25	0	0	39	0	0	0	0	0	0	0	6	7	0	11	24	5	7	2	0	14	98	
Apprch %	0	0	0	47.6	52.4		0	35.9	64.1	0	0		0	0	0	0	0	0	0	25	29.2	0	45.8		35.7	50	14.3	0			
Total %	0	0	0	10.2	11.2	21.4	0	14.3	25.5	0	0	39.8	0	0	0	0	0	0	0	6.1	7.1	0	11.2	24.5	5.1	7.1	2	0	14.3		

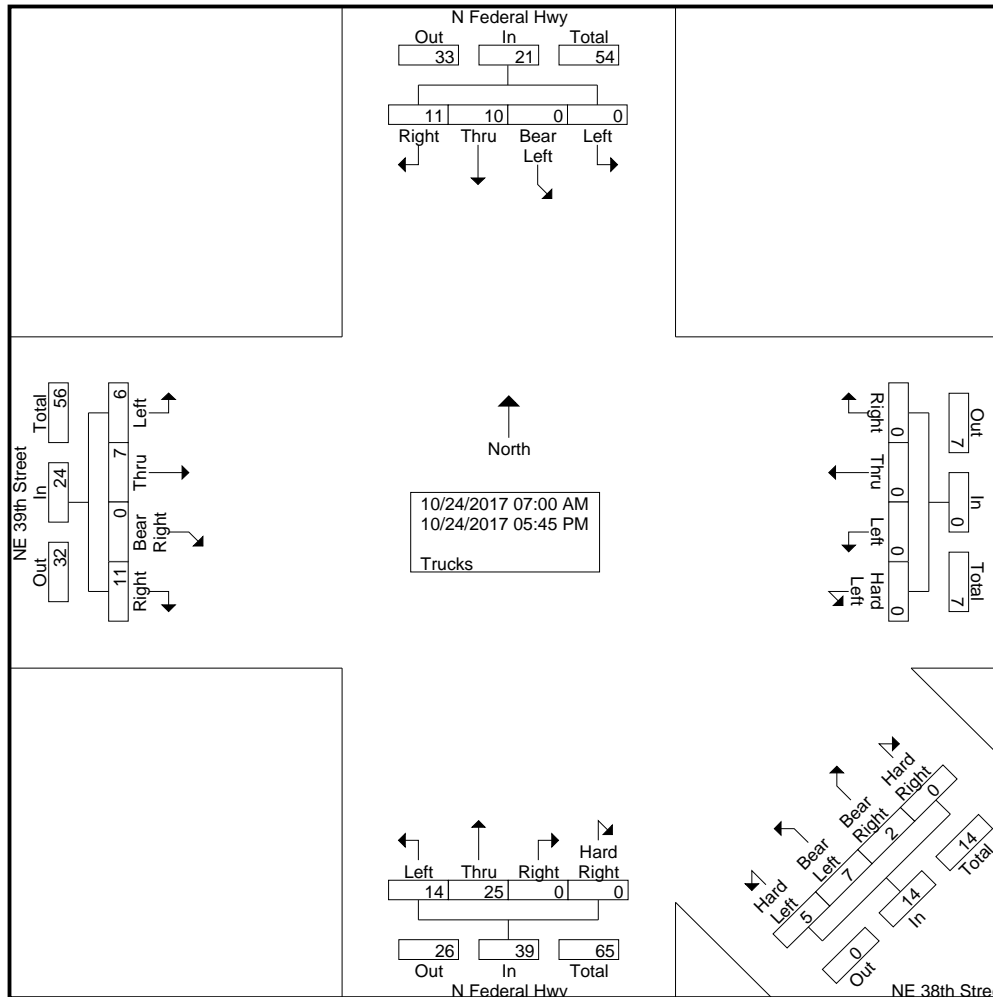
N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	N Federal Hwy Southbound						N Federal Hwy Northbound						Westbound						NE 39th Street Eastbound						NE 38th Street						Int. Total
	U-Turns	Left	Bear Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	Hard Right	App. Total	Hard Left	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Bear Right	Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																															
Peak Hour for Entire Intersection Begins at 08:00 AM																															
08:00 AM	0	0	0	1	0	1	0	1	3	0	0	4	0	0	0	0	0	0	0	1	0	0	1	2	1	0	0	0	1	8	
08:15 AM	0	0	0	1	0	1	0	1	1	0	0	2	0	0	0	0	0	0	0	1	1	0	3	5	0	1	0	0	1	9	
08:30 AM	0	0	0	1	1	2	0	5	0	0	0	5	0	0	0	0	0	0	0	1	1	0	1	3	0	1	0	0	1	11	
08:45 AM	0	0	0	1	1	2	0	1	2	0	0	3	0	0	0	0	0	0	0	0	0	0	1	1	1	2	1	0	4	10	
Total Volume	0	0	0	4	2	6	0	8	6	0	0	14	0	0	0	0	0	0	0	3	2	0	6	11	2	4	1	0	7	38	
% App. Total	0	0	0	66.7	33.3		0	57.1	42.9	0	0		0	0	0	0	0		0	27.3	18.2	0	54.5		28.6	57.1	14.3	0			
PHF	.000	.000	.000	1.0 0	.500	.750	.000	.400	.500	.000	.000	.700	.000	.000	.000	.000	.000	.000	.000	.750	.500	.000	.500	.550	.500	.500	.250	.000	.438	.864	

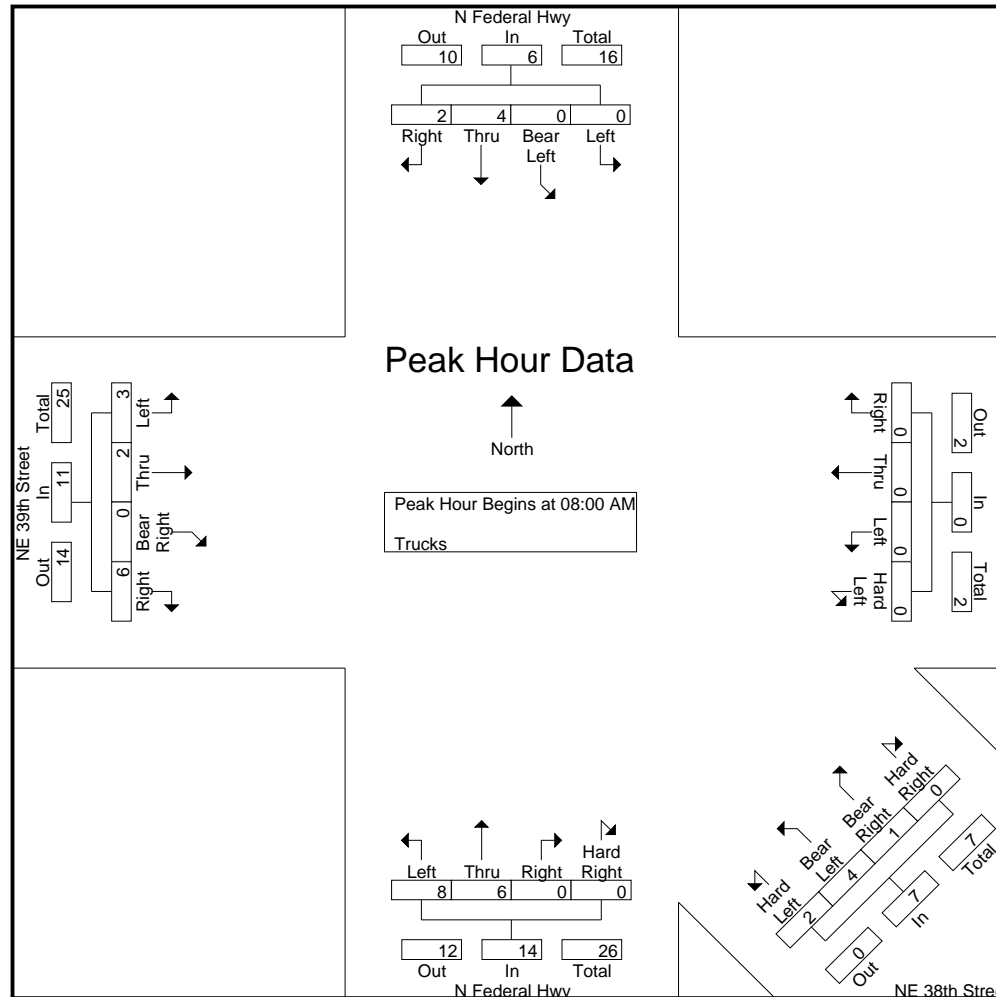
N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	N Federal Hwy Southbound						N Federal Hwy Northbound					Westbound					NE 39th Street Eastbound					NE 38th Street					Int. Total			
	U-Turns	Left	Bear Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	Hard Right	App. Total	Hard Left	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Bear Right	Right	App. Total	Hard Left	Bear Left		Bear Right	Hard Right	App. Total
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																														
Peak Hour for Entire Intersection Begins at 03:00 PM																														
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	1	3	0	2	0	0	2	5
03:15 PM	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2
03:30 PM	0	0	0	0	3	3	0	0	4	0	0	4	0	0	0	0	0	0	0	1	0	0	0	1	1	0	0	0	1	9
03:45 PM	0	0	0	1	1	2	0	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	1	1	1	0	0	0	1	6
Total Volume	0	0	0	1	4	5	0	0	7	0	0	7	0	0	0	0	0	0	0	1	3	0	2	6	2	2	0	0	4	22
% App. Total	0	0	0	20	80		0	0	100	0	0		0	0	0	0	0		0	16.7	50	0	33.3		50	50	0	0		
PHF	.000	.000	.000	.250	.333	.417	.000	.000	.438	.000	.000	.438	.000	.000	.000	.000	.000	.000	.000	.250	.375	.000	.500	.500	.500	.250	.000	.000	.500	.611

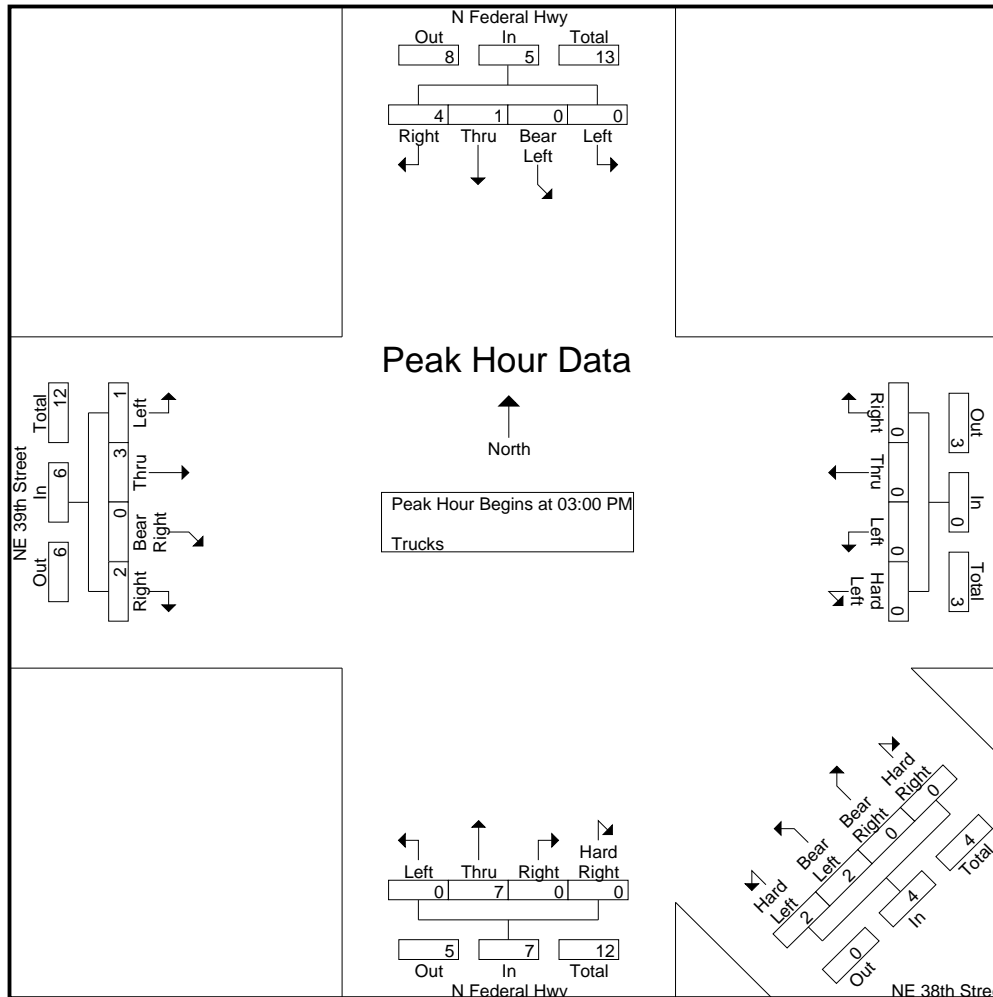
N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	N Federal Hwy Southbound						N Federal Hwy Northbound						Westbound						NE 39th Street Eastbound						NE 38th Street						Int. Total
	U-Turns	Left	Bear Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	Hard Right	App. Total	Hard Left	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Bear Right	Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	App. Total		
07:00 AM	0	0	0	58	13	71	0	4	27	7	0	38	0	0	0	0	0	0	0	7	5	0	7	19	1	18	2	0	21	149	
07:15 AM	0	2	0	69	26	97	0	7	32	3	0	42	0	0	0	0	0	0	0	9	10	0	13	32	11	33	4	0	48	219	
07:30 AM	0	1	0	84	22	107	0	4	63	1	0	68	0	0	0	0	0	0	0	18	8	0	26	52	5	23	3	3	34	261	
07:45 AM	0	3	0	137	19	159	0	2	66	4	0	72	0	0	0	0	0	0	0	11	9	0	13	33	6	36	2	0	44	308	
Total	0	6	0	348	80	434	0	17	188	15	0	220	0	0	0	0	0	0	0	45	32	0	59	136	23	110	11	3	147	937	
08:00 AM	0	6	0	110	21	137	0	6	57	6	0	69	0	0	0	0	0	0	0	17	5	0	12	34	8	22	3	0	33	273	
08:15 AM	0	3	0	117	17	137	0	4	56	0	0	60	0	0	0	0	0	0	0	15	13	0	12	40	2	28	10	0	40	277	
08:30 AM	0	3	0	113	30	146	0	11	64	3	0	78	0	0	0	0	0	0	0	22	15	0	19	56	8	18	10	1	37	317	
08:45 AM	0	5	0	108	25	138	0	6	80	7	0	93	0	0	0	0	0	0	0	36	16	0	15	67	13	32	8	1	54	352	
Total	0	17	0	448	93	558	0	27	257	16	0	300	0	0	0	0	0	0	0	90	49	0	58	197	31	100	31	2	164	1219	
*** BREAK ***																															
03:00 PM	0	0	0	52	8	60	0	6	59	7	0	72	0	0	0	0	0	0	0	11	7	0	10	28	9	42	8	0	59	219	
03:15 PM	0	0	0	30	15	45	0	6	80	2	0	88	0	0	0	0	0	0	0	16	8	0	8	32	10	37	8	0	55	220	
03:30 PM	0	1	0	45	18	64	0	5	116	5	1	127	0	0	0	0	0	0	0	18	4	0	13	35	11	12	5	0	28	254	
03:45 PM	0	3	0	52	12	67	0	13	161	1	0	175	0	0	0	0	0	0	0	25	3	0	17	45	23	31	4	7	65	352	
Total	0	4	0	179	53	236	0	30	416	15	1	462	0	0	0	0	0	0	0	70	22	0	48	140	53	122	25	7	207	1045	
04:00 PM	0	0	0	33	15	48	0	5	134	2	0	141	0	0	0	0	0	0	0	16	11	0	1	28	11	47	10	0	68	285	
04:15 PM	0	0	0	41	13	54	0	11	151	2	1	165	0	0	0	0	0	0	0	23	10	0	6	39	0	35	2	1	38	296	
04:30 PM	0	0	0	48	10	58	0	9	175	0	0	184	0	0	0	0	0	0	0	19	11	0	5	35	7	40	11	2	60	337	
04:45 PM	0	0	0	31	10	41	0	12	146	5	0	163	0	0	0	0	0	0	0	18	4	0	7	29	3	42	12	3	60	293	
Total	0	0	0	153	48	201	0	37	606	9	1	653	0	0	0	0	0	0	0	76	36	0	19	131	21	164	35	6	226	1211	
05:00 PM	0	1	0	48	16	65	0	5	143	5	0	153	0	0	0	0	0	0	0	22	6	0	8	36	4	34	11	0	49	303	
05:15 PM	0	2	0	42	16	60	0	6	176	5	0	187	0	0	0	0	0	0	0	24	8	0	6	38	4	32	13	0	49	334	
05:30 PM	0	1	0	41	15	57	0	5	149	2	0	156	0	0	0	0	0	0	0	34	13	0	6	53	3	32	16	0	51	317	
05:45 PM	0	2	0	40	12	54	0	4	140	0	1	145	0	0	0	0	0	0	0	19	7	0	7	33	5	31	12	0	48	280	
Total	0	6	0	171	59	236	0	20	608	12	1	641	0	0	0	0	0	0	0	99	34	0	27	160	16	129	52	0	197	1234	
Grand Total	0	33	0	1299	333	1665	0	131	2075	67	3	2276	0	0	0	0	0	0	0	380	173	0	211	764	144	625	154	18	941	5646	
Aprch %	0	2	0	78	20		0	5.8	91.2	2.9	0.1		0	0	0	0	0	0	0	49.7	22.6	0	27.6		15.3	66.4	16.4	1.9			
Total %	0	0.6	0	23	5.9	29.5	0	2.3	36.8	1.2	0.1	40.3	0	0	0	0	0	0	0	6.7	3.1	0	3.7	13.5	2.6	11.1	2.7	0.3	16.7		
Vehicle	0	33	0	1289	322	1644	0	117	2050	67	3	2237	0	0	0	0	0	0	0	374	166	0	200	740	139	618	152	18	927	5548	
% Vehicle	0	100	0	99.2	96.7	98.7	0	89.3	98.8	100	100	98.3	0	0	0	0	0	0	0	98.4	96	0	94.8	96.9	96.5	98.9	98.7	100	98.5	98.3	

N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	N Federal Hwy Southbound						N Federal Hwy Northbound						Westbound						NE 39th Street Eastbound						NE 38th Street					
	U-Turns	Left	Bear Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	Hard Right	App. Total	Hard Left	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Bear Right	Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	App. Total	Int. Total
Trucks	0	0	0	10	11	21	0	14	25	0	0	39	0	0	0	0	0	0	0	6	7	0	11	24	5	7	2	0	14	98
% Trucks	0	0	0	0.8	3.3	1.3	0	10.7	1.2	0	0	1.7	0	0	0	0	0	0	0	1.6	4	0	5.2	3.1	3.5	1.1	1.3	0	1.5	1.7

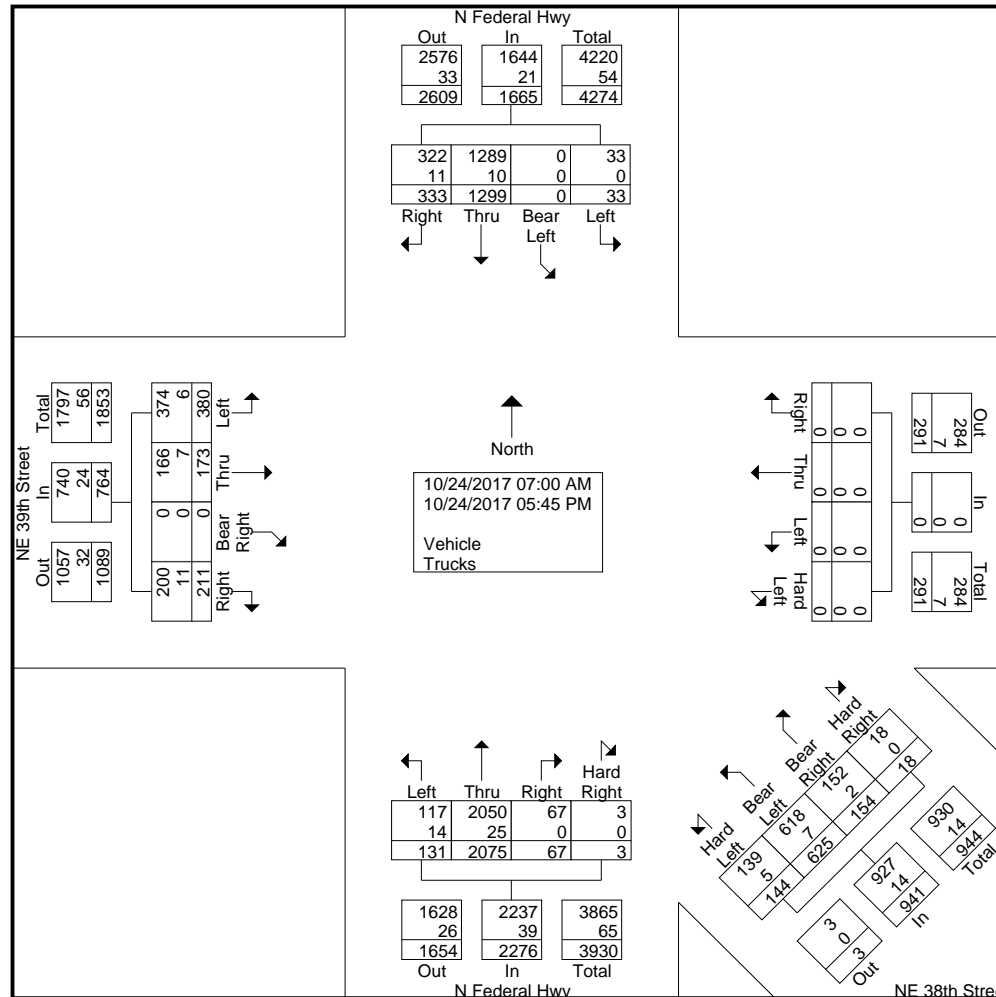
N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4

Start Time	N Federal Hwy Southbound						N Federal Hwy Northbound						Westbound						NE 39th Street Eastbound						NE 38th Street						Int. Total
	U-Turns	Left	Bear Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	Hard Right	App. Total	Hard Left	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Bear Right	Right	App. Total	Hard Left	Bear Left	Bear Right	Hard Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																															
Peak Hour for Entire Intersection Begins at 08:00 AM																															
08:00 AM	0	6	0	110	21	137	0	6	57	6	0	69	0	0	0	0	0	0	0	17	5	0	12	34	8	22	3	0	33	273	
08:15 AM	0	3	0	117	17	137	0	4	56	0	0	60	0	0	0	0	0	0	0	15	13	0	12	40	2	28	10	0	40	277	
08:30 AM	0	3	0	113	30	146	0	11	64	3	0	78	0	0	0	0	0	0	0	22	15	0	19	56	8	18	10	1	37	317	
08:45 AM	0	5	0	108	25	138	0	6	80	7	0	93	0	0	0	0	0	0	0	36	16	0	15	67	13	32	8	1	54	352	
Total Volume	0	17	0	448	93	558	0	27	257	16	0	300	0	0	0	0	0	0	0	90	49	0	58	197	31	100	31	2	164	1219	
% App. Total	0	3	0	80.3	16.7		0	9	85.7	5.3	0		0	0	0	0	0	0	0	45.7	24.9	0	29.4		18.9	61	18.9	1.2			
PHF	.000	.708	.000	.957	.775	.955	.000	.614	.803	.571	.000	.806	.000	.000	.000	.000	.000	.000	.000	.000	.625	.766	.000	.763	.735	.596	.781	.775	.500	.759	.866

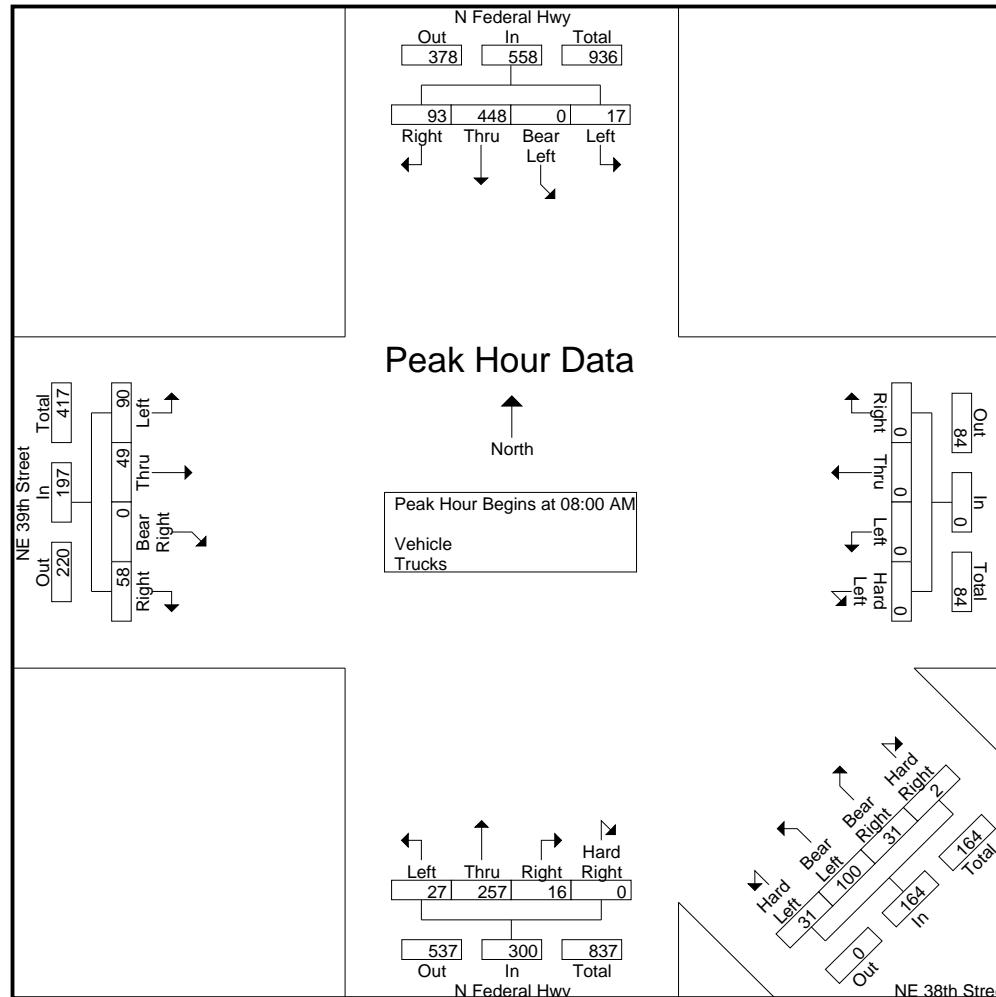
N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 6

Start Time	N Federal Hwy Southbound						N Federal Hwy Northbound					Westbound					NE 39th Street Eastbound					NE 38th Street					Int. Total				
	U-Turns	Left	Bear Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	Hard Right	App. Total	Hard Left	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Bear Right	Right	App. Total	Hard Left	Bear Left		Bear Right	Hard Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																															
Peak Hour for Entire Intersection Begins at 03:45 PM																															
03:45 PM	0	3	0	52	12	67	0	13	161	1	0	175	0	0	0	0	0	0	0	25	3	0	17	45	23	31	4	7	65	352	
04:00 PM	0	0	0	33	15	48	0	5	134	2	0	141	0	0	0	0	0	0	0	0	16	11	0	1	28	11	47	10	0	68	285
04:15 PM	0	0	0	41	13	54	0	11	151	2	1	165	0	0	0	0	0	0	0	0	23	10	0	6	39	0	35	2	1	38	296
04:30 PM	0	0	0	48	10	58	0	9	175	0	0	184	0	0	0	0	0	0	0	0	19	11	0	5	35	7	40	11	2	60	337
Total Volume	0	3	0	174	50	227	0	38	621	5	1	665	0	0	0	0	0	0	0	0	83	35	0	29	147	41	153	27	10	231	1270
% App. Total	0	1.3	0	76.7	22		0	5.7	93.4	0.8	0.2		0	0	0	0	0	0	0	0	56.5	23.8	0	19.7		17.7	66.2	11.7	4.3		
PHF	.000	.250	.000	.837	.833	.847	.000	.731	.887	.625	.250	.904	.000	.000	.000	.000	.000	.000	.000	.000	.830	.795	.000	.426	.817	.446	.814	.614	.357	.849	.902

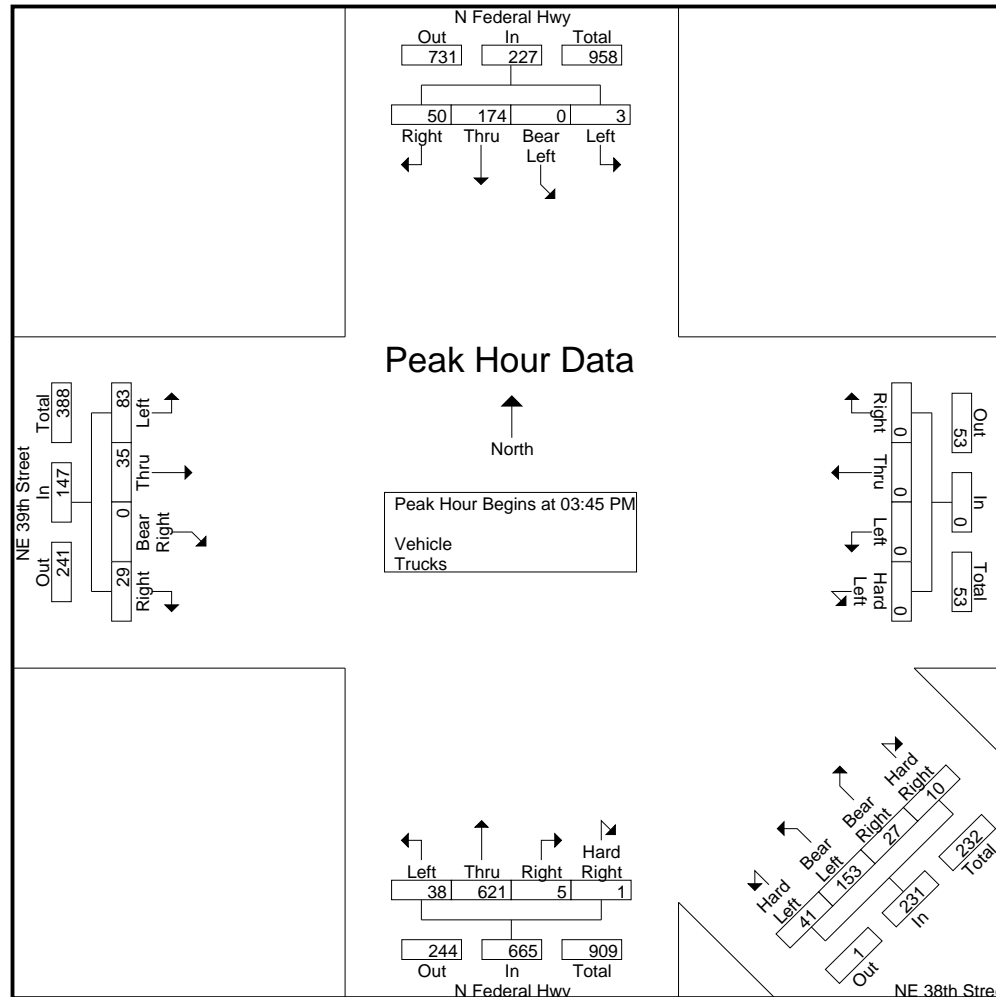
N Federal Hwy & NE 39th/38th Street

File Name : TMC-12 N Federal Hwy & NE 39th-38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Peds & Bikes

Start Time	US-1/Biscayne Blvd Southbound			US-1/Biscayne Blvd Northbound			NE 36th Street Westbound			NE 36th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	6	0	6	4	1	5	2	1	3	2	2	4	18
07:15 AM	4	0	4	4	0	4	3	0	3	5	1	6	17
07:30 AM	6	2	8	2	0	2	1	2	3	4	0	4	17
07:45 AM	2	0	2	3	0	3	1	3	4	2	2	4	13
Total	18	2	20	13	1	14	7	6	13	13	5	18	65
08:00 AM	3	1	4	3	0	3	3	2	5	6	2	8	20
08:15 AM	1	0	1	4	0	4	2	1	3	3	1	4	12
08:30 AM	1	0	1	5	0	5	7	1	8	11	3	14	28
08:45 AM	0	0	0	3	0	3	1	1	2	6	2	8	13
Total	5	1	6	15	0	15	13	5	18	26	8	34	73
*** BREAK ***													
03:00 PM	0	0	0	3	0	3	3	2	5	8	0	8	16
03:15 PM	7	0	7	2	1	3	5	0	5	4	0	4	19
03:30 PM	1	0	1	3	0	3	2	0	2	1	0	1	7
03:45 PM	4	2	6	2	0	2	3	1	4	3	0	3	15
Total	12	2	14	10	1	11	13	3	16	16	0	16	57
04:00 PM	3	1	4	5	0	5	2	0	2	1	1	2	13
04:15 PM	1	0	1	2	1	3	1	0	1	3	0	3	8
04:30 PM	0	0	0	4	0	4	2	0	2	7	3	10	16
04:45 PM	2	1	3	4	0	4	2	2	4	4	0	4	15
Total	6	2	8	15	1	16	7	2	9	15	4	19	52
05:00 PM	1	1	2	4	1	5	9	3	12	1	1	2	21
05:15 PM	6	0	6	10	1	11	5	3	8	8	1	9	34
05:30 PM	4	0	4	9	0	9	3	2	5	2	0	2	20
05:45 PM	0	0	0	5	0	5	9	1	10	6	2	8	23
Total	11	1	12	28	2	30	26	9	35	17	4	21	98
Grand Total	52	8	60	81	5	86	66	25	91	87	21	108	345
Apprch %	86.7	13.3		94.2	5.8		72.5	27.5		80.6	19.4		
Total %	15.1	2.3	17.4	23.5	1.4	24.9	19.1	7.2	26.4	25.2	6.1	31.3	

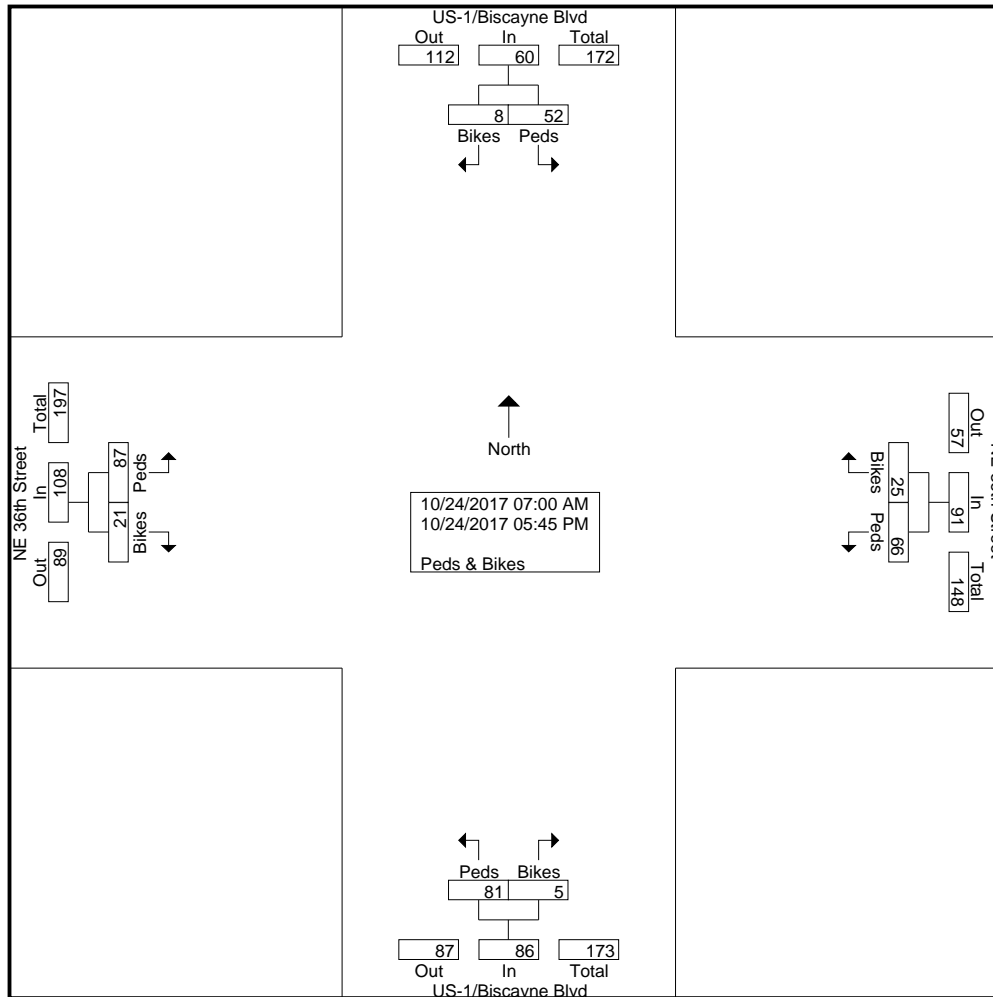
Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	US-1/Biscayne Blvd Southbound			US-1/Biscayne Blvd Northbound			NE 36th Street Westbound			NE 36th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:45 AM													
07:45 AM	2	0	2	3	0	3	1	3	4	2	2	4	13
08:00 AM	3	1	4	3	0	3	3	2	5	6	2	8	20
08:15 AM	1	0	1	4	0	4	2	1	3	3	1	4	12
08:30 AM	1	0	1	5	0	5	7	1	8	11	3	14	28
Total Volume	7	1	8	15	0	15	13	7	20	22	8	30	73
% App. Total	87.5	12.5		100	0		65	35		73.3	26.7		
PHF	.583	.250	.500	.750	.000	.750	.464	.583	.625	.500	.667	.536	.652

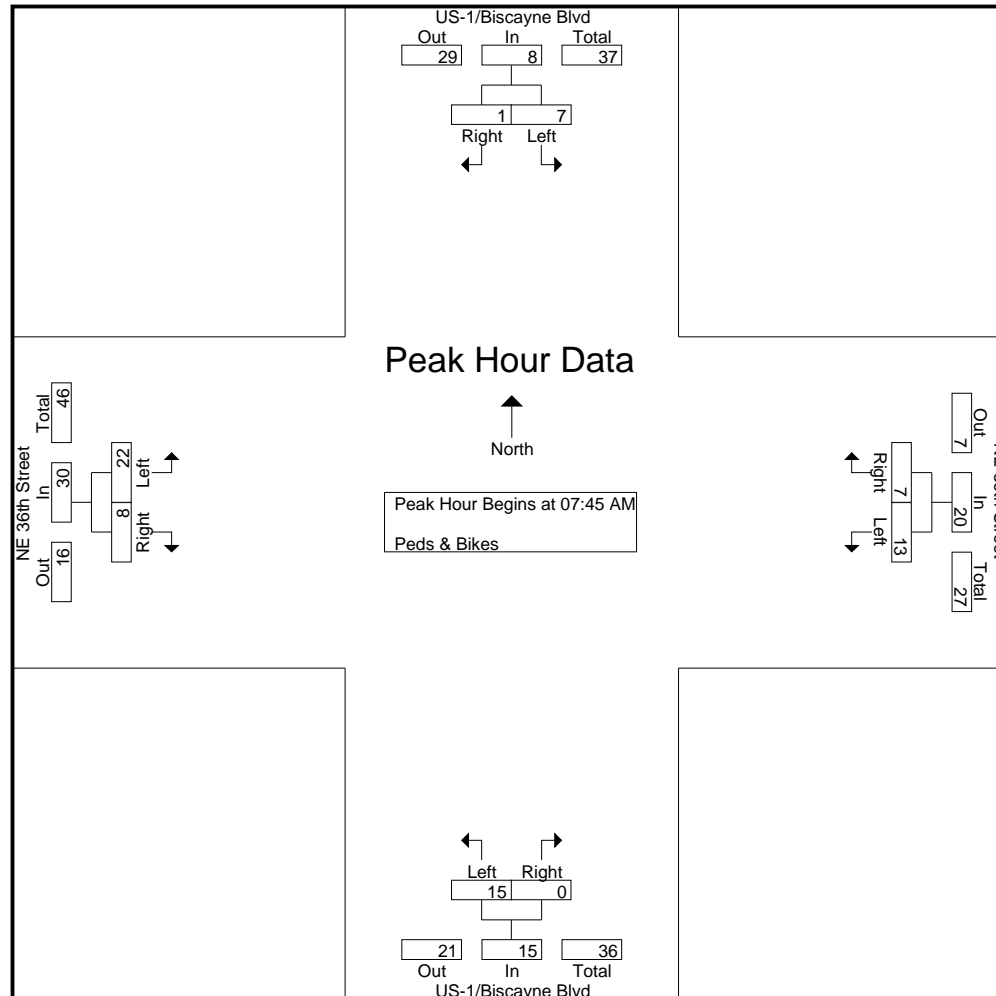
Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5

Start Time	US-1/Biscayne Blvd Southbound			US-1/Biscayne Blvd Northbound			NE 36th Street Westbound			NE 36th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 05:00 PM													
05:00 PM	1	1	2	4	1	5	9	3	12	1	1	2	21
05:15 PM	6	0	6	10	1	11	5	3	8	8	1	9	34
05:30 PM	4	0	4	9	0	9	3	2	5	2	0	2	20
05:45 PM	0	0	0	5	0	5	9	1	10	6	2	8	23
Total Volume	11	1	12	28	2	30	26	9	35	17	4	21	98
% App. Total	91.7	8.3		93.3	6.7		74.3	25.7		81	19		
PHF	.458	.250	.500	.700	.500	.682	.722	.750	.729	.531	.500	.583	.721

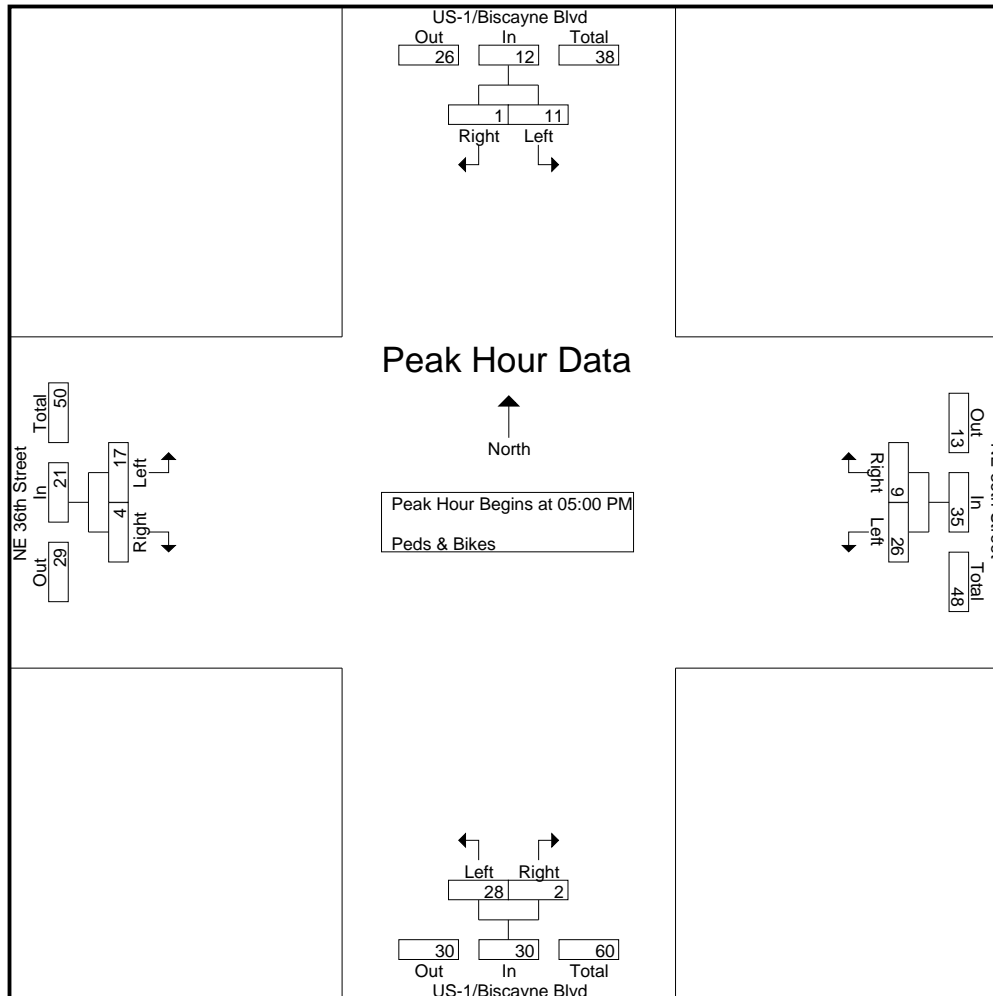
Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Trucks

Start Time	US-1/Biscayne Blvd Southbound					US-1/Biscayne Blvd Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	1	7	0	8	0	2	2	0	4	0	0	0	2	2	0	2	0	2	4	18
07:15 AM	0	2	5	1	8	0	1	5	0	6	0	4	3	1	8	0	0	2	2	4	26
07:30 AM	0	1	5	2	8	0	0	3	0	3	0	3	2	2	7	0	1	1	3	5	23
07:45 AM	0	4	3	0	7	0	0	5	3	8	0	9	0	0	9	0	2	4	2	8	32
Total	0	8	20	3	31	0	3	15	3	21	0	16	5	5	26	0	5	7	9	21	99
08:00 AM	0	3	2	1	6	0	2	3	1	6	0	6	1	2	9	0	2	0	1	3	24
08:15 AM	0	2	4	0	6	0	1	5	0	6	0	4	2	3	9	0	0	3	0	3	24
08:30 AM	0	4	5	4	13	0	0	3	1	4	0	6	0	5	11	0	0	3	3	6	34
08:45 AM	0	1	4	1	6	0	2	5	2	9	0	6	1	1	8	0	1	4	1	6	29
Total	0	10	15	6	31	0	5	16	4	25	0	22	4	11	37	0	3	10	5	18	111
*** BREAK ***																					
03:00 PM	0	0	5	1	6	0	1	4	1	6	0	3	1	3	7	0	1	3	0	4	23
03:15 PM	0	0	2	4	6	0	1	4	0	5	0	1	0	5	6	0	0	0	1	1	18
03:30 PM	0	2	3	0	5	0	0	4	0	4	0	1	1	2	4	0	0	1	2	3	16
03:45 PM	0	1	4	0	5	0	1	6	0	7	0	2	1	3	6	0	1	0	2	3	21
Total	0	3	14	5	22	0	3	18	1	22	0	7	3	13	23	0	2	4	5	11	78
04:00 PM	0	0	1	4	5	0	0	6	0	6	0	2	0	1	3	0	0	1	0	1	15
04:15 PM	0	0	4	0	4	0	0	5	0	5	0	1	0	1	2	0	0	1	0	1	12
04:30 PM	0	0	2	1	3	0	0	4	0	4	0	0	0	0	0	0	1	1	1	3	10
04:45 PM	0	1	0	0	1	0	0	6	1	7	0	0	0	3	3	0	0	1	1	2	13
Total	0	1	7	5	13	0	0	21	1	22	0	3	0	5	8	0	1	4	2	7	50
05:00 PM	0	2	3	1	6	0	1	3	0	4	0	1	0	2	3	0	1	0	0	1	14
05:15 PM	0	0	2	1	3	0	0	8	0	8	0	0	0	0	0	0	0	0	0	0	11
05:30 PM	0	1	5	3	9	0	0	1	0	1	0	1	0	0	1	0	0	2	2	4	15
05:45 PM	0	0	1	1	2	0	0	8	0	8	0	1	1	0	2	0	0	1	0	1	13
Total	0	3	11	6	20	0	1	20	0	21	0	3	1	2	6	0	1	3	2	6	53
Grand Total	0	25	67	25	117	0	12	90	9	111	0	51	13	36	100	0	12	28	23	63	391
Apprch %	0	21.4	57.3	21.4		0	10.8	81.1	8.1		0	51	13	36		0	19	44.4	36.5		
Total %	0	6.4	17.1	6.4	29.9	0	3.1	23	2.3	28.4	0	13	3.3	9.2	25.6	0	3.1	7.2	5.9	16.1	

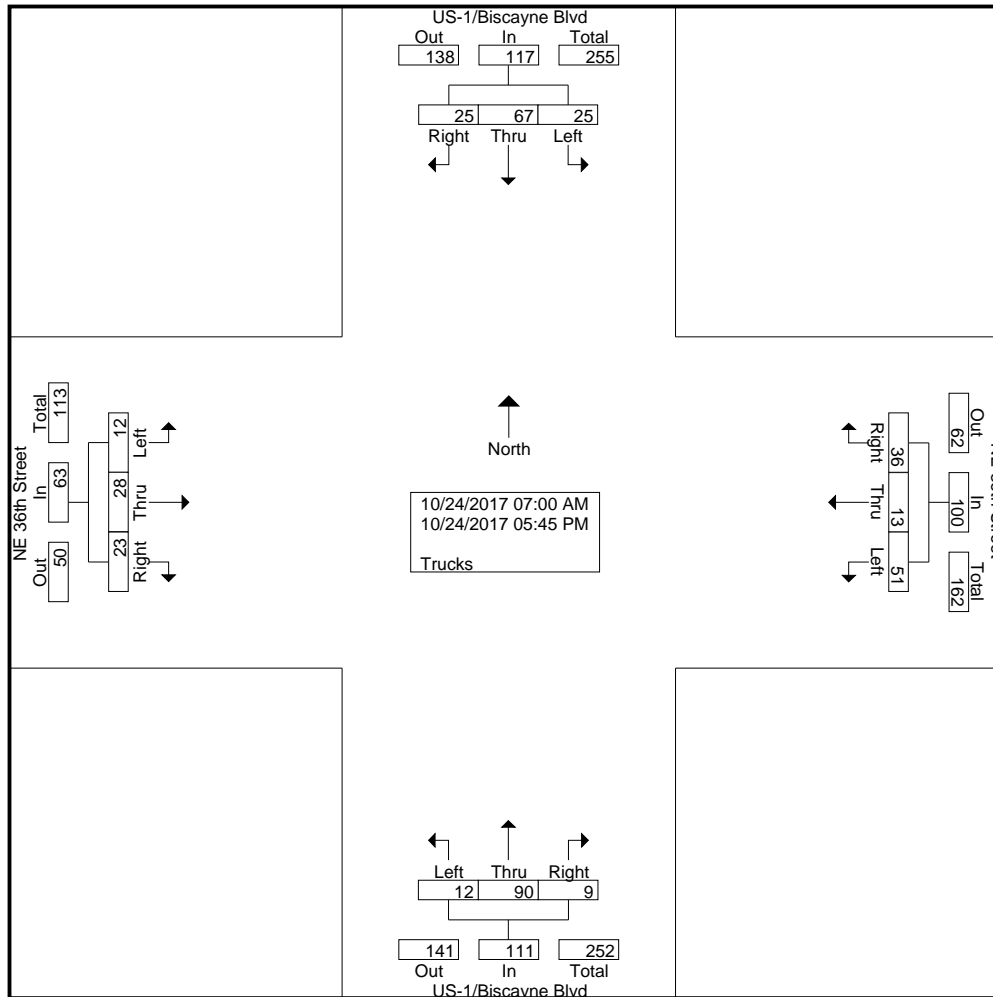
Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	US-1/Biscayne Blvd Southbound					US-1/Biscayne Blvd Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:45 AM																					
07:45 AM	0	4	3	0	7	0	0	5	3	8	0	9	0	0	9	0	2	4	2	8	32
08:00 AM	0	3	2	1	6	0	2	3	1	6	0	6	1	2	9	0	2	0	1	3	24
08:15 AM	0	2	4	0	6	0	1	5	0	6	0	4	2	3	9	0	0	3	0	3	24
08:30 AM	0	4	5	4	13	0	0	3	1	4	0	6	0	5	11	0	0	3	3	6	34
Total Volume	0	13	14	5	32	0	3	16	5	24	0	25	3	10	38	0	4	10	6	20	114
% App. Total	0	40.6	43.8	15.6		0	12.5	66.7	20.8		0	65.8	7.9	26.3		0	20	50	30		
PHF	.000	.813	.700	.313	.615	.000	.375	.800	.417	.750	.000	.694	.375	.500	.864	.000	.500	.625	.500	.625	.838

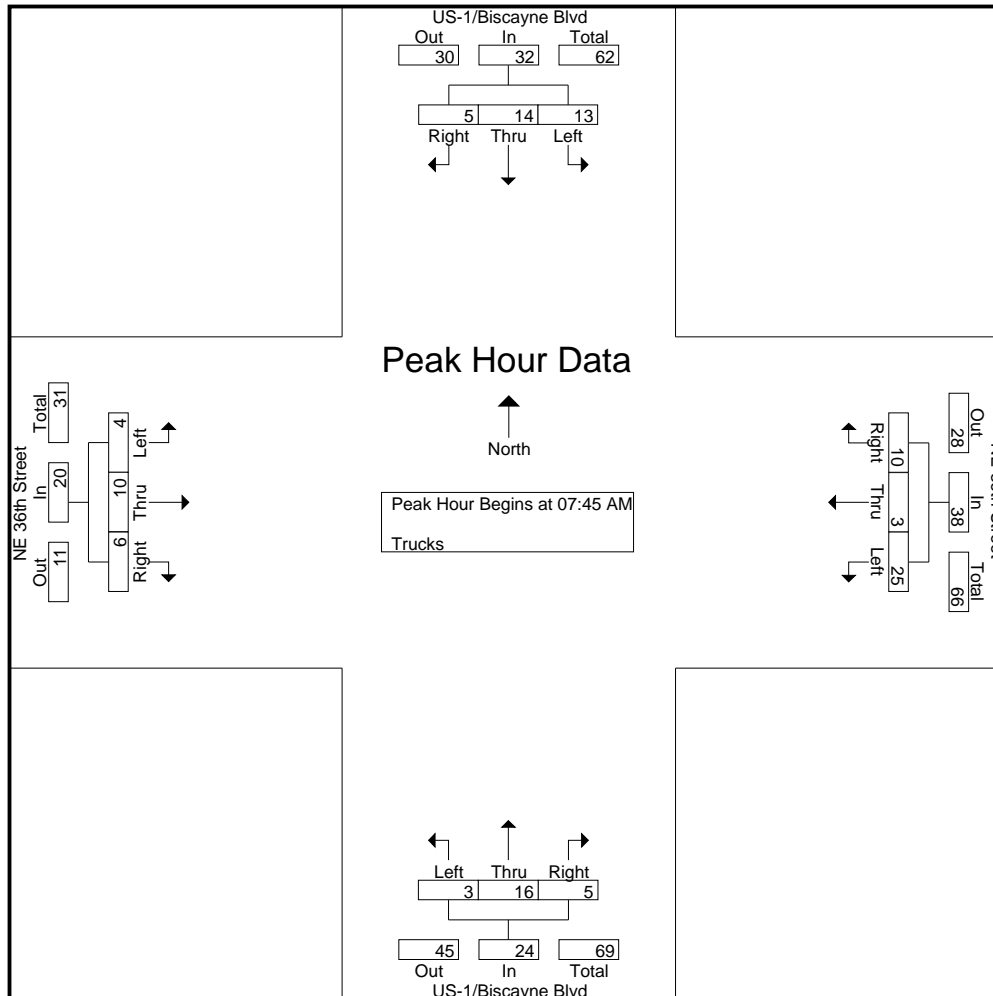
Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5

Start Time	US-1/Biscayne Blvd Southbound					US-1/Biscayne Blvd Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:00 PM																					
03:00 PM	0	0	5	1	6	0	1	4	1	6	0	3	1	3	7	0	1	3	0	4	23
03:15 PM	0	0	2	4	6	0	1	4	0	5	0	1	0	5	6	0	0	0	1	1	18
03:30 PM	0	2	3	0	5	0	0	4	0	4	0	1	1	2	4	0	0	1	2	3	16
03:45 PM	0	1	4	0	5	0	1	6	0	7	0	2	1	3	6	0	1	0	2	3	21
Total Volume	0	3	14	5	22	0	3	18	1	22	0	7	3	13	23	0	2	4	5	11	78
% App. Total	0	13.6	63.6	22.7		0	13.6	81.8	4.5		0	30.4	13	56.5		0	18.2	36.4	45.5		
PHF	.000	.375	.700	.313	.917	.000	.750	.750	.250	.786	.000	.583	.750	.650	.821	.000	.500	.333	.625	.688	.848

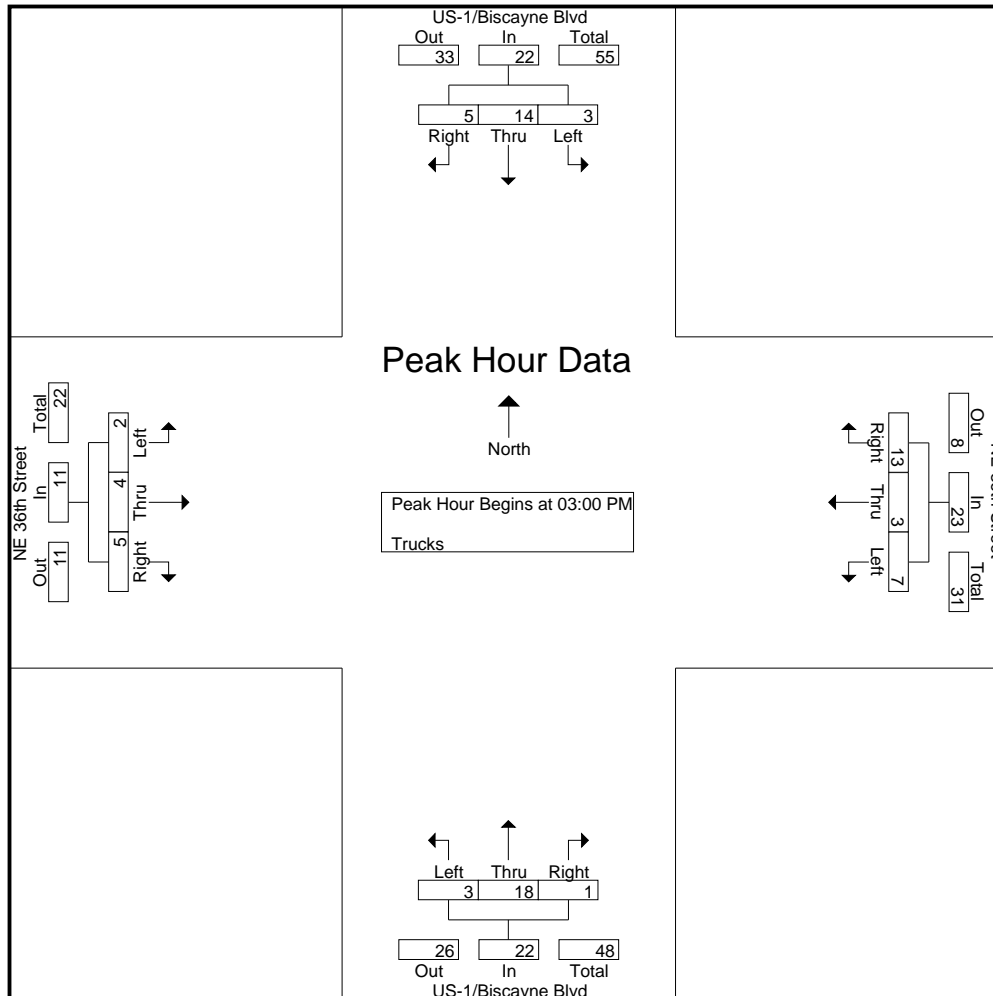
Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	US-1/Biscayne Blvd Southbound					US-1/Biscayne Blvd Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	88	225	21	334	0	10	142	40	192	0	81	21	61	163	0	9	52	41	102	791
07:15 AM	0	78	229	15	322	0	7	158	45	210	0	90	21	61	172	0	5	75	30	110	814
07:30 AM	0	80	257	26	363	0	4	169	44	217	0	109	26	83	218	0	19	83	33	135	933
07:45 AM	0	104	267	23	394	0	5	163	60	228	0	99	21	76	196	0	10	63	41	114	932
Total	0	350	978	85	1413	0	26	632	189	847	0	379	89	281	749	0	43	273	145	461	3470
08:00 AM	0	93	253	24	370	0	11	168	58	237	0	102	23	88	213	0	14	66	34	114	934
08:15 AM	0	114	207	19	340	0	7	169	35	211	0	112	21	84	217	0	12	92	39	143	911
08:30 AM	0	105	241	16	362	0	3	147	47	197	0	110	28	81	219	0	12	74	41	127	905
08:45 AM	0	102	276	30	408	0	4	174	41	219	0	108	25	92	225	0	18	67	31	116	968
Total	0	414	977	89	1480	0	25	658	181	864	0	432	97	345	874	0	56	299	145	500	3718
*** BREAK ***																					
03:00 PM	0	81	198	42	321	0	12	162	42	216	0	67	19	98	184	0	21	56	28	105	826
03:15 PM	0	82	148	72	302	0	13	167	42	222	0	51	13	84	148	0	18	50	26	94	766
03:30 PM	0	82	187	58	327	0	6	179	30	215	0	65	25	94	184	0	18	69	31	118	844
03:45 PM	0	60	149	86	295	0	9	163	29	201	0	64	27	89	180	0	28	74	26	128	804
Total	0	305	682	258	1245	0	40	671	143	854	0	247	84	365	696	0	85	249	111	445	3240
04:00 PM	0	86	170	57	313	0	12	191	49	252	0	62	28	55	145	0	32	76	38	146	856
04:15 PM	0	70	173	70	313	0	13	173	33	219	0	49	19	81	149	0	22	64	26	112	793
04:30 PM	0	73	157	78	308	0	12	154	42	208	0	53	22	103	178	0	18	69	20	107	801
04:45 PM	0	93	161	60	314	0	14	173	43	230	0	84	38	86	208	0	21	77	23	121	873
Total	0	322	661	265	1248	0	51	691	167	909	0	248	107	325	680	0	93	286	107	486	3323
05:00 PM	0	75	166	65	306	0	9	172	33	214	0	82	29	86	197	0	16	67	24	107	824
05:15 PM	0	82	165	76	323	0	7	145	43	195	0	67	23	93	183	0	22	66	35	123	824
05:30 PM	0	67	153	62	282	0	12	183	50	245	0	76	25	79	180	0	20	70	33	123	830
05:45 PM	0	85	163	63	311	0	4	187	48	239	0	84	30	107	221	0	22	57	28	107	878
Total	0	309	647	266	1222	0	32	687	174	893	0	309	107	365	781	0	80	260	120	460	3356
Grand Total	0	1700	3945	963	6608	0	174	3339	854	4367	0	1615	484	1681	3780	0	357	1367	628	2352	17107
Apprch %	0	25.7	59.7	14.6		0	4	76.5	19.6		0	42.7	12.8	44.5		0	15.2	58.1	26.7		
Total %	0	9.9	23.1	5.6	38.6	0	1	19.5	5	25.5	0	9.4	2.8	9.8	22.1	0	2.1	8	3.7	13.7	
Vehicle	0	1675	3878	938	6491	0	162	3249	845	4256	0	1564	471	1645	3680	0	345	1339	605	2289	16716
% Vehicle	0	98.5	98.3	97.4	98.2	0	93.1	97.3	98.9	97.5	0	96.8	97.3	97.9	97.4	0	96.6	98	96.3	97.3	97.7

Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2

Groups Printed- Vehicle - Trucks

	US-1/Biscayne Blvd Southbound					US-1/Biscayne Blvd Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	25	67	25	117	0	12	90	9	111	0	51	13	36	100	0	12	28	23	63	391
% Trucks	0	1.5	1.7	2.6	1.8	0	6.9	2.7	1.1	2.5	0	3.2	2.7	2.1	2.6	0	3.4	2	3.7	2.7	2.3

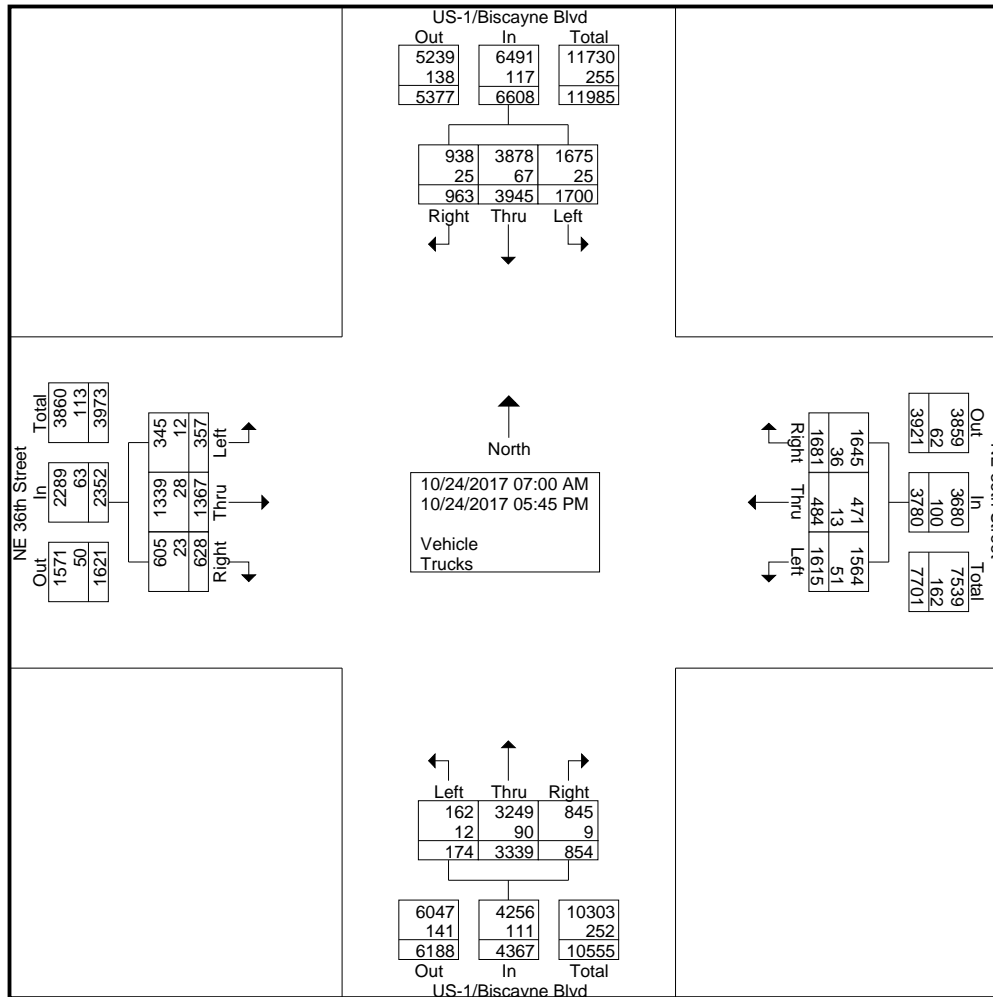
Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4

Start Time	US-1/Biscayne Blvd Southbound					US-1/Biscayne Blvd Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 08:00 AM																					
08:00 AM	0	93	253	24	370	0	11	168	58	237	0	102	23	88	213	0	14	66	34	114	934
08:15 AM	0	114	207	19	340	0	7	169	35	211	0	112	21	84	217	0	12	92	39	143	911
08:30 AM	0	105	241	16	362	0	3	147	47	197	0	110	28	81	219	0	12	74	41	127	905
08:45 AM	0	102	276	30	408	0	4	174	41	219	0	108	25	92	225	0	18	67	31	116	968
Total Volume	0	414	977	89	1480	0	25	658	181	864	0	432	97	345	874	0	56	299	145	500	3718
% App. Total	0	28	66	6		0	2.9	76.2	20.9		0	49.4	11.1	39.5		0	11.2	59.8	29		
PHF	.000	.908	.885	.742	.907	.000	.568	.945	.780	.911	.000	.964	.866	.938	.971	.000	.778	.813	.884	.874	.960

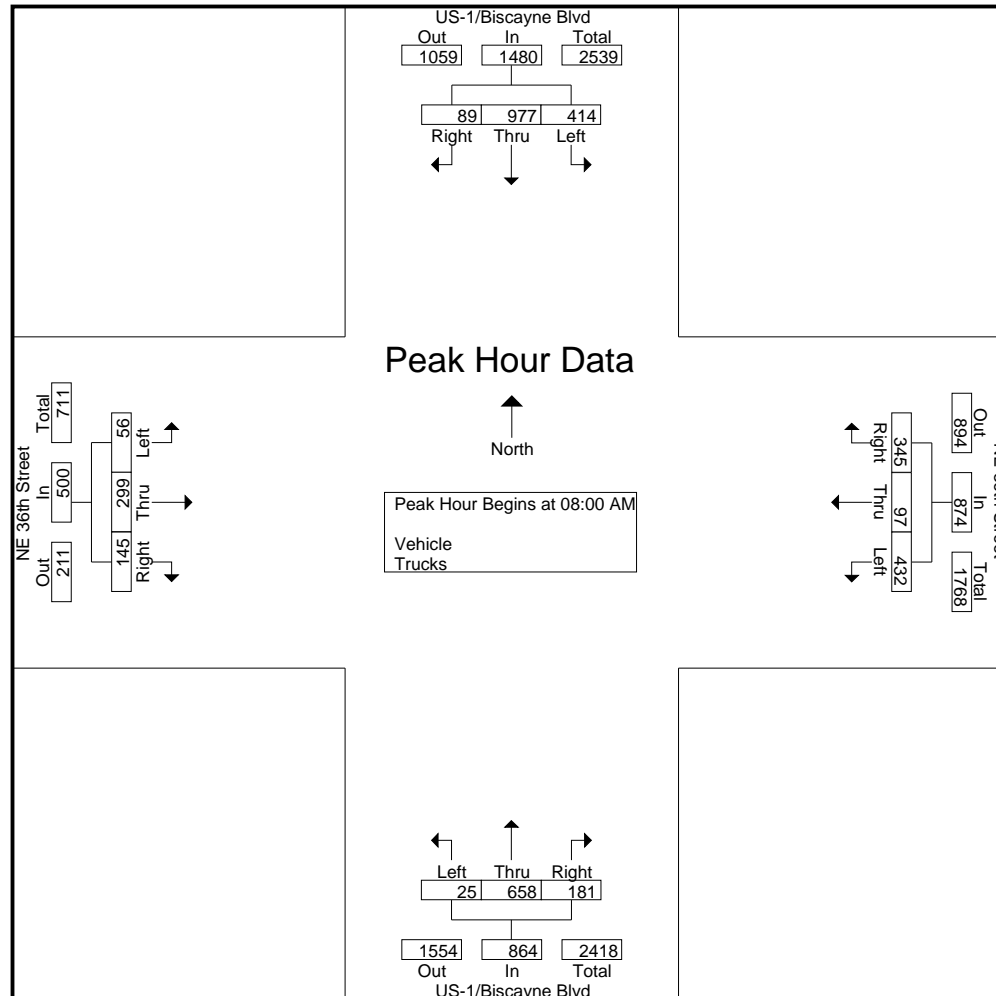
Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6

Start Time	US-1/Biscayne Blvd Southbound					US-1/Biscayne Blvd Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 05:00 PM																					
05:00 PM	0	75	166	65	306	0	9	172	33	214	0	82	29	86	197	0	16	67	24	107	824
05:15 PM	0	82	165	76	323	0	7	145	43	195	0	67	23	93	183	0	22	66	35	123	824
05:30 PM	0	67	153	62	282	0	12	183	50	245	0	76	25	79	180	0	20	70	33	123	830
05:45 PM	0	85	163	63	311	0	4	187	48	239	0	84	30	107	221	0	22	57	28	107	878
Total Volume	0	309	647	266	1222	0	32	687	174	893	0	309	107	365	781	0	80	260	120	460	3356
% App. Total	0	25.3	52.9	21.8		0	3.6	76.9	19.5		0	39.6	13.7	46.7		0	17.4	56.5	26.1		
PHF	.000	.909	.974	.875	.946	.000	.667	.918	.870	.911	.000	.920	.892	.853	.883	.000	.909	.929	.857	.935	.956

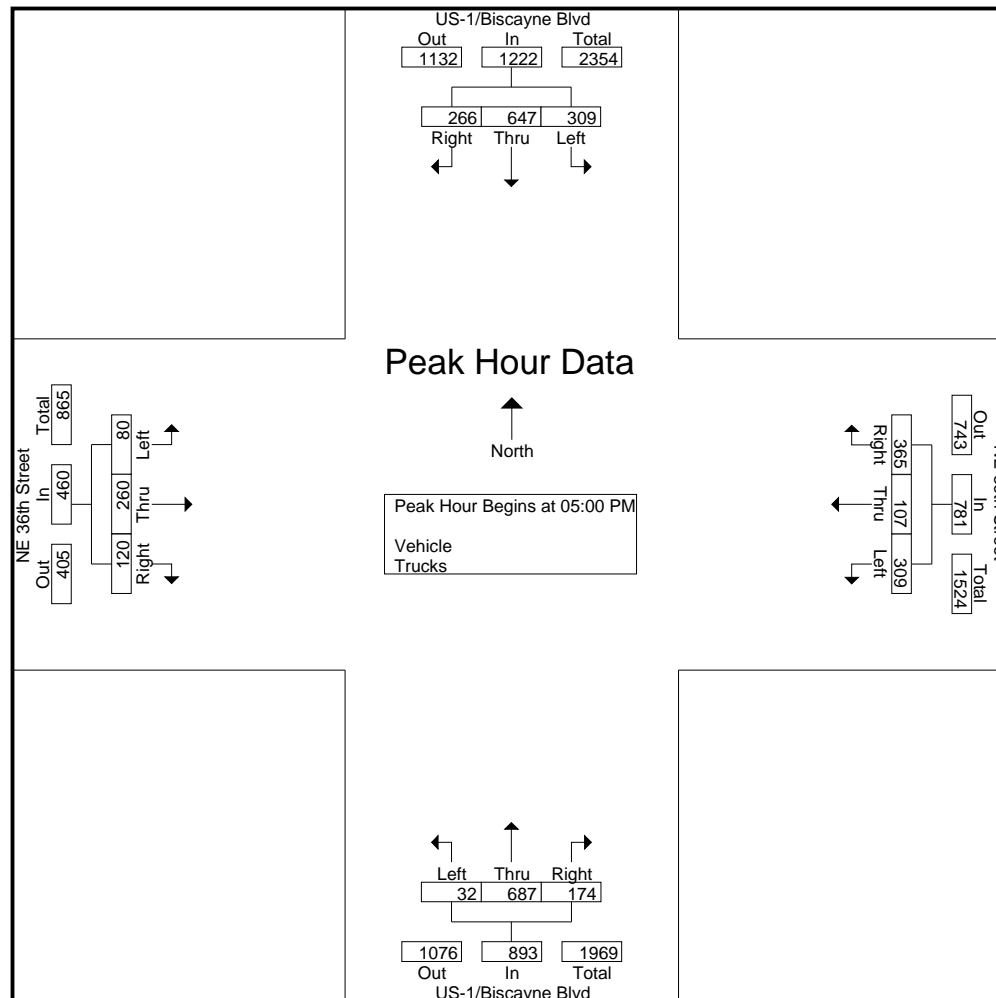
Biscayne Blvd & NE 36th Street

File Name : TMC-13 US-1-Biscayne Blvd & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Peds & Bikes

Start Time	US-1/Biscayne Blvd Southbound			US-1/Biscayne Blvd Northbound			NE 38th Street Westbound			NE 38th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	0	0	0	0	0	0	0	0	0	2	2	4	4
07:15 AM	1	0	1	0	0	0	2	0	2	4	2	6	9
07:30 AM	5	0	5	0	0	0	2	1	3	5	0	5	13
07:45 AM	1	0	1	0	0	0	2	0	2	4	1	5	8
Total	7	0	7	0	0	0	6	1	7	15	5	20	34
08:00 AM	4	0	4	0	0	0	5	0	5	4	1	5	14
08:15 AM	2	0	2	0	0	0	2	1	3	2	3	5	10
08:30 AM	2	0	2	1	0	1	1	1	2	5	2	7	12
08:45 AM	1	0	1	0	0	0	4	0	4	3	0	3	8
Total	9	0	9	1	0	1	12	2	14	14	6	20	44
*** BREAK ***													
03:00 PM	1	1	2	0	0	0	3	1	4	3	2	5	11
03:15 PM	0	0	0	0	0	0	2	0	2	1	0	1	3
03:30 PM	0	0	0	9	1	10	0	0	0	7	0	7	17
03:45 PM	0	0	0	0	0	0	2	3	5	1	1	2	7
Total	1	1	2	9	1	10	7	4	11	12	3	15	38
04:00 PM	1	0	1	0	0	0	3	1	4	2	1	3	8
04:15 PM	0	0	0	0	0	0	1	0	1	1	5	6	7
04:30 PM	3	0	3	0	0	0	2	1	3	5	2	7	13
04:45 PM	6	0	6	1	0	1	3	0	3	0	2	2	12
Total	10	0	10	1	0	1	9	2	11	8	10	18	40
05:00 PM	1	0	1	0	2	2	2	1	3	0	2	2	8
05:15 PM	0	0	0	0	0	0	10	0	10	1	0	1	11
05:30 PM	4	0	4	1	0	1	8	1	9	2	1	3	17
05:45 PM	1	0	1	0	0	0	3	1	4	5	2	7	12
Total	6	0	6	1	2	3	23	3	26	8	5	13	48
Grand Total	33	1	34	12	3	15	57	12	69	57	29	86	204
Apprch %	97.1	2.9		80	20		82.6	17.4		66.3	33.7		
Total %	16.2	0.5	16.7	5.9	1.5	7.4	27.9	5.9	33.8	27.9	14.2	42.2	

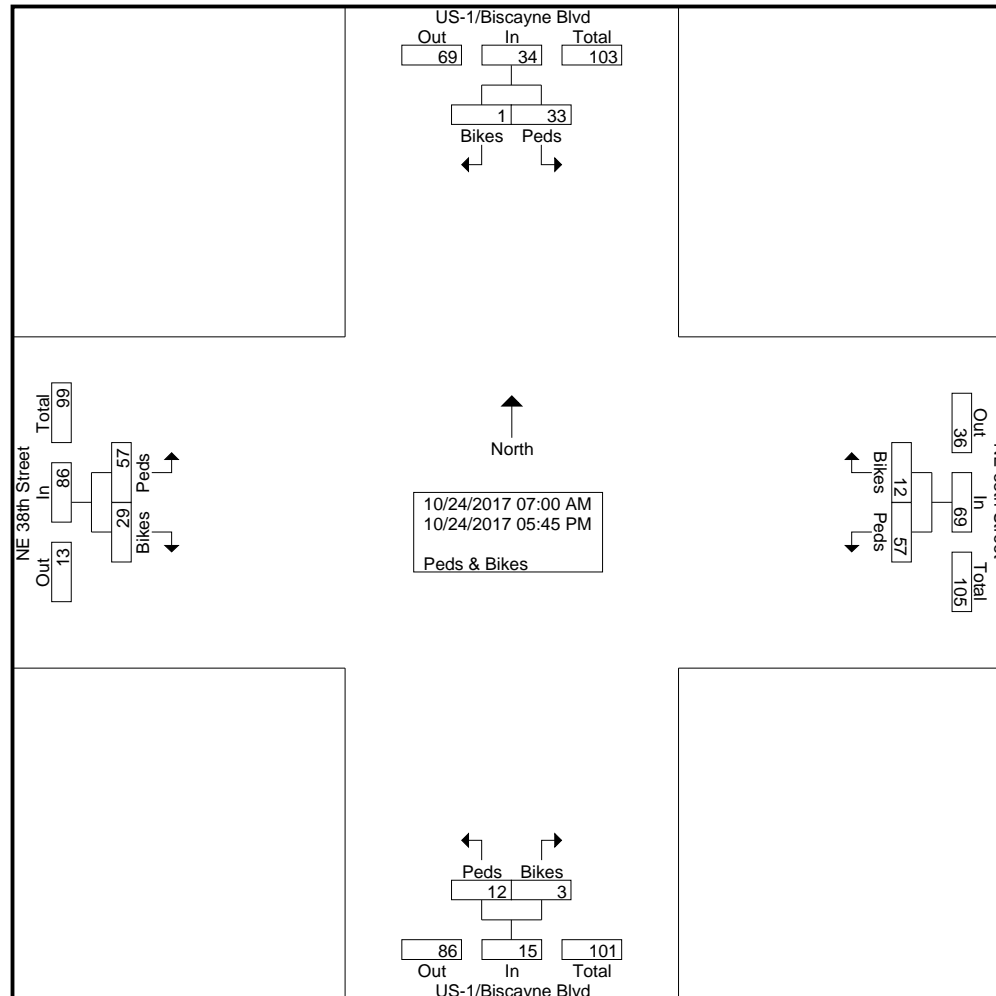
Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	US-1/Biscayne Blvd Southbound			US-1/Biscayne Blvd Northbound			NE 38th Street Westbound			NE 38th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:30 AM													
07:30 AM	5	0	5	0	0	0	2	1	3	5	0	5	13
07:45 AM	1	0	1	0	0	0	2	0	2	4	1	5	8
08:00 AM	4	0	4	0	0	0	5	0	5	4	1	5	14
08:15 AM	2	0	2	0	0	0	2	1	3	2	3	5	10
Total Volume	12	0	12	0	0	0	11	2	13	15	5	20	45
% App. Total	100	0		0	0		84.6	15.4		75	25		
PHF	.600	.000	.600	.000	.000	.000	.550	.500	.650	.750	.417	1.00	.804

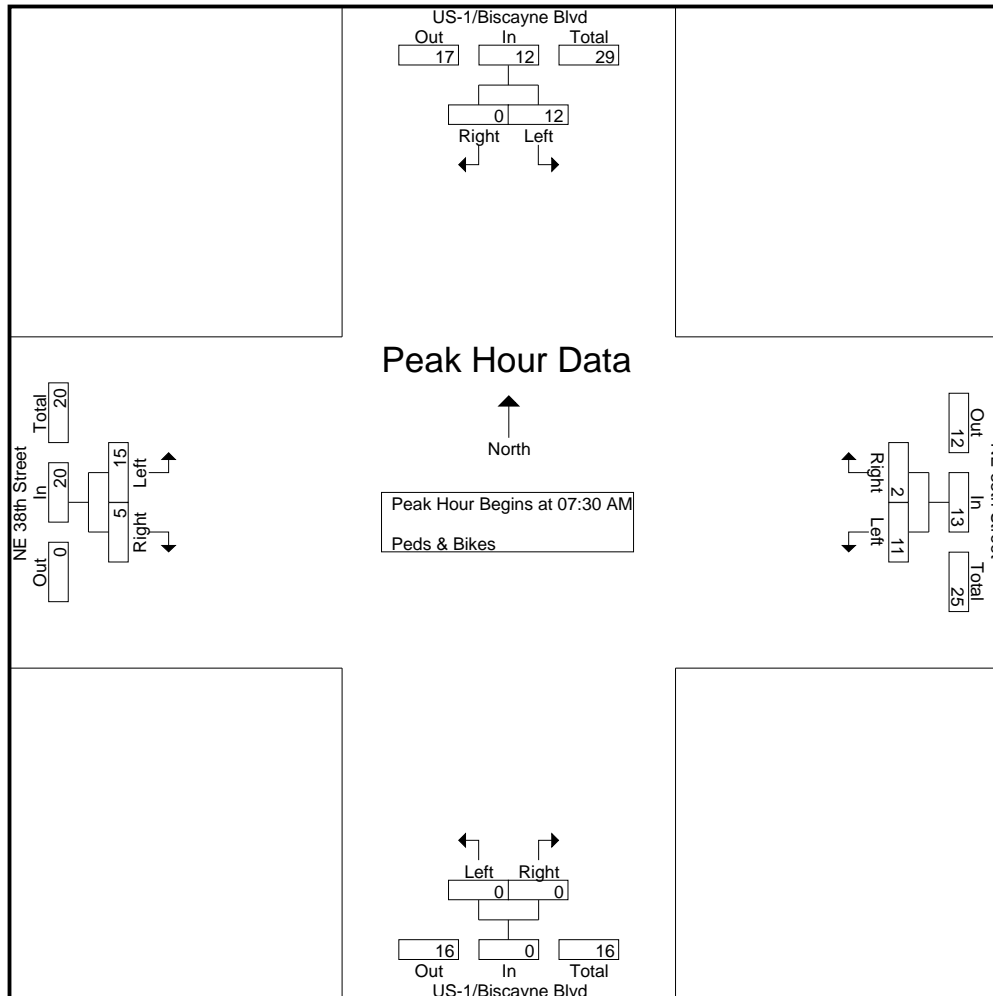
Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5

Start Time	US-1/Biscayne Blvd Southbound			US-1/Biscayne Blvd Northbound			NE 38th Street Westbound			NE 38th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 04:45 PM													
04:45 PM	6	0	6	1	0	1	3	0	3	0	2	2	12
05:00 PM	1	0	1	0	2	2	2	1	3	0	2	2	8
05:15 PM	0	0	0	0	0	0	10	0	10	1	0	1	11
05:30 PM	4	0	4	1	0	1	8	1	9	2	1	3	17
Total Volume	11	0	11	2	2	4	23	2	25	3	5	8	48
% App. Total	100	0		50	50		92	8		37.5	62.5		
PHF	.458	.000	.458	.500	.250	.500	.575	.500	.625	.375	.625	.667	.706

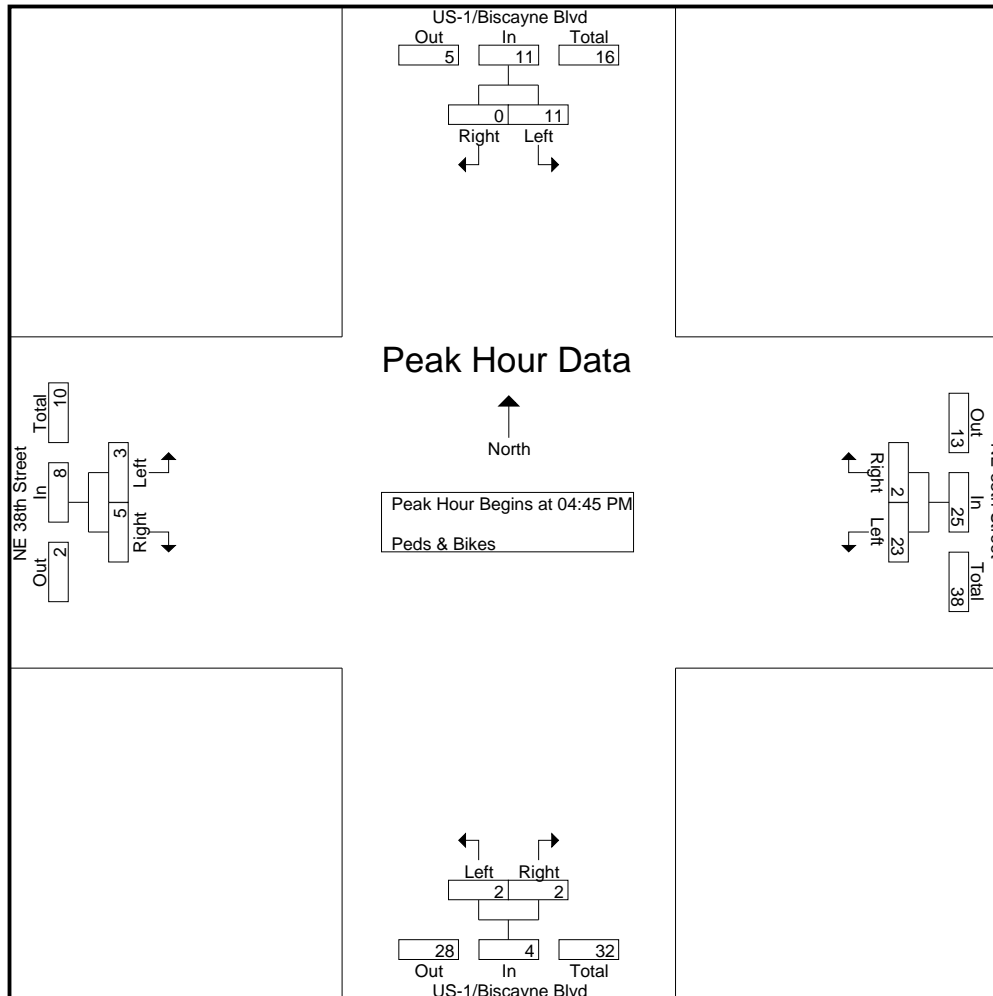
Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Trucks

Start Time	US-1/Biscayne Blvd Southbound					US-1/Biscayne Blvd Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	1	0	1	0	0	5	0	5	0	0	0	0	0	0	0	0	0	0	6
07:15 AM	0	0	2	0	2	0	0	8	7	15	0	0	0	0	0	0	0	0	0	0	17
07:30 AM	0	1	1	0	2	0	0	8	1	9	0	0	0	2	2	0	0	0	0	0	13
07:45 AM	0	1	3	1	5	0	0	4	2	6	0	0	0	1	1	0	0	0	0	0	12
Total	0	2	7	1	10	0	0	25	10	35	0	0	0	3	3	0	0	0	0	0	48
08:00 AM	0	0	1	0	1	0	0	5	2	7	0	0	0	1	1	0	0	0	0	0	9
08:15 AM	0	0	2	0	2	0	0	7	1	8	0	0	0	0	0	0	0	0	0	0	10
08:30 AM	0	3	0	1	4	0	0	2	2	4	0	0	0	3	3	0	0	0	0	0	11
08:45 AM	0	0	1	0	1	0	0	6	0	6	0	0	0	0	0	0	0	0	0	0	7
Total	0	3	4	1	8	0	0	20	5	25	0	0	0	4	4	0	0	0	0	0	37
*** BREAK ***																					
03:00 PM	0	2	7	1	10	0	0	8	4	12	0	2	0	3	5	0	0	0	0	0	27
03:15 PM	0	0	5	0	5	0	0	7	6	13	0	2	0	2	4	0	0	0	0	0	22
03:30 PM	0	3	4	0	7	0	0	6	4	10	0	2	1	2	5	0	0	0	0	0	22
03:45 PM	0	1	2	0	3	0	0	4	2	6	0	0	2	6	8	0	0	0	0	0	17
Total	0	6	18	1	25	0	0	25	16	41	0	6	3	13	22	0	0	0	0	0	88
04:00 PM	0	3	3	1	7	0	0	6	3	9	0	2	0	3	5	0	0	0	0	0	21
04:15 PM	0	3	3	0	6	0	0	6	2	8	0	3	1	1	5	0	0	0	0	0	19
04:30 PM	0	2	2	0	4	0	0	5	2	7	0	4	0	4	8	0	0	0	0	0	19
04:45 PM	0	3	4	0	7	0	0	3	6	9	0	2	0	3	5	0	0	0	0	0	21
Total	0	11	12	1	24	0	0	20	13	33	0	11	1	11	23	0	0	0	0	0	80
05:00 PM	0	1	1	0	2	0	0	7	3	10	0	3	0	3	6	0	0	0	0	0	18
05:15 PM	0	0	0	0	0	0	0	5	2	7	0	0	0	0	0	0	0	0	0	0	7
05:30 PM	0	2	5	1	8	0	0	3	1	4	0	1	0	3	4	0	0	0	0	0	16
05:45 PM	0	2	4	0	6	0	0	3	3	6	0	1	0	0	1	0	0	0	0	0	13
Total	0	5	10	1	16	0	0	18	9	27	0	5	0	6	11	0	0	0	0	0	54
Grand Total	0	27	51	5	83	0	0	108	53	161	0	22	4	37	63	0	0	0	0	0	307
Apprch %	0	32.5	61.4	6		0	0	67.1	32.9		0	34.9	6.3	58.7		0	0	0	0		
Total %	0	8.8	16.6	1.6	27	0	0	35.2	17.3	52.4	0	7.2	1.3	12.1	20.5	0	0	0	0	0	

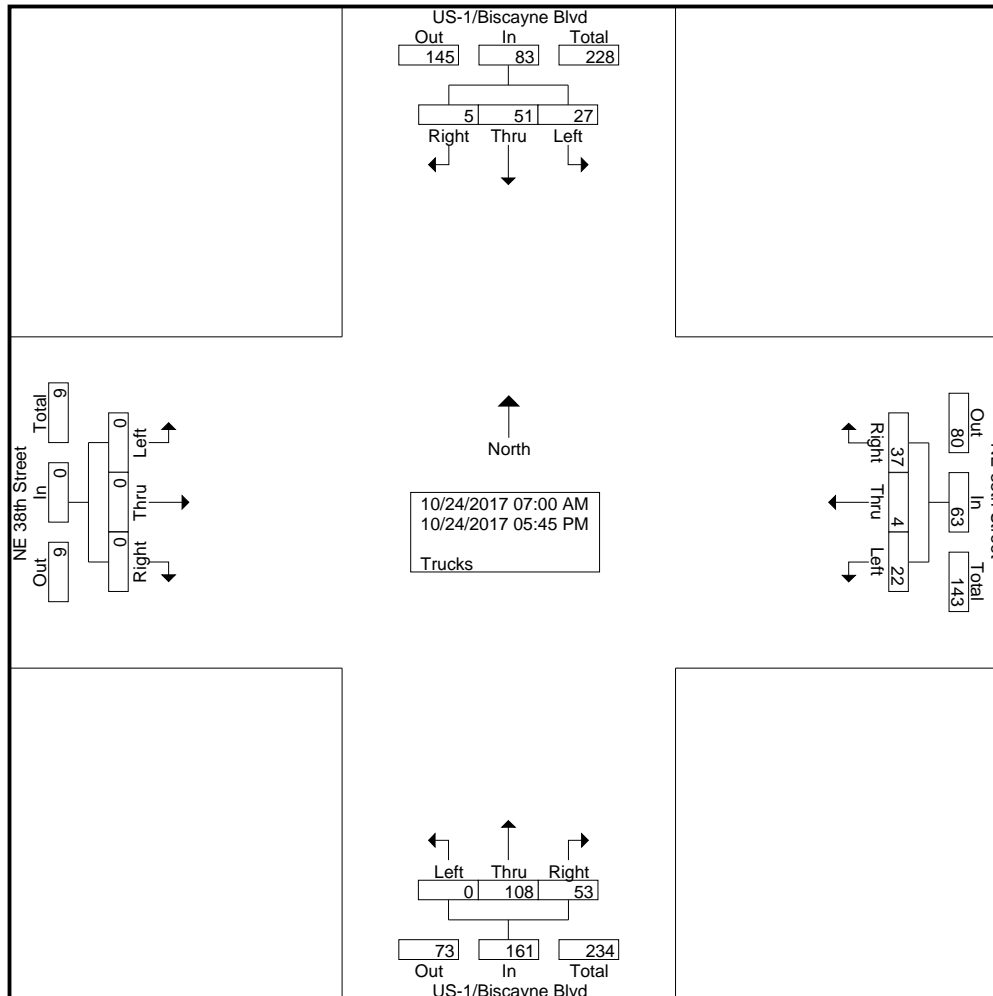
Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



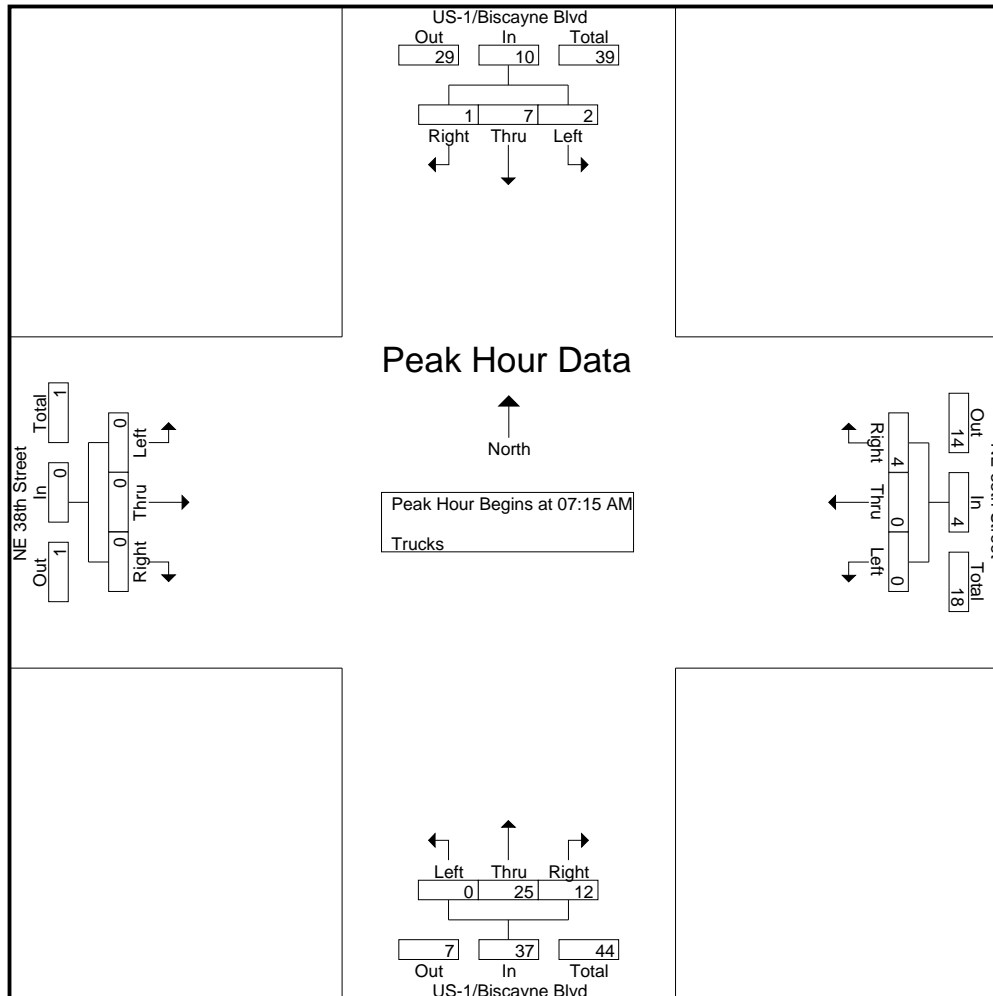
Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



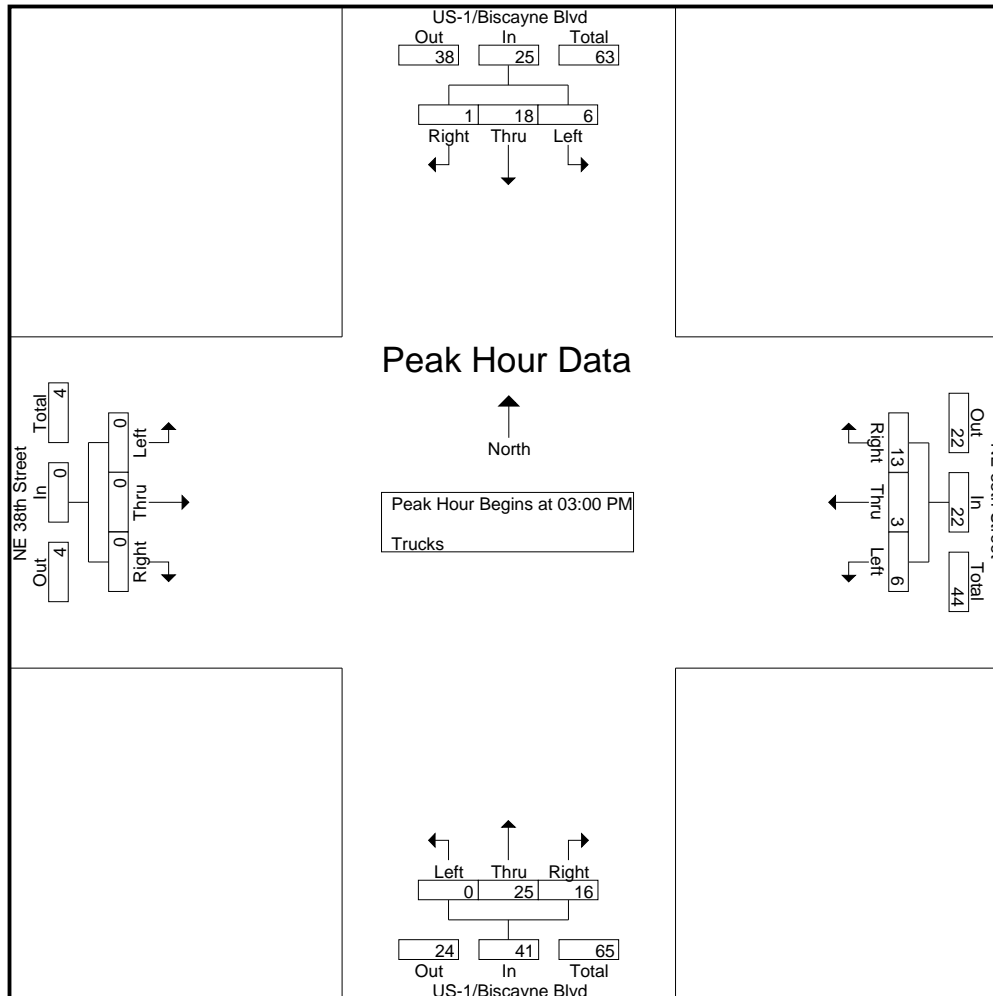
Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	US-1/Biscayne Blvd Southbound					US-1/Biscayne Blvd Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	1	159	279	2	441	0	10	118	104	232	0	47	9	44	100	0	0	0	0	0	773
07:15 AM	0	147	300	7	454	0	4	144	109	257	0	78	32	53	163	0	0	0	0	0	874
07:30 AM	1	147	326	6	480	0	4	145	110	259	0	99	25	60	184	0	0	0	0	0	923
07:45 AM	1	145	336	6	488	0	3	108	106	217	0	105	28	81	214	0	0	0	0	0	919
Total	3	598	1241	21	1863	0	21	515	429	965	0	329	94	238	661	0	0	0	0	0	3489
08:00 AM	0	119	359	5	483	0	4	118	115	237	0	96	20	63	179	0	0	0	0	0	899
08:15 AM	0	135	352	11	498	0	3	116	128	247	0	87	26	53	166	0	0	0	0	0	911
08:30 AM	3	137	343	7	490	0	4	97	107	208	0	72	25	58	155	0	0	0	0	0	853
08:45 AM	1	117	334	10	462	0	7	148	103	258	0	72	32	56	160	0	0	0	0	0	880
Total	4	508	1388	33	1933	0	18	479	453	950	0	327	103	230	660	0	0	0	0	0	3543
*** BREAK ***																					
03:00 PM	1	99	254	13	367	0	4	161	97	262	0	83	39	101	223	0	0	0	0	0	852
03:15 PM	1	79	216	9	305	0	11	197	97	305	0	99	24	124	247	0	0	0	0	0	857
03:30 PM	0	80	201	8	289	0	4	205	84	293	0	73	27	107	207	0	0	0	0	0	789
03:45 PM	1	82	172	12	267	0	7	163	107	277	0	58	37	96	191	0	0	0	0	0	735
Total	3	340	843	42	1228	0	26	726	385	1137	0	313	127	428	868	0	0	0	0	0	3233
04:00 PM	0	90	269	15	374	0	10	168	107	285	0	114	34	109	257	0	0	0	0	0	916
04:15 PM	1	108	206	8	323	0	4	195	87	286	0	103	22	122	247	0	0	0	0	0	856
04:30 PM	0	95	213	19	327	0	5	205	73	283	0	105	33	109	247	0	0	0	0	0	857
04:45 PM	1	76	212	17	306	1	6	211	60	278	0	117	34	134	285	0	0	0	0	0	869
Total	2	369	900	59	1330	1	25	779	327	1132	0	439	123	474	1036	0	0	0	0	0	3498
05:00 PM	0	77	211	11	299	1	3	217	70	291	0	119	29	132	280	0	0	0	0	0	870
05:15 PM	1	105	222	16	344	0	7	220	61	288	0	98	24	129	251	0	0	0	0	0	883
05:30 PM	2	81	225	15	323	0	7	227	70	304	0	117	23	135	275	0	0	0	0	0	902
05:45 PM	0	71	221	6	298	0	4	227	61	292	0	113	33	136	282	0	0	0	0	0	872
Total	3	334	879	48	1264	1	21	891	262	1175	0	447	109	532	1088	0	0	0	0	0	3527
Grand Total	15	2149	5251	203	7618	2	111	3390	1856	5359	0	1855	556	1902	4313	0	0	0	0	0	17290
Apprch %	0.2	28.2	68.9	2.7		0	2.1	63.3	34.6		0	43	12.9	44.1		0	0	0	0		
Total %	0.1	12.4	30.4	1.2	44.1	0	0.6	19.6	10.7	31	0	10.7	3.2	11	24.9	0	0	0	0	0	
Vehicle	15	2122	5200	198	7535	2	111	3282	1803	5198	0	1833	552	1865	4250	0	0	0	0	0	16983
% Vehicle	100	98.7	99	97.5	98.9	100	100	96.8	97.1	97	0	98.8	99.3	98.1	98.5	0	0	0	0	0	98.2

Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2

Groups Printed- Vehicle - Trucks

	US-1/Biscayne Blvd Southbound					US-1/Biscayne Blvd Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Trucks	0	27	51	5	83	0	0	108	53	161	0	22	4	37	63	0	0	0	0	0	0	307
% Trucks	0	1.3	1	2.5	1.1	0	0	3.2	2.9	3	0	1.2	0.7	1.9	1.5	0	0	0	0	0	0	1.8

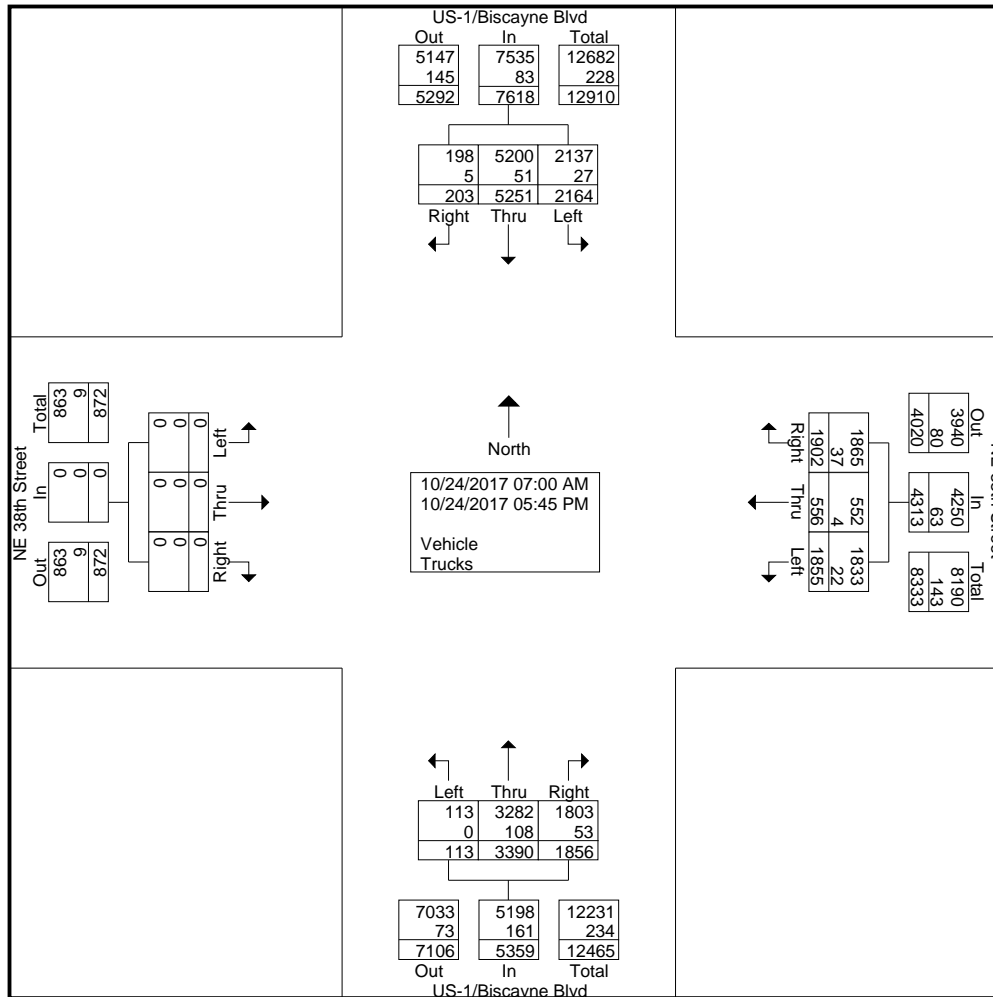
Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



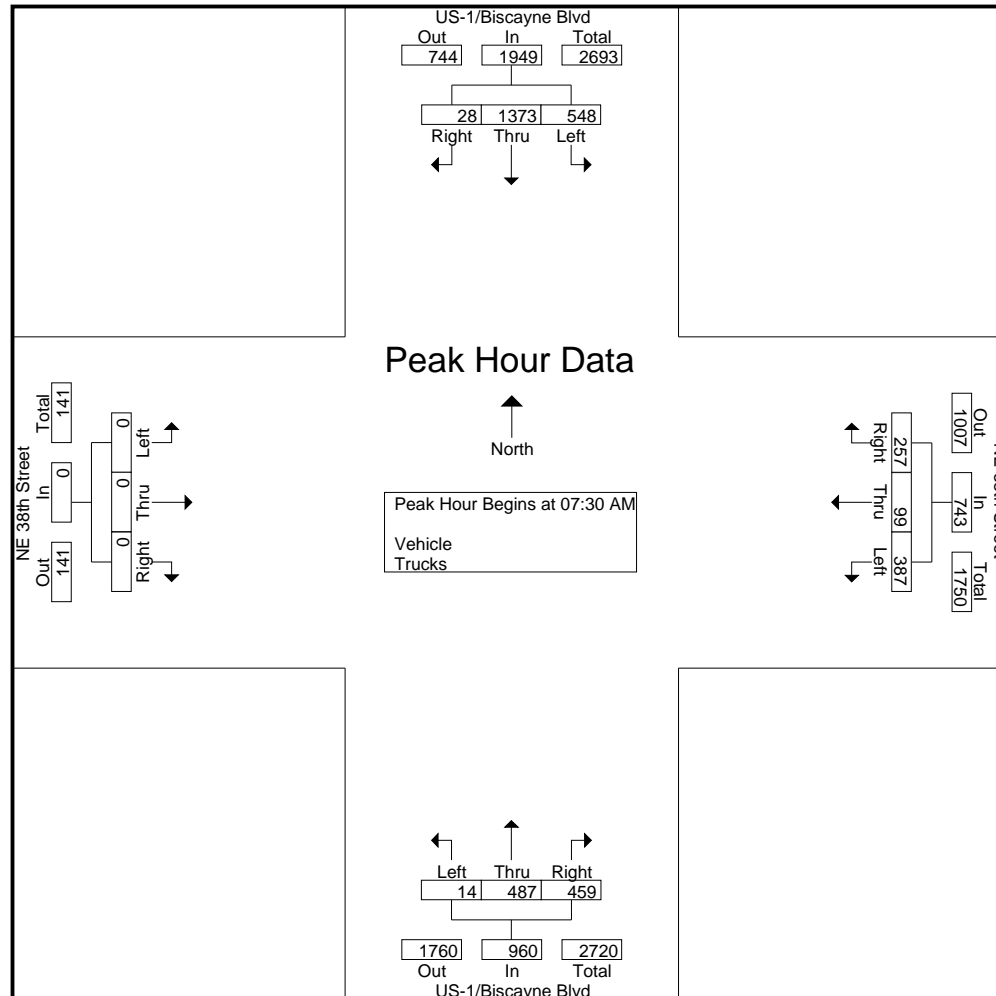
Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



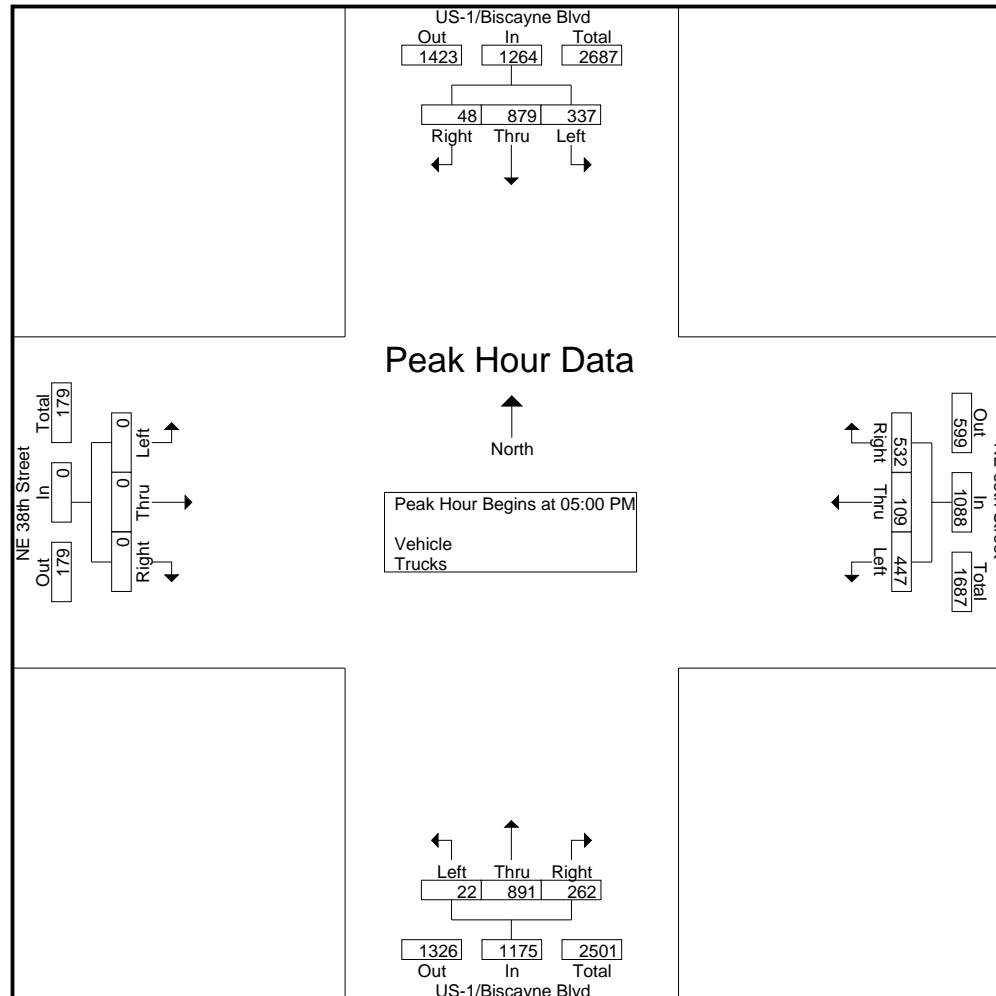
Biscayne Blvd & NE 38th Street

File Name : TMC-14 US-1-Biscayne Blvd & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	NE 5th Avenue Southbound			NE 5th Avenue Northbound			NE 36th Street Westbound			NE 36th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	1	0	1	0	0	0	0	0	0	0	0	0	1
07:15 AM	1	0	1	0	0	0	0	0	0	1	0	1	2
07:30 AM	0	0	0	1	0	1	1	0	1	0	0	0	2
07:45 AM	0	0	0	1	0	1	1	0	1	0	0	0	2
Total	2	0	2	2	0	2	2	0	2	1	0	1	7
08:00 AM	0	0	0	1	0	1	1	0	1	0	0	0	2
08:15 AM	0	0	0	1	0	1	1	0	1	0	0	0	2
08:30 AM	0	0	0	1	0	1	0	0	0	0	0	0	1
08:45 AM	1	0	1	2	0	2	0	0	0	0	0	0	3
Total	1	0	1	5	0	5	2	0	2	0	0	0	8
*** BREAK ***													
03:00 PM	0	1	1	0	2	2	0	2	2	0	0	0	5
03:30 PM	0	0	0	2	0	2	0	0	0	0	0	0	2
03:45 PM	0	0	0	1	0	1	0	0	0	0	0	0	1
Total	0	1	1	3	2	5	0	2	2	0	0	0	8
04:00 PM	0	0	0	1	1	2	0	0	0	0	0	0	2
04:15 PM	0	0	0	0	1	1	1	0	1	0	0	0	2
04:30 PM	1	0	1	1	0	1	4	0	4	0	0	0	6
04:45 PM	0	0	0	2	0	2	1	0	1	0	0	0	3
Total	1	0	1	4	2	6	6	0	6	0	0	0	13
05:00 PM	0	0	0	4	1	5	0	0	0	0	0	0	5
05:15 PM	0	0	0	2	0	2	0	0	0	0	0	0	2
05:30 PM	0	0	0	4	0	4	0	0	0	0	0	0	4
05:45 PM	2	0	2	3	0	3	2	0	2	0	0	0	7
Total	2	0	2	13	1	14	2	0	2	0	0	0	18
Grand Total	6	1	7	27	5	32	12	2	14	1	0	1	54
Apprch %	85.7	14.3		84.4	15.6		85.7	14.3		100	0		
Total %	11.1	1.9	13	50	9.3	59.3	22.2	3.7	25.9	1.9	0	1.9	

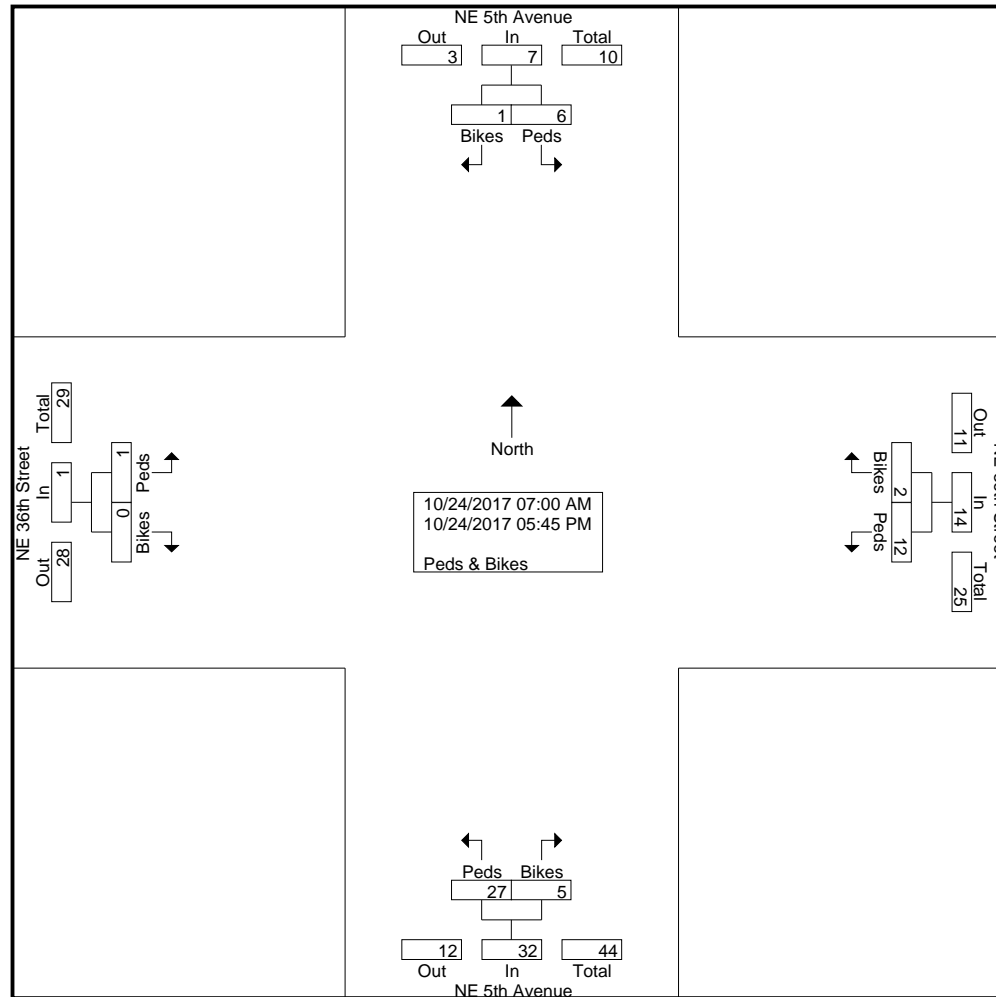
NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NE 5th Avenue Southbound			NE 5th Avenue Northbound			NE 36th Street Westbound			NE 36th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:15 AM													
07:15 AM	1	0	1	0	0	0	0	0	0	1	0	1	2
07:30 AM	0	0	0	1	0	1	1	0	1	0	0	0	2
07:45 AM	0	0	0	1	0	1	1	0	1	0	0	0	2
08:00 AM	0	0	0	1	0	1	1	0	1	0	0	0	2
Total Volume	1	0	1	3	0	3	3	0	3	1	0	1	8
% App. Total	100	0		100	0		100	0		100	0		
PHF	.250	.000	.250	.750	.000	.750	.750	.000	.750	.250	.000	.250	1.00

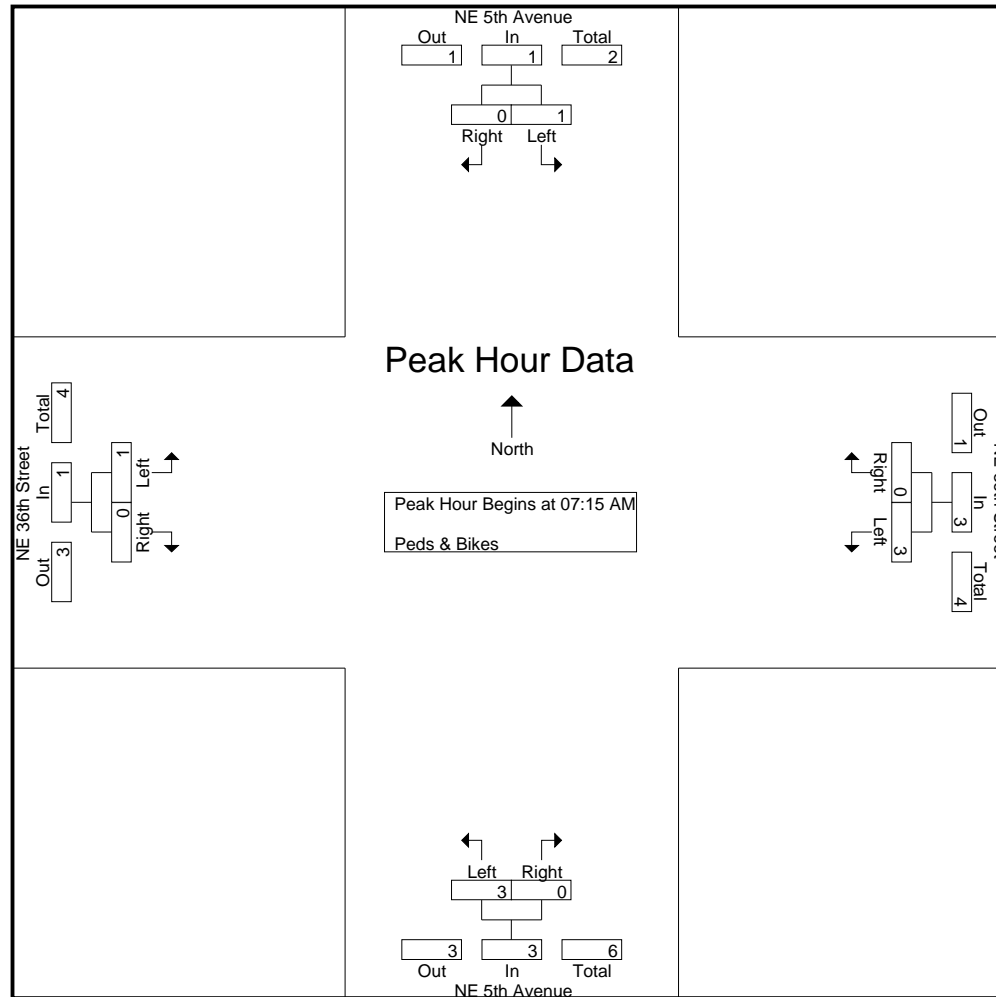
NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	NE 5th Avenue Southbound			NE 5th Avenue Northbound			NE 36th Street Westbound			NE 36th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 05:00 PM													
05:00 PM	0	0	0	4	1	5	0	0	0	0	0	0	5
05:15 PM	0	0	0	2	0	2	0	0	0	0	0	0	2
05:30 PM	0	0	0	4	0	4	0	0	0	0	0	0	4
05:45 PM	2	0	2	3	0	3	2	0	2	0	0	0	7
Total Volume	2	0	2	13	1	14	2	0	2	0	0	0	18
% App. Total	100	0		92.9	7.1		100	0		0	0		
PHF	.250	.000	.250	.813	.250	.700	.250	.000	.250	.000	.000	.000	.643

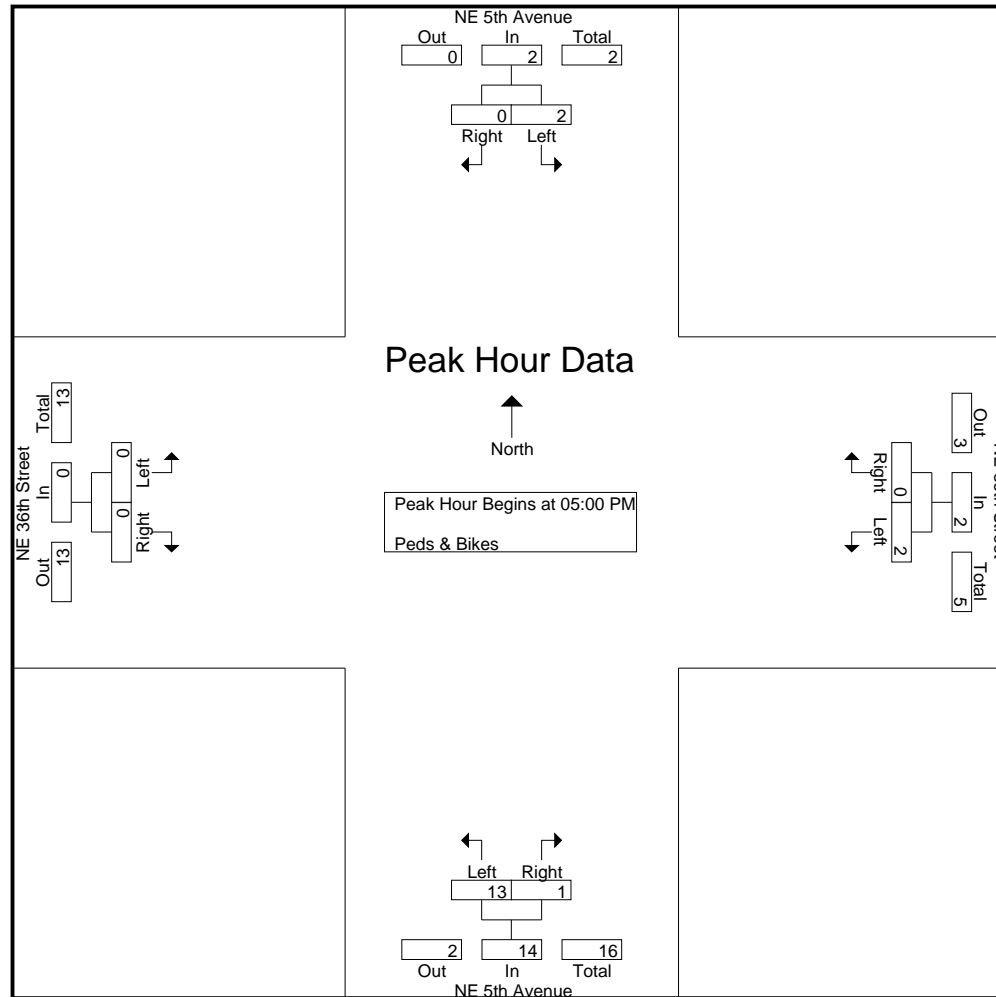
NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Trucks

Start Time	NE 5th Avenue Southbound					NE 5th Avenue Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	1
07:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	0	4	4
07:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	0	4	4
07:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	9	0	10	10
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	18	0	19	19
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	0	6	6
08:15 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	0	3	4
08:30 AM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	10	0	10	11
08:45 AM	0	0	0	0	0	0	0	0	2	2	0	0	0	0	0	0	0	8	0	8	10
Total	0	0	0	1	1	0	0	0	3	3	0	0	0	0	0	0	0	27	0	27	31
*** BREAK ***																					
03:00 PM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	2	0	2	3
*** BREAK ***																					
03:30 PM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	2	0	2	3
03:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	1
Total	0	0	0	0	0	0	1	0	1	2	0	0	0	0	0	0	0	5	0	5	7
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	1
04:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	1
04:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	1
04:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	0	5	5
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	0	8	8
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	2
*** BREAK ***																					
05:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	0	3	3
05:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	1
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	0	6	6
Grand Total	0	0	0	1	1	0	1	0	4	5	0	0	0	0	0	0	1	64	0	65	71
Apprch %	0	0	0	100		0	20	0	80		0	0	0	0		0	1.5	98.5	0		
Total %	0	0	0	1.4	1.4	0	1.4	0	5.6	7	0	0	0	0	0	0	1.4	90.1	0	91.5	

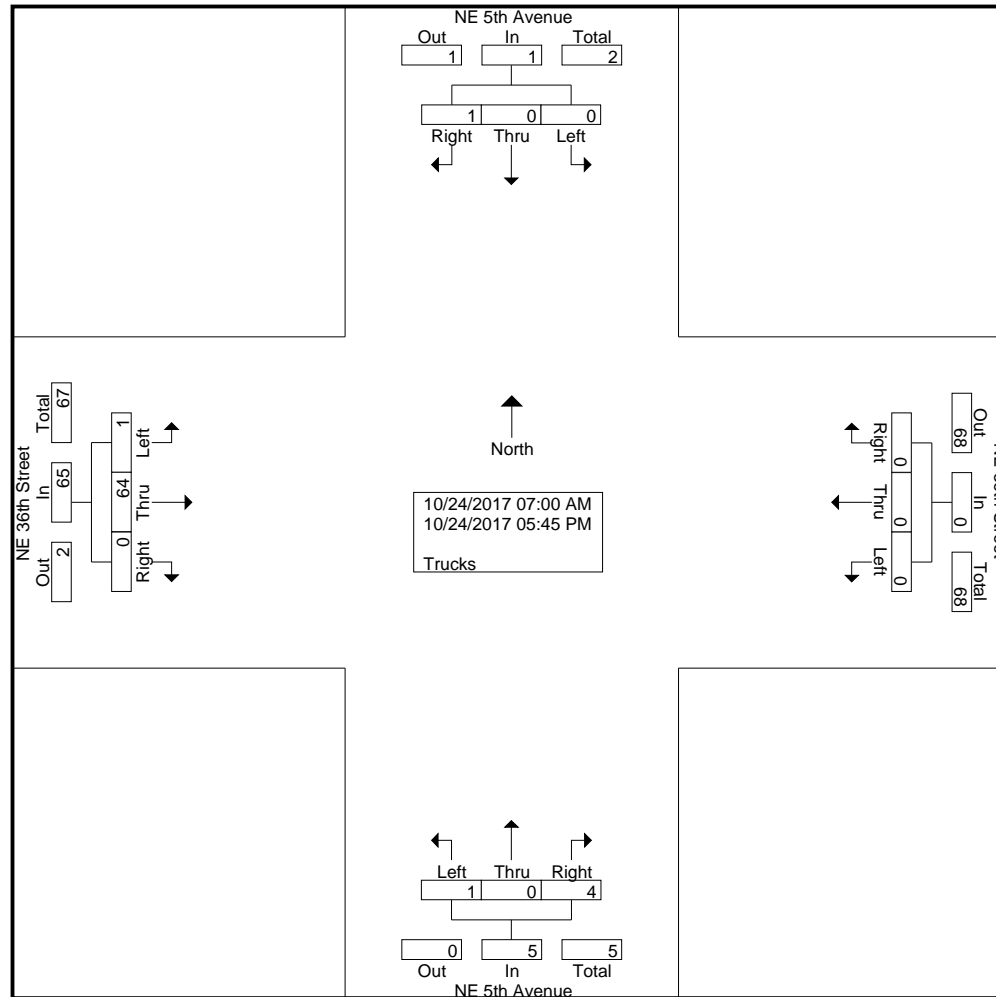
NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NE 5th Avenue Southbound					NE 5th Avenue Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 07:45 AM																						
07:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	9	0	10	10	
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	0	6	6	
08:15 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	0	3	4	
08:30 AM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	10	0	10	11	
Total Volume	0	0	0	1	1	0	0	0	1	1	0	0	0	0	0	0	1	28	0	29	31	
% App. Total	0	0	0	100		0	0	0	100		0	0	0	0		0	3.4	96.6	0			
PHF	.000	.000	.000	.250	.250	.000	.000	.000	.250	.250	.000	.000	.000	.000	.000	.000	.250	.700	.000	.725	.705	

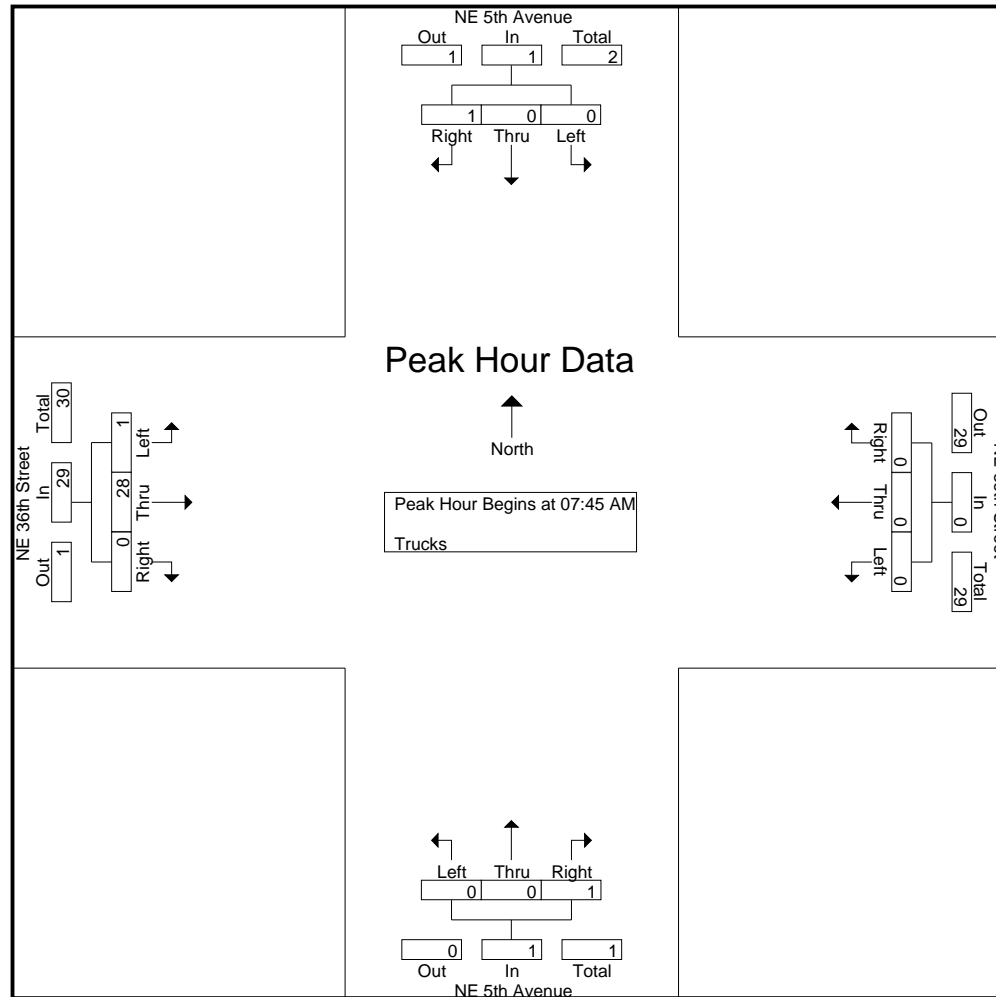
NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	NE 5th Avenue Southbound					NE 5th Avenue Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:45 PM																					
04:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	0	5	5
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	2
05:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	0	3	3
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	0	10	10
% App. Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	100	0		
PHF	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.500	.000	.500	.500

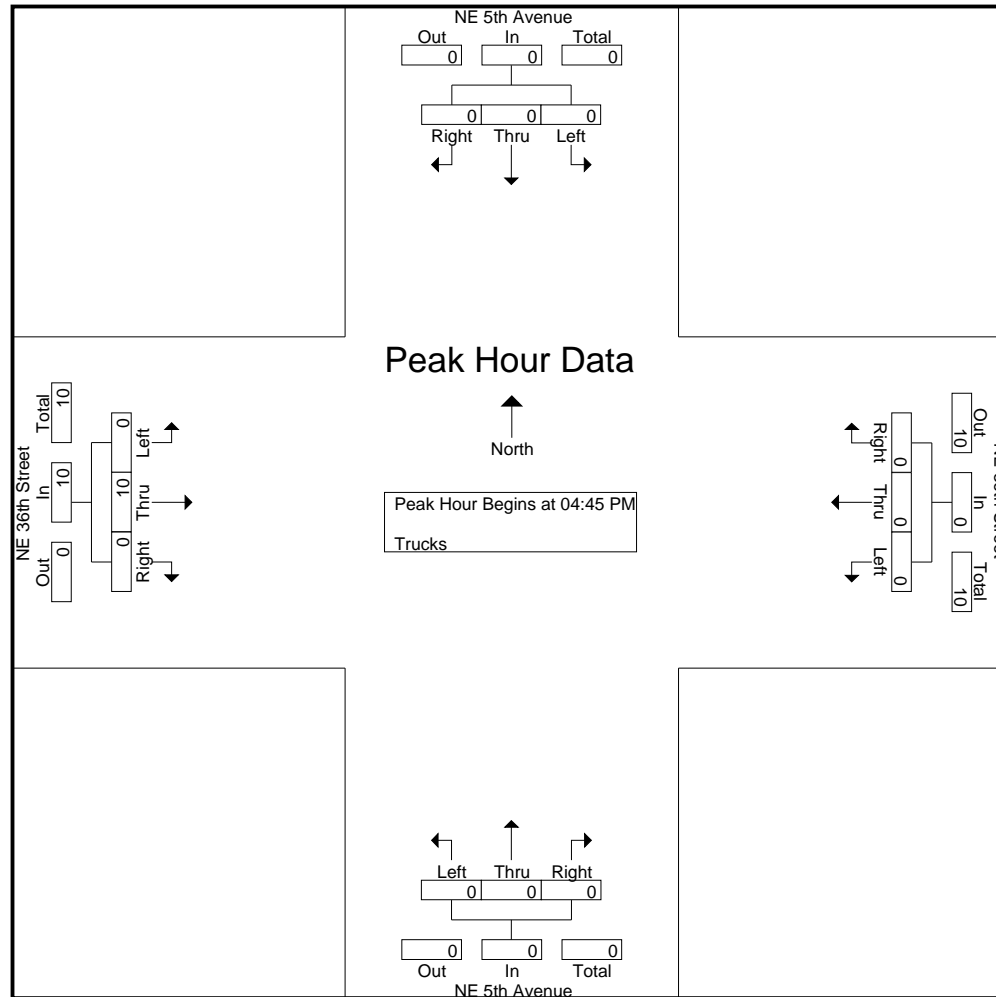
NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	NE 5th Avenue Southbound					NE 5th Avenue Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	0	6	0	7	13	0	0	13	0	13	0	0	195	1	196	222
07:15 AM	0	0	0	1	1	0	4	1	14	19	0	0	15	0	15	0	0	201	4	205	240
07:30 AM	0	0	0	1	1	0	15	0	29	44	0	0	20	0	20	0	1	209	3	213	278
07:45 AM	0	0	0	1	1	0	14	0	29	43	0	1	21	0	22	1	2	211	0	214	280
Total	0	0	0	3	3	0	39	1	79	119	0	1	69	0	70	1	3	816	8	828	1020
08:00 AM	0	0	1	2	3	0	19	0	27	46	0	0	38	0	38	0	2	224	0	226	313
08:15 AM	0	0	0	4	4	0	13	0	32	45	0	0	32	0	32	0	2	236	1	239	320
08:30 AM	0	0	1	1	2	0	14	1	36	51	0	0	21	0	21	0	2	231	0	233	307
08:45 AM	0	0	1	1	2	0	14	0	31	45	0	1	33	0	34	0	0	213	2	215	296
Total	0	0	3	8	11	0	60	1	126	187	0	1	124	0	125	0	6	904	3	913	1236
*** BREAK ***																					
03:00 PM	0	1	0	3	4	0	13	0	26	39	0	1	16	1	18	0	2	166	3	171	232
03:15 PM	0	0	0	4	4	0	12	1	42	55	0	0	13	0	13	1	1	170	1	173	245
03:30 PM	0	0	0	2	2	0	12	0	26	38	0	1	12	0	13	0	1	192	1	194	247
03:45 PM	0	0	0	1	1	0	8	0	32	40	0	0	8	0	8	0	1	182	3	186	235
Total	0	1	0	10	11	0	45	1	126	172	0	2	49	1	52	1	5	710	8	724	959
04:00 PM	0	0	0	0	0	0	6	0	27	33	0	0	10	0	10	0	1	205	1	207	250
04:15 PM	0	0	0	1	1	0	6	0	32	38	0	0	12	0	12	0	1	174	2	177	228
04:30 PM	0	0	0	1	1	0	6	0	27	33	0	0	12	0	12	0	0	186	0	186	232
04:45 PM	0	1	0	0	1	0	8	0	40	48	0	1	19	0	20	0	0	209	1	210	279
Total	0	1	0	2	3	0	26	0	126	152	0	1	53	0	54	0	2	774	4	780	989
05:00 PM	0	1	0	2	3	0	7	0	42	49	0	0	7	0	7	0	2	176	1	179	238
05:15 PM	0	0	0	0	0	0	7	0	34	41	0	1	16	0	17	0	0	187	2	189	247
05:30 PM	0	0	0	5	5	0	2	0	36	38	0	1	16	0	17	0	6	188	6	200	260
05:45 PM	0	0	0	1	1	0	8	1	36	45	0	2	18	0	20	1	0	176	0	177	243
Total	0	1	0	8	9	0	24	1	148	173	0	4	57	0	61	1	8	727	9	745	988
Grand Total	0	3	3	31	37	0	194	4	605	803	0	9	352	1	362	3	24	3931	32	3990	5192
Apprch %	0	8.1	8.1	83.8		0	24.2	0.5	75.3		0	2.5	97.2	0.3		0.1	0.6	98.5	0.8		
Total %	0	0.1	0.1	0.6	0.7	0	3.7	0.1	11.7	15.5	0	0.2	6.8	0	7	0.1	0.5	75.7	0.6	76.8	
Vehicle	0	3	3	30	36	0	193	4	601	798	0	9	352	1	362	3	23	3867	32	3925	5121
% Vehicle	0	100	100	96.8	97.3	0	99.5	100	99.3	99.4	0	100	100	100	100	100	95.8	98.4	100	98.4	98.6

NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	NE 5th Avenue Southbound					NE 5th Avenue Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	0	0	1	1	0	1	0	4	5	0	0	0	0	0	0	1	64	0	65	71
% Trucks	0	0	0	3.2	2.7	0	0.5	0	0.7	0.6	0	0	0	0	0	0	4.2	1.6	0	1.6	1.4

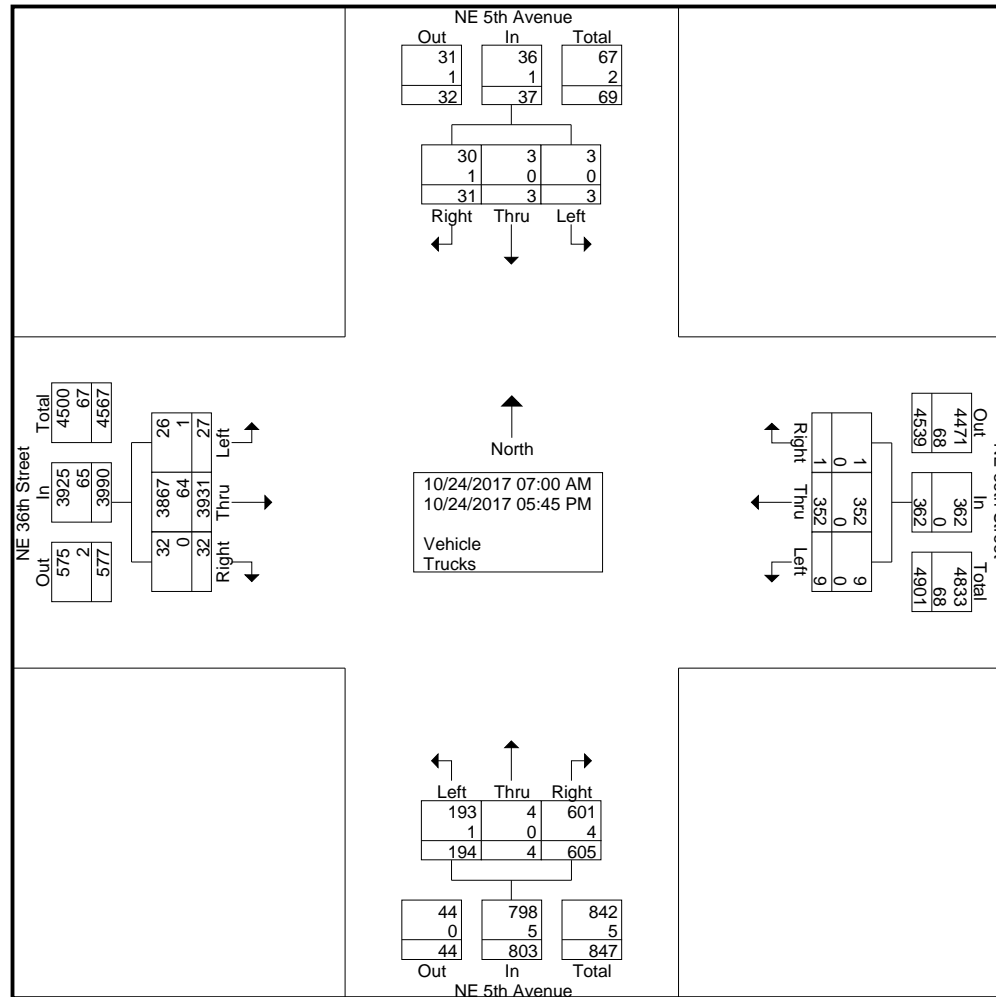
NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4

Start Time	NE 5th Avenue Southbound					NE 5th Avenue Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 08:00 AM																					
08:00 AM	0	0	1	2	3	0	19	0	27	46	0	0	38	0	38	0	2	224	0	226	313
08:15 AM	0	0	0	4	4	0	13	0	32	45	0	0	32	0	32	0	2	236	1	239	320
08:30 AM	0	0	1	1	2	0	14	1	36	51	0	0	21	0	21	0	2	231	0	233	307
08:45 AM	0	0	1	1	2	0	14	0	31	45	0	1	33	0	34	0	0	213	2	215	296
Total Volume	0	0	3	8	11	0	60	1	126	187	0	1	124	0	125	0	6	904	3	913	1236
% App. Total	0	0	27.3	72.7		0	32.1	0.5	67.4		0	0.8	99.2	0		0	0.7	99	0.3		
PHF	.000	.000	.750	.500	.688	.000	.789	.250	.875	.917	.000	.250	.816	.000	.822	.000	.750	.958	.375	.955	.966

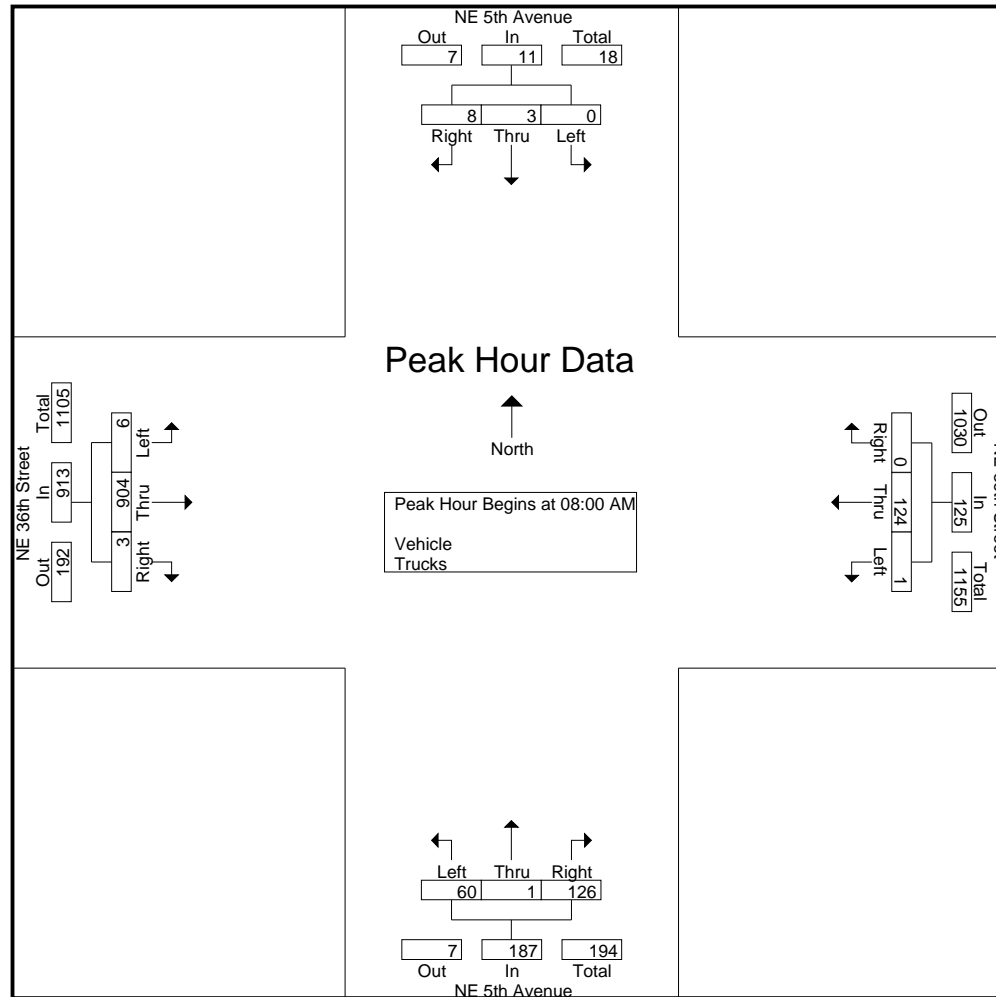
NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 6

Start Time	NE 5th Avenue Southbound					NE 5th Avenue Northbound					NE 36th Street Westbound					NE 36th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:45 PM																					
04:45 PM	0	1	0	0	1	0	8	0	40	48	0	1	19	0	20	0	0	209	1	210	279
05:00 PM	0	1	0	2	3	0	7	0	42	49	0	0	7	0	7	0	2	176	1	179	238
05:15 PM	0	0	0	0	0	0	7	0	34	41	0	1	16	0	17	0	0	187	2	189	247
05:30 PM	0	0	0	5	5	0	2	0	36	38	0	1	16	0	17	0	6	188	6	200	260
Total Volume	0	2	0	7	9	0	24	0	152	176	0	3	58	0	61	0	8	760	10	778	1024
% App. Total	0	22.2	0	77.8		0	13.6	0	86.4		0	4.9	95.1	0		0	1	97.7	1.3		
PHF	.000	.500	.000	.350	.450	.000	.750	.000	.905	.898	.000	.750	.763	.000	.763	.000	.333	.909	.417	.926	.918

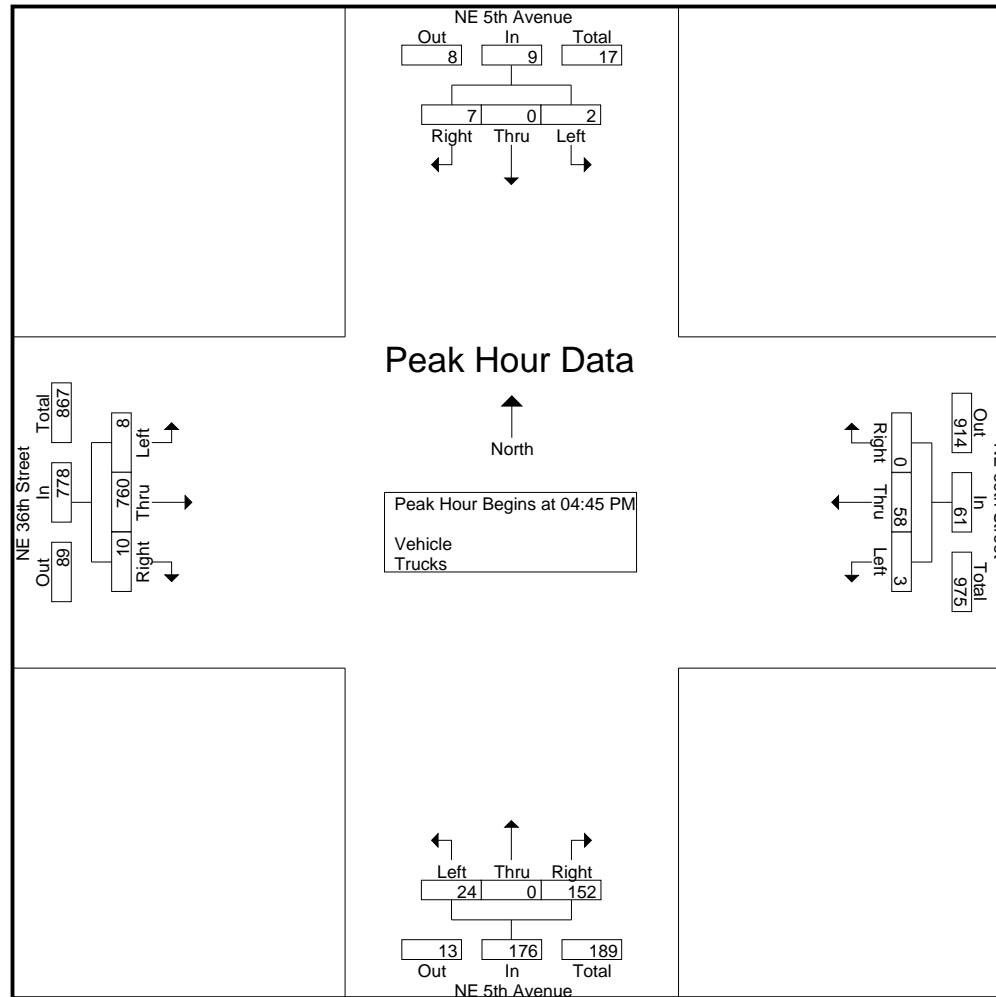
NE 5th Avenue & NE 36th Street

File Name : TMC-15 NE 5th Avenue & NE 36th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	NE 6th Avenue Southbound			Northbound			NE 38th Street Westbound			NE 38th Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
*** BREAK ***													
07:30 AM	1	0	1	0	0	0	0	0	0	0	0	0	1
*** BREAK ***													
Total	1	0	1	0	0	0	0	0	0	0	0	0	1
08:00 AM	1	0	1	0	0	0	0	0	0	0	0	0	1
*** BREAK ***													
Total	1	0	1	0	0	0	0	0	0	0	0	0	1
*** BREAK ***													
03:15 PM	0	0	0	0	0	0	2	0	2	0	0	0	2
03:30 PM	0	0	0	0	0	0	1	0	1	0	0	0	1
03:45 PM	0	0	0	0	0	0	1	0	1	0	0	0	1
Total	0	0	0	0	0	0	4	0	4	0	0	0	4
*** BREAK ***													
05:00 PM	3	1	4	0	0	0	0	0	0	0	0	0	4
05:15 PM	2	0	2	0	0	0	0	0	0	0	0	0	2
*** BREAK ***													
Total	5	1	6	0	0	0	0	0	0	0	0	0	6
Grand Total	7	1	8	0	0	0	4	0	4	0	0	0	12
Apprch %	87.5	12.5		0	0		100	0		0	0		
Total %	58.3	8.3	66.7	0	0		33.3	0	33.3	0	0		

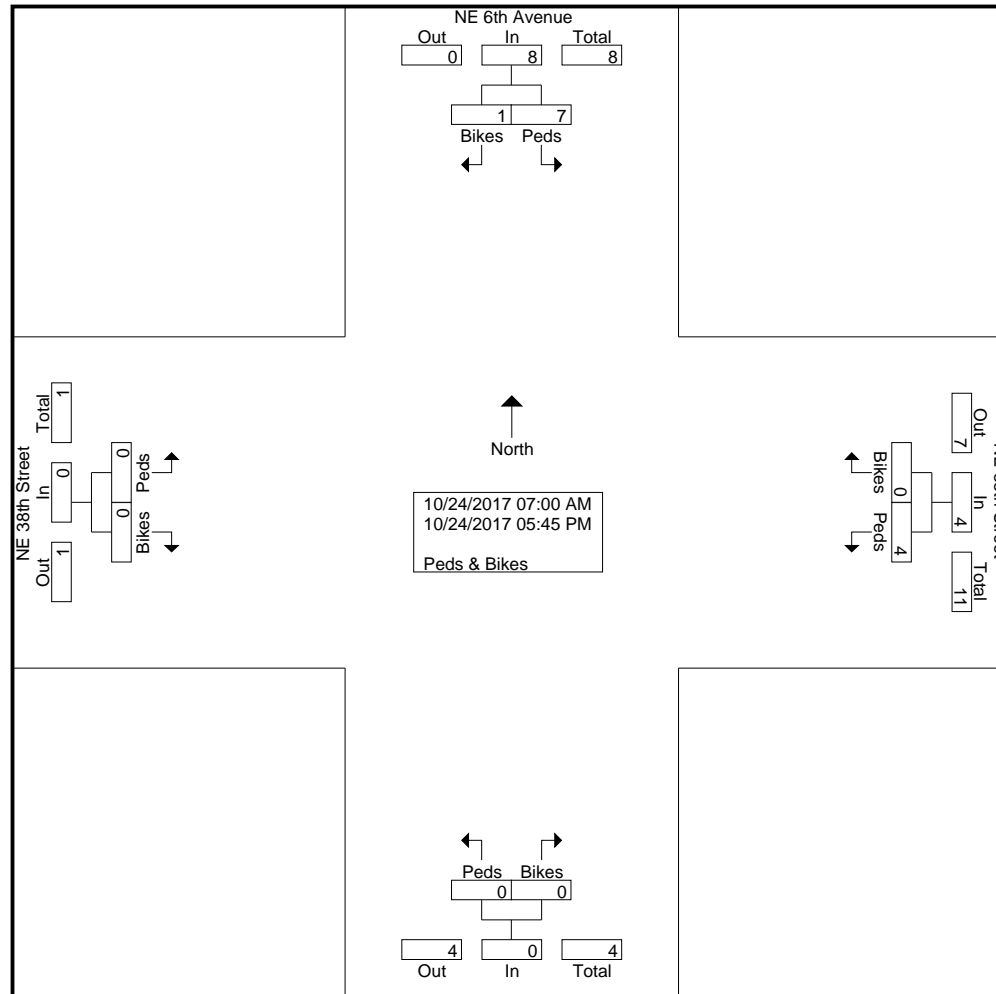
NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



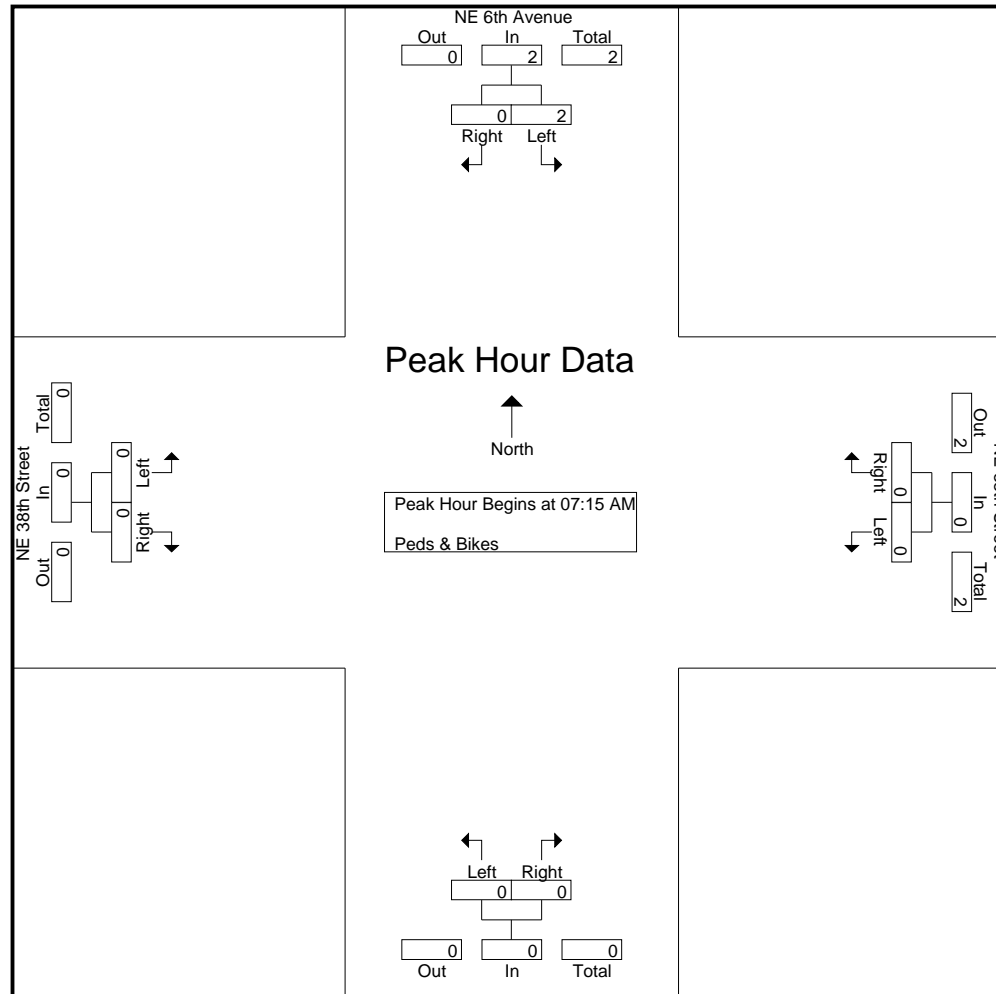
NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



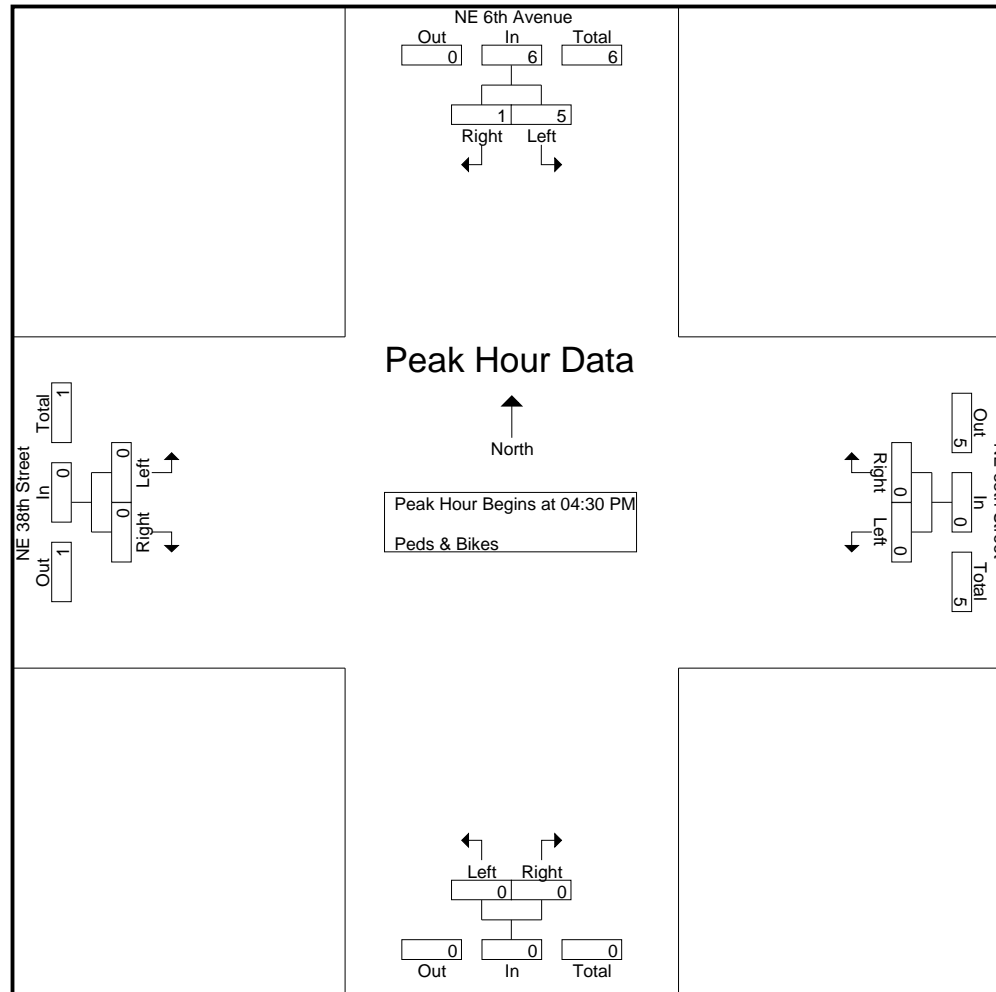
NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Trucks

Start Time	NE 6th Avenue Southbound					Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	1
07:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	2	2
07:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	2
07:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	4	1	5	0	0	0	0	0	5
Total	0	0	0	0	0	0	0	0	0	0	0	0	9	1	10	0	0	0	2	2	12
08:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	3
08:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	3
08:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	5	2	7	0	0	0	1	1	8
08:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	1
Total	0	0	0	1	1	0	0	0	0	0	0	0	11	2	13	0	0	0	1	1	15
*** BREAK ***																					
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1
*** BREAK ***																					
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1
*** BREAK ***																					
04:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1
*** BREAK ***																					
Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1
*** BREAK ***																					
Grand Total	0	0	0	1	1	0	0	0	0	0	0	0	20	3	23	0	0	0	5	5	29
Apprch %	0	0	0	100		0	0	0	0		0	0	87	13		0	0	0	100		
Total %	0	0	0	3.4	3.4	0	0	0	0	0	0	0	69	10.3	79.3	0	0	0	17.2	17.2	

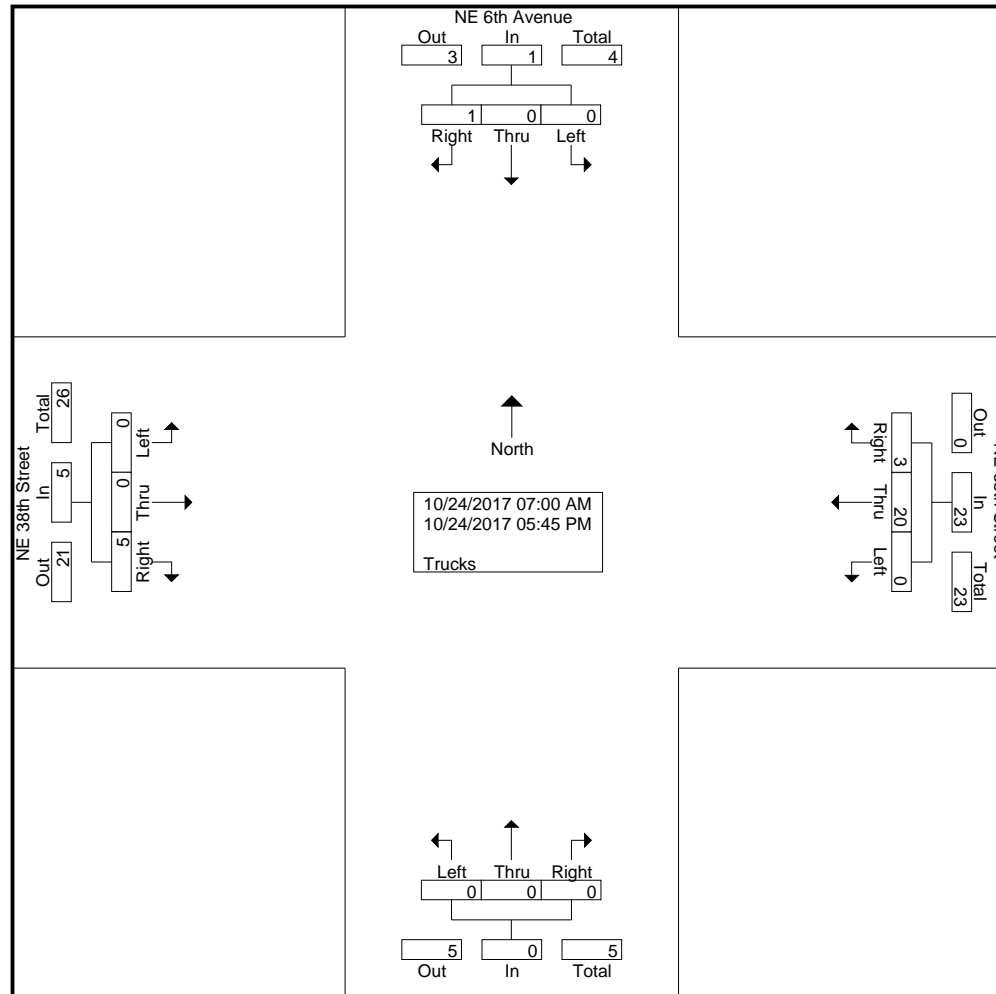
NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 2



NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 3

Start Time	NE 6th Avenue Southbound					Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 07:45 AM																						
07:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	4	1	5	0	0	0	0	0	5	
08:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	3	
08:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	3	
08:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	5	2	7	0	0	0	0	1	8	
Total Volume	0	0	0	1	1	0	0	0	0	0	0	0	14	3	17	0	0	0	0	1	19	
% App. Total	0	0	0	100		0	0	0	0		0	0	82.4	17.6		0	0	0	100			
PHF	.000	.000	.000	.250	.250	.000	.000	.000	.000	.000	.000	.000	.700	.375	.607	.000	.000	.000	.250	.250	.594	

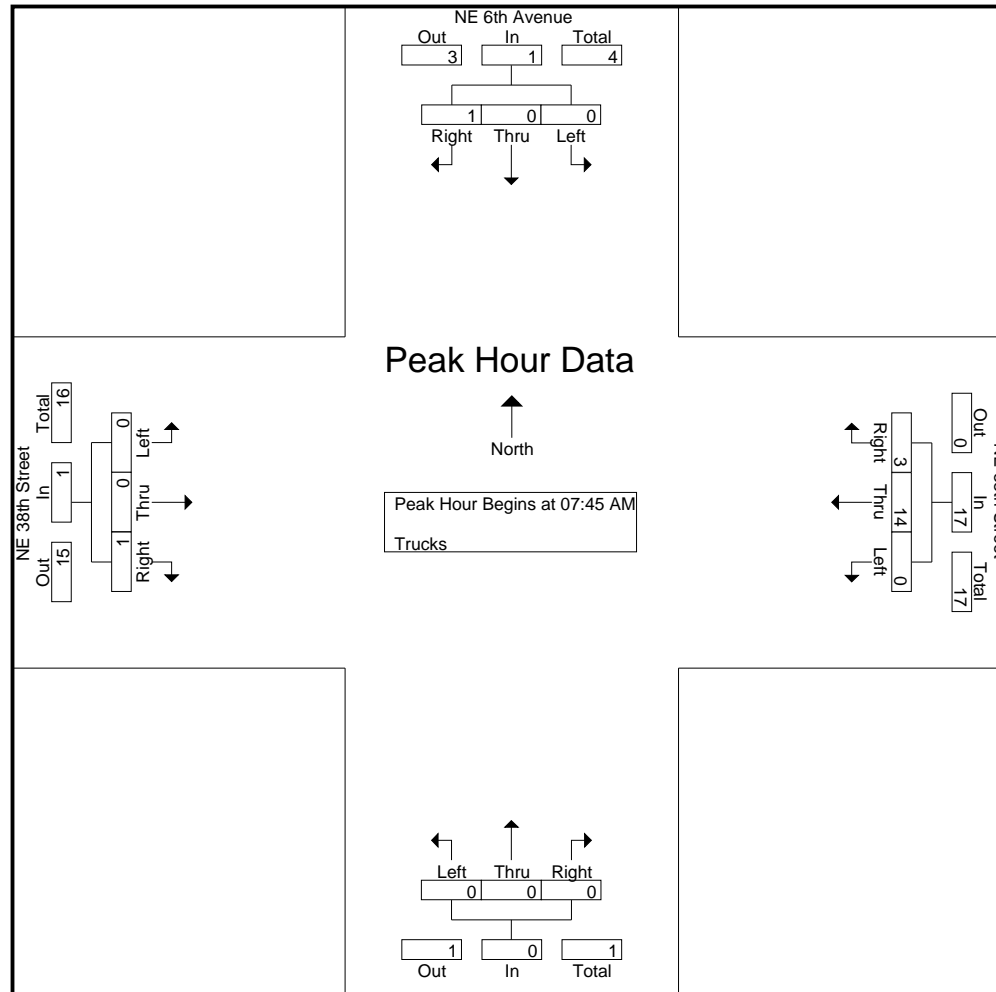
NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 4



NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 5

Start Time	NE 6th Avenue Southbound					Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 03:00 PM																						
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1	
03:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
03:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
03:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Total Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1	
% App. Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	100	100	100	
PHF	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.250	.250	.250	

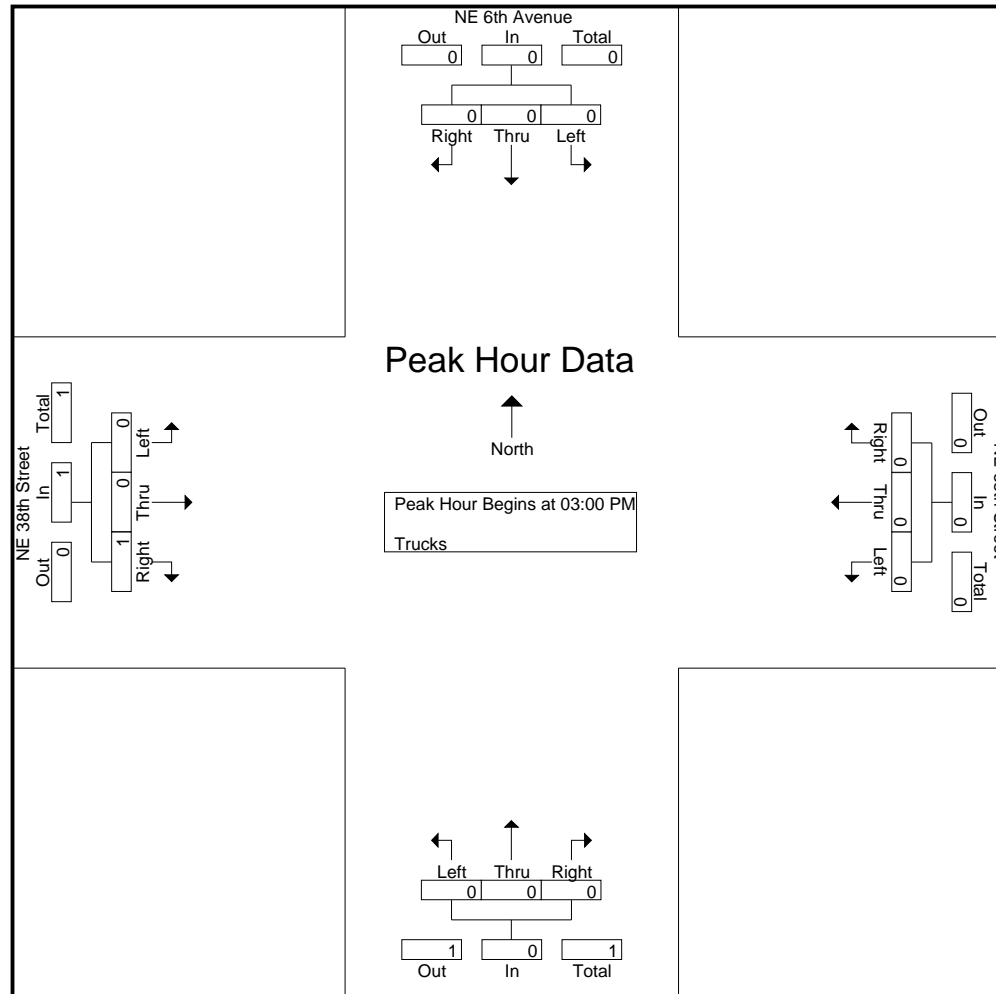
NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 6



NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	NE 6th Avenue Southbound					Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	0	4	4	0	0	0	0	0	0	0	103	3	106	0	0	0	2	2	112
07:15 AM	0	0	0	1	1	0	0	0	0	0	0	0	164	9	173	0	0	0	5	5	179
07:30 AM	0	0	0	8	8	0	0	0	0	0	0	0	180	11	191	0	0	0	1	1	200
07:45 AM	0	0	0	5	5	0	0	0	0	0	0	1	227	23	251	0	0	0	2	2	258
Total	0	0	0	18	18	0	0	0	0	0	0	1	674	46	721	0	0	0	10	10	749
08:00 AM	0	0	0	11	11	0	0	0	0	0	0	0	154	11	165	0	0	0	1	1	177
08:15 AM	0	0	0	11	11	0	0	0	0	0	0	0	148	12	160	0	0	0	0	0	171
08:30 AM	0	0	0	8	8	0	0	0	0	0	0	4	152	16	172	0	0	0	4	4	184
08:45 AM	0	0	0	5	5	0	0	0	0	0	0	1	155	27	183	0	0	0	4	4	192
Total	0	0	0	35	35	0	0	0	0	0	0	5	609	66	680	0	0	0	9	9	724
*** BREAK ***																					
03:00 PM	0	0	0	6	6	0	0	0	0	0	0	0	185	27	212	0	0	0	3	3	221
03:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	209	60	269	0	0	0	8	8	277
03:30 PM	0	0	0	6	6	0	0	0	0	0	0	0	221	43	264	0	0	0	6	6	276
03:45 PM	0	0	0	10	10	0	0	0	0	0	0	1	187	42	230	0	0	0	2	2	242
Total	0	0	0	22	22	0	0	0	0	0	0	1	802	172	975	0	0	0	19	19	1016
04:00 PM	0	0	0	5	5	0	0	0	0	0	0	1	230	52	283	0	0	0	2	2	290
04:15 PM	0	0	0	5	5	0	0	0	0	0	0	0	233	42	275	0	0	0	3	3	283
04:30 PM	0	0	0	4	4	0	0	0	0	0	0	0	208	49	257	0	0	0	2	2	263
04:45 PM	0	0	0	4	4	0	0	0	0	0	0	0	263	61	324	0	0	0	1	1	329
Total	0	0	0	18	18	0	0	0	0	0	0	1	934	204	1139	0	0	0	8	8	1165
05:00 PM	0	0	0	4	4	0	0	0	0	0	0	0	256	53	309	0	0	0	5	5	318
05:15 PM	0	0	0	6	6	0	0	0	0	0	0	0	237	45	282	0	0	0	2	2	290
05:30 PM	0	0	0	4	4	0	0	0	0	0	0	0	263	63	326	0	0	0	5	5	335
05:45 PM	0	0	0	4	4	0	0	0	0	0	0	0	246	47	293	0	0	0	2	2	299
Total	0	0	0	18	18	0	0	0	0	0	0	0	1002	208	1210	0	0	0	14	14	1242
Grand Total	0	0	0	111	111	0	0	0	0	0	0	8	4021	696	4725	0	0	0	60	60	4896
Apprch %	0	0	0	100		0	0	0	0		0	0.2	85.1	14.7		0	0	0	100		
Total %	0	0	0	2.3	2.3	0	0	0	0	0	0	0.2	82.1	14.2	96.5	0	0	0	1.2	1.2	
Vehicle	0	0	0	110	110	0	0	0	0	0	0	8	4001	693	4702	0	0	0	55	55	4867
% Vehicle	0	0	0	99.1	99.1	0	0	0	0	0	0	100	99.5	99.6	99.5	0	0	0	91.7	91.7	99.4

NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	NE 6th Avenue Southbound					Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	0	0	1	1	0	0	0	0	0	0	0	20	3	23	0	0	0	5	5	29
% Trucks	0	0	0	0.9	0.9	0	0	0	0	0	0	0	0.5	0.4	0.5	0	0	0	8.3	8.3	0.6

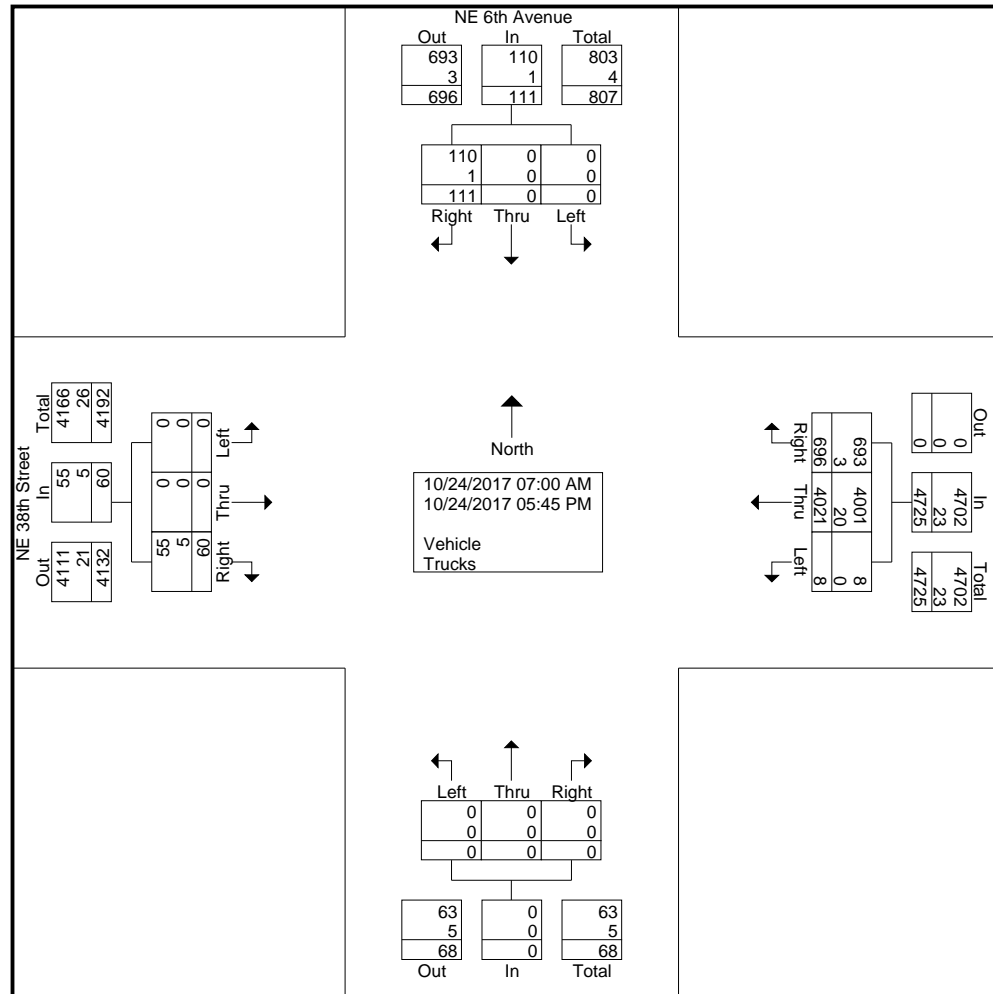
NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 3



NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 4

Start Time	NE 6th Avenue Southbound					Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 07:15 AM																						
07:15 AM	0	0	0	1	1	0	0	0	0	0	0	0	164	9	173	0	0	0	5	5	179	
07:30 AM	0	0	0	8	8	0	0	0	0	0	0	0	180	11	191	0	0	0	1	1	200	
07:45 AM	0	0	0	5	5	0	0	0	0	0	0	1	227	23	251	0	0	0	2	2	258	
08:00 AM	0	0	0	11	11	0	0	0	0	0	0	0	154	11	165	0	0	0	1	1	177	
Total Volume	0	0	0	25	25	0	0	0	0	0	0	1	725	54	780	0	0	0	9	9	814	
% App. Total	0	0	0	100		0	0	0	0		0	0.1	92.9	6.9		0	0	0	100			
PHF	.000	.000	.000	.568	.568	.000	.000	.000	.000	.000	.000	.250	.798	.587	.777	.000	.000	.000	.450	.450	.789	

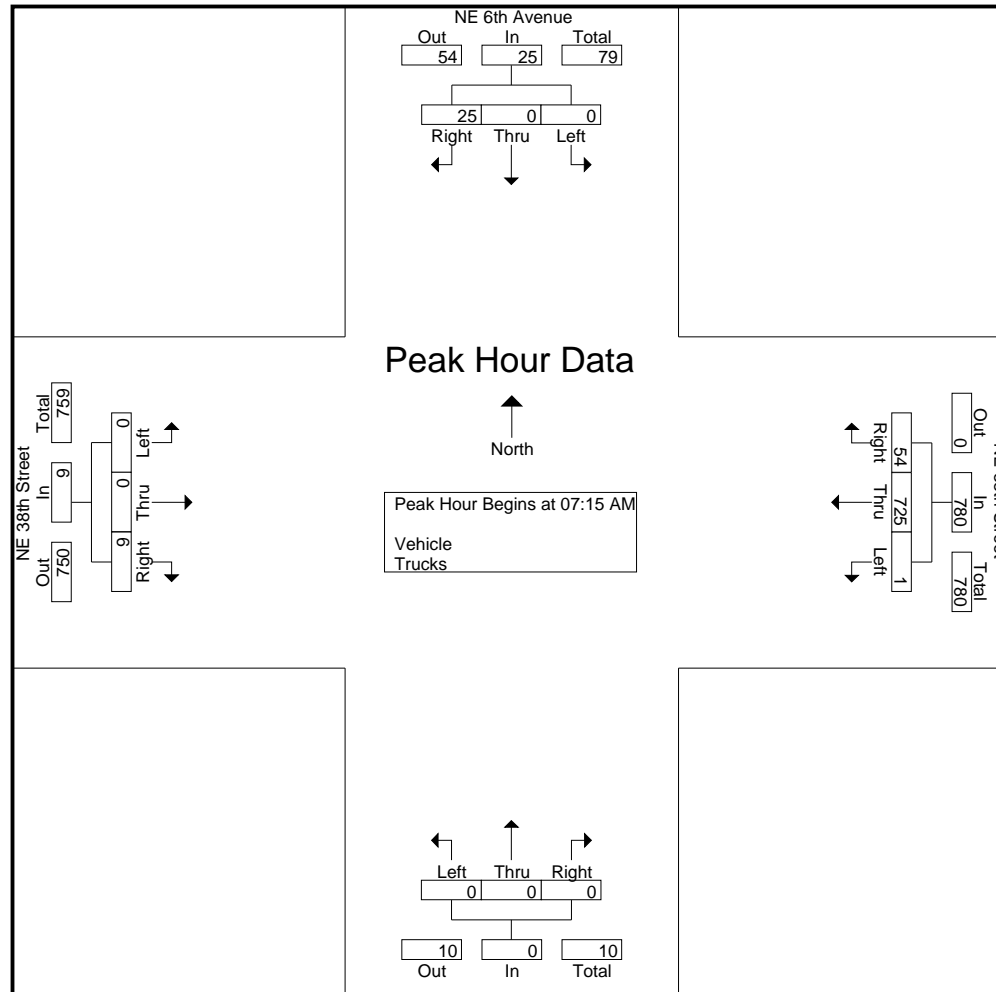
NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 5



NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street
 Site Code : 00000000
 Start Date : 10/24/2017
 Page No : 6

Start Time	NE 6th Avenue Southbound					Northbound					NE 38th Street Westbound					NE 38th Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:45 PM																					
04:45 PM	0	0	0	4	4	0	0	0	0	0	0	0	263	61	324	0	0	0	1	1	329
05:00 PM	0	0	0	4	4	0	0	0	0	0	0	0	256	53	309	0	0	0	5	5	318
05:15 PM	0	0	0	6	6	0	0	0	0	0	0	0	237	45	282	0	0	0	2	2	290
05:30 PM	0	0	0	4	4	0	0	0	0	0	0	0	263	63	326	0	0	0	5	5	335
Total Volume	0	0	0	18	18	0	0	0	0	0	0	0	1019	222	1241	0	0	0	13	13	1272
% App. Total	0	0	0	100		0	0	0	0		0	0	82.1	17.9		0	0	0	100		
PHF	.000	.000	.000	.750	.750	.000	.000	.000	.000	.000	.000	.000	.969	.881	.952	.000	.000	.000	.650	.650	.949

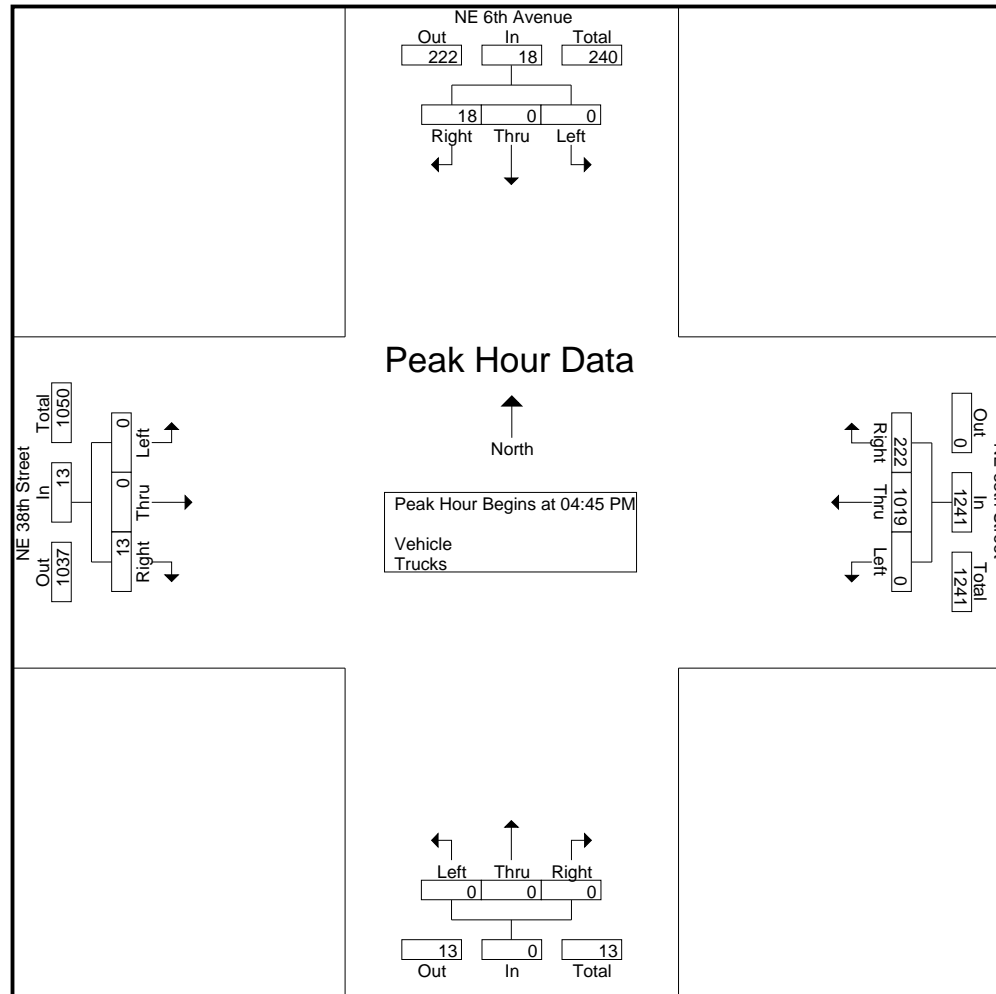
NE 6th Avenue & NE 38th Street

File Name : TMC-16 NE 6th Avenue & NE 38th Street

Site Code : 00000000

Start Date : 10/24/2017

Page No : 7



Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 1

Groups Printed- Peds & Bikes

	Alton Road Southbound			Alton Road Northbound			Chase Avenue Westbound			Chase Avenue Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	1	0	1	0	0	0	0	0	0	0	1	1	2
07:15 AM	2	0	2	0	0	0	0	0	0	0	0	0	2
07:30 AM	3	0	3	1	0	1	0	0	0	2	0	2	6
*** BREAK ***													
Total	6	0	6	1	0	1	0	0	0	2	1	3	10
*** BREAK ***													
08:15 AM	0	0	0	0	0	0	0	0	0	1	0	1	1
08:30 AM	2	0	2	0	0	0	0	0	0	1	0	1	3
08:45 AM	0	1	1	1	0	1	0	0	0	0	0	0	2
Total	2	1	3	1	0	1	0	0	0	2	0	2	6
*** BREAK ***													
03:00 PM	0	0	0	0	0	0	0	0	0	2	0	2	2
03:30 PM	0	1	1	1	0	1	0	0	0	0	0	0	2
*** BREAK ***													
Total	0	1	1	1	0	1	0	0	0	2	0	2	4
04:00 PM	0	0	0	0	0	0	0	0	0	0	1	1	1
04:15 PM	0	0	0	0	0	0	0	0	0	2	1	3	3
04:30 PM	0	0	0	0	0	0	0	0	0	0	1	1	1
04:45 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
Total	0	0	0	0	0	0	0	0	0	3	3	6	6
05:00 PM	0	1	1	0	0	0	0	0	0	2	0	2	3
05:15 PM	0	0	0	1	0	1	0	0	0	0	0	0	1
05:30 PM	0	0	0	1	0	1	0	0	0	2	0	2	3
05:45 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
Total	0	1	1	2	0	2	0	0	0	5	0	5	8
Grand Total	8	3	11	5	0	5	0	0	0	14	4	18	34
Apprch %	72.7	27.3		100	0		0	0		77.8	22.2		
Total %	23.5	8.8	32.4	14.7	0	14.7	0	0	0	41.2	11.8	52.9	

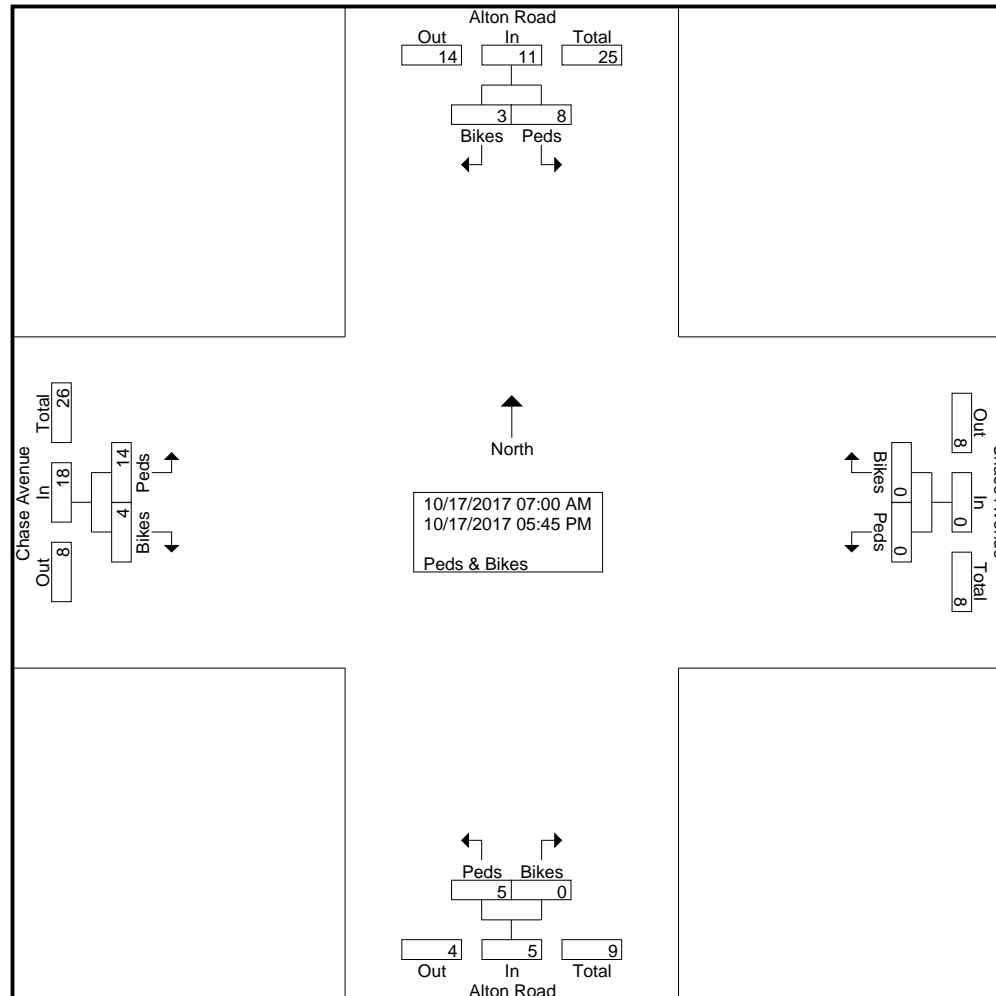
Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Alton Road Southbound			Alton Road Northbound			Chase Avenue Westbound			Chase Avenue Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:00 AM													
07:00 AM	1	0	1	0	0	0	0	0	0	0	1	1	2
07:15 AM	2	0	2	0	0	0	0	0	0	0	0	0	2
07:30 AM	3	0	3	1	0	1	0	0	0	2	0	2	6
07:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	6	0	6	1	0	1	0	0	0	2	1	3	10
% App. Total	100	0		100	0		0	0		66.7	33.3		
PHF	.500	.000	.500	.250	.000	.250	.000	.000	.000	.250	.250	.375	.417

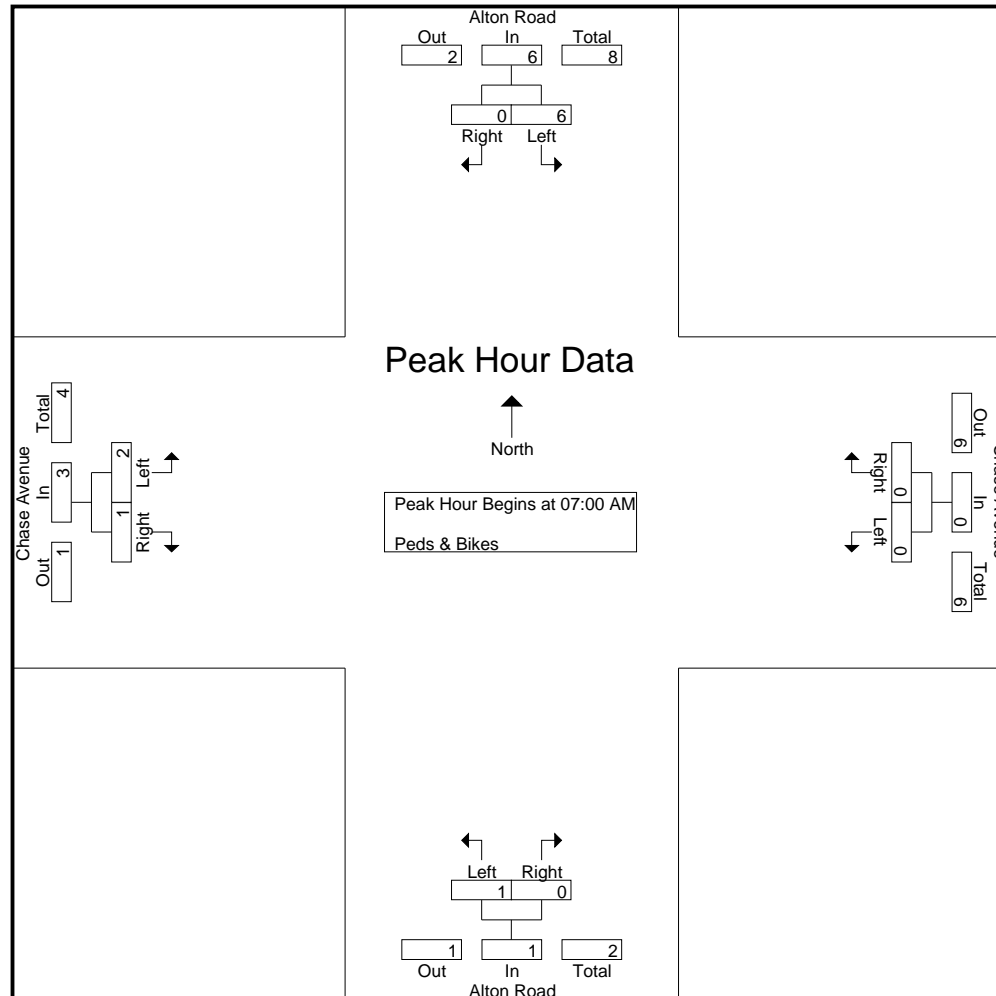
Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 5

Start Time	Alton Road Southbound			Alton Road Northbound			Chase Avenue Westbound			Chase Avenue Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 04:15 PM													
04:15 PM	0	0	0	0	0	0	0	0	0	2	1	3	3
04:30 PM	0	0	0	0	0	0	0	0	0	0	1	1	1
04:45 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
05:00 PM	0	1	1	0	0	0	0	0	0	2	0	2	3
Total Volume	0	1	1	0	0	0	0	0	0	5	2	7	8
% App. Total	0	100		0	0		0	0		71.4	28.6		
PHF	.000	.250	.250	.000	.000	.000	.000	.000	.000	.625	.500	.583	.667

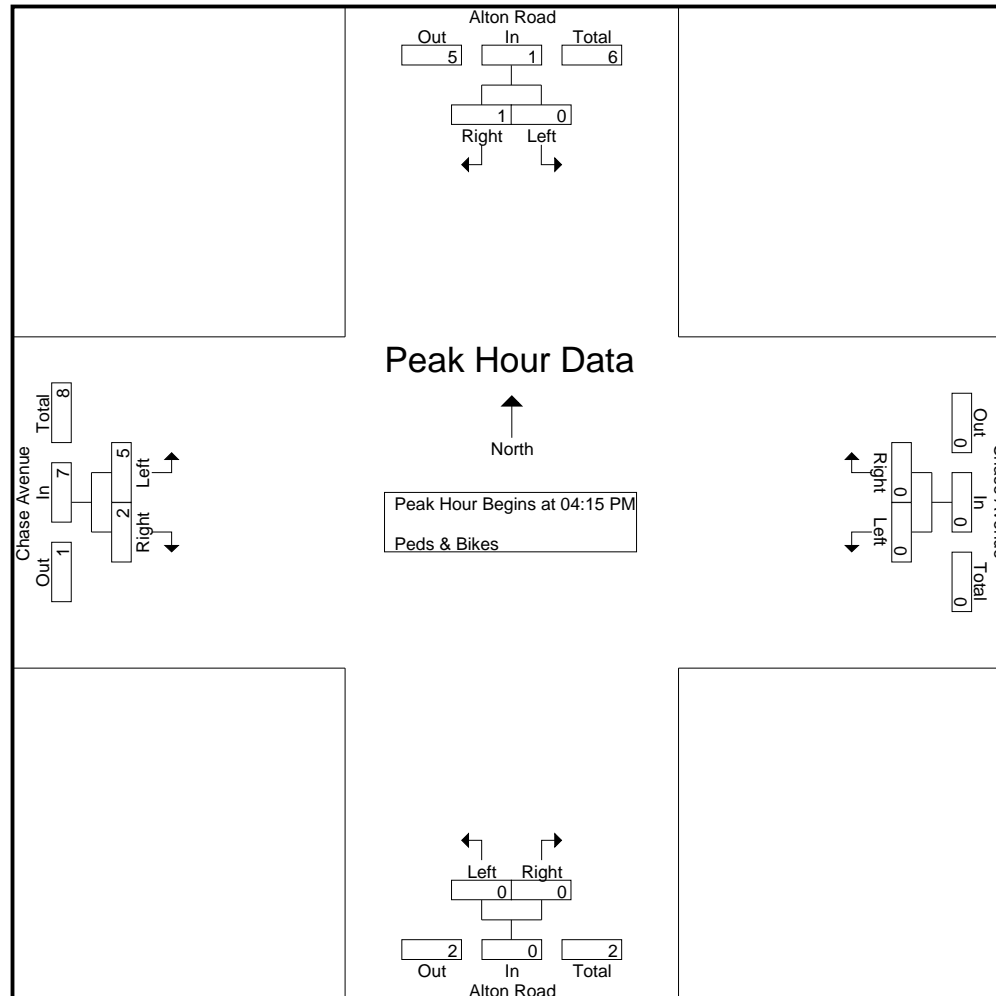
Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 1

Groups Printed- Trucks

Start Time	Alton Road Southbound					Alton Road Northbound					Chase Avenue Westbound					Chase Avenue Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	4	0	4	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	5
07:15 AM	0	0	6	0	6	0	0	4	0	4	0	1	0	2	3	0	0	0	0	0	13
07:30 AM	0	0	6	0	6	0	0	4	0	4	0	0	0	0	0	0	0	0	0	0	10
07:45 AM	0	0	10	0	10	0	0	7	0	7	0	0	0	2	2	0	0	0	0	0	19
Total	0	0	26	0	26	0	0	16	0	16	0	1	0	4	5	0	0	0	0	0	47
08:00 AM	0	0	2	0	2	0	0	6	0	6	0	0	0	0	0	0	0	0	0	0	8
08:15 AM	0	0	5	0	5	0	0	6	0	6	0	0	0	0	0	0	0	0	0	0	11
08:30 AM	0	0	4	0	4	0	0	4	0	4	0	0	0	0	0	0	0	0	0	0	8
08:45 AM	0	0	11	0	11	0	0	9	0	9	0	0	0	0	0	0	0	0	0	0	20
Total	0	0	22	0	22	0	0	25	0	25	0	0	0	0	0	0	0	0	0	0	47
*** BREAK ***																					
03:00 PM	0	0	2	0	2	0	0	6	0	6	0	0	0	1	1	0	0	0	0	0	9
03:15 PM	0	0	2	0	2	0	0	16	0	16	0	0	0	0	0	0	0	0	0	0	18
03:30 PM	0	0	9	0	9	0	0	9	0	9	0	0	0	0	0	0	0	0	0	0	18
03:45 PM	0	0	3	0	3	0	0	4	0	4	0	0	0	1	1	0	0	0	0	0	8
Total	0	0	16	0	16	0	0	35	0	35	0	0	0	2	2	0	0	0	0	0	53
04:00 PM	0	0	3	0	3	0	0	2	0	2	0	0	0	0	0	0	2	0	0	2	7
04:15 PM	0	0	3	0	3	0	0	2	0	2	0	0	0	1	1	0	0	0	0	0	6
04:30 PM	0	0	1	0	1	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	3
04:45 PM	0	0	2	0	2	0	0	4	0	4	0	0	0	1	1	0	0	0	0	0	7
Total	0	0	9	0	9	0	0	10	0	10	0	0	0	2	2	0	2	0	0	2	23
05:00 PM	0	0	3	0	3	0	0	5	0	5	0	0	0	0	0	0	0	0	0	0	8
05:15 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	2
05:30 PM	0	0	3	0	3	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	6
05:45 PM	0	0	2	0	2	0	0	5	0	5	0	0	0	0	0	0	0	0	0	0	7
Total	0	0	9	0	9	0	0	13	0	13	0	0	0	0	0	0	1	0	0	1	23
Grand Total	0	0	82	0	82	0	0	99	0	99	0	1	0	8	9	0	3	0	0	3	193
Apprch %	0	0	100	0		0	0	100	0		0	11.1	0	88.9		0	100	0	0		
Total %	0	0	42.5	0	42.5	0	0	51.3	0	51.3	0	0.5	0	4.1	4.7	0	1.6	0	0	1.6	

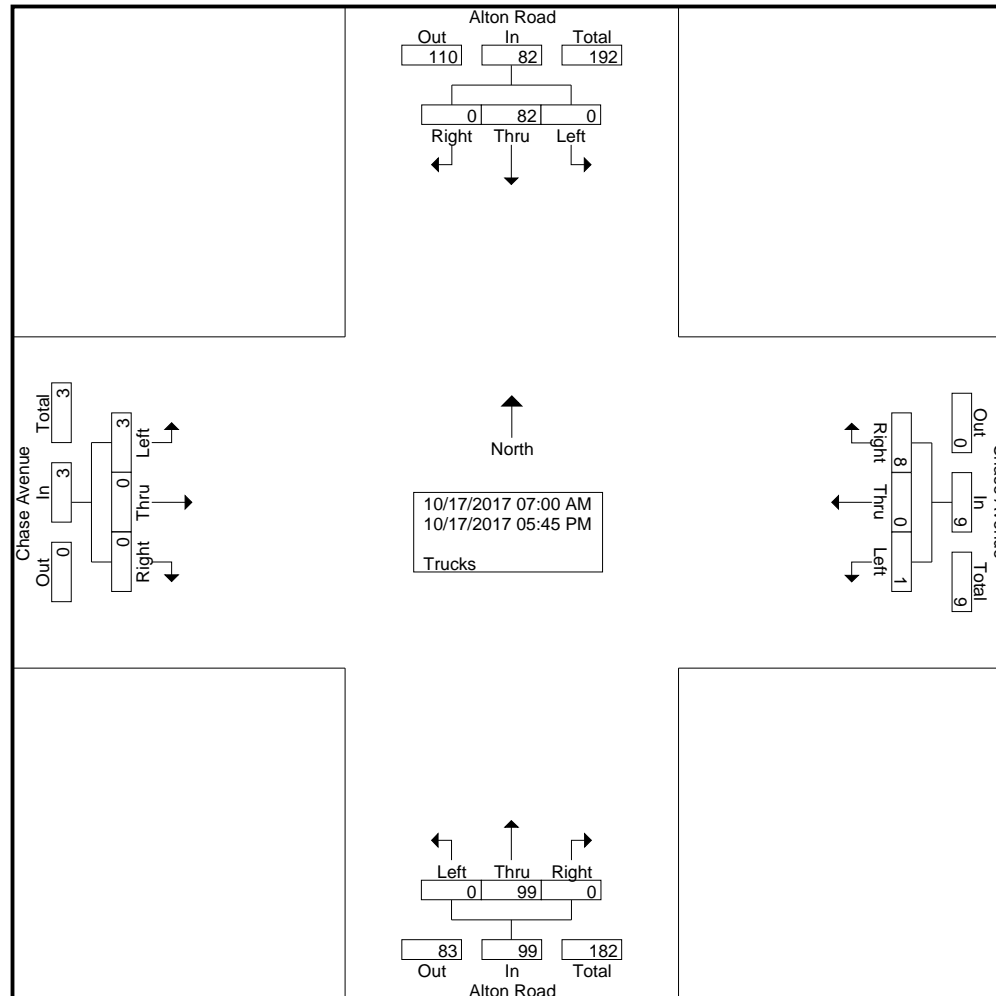
Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



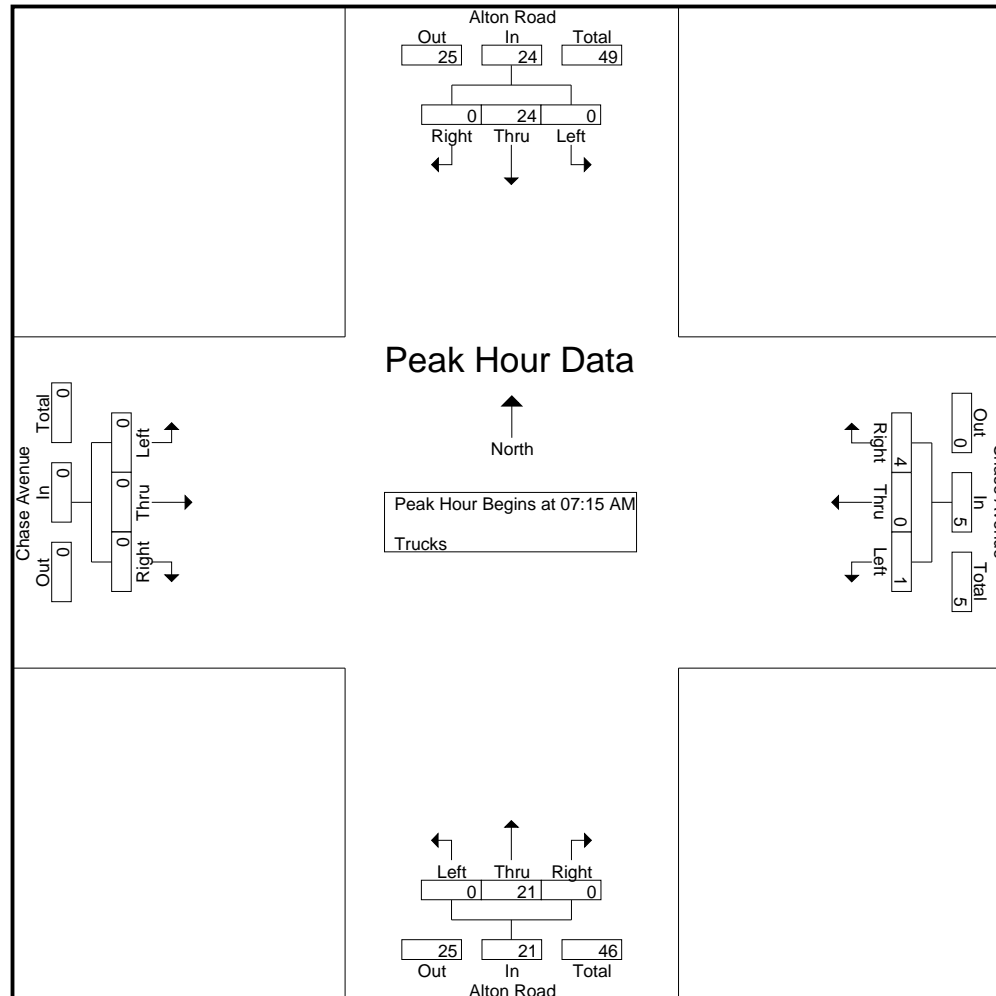
Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



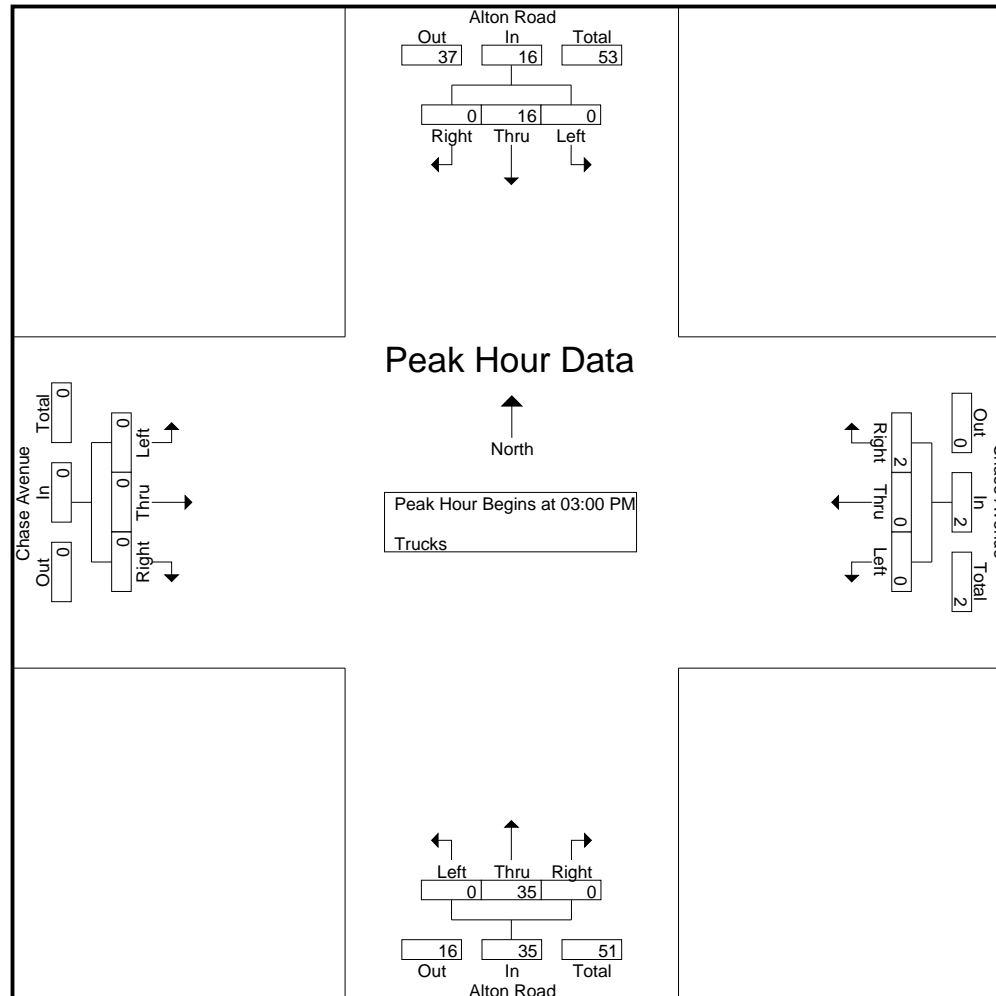
Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	Alton Road Southbound					Alton Road Northbound					Chase Avenue Westbound					Chase Avenue Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	2	391	0	393	0	0	212	3	215	0	5	0	28	33	0	10	0	0	10	651
07:15 AM	0	0	420	0	420	0	0	211	4	215	0	4	0	49	53	0	14	0	0	14	702
07:30 AM	0	0	368	0	368	0	0	185	3	188	0	7	0	24	31	0	14	4	0	18	605
07:45 AM	0	0	420	0	420	0	0	180	7	187	0	7	0	36	43	0	20	6	3	29	679
Total	0	2	1599	0	1601	0	0	788	17	805	0	23	0	137	160	0	58	10	3	71	2637
08:00 AM	0	0	403	0	403	0	0	207	8	215	0	6	0	49	55	0	18	14	1	33	706
08:15 AM	0	0	422	0	422	0	0	246	23	269	0	5	0	34	39	0	9	7	1	17	747
08:30 AM	0	1	434	0	435	0	0	285	30	315	0	9	0	39	48	0	15	6	1	22	820
08:45 AM	0	0	449	0	449	0	0	253	14	267	0	22	0	37	59	0	18	3	1	22	797
Total	0	1	1708	0	1709	0	0	991	75	1066	0	42	0	159	201	0	60	30	4	94	3070
*** BREAK ***																					
03:00 PM	0	0	340	0	340	0	0	443	11	454	0	17	0	42	59	0	35	5	3	43	896
03:15 PM	0	0	363	0	363	0	0	498	8	506	0	14	0	44	58	0	33	5	1	39	966
03:30 PM	0	1	388	0	389	0	0	476	9	485	0	18	0	46	64	0	39	6	1	46	984
03:45 PM	0	1	360	0	361	0	0	417	15	432	0	8	0	36	44	0	30	3	2	35	872
Total	0	2	1451	0	1453	0	0	1834	43	1877	0	57	0	168	225	0	137	19	7	163	3718
04:00 PM	0	0	360	0	360	0	0	526	7	533	0	26	0	51	77	0	41	7	0	48	1018
04:15 PM	0	1	345	0	346	1	0	478	15	494	0	15	0	41	56	0	26	8	0	34	930
04:30 PM	0	0	425	0	425	0	0	450	11	461	0	18	0	60	78	0	35	8	1	44	1008
04:45 PM	1	0	365	0	366	0	0	428	10	438	0	12	0	43	55	0	29	6	0	35	894
Total	1	1	1495	0	1497	1	0	1882	43	1926	0	71	0	195	266	0	131	29	1	161	3850
05:00 PM	0	0	323	0	323	0	0	445	10	455	0	19	0	27	46	0	34	4	0	38	862
05:15 PM	0	0	374	0	374	0	0	481	8	489	0	13	0	26	39	0	33	1	1	35	937
05:30 PM	0	0	352	0	352	0	0	443	8	451	0	15	0	26	41	0	28	7	1	36	880
05:45 PM	0	0	352	0	352	0	0	426	7	433	0	8	0	27	35	0	31	3	0	34	854
Total	0	0	1401	0	1401	0	0	1795	33	1828	0	55	0	106	161	0	126	15	2	143	3533
Grand Total	1	6	7654	0	7661	1	0	7290	211	7502	0	248	0	765	1013	0	512	103	17	632	16808
Apprch %	0	0.1	99.9	0		0	0	97.2	2.8		0	24.5	0	75.5		0	81	16.3	2.7		
Total %	0	0	45.5	0	45.6	0	0	43.4	1.3	44.6	0	1.5	0	4.6	6	0	3	0.6	0.1	3.8	
Vehicle	1	6	7572	0	7579	1	0	7191	211	7403	0	247	0	757	1004	0	509	103	17	629	16615
% Vehicle	100	100	98.9	0	98.9	100	0	98.6	100	98.7	0	99.6	0	99	99.1	0	99.4	100	100	99.5	98.9

Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2

Groups Printed- Vehicle - Trucks

	Alton Road Southbound					Alton Road Northbound					Chase Avenue Westbound					Chase Avenue Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	0	82	0	82	0	0	99	0	99	0	1	0	8	9	0	3	0	0	3	193
% Trucks	0	0	1.1	0	1.1	0	0	1.4	0	1.3	0	0.4	0	1	0.9	0	0.6	0	0	0.5	1.1

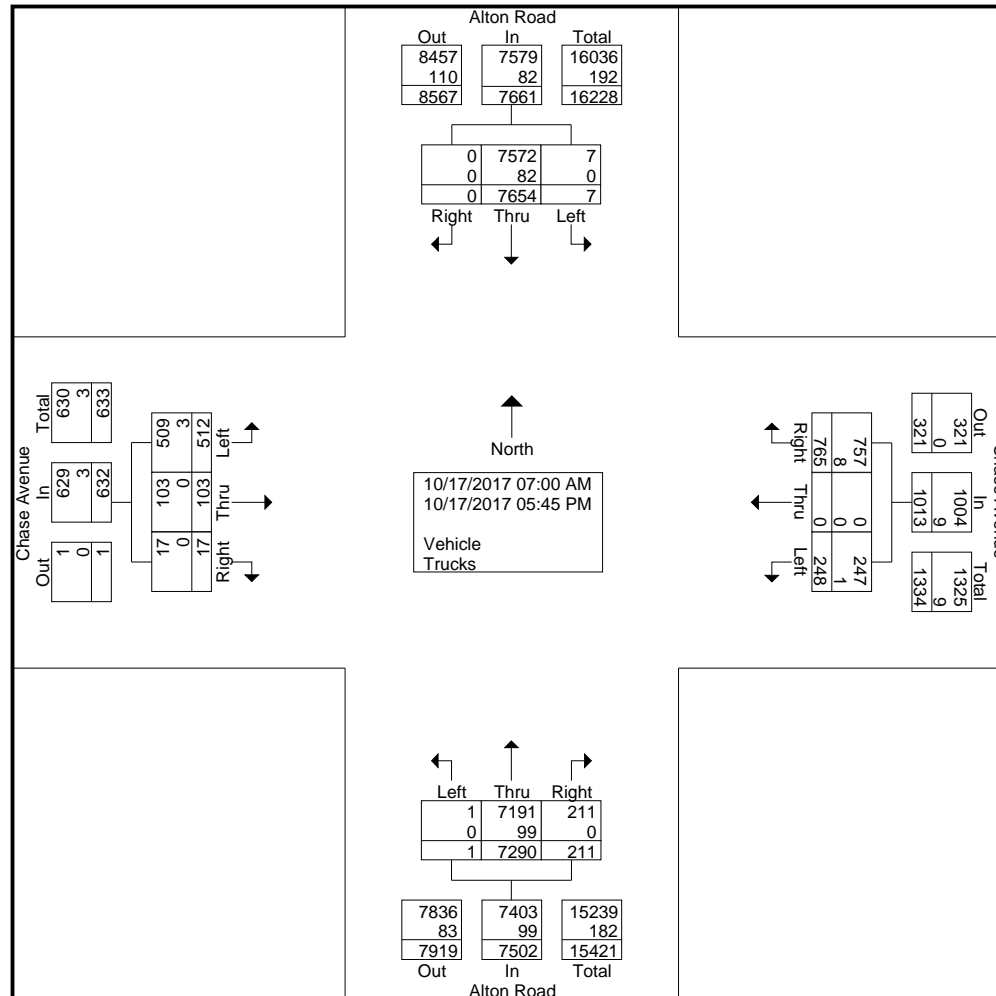
Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 3



Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4

Start Time	Alton Road Southbound					Alton Road Northbound					Chase Avenue Westbound					Chase Avenue Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	0	403	0	403	0	0	207	8	215	0	6	0	49	55	0	18	14	1	33	706	
08:15 AM	0	0	422	0	422	0	0	246	23	269	0	5	0	34	39	0	9	7	1	17	747	
08:30 AM	0	1	434	0	435	0	0	285	30	315	0	9	0	39	48	0	15	6	1	22	820	
08:45 AM	0	0	449	0	449	0	0	253	14	267	0	22	0	37	59	0	18	3	1	22	797	
Total Volume	0	1	1708	0	1709	0	0	991	75	1066	0	42	0	159	201	0	60	30	4	94	3070	
% App. Total	0	0.1	99.9	0		0	0	93	7		0	20.9	0	79.1		0	63.8	31.9	4.3			
PHF	.000	.250	.951	.000	.952	.000	.000	.869	.625	.846	.000	.477	.000	.811	.852	.000	.833	.536	1.00	.712	.936	

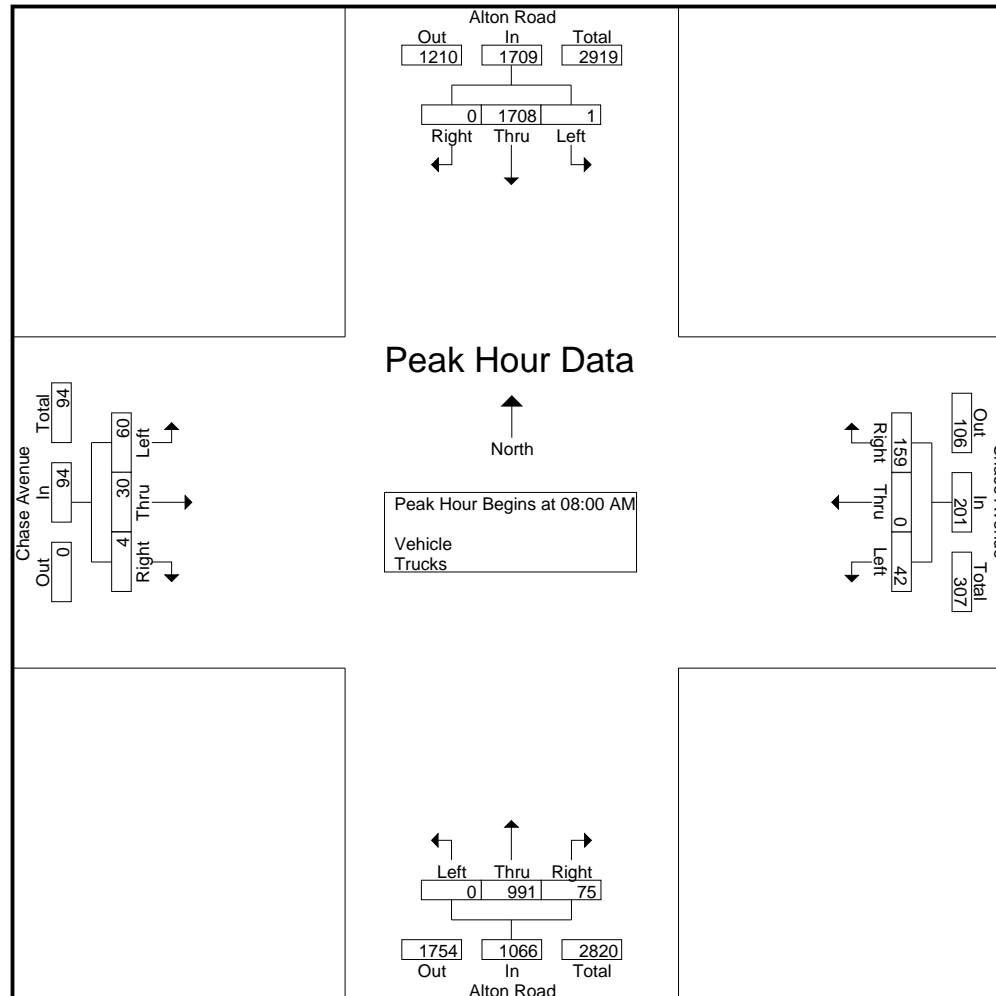
Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 5



Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6

Start Time	Alton Road Southbound					Alton Road Northbound					Chase Avenue Westbound					Chase Avenue Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:00 PM																					
04:00 PM	0	0	360	0	360	0	0	526	7	533	0	26	0	51	77	0	41	7	0	48	1018
04:15 PM	0	1	345	0	346	1	0	478	15	494	0	15	0	41	56	0	26	8	0	34	930
04:30 PM	0	0	425	0	425	0	0	450	11	461	0	18	0	60	78	0	35	8	1	44	1008
04:45 PM	1	0	365	0	366	0	0	428	10	438	0	12	0	43	55	0	29	6	0	35	894
Total Volume	1	1	1495	0	1497	1	0	1882	43	1926	0	71	0	195	266	0	131	29	1	161	3850
% App. Total	0.1	0.1	99.9	0		0.1	0	97.7	2.2		0	26.7	0	73.3		0	81.4	18	0.6		
PHF	.250	.250	.879	.000	.881	.250	.000	.894	.717	.903	.000	.683	.000	.813	.853	.000	.799	.906	.250	.839	.945

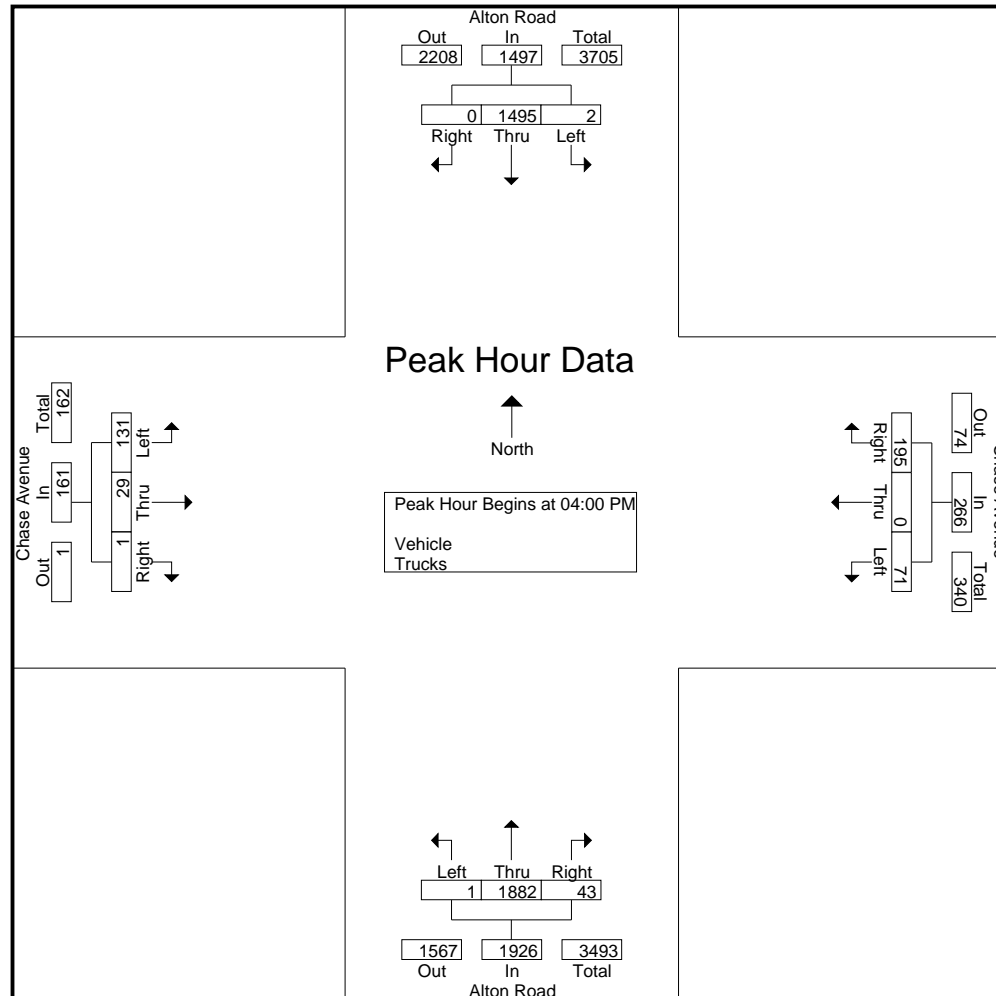
Alton Road & Chase Avenue

File Name : TMC-17 Alton Rd & Chase Avenue

Site Code : 00000000

Start Date : 10/17/2017

Page No : 7



Alton Road & W 34th Street

File Name : TMC-18 Alton Rd & W 34th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	Southbound			Alton Road Northbound			W 34th Street Westbound			Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
*** BREAK ***													
07:30 AM	0	0	0	1	0	1	3	0	3	0	0	0	4
*** BREAK ***													
Total	0	0	0	1	0	1	3	0	3	0	0	0	4
*** BREAK ***													
03:30 PM	0	0	0	0	0	0	1	0	1	0	0	0	1
*** BREAK ***													
Total	0	0	0	0	0	0	1	0	1	0	0	0	1
04:00 PM	0	0	0	0	0	0	2	0	2	0	0	0	2
04:15 PM	0	0	0	0	0	0	1	0	1	0	0	0	1
*** BREAK ***													
Total	0	0	0	0	0	0	3	0	3	0	0	0	3
05:00 PM	0	0	0	0	0	0	2	0	2	0	0	0	2
05:15 PM	0	0	0	0	0	0	2	0	2	0	0	0	2
05:30 PM	0	0	0	0	0	0	5	0	5	0	0	0	5
05:45 PM	0	0	0	0	0	0	1	0	1	0	0	0	1
Total	0	0	0	0	0	0	10	0	10	0	0	0	10
Grand Total	0	0	0	1	0	1	17	0	17	0	0	0	18
Apprch %	0	0		100	0		100	0		0	0		
Total %	0	0		5.6	0	5.6	94.4	0	94.4	0	0		

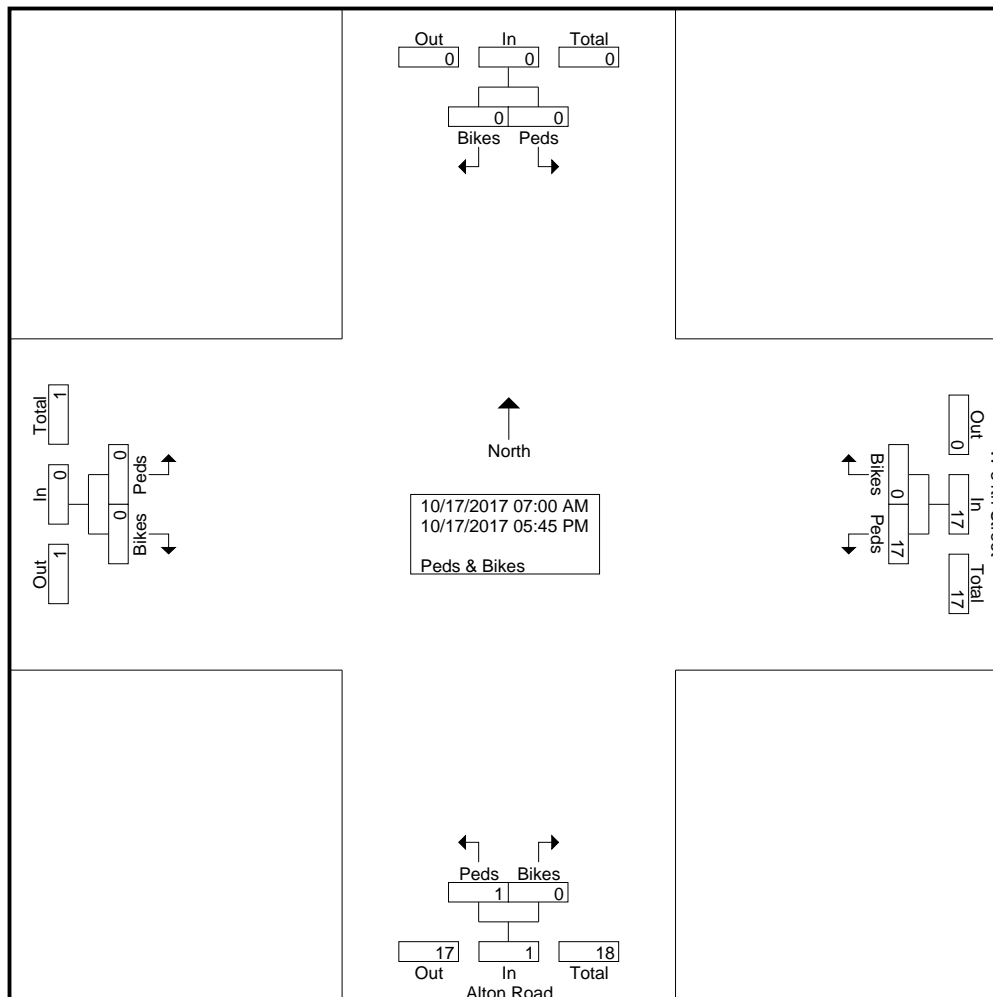
Alton Road & W 34th Street

File Name : TMC-18 Alton Rd & W 34th Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



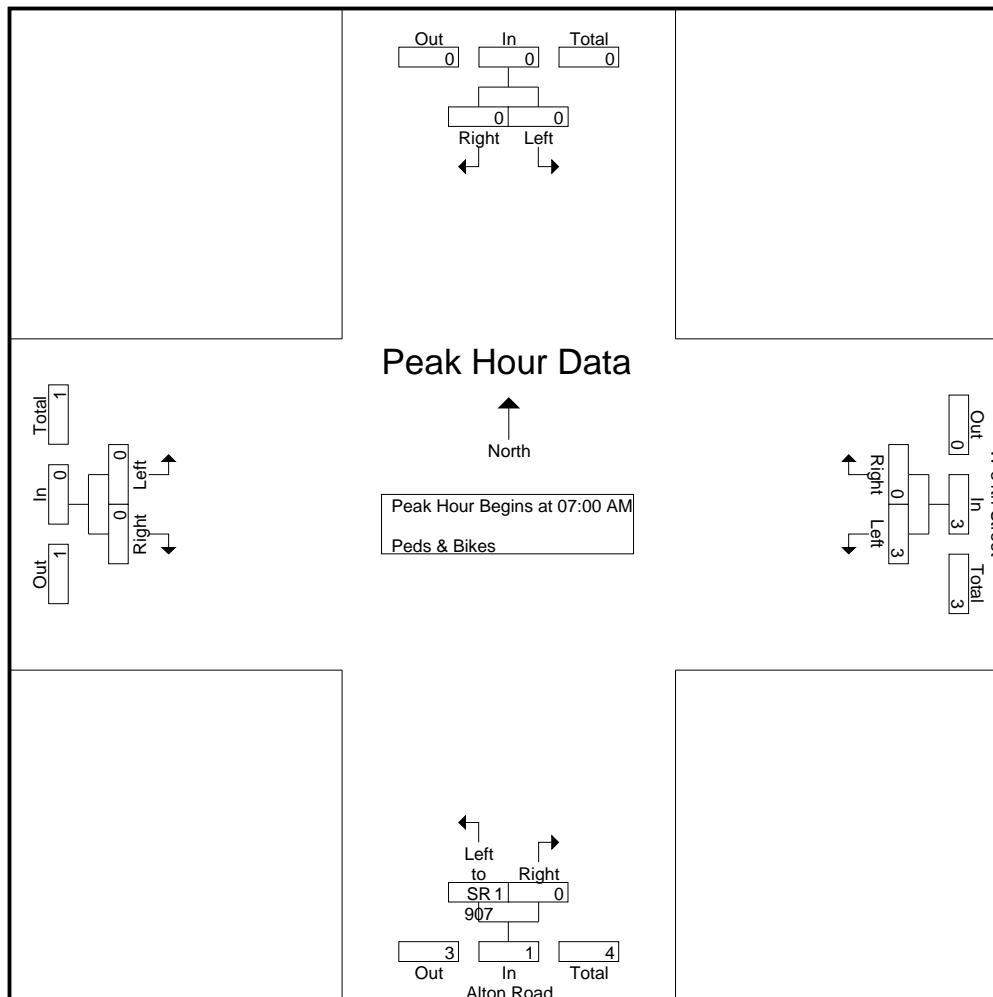
Alton Road & W 34th Street

File Name : TMC-18 Alton Rd & W 34th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Southbound			Alton Road Northbound			W 34th Street Westbound			Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:00 AM													
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
07:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
07:30 AM	0	0	0	1	0	1	3	0	3	0	0	0	4
07:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	1	0	1	3	0	3	0	0	0	4
% App. Total	0	0		100	0		100	0		0	0		
PHF	.000	.000	.000	.250	.000	.250	.250	.000	.250	.000	.000	.000	.250

Alton Road & W 34th Street

File Name : TMC-18 Alton Rd & W 34th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4



Alton Road & W 34th Street

File Name : TMC-18 Alton Rd & W 34th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	Southbound			Alton Road Northbound			W 34th Street Westbound			Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 05:00 PM													
05:00 PM	0	0	0	0	0	0	2	0	2	0	0	0	2
05:15 PM	0	0	0	0	0	0	2	0	2	0	0	0	2
05:30 PM	0	0	0	0	0	0	5	0	5	0	0	0	5
05:45 PM	0	0	0	0	0	0	1	0	1	0	0	0	1
Total Volume	0	0	0	0	0	0	10	0	10	0	0	0	10
% App. Total	0	0		0	0		100	0		0	0		
PHF	.000	.000	.000	.000	.000	.000	.500	.000	.500	.000	.000	.000	.500

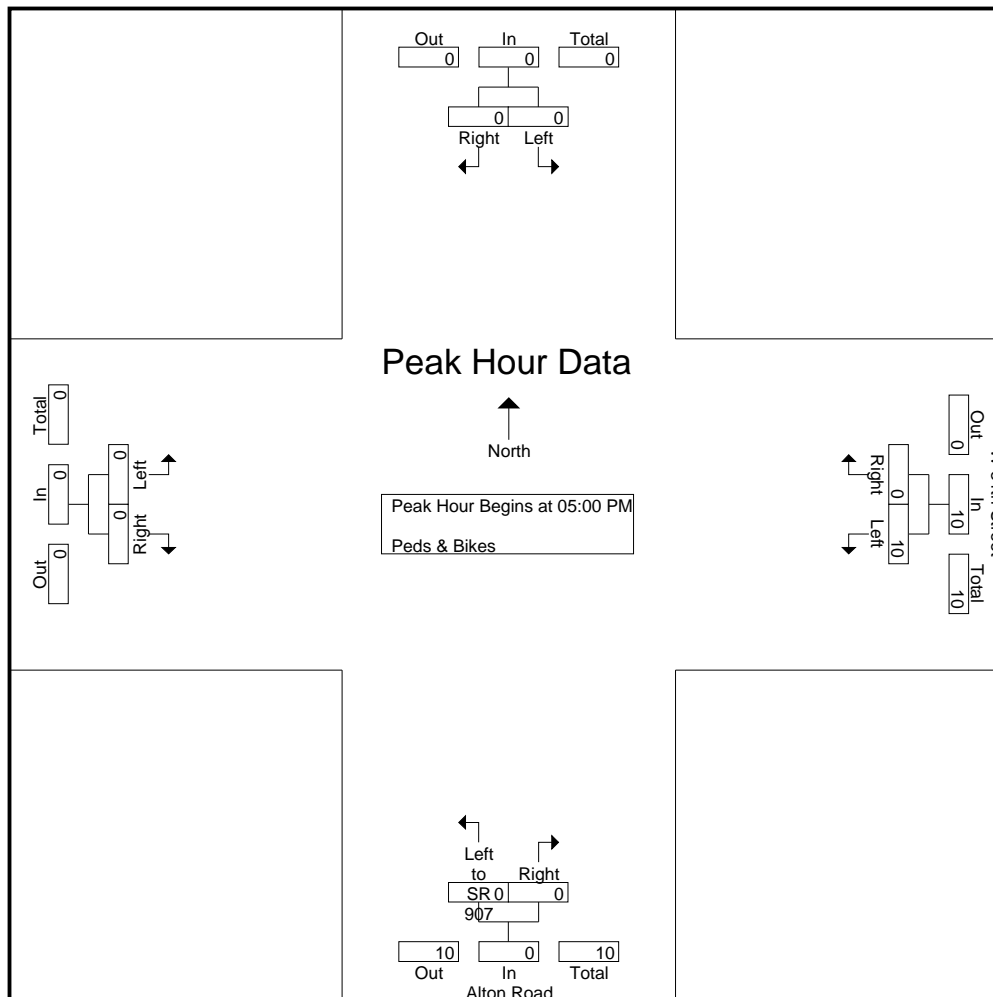
Alton Road & W 34th Street

File Name : TMC-18 Alton Rd & W 34th Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



Alton Road & W 34th Street

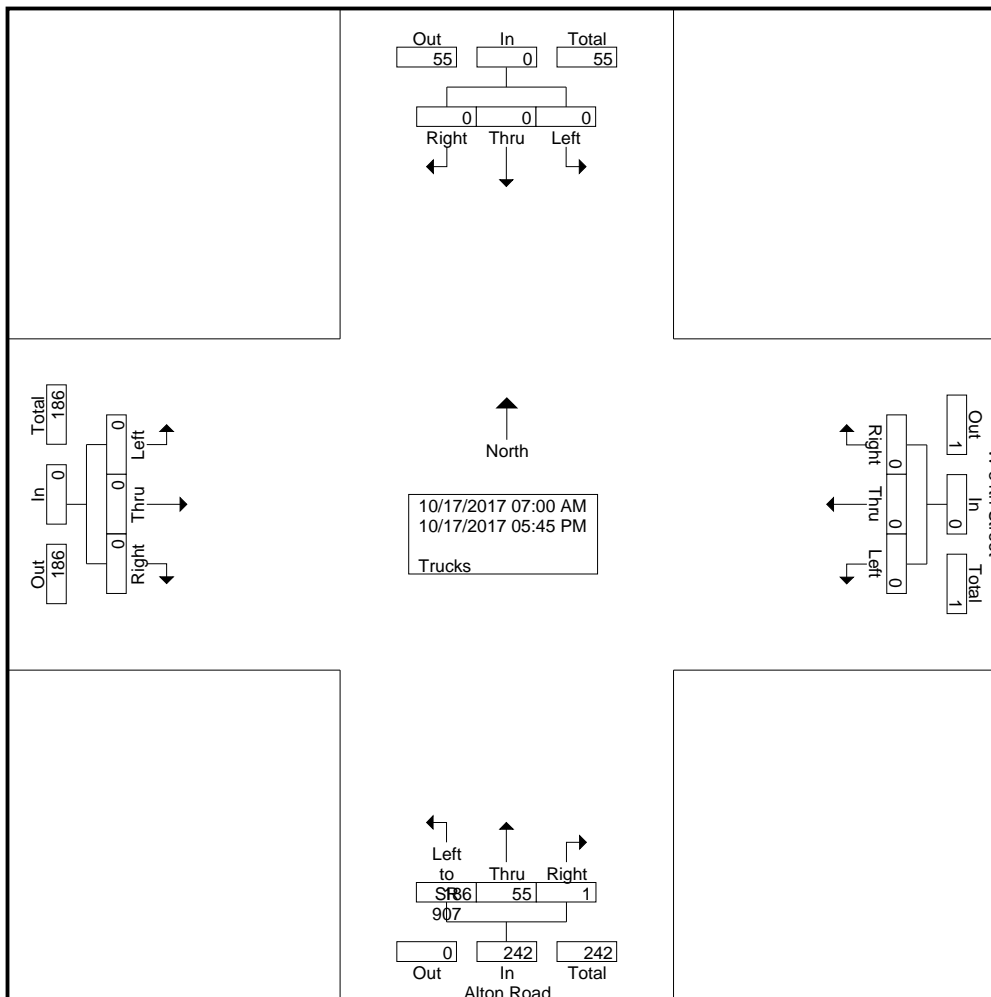
File Name : TMC-18 Alton Rd & W 34th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Trucks

Start Time	Southbound					Alton Road Northbound					W 34th Street Westbound					Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left to SR 907	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	0	4	1	0	5	0	0	0	0	0	0	0	0	0	0	5
07:15 AM	0	0	0	0	0	0	6	2	0	8	0	0	0	0	0	0	0	0	0	0	8
07:30 AM	0	0	0	0	0	0	4	1	0	5	0	0	0	0	0	0	0	0	0	0	5
07:45 AM	0	0	0	0	0	0	6	6	0	12	0	0	0	0	0	0	0	0	0	0	12
Total	0	0	0	0	0	0	20	10	0	30	0	0	0	0	0	0	0	0	0	0	30
08:00 AM	0	0	0	0	0	0	12	2	0	14	0	0	0	0	0	0	0	0	0	0	14
08:15 AM	0	0	0	0	0	0	7	1	1	9	0	0	0	0	0	0	0	0	0	0	9
08:30 AM	0	0	0	0	0	0	11	8	0	19	0	0	0	0	0	0	0	0	0	0	19
08:45 AM	0	0	0	0	0	0	8	4	0	12	0	0	0	0	0	0	0	0	0	0	12
Total	0	0	0	0	0	0	38	15	1	54	0	0	0	0	0	0	0	0	0	0	54
*** BREAK ***																					
03:00 PM	0	0	0	0	0	0	11	4	0	15	0	0	0	0	0	0	0	0	0	0	15
03:15 PM	0	0	0	0	0	0	14	5	0	19	0	0	0	0	0	0	0	0	0	0	19
03:30 PM	0	0	0	0	0	0	17	2	0	19	0	0	0	0	0	0	0	0	0	0	19
03:45 PM	0	0	0	0	0	0	12	5	0	17	0	0	0	0	0	0	0	0	0	0	17
Total	0	0	0	0	0	0	54	16	0	70	0	0	0	0	0	0	0	0	0	0	70
04:00 PM	0	0	0	0	0	0	15	1	0	16	0	0	0	0	0	0	0	0	0	0	16
04:15 PM	0	0	0	0	0	0	12	0	0	12	0	0	0	0	0	0	0	0	0	0	12
04:30 PM	0	0	0	0	0	0	9	4	0	13	0	0	0	0	0	0	0	0	0	0	13
04:45 PM	0	0	0	0	0	0	13	1	0	14	0	0	0	0	0	0	0	0	0	0	14
Total	0	0	0	0	0	0	49	6	0	55	0	0	0	0	0	0	0	0	0	0	55
05:00 PM	0	0	0	0	0	0	8	3	0	11	0	0	0	0	0	0	0	0	0	0	11
05:15 PM	0	0	0	0	0	0	4	3	0	7	0	0	0	0	0	0	0	0	0	0	7
05:30 PM	0	0	0	0	0	0	6	1	0	7	0	0	0	0	0	0	0	0	0	0	7
05:45 PM	0	0	0	0	0	0	7	1	0	8	0	0	0	0	0	0	0	0	0	0	8
Total	0	0	0	0	0	0	25	8	0	33	0	0	0	0	0	0	0	0	0	0	33
Grand Total	0	0	0	0	0	0	186	55	1	242	0	0	0	0	0	0	0	0	0	0	242
Apprch %	0	0	0	0	0	0	76.9	22.7	0.4		0	0	0	0	0	0	0	0	0	0	
Total %	0	0	0	0	0	0	76.9	22.7	0.4	100	0	0	0	0	0	0	0	0	0	0	

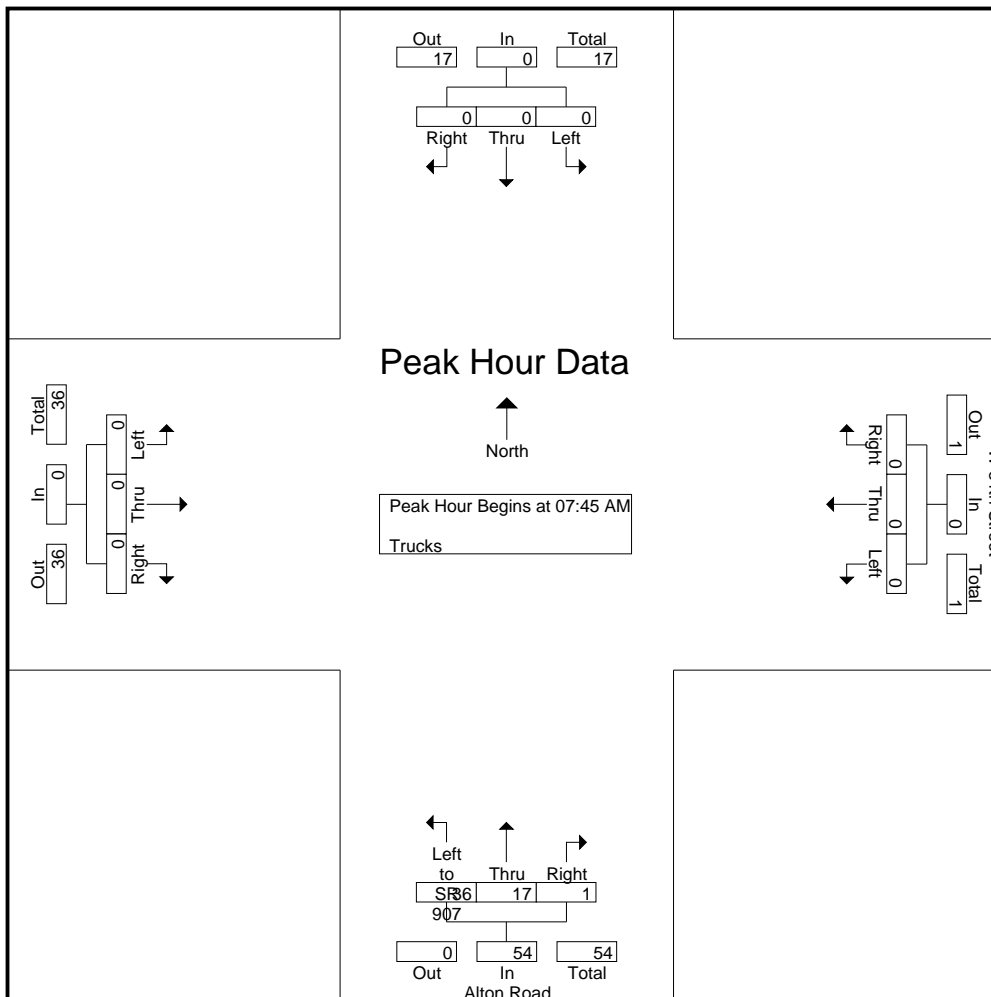
Alton Road & W 34th Street

File Name : TMC-18 Alton Rd & W 34th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 2



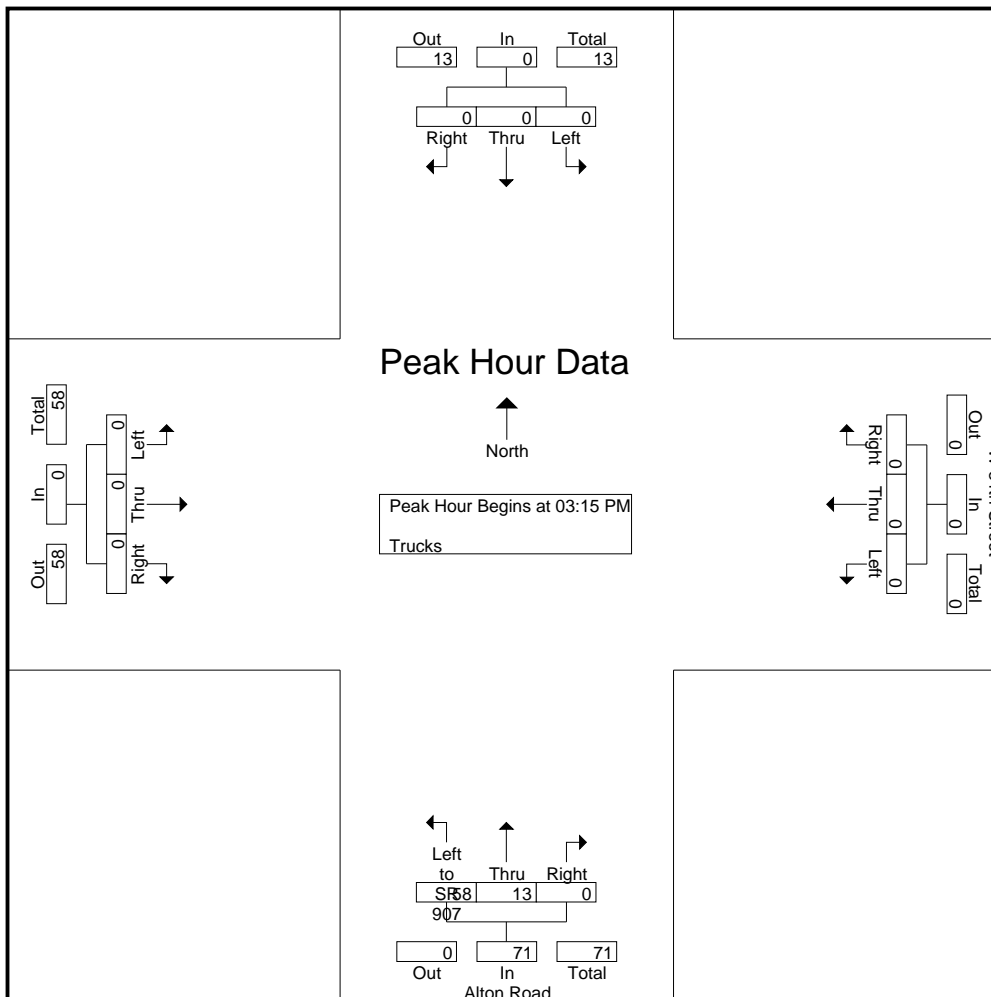
Alton Road & W 34th Street

File Name : TMC-18 Alton Rd & W 34th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4



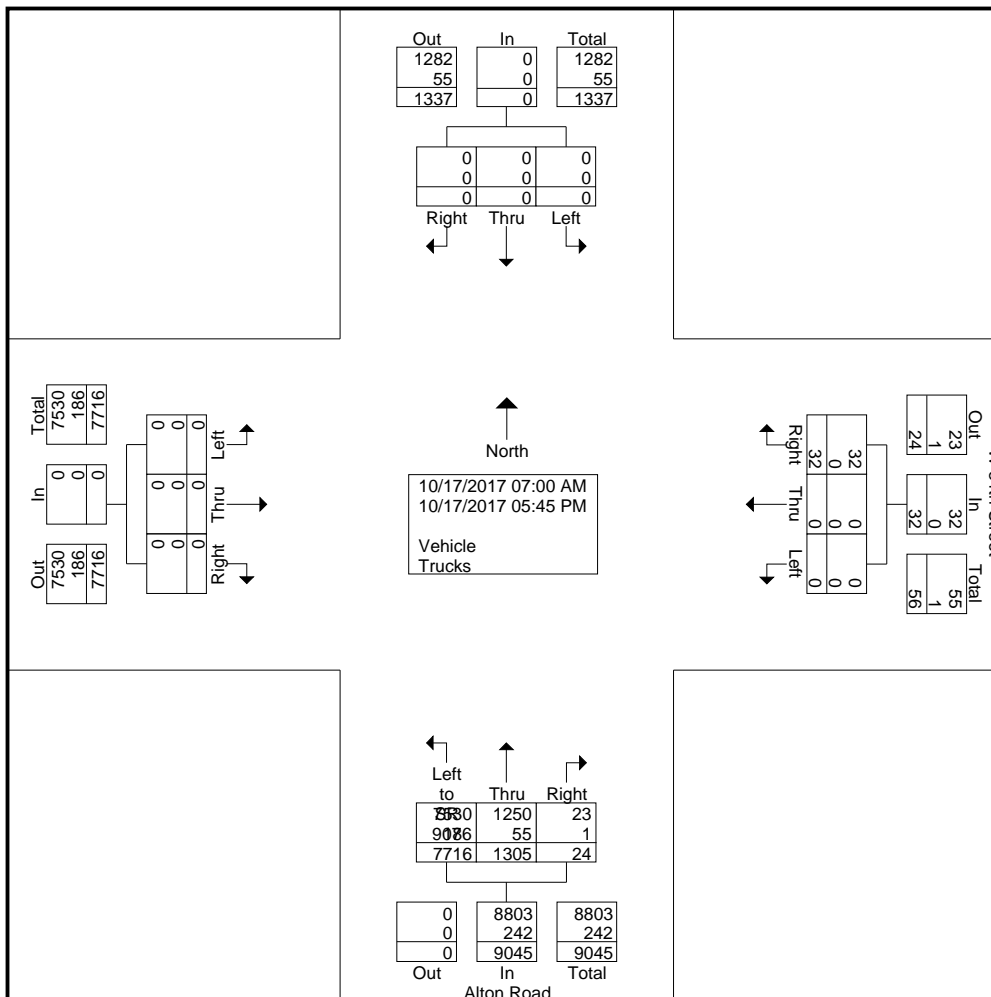
Alton Road & W 34th Street

File Name : TMC-18 Alton Rd & W 34th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6



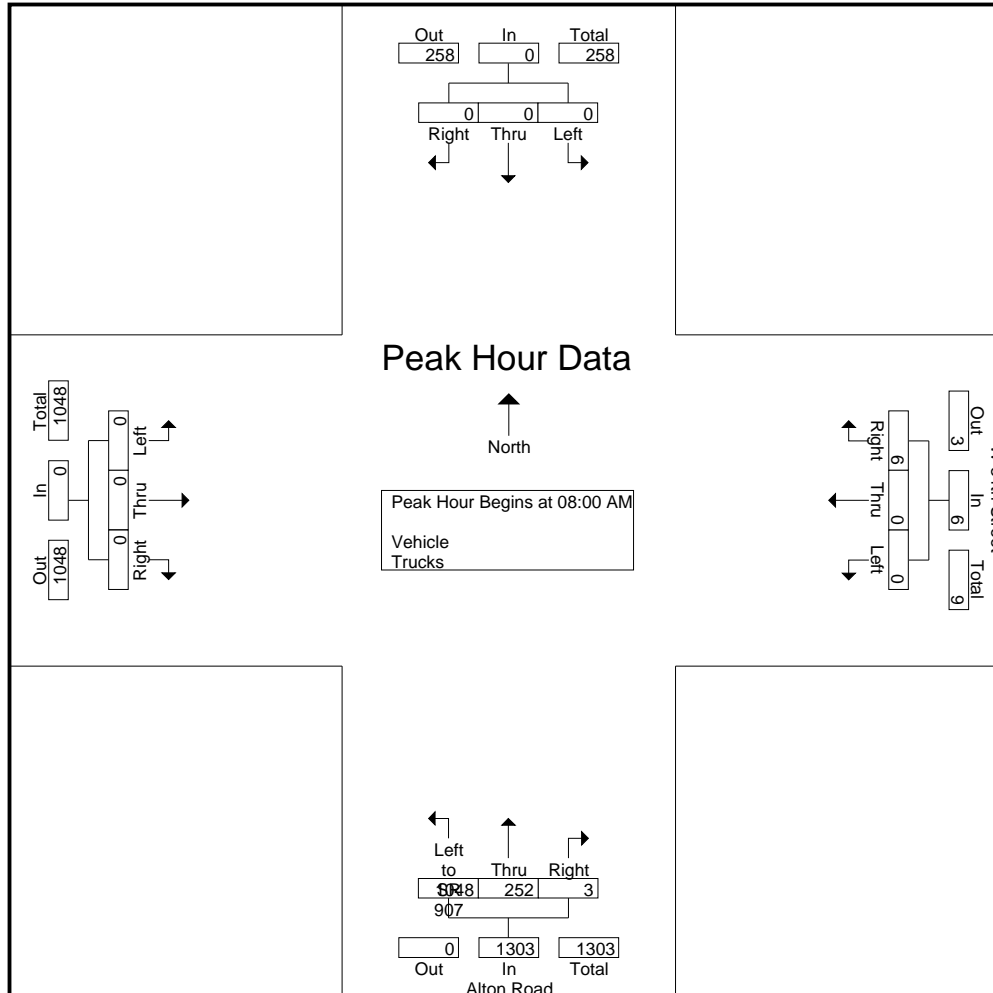
Alton Road & W 34th Street

File Name : TMC-18 Alton Rd & W 34th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 2



Alton Road & W 34th Street

File Name : TMC-18 Alton Rd & W 34th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4



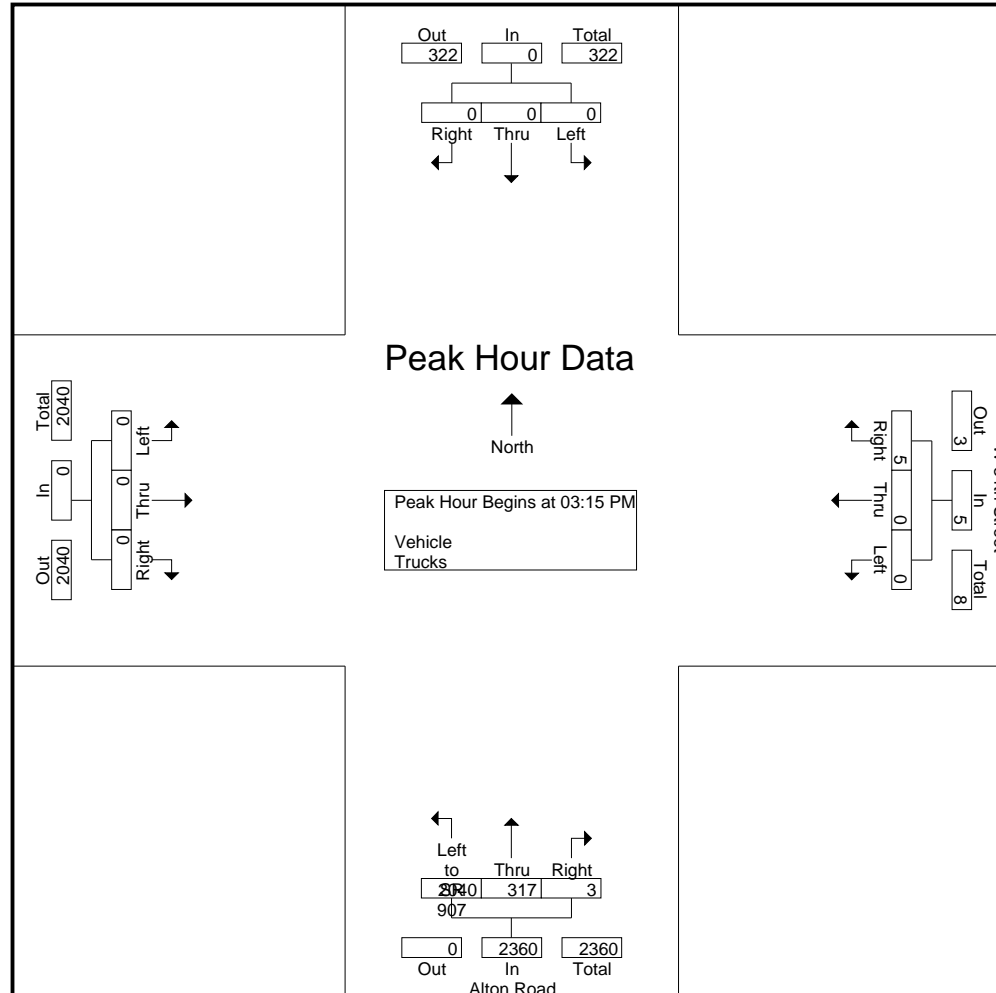
Alton Road & W 34th Street

File Name : TMC-18 Alton Rd & W 34th Street

Site Code : 00000000

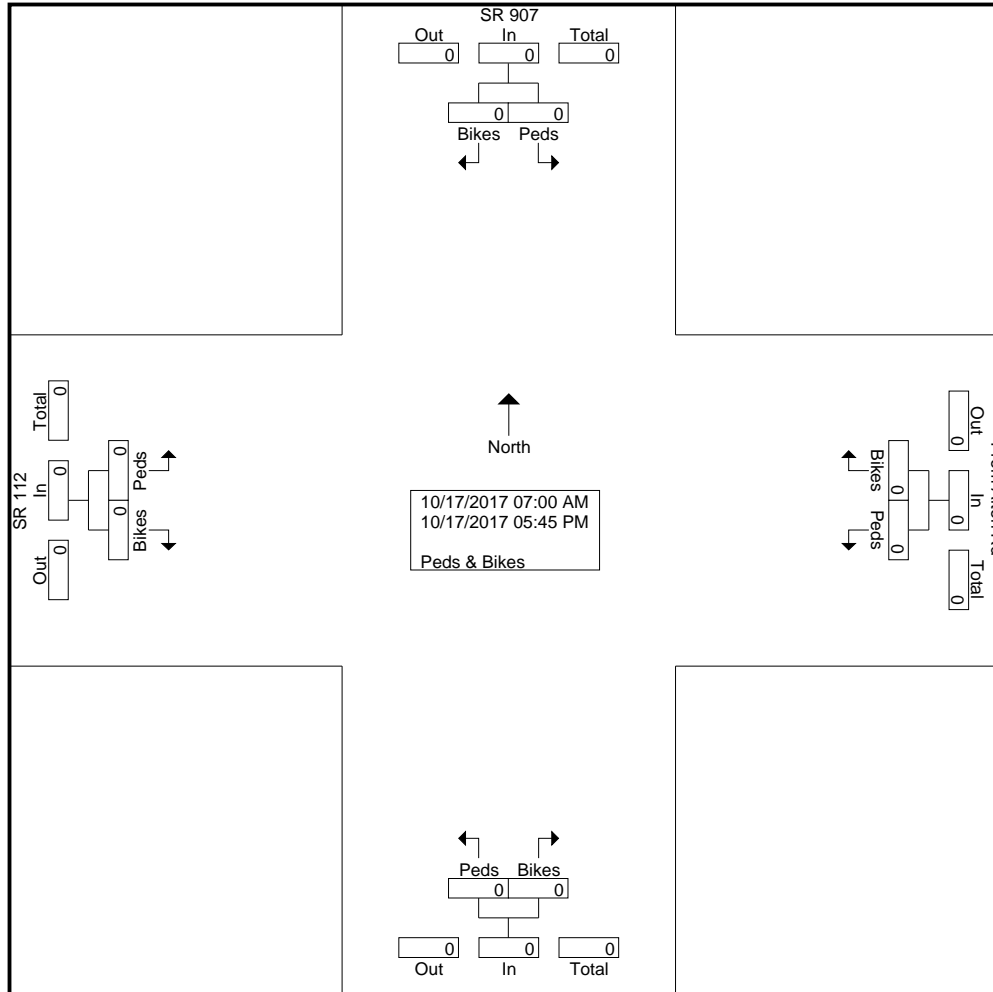
Start Date : 10/17/2017

Page No : 6



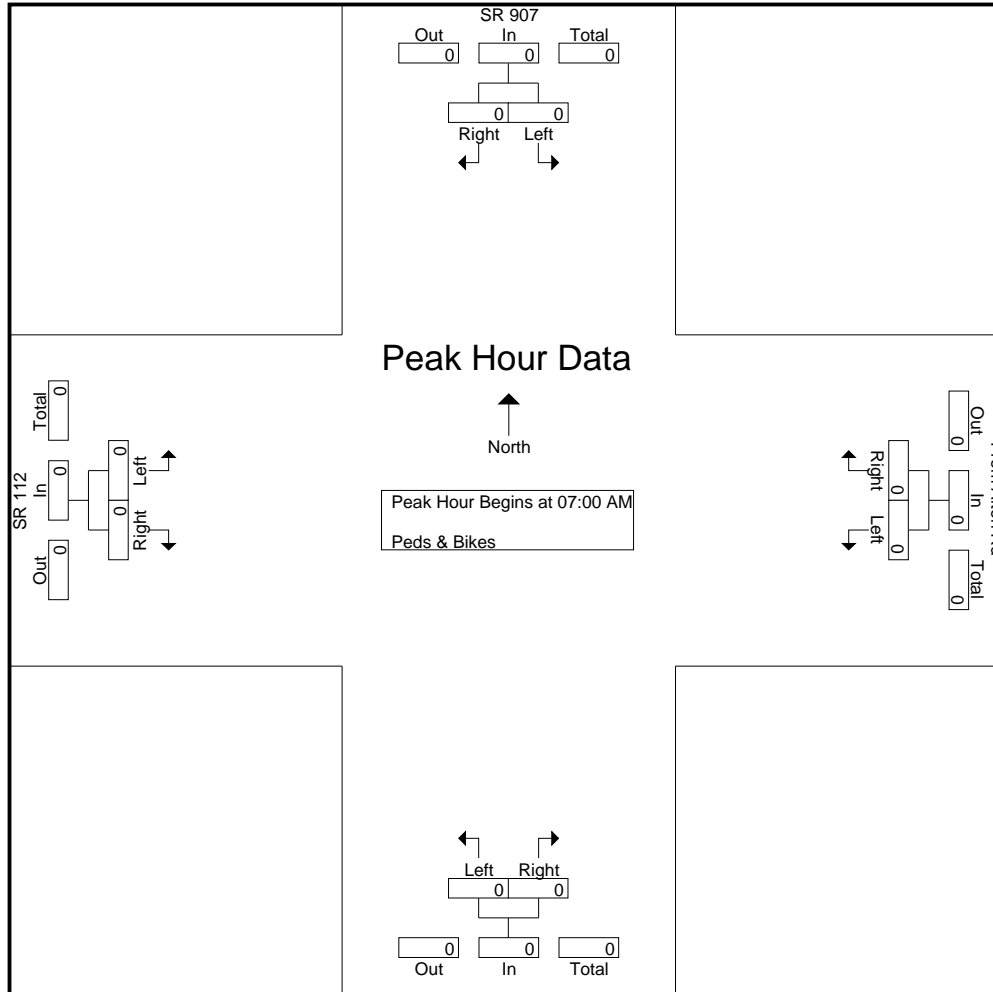
SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 2



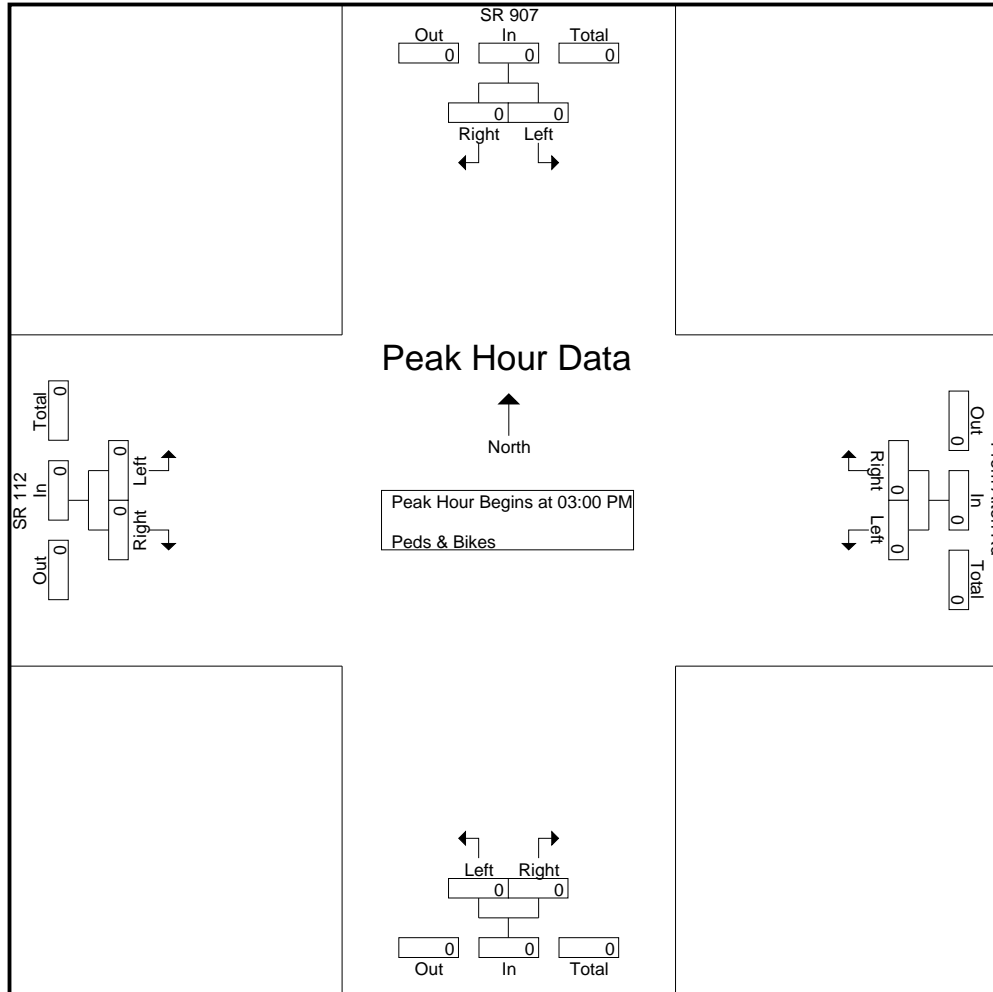
SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112
Site Code : 00000000
Start Date : 10/17/2017
Page No : 4



SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6



SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Trucks

Start Time	SR 907 Southbound					Northbound					From Alton Rd Westbound					SR 112 Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	12	12	13
07:15 AM	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	14	14	17
07:30 AM	0	0	3	0	3	0	0	0	0	0	0	1	0	0	1	0	0	0	8	8	12
07:45 AM	0	0	2	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	14	14	17
Total	0	0	8	0	8	0	0	0	0	0	0	3	0	0	3	0	0	0	48	48	59
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	11	11
08:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13	13	13
08:30 AM	0	0	8	0	8	0	0	0	0	0	0	1	0	0	1	0	0	0	20	20	29
08:45 AM	0	0	9	0	9	0	0	0	0	0	0	2	0	0	2	0	0	0	14	14	25
Total	0	0	17	0	17	0	0	0	0	0	0	3	0	0	3	0	0	0	58	58	78
*** BREAK ***																					
03:00 PM	0	0	2	0	2	0	0	0	0	0	0	2	0	0	2	0	0	0	2	2	6
03:15 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	5	5	6
03:30 PM	0	0	3	0	3	0	0	0	0	0	0	2	0	0	2	0	0	0	1	1	6
03:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	4	4
Total	0	0	6	0	6	0	0	0	0	0	0	4	0	0	4	0	0	0	12	12	22
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	1	1	3
04:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1
04:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	2
*** BREAK ***																					
Total	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	0	4	4	6
05:00 PM	0	0	1	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	1	1	3
05:15 PM	0	0	2	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	1	1	4
05:30 PM	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	5
05:45 PM	0	0	2	0	2	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	3
Total	0	0	8	0	8	0	0	0	0	0	0	3	0	0	3	0	0	0	4	4	15
Grand Total	0	0	39	0	39	0	0	0	0	0	0	15	0	0	15	0	0	0	126	126	180
Apprch %	0	0	100	0		0	0	0	0		0	100	0	0		0	0	0	100		
Total %	0	0	21.7	0	21.7	0	0	0	0	0	0	8.3	0	0	8.3	0	0	0	70	70	

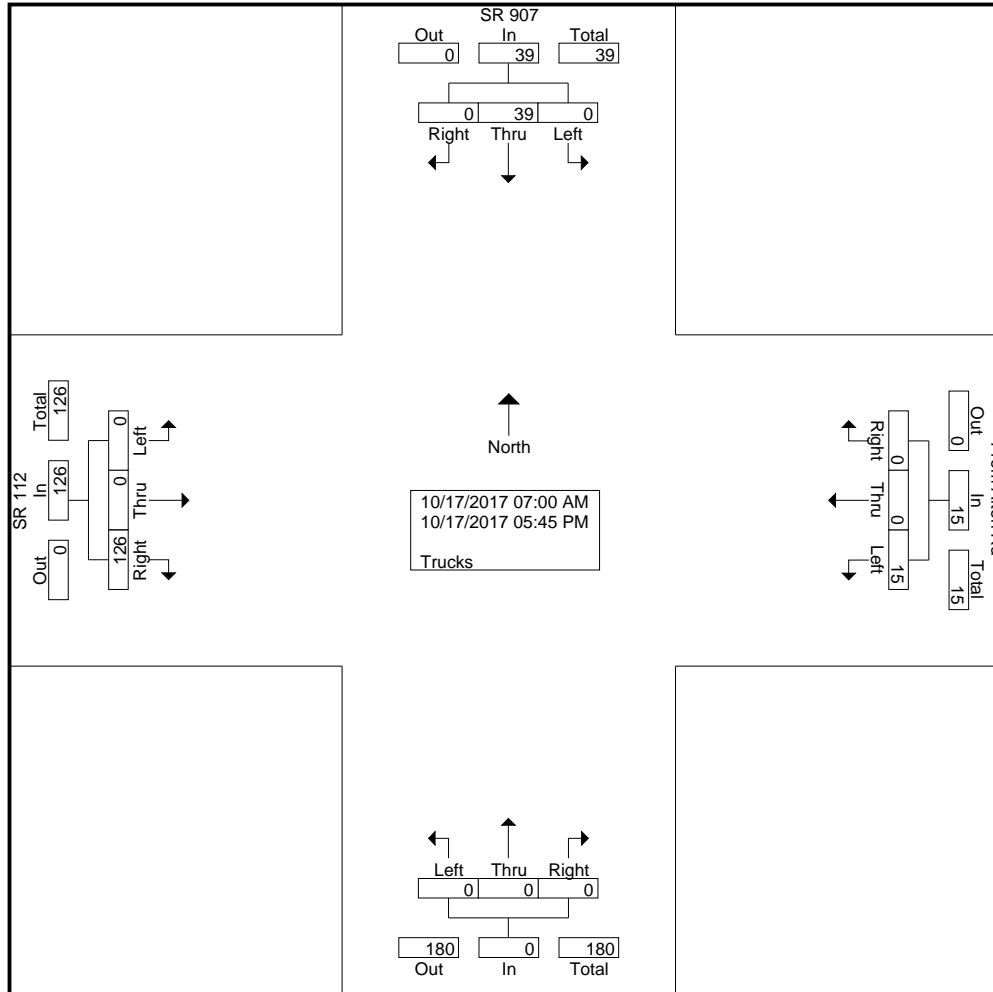
SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	SR 907 Southbound					Northbound					From Alton Rd Westbound					SR 112 Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	11	11
08:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13	13	13
08:30 AM	0	0	8	0	8	0	0	0	0	0	0	1	0	0	1	0	0	0	0	20	20	29
08:45 AM	0	0	9	0	9	0	0	0	0	0	0	2	0	0	2	0	0	0	0	14	14	25
Total Volume	0	0	17	0	17	0	0	0	0	0	0	3	0	0	3	0	0	0	0	58	58	78
% App. Total	0	0	100	0	100	0	0	0	0	0	0	100	0	0	100	0	0	0	0	100	100	78
PHF	.000	.000	.472	.000	.472	.000	.000	.000	.000	.000	.000	.375	.000	.000	.375	.000	.000	.000	.000	.725	.725	.672

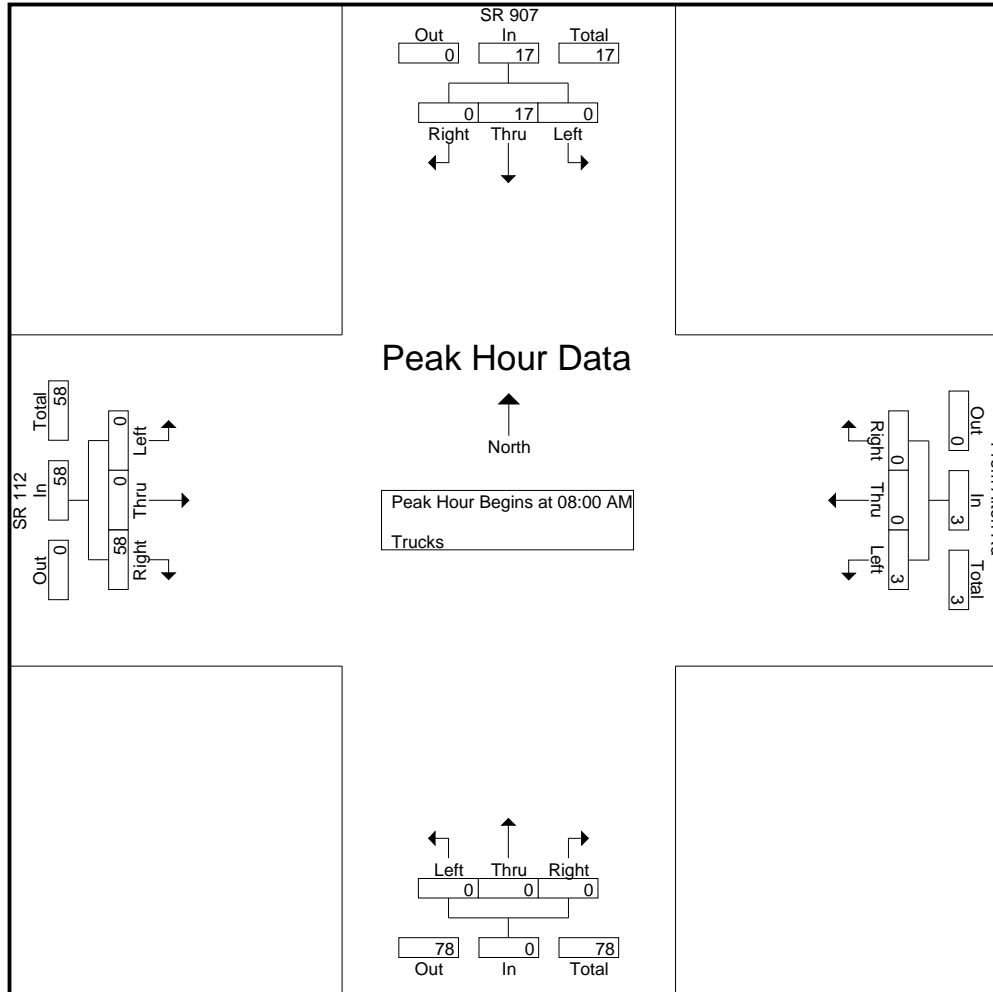
SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



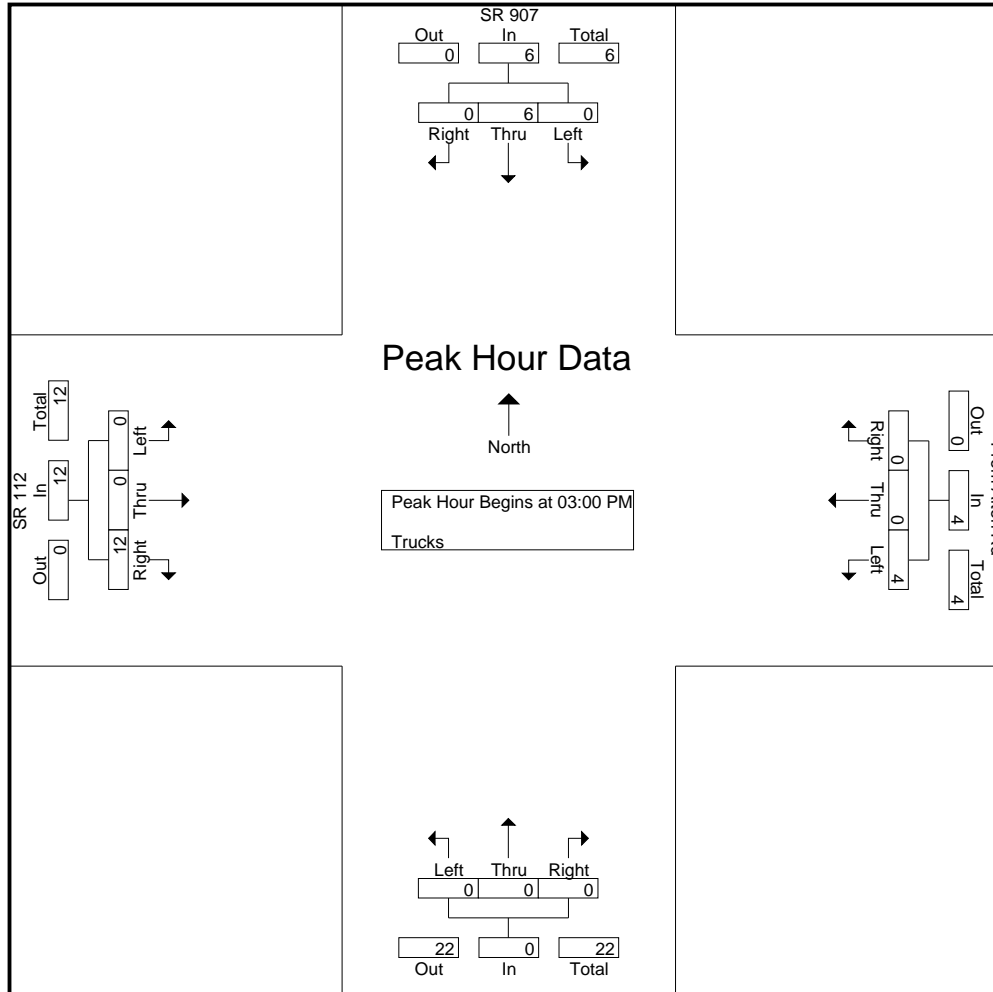
SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	SR 907 Southbound					Northbound					From Alton Rd Westbound					SR 112 Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:00 PM																					
03:00 PM	0	0	2	0	2	0	0	0	0	0	0	2	0	0	2	0	0	0	2	2	6
03:15 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	5	5	6
03:30 PM	0	0	3	0	3	0	0	0	0	0	0	2	0	2	0	0	0	0	1	1	6
03:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	4	4
Total Volume	0	0	6	0	6	0	0	0	0	0	0	4	0	4	0	0	0	0	12	12	22
% App. Total	0	0	100	0		0	0	0	0		0	100	0	0		0	0	0	100		
PHF	.000	.000	.500	.000	.500	.000	.000	.000	.000	.000	.000	.500	.000	.000	.500	.000	.000	.000	.600	.600	.917

SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6



SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	SR 907 Southbound					Northbound					From Alton Rd Westbound					SR 112 Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	50	0	50	0	0	0	0	0	0	8	0	0	8	0	0	0	349	349	407
07:15 AM	0	0	77	0	77	0	0	0	0	0	0	21	0	0	21	0	0	0	336	336	434
07:30 AM	0	0	56	0	56	0	0	0	0	0	0	31	0	0	31	0	0	0	297	297	384
07:45 AM	0	0	69	0	69	0	0	0	0	0	0	53	0	0	53	0	0	0	340	340	462
Total	0	0	252	0	252	0	0	0	0	0	0	113	0	0	113	0	0	0	1322	1322	1687
08:00 AM	0	0	76	0	76	0	0	0	0	0	0	37	0	0	37	0	0	0	309	309	422
08:15 AM	0	0	80	0	80	0	0	0	0	0	0	25	0	0	25	0	0	0	324	324	429
08:30 AM	0	0	95	0	95	0	0	0	0	0	0	58	0	0	58	0	0	0	362	362	515
08:45 AM	0	0	96	0	96	0	0	0	0	0	0	57	0	0	57	0	0	0	359	359	512
Total	0	0	347	0	347	0	0	0	0	0	0	177	0	0	177	0	0	0	1354	1354	1878
*** BREAK ***																					
03:00 PM	0	0	76	0	76	0	0	0	0	0	0	34	0	0	34	0	0	0	236	236	346
03:15 PM	0	0	91	0	91	0	0	0	0	0	0	31	0	0	31	0	0	0	208	208	330
03:30 PM	0	0	103	0	103	0	0	0	0	0	0	31	0	0	31	0	0	0	255	255	389
03:45 PM	0	0	103	0	103	0	0	0	0	0	0	32	0	0	32	0	0	0	223	223	358
Total	0	0	373	0	373	0	0	0	0	0	0	128	0	0	128	0	0	0	922	922	1423
04:00 PM	0	0	94	0	94	0	0	0	0	0	0	33	0	0	33	0	0	0	201	201	328
04:15 PM	0	0	107	0	107	0	0	0	0	0	0	34	0	0	34	0	0	0	193	193	334
04:30 PM	0	0	129	0	129	0	0	0	0	0	0	55	0	0	55	0	0	0	227	227	411
04:45 PM	0	0	92	0	92	0	0	0	0	0	0	41	0	0	41	0	0	0	206	206	339
Total	0	0	422	0	422	0	0	0	0	0	0	163	0	0	163	0	0	0	827	827	1412
05:00 PM	0	0	110	0	110	0	0	0	0	0	0	34	0	0	34	0	0	0	204	204	348
05:15 PM	0	0	112	0	112	0	0	0	0	0	0	34	0	0	34	0	0	0	205	205	351
05:30 PM	0	0	95	0	95	0	0	0	0	0	0	35	0	0	35	0	0	0	234	234	364
05:45 PM	0	0	119	0	119	0	0	0	0	0	0	34	0	0	34	0	0	0	206	206	359
Total	0	0	436	0	436	0	0	0	0	0	0	137	0	0	137	0	0	0	849	849	1422
Grand Total	0	0	1830	0	1830	0	0	0	0	0	0	718	0	0	718	0	0	0	5274	5274	7822
Apprch %	0	0	100	0	100	0	0	0	0	0	0	100	0	0	100	0	0	0	100	100	
Total %	0	0	23.4	0	23.4	0	0	0	0	0	0	9.2	0	0	9.2	0	0	0	67.4	67.4	
Vehicle	0	0	1791	0	1791	0	0	0	0	0	0	703	0	0	703	0	0	0	5148	5148	7642
% Vehicle	0	0	97.9	0	97.9	0	0	0	0	0	0	97.9	0	0	97.9	0	0	0	97.6	97.6	97.7
Trucks	0	0	39	0	39	0	0	0	0	0	0	15	0	0	15	0	0	0	126	126	180
% Trucks	0	0	2.1	0	2.1	0	0	0	0	0	0	2.1	0	0	2.1	0	0	0	2.4	2.4	2.3

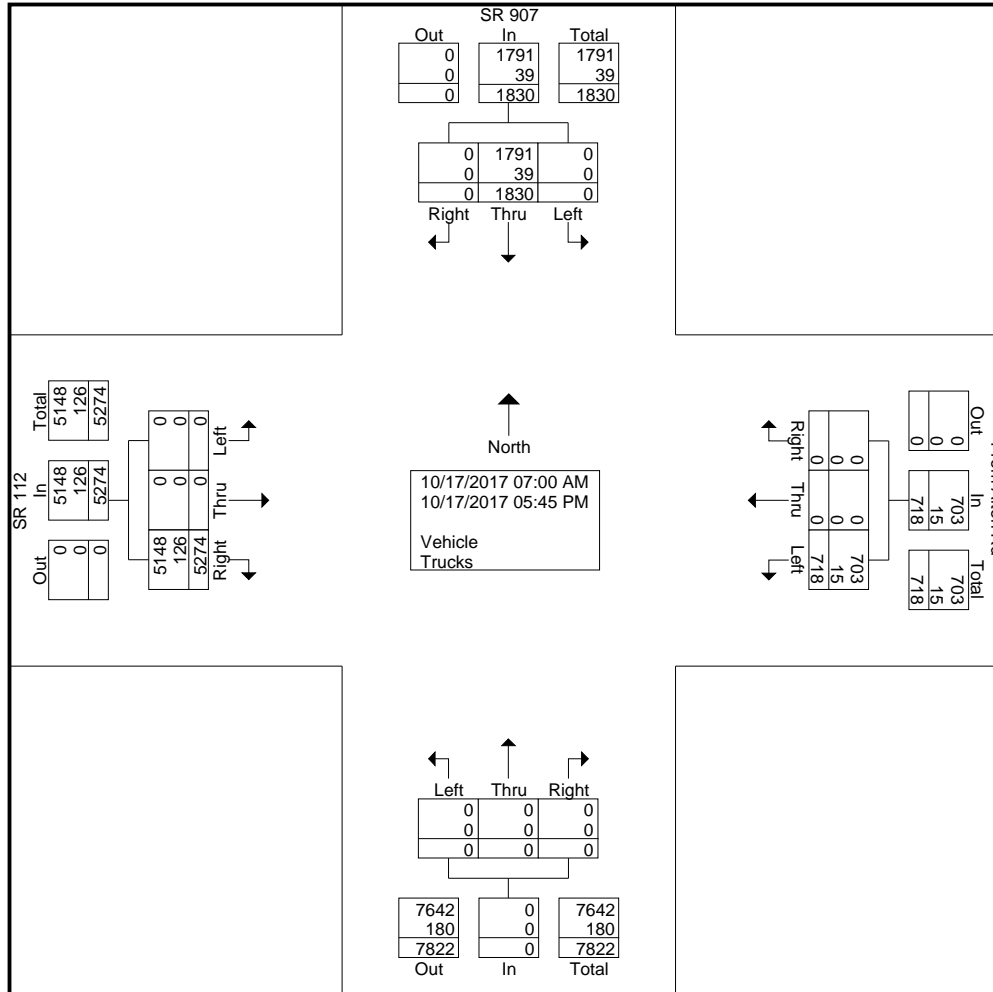
SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



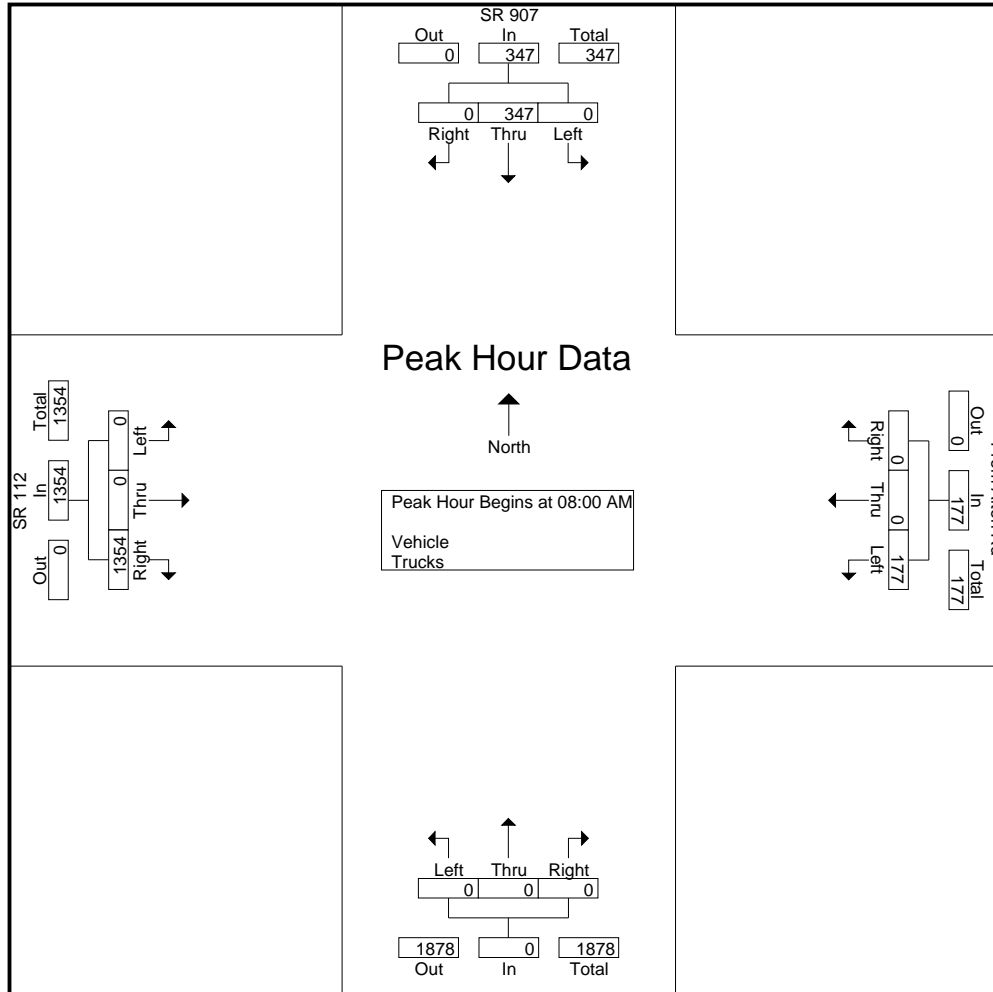
SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	SR 907 Southbound					Northbound					From Alton Rd Westbound					SR 112 Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	0	76	0	76	0	0	0	0	0	0	37	0	0	37	0	0	0	309	309	422	
08:15 AM	0	0	80	0	80	0	0	0	0	0	0	25	0	0	25	0	0	0	324	324	429	
08:30 AM	0	0	95	0	95	0	0	0	0	0	0	58	0	0	58	0	0	0	362	362	515	
08:45 AM	0	0	96	0	96	0	0	0	0	0	0	57	0	0	57	0	0	0	359	359	512	
Total Volume	0	0	347	0	347	0	0	0	0	0	0	177	0	0	177	0	0	0	1354	1354	1878	
% App. Total	0	0	100	0		0	0	0	0		0	100	0	0		0	0	0	100			
PHF	.000	.000	.904	.000	.904	.000	.000	.000	.000	.000	.000	.763	.000	.000	.763	.000	.000	.000	.935	.935	.912	

SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4



SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	SR 907 Southbound					Northbound					From Alton Rd Westbound					SR 112 Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 04:30 PM																						
04:30 PM	0	0	129	0	129	0	0	0	0	0	0	55	0	0	55	0	0	0	227	227	411	
04:45 PM	0	0	92	0	92	0	0	0	0	0	0	41	0	0	41	0	0	0	206	206	339	
05:00 PM	0	0	110	0	110	0	0	0	0	0	0	34	0	0	34	0	0	0	204	204	348	
05:15 PM	0	0	112	0	112	0	0	0	0	0	0	34	0	0	34	0	0	0	205	205	351	
Total Volume	0	0	443	0	443	0	0	0	0	0	0	164	0	0	164	0	0	0	842	842	1449	
% App. Total	0	0	100	0		0	0	0	0		0	100	0	0		0	0	0	100			
PHF	.000	.000	.859	.000	.859	.000	.000	.000	.000	.000	.000	.745	.000	.000	.745	.000	.000	.000	.927	.927	.881	

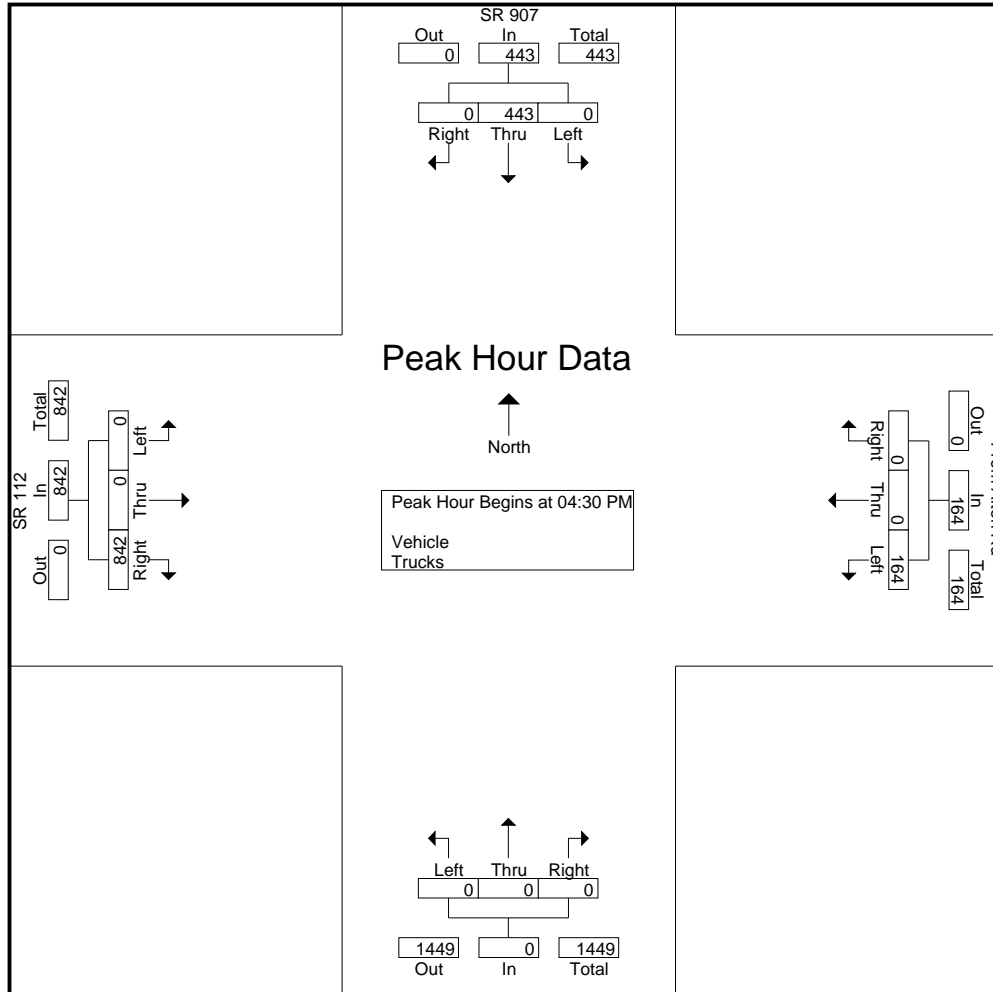
SR 907 & SR 112

File Name : TMC-19 SR 907 & SR 112

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	Alton Road Southbound			Alton Road Northbound			Barry St Westbound			Residences Driveway Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	0	0	0	0	0	0	0	1	1	0	0	0	1
07:15 AM	0	0	0	0	0	0	1	0	1	0	0	0	1
07:30 AM	0	0	0	0	0	0	2	0	2	4	1	5	7
07:45 AM	0	0	0	0	0	0	1	0	1	1	0	1	2
Total	0	0	0	0	0	0	4	1	5	5	1	6	11
08:00 AM	0	0	0	0	0	0	1	0	1	0	0	0	1
08:15 AM	0	0	0	0	0	0	1	0	1	0	0	0	1
*** BREAK ***													
Total	0	0	0	0	0	0	2	0	2	0	0	0	2
*** BREAK ***													
03:15 PM	0	0	0	0	0	0	1	1	2	0	0	0	2
*** BREAK ***													
Total	0	0	0	0	0	0	1	1	2	0	0	0	2
04:00 PM	0	0	0	0	0	0	2	1	3	0	0	0	3
04:15 PM	0	0	0	0	0	0	0	1	1	0	0	0	1
*** BREAK ***													
Total	0	0	0	0	0	0	2	2	4	0	0	0	4
05:00 PM	0	0	0	0	0	0	1	0	1	0	0	0	1
05:15 PM	0	0	0	0	0	0	0	1	1	0	0	0	1
05:30 PM	0	0	0	0	0	0	1	0	1	2	0	2	3
05:45 PM	0	0	0	0	0	0	3	0	3	0	0	0	3
Total	0	0	0	0	0	0	5	1	6	2	0	2	8
Grand Total	0	0	0	0	0	0	14	5	19	7	1	8	27
Apprch %	0	0	0	0	0	0	73.7	26.3		87.5	12.5		
Total %	0	0	0	0	0	0	51.9	18.5	70.4	25.9	3.7	29.6	

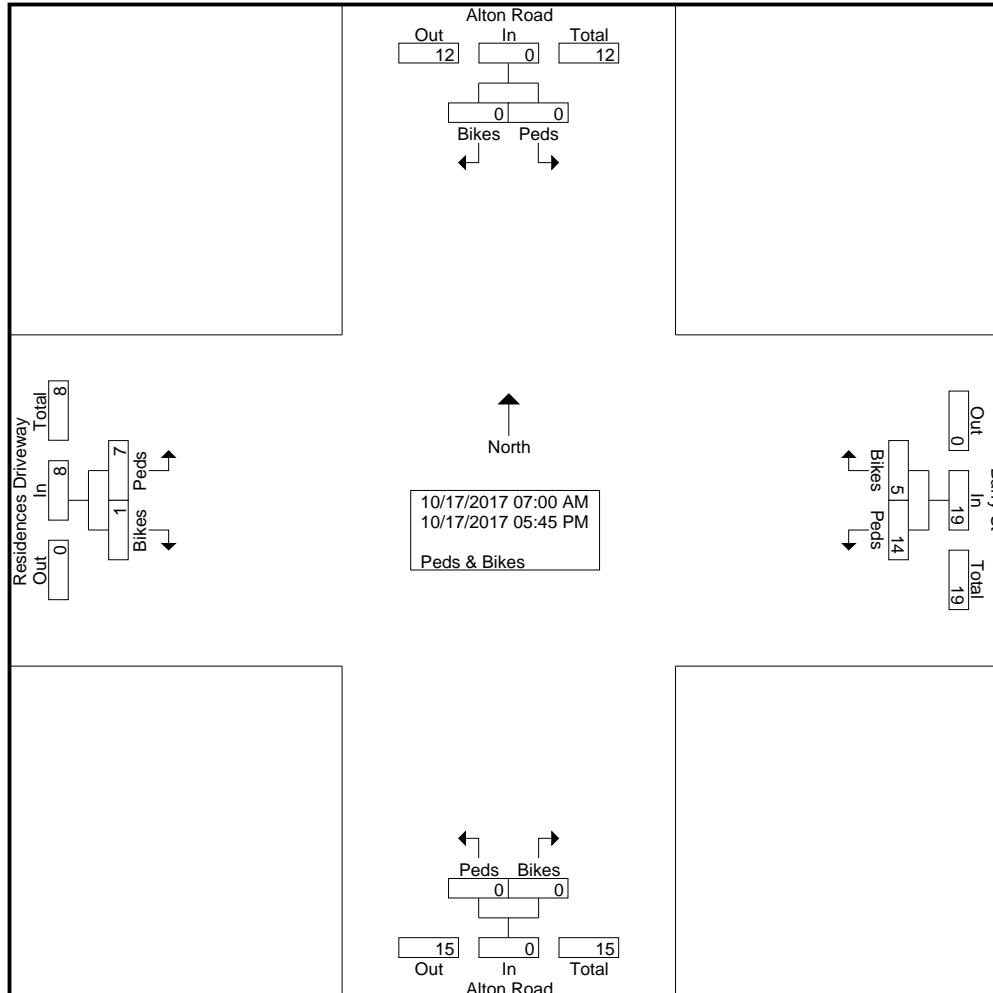
Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Alton Road Southbound			Alton Road Northbound			Barry St Westbound			Residences Driveway Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:00 AM													
07:00 AM	0	0	0	0	0	0	0	1	1	0	0	0	1
07:15 AM	0	0	0	0	0	0	1	0	1	0	0	0	1
07:30 AM	0	0	0	0	0	0	2	0	2	4	1	5	7
07:45 AM	0	0	0	0	0	0	1	0	1	1	0	1	2
Total Volume	0	0	0	0	0	0	4	1	5	5	1	6	11
% App. Total	0	0		0	0		80	20		83.3	16.7		
PHF	.000	.000	.000	.000	.000	.000	.500	.250	.625	.313	.250	.300	.393

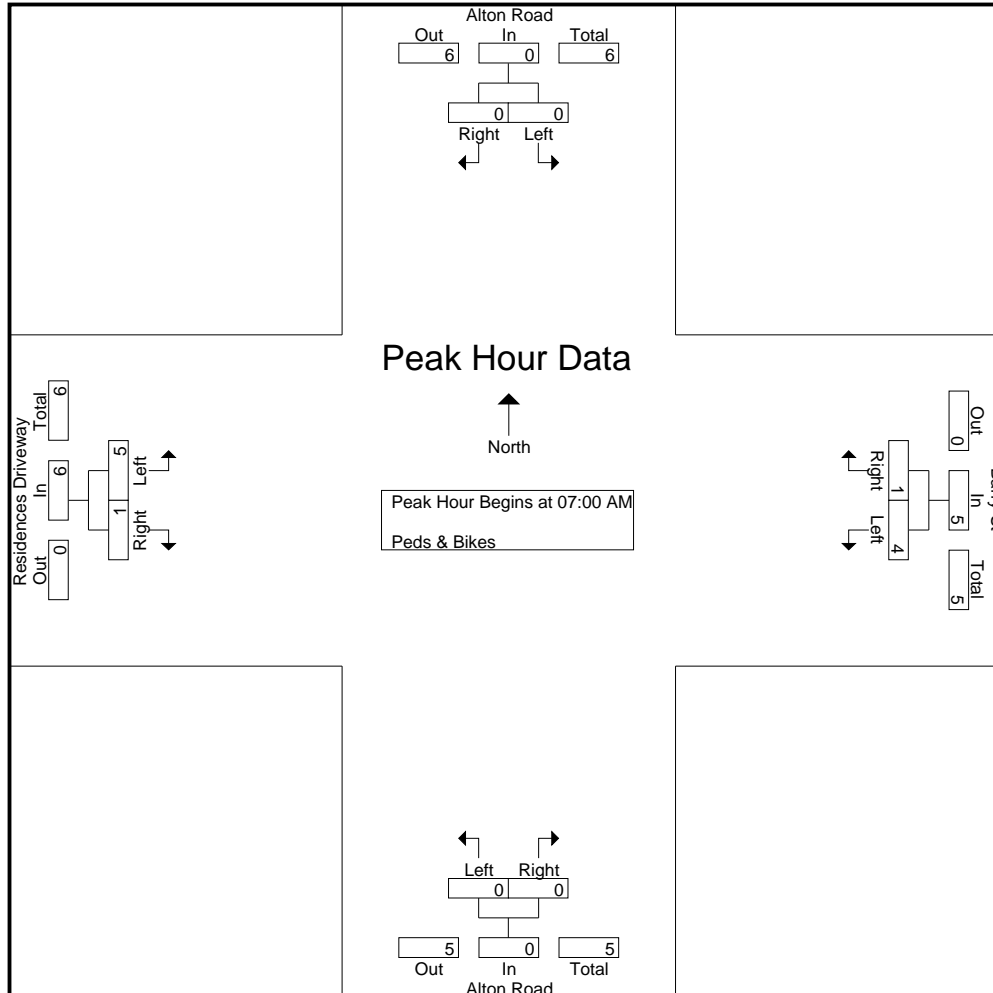
Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



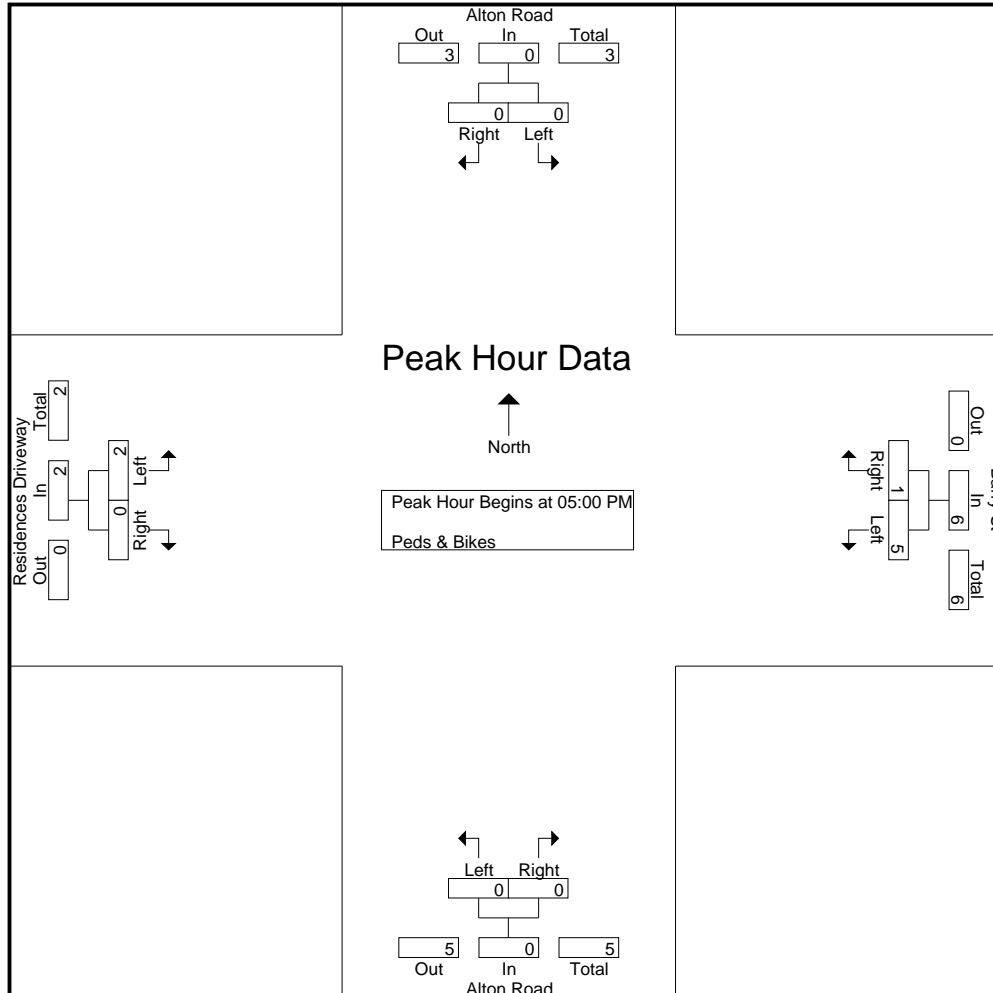
Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	Alton Road Southbound			Alton Road Northbound			Barry St Westbound			Residences Driveway Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 05:00 PM													
05:00 PM	0	0	0	0	0	0	1	0	1	0	0	0	1
05:15 PM	0	0	0	0	0	0	0	1	1	0	0	0	1
05:30 PM	0	0	0	0	0	0	1	0	1	2	0	2	3
05:45 PM	0	0	0	0	0	0	3	0	3	0	0	0	3
Total Volume	0	0	0	0	0	0	5	1	6	2	0	2	8
% App. Total	0	0		0	0		83.3	16.7		100	0		
PHF	.000	.000	.000	.000	.000	.000	.417	.250	.500	.250	.000	.250	.667

Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6



Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Trucks

Start Time	Alton Road Southbound					Alton Road Northbound					Barry St Westbound					Residences Driveway Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
07:00 AM	0	0	1	0	1	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	
07:15 AM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1	
07:30 AM	0	1	1	0	2	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	3	
07:45 AM	0	0	2	1	3	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	4	
Total	0	1	4	1	6	0	0	4	0	4	0	0	0	0	0	0	0	0	0	0	10	
*** BREAK ***																						
08:15 AM	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
08:30 AM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1	
08:45 AM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1	
Total	0	0	0	2	2	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	4	
*** BREAK ***																						
03:00 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
03:15 PM	0	0	2	0	2	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	3	
03:30 PM	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
03:45 PM	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	2	
Total	0	0	5	0	5	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	8	
04:00 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
*** BREAK ***																						
04:30 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1	
04:45 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1	
Total	0	0	1	0	1	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	3	
05:00 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1	
05:15 PM	0	0	2	0	2	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	5	
05:30 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1	
05:45 PM	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
Total	0	0	4	0	4	0	0	5	0	5	0	0	0	0	0	0	0	0	0	0	9	
Grand Total	0	1	14	3	18	0	0	16	0	16	0	0	0	0	0	0	0	0	0	0	34	
Apprch %	0	5.6	77.8	16.7		0	0	100	0		0	0	0	0		0	0	0	0			
Total %	0	2.9	41.2	8.8	52.9	0	0	47.1	0	47.1	0	0	0	0	0	0	0	0	0	0		

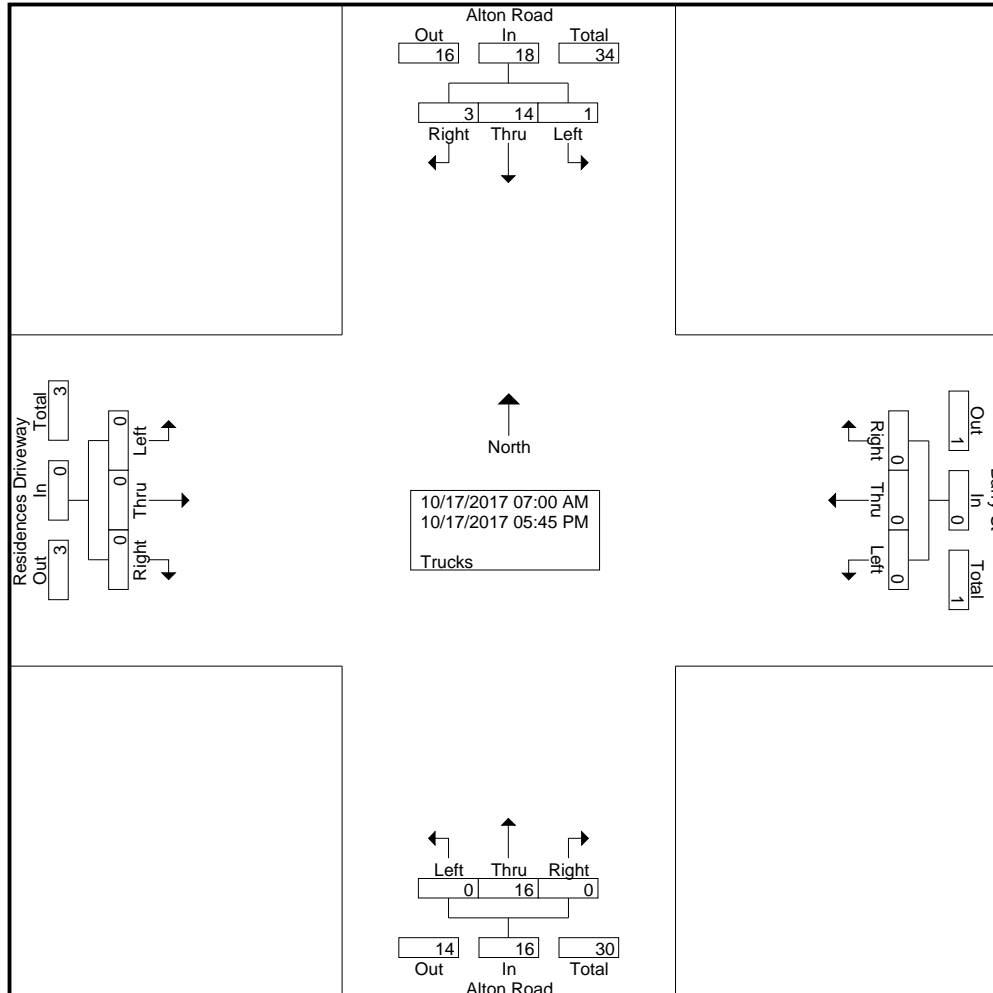
Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



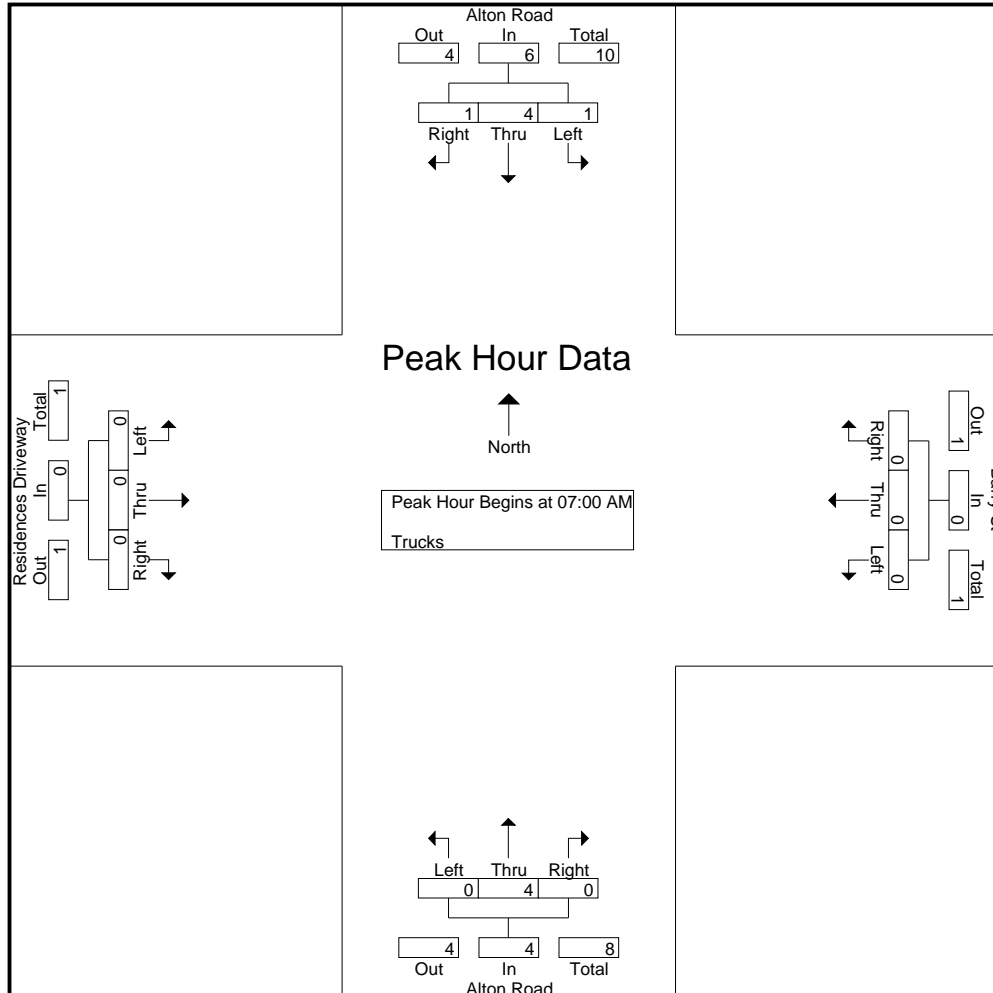
Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



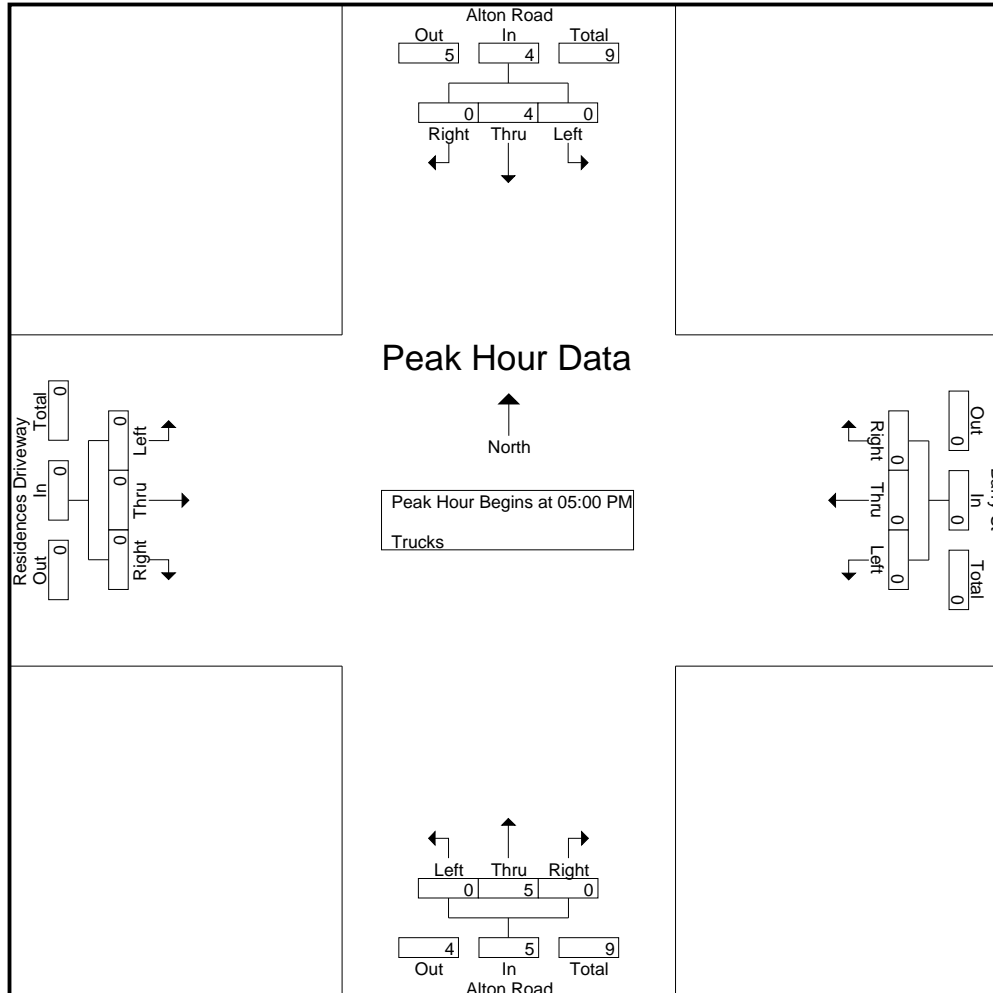
Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	Alton Road Southbound					Alton Road Northbound					Barry St Westbound					Residences Driveway Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	7	9	2	18	0	1	36	1	38	0	0	0	1	1	0	0	0	0	0	57
07:15 AM	0	28	19	4	51	1	1	29	2	33	0	0	0	0	0	0	0	0	0	0	84
07:30 AM	0	61	41	2	104	1	1	32	18	52	0	2	0	6	8	0	1	0	0	1	165
07:45 AM	0	26	58	4	88	2	5	38	7	52	0	7	0	2	9	0	2	0	1	3	152
Total	0	122	127	12	261	4	8	135	28	175	0	9	0	9	18	0	3	0	1	4	458
08:00 AM	1	4	44	5	54	0	3	40	7	50	0	4	0	0	4	0	0	0	0	0	108
08:15 AM	0	5	24	5	34	1	3	49	1	54	0	5	0	2	7	0	3	0	1	4	99
08:30 AM	1	4	43	4	52	0	2	70	2	74	0	4	0	0	4	0	1	0	1	2	132
08:45 AM	0	2	50	3	55	2	3	46	1	52	0	4	0	1	5	0	2	0	3	5	117
Total	2	15	161	17	195	3	11	205	11	230	0	17	0	3	20	0	6	0	5	11	456
*** BREAK ***																					
03:00 PM	0	14	36	2	52	1	1	71	7	80	0	3	0	3	6	0	0	0	1	1	139
03:15 PM	1	9	33	0	43	0	1	79	2	82	0	1	0	1	2	0	2	0	2	4	131
03:30 PM	0	3	29	0	32	0	1	68	0	69	0	2	0	3	5	0	2	0	1	3	109
03:45 PM	0	5	27	2	34	1	3	80	4	88	0	2	0	3	5	0	0	0	0	0	127
Total	1	31	125	4	161	2	6	298	13	319	0	8	0	10	18	0	4	0	4	8	506
04:00 PM	0	6	35	3	44	1	2	64	3	70	0	3	0	0	3	0	0	0	0	0	117
04:15 PM	0	3	35	2	40	2	1	63	3	69	0	1	0	0	1	0	2	0	0	2	112
04:30 PM	0	2	43	6	51	2	2	48	2	54	0	3	0	2	5	0	3	0	1	4	114
04:45 PM	0	3	38	2	43	0	5	68	2	75	0	2	0	4	6	0	1	0	1	2	126
Total	0	14	151	13	178	5	10	243	10	268	0	9	0	6	15	0	6	0	2	8	469
05:00 PM	0	2	32	4	38	0	1	62	0	63	0	2	0	3	5	0	1	0	0	1	107
05:15 PM	0	4	29	4	37	1	0	55	2	58	0	1	0	0	1	0	1	0	0	1	97
05:30 PM	0	3	34	2	39	3	1	77	1	82	0	1	0	0	1	0	0	0	0	0	122
05:45 PM	1	1	35	9	46	0	7	68	0	75	0	2	0	2	4	0	5	0	2	7	132
Total	1	10	130	19	160	4	9	262	3	278	0	6	0	5	11	0	7	0	2	9	458
Grand Total	4	192	694	65	955	18	44	1143	65	1270	0	49	0	33	82	0	26	0	14	40	2347
Apprch %	0.4	20.1	72.7	6.8		1.4	3.5	90	5.1		0	59.8	0	40.2		0	65	0	35		
Total %	0.2	8.2	29.6	2.8	40.7	0.8	1.9	48.7	2.8	54.1	0	2.1	0	1.4	3.5	0	1.1	0	0.6	1.7	
Vehicle	4	191	680	62	937	18	44	1127	65	1254	0	49	0	33	82	0	26	0	14	40	2313
% Vehicle	100	99.5	98	95.4	98.1	100	100	98.6	100	98.7	0	100	0	100	100	0	100	0	100	100	98.6
Trucks	0	1	14	3	18	0	0	16	0	16	0	0	0	0	0	0	0	0	0	0	34
% Trucks	0	0.5	2	4.6	1.9	0	0	1.4	0	1.3	0	0	0	0	0	0	0	0	0	0	1.4

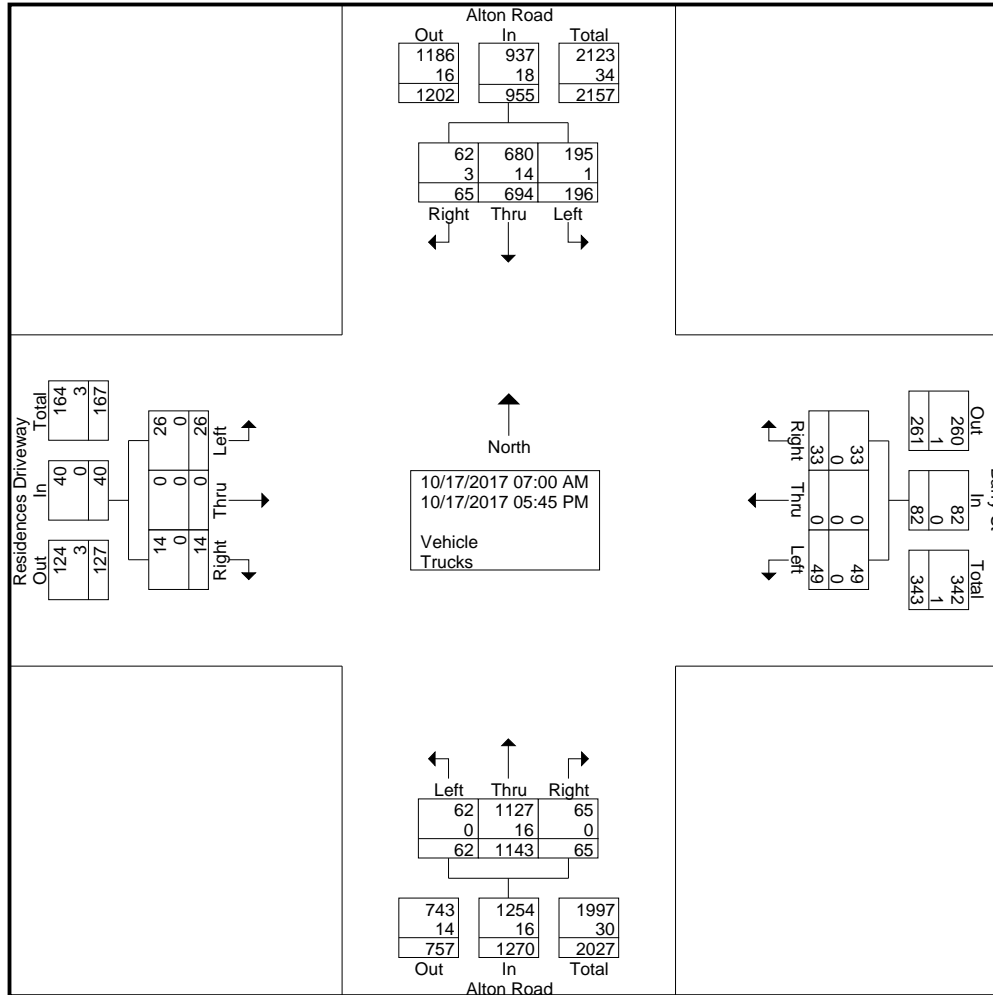
Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Alton Road Southbound					Alton Road Northbound					Barry St Westbound					Residences Driveway Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:30 AM																					
07:30 AM	0	61	41	2	104	1	1	32	18	52	0	2	0	6	8	0	1	0	0	1	165
07:45 AM	0	26	58	4	88	2	5	38	7	52	0	7	0	2	9	0	2	0	1	3	152
08:00 AM	1	4	44	5	54	0	3	40	7	50	0	4	0	0	4	0	0	0	0	0	108
08:15 AM	0	5	24	5	34	1	3	49	1	54	0	5	0	2	7	0	3	0	1	4	99
Total Volume	1	96	167	16	280	4	12	159	33	208	0	18	0	10	28	0	6	0	2	8	524
% App. Total	0.4	34.3	59.6	5.7		1.9	5.8	76.4	15.9		0	64.3	0	35.7		0	75	0	25		
PHF	.250	.393	.720	.800	.673	.500	.600	.811	.458	.963	.000	.643	.000	.417	.778	.000	.500	.000	.500	.500	.794

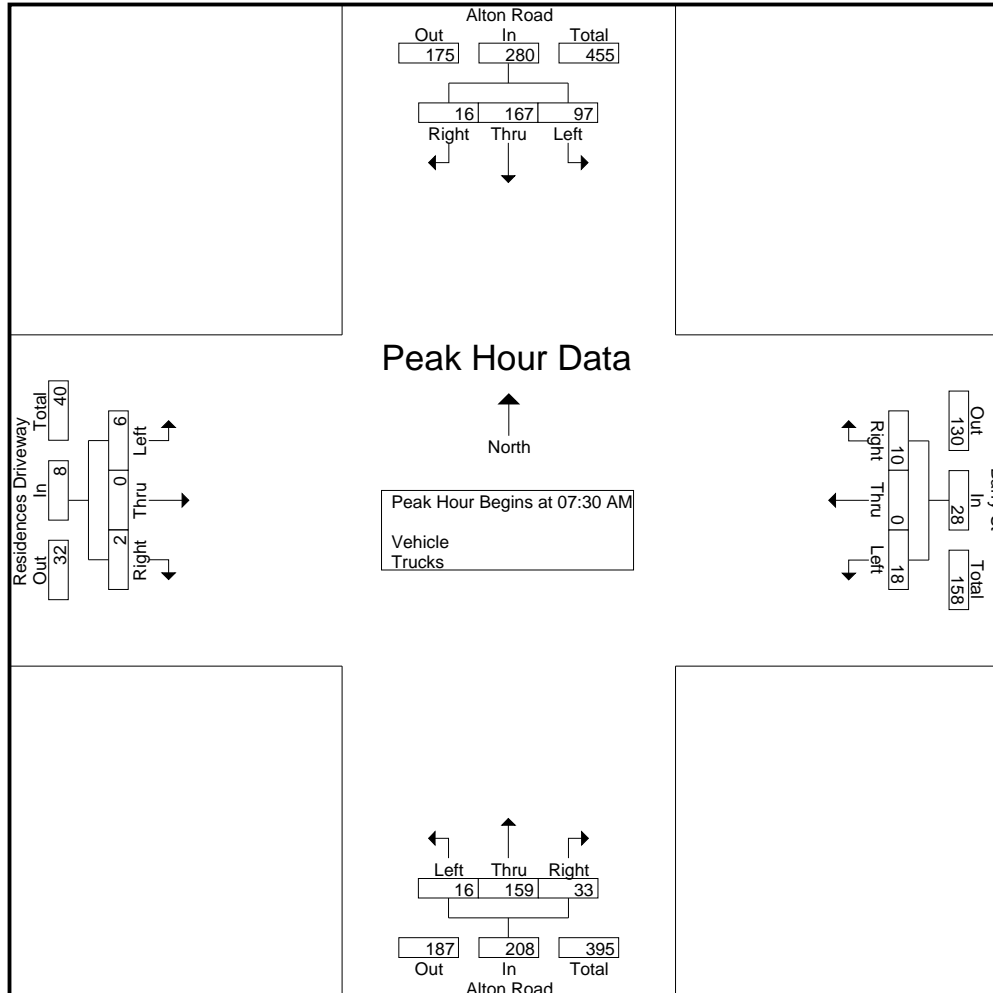
Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	Alton Road Southbound					Alton Road Northbound					Barry St Westbound					Residences Driveway Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:00 PM																					
03:00 PM	0	14	36	2	52	1	1	71	7	80	0	3	0	3	6	0	0	0	1	1	139
03:15 PM	1	9	33	0	43	0	1	79	2	82	0	1	0	1	2	0	2	0	2	4	131
03:30 PM	0	3	29	0	32	0	1	68	0	69	0	2	0	3	5	0	2	0	1	3	109
03:45 PM	0	5	27	2	34	1	3	80	4	88	0	2	0	3	5	0	0	0	0	0	127
Total Volume	1	31	125	4	161	2	6	298	13	319	0	8	0	10	18	0	4	0	4	8	506
% App. Total	0.6	19.3	77.6	2.5		0.6	1.9	93.4	4.1		0	44.4	0	55.6		0	50	0	50		
PHF	.250	.554	.868	.500	.774	.500	.500	.931	.464	.906	.000	.667	.000	.833	.750	.000	.500	.000	.500	.500	.910

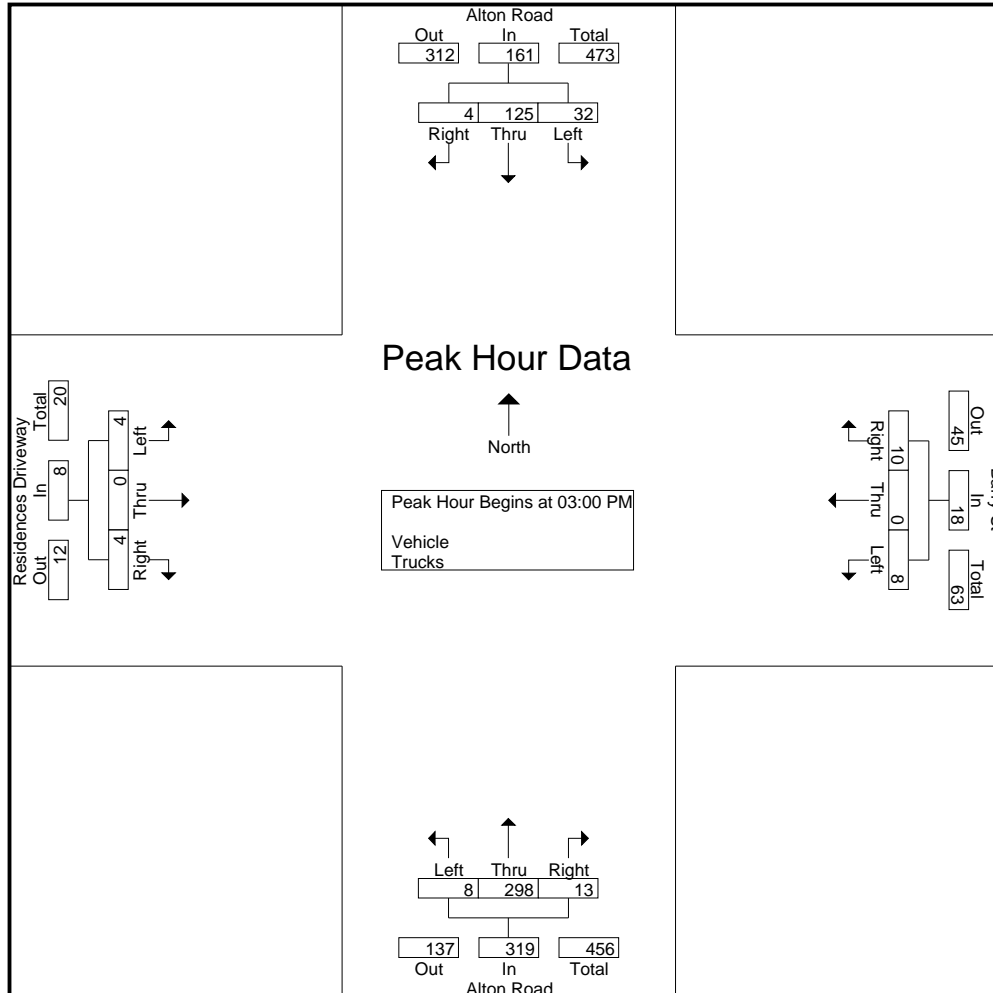
Alton Road & Barry Street

File Name : TMC-20 Alton Rd & Barry Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



Alton Road & W 39th Street

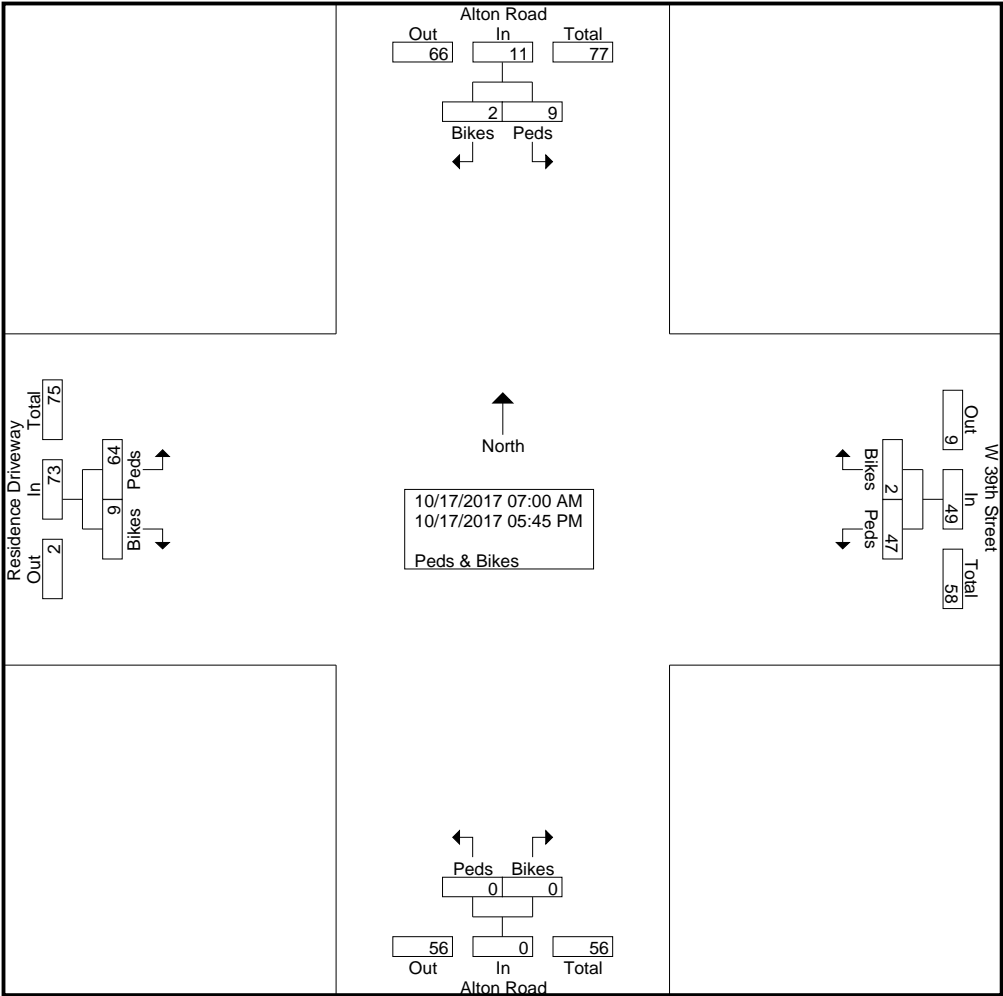
File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	Alton Road Southbound			Alton Road Northbound			W 39th Street Westbound			Residence Driveway Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	0	0	0	0	0	0	0	0	0	1	0	1	1
07:15 AM	0	0	0	0	0	0	1	0	1	0	0	0	1
07:30 AM	1	0	1	0	0	0	1	0	1	16	4	20	22
07:45 AM	2	0	2	0	0	0	5	0	5	9	0	9	16
Total	3	0	3	0	0	0	7	0	7	26	4	30	40
08:00 AM	1	1	2	0	0	0	1	1	2	4	0	4	8
08:15 AM	0	0	0	0	0	0	3	0	3	2	0	2	5
08:30 AM	0	0	0	0	0	0	1	0	1	0	0	0	1
08:45 AM	0	0	0	0	0	0	2	0	2	2	0	2	4
Total	1	1	2	0	0	0	7	1	8	8	0	8	18
*** BREAK ***													
03:00 PM	0	0	0	0	0	0	0	0	0	3	0	3	3
03:15 PM	0	0	0	0	0	0	4	1	5	1	0	1	6
03:30 PM	0	0	0	0	0	0	3	0	3	0	0	0	3
03:45 PM	0	0	0	0	0	0	1	0	1	2	0	2	3
Total	0	0	0	0	0	0	8	1	9	6	0	6	15
04:00 PM	0	0	0	0	0	0	4	0	4	0	0	0	4
04:15 PM	0	0	0	0	0	0	3	0	3	2	0	2	5
04:30 PM	1	0	1	0	0	0	1	0	1	3	0	3	5
04:45 PM	0	0	0	0	0	0	0	0	0	6	0	6	6
Total	1	0	1	0	0	0	8	0	8	11	0	11	20
05:00 PM	2	0	2	0	0	0	4	0	4	6	0	6	12
05:15 PM	1	0	1	0	0	0	5	0	5	1	0	1	7
05:30 PM	1	0	1	0	0	0	6	0	6	3	1	4	11
05:45 PM	0	1	1	0	0	0	2	0	2	3	4	7	10
Total	4	1	5	0	0	0	17	0	17	13	5	18	40
Grand Total	9	2	11	0	0	0	47	2	49	64	9	73	133
Apprch %	81.8	18.2		0	0		95.9	4.1		87.7	12.3		
Total %	6.8	1.5	8.3	0	0	0	35.3	1.5	36.8	48.1	6.8	54.9	

Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 2



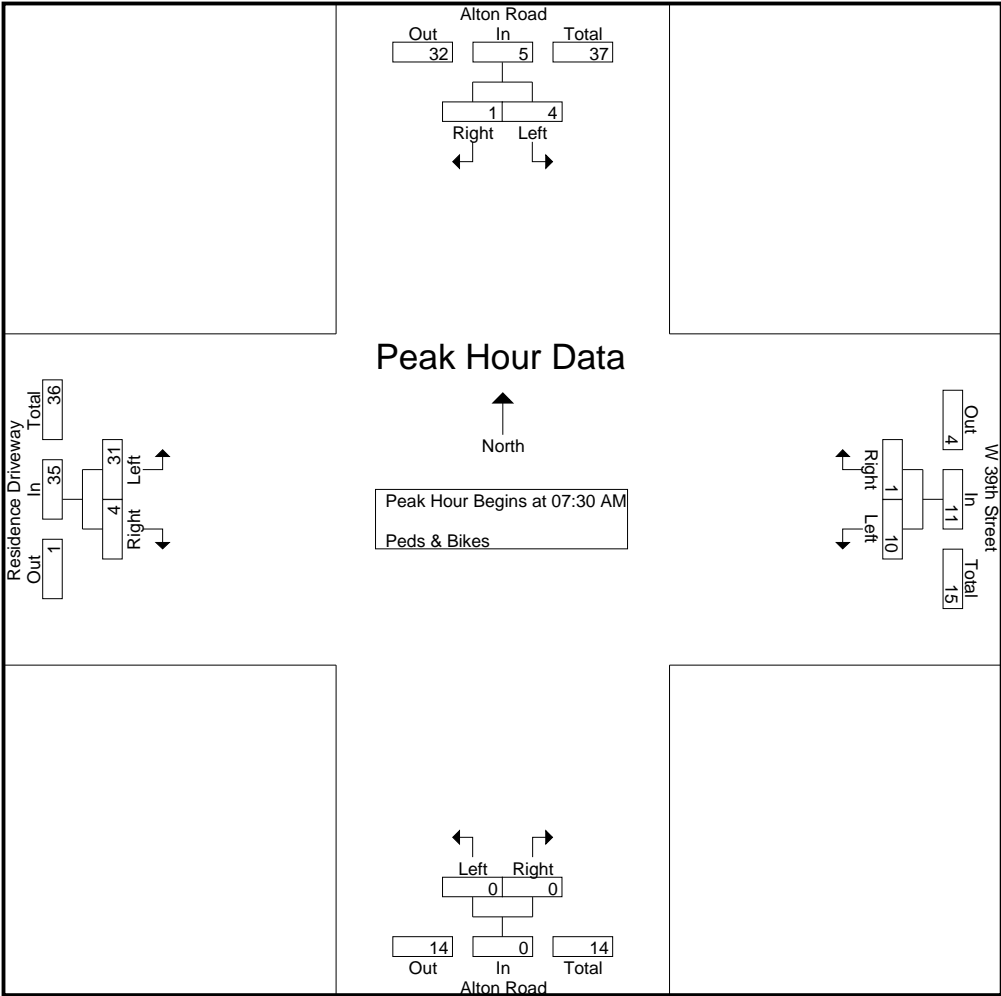
Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Alton Road Southbound			Alton Road Northbound			W 39th Street Westbound			Residence Driveway Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:30 AM													
07:30 AM	1	0	1	0	0	0	1	0	1	16	4	20	22
07:45 AM	2	0	2	0	0	0	5	0	5	9	0	9	16
08:00 AM	1	1	2	0	0	0	1	1	2	4	0	4	8
08:15 AM	0	0	0	0	0	0	3	0	3	2	0	2	5
Total Volume	4	1	5	0	0	0	10	1	11	31	4	35	51
% App. Total	80	20		0	0		90.9	9.1		88.6	11.4		
PHF	.500	.250	.625	.000	.000	.000	.500	.250	.550	.484	.250	.438	.580

Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4



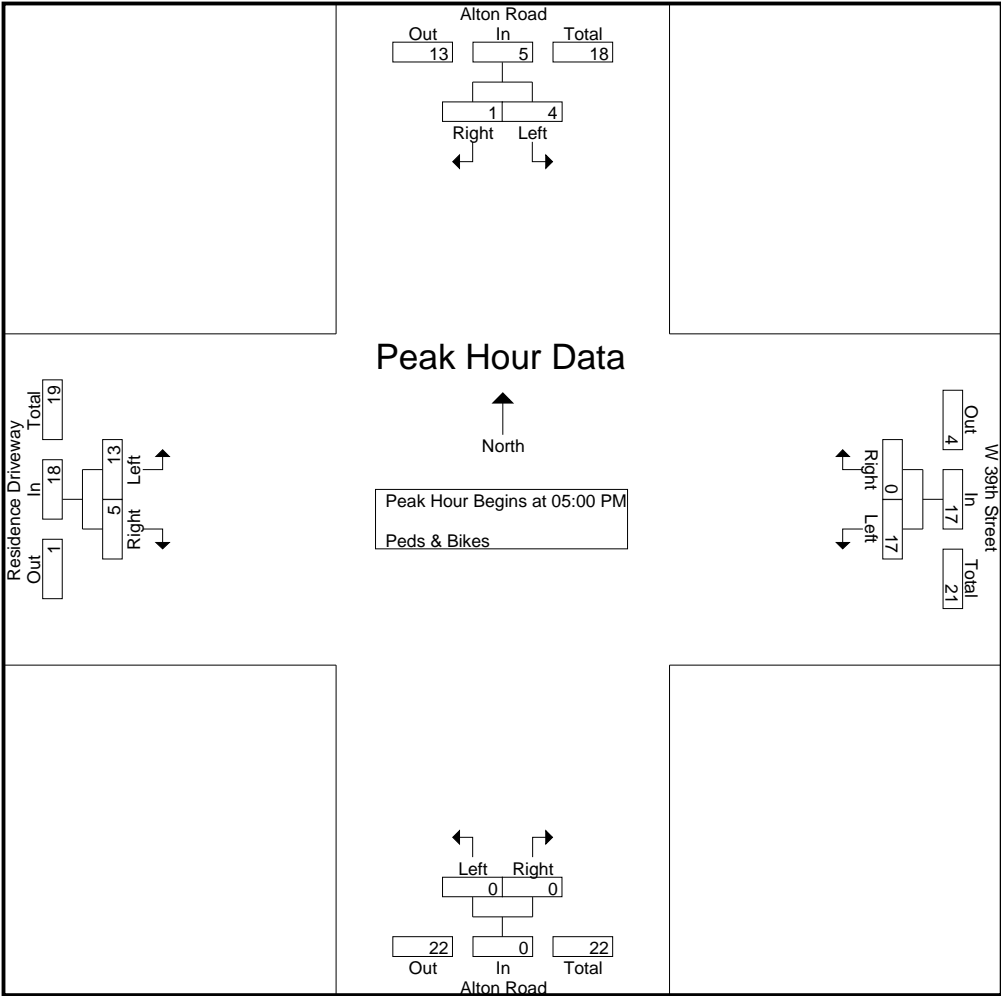
Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	Alton Road Southbound			Alton Road Northbound			W 39th Street Westbound			Residence Driveway Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 05:00 PM													
05:00 PM	2	0	2	0	0	0	4	0	4	6	0	6	12
05:15 PM	1	0	1	0	0	0	5	0	5	1	0	1	7
05:30 PM	1	0	1	0	0	0	6	0	6	3	1	4	11
05:45 PM	0	1	1	0	0	0	2	0	2	3	4	7	10
Total Volume	4	1	5	0	0	0	17	0	17	13	5	18	40
% App. Total	80	20		0	0		100	0		72.2	27.8		
PHF	.500	.250	.625	.000	.000	.000	.708	.000	.708	.542	.313	.643	.833

Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6



Alton Road & W 39th Street

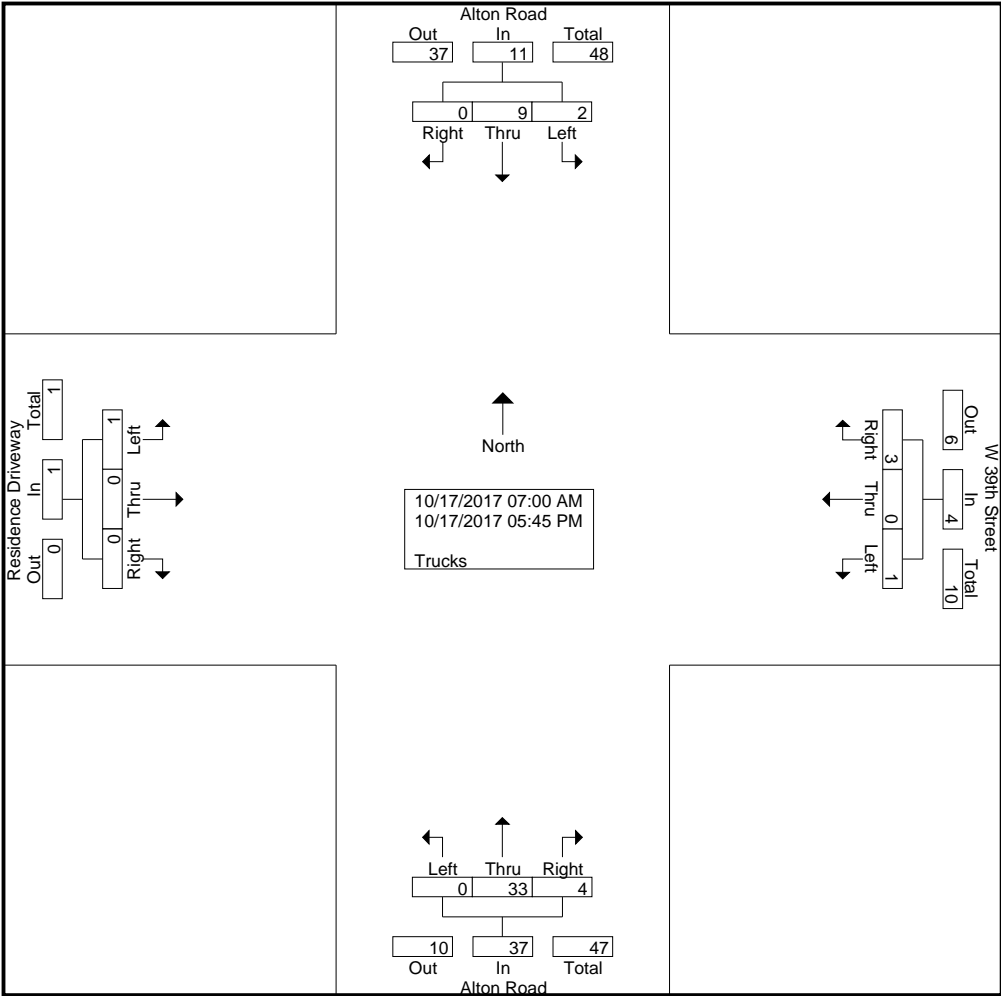
File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Trucks

Start Time	Alton Road Southbound					Alton Road Northbound					W 39th Street Westbound					Residence Driveway Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0
07:15 AM	0	0	1	0	1	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0
07:30 AM	0	0	1	0	1	0	0	2	1	3	0	0	0	0	0	0	0	0	0	0	0
07:45 AM	0	0	1	0	1	0	0	4	0	4	0	0	0	0	0	0	0	0	0	0	0
Total	0	0	3	0	3	0	0	9	1	10	0	0	0	0	0	0	0	0	0	0	13
*** BREAK ***																					
08:15 AM	0	1	0	0	1	0	0	2	1	3	0	0	0	0	0	0	1	0	0	1	5
08:30 AM	0	0	1	0	1	0	0	4	1	5	0	0	0	0	0	0	0	0	0	0	6
08:45 AM	0	0	0	0	0	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	3
Total	0	1	1	0	2	0	0	9	2	11	0	0	0	0	0	0	1	0	0	1	14
*** BREAK ***																					
03:00 PM	0	0	0	0	0	0	0	1	0	1	0	1	0	0	1	0	0	0	0	0	2
03:15 PM	0	0	1	0	1	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	3
03:30 PM	0	1	1	0	2	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	4
03:45 PM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	1
Total	0	1	2	0	3	0	0	5	1	6	0	1	0	0	1	0	0	0	0	0	10
04:00 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1
04:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	1
04:30 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1
04:45 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	1	1	0	0	0	0	0	2
Total	0	0	0	0	0	0	0	3	0	3	0	0	0	2	2	0	0	0	0	0	5
05:00 PM	0	0	2	0	2	0	0	1	0	1	0	0	0	1	1	0	0	0	0	0	4
05:15 PM	0	0	0	0	0	0	0	4	0	4	0	0	0	0	0	0	0	0	0	0	4
05:30 PM	0	0	1	0	1	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2
05:45 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1
Total	0	0	3	0	3	0	0	7	0	7	0	0	0	1	1	0	0	0	0	0	11
Grand Total	0	2	9	0	11	0	0	33	4	37	0	1	0	3	4	0	1	0	0	1	53
Apprch %	0	18.2	81.8	0		0	0	89.2	10.8		0	25	0	75		0	100	0	0		
Total %	0	3.8	17	0	20.8	0	0	62.3	7.5	69.8	0	1.9	0	5.7	7.5	0	1.9	0	0	1.9	

Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 2



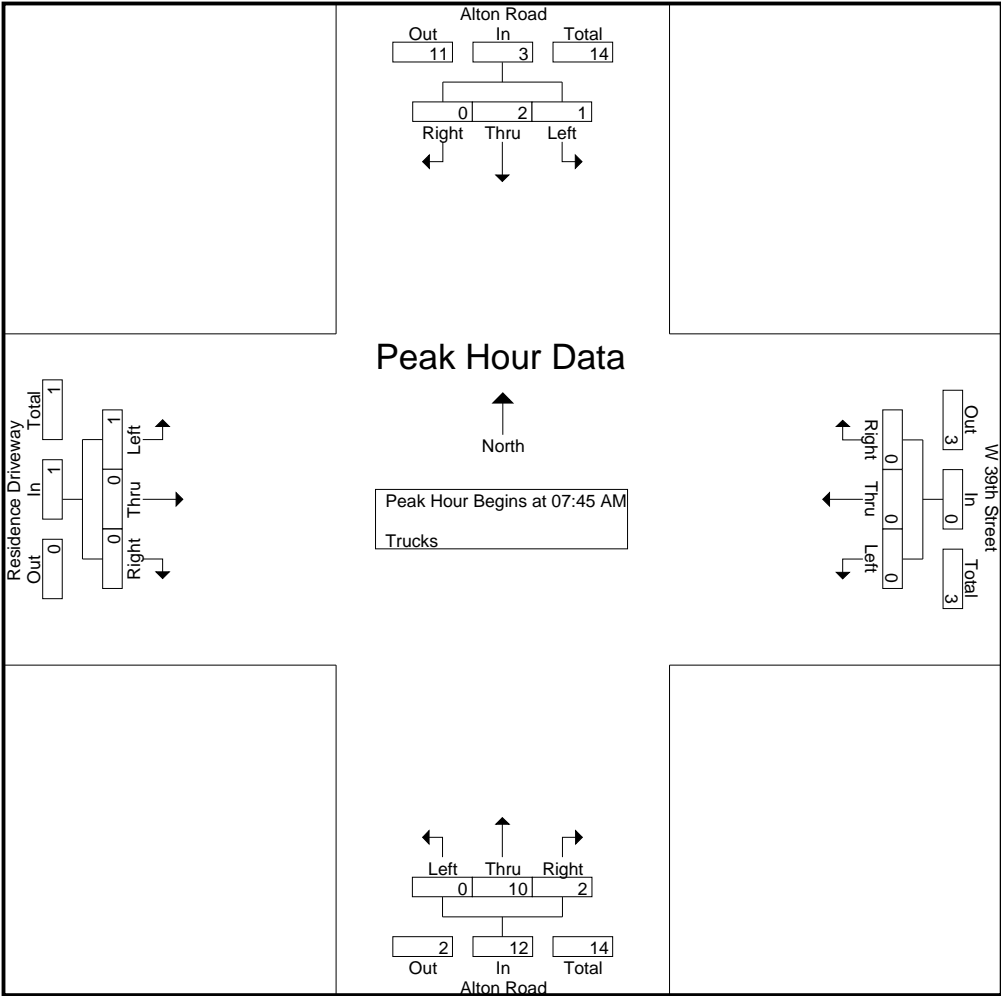
Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Alton Road Southbound					Alton Road Northbound					W 39th Street Westbound					Residence Driveway Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 07:45 AM																						
07:45 AM	0	0	1	0	1	0	0	4	0	4	0	0	0	0	0	0	0	0	0	0	0	5
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15 AM	0	1	0	0	1	0	0	2	1	3	0	0	0	0	0	0	1	0	0	0	1	5
08:30 AM	0	0	1	0	1	0	0	4	1	5	0	0	0	0	0	0	0	0	0	0	0	6
Total Volume	0	1	2	0	3	0	0	10	2	12	0	0	0	0	0	0	1	0	0	0	1	16
% App. Total	0	33.3	66.7	0		0	0	83.3	16.7		0	0	0	0		0	100	0	0			
PHF	.000	.250	.500	.000	.750	.000	.000	.625	.500	.600	.000	.000	.000	.000	.000	.000	.250	.000	.000	.000	.250	.667

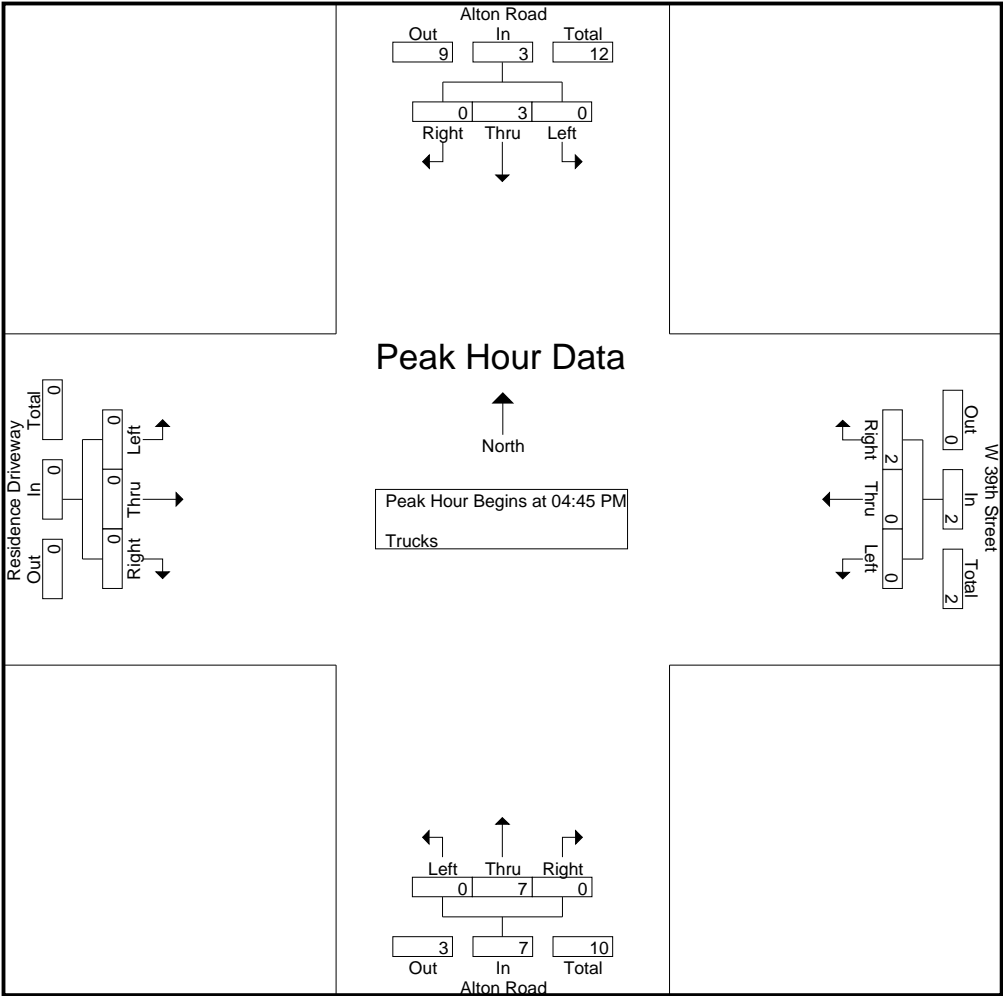
Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4



Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6



Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	Alton Road Southbound					Alton Road Northbound					W 39th Street Westbound					Residence Driveway Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	5	18	0	23	0	0	32	7	39	0	1	0	2	3	0	1	0	0	1	66
07:15 AM	0	7	50	0	57	0	0	25	3	28	0	7	0	11	18	0	0	0	0	0	103
07:30 AM	1	15	66	0	82	0	0	29	6	35	0	19	0	29	48	0	1	0	0	1	166
07:45 AM	1	14	57	0	72	0	0	27	6	33	0	26	0	29	55	0	5	4	2	11	171
Total	2	41	191	0	234	0	0	113	22	135	0	53	0	71	124	0	7	4	2	13	506
08:00 AM	4	9	28	0	41	0	0	38	3	41	0	16	0	19	35	0	3	0	2	5	122
08:15 AM	1	7	20	0	28	0	0	41	9	50	0	7	0	7	14	0	4	2	2	8	100
08:30 AM	1	15	26	0	42	0	0	47	24	71	0	16	0	17	33	0	1	0	1	2	148
08:45 AM	1	12	42	0	55	0	0	41	11	52	0	13	0	13	26	0	3	0	1	4	137
Total	7	43	116	0	166	0	0	167	47	214	0	52	0	56	108	0	11	2	6	19	507
*** BREAK ***																					
03:00 PM	1	20	36	0	57	0	0	56	20	76	1	12	0	25	38	0	1	0	2	3	174
03:15 PM	0	14	25	0	39	1	0	62	26	89	0	17	0	39	56	0	0	0	1	1	185
03:30 PM	3	6	25	0	34	0	0	63	18	81	0	7	0	18	25	0	3	0	0	3	143
03:45 PM	0	10	26	0	36	2	0	63	20	85	0	6	0	13	19	0	0	0	0	0	140
Total	4	50	112	0	166	3	0	244	84	331	1	42	0	95	138	0	4	0	3	7	642
04:00 PM	2	6	32	0	40	0	0	63	10	73	0	9	0	13	22	0	1	1	1	3	138
04:15 PM	1	19	32	0	52	0	0	55	13	68	0	9	0	17	26	0	0	0	1	1	147
04:30 PM	0	13	40	0	53	0	0	61	8	69	1	17	0	15	33	0	0	0	0	0	155
04:45 PM	0	10	26	0	36	0	0	73	13	86	0	15	0	23	38	0	1	2	0	3	163
Total	3	48	130	0	181	0	0	252	44	296	1	50	0	68	119	0	2	3	2	7	603
05:00 PM	3	13	37	0	53	0	0	58	14	72	1	1	0	16	18	0	2	0	2	4	147
05:15 PM	2	12	31	0	45	0	0	50	11	61	1	9	0	16	26	0	1	0	1	2	134
05:30 PM	0	14	31	0	45	0	0	61	16	77	0	8	0	17	25	0	0	1	0	1	148
05:45 PM	4	10	34	0	48	0	0	63	16	79	1	7	0	9	17	0	3	3	2	8	152
Total	9	49	133	0	191	0	0	232	57	289	3	25	0	58	86	0	6	4	5	15	581
Grand Total	25	231	682	0	938	3	0	1008	254	1265	5	222	0	348	575	0	30	13	18	61	2839
Apprch %	2.7	24.6	72.7	0		0.2	0	79.7	20.1		0.9	38.6	0	60.5		0	49.2	21.3	29.5		
Total %	0.9	8.1	24	0	33	0.1	0	35.5	8.9	44.6	0.2	7.8	0	12.3	20.3	0	1.1	0.5	0.6	2.1	
Vehicle	25	229	673	0	927	3	0	975	250	1228	5	221	0	345	571	0	29	13	18	60	2786
% Vehicle	100	99.1	98.7	0	98.8	100	0	96.7	98.4	97.1	100	99.5	0	99.1	99.3	0	96.7	100	100	98.4	98.1

Alton Road & W 39th Street

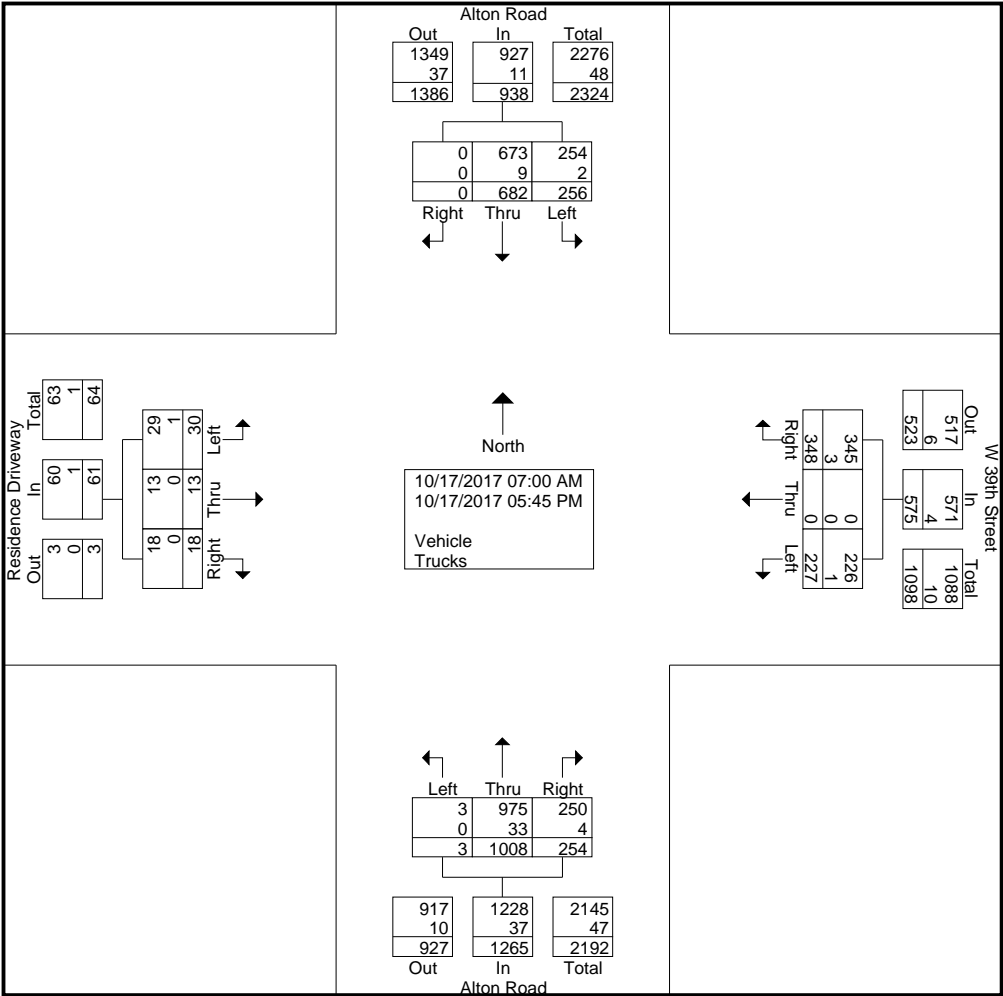
File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	Alton Road Southbound					Alton Road Northbound					W 39th Street Westbound					Residence Driveway Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	2	9	0	11	0	0	33	4	37	0	1	0	3	4	0	1	0	0	1	53
% Trucks	0	0.9	1.3	0	1.2	0	0	3.3	1.6	2.9	0	0.5	0	0.9	0.7	0	3.3	0	0	1.6	1.9

Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3



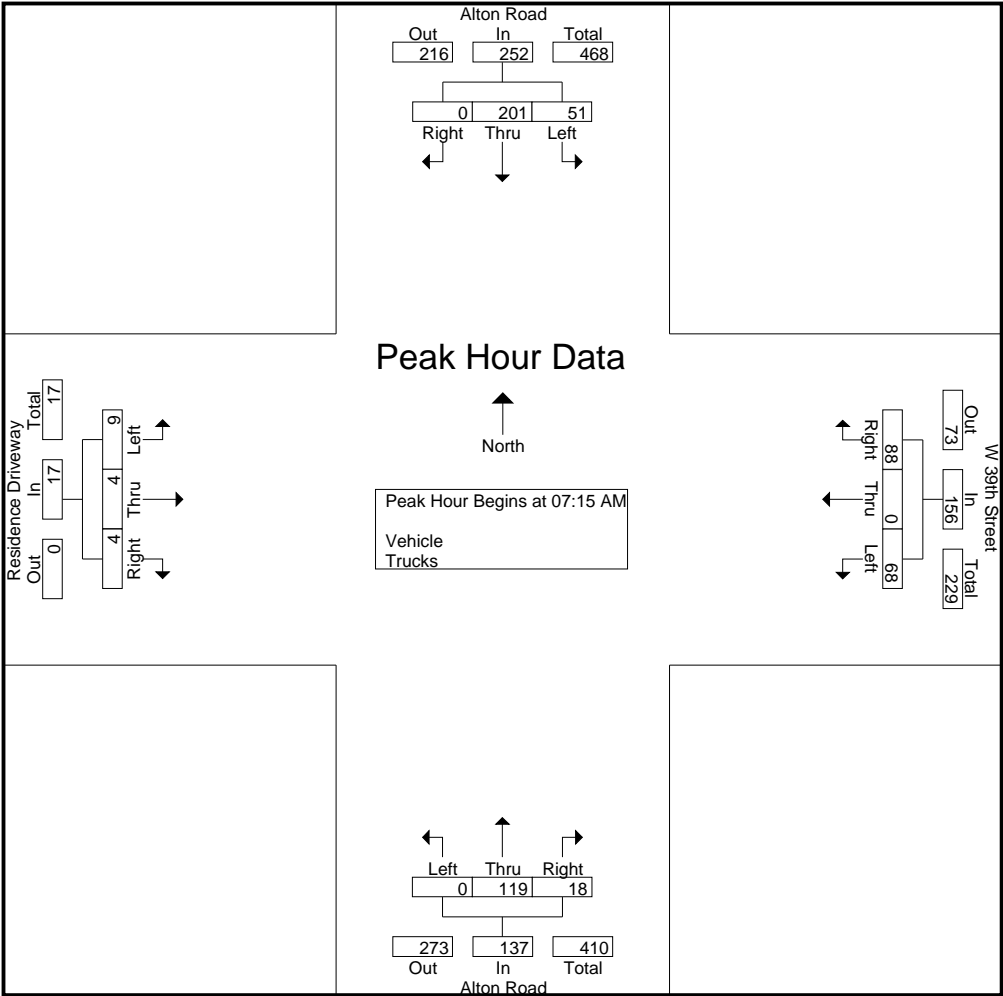
Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4

Start Time	Alton Road Southbound					Alton Road Northbound					W 39th Street Westbound					Residence Driveway Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 07:15 AM																						
07:15 AM	0	7	50	0	57	0	0	25	3	28	0	7	0	11	18	0	0	0	0	0	103	
07:30 AM	1	15	66	0	82	0	0	29	6	35	0	19	0	29	48	0	1	0	0	1	166	
07:45 AM	1	14	57	0	72	0	0	27	6	33	0	26	0	29	55	0	5	4	2	11	171	
08:00 AM	4	9	28	0	41	0	0	38	3	41	0	16	0	19	35	0	3	0	2	5	122	
Total Volume	6	45	201	0	252	0	0	119	18	137	0	68	0	88	156	0	9	4	4	17	562	
% App. Total	2.4	17.9	79.8	0		0	0	86.9	13.1		0	43.6	0	56.4		0	52.9	23.5	23.5			
PHF	.375	.750	.761	.000	.768	.000	.000	.783	.750	.835	.000	.654	.000	.759	.709	.000	.450	.250	.500	.386	.822	

Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5



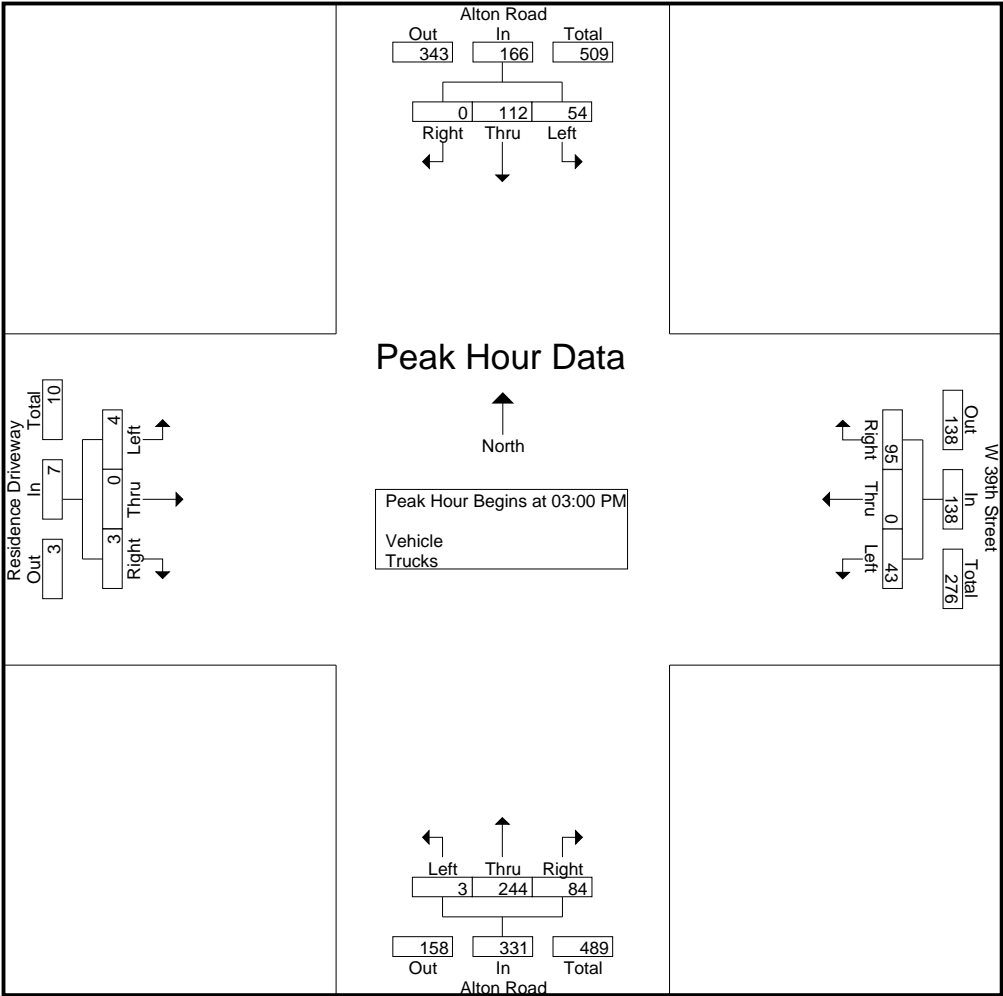
Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6

Start Time	Alton Road Southbound					Alton Road Northbound					W 39th Street Westbound					Residence Driveway Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:00 PM																					
03:00 PM	1	20	36	0	57	0	0	56	20	76	1	12	0	25	38	0	1	0	2	3	174
03:15 PM	0	14	25	0	39	1	0	62	26	89	0	17	0	39	56	0	0	0	1	1	185
03:30 PM	3	6	25	0	34	0	0	63	18	81	0	7	0	18	25	0	3	0	0	3	143
03:45 PM	0	10	26	0	36	2	0	63	20	85	0	6	0	13	19	0	0	0	0	0	140
Total Volume	4	50	112	0	166	3	0	244	84	331	1	42	0	95	138	0	4	0	3	7	642
% App. Total	2.4	30.1	67.5	0		0.9	0	73.7	25.4		0.7	30.4	0	68.8		0	57.1	0	42.9		
PHF	.333	.625	.778	.000	.728	.375	.000	.968	.808	.930	.250	.618	.000	.609	.616	.000	.333	.000	.375	.583	.868

Alton Road & W 39th Street

File Name : TMC-21 Alton Rd & W 39th Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 7



Alton Road & 41st Street

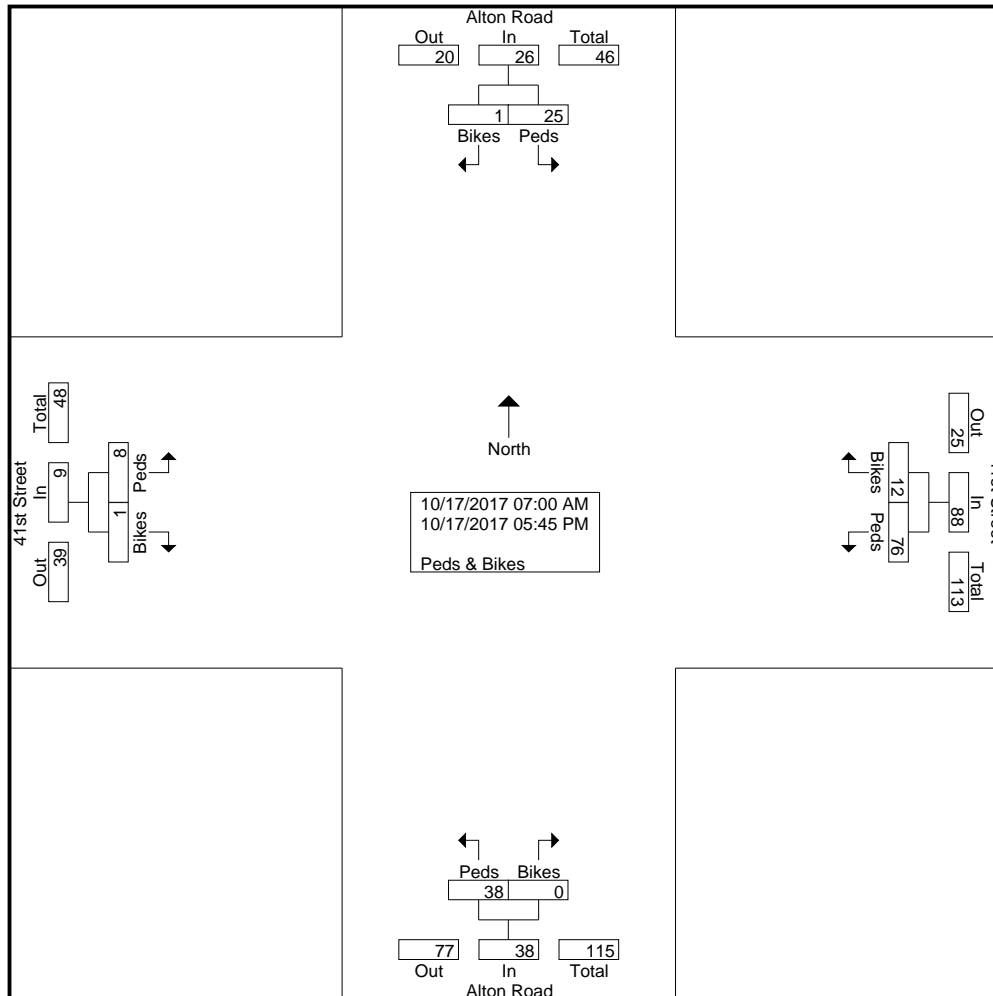
File Name : TMC-22 Alton Rd & 41st Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	Alton Road Southbound			Alton Road Northbound			41st Street Westbound			41st Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	1	0	1	2	0	2	6	0	6	0	0	0	9
07:15 AM	0	0	0	0	0	0	2	0	2	0	0	0	2
07:30 AM	2	0	2	8	0	8	6	2	8	1	1	2	20
07:45 AM	0	0	0	6	0	6	5	0	5	0	0	0	11
Total	3	0	3	16	0	16	19	2	21	1	1	2	42
08:00 AM	2	0	2	0	0	0	5	0	5	0	0	0	7
08:15 AM	2	0	2	1	0	1	4	0	4	2	0	2	9
08:30 AM	2	1	3	0	0	0	2	1	3	0	0	0	6
08:45 AM	0	0	0	3	0	3	10	0	10	1	0	1	14
Total	6	1	7	4	0	4	21	1	22	3	0	3	36
*** BREAK ***													
03:00 PM	0	0	0	0	0	0	2	0	2	1	0	1	3
03:15 PM	0	0	0	1	0	1	0	1	1	0	0	0	2
03:30 PM	4	0	4	0	0	0	2	0	2	0	0	0	6
03:45 PM	1	0	1	0	0	0	5	0	5	1	0	1	7
Total	5	0	5	1	0	1	9	1	10	2	0	2	18
04:00 PM	0	0	0	0	0	0	8	0	8	0	0	0	8
04:15 PM	5	0	5	1	0	1	5	0	5	1	0	1	12
04:30 PM	0	0	0	0	0	0	1	0	1	1	0	1	2
04:45 PM	2	0	2	2	0	2	4	0	4	0	0	0	8
Total	7	0	7	3	0	3	18	0	18	2	0	2	30
05:00 PM	1	0	1	6	0	6	3	1	4	0	0	0	11
05:15 PM	2	0	2	1	0	1	1	3	4	0	0	0	7
05:30 PM	1	0	1	2	0	2	2	0	2	0	0	0	5
05:45 PM	0	0	0	5	0	5	3	4	7	0	0	0	12
Total	4	0	4	14	0	14	9	8	17	0	0	0	35
Grand Total	25	1	26	38	0	38	76	12	88	8	1	9	161
Apprch %	96.2	3.8		100	0		86.4	13.6		88.9	11.1		
Total %	15.5	0.6	16.1	23.6	0	23.6	47.2	7.5	54.7	5	0.6	5.6	

Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 2



Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Alton Road Southbound			Alton Road Northbound			41st Street Westbound			41st Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:30 AM													
07:30 AM	2	0	2	8	0	8	6	2	8	1	1	2	20
07:45 AM	0	0	0	6	0	6	5	0	5	0	0	0	11
08:00 AM	2	0	2	0	0	0	5	0	5	0	0	0	7
08:15 AM	2	0	2	1	0	1	4	0	4	2	0	2	9
Total Volume	6	0	6	15	0	15	20	2	22	3	1	4	47
% App. Total	100	0		100	0		90.9	9.1		75	25		
PHF	.750	.000	.750	.469	.000	.469	.833	.250	.688	.375	.250	.500	.588

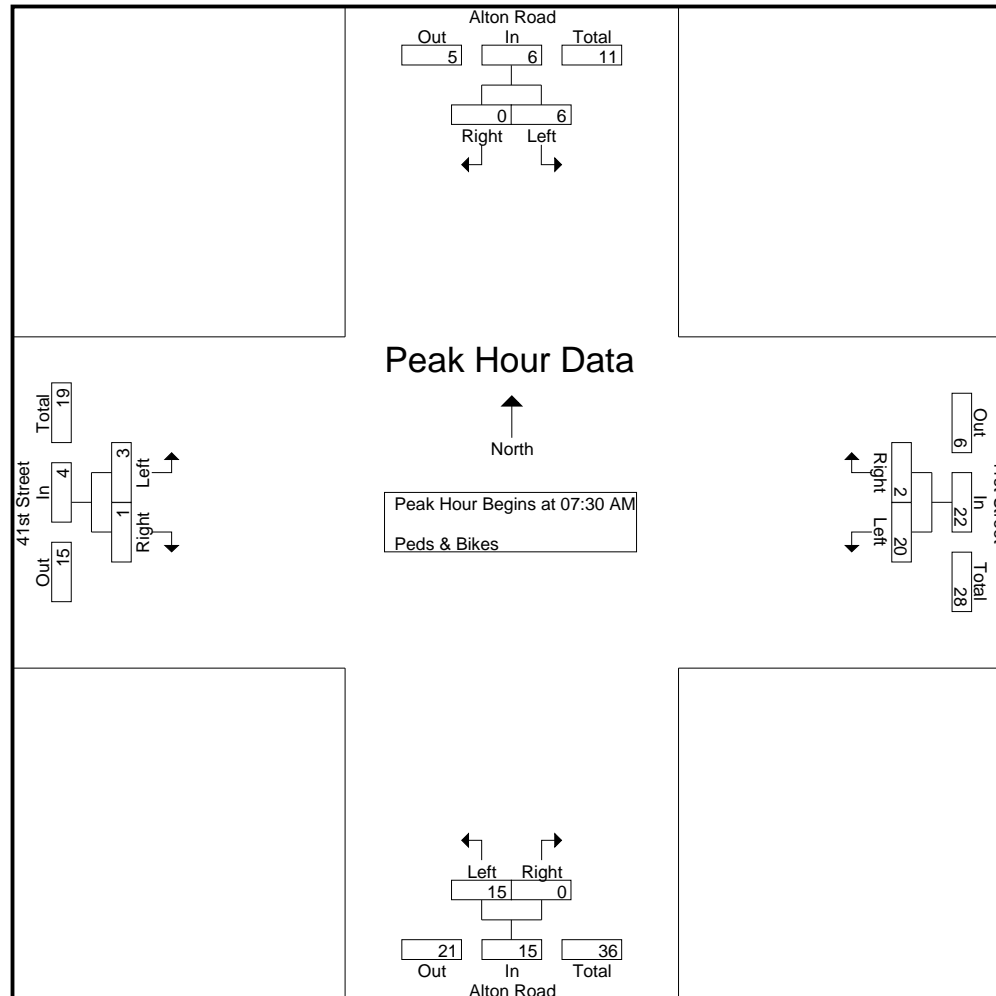
Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	Alton Road Southbound			Alton Road Northbound			41st Street Westbound			41st Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 05:00 PM													
05:00 PM	1	0	1	6	0	6	3	1	4	0	0	0	11
05:15 PM	2	0	2	1	0	1	1	3	4	0	0	0	7
05:30 PM	1	0	1	2	0	2	2	0	2	0	0	0	5
05:45 PM	0	0	0	5	0	5	3	4	7	0	0	0	12
Total Volume	4	0	4	14	0	14	9	8	17	0	0	0	35
% App. Total	100	0		100	0		52.9	47.1		0	0		
PHF	.500	.000	.500	.583	.000	.583	.750	.500	.607	.000	.000	.000	.729

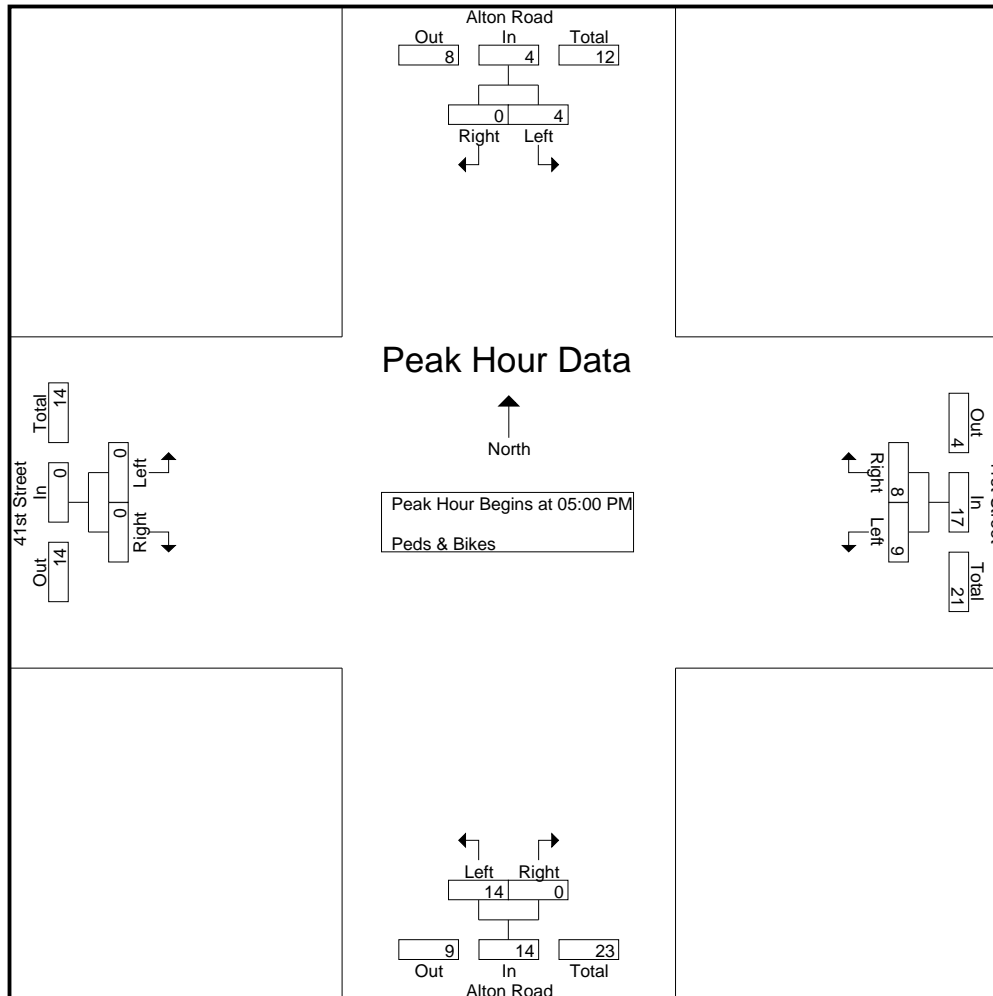
Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Trucks

Start Time	Alton Road Southbound					Alton Road Northbound					41st Street Westbound					41st Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	1	0	0	1	0	0	0	1	1	0	0	2	1	3	0	1	2	0	3	8
07:15 AM	0	0	0	0	0	0	0	1	0	1	0	0	2	0	2	0	1	9	0	10	13
07:30 AM	0	0	0	0	0	0	1	0	0	1	0	0	3	1	4	0	1	2	0	3	8
07:45 AM	0	0	2	0	2	0	1	0	0	1	0	0	3	1	4	0	2	10	0	12	19
Total	0	1	2	0	3	0	2	1	1	4	0	0	10	3	13	0	5	23	0	28	48
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	2	1	3	0	1	5	0	6	9
08:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	4	3	7	0	0	10	0	10	17
08:30 AM	0	0	0	0	0	0	0	4	1	5	0	0	2	0	2	0	3	7	0	10	17
08:45 AM	0	0	0	1	1	0	0	1	0	1	0	0	5	1	6	0	4	3	0	7	15
Total	0	0	0	1	1	0	0	5	1	6	0	0	13	5	18	0	8	25	0	33	58
*** BREAK ***																					
03:00 PM	0	0	0	0	0	0	1	0	1	2	0	0	2	1	3	0	0	2	1	3	8
03:15 PM	0	0	0	0	0	0	0	1	0	1	0	1	1	0	2	0	1	1	0	2	5
03:30 PM	0	0	1	0	1	0	0	1	1	2	0	0	10	0	10	0	0	4	0	4	17
03:45 PM	0	1	0	0	1	0	0	1	1	2	0	0	2	0	2	0	0	6	0	6	11
Total	0	1	1	0	2	0	1	3	3	7	0	1	15	1	17	0	1	13	1	15	41
04:00 PM	0	0	1	1	2	0	0	0	0	0	0	0	2	0	2	0	0	4	0	4	8
04:15 PM	0	1	0	1	2	0	0	0	0	0	0	0	5	1	6	0	0	3	0	3	11
04:30 PM	0	1	0	1	2	0	1	1	0	2	0	0	3	0	3	0	0	3	0	3	10
04:45 PM	0	1	0	0	1	0	0	1	1	2	0	0	4	0	4	0	2	8	0	10	17
Total	0	3	1	3	7	0	1	2	1	4	0	0	14	1	15	0	2	18	0	20	46
05:00 PM	0	1	1	1	3	0	0	1	0	1	0	0	6	1	7	0	0	2	0	2	13
05:15 PM	0	3	0	0	3	0	0	1	0	1	0	0	5	0	5	0	1	3	0	4	13
05:30 PM	0	2	1	0	3	0	0	1	0	1	0	0	5	1	6	0	0	1	0	1	11
05:45 PM	0	2	1	0	3	0	0	0	0	0	0	0	1	1	2	0	0	2	0	2	7
Total	0	8	3	1	12	0	0	3	0	3	0	0	17	3	20	0	1	8	0	9	44
Grand Total	0	13	7	5	25	0	4	14	6	24	0	1	69	13	83	0	17	87	1	105	237
Apprch %	0	52	28	20		0	16.7	58.3	25		0	1.2	83.1	15.7		0	16.2	82.9	1		
Total %	0	5.5	3	2.1	10.5	0	1.7	5.9	2.5	10.1	0	0.4	29.1	5.5	35	0	7.2	36.7	0.4	44.3	

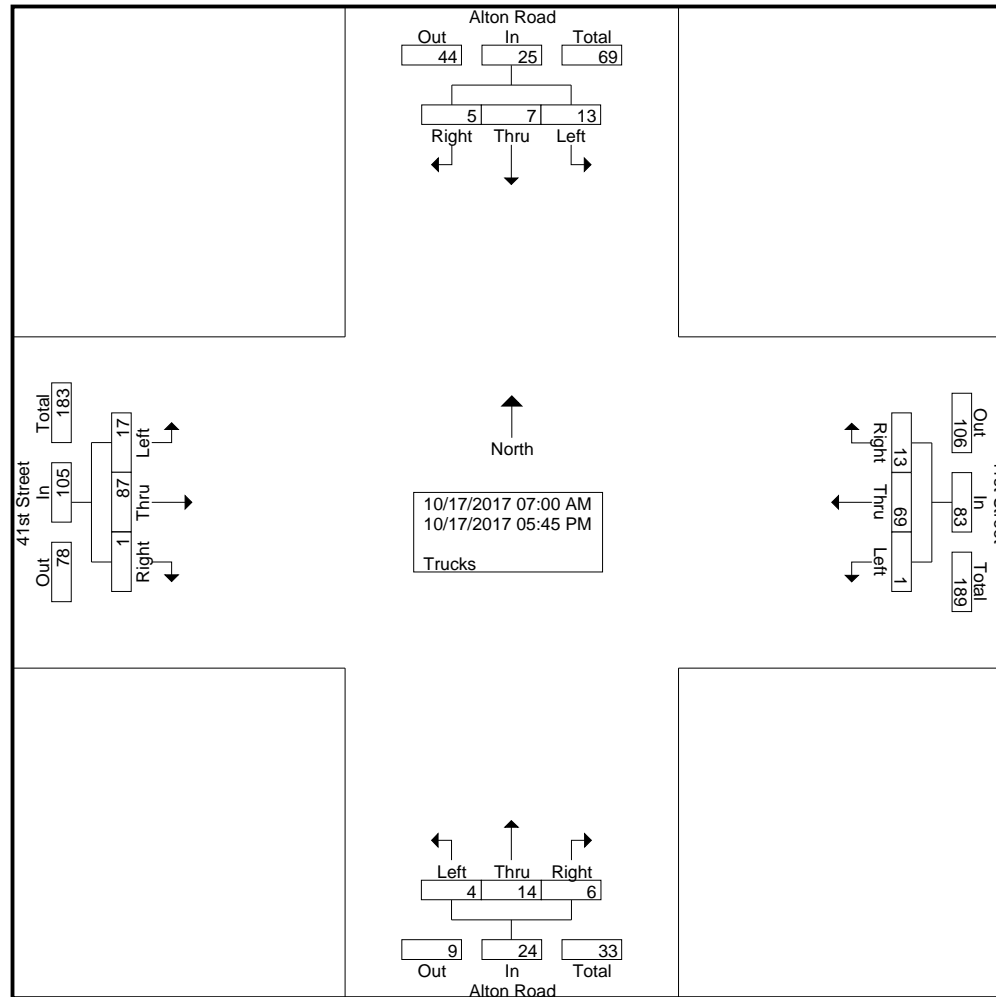
Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Alton Road Southbound					Alton Road Northbound					41st Street Westbound					41st Street Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 07:45 AM																						
07:45 AM	0	0	2	0	2	0	1	0	0	1	0	0	3	1	4	0	2	10	0	12	19	
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	2	1	3	0	1	5	0	6	9	
08:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	4	3	7	0	0	10	0	10	17	
08:30 AM	0	0	0	0	0	0	0	4	1	5	0	0	2	0	2	0	3	7	0	10	17	
Total Volume	0	0	2	0	2	0	1	4	1	6	0	0	11	5	16	0	6	32	0	38	62	
% App. Total	0	0	100	0		0	16.7	66.7	16.7		0	0	68.8	31.2		0	15.8	84.2	0			
PHF	.000	.000	.250	.000	.250	.000	.250	.250	.250	.300	.000	.000	.688	.417	.571	.000	.500	.800	.000	.792	.816	

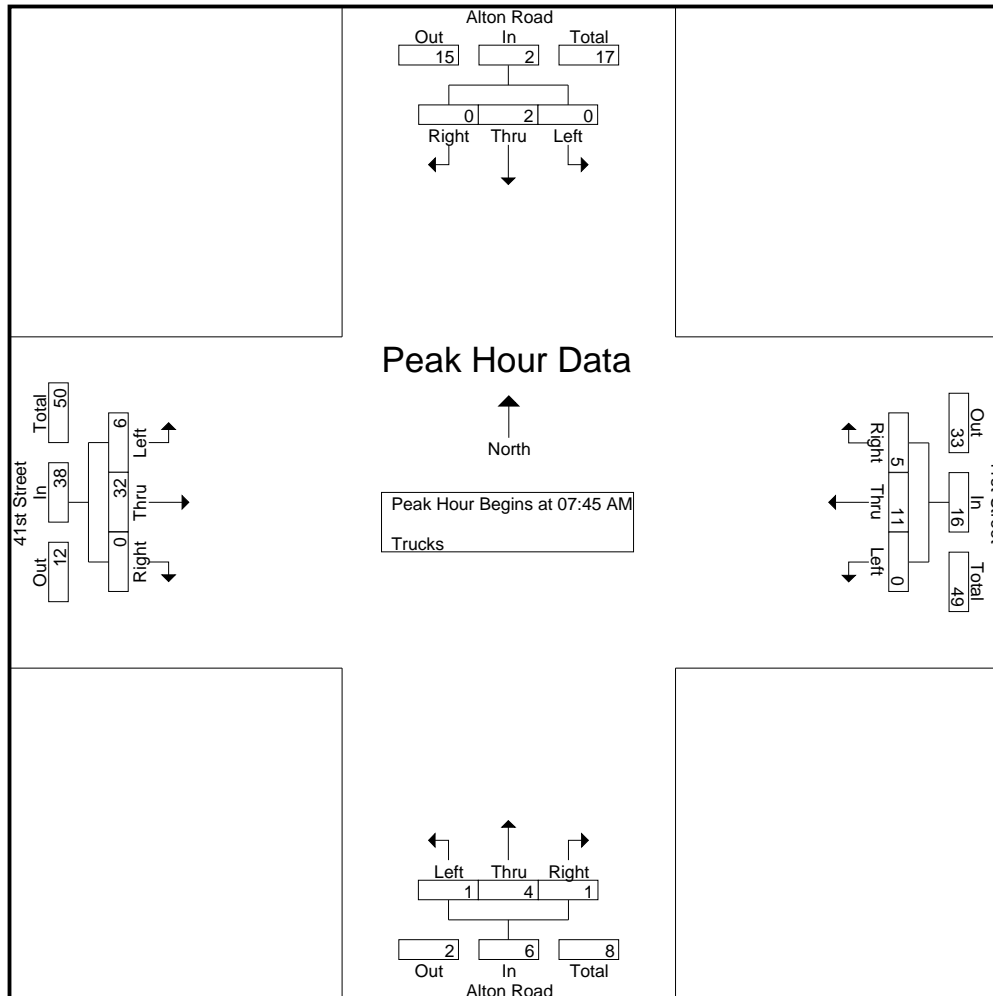
Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	Alton Road Southbound					Alton Road Northbound					41st Street Westbound					41st Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:45 PM																					
04:45 PM	0	1	0	0	1	0	0	1	1	2	0	0	4	0	4	0	2	8	0	10	17
05:00 PM	0	1	1	1	3	0	0	1	0	1	0	0	6	1	7	0	0	2	0	2	13
05:15 PM	0	3	0	0	3	0	0	1	0	1	0	0	5	0	5	0	1	3	0	4	13
05:30 PM	0	2	1	0	3	0	0	1	0	1	0	0	5	1	6	0	0	1	0	1	11
Total Volume	0	7	2	1	10	0	0	4	1	5	0	0	20	2	22	0	3	14	0	17	54
% App. Total	0	70	20	10		0	0	80	20		0	0	90.9	9.1		0	17.6	82.4	0		
PHF	.000	.583	.500	.250	.833	.000	.000	1.00	.250	.625	.000	.000	.833	.500	.786	.000	.375	.438	.000	.425	.794

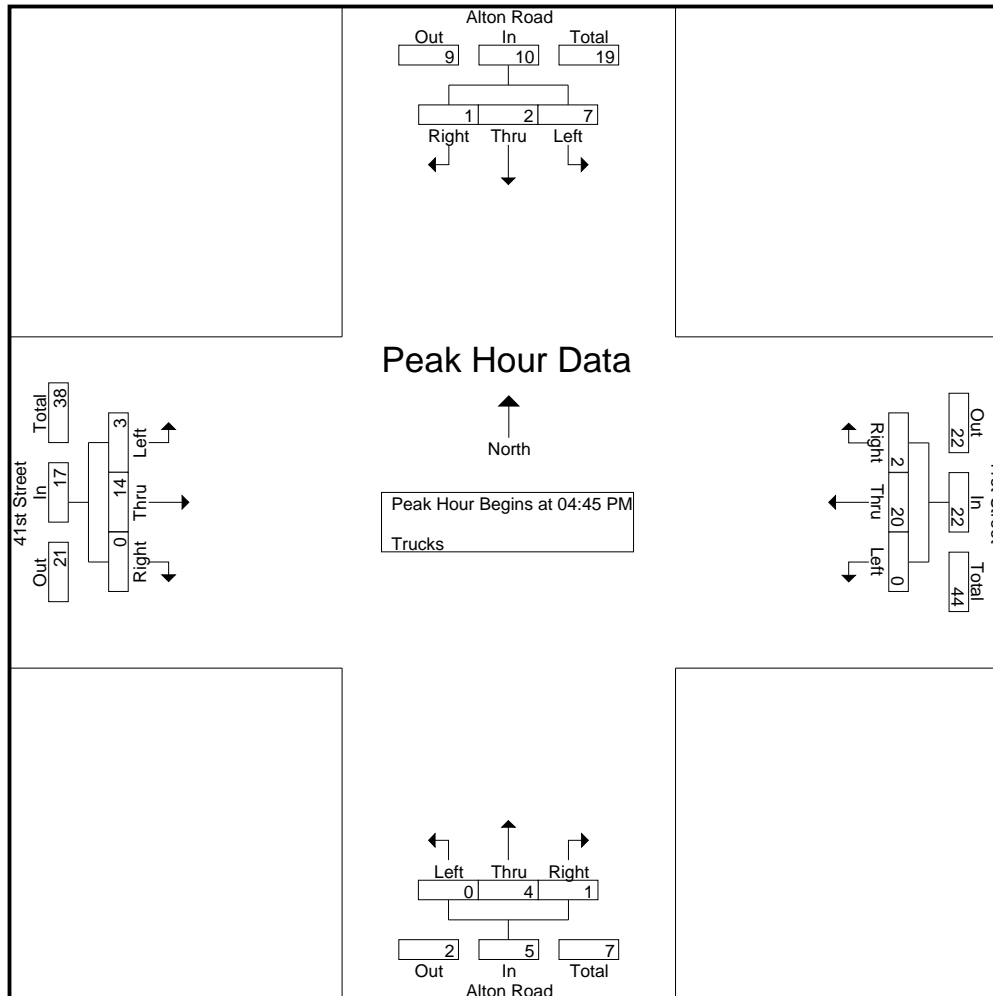
Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	Alton Road Southbound					Alton Road Northbound					41st Street Westbound					41st Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	14	10	36	60	0	7	18	12	37	0	2	217	12	231	0	98	389	15	502	830
07:15 AM	0	12	25	64	101	0	8	17	9	34	0	6	259	8	273	0	77	327	28	432	840
07:30 AM	0	6	38	82	126	0	30	24	8	62	0	18	260	9	287	0	55	293	37	385	860
07:45 AM	0	14	34	83	131	0	32	27	8	67	0	14	295	16	325	0	60	352	30	442	965
Total	0	46	107	265	418	0	77	86	37	200	0	40	1031	45	1116	0	290	1361	110	1761	3495
08:00 AM	0	13	29	81	123	0	22	30	18	70	0	5	280	28	313	0	89	375	11	475	981
08:15 AM	0	12	13	61	86	0	7	28	12	47	0	9	265	18	292	0	98	361	10	469	894
08:30 AM	0	20	23	72	115	0	25	41	16	82	0	10	300	25	335	0	80	313	13	406	938
08:45 AM	0	13	30	67	110	0	15	38	13	66	0	15	297	21	333	0	76	290	9	375	884
Total	0	58	95	281	434	0	69	137	59	265	0	39	1142	92	1273	0	343	1339	43	1725	3697
*** BREAK ***																					
03:00 PM	0	26	23	39	88	0	31	36	20	87	0	16	302	19	337	0	31	236	20	287	799
03:15 PM	0	19	10	29	58	0	32	36	22	90	0	15	337	21	373	0	37	246	17	300	821
03:30 PM	0	18	16	84	118	0	31	36	26	93	0	9	338	23	370	1	31	263	6	301	882
03:45 PM	0	28	22	79	129	0	20	46	12	78	0	7	309	22	338	0	38	227	10	275	820
Total	0	91	71	231	393	0	114	154	80	348	0	47	1286	85	1418	1	137	972	53	1163	3322
04:00 PM	0	16	17	69	102	0	29	28	25	82	0	8	356	34	398	0	42	260	9	311	893
04:15 PM	0	20	20	53	93	0	28	28	20	76	0	11	290	14	315	0	33	278	22	333	817
04:30 PM	0	24	25	40	89	0	19	28	24	71	0	13	312	14	339	1	34	280	13	328	827
04:45 PM	0	23	14	52	89	0	37	47	23	107	0	6	274	12	292	0	36	251	11	298	786
Total	0	83	76	214	373	0	113	131	92	336	0	38	1232	74	1344	1	145	1069	55	1270	3323
05:00 PM	0	21	34	63	118	0	25	35	22	82	0	13	264	12	289	0	28	224	12	264	753
05:15 PM	0	24	16	51	91	0	22	34	15	71	0	14	342	15	371	0	42	291	15	348	881
05:30 PM	0	22	26	40	88	0	21	41	24	86	0	8	295	9	312	2	55	316	7	380	866
05:45 PM	0	21	23	25	69	0	20	42	26	88	0	16	310	14	340	0	46	224	6	276	773
Total	0	88	99	179	366	0	88	152	87	327	0	51	1211	50	1312	2	171	1055	40	1268	3273
Grand Total	0	366	448	1170	1984	0	461	660	355	1476	0	215	5902	346	6463	4	1086	5796	301	7187	17110
Apprch %	0	18.4	22.6	59		0	31.2	44.7	24.1		0	3.3	91.3	5.4		0.1	15.1	80.6	4.2		
Total %	0	2.1	2.6	6.8	11.6	0	2.7	3.9	2.1	8.6	0	1.3	34.5	2	37.8	0	6.3	33.9	1.8	42	
Vehicle	0	353	441	1165	1959	0	457	646	349	1452	0	214	5833	333	6380	4	1069	5709	300	7082	16873
% Vehicle	0	96.4	98.4	99.6	98.7	0	99.1	97.9	98.3	98.4	0	99.5	98.8	96.2	98.7	100	98.4	98.5	99.7	98.5	98.6

Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	Alton Road Southbound					Alton Road Northbound					41st Street Westbound					41st Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	13	7	5	25	0	4	14	6	24	0	1	69	13	83	0	17	87	1	105	237
% Trucks	0	3.6	1.6	0.4	1.3	0	0.9	2.1	1.7	1.6	0	0.5	1.2	3.8	1.3	0	1.6	1.5	0.3	1.5	1.4

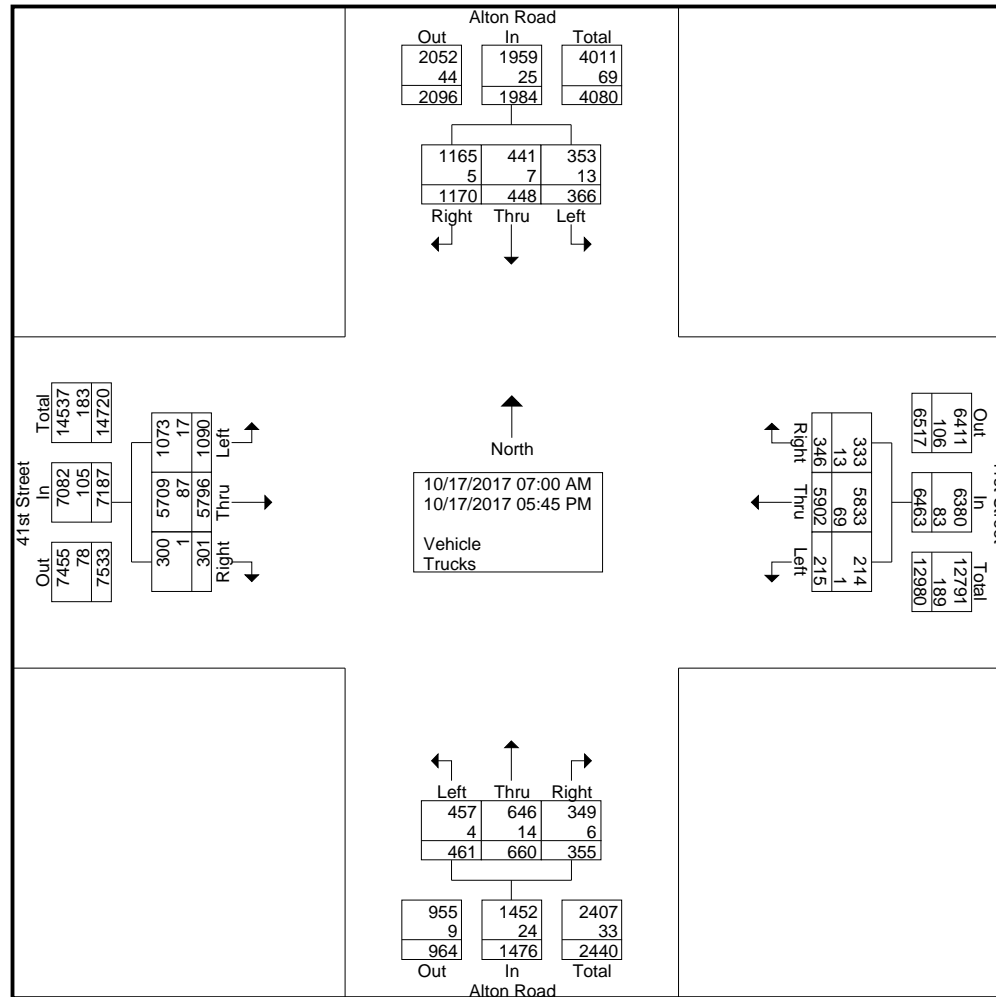
Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 3



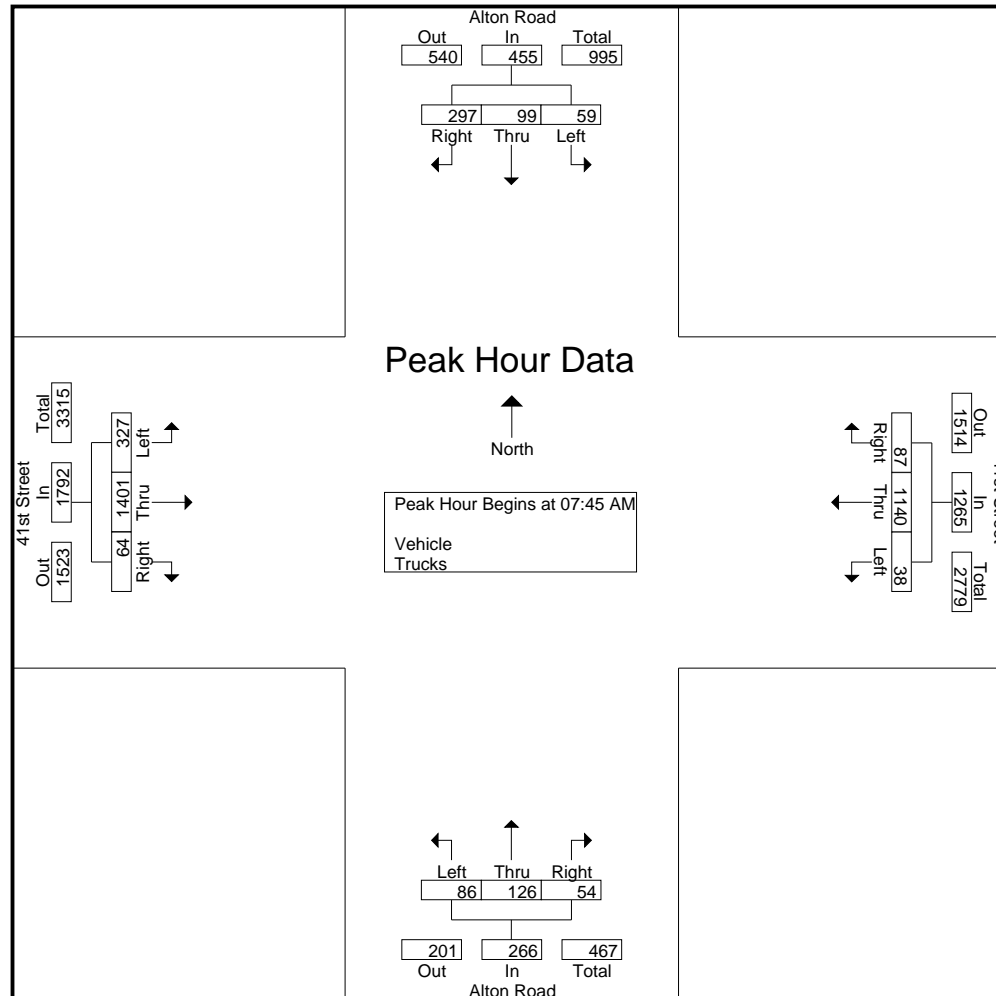
Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4

Start Time	Alton Road Southbound					Alton Road Northbound					41st Street Westbound					41st Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:45 AM																					
07:45 AM	0	14	34	83	131	0	32	27	8	67	0	14	295	16	325	0	60	352	30	442	965
08:00 AM	0	13	29	81	123	0	22	30	18	70	0	5	280	28	313	0	89	375	11	475	981
08:15 AM	0	12	13	61	86	0	7	28	12	47	0	9	265	18	292	0	98	361	10	469	894
08:30 AM	0	20	23	72	115	0	25	41	16	82	0	10	300	25	335	0	80	313	13	406	938
Total Volume	0	59	99	297	455	0	86	126	54	266	0	38	1140	87	1265	0	327	1401	64	1792	3778
% App. Total	0	13	21.8	65.3		0	32.3	47.4	20.3		0	3	90.1	6.9		0	18.2	78.2	3.6		
PHF	.000	.738	.728	.895	.868	.000	.672	.768	.750	.811	.000	.679	.950	.777	.944	.000	.834	.934	.533	.943	.963

Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5



Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6

Start Time	Alton Road Southbound					Alton Road Northbound					41st Street Westbound					41st Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:15 PM																					
03:15 PM	0	19	10	29	58	0	32	36	22	90	0	15	337	21	373	0	37	246	17	300	821
03:30 PM	0	18	16	84	118	0	31	36	26	93	0	9	338	23	370	1	31	263	6	301	882
03:45 PM	0	28	22	79	129	0	20	46	12	78	0	7	309	22	338	0	38	227	10	275	820
04:00 PM	0	16	17	69	102	0	29	28	25	82	0	8	356	34	398	0	42	260	9	311	893
Total Volume	0	81	65	261	407	0	112	146	85	343	0	39	1340	100	1479	1	148	996	42	1187	3416
% App. Total	0	19.9	16	64.1		0	32.7	42.6	24.8		0	2.6	90.6	6.8		0.1	12.5	83.9	3.5		
PHF	.000	.723	.739	.777	.789	.000	.875	.793	.817	.922	.000	.650	.941	.735	.929	.250	.881	.947	.618	.954	.956

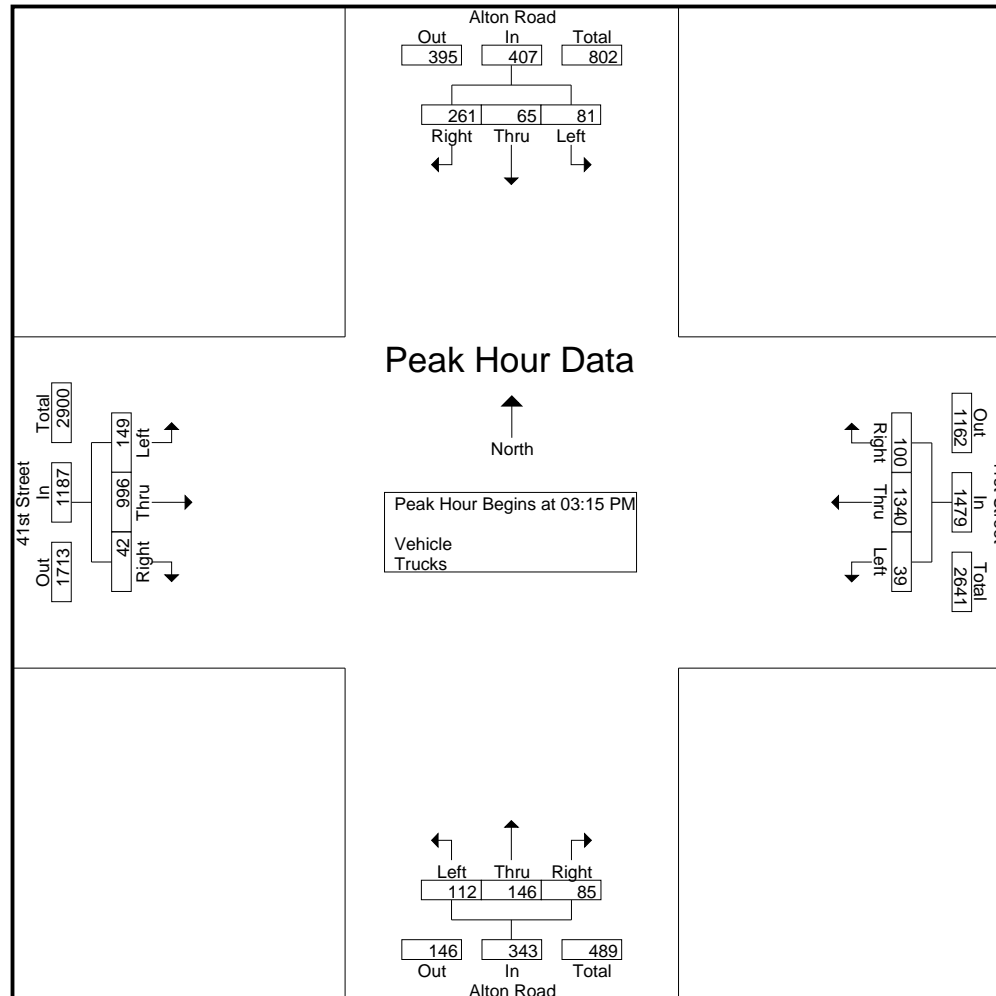
Alton Road & 41st Street

File Name : TMC-22 Alton Rd & 41st Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 7



Alton Road & Nautilus Road

File Name : TMC-23 Alton Rd & Nautilus Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	Alton Road Southbound			Alton Road Northbound			Nautilus Rd Westbound			Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
*** BREAK ***													
07:30 AM	0	0	0	1	0	1	0	0	0	0	0	0	1
*** BREAK ***													
Total	0	0	0	1	0	1	0	0	0	0	0	0	1
*** BREAK ***													
Grand Total	0	0	0	1	0	1	0	0	0	0	0	0	1
Apprch %	0	0		100	0		0	0		0	0		
Total %	0	0		100	0	100	0	0		0	0		

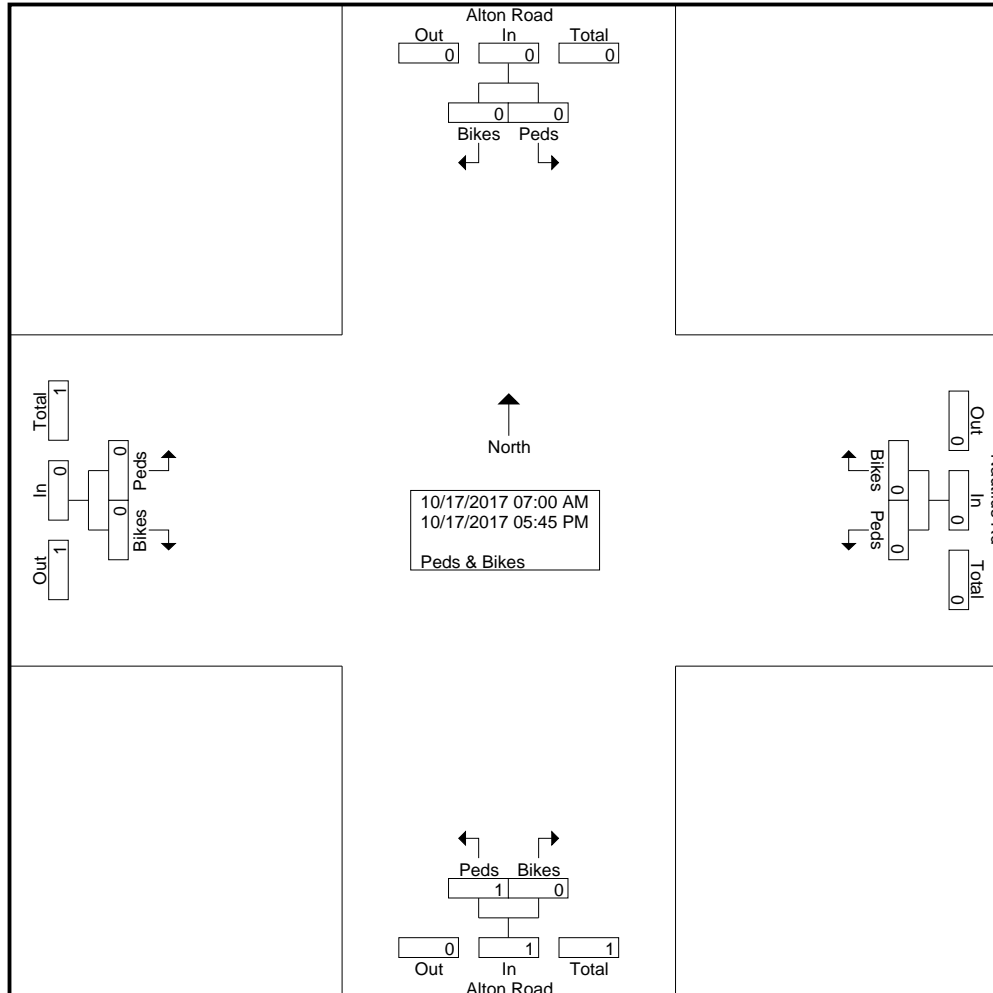
Alton Road & Nautilus Road

File Name : TMC-23 Alton Rd & Nautilus Rd

Site Code : 00000000

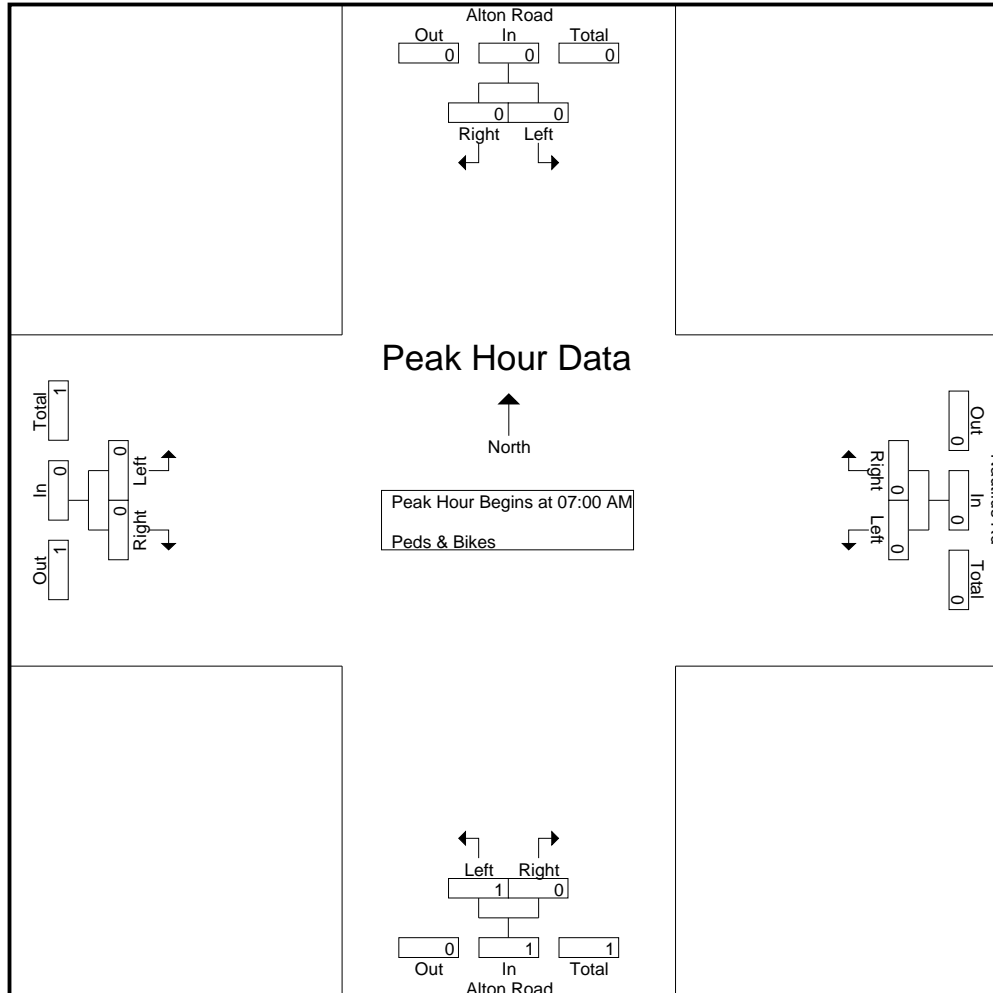
Start Date : 10/17/2017

Page No : 2



Alton Road & Nautilus Road

File Name : TMC-23 Alton Rd & Nautilus Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4



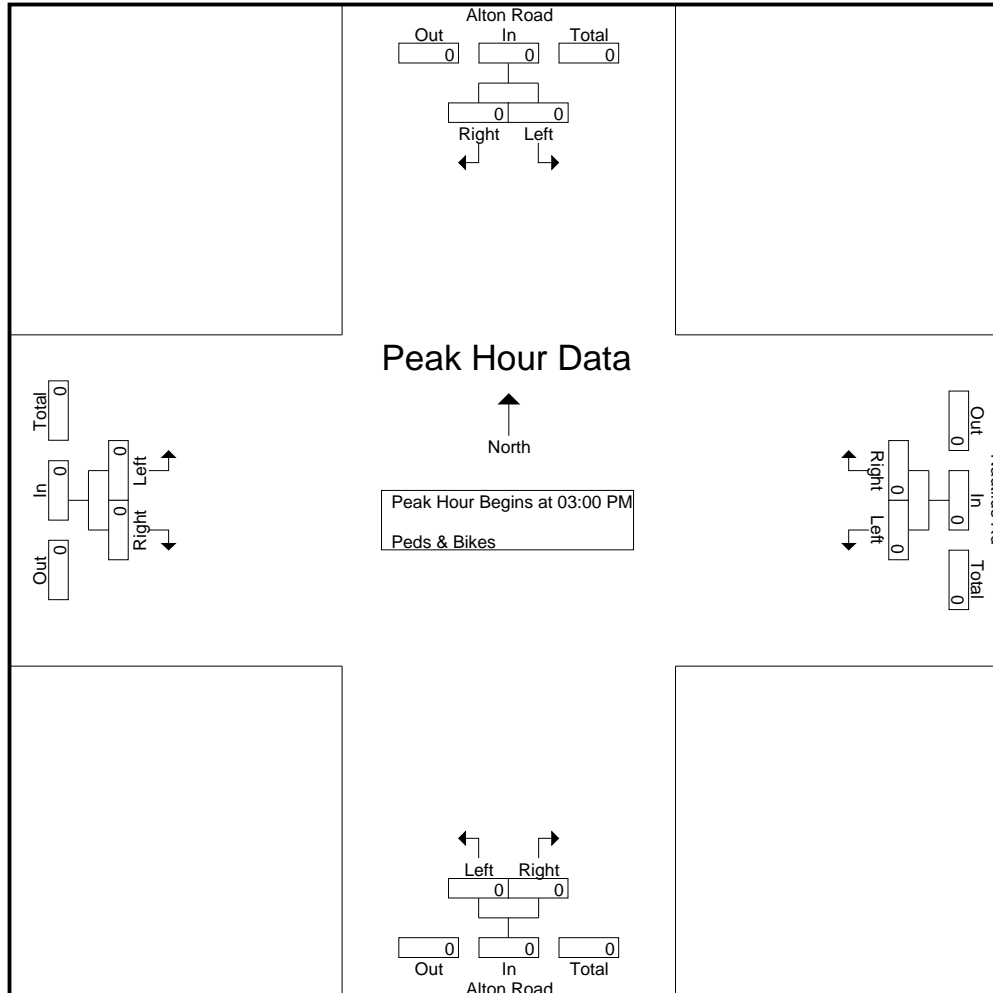
Alton Road & Nautilus Road

File Name : TMC-23 Alton Rd & Nautilus Rd

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



Alton Road & Nautilus Road

File Name : TMC-23 Alton Rd & Nautilus Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Trucks

Start Time	Alton Road Southbound					Alton Road Northbound					Nautilus Rd Westbound					Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
07:00 AM	0	0	1	0	1	0	0	5	1	6	0	0	0	0	0	0	0	0	0	0	0	7
07:15 AM	0	0	0	0	0	0	0	5	1	6	0	0	0	0	0	0	0	0	0	0	0	6
07:30 AM	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	2
07:45 AM	0	0	1	0	1	0	0	4	1	5	0	1	0	0	1	0	0	0	0	0	0	7
Total	0	0	2	0	2	0	0	16	3	19	0	1	0	0	1	0	0	0	0	0	0	22
08:00 AM	0	0	0	0	0	0	0	8	0	8	0	0	0	0	0	0	0	0	0	0	0	8
08:15 AM	0	0	0	0	0	0	0	7	0	7	0	0	0	0	0	0	0	0	0	0	0	7
08:30 AM	0	0	0	0	0	0	0	10	4	14	0	4	0	0	4	0	0	0	0	0	0	18
08:45 AM	0	0	0	0	0	0	0	7	1	8	0	5	0	0	5	0	0	0	0	0	0	13
Total	0	0	0	0	0	0	0	32	5	37	0	9	0	0	9	0	0	0	0	0	0	46
*** BREAK ***																						
03:00 PM	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	2
03:15 PM	0	1	0	0	1	0	0	4	1	5	0	0	0	0	0	0	0	0	0	0	0	6
03:30 PM	0	0	1	0	1	0	0	4	1	5	0	0	0	0	0	0	0	0	0	0	0	6
03:45 PM	0	0	1	0	1	0	0	2	1	3	0	0	0	0	0	0	0	0	0	0	0	4
Total	0	1	2	0	3	0	0	12	3	15	0	0	0	0	0	0	0	0	0	0	0	18
04:00 PM	0	0	0	0	0	0	0	3	0	3	0	2	0	0	2	0	0	0	0	0	0	5
04:15 PM	0	1	0	0	1	0	0	3	0	3	0	2	0	0	2	0	0	0	0	0	0	6
04:30 PM	0	0	2	0	2	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	4
04:45 PM	0	0	1	0	1	0	0	7	1	8	0	0	0	0	0	0	0	0	0	0	0	9
Total	0	1	3	0	4	0	0	15	1	16	0	4	0	0	4	0	0	0	0	0	0	24
05:00 PM	0	0	2	0	2	0	0	3	0	3	0	1	0	0	1	0	0	0	0	0	0	6
05:15 PM	0	0	3	0	3	0	0	4	1	5	0	0	0	0	0	0	0	0	0	0	0	8
05:30 PM	0	0	3	0	3	0	0	5	0	5	0	0	0	0	0	0	0	0	0	0	0	8
05:45 PM	0	0	3	0	3	0	0	4	0	4	0	0	0	0	0	0	0	0	0	0	0	7
Total	0	0	11	0	11	0	0	16	1	17	0	1	0	0	1	0	0	0	0	0	0	29
Grand Total	0	2	18	0	20	0	0	91	13	104	0	15	0	0	15	0	0	0	0	0	0	139
Apprch %	0	10	90	0		0	0	87.5	12.5		0	100	0	0		0	0	0	0	0		
Total %	0	1.4	12.9	0	14.4	0	0	65.5	9.4	74.8	0	10.8	0	0	10.8	0	0	0	0	0	0	

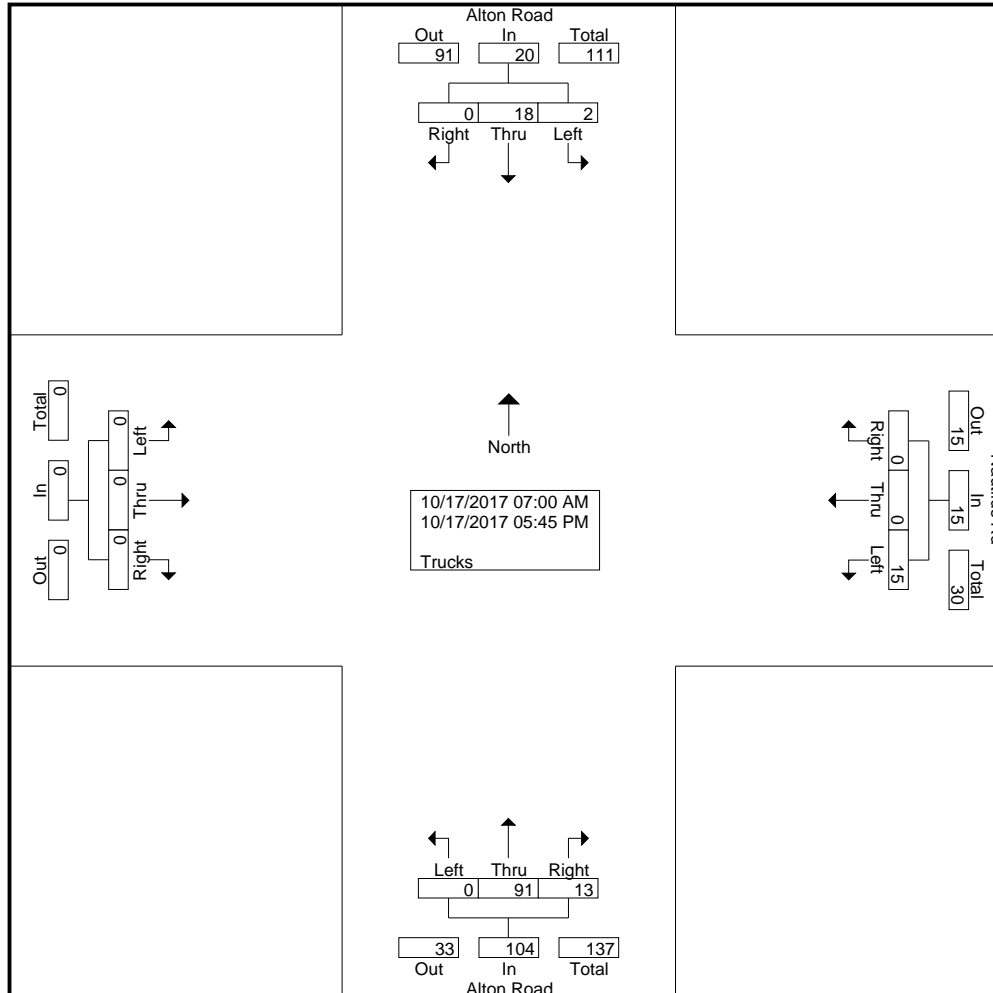
Alton Road & Nautilus Road

File Name : TMC-23 Alton Rd & Nautilus Rd

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



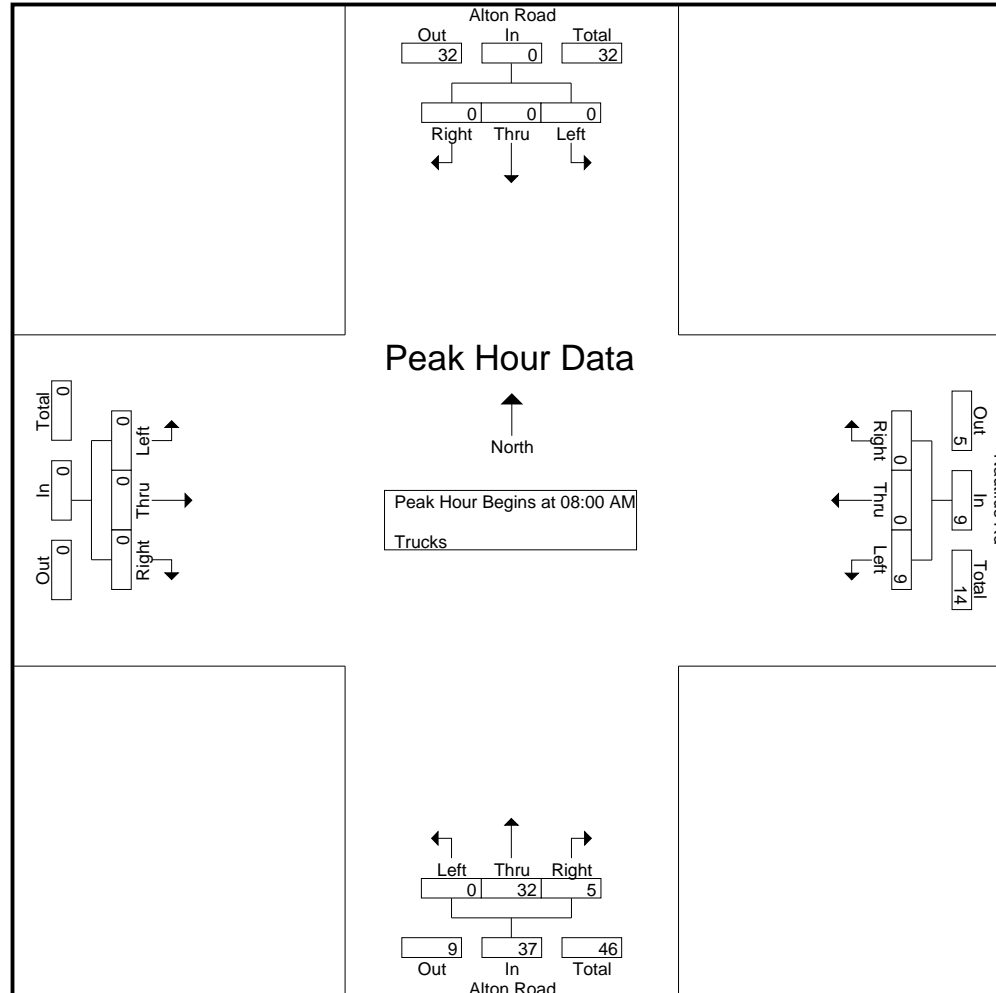
Alton Road & Nautilus Road

File Name : TMC-23 Alton Rd & Nautilus Rd

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



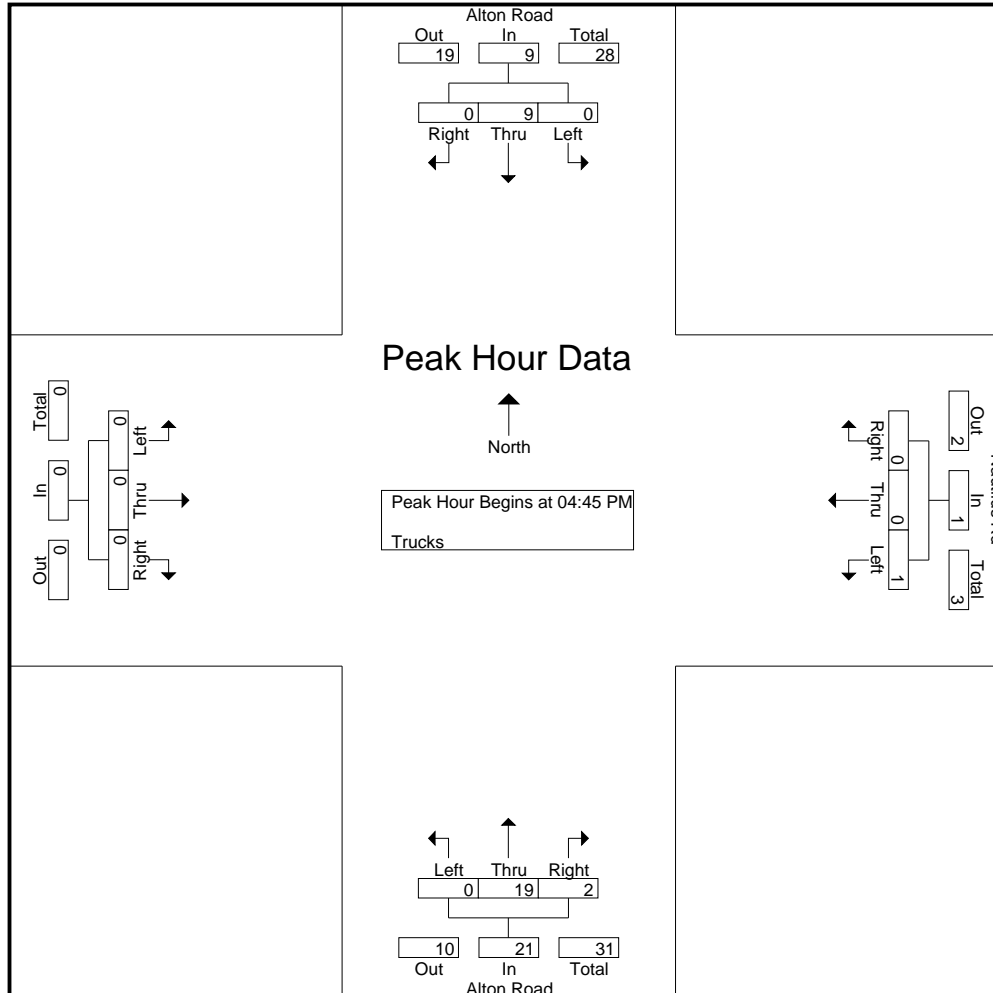
Alton Road & Nautilus Road

File Name : TMC-23 Alton Rd & Nautilus Rd

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



Alton Road & Nautilus Road

File Name : TMC-23 Alton Rd & Nautilus Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	Alton Road Southbound					Alton Road Northbound					Nautilus Rd Westbound					Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	3	19	0	22	0	0	109	21	130	0	41	0	4	45	0	0	0	0	0	197
07:15 AM	1	2	22	0	25	0	0	85	17	102	0	80	0	5	85	0	0	0	0	0	212
07:30 AM	1	4	18	0	23	1	0	75	19	95	0	109	0	4	113	0	0	0	0	0	231
07:45 AM	0	0	22	0	22	2	0	70	31	103	0	109	0	9	118	0	0	0	0	0	243
Total	2	9	81	0	92	3	0	339	88	430	0	339	0	22	361	0	0	0	0	0	883
08:00 AM	0	6	30	0	36	2	0	109	31	142	0	94	0	6	100	0	0	0	0	0	278
08:15 AM	2	7	19	0	28	0	0	115	37	152	0	67	0	5	72	0	0	0	0	0	252
08:30 AM	0	4	34	0	38	1	0	89	47	137	0	86	0	5	91	0	0	0	0	0	266
08:45 AM	2	7	19	0	28	1	0	78	53	132	0	95	0	6	101	0	0	0	0	0	261
Total	4	24	102	0	130	4	0	391	168	563	0	342	0	22	364	0	0	0	0	0	1057
*** BREAK ***																					
03:00 PM	1	9	39	0	49	2	0	61	27	90	0	49	0	7	56	0	0	0	0	0	195
03:15 PM	4	6	25	0	35	0	0	71	34	105	0	36	0	11	47	0	0	0	0	0	187
03:30 PM	0	8	29	0	37	0	0	65	29	94	0	89	0	7	96	0	0	0	0	0	227
03:45 PM	0	7	46	0	53	0	0	62	40	102	0	83	0	7	90	0	0	0	0	0	245
Total	5	30	139	0	174	2	0	259	130	391	0	257	0	32	289	0	0	0	0	0	854
04:00 PM	0	2	21	0	23	0	0	60	38	98	0	82	0	8	90	0	0	0	0	0	211
04:15 PM	1	4	28	0	33	0	0	54	27	81	0	66	0	4	70	0	0	0	0	0	184
04:30 PM	0	4	48	0	52	4	0	62	19	85	0	41	0	2	43	0	0	0	0	0	180
04:45 PM	1	5	39	0	45	0	0	89	13	102	0	50	0	3	53	0	0	0	0	0	200
Total	2	15	136	0	153	4	0	265	97	366	0	239	0	17	256	0	0	0	0	0	775
05:00 PM	0	4	62	0	66	0	0	55	28	83	0	58	0	7	65	0	0	0	0	0	214
05:15 PM	4	6	33	0	43	0	0	61	34	95	0	58	0	4	62	0	0	0	0	0	200
05:30 PM	1	1	42	0	44	0	0	81	28	109	0	48	0	5	53	0	0	0	0	0	206
05:45 PM	1	1	45	0	47	1	0	79	24	104	0	25	0	3	28	0	0	0	0	0	179
Total	6	12	182	0	200	1	0	276	114	391	0	189	0	19	208	0	0	0	0	0	799
Grand Total	19	90	640	0	749	14	0	1530	597	2141	0	1366	0	112	1478	0	0	0	0	0	4368
Apprch %	2.5	12	85.4	0		0.7	0	71.5	27.9		0	92.4	0	7.6		0	0	0	0	0	
Total %	0.4	2.1	14.7	0	17.1	0.3	0	35	13.7	49	0	31.3	0	2.6	33.8	0	0	0	0	0	
Vehicle	19	88	622	0	729	14	0	1439	584	2037	0	1351	0	112	1463	0	0	0	0	0	4229
% Vehicle	100	97.8	97.2	0	97.3	100	0	94.1	97.8	95.1	0	98.9	0	100	99	0	0	0	0	0	96.8
Trucks	0	2	18	0	20	0	0	91	13	104	0	15	0	0	15	0	0	0	0	0	139
% Trucks	0	2.2	2.8	0	2.7	0	0	5.9	2.2	4.9	0	1.1	0	0	1	0	0	0	0	0	3.2

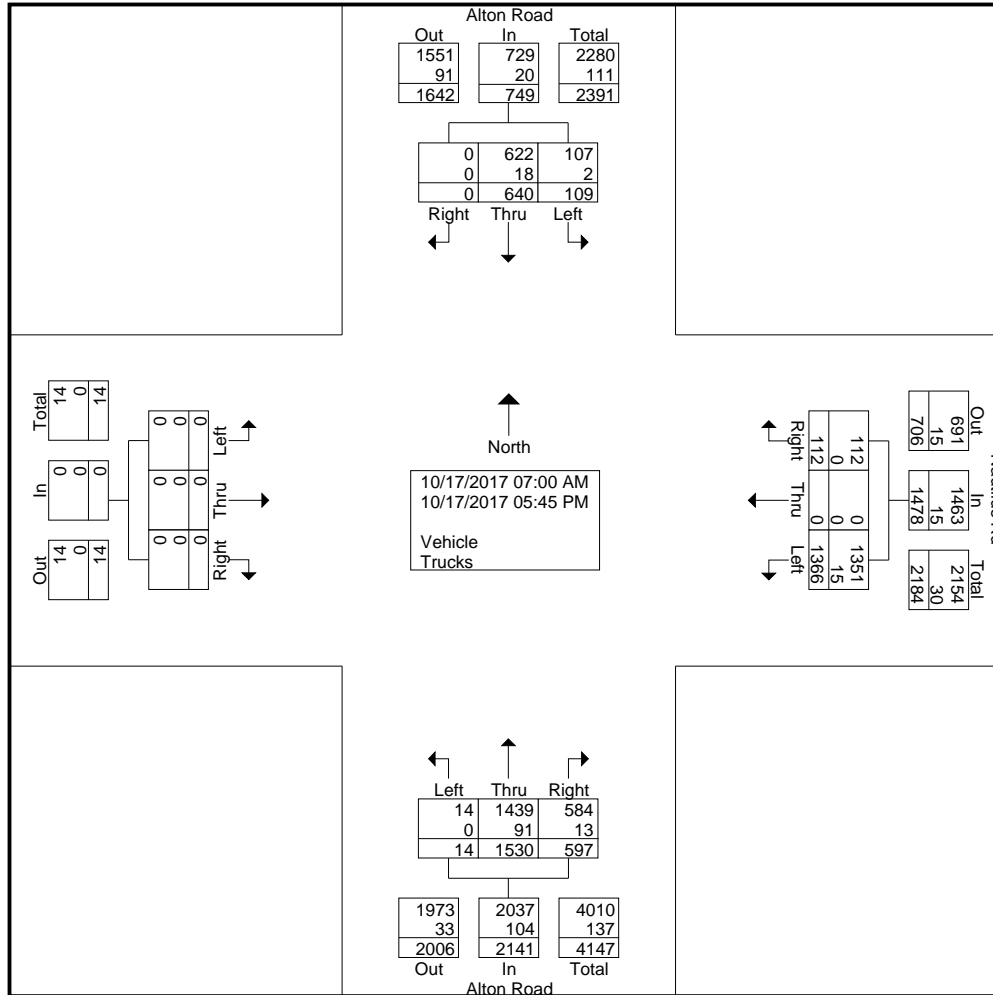
Alton Road & Nautilus Road

File Name : TMC-23 Alton Rd & Nautilus Rd

Site Code : 00000000

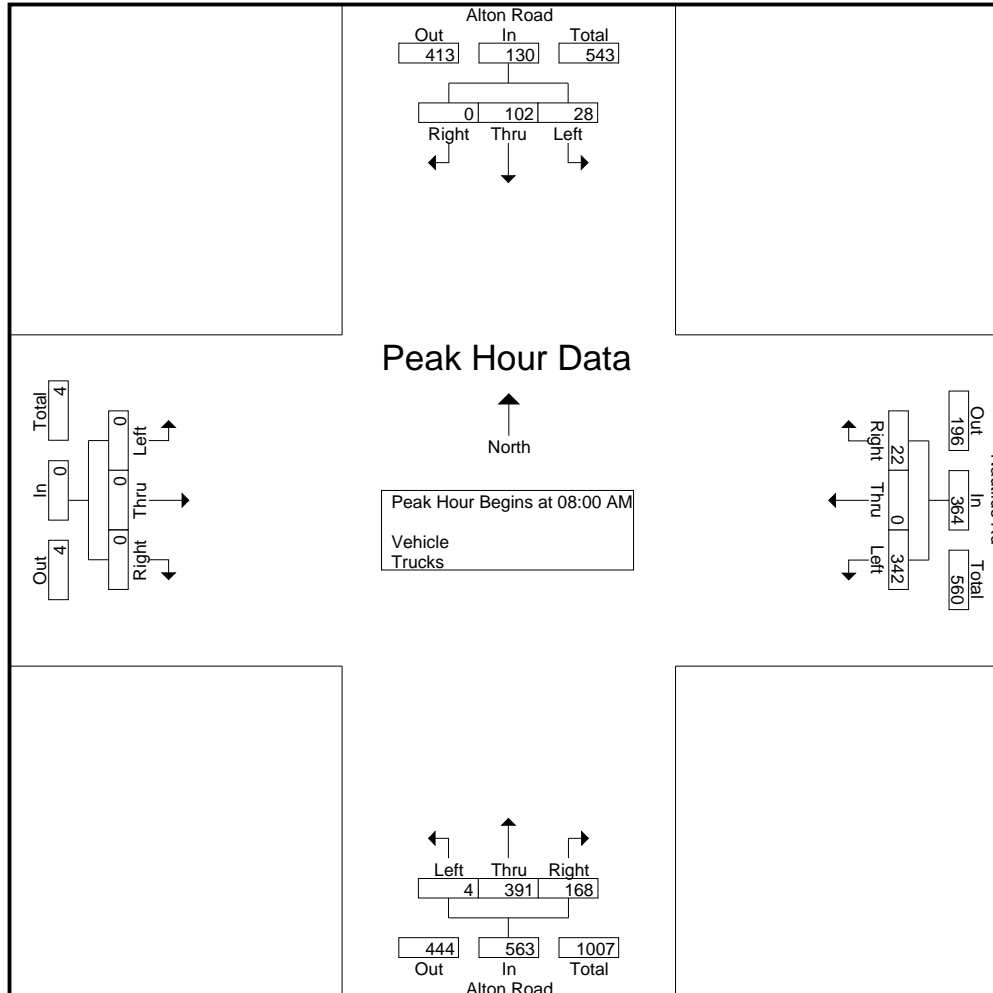
Start Date : 10/17/2017

Page No : 2



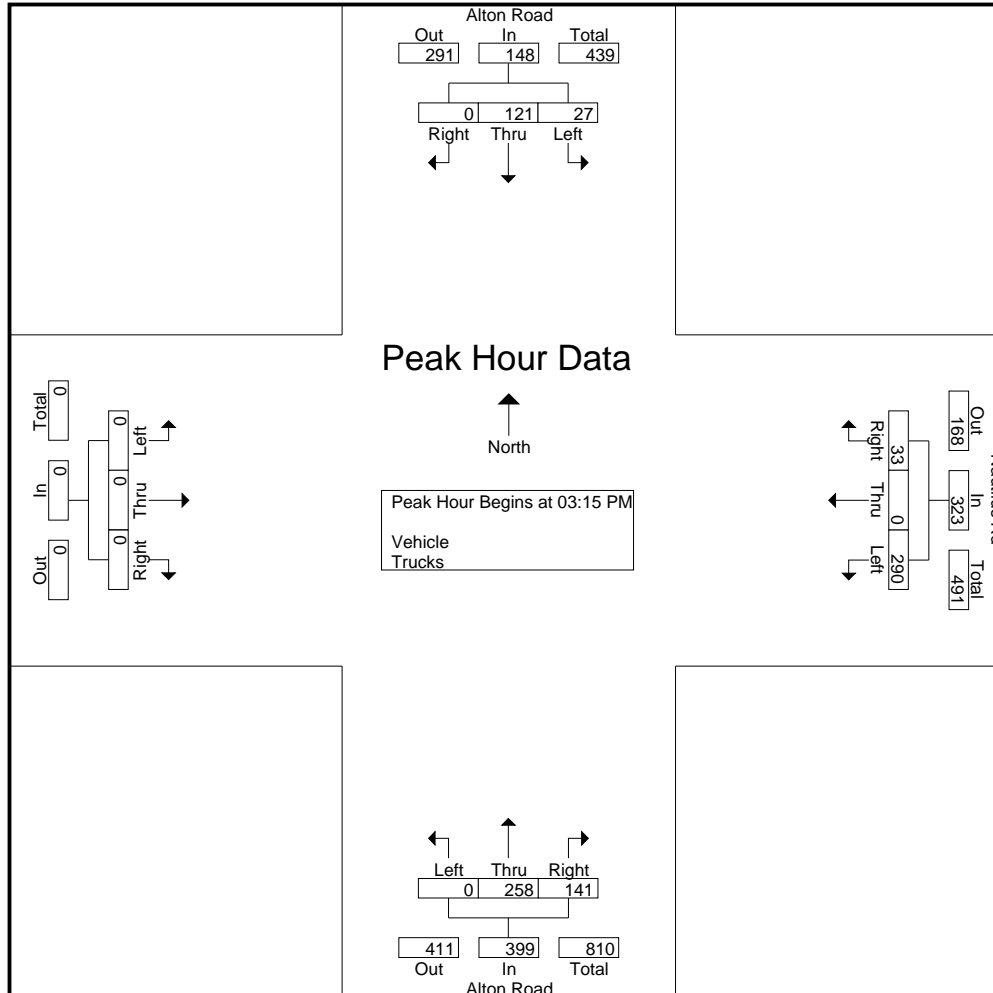
Alton Road & Nautilus Road

File Name : TMC-23 Alton Rd & Nautilus Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4



Alton Road & Nautilus Road

File Name : TMC-23 Alton Rd & Nautilus Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6



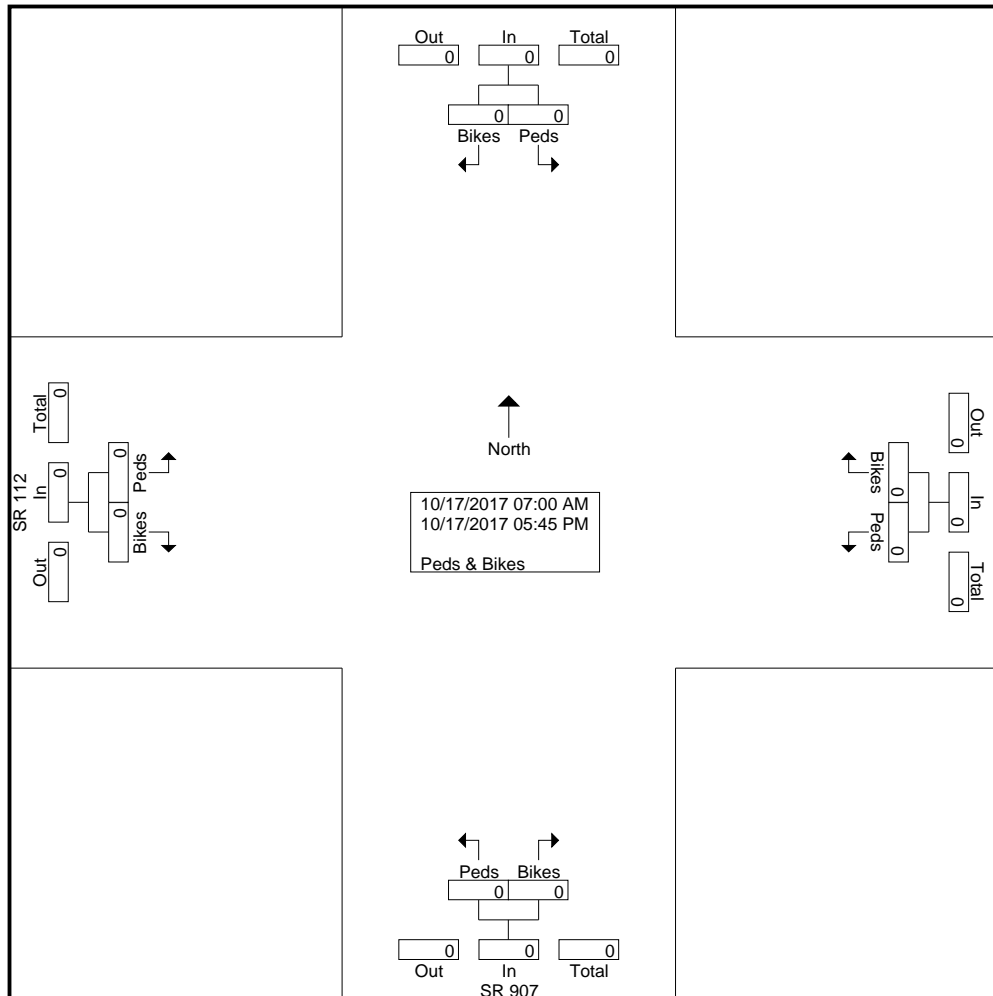
SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



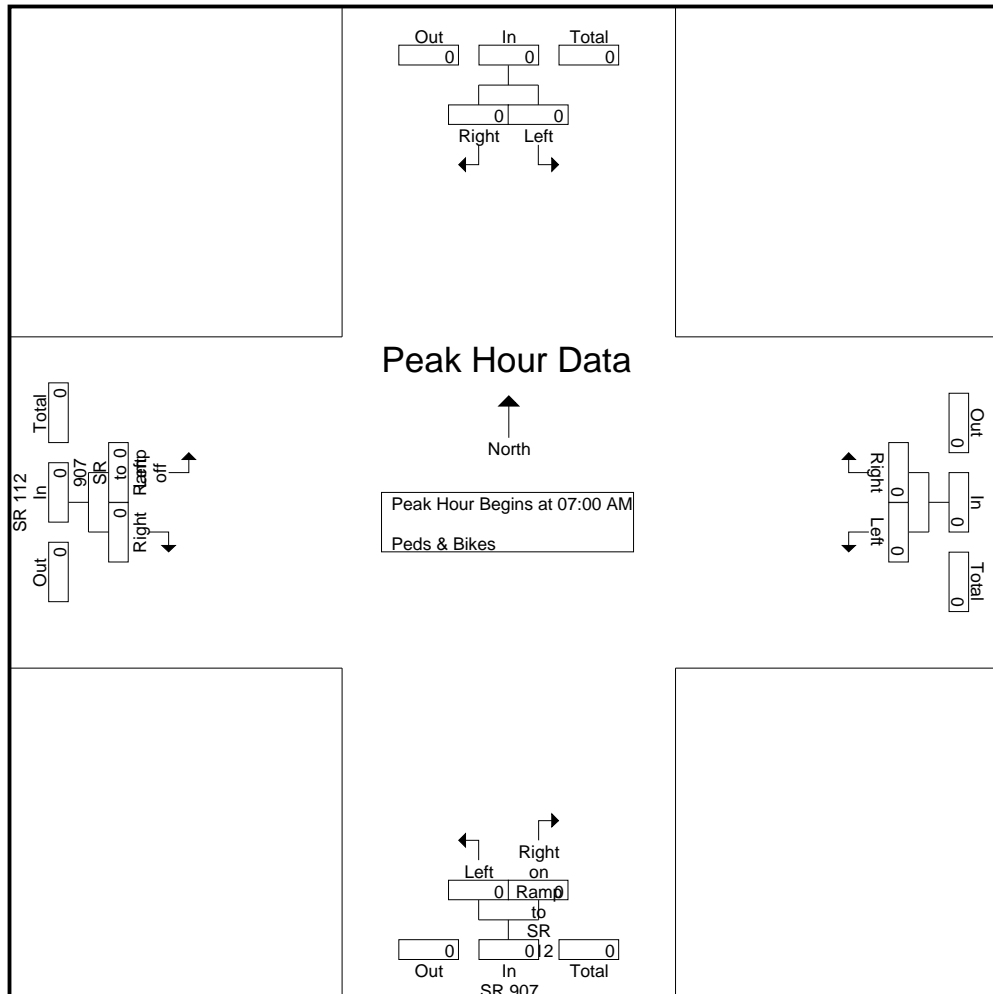
SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



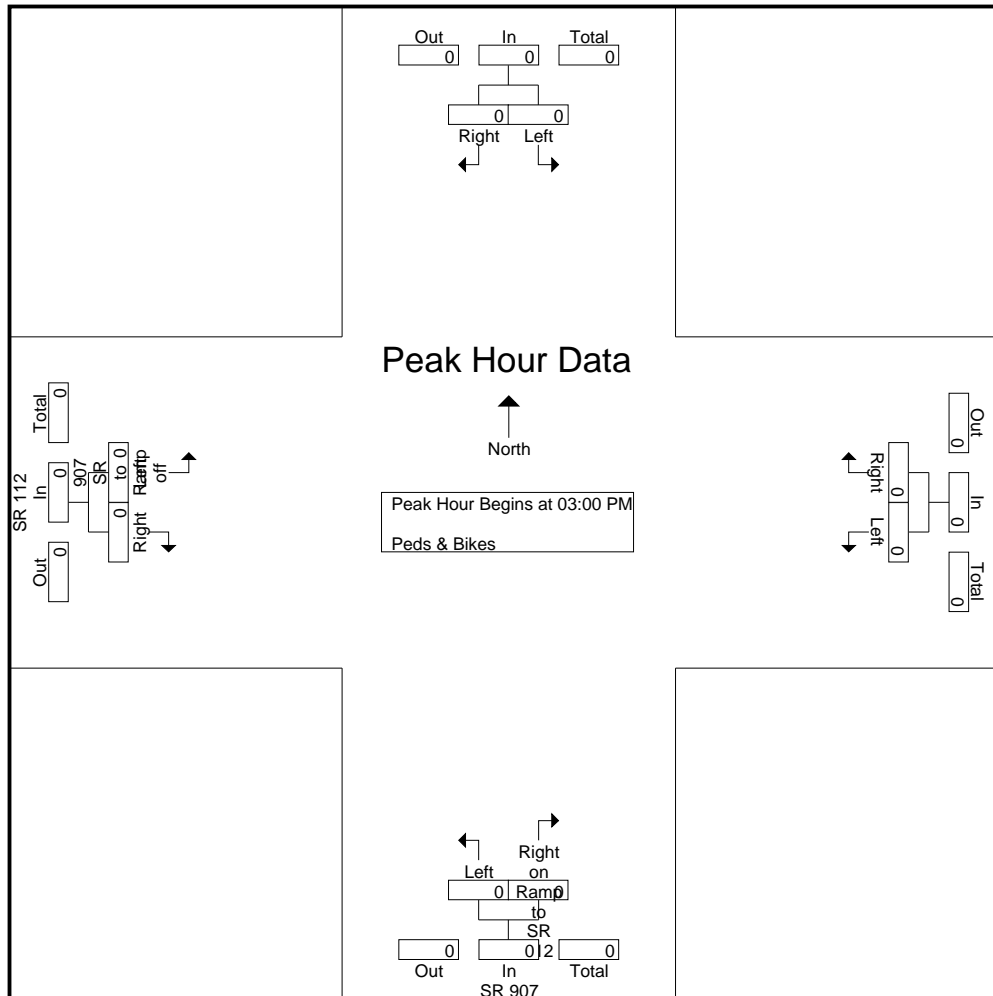
SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Trucks

Start Time	Southbound					SR 907 Northbound					Westbound					SR 112 Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right on Ramp to SR 112	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left off Ramp to SR 907	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	0	0	1	7	8	0	0	0	0	0	0	2	0	0	2	10
07:15 AM	0	0	0	0	0	0	0	1	2	3	0	0	0	0	0	0	3	0	0	3	6
07:30 AM	0	0	0	0	0	0	0	3	2	5	0	0	0	0	0	0	3	0	0	3	8
07:45 AM	0	0	0	0	0	0	0	3	4	7	0	0	0	0	0	0	4	0	0	4	11
Total	0	0	0	0	0	0	0	8	15	23	0	0	0	0	0	0	12	0	0	12	35
08:00 AM	0	0	0	0	0	0	0	4	4	8	0	0	0	0	0	0	4	0	0	4	12
08:15 AM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	4	0	0	4	5
08:30 AM	0	0	0	0	0	0	0	2	6	8	0	0	0	0	0	0	4	0	0	4	12
08:45 AM	0	0	0	0	0	0	0	2	4	6	0	0	0	0	0	0	8	0	0	8	14
Total	0	0	0	0	0	0	0	9	14	23	0	0	0	0	0	0	20	0	0	20	43
*** BREAK ***																					
03:00 PM	0	0	0	0	0	0	0	2	5	7	0	0	0	0	0	0	2	0	0	2	9
03:15 PM	0	0	0	0	0	0	0	0	4	4	0	0	0	0	0	0	1	0	0	1	5
03:30 PM	0	0	0	0	0	0	0	0	9	9	0	0	0	0	0	0	3	0	0	3	12
03:45 PM	0	0	0	0	0	0	0	3	6	9	0	0	0	0	0	0	5	0	0	5	14
Total	0	0	0	0	0	0	0	5	24	29	0	0	0	0	0	0	11	0	0	11	40
04:00 PM	0	0	0	0	0	0	0	1	5	6	0	0	0	0	0	0	2	0	0	2	8
04:15 PM	0	0	0	0	0	0	0	1	4	5	0	0	0	0	0	0	1	0	0	1	6
04:30 PM	0	0	0	0	0	0	0	2	5	7	0	0	0	0	0	0	0	0	0	0	7
04:45 PM	0	0	0	0	0	0	0	0	6	6	0	0	0	0	0	0	0	0	0	0	6
Total	0	0	0	0	0	0	0	4	20	24	0	0	0	0	0	0	3	0	0	3	27
05:00 PM	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	2
05:15 PM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	1	0	0	1	2
05:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
05:45 PM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	1
Total	0	0	0	0	0	0	0	2	2	4	0	0	0	0	0	0	2	0	0	2	6
Grand Total	0	0	0	0	0	0	0	28	75	103	0	0	0	0	0	0	48	0	0	48	151
Apprch %	0	0	0	0	0	0	0	27.2	72.8		0	0	0	0	0	0	100	0	0		
Total %	0	0	0	0	0	0	0	18.5	49.7	68.2	0	0	0	0	0	0	31.8	0	0	31.8	

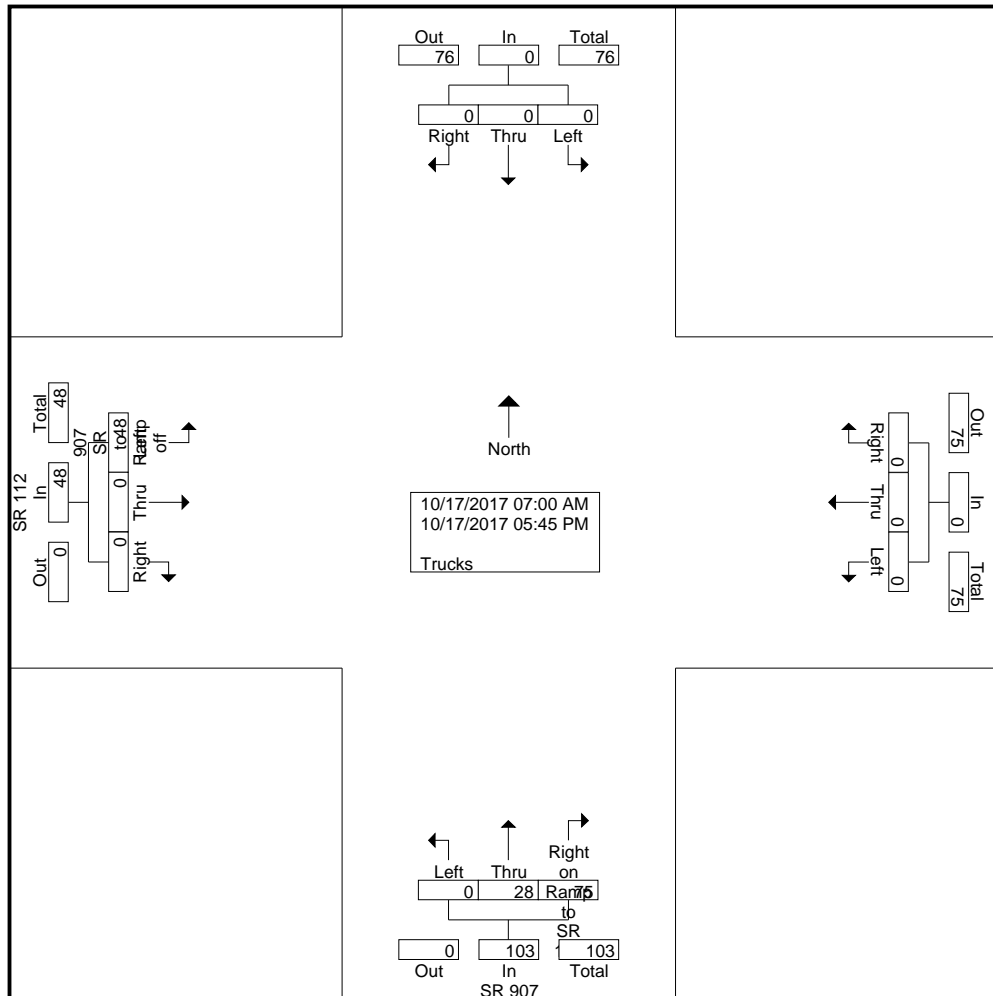
SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Southbound					SR 907 Northbound					Westbound					SR 112 Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right on Ramp to SR 112	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left off Ramp to SR 907	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	0	0	0	0	0	0	4	4	8	0	0	0	0	0	0	4	0	0	4	12	
08:15 AM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	4	0	0	4	5	
08:30 AM	0	0	0	0	0	0	0	2	6	8	0	0	0	0	0	0	4	0	0	4	12	
08:45 AM	0	0	0	0	0	0	0	2	4	6	0	0	0	0	0	0	8	0	0	8	14	
Total Volume	0	0	0	0	0	0	0	9	14	23	0	0	0	0	0	0	20	0	0	20	43	
% App. Total	0	0	0	0	0	0	0	39.1	60.9		0	0	0	0	0	0	100	0	0			
PHF	.000	.000	.000	.000	.000	.000	.000	.563	.583	.719	.000	.000	.000	.000	.000	.000	.625	.000	.000	.625	.768	

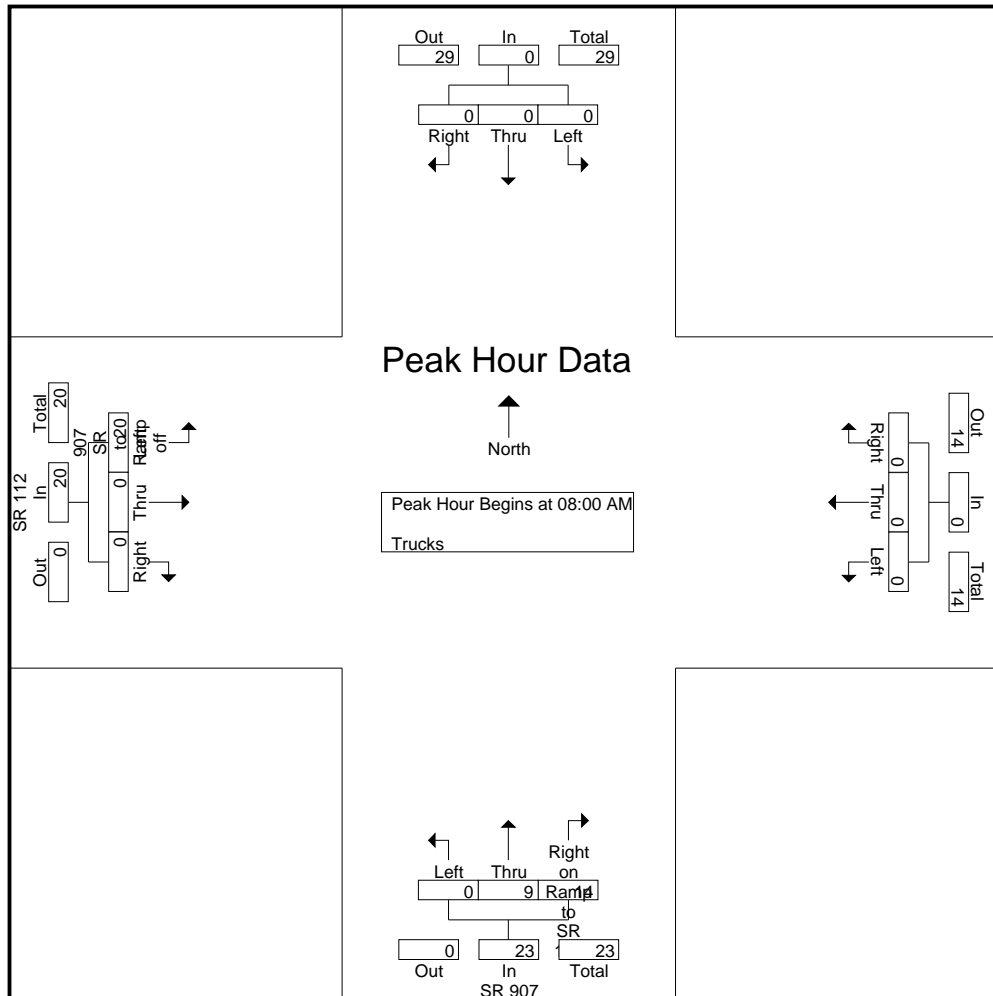
SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	Southbound					SR 907 Northbound					Westbound					SR 112 Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right on Ramp to SR 112	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left off Ramp to SR 907	Thru	Right	App. Total		
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 03:00 PM																						
03:00 PM	0	0	0	0	0	0	0	2	5	7	0	0	0	0	0	0	2	0	0	2	9	
03:15 PM	0	0	0	0	0	0	0	0	4	4	0	0	0	0	0	0	1	0	0	1	5	
03:30 PM	0	0	0	0	0	0	0	0	9	9	0	0	0	0	0	0	3	0	0	3	12	
03:45 PM	0	0	0	0	0	0	0	3	6	9	0	0	0	0	0	0	5	0	0	5	14	
Total Volume	0	0	0	0	0	0	0	5	24	29	0	0	0	0	0	0	11	0	0	11	40	
% App. Total	0	0	0	0	0	0	0	17.2	82.8		0	0	0	0	0	0	100	0	0			
PHF	.000	.000	.000	.000	.000	.000	.000	.417	.667	.806	.000	.000	.000	.000	.000	.000	.550	.000	.000	.550	.714	

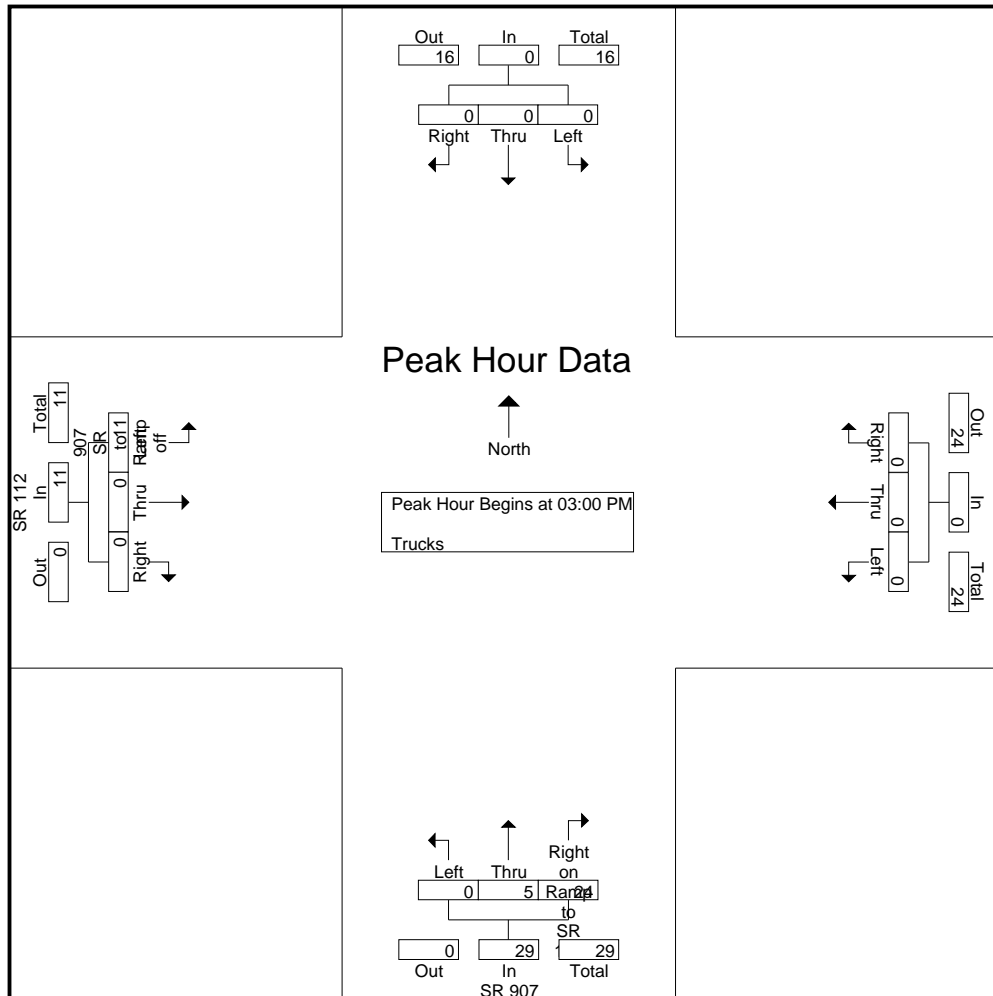
SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	Southbound					SR 907 Northbound					Westbound					SR 112 Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right on Ramp to SR 112	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left off Ramp to SR 907	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	0	0	49	158	207	0	0	0	0	0	0	314	0	0	314	521
07:15 AM	0	0	0	0	0	0	0	57	200	257	0	0	0	0	0	0	284	0	0	284	541
07:30 AM	0	0	0	0	0	0	0	39	169	208	0	0	0	0	0	0	325	0	0	325	533
07:45 AM	0	0	0	0	0	0	0	84	179	263	0	0	0	0	0	0	328	0	0	328	591
Total	0	0	0	0	0	0	0	229	706	935	0	0	0	0	0	0	1251	0	0	1251	2186
08:00 AM	0	0	0	0	0	0	0	71	191	262	0	0	0	0	0	0	327	0	0	327	589
08:15 AM	0	0	0	0	0	0	0	90	194	284	0	0	0	0	0	0	286	0	0	286	570
08:30 AM	0	0	0	0	0	0	0	91	203	294	0	0	0	0	0	0	310	0	0	310	604
08:45 AM	0	0	0	0	0	0	0	100	182	282	0	0	0	0	0	0	326	0	0	326	608
Total	0	0	0	0	0	0	0	352	770	1122	0	0	0	0	0	0	1249	0	0	1249	2371
*** BREAK ***																					
03:00 PM	0	0	0	0	0	0	0	106	381	487	0	0	0	0	0	0	242	0	0	242	729
03:15 PM	0	0	0	0	0	0	0	97	412	509	0	0	0	0	0	0	209	0	0	209	718
03:30 PM	0	0	0	0	0	0	0	93	422	515	0	0	0	0	0	0	212	0	0	212	727
03:45 PM	0	0	0	0	0	0	0	107	332	439	0	0	0	0	0	0	223	0	0	223	662
Total	0	0	0	0	0	0	0	403	1547	1950	0	0	0	0	0	0	886	0	0	886	2836
04:00 PM	0	0	0	0	0	0	0	120	406	526	0	0	0	0	0	0	223	0	0	223	749
04:15 PM	0	0	0	0	0	0	0	118	393	511	0	0	0	0	0	0	240	0	0	240	751
04:30 PM	0	0	0	0	0	0	0	120	370	490	0	0	0	0	0	0	234	0	0	234	724
04:45 PM	0	0	0	0	0	0	0	114	340	454	0	0	0	0	0	0	236	0	0	236	690
Total	0	0	0	0	0	0	0	472	1509	1981	0	0	0	0	0	0	933	0	0	933	2914
05:00 PM	0	0	0	0	0	0	0	117	347	464	0	0	0	0	0	0	263	0	0	263	727
05:15 PM	0	0	0	0	0	0	0	132	371	503	0	0	0	0	0	0	269	0	0	269	772
05:30 PM	0	0	0	0	0	0	0	116	349	465	0	0	0	0	0	0	286	0	0	286	751
05:45 PM	0	0	0	0	0	0	0	116	335	451	0	0	0	0	0	0	247	0	0	247	698
Total	0	0	0	0	0	0	0	481	1402	1883	0	0	0	0	0	0	1065	0	0	1065	2948
Grand Total	0	0	0	0	0	0	0	1937	5934	7871	0	0	0	0	0	0	5384	0	0	5384	13255
Apprch %	0	0	0	0	0	0	0	24.6	75.4		0	0	0	0	0	0	100	0	0		
Total %	0	0	0	0	0	0	0	14.6	44.8	59.4	0	0	0	0	0	0	40.6	0	0	40.6	
Vehicle	0	0	0	0	0	0	0	1909	5859	7768	0	0	0	0	0	0	5336	0	0	5336	13104
% Vehicle	0	0	0	0	0	0	0	98.6	98.7	98.7	0	0	0	0	0	0	99.1	0	0	99.1	98.9

SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	Southbound					SR 907 Northbound					Westbound					SR 112 Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right on Ramp to SR 112	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left off Ramp to SR 907	Thru	Right	App. Total	
Trucks	0	0	0	0	0	0	0	28	75	103	0	0	0	0	0	0	48	0	0	48	151
% Trucks	0	0	0	0	0	0	0	1.4	1.3	1.3	0	0	0	0	0	0	0.9	0	0	0.9	1.1

SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4

Start Time	Southbound					SR 907 Northbound					Westbound					SR 112 Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right on Ramp to SR 112	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left off Ramp to SR 907	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	0	0	0	0	0	0	71	191	262	0	0	0	0	0	0	327	0	0	327	589	
08:15 AM	0	0	0	0	0	0	0	90	194	284	0	0	0	0	0	0	286	0	0	286	570	
08:30 AM	0	0	0	0	0	0	0	91	203	294	0	0	0	0	0	0	310	0	0	310	604	
08:45 AM	0	0	0	0	0	0	0	100	182	282	0	0	0	0	0	0	326	0	0	326	608	
Total Volume	0	0	0	0	0	0	0	352	770	1122	0	0	0	0	0	0	1249	0	0	1249	2371	
% App. Total	0	0	0	0	0	0	0	31.4	68.6		0	0	0	0	0	0	100	0	0			
PHF	.000	.000	.000	.000	.000	.000	.000	.880	.948	.954	.000	.000	.000	.000	.000	.000	.955	.000	.000	.955	.975	

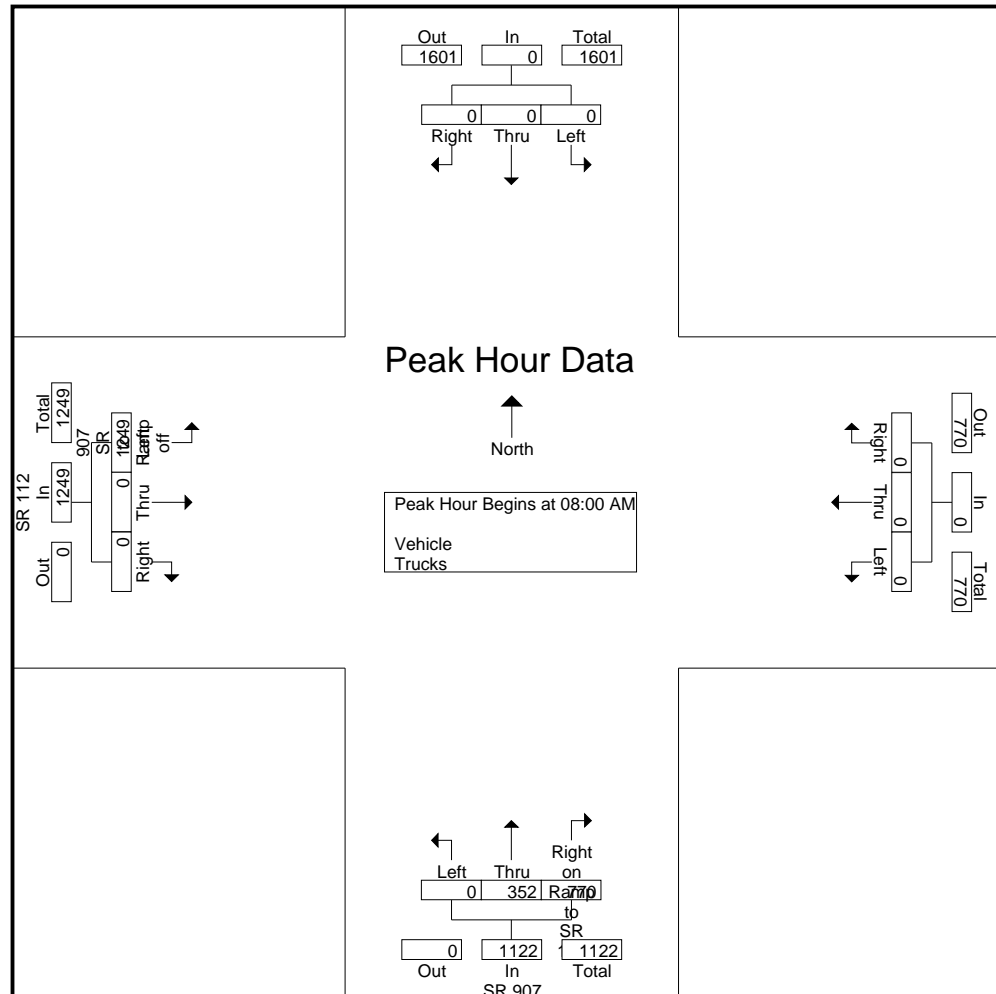
SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)

Site Code : 00000000

Start Date : 10/17/2017

Page No : 5



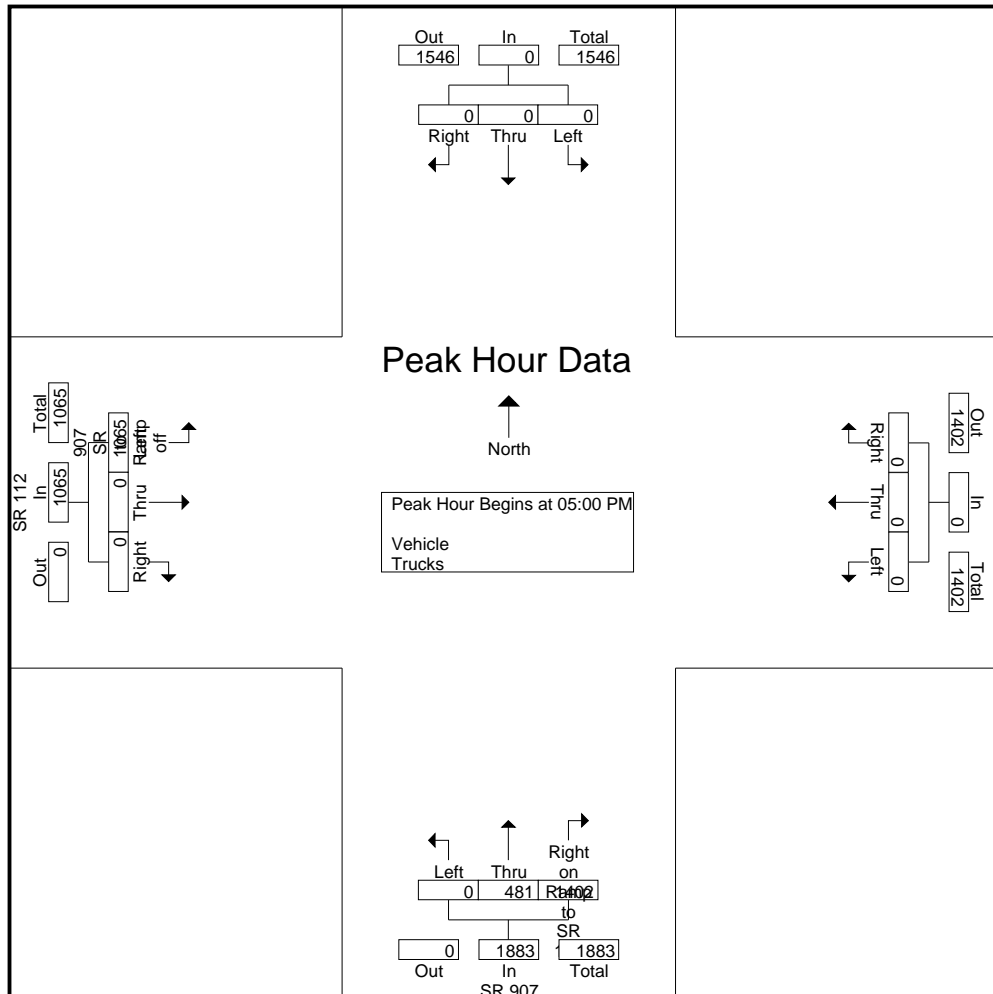
SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6

Start Time	Southbound					SR 907 Northbound					Westbound					SR 112 Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right on Ramp to SR 112	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left off Ramp to SR 907	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 05:00 PM																					
05:00 PM	0	0	0	0	0	0	0	117	347	464	0	0	0	0	0	0	263	0	0	263	727
05:15 PM	0	0	0	0	0	0	0	132	371	503	0	0	0	0	0	0	269	0	0	269	772
05:30 PM	0	0	0	0	0	0	0	116	349	465	0	0	0	0	0	0	286	0	0	286	751
05:45 PM	0	0	0	0	0	0	0	116	335	451	0	0	0	0	0	0	247	0	0	247	698
Total Volume	0	0	0	0	0	0	0	481	1402	1883	0	0	0	0	0	0	1065	0	0	1065	2948
% App. Total	0	0	0	0	0	0	0	25.5	74.5		0	0	0	0	0	0	100	0	0		
PHF	.000	.000	.000	.000	.000	.000	.000	.911	.945	.936	.000	.000	.000	.000	.000	.000	.931	.000	.000	.931	.955

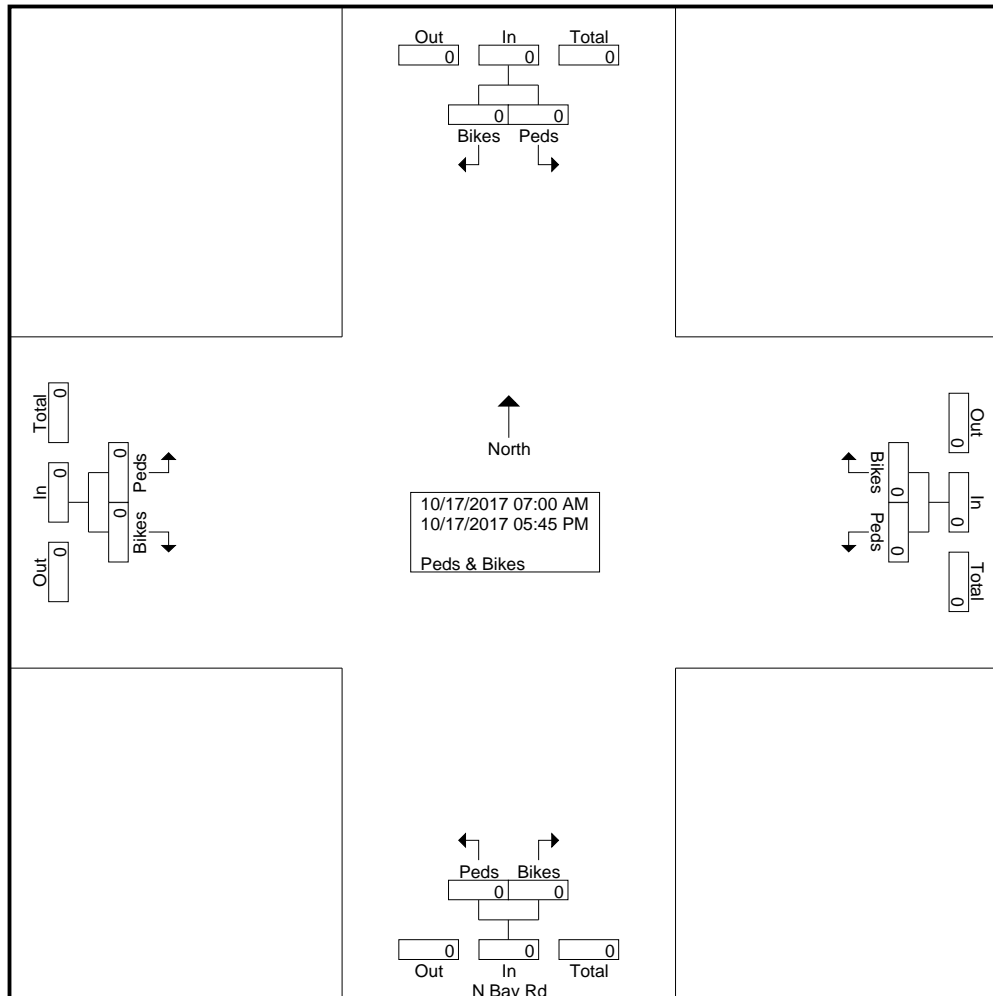
SR 907 & SR 112 (on/off Ramps SR 112)

File Name : TMC-24 SR 907 & SR 112 (Ramps)
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 7



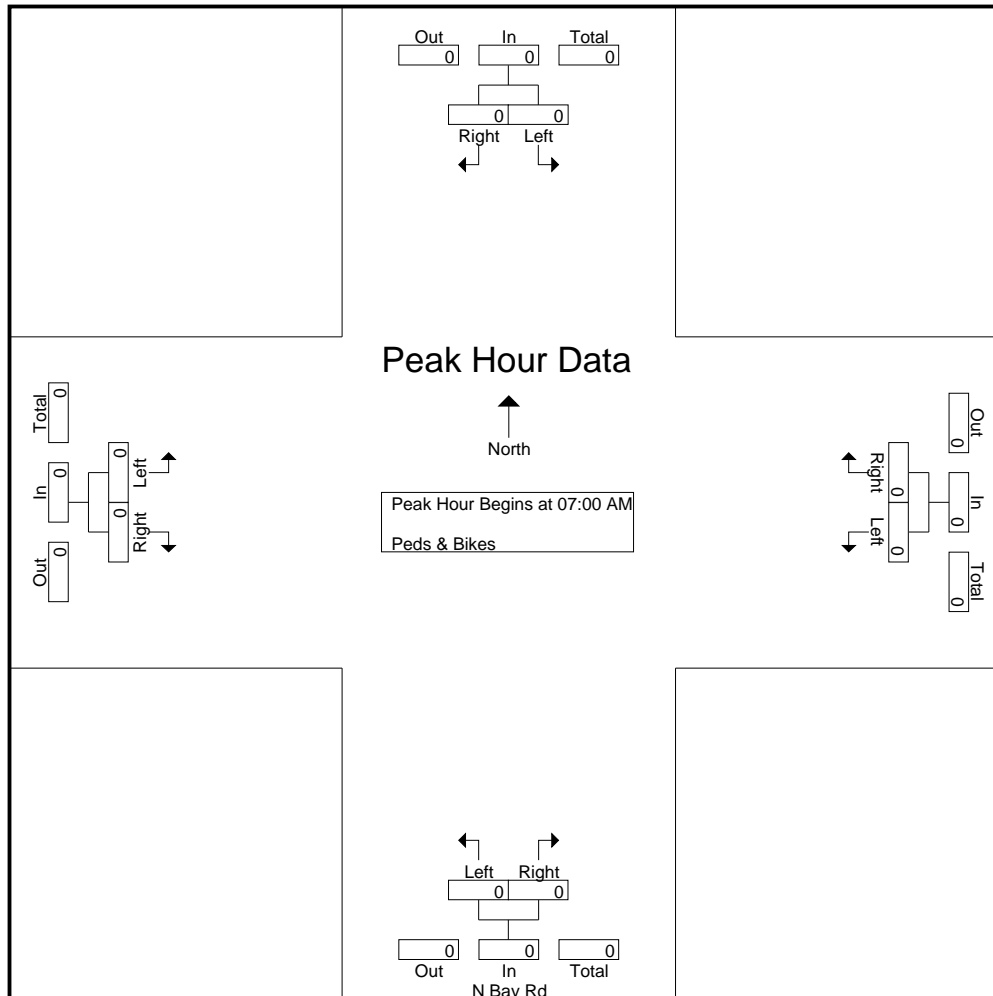
N Bay Road

File Name : TMC-25 N Bay Rd
Site Code : 00000000
Start Date : 10/17/2017
Page No : 2



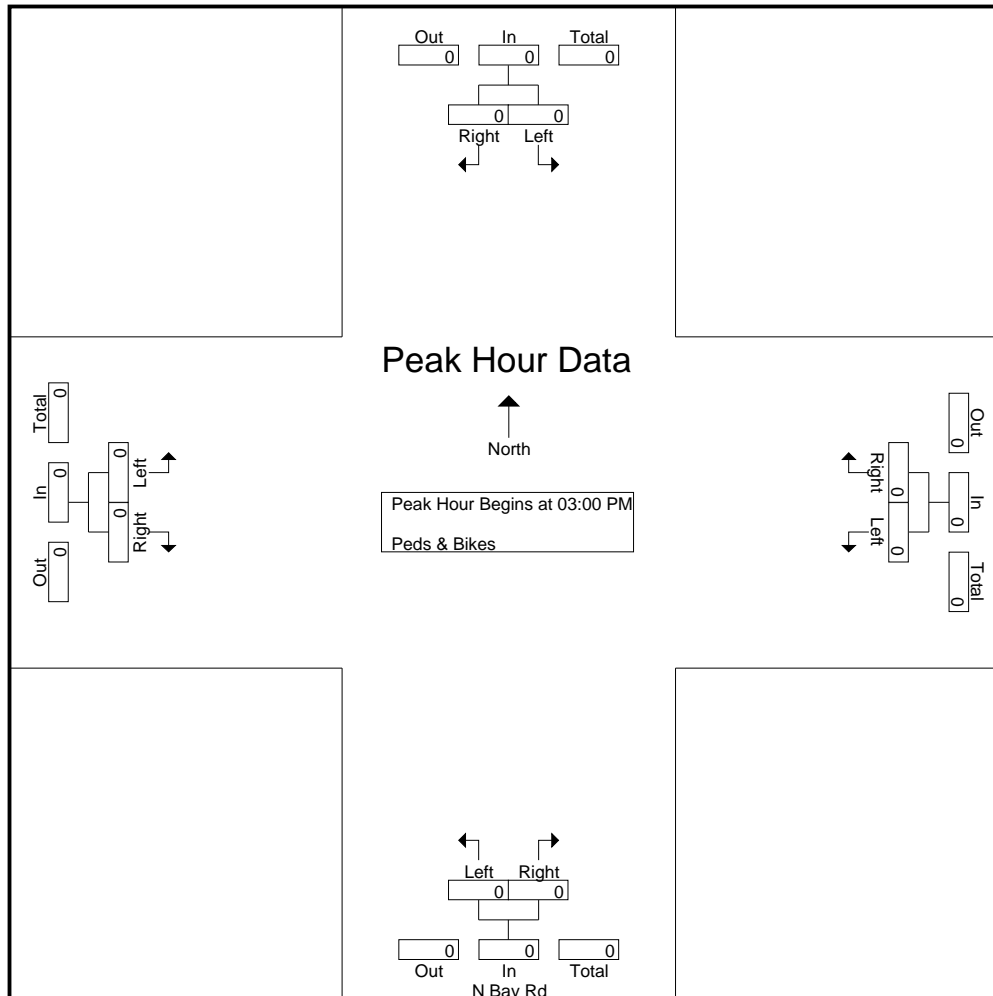
N Bay Road

File Name : TMC-25 N Bay Rd
Site Code : 00000000
Start Date : 10/17/2017
Page No : 4



N Bay Road

File Name : TMC-25 N Bay Rd
Site Code : 00000000
Start Date : 10/17/2017
Page No : 6



N Bay Road

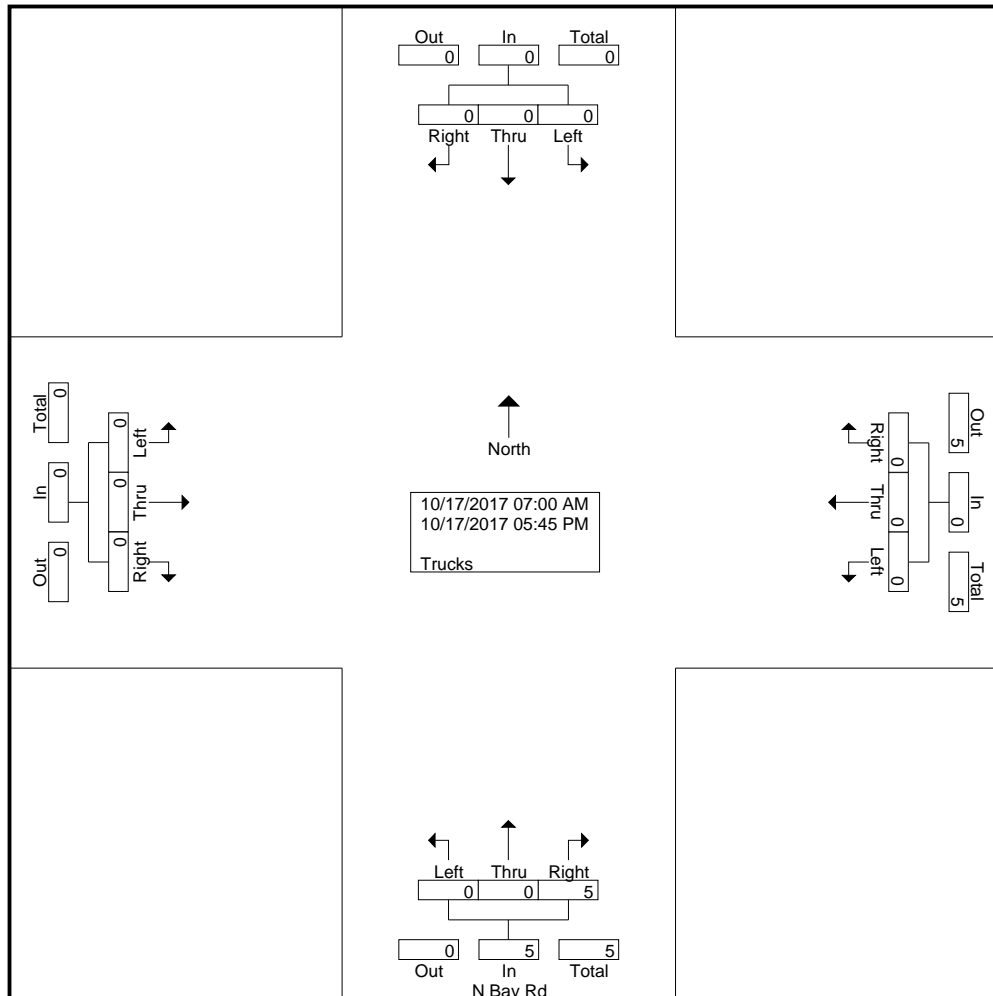
File Name : TMC-25 N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Trucks

Start Time	Southbound					N Bay Rd Northbound					Westbound					Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
07:00 AM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	1	
*** BREAK ***																						
Total	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	1	
*** BREAK ***																						
03:00 PM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	1	
03:15 PM	0	0	0	0	0	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	2	
03:30 PM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	1	
*** BREAK ***																						
Total	0	0	0	0	0	0	0	0	4	4	0	0	0	0	0	0	0	0	0	0	4	
*** BREAK ***																						
Grand Total	0	0	0	0	0	0	0	0	5	5	0	0	0	0	0	0	0	0	0	0	5	
Apprch %	0	0	0	0	0	0	0	0	100	100	0	0	0	0	0	0	0	0	0	0		
Total %	0	0	0	0	0	0	0	0	100	100	0	0	0	0	0	0	0	0	0	0		

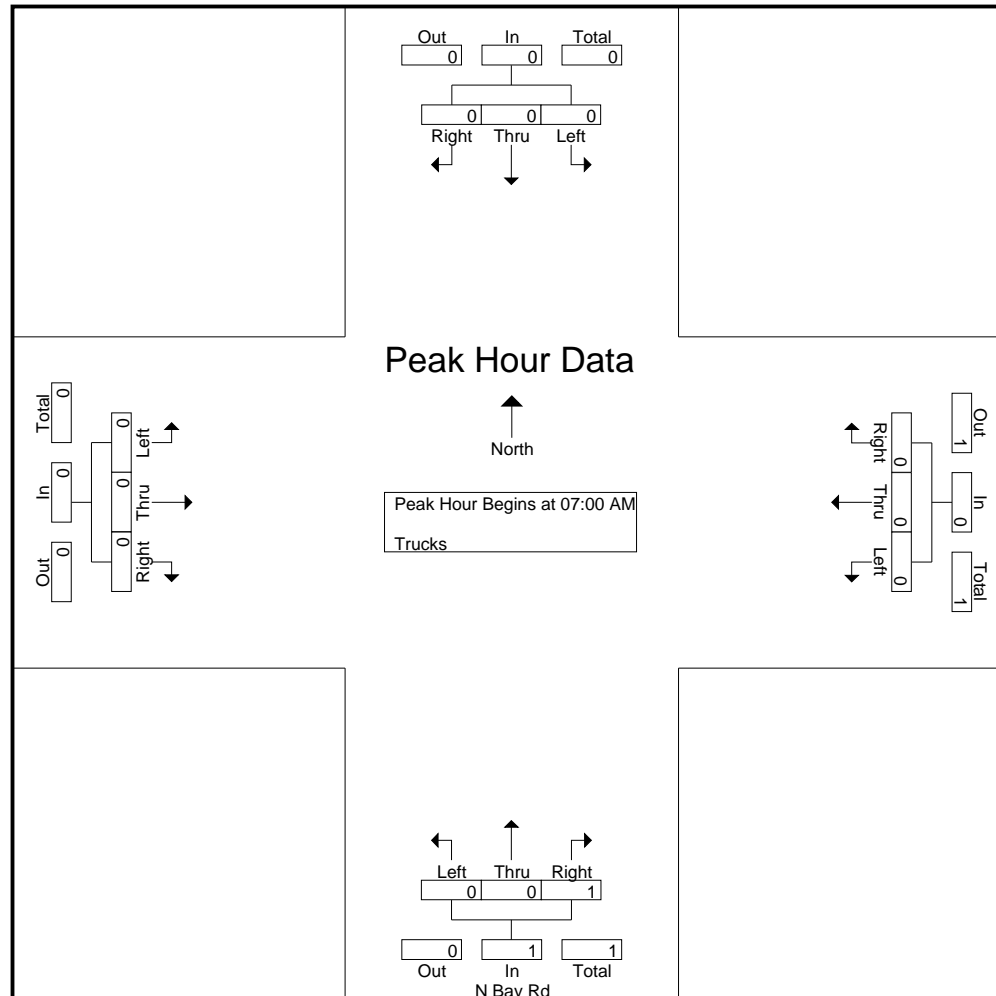
N Bay Road

File Name : TMC-25 N Bay Rd
Site Code : 00000000
Start Date : 10/17/2017
Page No : 2



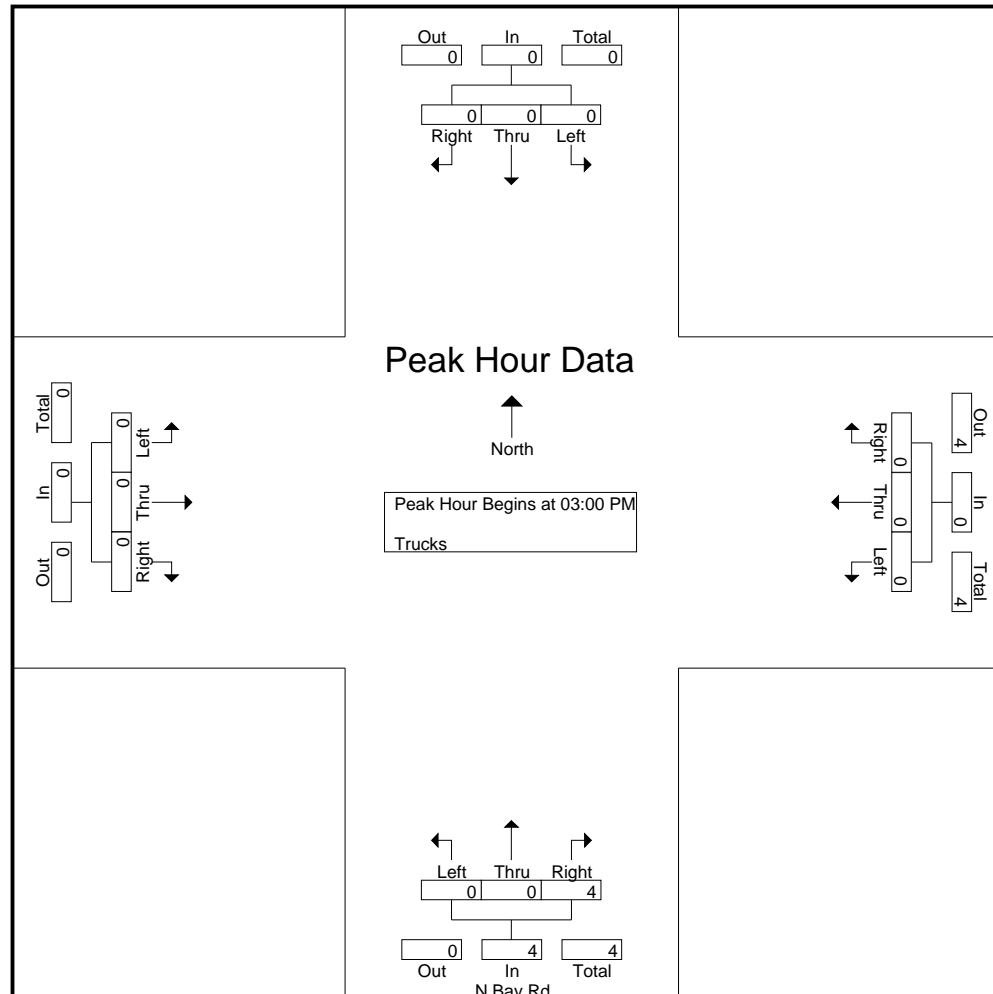
N Bay Road

File Name : TMC-25 N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4



N Bay Road

File Name : TMC-25 N Bay Rd
Site Code : 00000000
Start Date : 10/17/2017
Page No : 6



N Bay Road

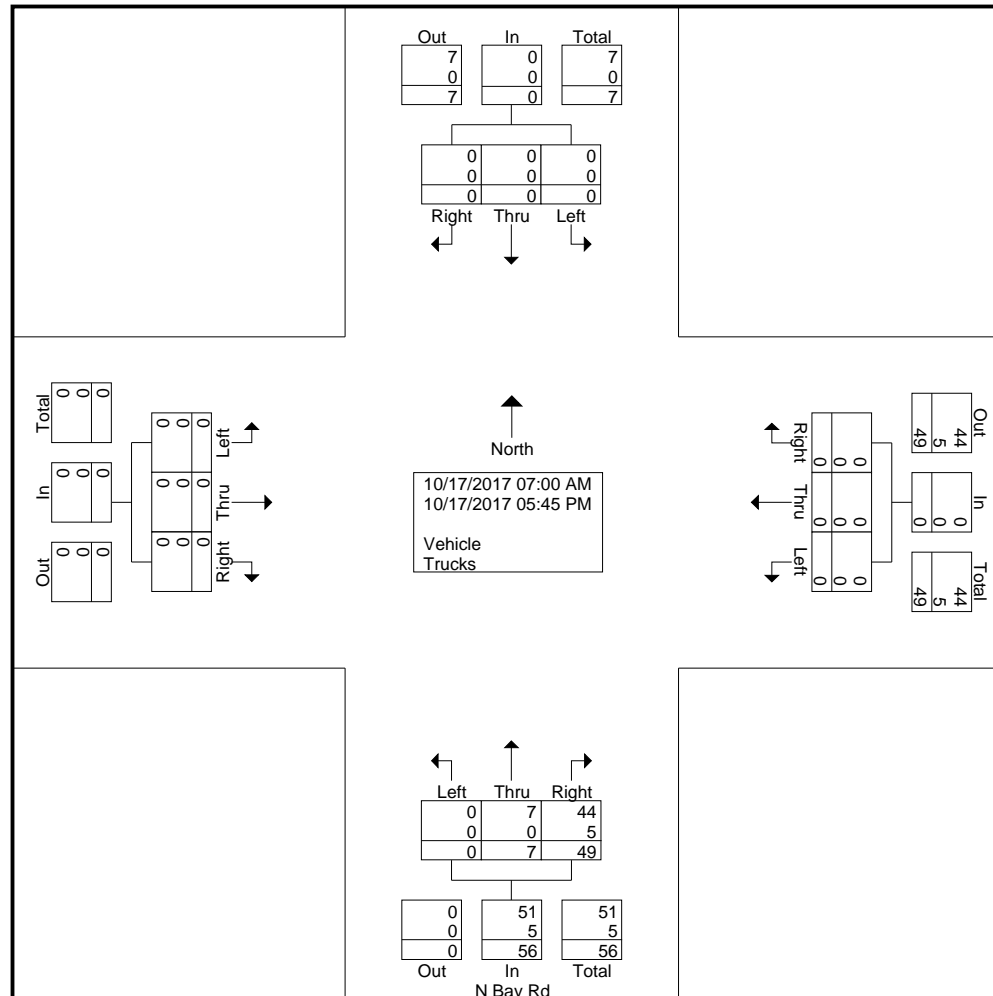
File Name : TMC-25 N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	Southbound					N Bay Rd Northbound					Westbound					Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
07:00 AM	0	0	0	0	0	0	0	0	3	3	0	0	0	0	0	0	0	0	0	0	0	3
*** BREAK ***																						
07:30 AM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	1
07:45 AM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	0	0	0	0	0	0	0	5	5	0	0	0	0	0	0	0	0	0	0	0	5
08:00 AM	0	0	0	0	0	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	2
08:15 AM	0	0	0	0	0	0	0	0	5	5	0	0	0	0	0	0	0	0	0	0	0	5
08:30 AM	0	0	0	0	0	0	0	0	3	3	0	0	0	0	0	0	0	0	0	0	0	3
*** BREAK ***																						
Total	0	0	0	0	0	0	0	0	10	10	0	0	0	0	0	0	0	0	0	0	0	10
*** BREAK ***																						
03:00 PM	0	0	0	0	0	0	0	0	4	4	0	0	0	0	0	0	0	0	0	0	0	4
03:15 PM	0	0	0	0	0	0	0	1	6	7	0	0	0	0	0	0	0	0	0	0	0	7
03:30 PM	0	0	0	0	0	0	0	0	6	6	0	0	0	0	0	0	0	0	0	0	0	6
03:45 PM	0	0	0	0	0	0	0	2	1	3	0	0	0	0	0	0	0	0	0	0	0	3
Total	0	0	0	0	0	0	0	3	17	20	0	0	0	0	0	0	0	0	0	0	0	20
04:00 PM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	1
04:15 PM	0	0	0	0	0	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	0	3
04:30 PM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	1
04:45 PM	0	0	0	0	0	0	0	2	3	5	0	0	0	0	0	0	0	0	0	0	0	5
Total	0	0	0	0	0	0	0	3	7	10	0	0	0	0	0	0	0	0	0	0	0	10
05:00 PM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	1
05:15 PM	0	0	0	0	0	0	0	1	3	4	0	0	0	0	0	0	0	0	0	0	0	4
05:30 PM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	1
05:45 PM	0	0	0	0	0	0	0	0	5	5	0	0	0	0	0	0	0	0	0	0	0	5
Total	0	0	0	0	0	0	0	1	10	11	0	0	0	0	0	0	0	0	0	0	0	11
Grand Total	0	0	0	0	0	0	0	7	49	56	0	0	0	0	0	0	0	0	0	0	0	56
Apprch %	0	0	0	0	0	0	0	12.5	87.5		0	0	0	0	0	0	0	0	0	0	0	
Total %	0	0	0	0	0	0	0	12.5	87.5	100	0	0	0	0	0	0	0	0	0	0	0	
Vehicle	0	0	0	0	0	0	0	7	44	51	0	0	0	0	0	0	0	0	0	0	0	51
% Vehicle	0	0	0	0	0	0	0	100	89.8	91.1	0	0	0	0	0	0	0	0	0	0	0	91.1

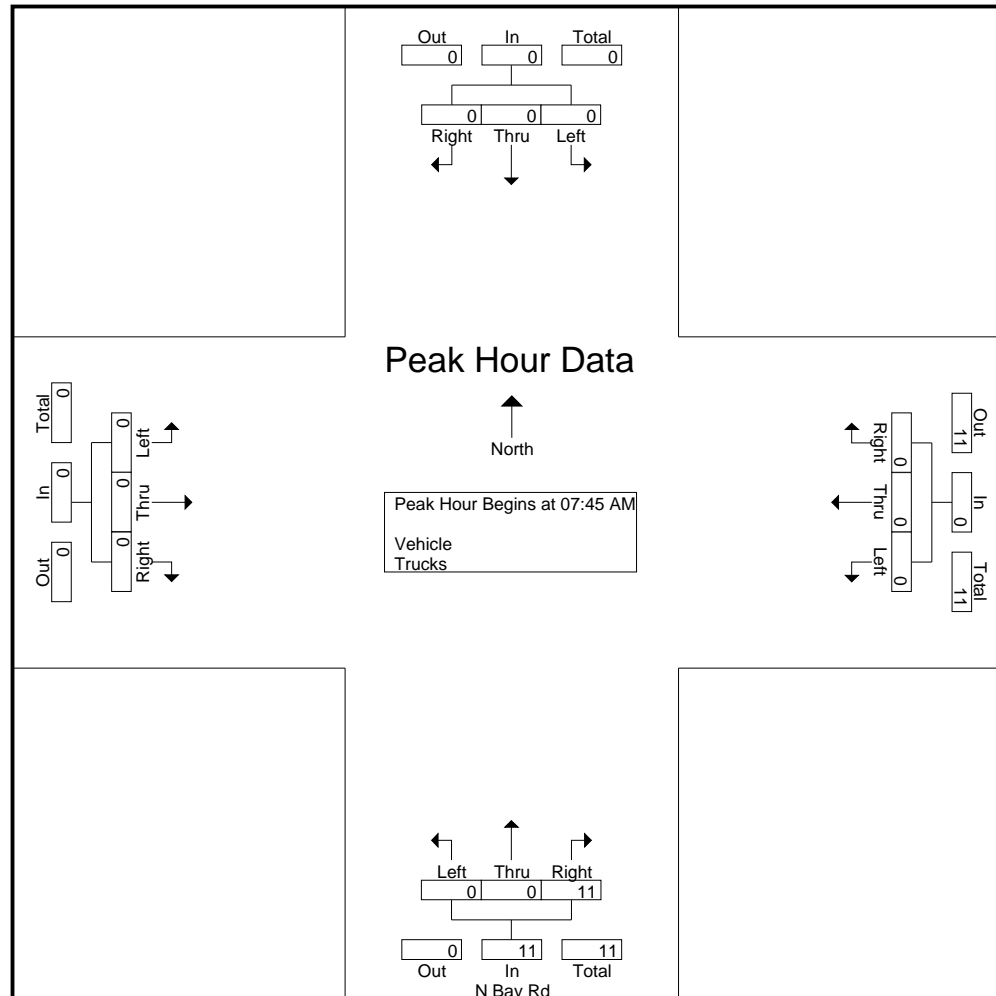
N Bay Road

File Name : TMC-25 N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3



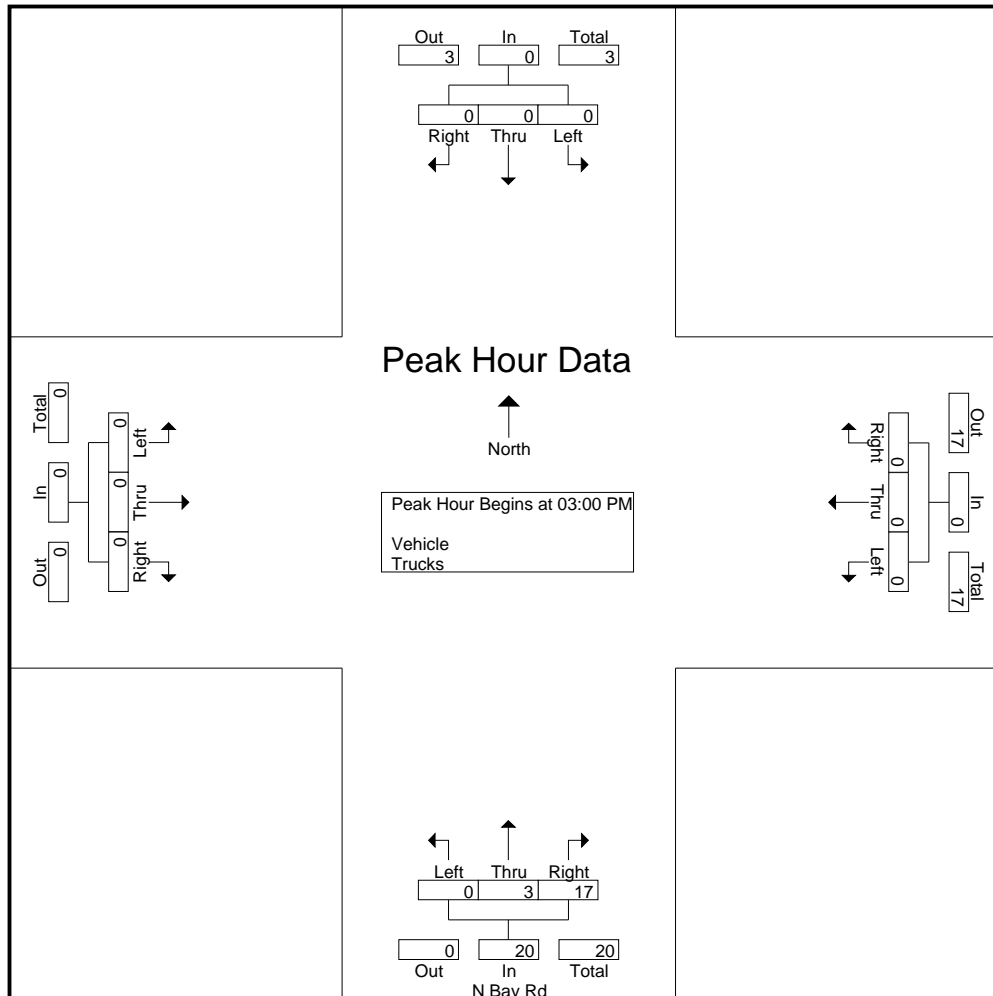
N Bay Road

File Name : TMC-25 N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5



N Bay Road

File Name : TMC-25 N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 7



Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street

Site Code : 00000000

Start Date : 10/17/2017

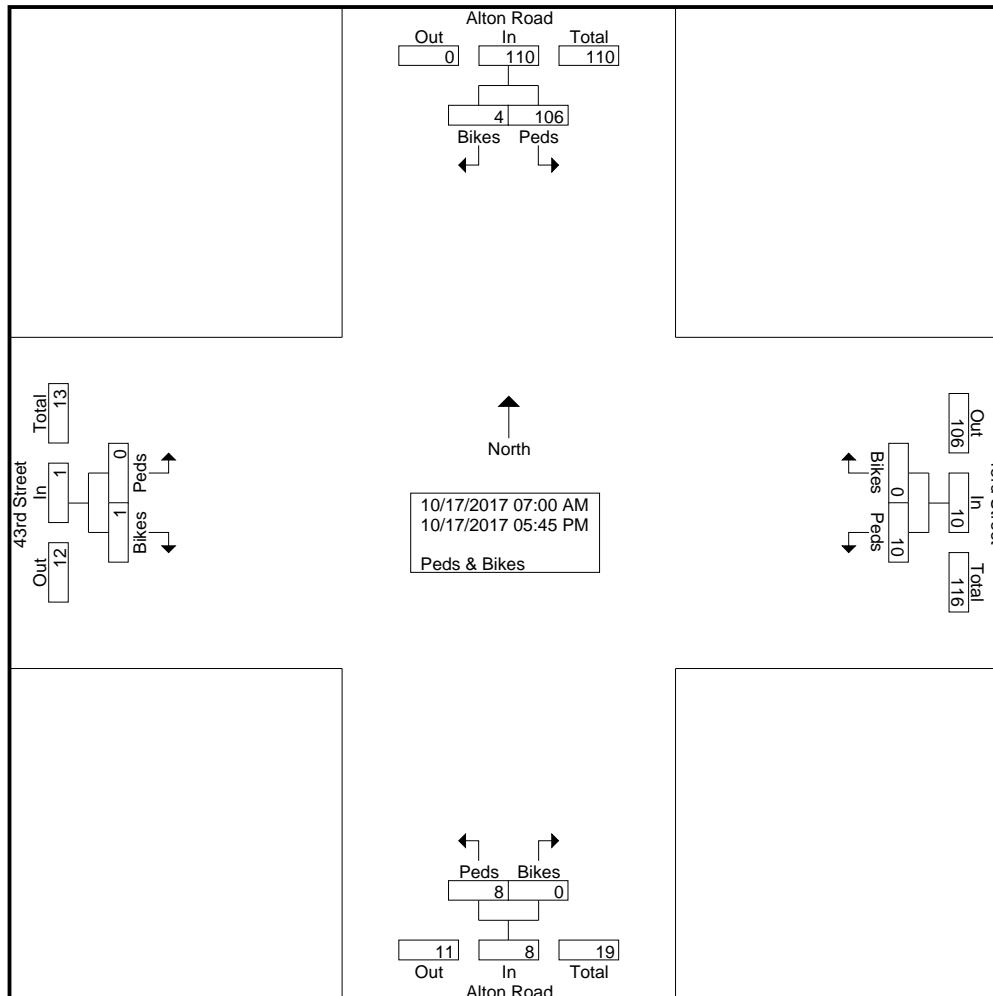
Page No : 1

Groups Printed- Peds & Bikes

Start Time	Alton Road Southbound			Alton Road Northbound			43rd Street Westbound			43rd Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	0	0	0	0	0	0	1	0	1	0	0	0	1
07:15 AM	2	1	3	0	0	0	0	0	0	0	0	0	3
07:30 AM	5	0	5	0	0	0	0	0	0	0	0	0	5
07:45 AM	7	0	7	1	0	1	0	0	0	0	0	0	8
Total	14	1	15	1	0	1	1	0	1	0	0	0	17
08:00 AM	2	0	2	0	0	0	0	0	0	0	0	0	2
08:15 AM	5	0	5	0	0	0	3	0	3	0	0	0	8
08:30 AM	2	0	2	0	0	0	0	0	0	0	0	0	2
08:45 AM	1	0	1	0	0	0	1	0	1	0	0	0	2
Total	10	0	10	0	0	0	4	0	4	0	0	0	14
*** BREAK ***													
03:00 PM	4	0	4	0	0	0	1	0	1	0	0	0	5
03:15 PM	7	0	7	2	0	2	1	0	1	0	0	0	10
03:30 PM	10	0	10	1	0	1	0	0	0	0	0	0	11
03:45 PM	5	2	7	0	0	0	1	0	1	0	1	1	9
Total	26	2	28	3	0	3	3	0	3	0	1	1	35
04:00 PM	14	0	14	2	0	2	0	0	0	0	0	0	16
04:15 PM	6	0	6	0	0	0	1	0	1	0	0	0	7
04:30 PM	9	0	9	0	0	0	0	0	0	0	0	0	9
04:45 PM	8	0	8	0	0	0	0	0	0	0	0	0	8
Total	37	0	37	2	0	2	1	0	1	0	0	0	40
05:00 PM	8	0	8	0	0	0	0	0	0	0	0	0	8
05:15 PM	2	0	2	2	0	2	0	0	0	0	0	0	4
05:30 PM	5	0	5	0	0	0	1	0	1	0	0	0	6
05:45 PM	4	1	5	0	0	0	0	0	0	0	0	0	5
Total	19	1	20	2	0	2	1	0	1	0	0	0	23
Grand Total	106	4	110	8	0	8	10	0	10	0	1	1	129
Aprch %	96.4	3.6		100	0		100	0		0	100		
Total %	82.2	3.1	85.3	6.2	0	6.2	7.8	0	7.8	0	0.8	0.8	

Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 2



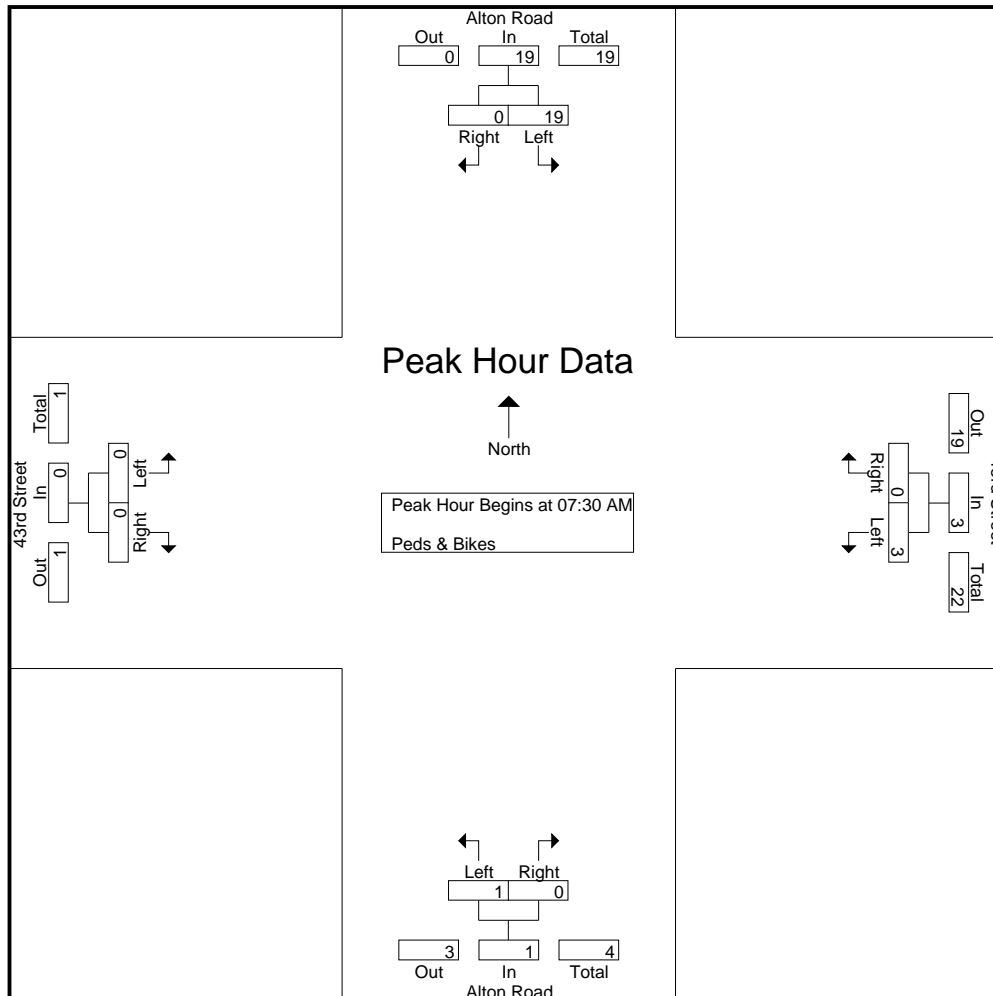
Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Alton Road Southbound			Alton Road Northbound			43rd Street Westbound			43rd Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:30 AM													
07:30 AM	5	0	5	0	0	0	0	0	0	0	0	0	5
07:45 AM	7	0	7	1	0	1	0	0	0	0	0	0	8
08:00 AM	2	0	2	0	0	0	0	0	0	0	0	0	2
08:15 AM	5	0	5	0	0	0	3	0	3	0	0	0	8
Total Volume	19	0	19	1	0	1	3	0	3	0	0	0	23
% App. Total	100	0		100	0		100	0		0	0		
PHF	.679	.000	.679	.250	.000	.250	.250	.000	.250	.000	.000	.000	.719

Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4



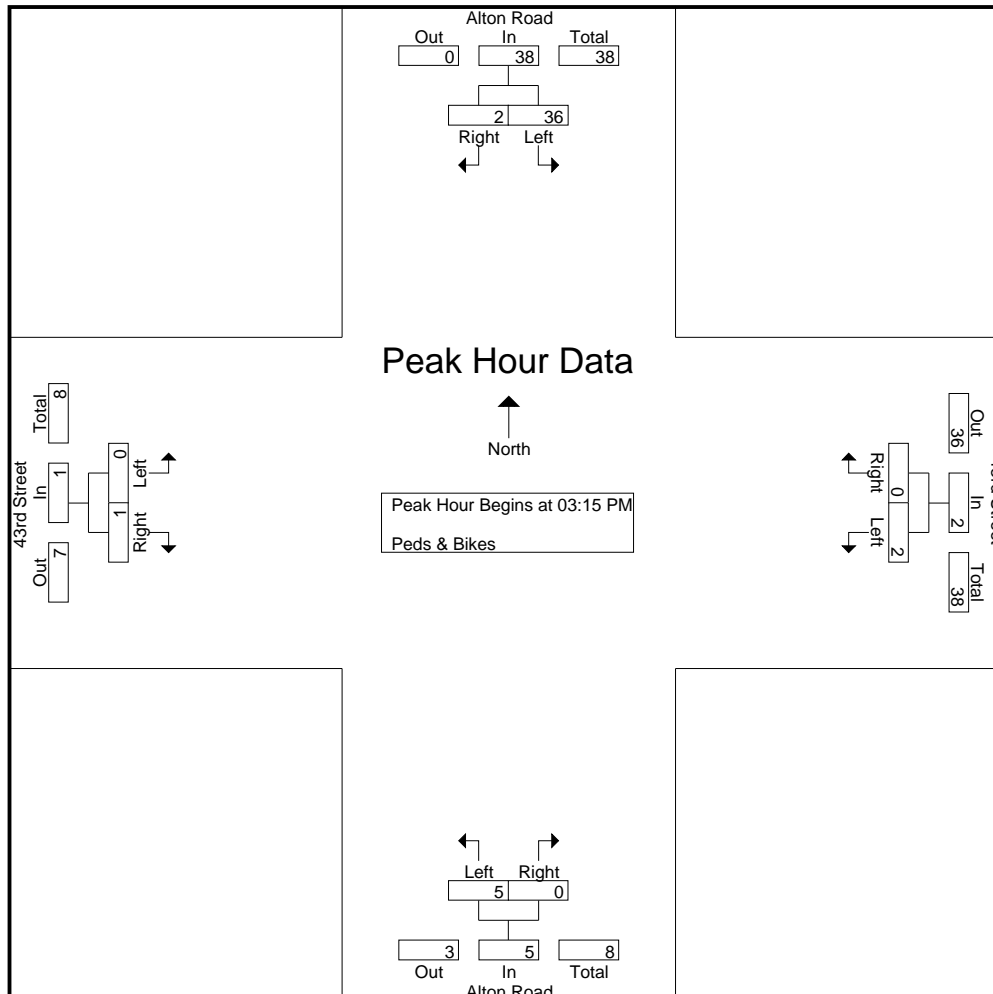
Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	Alton Road Southbound			Alton Road Northbound			43rd Street Westbound			43rd Street Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 03:15 PM													
03:15 PM	7	0	7	2	0	2	1	0	1	0	0	0	10
03:30 PM	10	0	10	1	0	1	0	0	0	0	0	0	11
03:45 PM	5	2	7	0	0	0	1	0	1	0	1	1	9
04:00 PM	14	0	14	2	0	2	0	0	0	0	0	0	16
Total Volume	36	2	38	5	0	5	2	0	2	0	1	1	46
% App. Total	94.7	5.3		100	0		100	0		0	100		
PHF	.643	.250	.679	.625	.000	.625	.500	.000	.500	.000	.250	.250	.719

Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6



Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 1

Groups Printed- Trucks

Start Time	Alton Road Southbound					Alton Road Northbound					43rd Street Westbound					43rd Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	1	1	0	2	0	1	7	1	9	0	1	2	2	5	0	0	3	0	3	19
07:15 AM	0	0	1	0	1	0	0	6	0	6	0	0	4	1	5	0	0	4	0	4	16
07:30 AM	0	0	2	0	2	0	2	9	0	11	0	0	3	2	5	0	2	1	1	4	22
07:45 AM	0	0	0	0	0	0	2	10	0	12	0	2	2	1	5	0	0	5	1	6	23
Total	0	1	4	0	5	0	5	32	1	38	0	3	11	6	20	0	2	13	2	17	80
08:00 AM	0	0	1	0	1	0	1	13	0	14	0	0	2	3	5	0	0	2	0	2	22
08:15 AM	0	0	2	0	2	0	2	6	0	8	0	0	5	1	6	0	1	1	0	2	18
08:30 AM	0	1	2	1	4	0	6	11	0	17	0	1	1	0	2	0	0	4	0	4	27
08:45 AM	0	0	2	0	2	0	1	10	0	11	0	1	4	2	7	0	0	3	0	3	23
Total	0	1	7	1	9	0	10	40	0	50	0	2	12	6	20	0	1	10	0	11	90
*** BREAK ***																					
03:00 PM	0	1	10	2	13	0	4	8	2	14	0	3	1	0	4	0	2	5	1	8	39
03:15 PM	0	1	12	0	13	0	2	8	2	12	0	1	3	1	5	0	0	5	2	7	37
03:30 PM	0	0	5	2	7	0	0	2	1	3	0	0	1	0	1	0	0	1	1	2	13
03:45 PM	0	0	1	0	1	0	4	7	1	12	0	0	2	0	2	0	1	1	1	3	18
Total	0	2	28	4	34	0	10	25	6	41	0	4	7	1	12	0	3	12	5	20	107
04:00 PM	0	0	9	1	10	0	0	2	0	2	0	0	1	0	1	0	1	4	0	5	18
04:15 PM	0	0	8	1	9	0	1	3	0	4	0	1	4	0	5	0	0	2	0	2	20
04:30 PM	0	0	9	0	9	0	1	0	0	1	0	1	2	0	3	0	0	4	0	4	17
04:45 PM	0	1	12	2	15	0	2	3	0	5	0	0	1	0	1	0	1	2	0	3	24
Total	0	1	38	4	43	0	4	8	0	12	0	2	8	0	10	0	2	12	0	14	79
05:00 PM	0	0	3	1	4	0	1	1	0	2	0	1	1	0	2	0	0	3	0	3	11
05:15 PM	0	0	7	0	7	0	1	3	0	4	0	0	3	0	3	0	0	2	1	3	17
05:30 PM	0	0	5	1	6	0	1	0	0	1	0	2	2	0	4	0	1	3	0	4	15
05:45 PM	0	0	4	0	4	0	1	0	1	2	0	0	2	2	4	0	0	3	1	4	14
Total	0	0	19	2	21	0	4	4	1	9	0	3	8	2	13	0	1	11	2	14	57
Grand Total	0	5	96	11	112	0	33	109	8	150	0	14	46	15	75	0	9	58	9	76	413
Apprch %	0	4.5	85.7	9.8		0	22	72.7	5.3		0	18.7	61.3	20		0	11.8	76.3	11.8		
Total %	0	1.2	23.2	2.7	27.1	0	8	26.4	1.9	36.3	0	3.4	11.1	3.6	18.2	0	2.2	14	2.2	18.4	

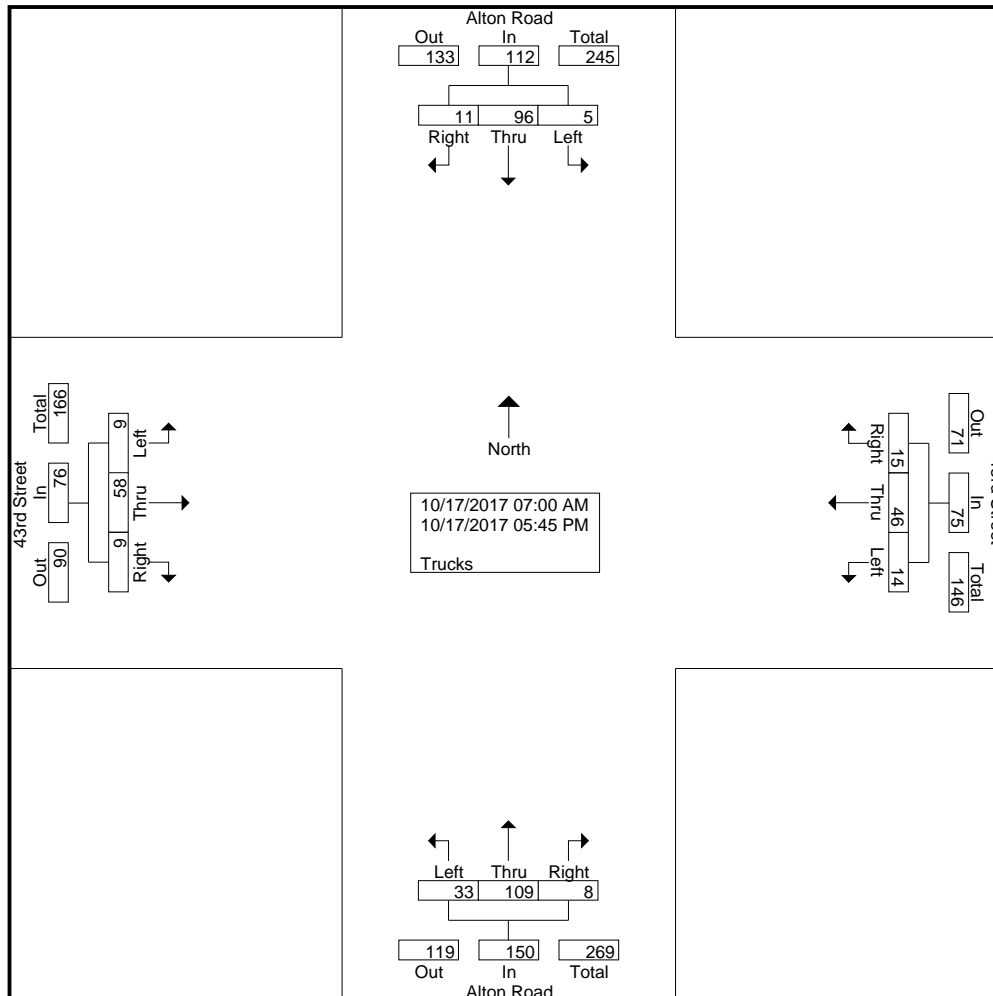
Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



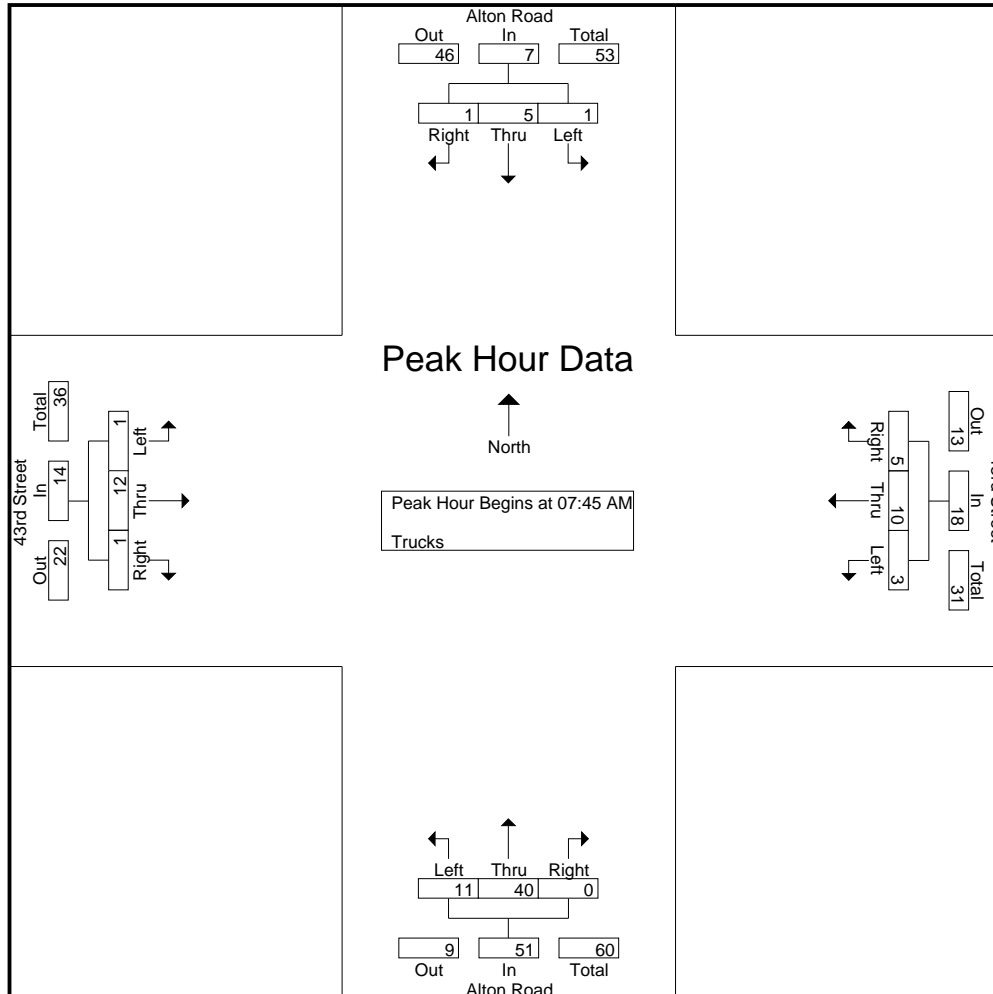
Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Alton Road Southbound					Alton Road Northbound					43rd Street Westbound					43rd Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:45 AM																					
07:45 AM	0	0	0	0	0	0	2	10	0	12	0	2	2	1	5	0	0	5	1	6	23
08:00 AM	0	0	1	0	1	0	1	13	0	14	0	0	2	3	5	0	0	2	0	2	22
08:15 AM	0	0	2	0	2	0	2	6	0	8	0	0	5	1	6	0	1	1	0	2	18
08:30 AM	0	1	2	1	4	0	6	11	0	17	0	1	1	0	2	0	0	4	0	4	27
Total Volume	0	1	5	1	7	0	11	40	0	51	0	3	10	5	18	0	1	12	1	14	90
% App. Total	0	14.3	71.4	14.3		0	21.6	78.4	0		0	16.7	55.6	27.8		0	7.1	85.7	7.1		
PHF	.000	.250	.625	.250	.438	.000	.458	.769	.000	.750	.000	.375	.500	.417	.750	.000	.250	.600	.250	.583	.833

Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4



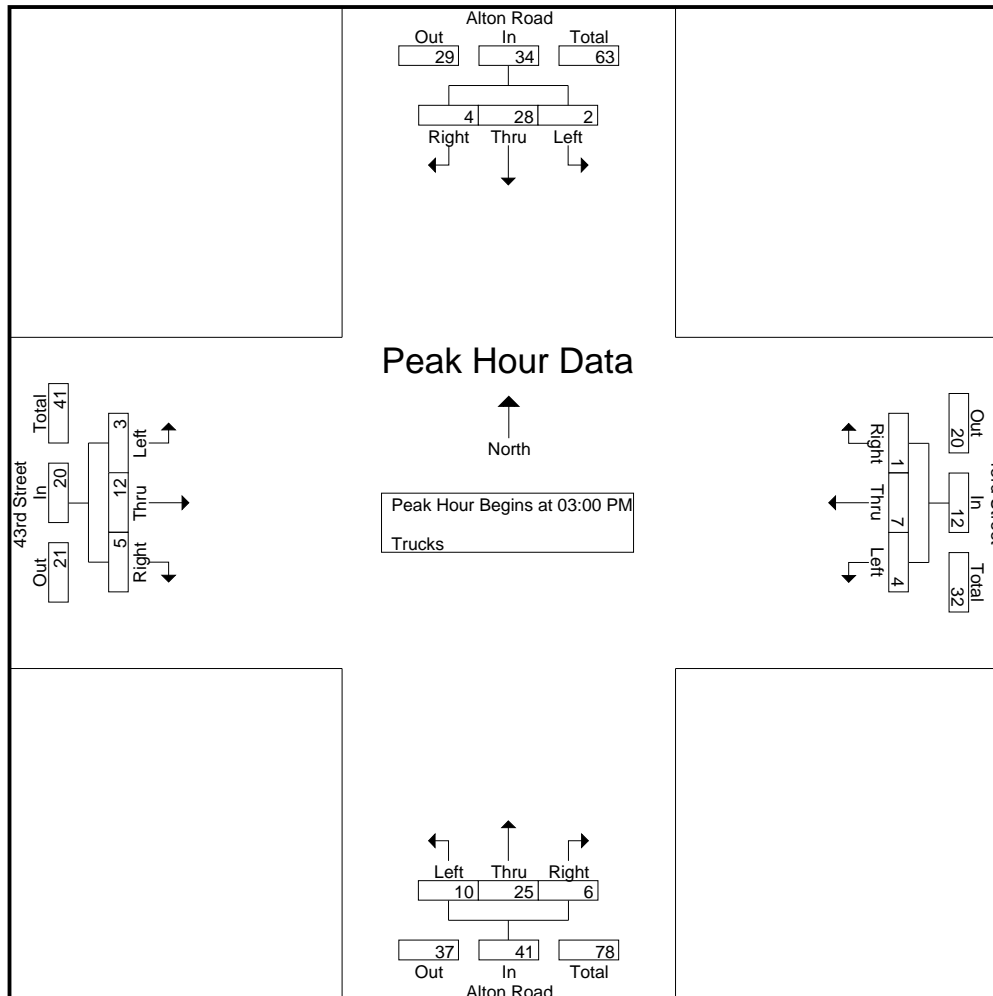
Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	Alton Road Southbound					Alton Road Northbound					43rd Street Westbound					43rd Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:00 PM																					
03:00 PM	0	1	10	2	13	0	4	8	2	14	0	3	1	0	4	0	2	5	1	8	39
03:15 PM	0	1	12	0	13	0	2	8	2	12	0	1	3	1	5	0	0	5	2	7	37
03:30 PM	0	0	5	2	7	0	0	2	1	3	0	0	1	0	1	0	0	1	1	2	13
03:45 PM	0	0	1	0	1	0	4	7	1	12	0	0	2	0	2	0	1	1	1	3	18
Total Volume	0	2	28	4	34	0	10	25	6	41	0	4	7	1	12	0	3	12	5	20	107
% App. Total	0	5.9	82.4	11.8		0	24.4	61	14.6		0	33.3	58.3	8.3		0	15	60	25		
PHF	.000	.500	.583	.500	.654	.000	.625	.781	.750	.732	.000	.333	.583	.250	.600	.000	.375	.600	.625	.625	.686

Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6



Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street

Site Code : 00000000

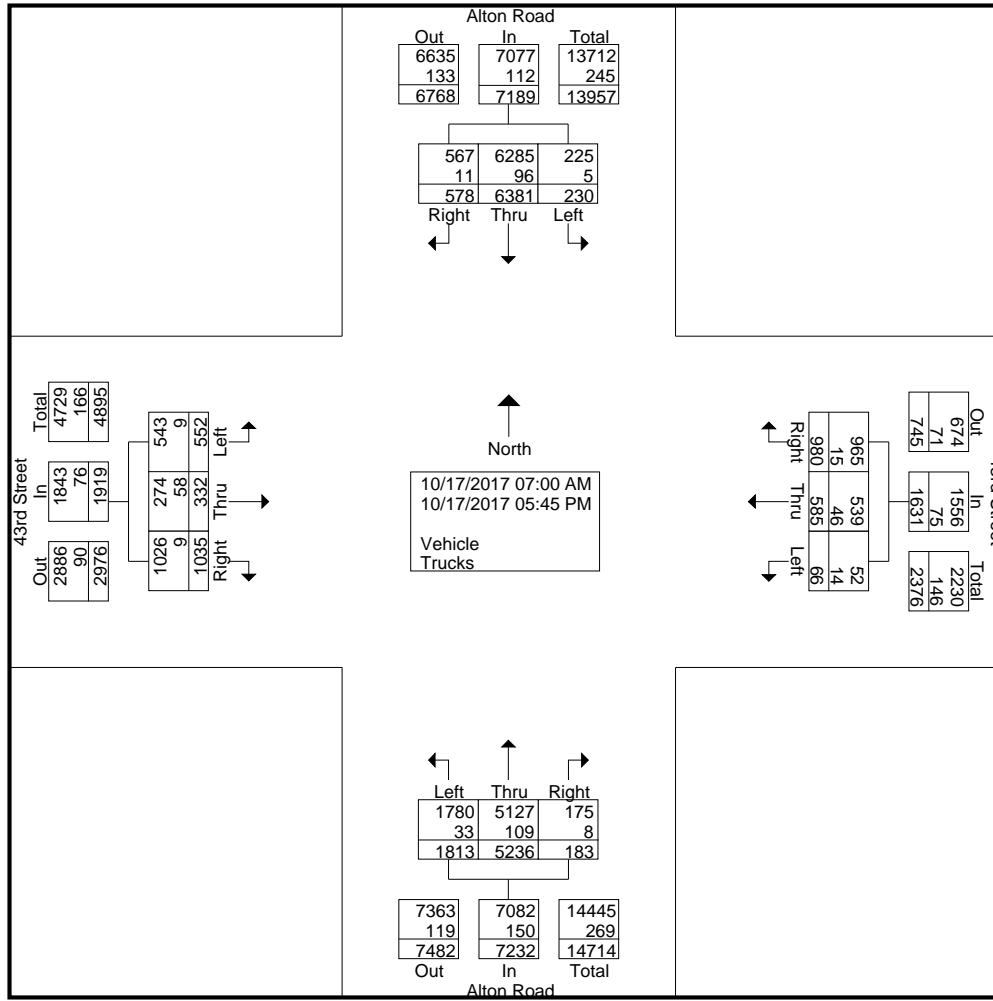
Start Date : 10/17/2017

Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	Alton Road Southbound					Alton Road Northbound					43rd Street Westbound					43rd Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	9	300	34	343	0	189	182	5	376	0	1	44	49	94	0	3	7	16	26	839
07:15 AM	0	5	325	40	370	2	127	181	6	316	0	2	45	48	95	0	5	8	26	39	820
07:30 AM	0	9	338	50	397	0	141	190	7	338	0	1	23	57	81	0	9	4	37	50	866
07:45 AM	0	7	334	53	394	1	154	246	6	407	0	4	38	50	92	0	1	9	32	42	935
Total	0	30	1297	177	1504	3	611	799	24	1437	0	8	150	204	362	0	18	28	111	157	3460
08:00 AM	0	5	372	66	443	0	147	217	5	369	0	3	73	53	129	1	13	11	29	54	995
08:15 AM	0	6	387	61	454	0	175	182	10	367	0	1	55	58	114	0	10	14	39	63	998
08:30 AM	0	9	383	56	448	0	161	250	12	423	0	2	46	37	85	0	15	12	27	54	1010
08:45 AM	0	7	398	46	451	2	156	250	15	423	0	3	35	44	82	0	10	11	33	54	1010
Total	0	27	1540	229	1796	2	639	899	42	1582	0	9	209	192	410	1	48	48	128	225	4013
*** BREAK ***																					
03:00 PM	1	10	280	30	321	5	79	245	8	337	1	5	22	42	70	0	28	20	59	107	835
03:15 PM	0	8	298	24	330	2	66	236	17	321	0	4	34	51	89	0	26	21	73	120	860
03:30 PM	0	15	326	17	358	3	48	234	17	302	0	3	24	56	83	0	52	15	63	130	873
03:45 PM	0	15	358	16	389	2	61	285	11	359	0	3	18	29	50	0	40	20	61	121	919
Total	1	48	1262	87	1398	12	254	1000	53	1319	1	15	98	178	292	0	146	76	256	478	3487
04:00 PM	1	9	318	12	340	7	37	292	9	345	0	5	19	37	61	0	29	20	75	124	870
04:15 PM	1	9	270	14	294	1	48	279	7	335	0	3	20	35	58	0	40	19	74	133	820
04:30 PM	1	17	322	13	353	2	36	291	10	339	0	6	14	40	60	0	50	22	70	142	894
04:45 PM	0	21	215	13	249	5	24	312	5	346	0	4	18	56	78	0	45	25	66	136	809
Total	3	56	1125	52	1236	15	145	1174	31	1365	0	18	71	168	257	0	164	86	285	535	3393
05:00 PM	1	16	267	9	293	2	32	344	9	387	0	3	16	43	62	0	45	32	84	161	903
05:15 PM	2	15	358	7	382	2	20	286	7	315	0	1	14	45	60	0	51	28	67	146	903
05:30 PM	2	19	267	10	298	5	37	359	8	409	1	6	15	73	95	0	50	18	64	132	934
05:45 PM	0	10	265	7	282	3	31	375	9	418	0	4	12	77	93	0	29	16	40	85	878
Total	5	60	1157	33	1255	12	120	1364	33	1529	1	14	57	238	310	0	175	94	255	524	3618
Grand Total	9	221	6381	578	7189	44	1769	5236	183	7232	2	64	585	980	1631	1	551	332	1035	1919	17971
Apprch %	0.1	3.1	88.8	8		0.6	24.5	72.4	2.5		0.1	3.9	35.9	60.1		0.1	28.7	17.3	53.9		
Total %	0.1	1.2	35.5	3.2	40	0.2	9.8	29.1	1	40.2	0	0.4	3.3	5.5	9.1	0	3.1	1.8	5.8	10.7	
Vehicle	9	216	6285	567	7077	44	1736	5127	175	7082	2	50	539	965	1556	1	542	274	1026	1843	17558
% Vehicle	100	97.7	98.5	98.1	98.4	100	98.1	97.9	95.6	97.9	100	78.1	92.1	98.5	95.4	100	98.4	82.5	99.1	96	97.7
Trucks	0	5	96	11	112	0	33	109	8	150	0	14	46	15	75	0	9	58	9	76	413
% Trucks	0	2.3	1.5	1.9	1.6	0	1.9	2.1	4.4	2.1	0	21.9	7.9	1.5	4.6	0	1.6	17.5	0.9	4	2.3

Alton Road & 43rd Street



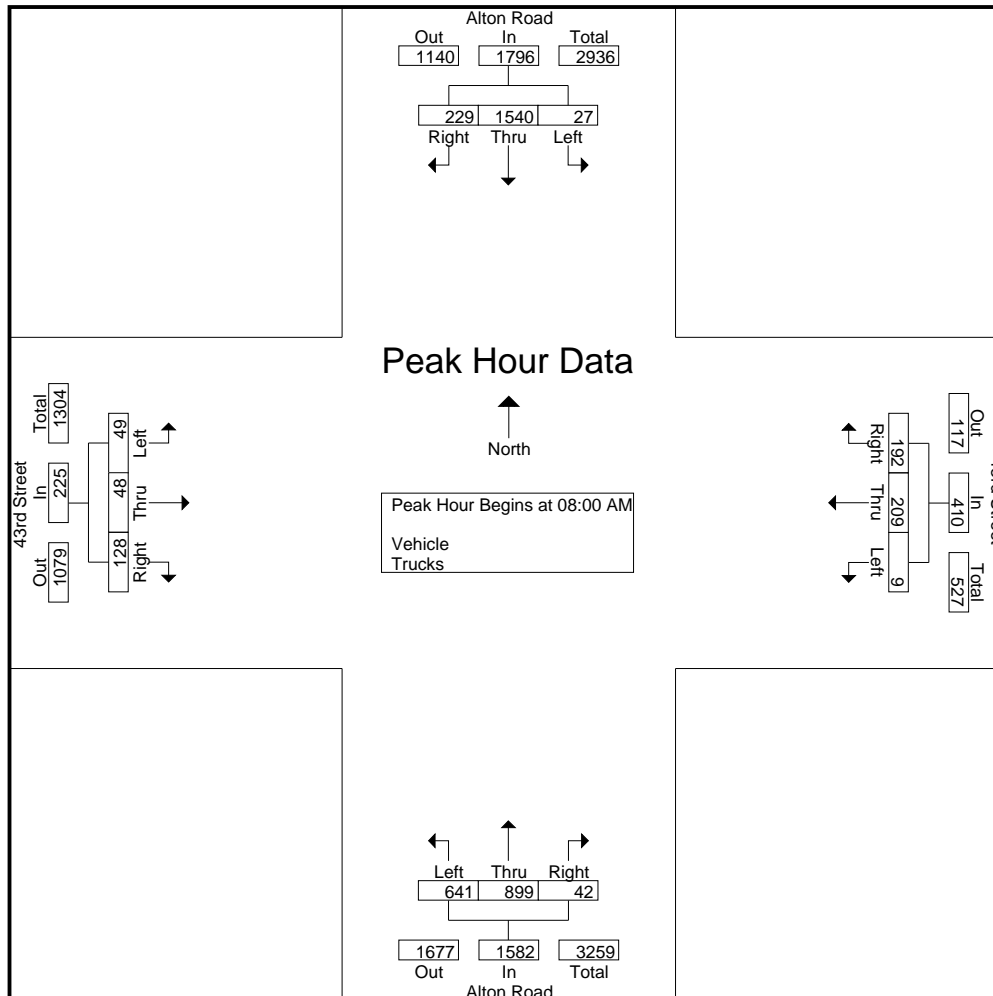
Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Alton Road Southbound					Alton Road Northbound					43rd Street Westbound					43rd Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 08:00 AM																					
08:00 AM	0	5	372	66	443	0	147	217	5	369	0	3	73	53	129	1	13	11	29	54	995
08:15 AM	0	6	387	61	454	0	175	182	10	367	0	1	55	58	114	0	10	14	39	63	998
08:30 AM	0	9	383	56	448	0	161	250	12	423	0	2	46	37	85	0	15	12	27	54	1010
08:45 AM	0	7	398	46	451	2	156	250	15	423	0	3	35	44	82	0	10	11	33	54	1010
Total Volume	0	27	1540	229	1796	2	639	899	42	1582	0	9	209	192	410	1	48	48	128	225	4013
% App. Total	0	1.5	85.7	12.8		0.1	40.4	56.8	2.7		0	2.2	51	46.8		0.4	21.3	21.3	56.9		
PHF	.000	.750	.967	.867	.989	.250	.913	.899	.700	.935	.000	.750	.716	.828	.795	.250	.800	.857	.821	.893	.993

Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4



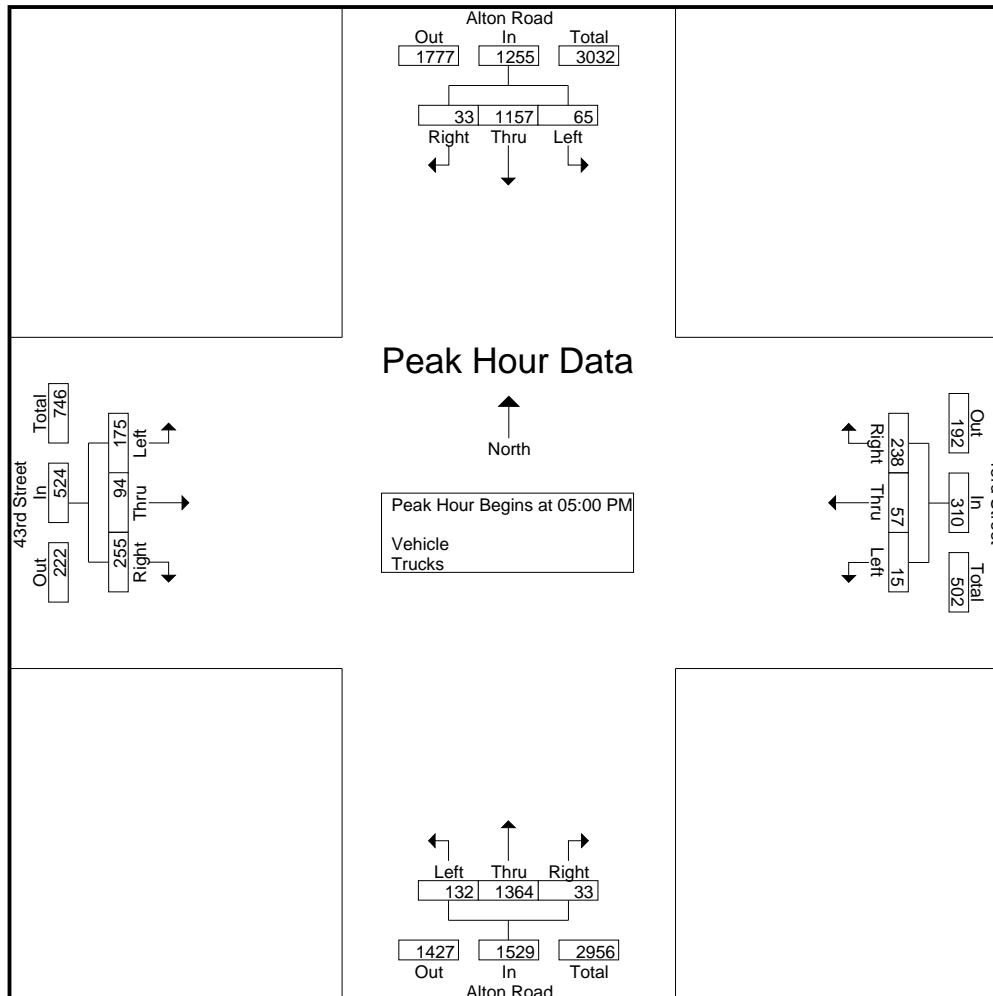
Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	Alton Road Southbound					Alton Road Northbound					43rd Street Westbound					43rd Street Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 05:00 PM																					
05:00 PM	1	16	267	9	293	2	32	344	9	387	0	3	16	43	62	0	45	32	84	161	903
05:15 PM	2	15	358	7	382	2	20	286	7	315	0	1	14	45	60	0	51	28	67	146	903
05:30 PM	2	19	267	10	298	5	37	359	8	409	1	6	15	73	95	0	50	18	64	132	934
05:45 PM	0	10	265	7	282	3	31	375	9	418	0	4	12	77	93	0	29	16	40	85	878
Total Volume	5	60	1157	33	1255	12	120	1364	33	1529	1	14	57	238	310	0	175	94	255	524	3618
% App. Total	0.4	4.8	92.2	2.6		0.8	7.8	89.2	2.2		0.3	4.5	18.4	76.8		0	33.4	17.9	48.7		
PHF	.625	.789	.808	.825	.821	.600	.811	.909	.917	.914	.250	.583	.891	.773	.816	.000	.858	.734	.759	.814	.968

Alton Road & 43rd Street

File Name : TMC-26 Alton Rd & 43rd Street
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6



Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr

Site Code : 00000000

Start Date : 10/17/2017

Page No : 1

Groups Printed- Peds & Bikes

Start Time	Southbound			Sullivan Drive Northbound			Mount Sinai Emergency Entrance Westbound			Mount Sinai Emergency Entrance Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
07:00 AM	0	0	0	0	0	0	1	0	1	5	0	5	6
07:15 AM	0	0	0	0	0	0	0	0	0	10	1	11	11
07:30 AM	0	0	0	1	0	1	0	0	0	13	0	13	14
07:45 AM	0	0	0	1	0	1	1	0	1	17	0	17	19
Total	0	0	0	2	0	2	2	0	2	45	1	46	50
08:00 AM	0	0	0	1	0	1	0	0	0	25	0	25	26
08:15 AM	0	0	0	2	0	2	0	0	0	37	0	37	39
08:30 AM	0	0	0	0	0	0	0	0	0	27	0	27	27
08:45 AM	0	0	0	2	0	2	2	0	2	17	0	17	21
Total	0	0	0	5	0	5	2	0	2	106	0	106	113
*** BREAK ***													
03:00 PM	0	0	0	0	0	0	2	0	2	26	0	26	28
03:15 PM	0	0	0	3	0	3	0	0	0	21	0	21	24
03:30 PM	0	0	0	3	0	3	1	0	1	25	0	25	29
03:45 PM	0	0	0	1	0	1	0	0	0	29	0	29	30
Total	0	0	0	7	0	7	3	0	3	101	0	101	111
04:00 PM	0	0	0	2	0	2	2	0	2	26	0	26	30
04:15 PM	0	0	0	4	0	4	2	0	2	23	1	24	30
04:30 PM	0	0	0	1	0	1	0	0	0	22	0	22	23
04:45 PM	0	0	0	0	0	0	0	0	0	30	0	30	30
Total	0	0	0	7	0	7	4	0	4	101	1	102	113
05:00 PM	0	0	0	3	0	3	2	0	2	31	0	31	36
05:15 PM	0	0	0	2	0	2	2	0	2	13	0	13	17
05:30 PM	0	0	0	2	0	2	3	0	3	13	0	13	18
05:45 PM	0	0	0	0	0	0	1	0	1	7	0	7	8
Total	0	0	0	7	0	7	8	0	8	64	0	64	79
Grand Total	0	0	0	28	0	28	19	0	19	417	2	419	466
Apprch %	0	0	0	100	0	100	100	0	100	99.5	0.5	99.5	466
Total %	0	0	0	6	0	6	4.1	0	4.1	89.5	0.4	89.9	466

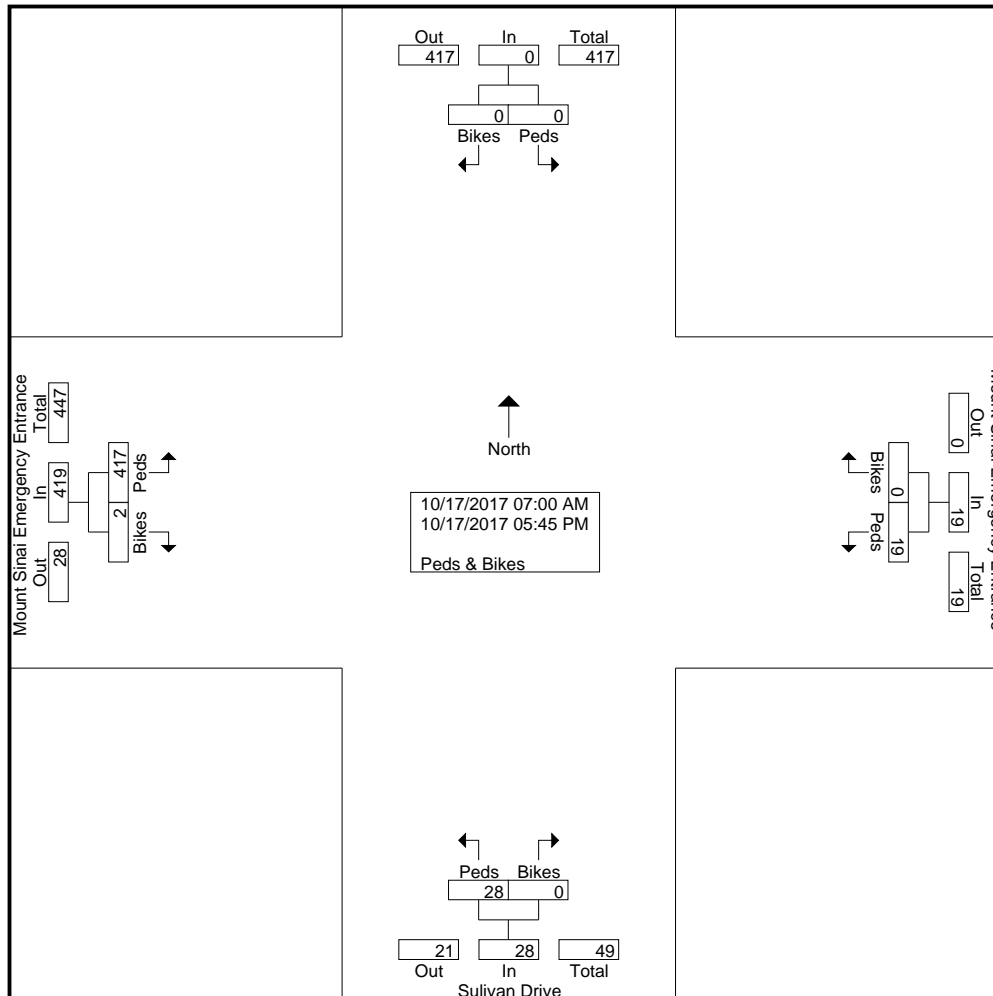
Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Southbound			Sullivan Drive Northbound			Mount Sinai Emergency Entrance Westbound			Mount Sinai Emergency Entrance Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 08:00 AM													
08:00 AM	0	0	0	1	0	1	0	0	0	25	0	25	26
08:15 AM	0	0	0	2	0	2	0	0	0	37	0	37	39
08:30 AM	0	0	0	0	0	0	0	0	0	27	0	27	27
08:45 AM	0	0	0	2	0	2	2	0	2	17	0	17	21
Total Volume	0	0	0	5	0	5	2	0	2	106	0	106	113
% App. Total	0	0		100	0		100	0		100	0		
PHF	.000	.000	.000	.625	.000	.625	.250	.000	.250	.716	.000	.716	.724

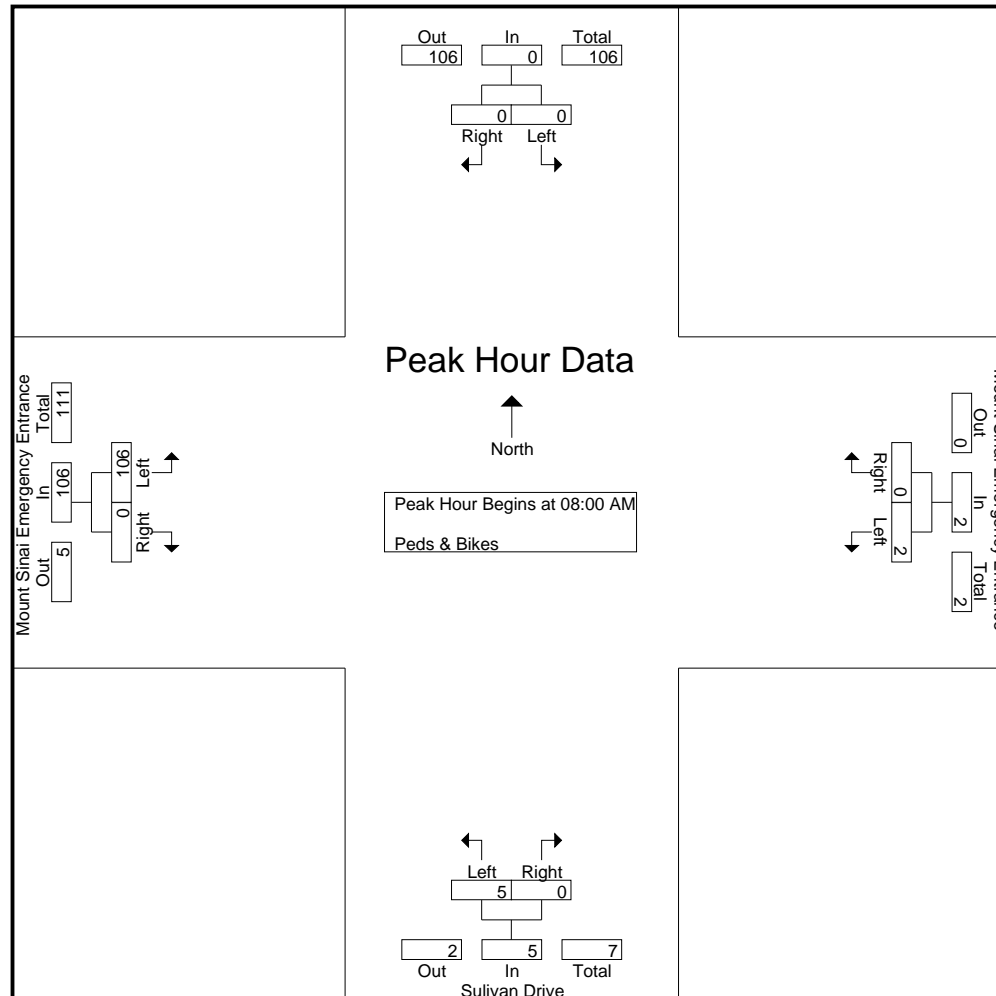
Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	Southbound			Sullivan Drive Northbound			Mount Sinai Emergency Entrance Westbound			Mount Sinai Emergency Entrance Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 03:30 PM													
03:30 PM	0	0	0	3	0	3	1	0	1	25	0	25	29
03:45 PM	0	0	0	1	0	1	0	0	0	29	0	29	30
04:00 PM	0	0	0	2	0	2	2	0	2	26	0	26	30
04:15 PM	0	0	0	4	0	4	2	0	2	23	1	24	30
Total Volume	0	0	0	10	0	10	5	0	5	103	1	104	119
% App. Total	0	0		100	0		100	0		99	1		
PHF	.000	.000	.000	.625	.000	.625	.625	.000	.625	.888	.250	.897	.992

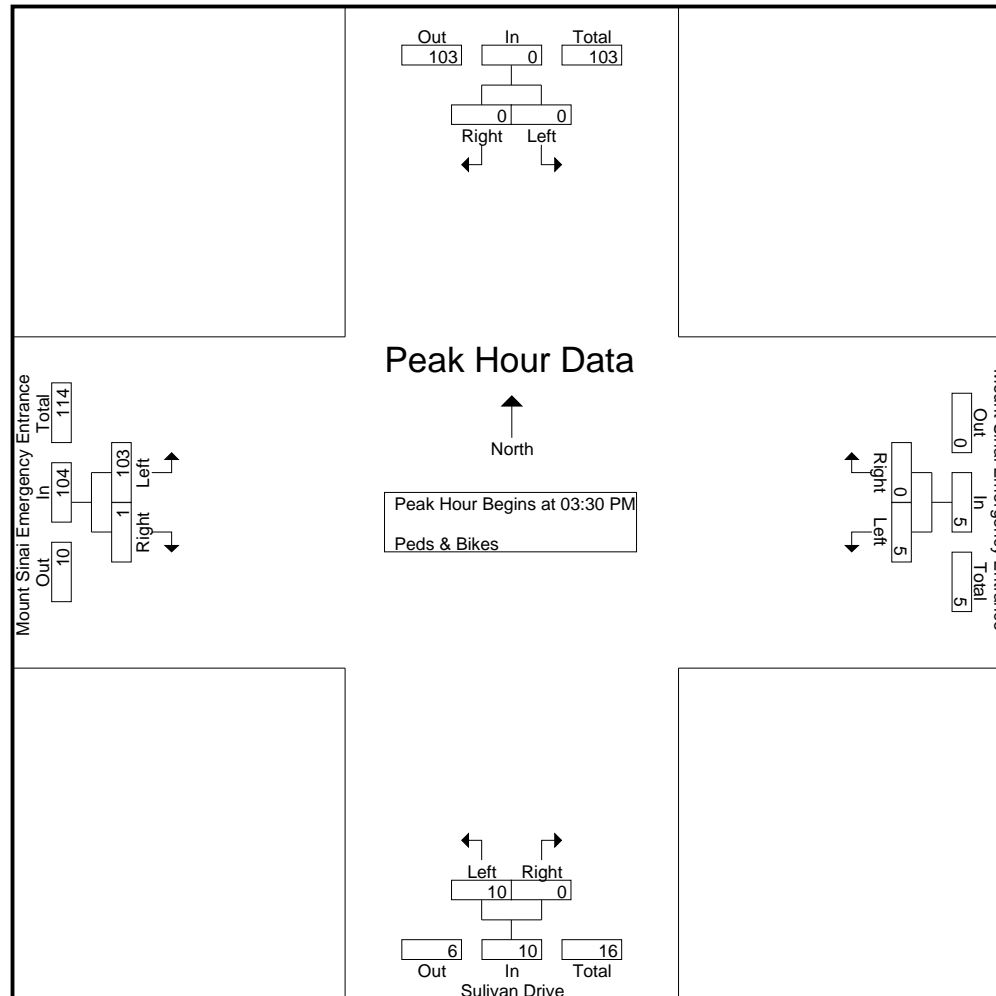
Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr

Site Code : 00000000

Start Date : 10/17/2017

Page No : 1

Groups Printed- Trucks

Start Time	Southbound					Sullivan Drive Northbound					Mount Sinai Emergency Entrance Westbound					Mount Sinai Emergency Entrance Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	0	0	0	2	2	0	2	3	0	5	0	0	0	0	0	7
07:15 AM	0	0	0	0	0	0	0	0	7	7	0	3	2	0	5	0	0	0	0	0	12
07:30 AM	0	0	0	0	0	0	0	0	7	7	0	4	2	0	6	0	0	0	0	0	13
07:45 AM	0	0	0	0	0	0	0	0	6	6	0	3	4	0	7	0	0	0	0	0	13
Total	0	0	0	0	0	0	0	0	22	22	0	12	11	0	23	0	0	0	0	0	45
08:00 AM	0	0	0	0	0	0	0	0	4	4	0	2	3	0	5	0	0	0	0	0	9
08:15 AM	0	0	0	0	0	0	0	0	9	9	0	5	5	0	10	0	0	0	0	0	19
08:30 AM	0	0	0	0	0	0	0	0	3	3	0	4	5	0	9	0	0	0	0	0	12
08:45 AM	0	0	0	0	0	0	0	0	7	7	0	4	2	0	6	0	0	1	0	1	14
Total	0	0	0	0	0	0	0	0	23	23	0	15	15	0	30	0	0	1	0	1	54
*** BREAK ***																					
03:00 PM	0	0	0	0	0	0	0	0	8	8	0	2	7	0	9	0	0	1	0	1	18
03:15 PM	0	0	0	0	0	0	1	0	7	8	0	4	2	0	6	0	0	0	0	0	14
03:30 PM	0	0	0	0	0	0	1	0	1	2	0	2	2	0	4	0	0	1	0	1	7
03:45 PM	0	0	0	0	0	0	0	0	3	3	0	6	1	0	7	0	0	1	0	1	11
Total	0	0	0	0	0	0	2	0	19	21	0	14	12	0	26	0	0	3	0	3	50
04:00 PM	0	0	0	0	0	0	0	0	4	4	0	2	0	0	2	0	0	0	0	0	6
04:15 PM	0	0	0	0	0	0	0	0	3	3	0	4	2	0	6	0	0	0	0	0	9
04:30 PM	0	0	0	0	0	0	0	0	5	5	0	3	0	0	3	0	0	0	0	0	8
04:45 PM	0	0	0	0	0	0	1	0	3	4	0	4	0	0	4	0	0	0	1	1	9
Total	0	0	0	0	0	0	1	0	15	16	0	13	2	0	15	0	0	0	1	1	32
05:00 PM	0	0	0	0	0	0	0	0	3	3	0	2	1	0	3	0	0	0	0	0	6
05:15 PM	0	0	0	0	0	0	1	0	2	3	0	6	1	0	7	0	0	0	0	0	10
05:30 PM	0	0	0	0	0	0	2	0	4	6	0	3	1	0	4	0	0	0	0	0	10
05:45 PM	0	0	0	0	0	0	2	0	4	6	0	3	0	0	3	0	0	1	0	1	10
Total	0	0	0	0	0	0	5	0	13	18	0	14	3	0	17	0	0	1	0	1	36
Grand Total	0	0	0	0	0	0	8	0	92	100	0	68	43	0	111	0	0	5	1	6	217
Apprch %	0	0	0	0	0	0	8	0	92	100	0	61.3	38.7	0	111	0	0	83.3	16.7	6	217
Total %	0	0	0	0	0	0	3.7	0	42.4	46.1	0	31.3	19.8	0	51.2	0	0	2.3	0.5	2.8	217

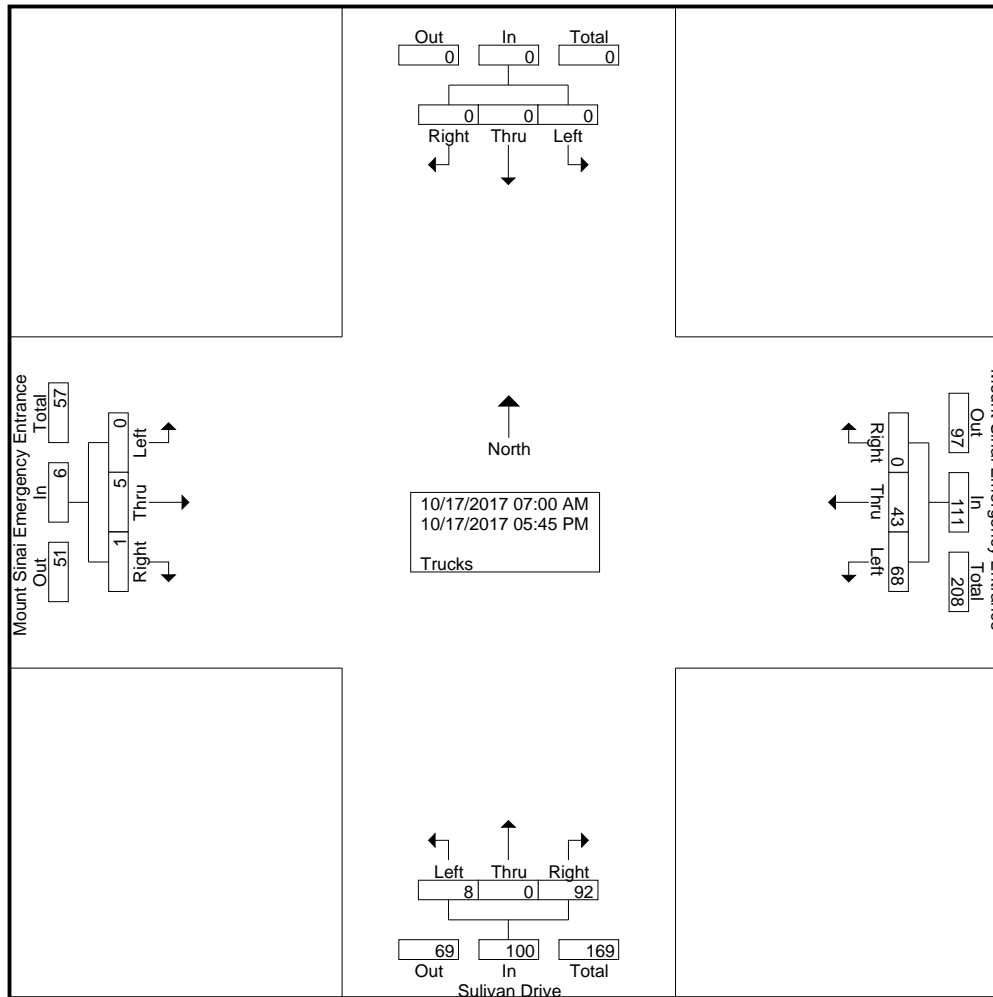
Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2



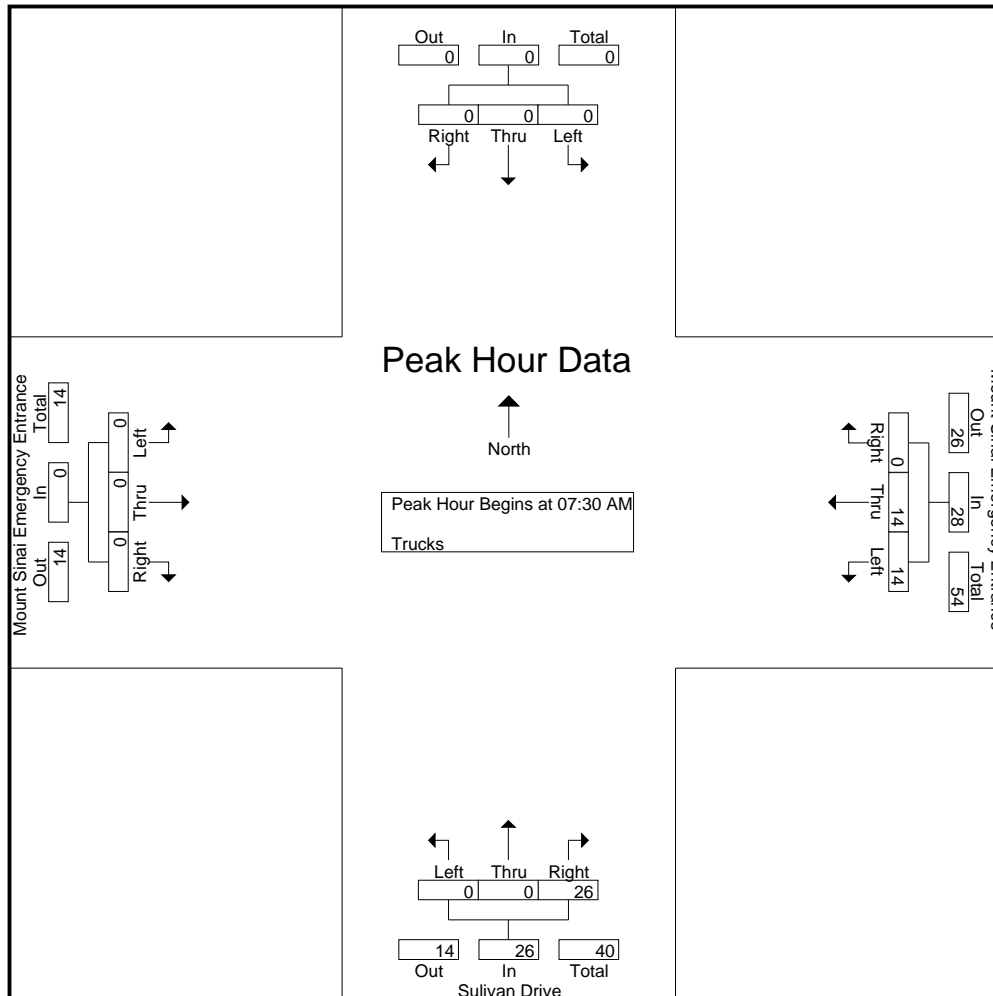
Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	Southbound					Sullivan Drive Northbound					Mount Sinai Emergency Entrance Westbound					Mount Sinai Emergency Entrance Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:00 PM																					
03:00 PM	0	0	0	0	0	0	0	0	8	8	0	2	7	0	9	0	0	1	0	1	18
03:15 PM	0	0	0	0	0	0	1	0	7	8	0	4	2	0	6	0	0	0	0	0	14
03:30 PM	0	0	0	0	0	0	1	0	1	2	0	2	2	0	4	0	0	1	0	1	7
03:45 PM	0	0	0	0	0	0	0	0	3	3	0	6	1	0	7	0	0	1	0	1	11
Total Volume	0	0	0	0	0	0	2	0	19	21	0	14	12	0	26	0	0	3	0	3	50
% App. Total	0	0	0	0	0	0	9.5	0	90.5		0	53.8	46.2	0		0	0	100	0		
PHF	.000	.000	.000	.000	.000	.000	.500	.000	.594	.656	.000	.583	.429	.000	.722	.000	.000	.750	.000	.750	.694

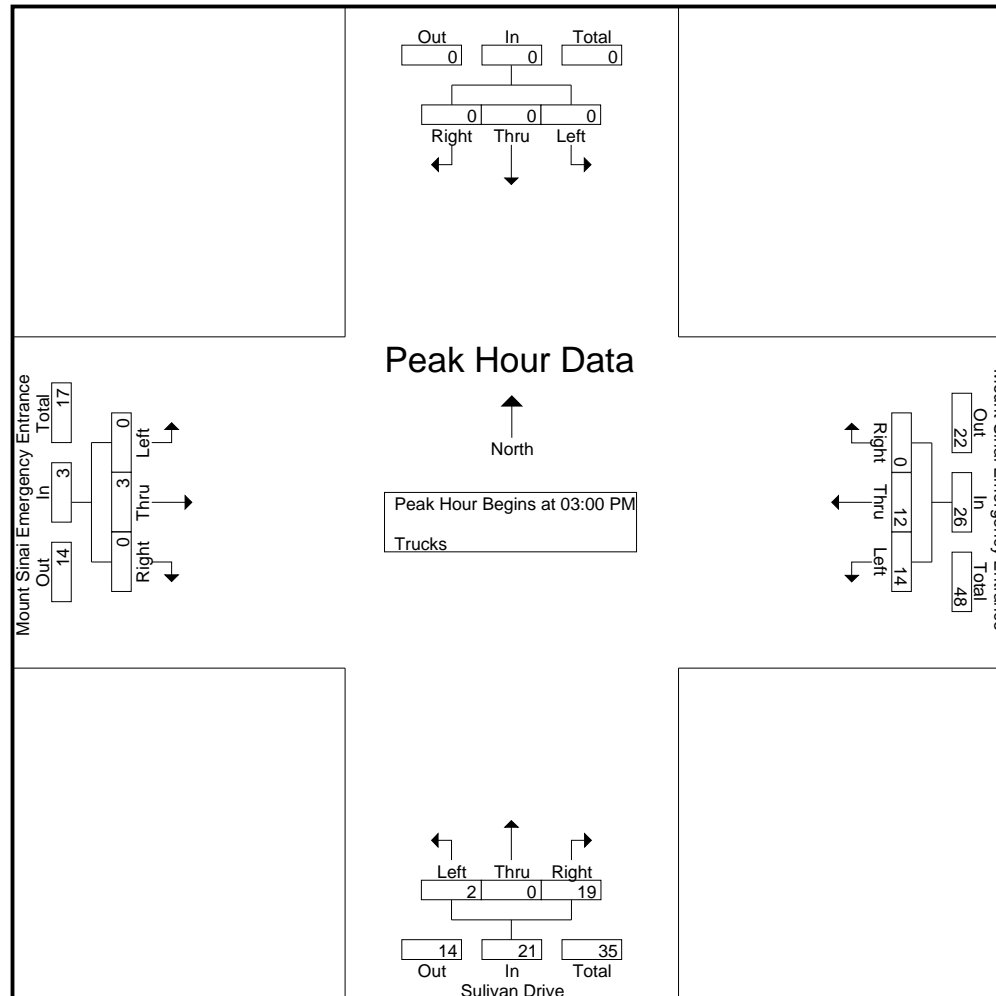
Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr

Site Code : 00000000

Start Date : 10/17/2017

Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	Southbound					Sullivan Drive Northbound					Mount Sinai Emergency Entrance Westbound					Mount Sinai Emergency Entrance Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	0	0	0	1	11	0	19	31	0	58	182	0	240	0	0	12	0	12	283
07:15 AM	0	0	0	0	0	0	8	0	33	41	1	46	142	0	189	0	0	6	4	10	240
07:30 AM	0	0	0	0	0	1	7	0	45	53	3	63	140	0	206	0	0	5	0	5	264
07:45 AM	0	0	0	0	0	1	12	0	43	56	0	69	176	0	245	0	0	2	2	4	305
Total	0	0	0	0	0	3	38	0	140	181	4	236	640	0	880	0	0	25	6	31	1092
08:00 AM	0	0	0	0	0	2	7	0	50	59	4	65	169	0	238	0	0	6	1	7	304
08:15 AM	0	0	0	0	0	1	9	0	63	73	2	85	194	0	281	0	0	7	0	7	361
08:30 AM	0	0	0	0	0	0	11	0	43	54	2	89	155	0	246	0	0	14	4	18	318
08:45 AM	0	0	0	0	0	0	7	0	39	46	2	102	117	0	221	0	0	16	3	19	286
Total	0	0	0	0	0	3	34	0	195	232	10	341	635	0	986	0	0	43	8	51	1269
*** BREAK ***																					
03:00 PM	0	0	0	0	0	3	5	0	82	90	4	60	67	0	131	0	0	40	7	47	268
03:15 PM	0	0	0	0	0	3	5	0	77	85	5	75	47	0	127	0	0	51	2	53	265
03:30 PM	0	0	0	0	0	2	6	0	89	97	4	43	42	0	89	0	0	38	6	44	230
03:45 PM	0	0	0	0	0	6	4	0	81	91	4	53	38	0	95	0	0	38	6	44	230
Total	0	0	0	0	0	14	20	0	329	363	17	231	194	0	442	0	0	167	21	188	993
04:00 PM	0	0	0	0	0	6	4	0	84	94	1	46	24	0	71	0	0	42	7	49	214
04:15 PM	0	0	0	0	0	2	4	0	93	99	5	48	34	0	87	0	0	43	6	49	235
04:30 PM	0	0	0	0	0	3	5	0	91	99	3	49	22	0	74	0	0	56	3	59	232
04:45 PM	0	0	0	0	0	4	3	0	76	83	4	40	19	0	63	0	0	58	15	73	219
Total	0	0	0	0	0	15	16	0	344	375	13	183	99	0	295	0	0	199	31	230	900
05:00 PM	0	0	0	0	0	3	5	0	95	103	4	31	33	0	68	1	0	58	4	63	234
05:15 PM	0	0	0	0	0	4	4	0	92	100	2	27	18	0	47	0	0	50	2	52	199
05:30 PM	0	0	0	0	0	3	4	0	80	87	1	30	31	0	62	0	0	47	2	49	198
05:45 PM	0	0	0	0	0	0	3	0	57	60	2	30	21	0	53	1	0	27	1	29	142
Total	0	0	0	0	0	10	16	0	324	350	9	118	103	0	230	2	0	182	9	193	773
Grand Total	0	0	0	0	0	45	124	0	1332	1501	53	1109	1671	0	2833	2	0	616	75	693	5027
Apprch %	0	0	0	0	0	3	8.3	0	88.7		1.9	39.1	59	0		0.3	0	88.9	10.8		
Total %	0	0	0	0	0	0.9	2.5	0	26.5	29.9	1.1	22.1	33.2	0	56.4	0	0	12.3	1.5	13.8	
Vehicle	0	0	0	0	0	45	116	0	1240	1401	53	1041	1628	0	2722	2	0	611	74	687	4810
% Vehicle	0	0	0	0	0	100	93.5	0	93.1	93.3	100	93.9	97.4	0	96.1	100	0	99.2	98.7	99.1	95.7

Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr

Site Code : 00000000

Start Date : 10/17/2017

Page No : 2

Groups Printed- Vehicle - Trucks

	Southbound					Sullivan Drive Northbound					Mount Sinai Emergency Entrance Westbound					Mount Sinai Emergency Entrance Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	0	0	0	0	0	8	0	92	100	0	68	43	0	111	0	0	5	1	6	217
% Trucks	0	0	0	0	0	0	6.5	0	6.9	6.7	0	6.1	2.6	0	3.9	0	0	0.8	1.3	0.9	4.3

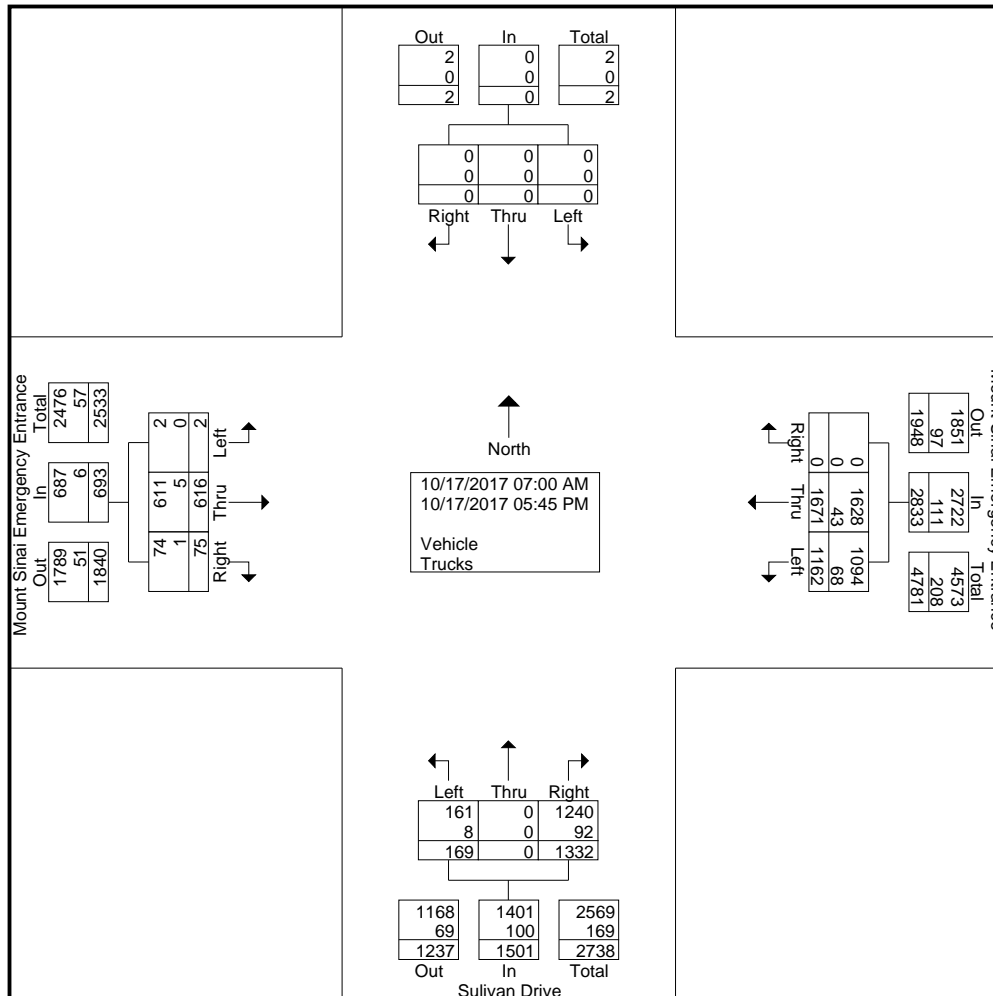
Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr

Site Code : 00000000

Start Date : 10/17/2017

Page No : 3



Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4

Start Time	Southbound					Sullivan Drive Northbound					Mount Sinai Emergency Entrance Westbound					Mount Sinai Emergency Entrance Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 07:45 AM																					
07:45 AM	0	0	0	0	0	1	12	0	43	56	0	69	176	0	245	0	0	2	2	4	305
08:00 AM	0	0	0	0	0	2	7	0	50	59	4	65	169	0	238	0	0	6	1	7	304
08:15 AM	0	0	0	0	0	1	9	0	63	73	2	85	194	0	281	0	0	7	0	7	361
08:30 AM	0	0	0	0	0	0	11	0	43	54	2	89	155	0	246	0	0	14	4	18	318
Total Volume	0	0	0	0	0	4	39	0	199	242	8	308	694	0	1010	0	0	29	7	36	1288
% App. Total	0	0	0	0	0	1.7	16.1	0	82.2		0.8	30.5	68.7	0		0	0	80.6	19.4		
PHF	.000	.000	.000	.000	.000	.500	.813	.000	.790	.829	.500	.865	.894	.000	.899	.000	.000	.518	.438	.500	.892

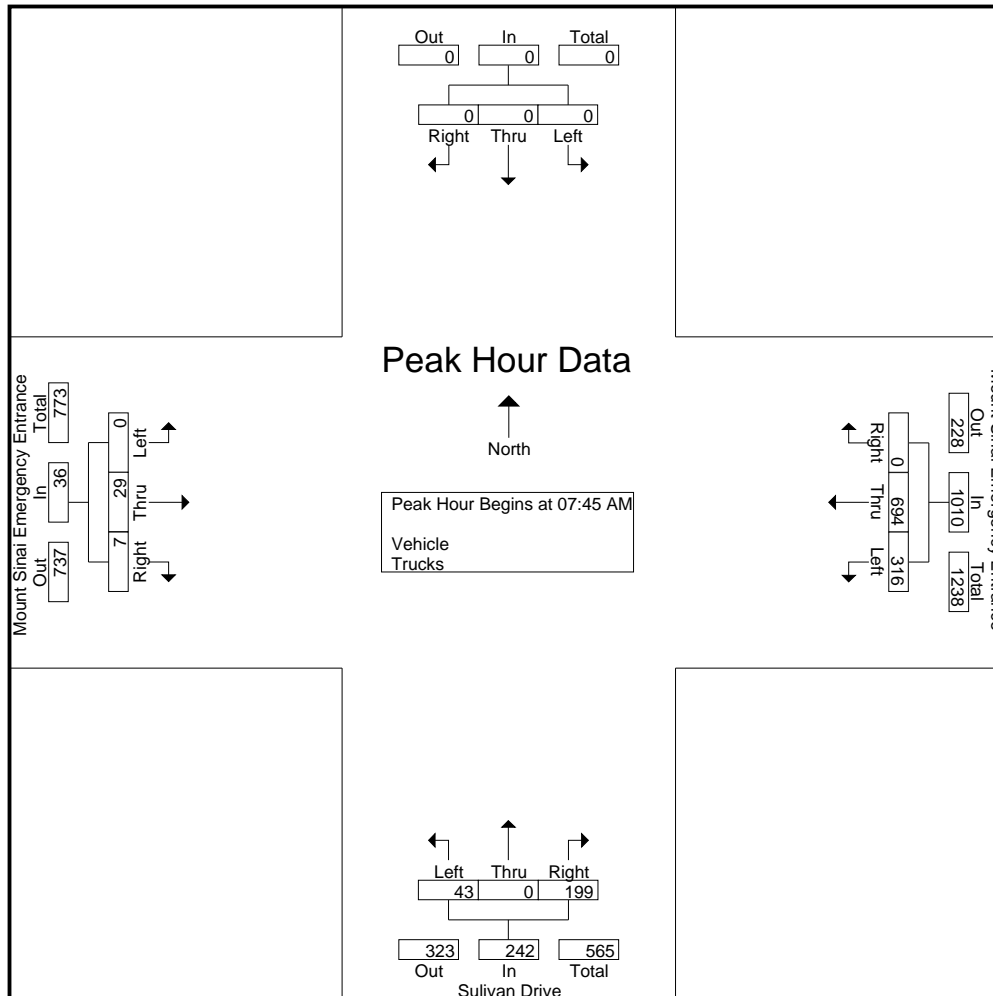
Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr

Site Code : 00000000

Start Date : 10/17/2017

Page No : 5



Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6

Start Time	Southbound					Sullivan Drive Northbound					Mount Sinai Emergency Entrance Westbound					Mount Sinai Emergency Entrance Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:00 PM																					
03:00 PM	0	0	0	0	0	3	5	0	82	90	4	60	67	0	131	0	0	40	7	47	268
03:15 PM	0	0	0	0	0	3	5	0	77	85	5	75	47	0	127	0	0	51	2	53	265
03:30 PM	0	0	0	0	0	2	6	0	89	97	4	43	42	0	89	0	0	38	6	44	230
03:45 PM	0	0	0	0	0	6	4	0	81	91	4	53	38	0	95	0	0	38	6	44	230
Total Volume	0	0	0	0	0	14	20	0	329	363	17	231	194	0	442	0	0	167	21	188	993
% App. Total	0	0	0	0	0	3.9	5.5	0	90.6		3.8	52.3	43.9	0		0	0	88.8	11.2		
PHF	.000	.000	.000	.000	.000	.583	.833	.000	.924	.936	.850	.770	.724	.000	.844	.000	.000	.819	.750	.887	.926

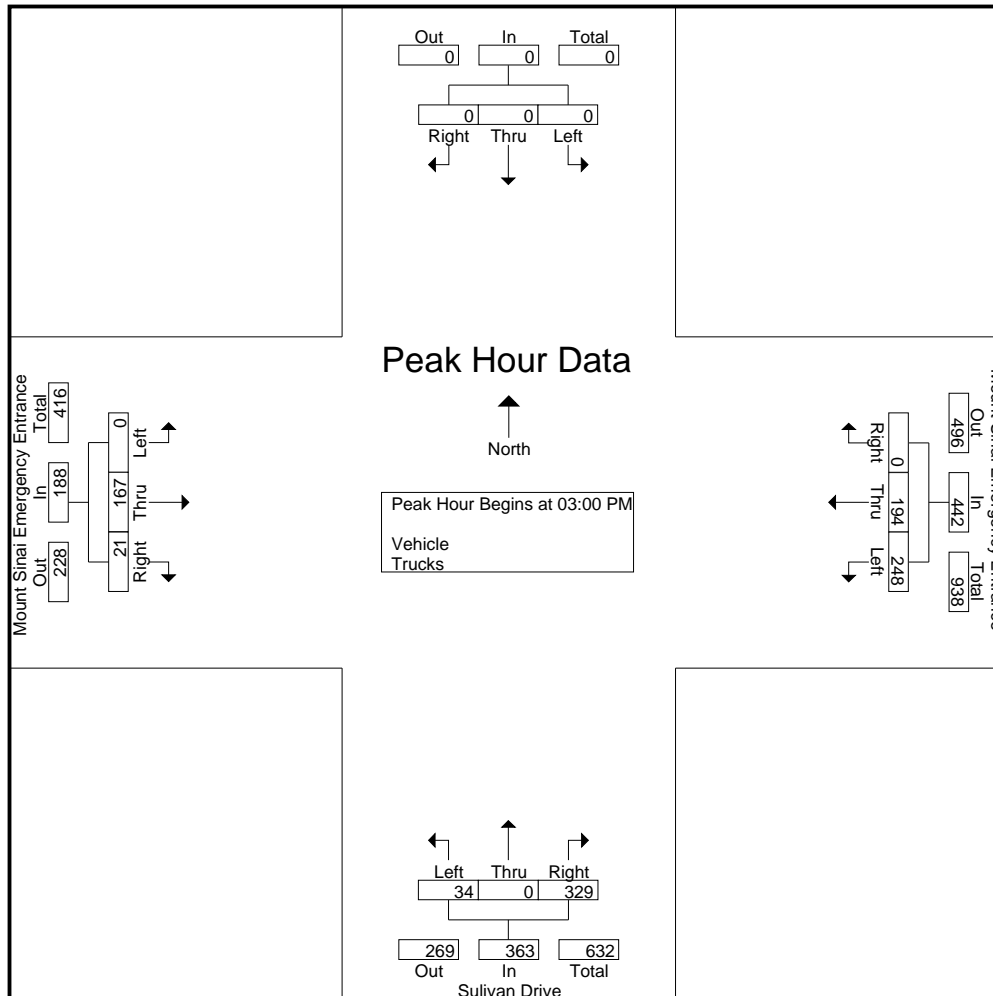
Mount Sinai Emergency Entrance & Sullivan Dr

File Name : TMC-27 Mount Sinai Emergency Entrance & Sullivan Dr

Site Code : 00000000

Start Date : 10/17/2017

Page No : 7



Alton Road & N Bay Rd

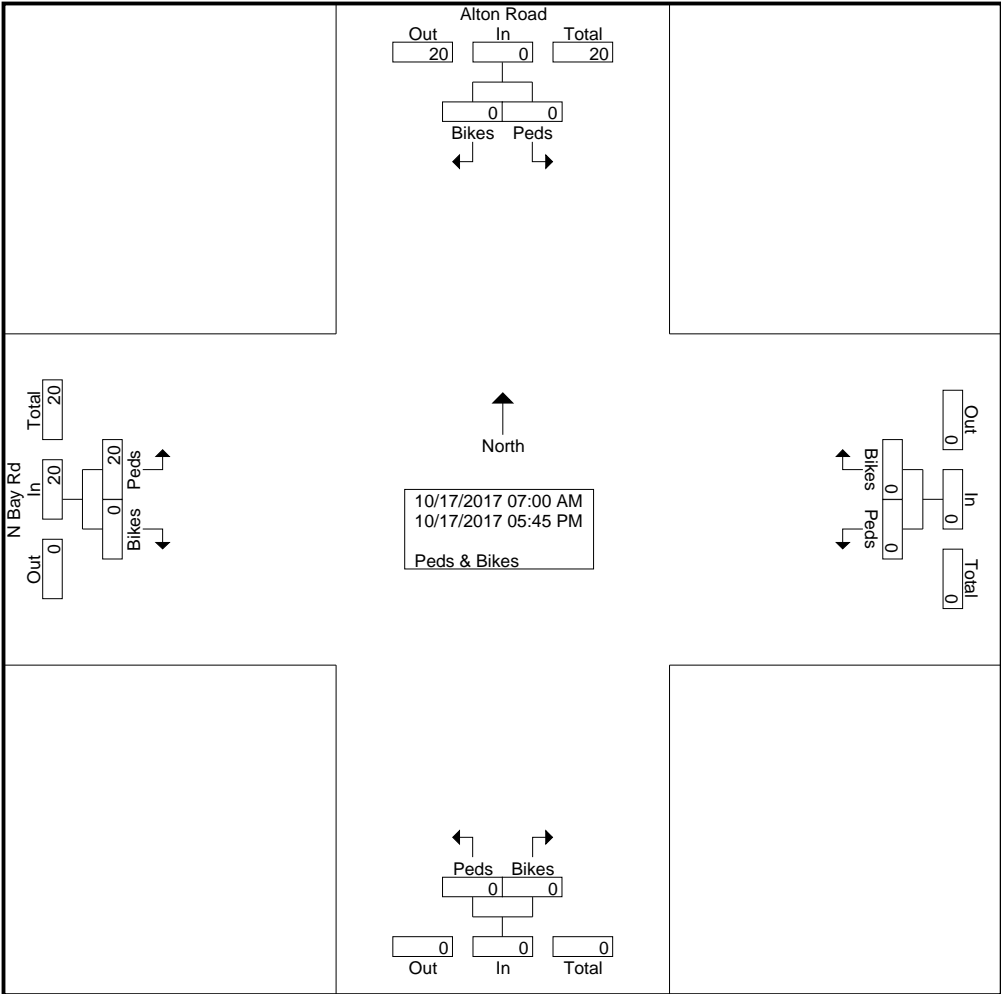
File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Peds & Bikes

Start Time	Alton Road Southbound			Northbound			Westbound			N Bay Rd Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
*** BREAK ***													
07:30 AM	0	0	0	0	0	0	0	0	0	2	0	2	2
07:45 AM	0	0	0	0	0	0	0	0	0	2	0	2	2
Total	0	0	0	0	0	0	0	0	0	4	0	4	4
*** BREAK ***													
08:15 AM	0	0	0	0	0	0	0	0	0	1	0	1	1
08:30 AM	0	0	0	0	0	0	0	0	0	2	0	2	2
*** BREAK ***													
Total	0	0	0	0	0	0	0	0	0	3	0	3	3
*** BREAK ***													
03:00 PM	0	0	0	0	0	0	0	0	0	2	0	2	2
03:15 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
*** BREAK ***													
Total	0	0	0	0	0	0	0	0	0	3	0	3	3
04:00 PM	0	0	0	0	0	0	0	0	0	3	0	3	3
*** BREAK ***													
04:30 PM	0	0	0	0	0	0	0	0	0	2	0	2	2
*** BREAK ***													
Total	0	0	0	0	0	0	0	0	0	5	0	5	5
05:00 PM	0	0	0	0	0	0	0	0	0	1	0	1	1
*** BREAK ***													
05:30 PM	0	0	0	0	0	0	0	0	0	4	0	4	4
*** BREAK ***													
Total	0	0	0	0	0	0	0	0	0	5	0	5	5
Grand Total	0	0	0	0	0	0	0	0	0	20	0	20	20
Apprch %	0	0		0	0		0	0		100	0		
Total %	0	0		0	0		0	0		100	0	100	

Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 2



Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Alton Road Southbound			Northbound			Westbound			N Bay Rd Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 07:30 AM													
07:30 AM	0	0	0	0	0	0	0	0	0	2	0	2	2
07:45 AM	0	0	0	0	0	0	0	0	0	2	0	2	2
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15 AM	0	0	0	0	0	0	0	0	0	1	0	1	1
Total Volume	0	0	0	0	0	0	0	0	0	5	0	5	5
% App. Total	0	0		0	0		0	0		100	0		
PHF	.000	.000	.000	.000	.000	.000	.000	.000	.000	.625	.000	.625	.625

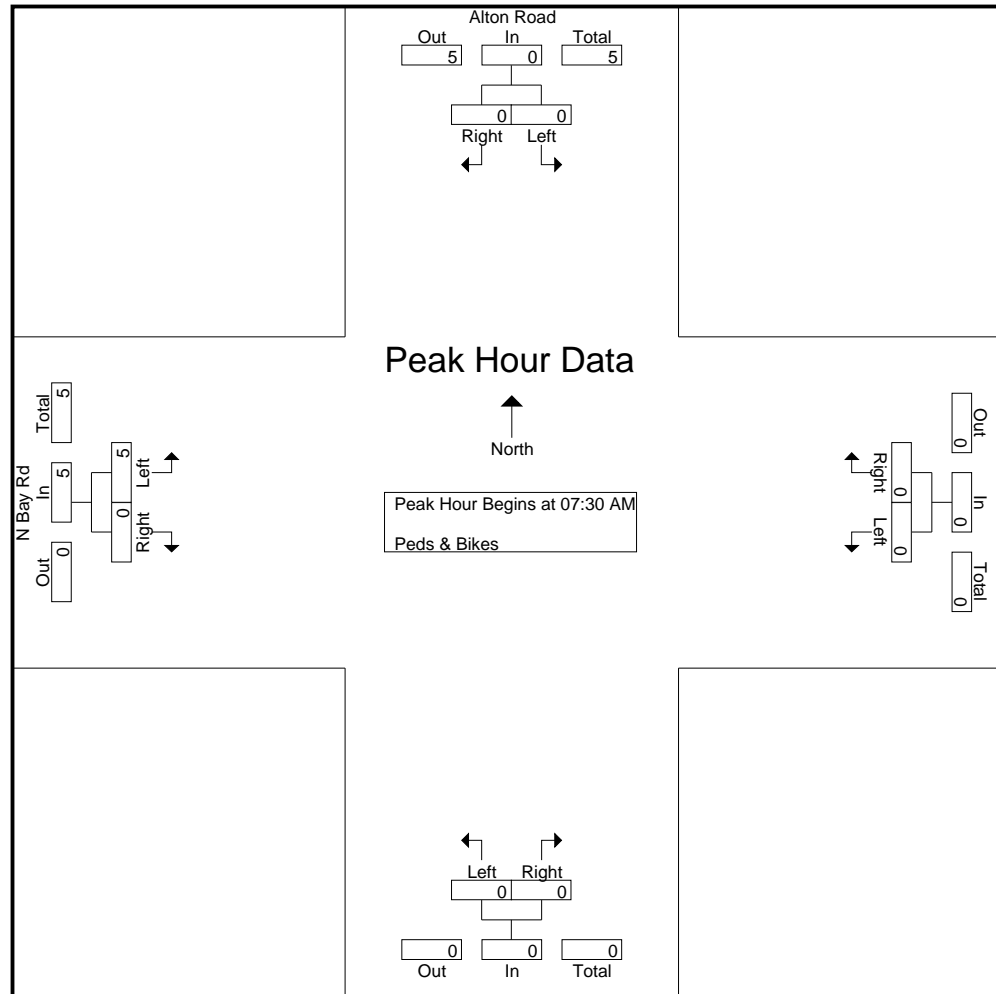
Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	Alton Road Southbound			Northbound			Westbound			N Bay Rd Eastbound			Int. Total
	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	Peds	Bikes	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1													
Peak Hour for Entire Intersection Begins at 03:45 PM													
03:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	0	0	0	0	0	0	0	0	3	0	3	3
04:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
04:30 PM	0	0	0	0	0	0	0	0	0	2	0	2	2
Total Volume	0	0	0	0	0	0	0	0	0	5	0	5	5
% App. Total	0	0	0	0	0	0	0	0	0	100	0	100	100
PHF	.000	.000	.000	.000	.000	.000	.000	.000	.000	.417	.000	.417	.417

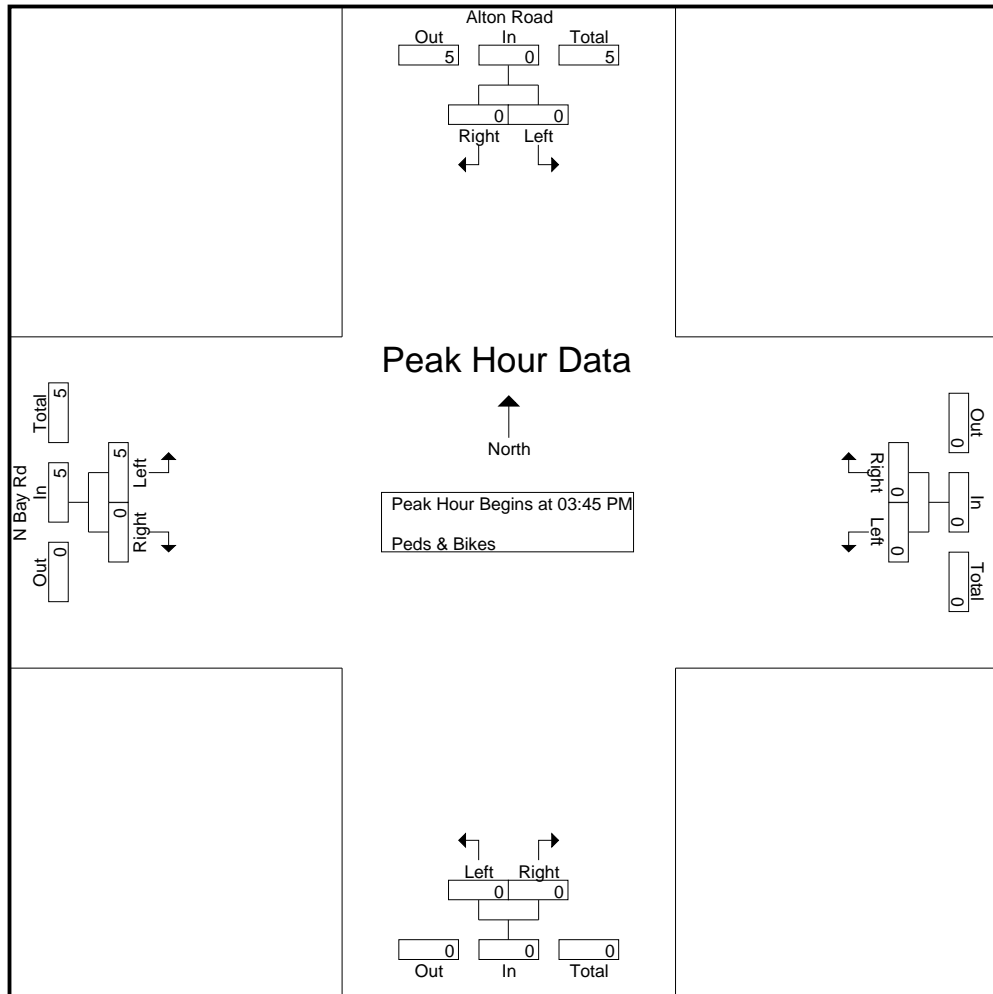
Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd

Site Code : 00000000

Start Date : 10/17/2017

Page No : 6



Alton Road & N Bay Rd

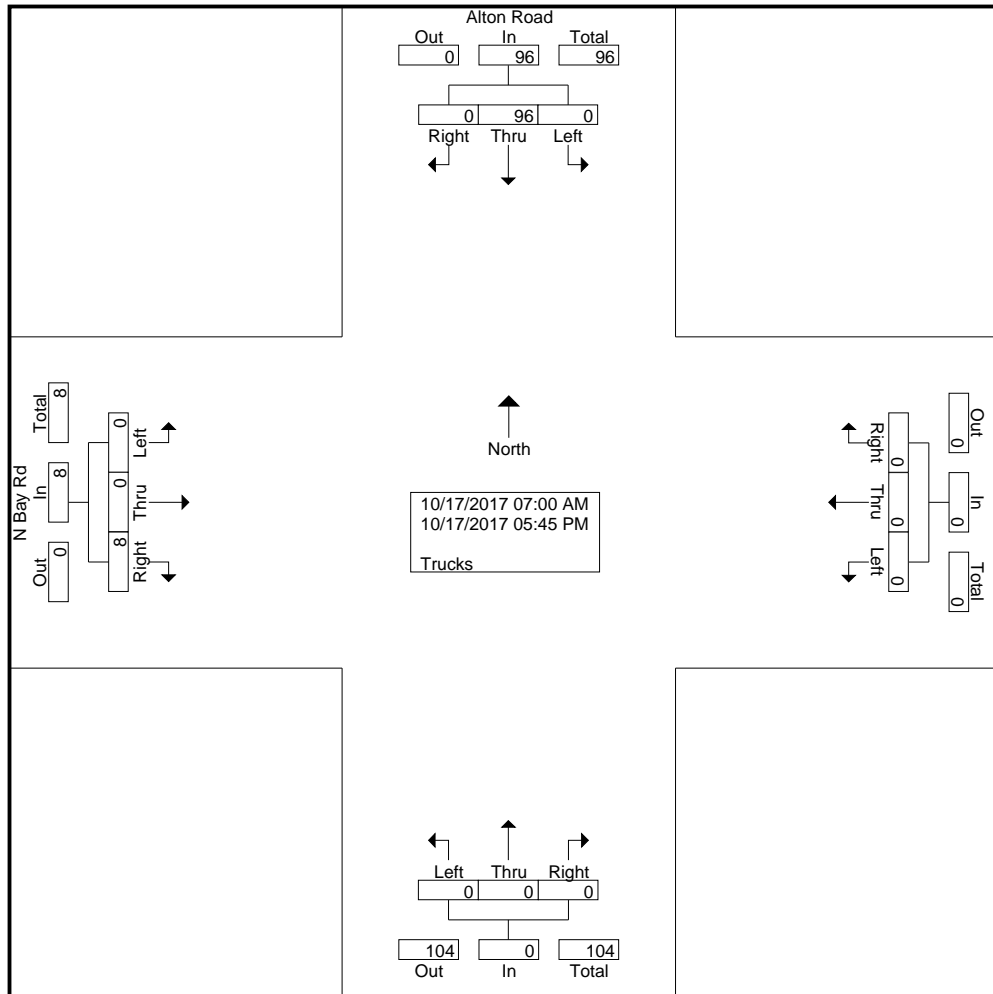
File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Trucks

Start Time	Alton Road Southbound					Northbound					Westbound					N Bay Rd Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:15 AM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:30 AM	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
*** BREAK ***																					
Total	0	0	4	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15 AM	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	3
08:30 AM	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	3
08:45 AM	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
Total	0	0	7	0	7	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	9
*** BREAK ***																					
03:00 PM	0	0	10	0	10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10
03:15 PM	0	0	12	0	12	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12
03:30 PM	0	0	5	0	5	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	6
03:45 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Total	0	0	28	0	28	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	29
04:00 PM	0	0	9	0	9	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	11
04:15 PM	0	0	8	0	8	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	9
04:30 PM	0	0	9	0	9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9
04:45 PM	0	0	12	0	12	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12
Total	0	0	38	0	38	0	0	0	0	0	0	0	0	0	0	0	0	0	3	3	41
05:00 PM	0	0	3	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
05:15 PM	0	0	7	0	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7
05:30 PM	0	0	5	0	5	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	7
05:45 PM	0	0	4	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
Total	0	0	19	0	19	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	21
Grand Total	0	0	96	0	96	0	0	0	0	0	0	0	0	0	0	0	0	0	8	8	104
Apprch %	0	0	100	0		0	0	0	0		0	0	0	0		0	0	0	100		
Total %	0	0	92.3	0	92.3	0	0	0	0	0	0	0	0	0	0	0	0	0	7.7	7.7	

Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 2



Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3

Start Time	Alton Road Southbound					Northbound					Westbound					N Bay Rd Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
08:15 AM	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	3	
08:30 AM	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	3	
08:45 AM	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
Total Volume	0	0	7	0	7	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	9	
% App. Total	0	0	100	0		0	0	0	0		0	0	0	0		0	0	0	100			
PHF	.000	.000	.875	.000	.875	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.500	.500	.750	

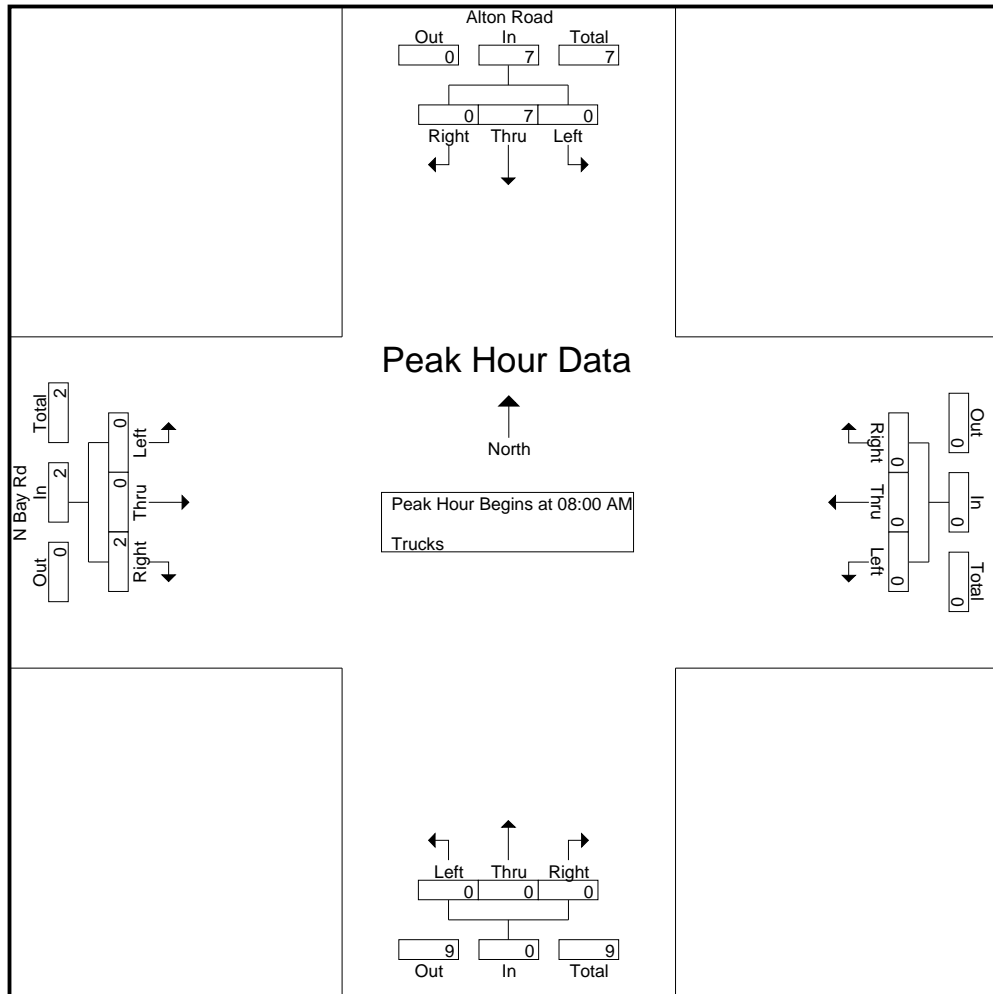
Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd

Site Code : 00000000

Start Date : 10/17/2017

Page No : 4



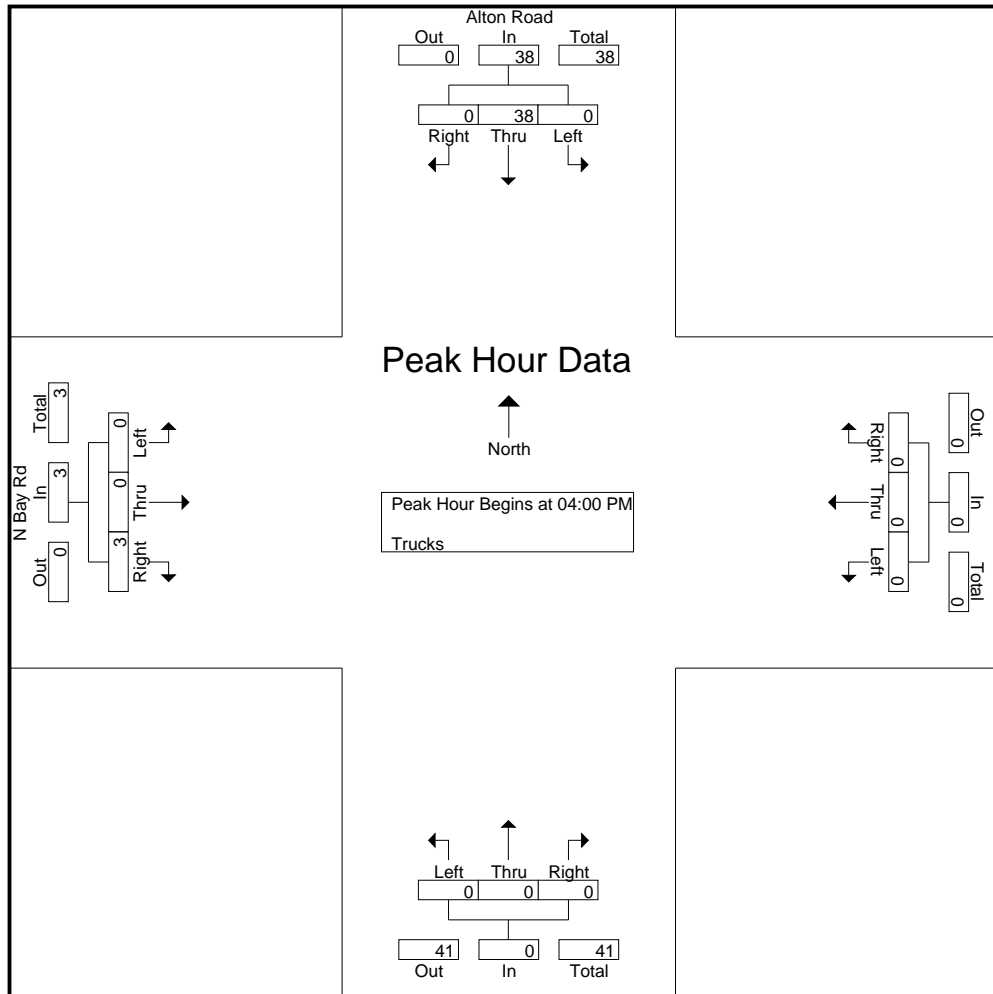
Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5

Start Time	Alton Road Southbound					Northbound					Westbound					N Bay Rd Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:00 PM																					
04:00 PM	0	0	9	0	9	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2	11
04:15 PM	0	0	8	0	8	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	9
04:30 PM	0	0	9	0	9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9
04:45 PM	0	0	12	0	12	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12
Total Volume	0	0	38	0	38	0	0	0	0	0	0	0	0	0	0	0	0	0	3	3	41
% App. Total	0	0	100	0		0	0	0	0		0	0	0	0		0	0	0	100		
PHF	.000	.000	.792	.000	.792	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.375	.375	.854

Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6



Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 1

Groups Printed- Vehicle - Trucks

Start Time	Alton Road Southbound					Northbound					Westbound					N Bay Rd Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
07:00 AM	0	0	333	1	334	0	0	0	0	0	0	0	0	0	0	0	0	0	9	9	343
07:15 AM	0	0	349	0	349	0	0	0	0	0	0	0	0	0	0	0	0	0	19	19	368
07:30 AM	0	0	348	0	348	0	0	0	0	0	0	0	0	0	0	0	0	0	49	49	397
07:45 AM	0	0	351	0	351	0	0	0	0	0	0	0	0	0	0	0	0	0	43	43	394
Total	0	0	1381	1	1382	0	0	0	0	0	0	0	0	0	0	0	0	0	120	120	1502
08:00 AM	0	0	364	0	364	0	0	0	0	0	0	0	0	0	0	0	0	0	29	29	393
08:15 AM	0	0	437	0	437	0	0	0	0	0	0	0	0	0	0	0	0	0	18	18	455
08:30 AM	0	0	408	1	409	0	0	0	0	0	0	0	0	0	0	0	0	0	39	39	448
08:45 AM	0	0	423	1	424	0	0	0	0	0	0	0	0	0	0	0	0	0	28	28	452
Total	0	0	1632	2	1634	0	0	0	0	0	0	0	0	0	0	0	0	0	114	114	1748
*** BREAK ***																					
03:00 PM	0	0	303	0	303	0	0	0	0	0	0	0	0	0	0	0	0	0	14	14	317
03:15 PM	0	0	310	0	310	0	0	0	0	0	0	0	0	0	0	0	0	0	19	19	329
03:30 PM	0	0	310	1	311	0	0	0	0	0	0	0	0	0	0	0	0	0	17	17	328
03:45 PM	0	0	313	0	313	0	0	0	0	0	0	0	0	0	0	0	0	0	26	26	339
Total	0	0	1236	1	1237	0	0	0	0	0	0	0	0	0	0	0	0	0	76	76	1313
04:00 PM	0	0	292	0	292	0	0	0	0	0	0	0	0	0	0	0	0	0	28	28	320
04:15 PM	0	0	300	0	300	0	0	0	0	0	0	0	0	0	0	0	0	0	23	23	323
04:30 PM	0	0	288	2	290	0	0	0	0	0	0	0	0	0	0	0	0	0	20	20	310
04:45 PM	0	0	274	1	275	0	0	0	0	0	0	0	0	0	0	0	0	0	12	12	287
Total	0	0	1154	3	1157	0	0	0	0	0	0	0	0	0	0	0	0	0	83	83	1240
05:00 PM	0	0	270	1	271	0	0	0	0	0	0	0	0	0	0	0	0	0	21	21	292
05:15 PM	0	0	336	1	337	0	0	0	0	0	0	0	0	0	0	0	0	0	14	14	351
05:30 PM	0	0	275	2	277	0	0	0	0	0	0	0	0	0	0	0	0	0	22	22	299
05:45 PM	0	0	275	1	276	0	0	0	0	0	0	0	0	0	0	0	0	0	7	7	283
Total	0	0	1156	5	1161	0	0	0	0	0	0	0	0	0	0	0	0	0	64	64	1225
Grand Total	0	0	6559	12	6571	0	0	0	0	0	0	0	0	0	0	0	0	0	457	457	7028
Apprch %	0	0	99.8	0.2		0	0	0	0		0	0	0	0		0	0	0	100		
Total %	0	0	93.3	0.2	93.5	0	0	0	0	0	0	0	0	0	0	0	0	0	6.5	6.5	
Vehicle	0	0	6463	12	6475	0	0	0	0	0	0	0	0	0	0	0	0	0	449	449	6924
% Vehicle	0	0	98.5	100	98.5	0	0	0	0	0	0	0	0	0	0	0	0	0	98.2	98.2	98.5

Alton Road & N Bay Rd

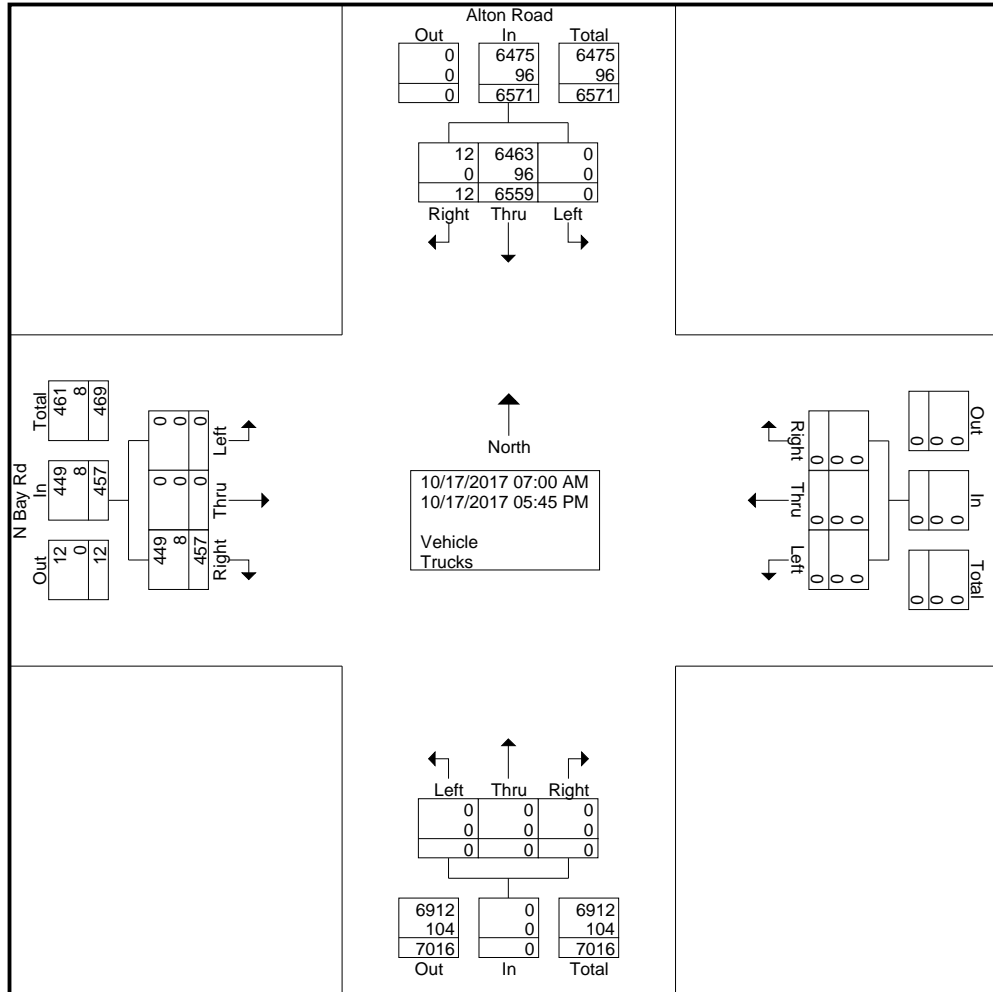
File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 2

Groups Printed- Vehicle - Trucks

	Alton Road Southbound					Northbound					Westbound					N Bay Rd Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Trucks	0	0	96	0	96	0	0	0	0	0	0	0	0	0	0	0	0	0	8	8	104
% Trucks	0	0	1.5	0	1.5	0	0	0	0	0	0	0	0	0	0	0	0	0	1.8	1.8	1.5

Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 3



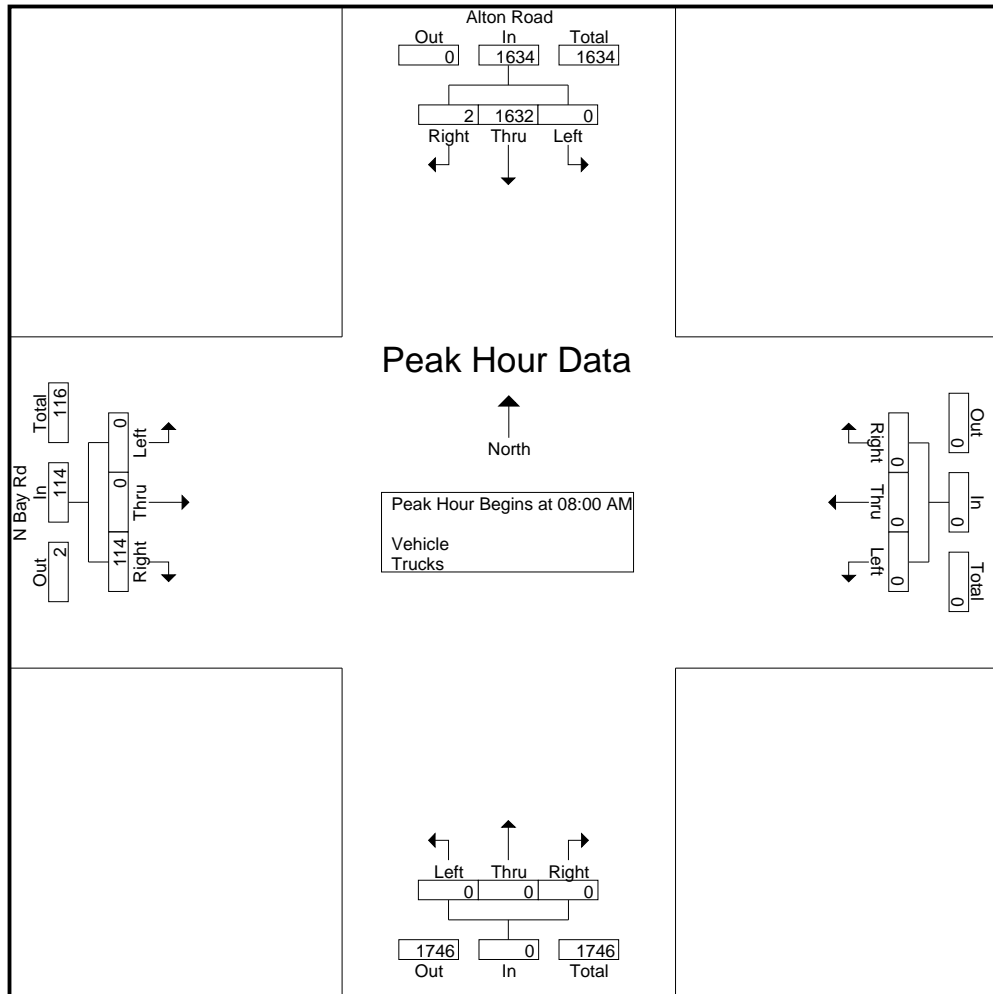
Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 4

Start Time	Alton Road Southbound					Northbound					Westbound					N Bay Rd Eastbound					Int. Total	
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total		
Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1																						
Peak Hour for Entire Intersection Begins at 08:00 AM																						
08:00 AM	0	0	364	0	364	0	0	0	0	0	0	0	0	0	0	0	0	0	29	29	393	
08:15 AM	0	0	437	0	437	0	0	0	0	0	0	0	0	0	0	0	0	0	18	18	455	
08:30 AM	0	0	408	1	409	0	0	0	0	0	0	0	0	0	0	0	0	0	39	39	448	
08:45 AM	0	0	423	1	424	0	0	0	0	0	0	0	0	0	0	0	0	0	28	28	452	
Total Volume	0	0	1632	2	1634	0	0	0	0	0	0	0	0	0	0	0	0	0	114	114	1748	
% App. Total	0	0	99.9	0.1		0	0	0	0		0	0	0	0		0	0	0	100			
PHF	.000	.000	.934	.500	.935	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.731	.731	.960	

Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 5



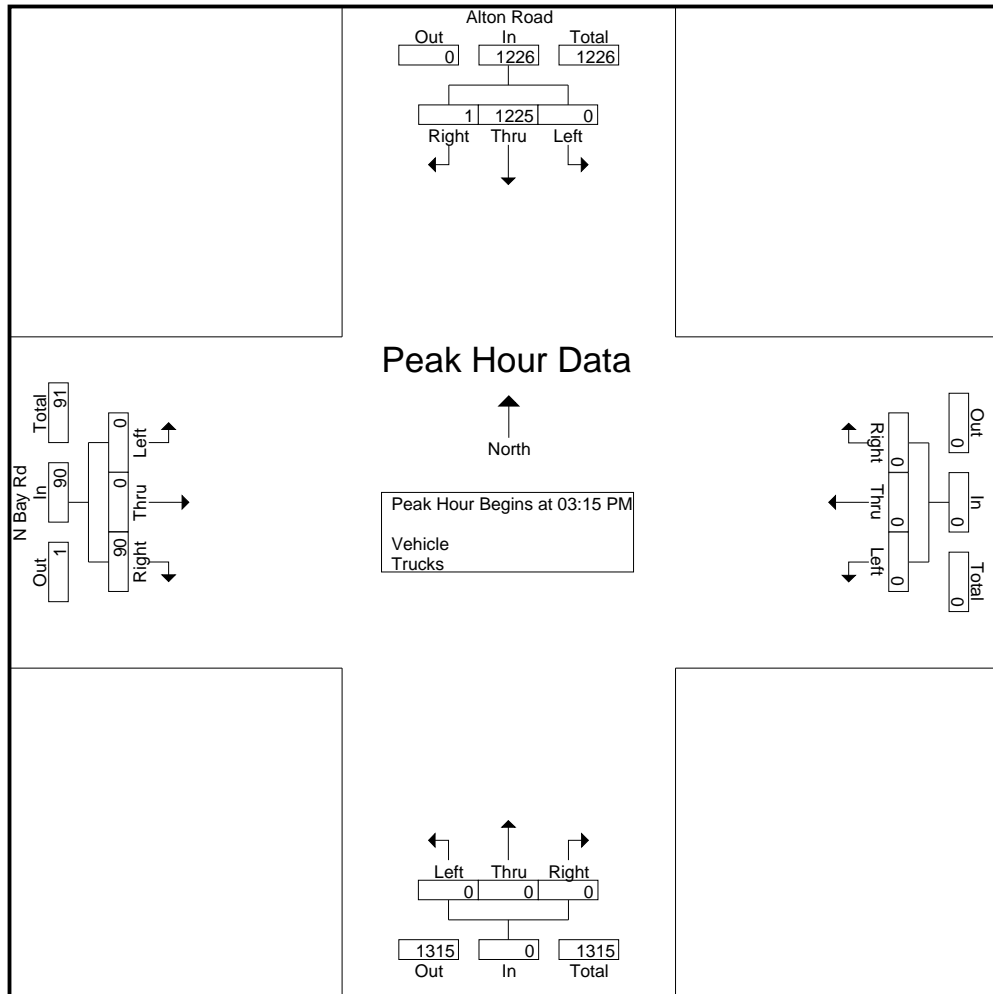
Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 6

Start Time	Alton Road Southbound					Northbound					Westbound					N Bay Rd Eastbound					Int. Total
	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	U-Turns	Left	Thru	Right	App. Total	
Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 03:15 PM																					
03:15 PM	0	0	310	0	310	0	0	0	0	0	0	0	0	0	0	0	0	0	19	19	329
03:30 PM	0	0	310	1	311	0	0	0	0	0	0	0	0	0	0	0	0	0	17	17	328
03:45 PM	0	0	313	0	313	0	0	0	0	0	0	0	0	0	0	0	0	0	26	26	339
04:00 PM	0	0	292	0	292	0	0	0	0	0	0	0	0	0	0	0	0	0	28	28	320
Total Volume	0	0	1225	1	1226	0	0	0	0	0	0	0	0	0	0	0	0	0	90	90	1316
% App. Total	0	0	99.9	0.1		0	0	0	0		0	0	0	0		0	0	0	100		
PHF	.000	.000	.978	.250	.979	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.000	.804	.804	.971

Alton Road & N Bay Rd

File Name : TMC-28 Alton Rd & N Bay Rd
 Site Code : 00000000
 Start Date : 10/17/2017
 Page No : 7



72-Hour Classification Counts

County: 87
 Station: 0041
 Description: SR 112, E OF NW 2 AVE
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	248	218	211	208	885	349	284	248	246	1127	2012	
0100	163	136	134	88	521	235	220	182	145	782	1303	
0200	98	81	74	76	329	141	148	155	122	566	895	
0300	60	79	76	91	306	111	97	112	98	418	724	
0400	95	135	204	235	669	116	141	170	173	600	1269	
0500	261	343	562	646	1812	208	232	306	391	1137	2949	
0600	725	990	1162	1320	4197	445	537	750	796	2528	6725	
0700	1306	1255	1263	1350	5174	1118	1146	1222	1318	4804	9978	
0800	1338	1212	1257	1288	5095	1301	1188	1189	1221	4899	9994	
0900	1201	1205	1143	1086	4635	1130	1054	1035	950	4169	8804	
1000	1033	1124	1075	964	4196	950	1001	989	949	3889	8085	
1100	901	892	941	957	3691	1083	989	979	968	4019	7710	
1200	872	977	1024	984	3857	971	970	932	900	3773	7630	
1300	945	840	923	945	3653	983	946	1034	982	3945	7598	
1400	1017	1071	1185	1101	4374	1071	1145	1122	1176	4514	8888	
1500	1035	1030	1048	953	4066	1292	1433	1461	1214	5400	9466	
1600	948	998	1039	930	3915	1286	1186	1273	1248	4993	8908	
1700	892	1019	1183	1189	4283	1306	1242	1204	1124	4876	9159	
1800	1122	1214	1359	1151	4846	1105	1102	972	902	4081	8927	
1900	1062	1019	939	805	3825	998	933	931	764	3626	7451	
2000	749	691	648	700	2788	807	731	667	561	2766	5554	
2100	558	640	573	563	2334	565	521	481	513	2080	4414	
2200	560	601	648	561	2370	484	505	467	444	1900	4270	
2300	402	350	310	274	1336	476	491	405	397	1769	3105	
24-Hour Totals:					73157						72661	145818

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	715	5206	730	5029	715	10193
P.M.	1745	4884	1500	5400	1445	9576
Daily	715	5206	1500	5400	715	10193
Truck Percentage	3.20		3.97		3.58	

Classification Summary Database																	
Dir	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TotTrk	TotVol
E	171	66152	4465	270	1431	284	64	109	139	10	5	7	19	0	31	2338	73157
W	264	62627	6156	395	1214	403	223	223	255	130	4	4	36	0	727	2887	72661

County: 87
 Station: 0041
 Description: SR 112, E OF NW 2 AVE
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	224	201	170	165	760	353	327	240	195	1115	1875	
0100	106	135	88	71	400	191	158	148	103	600	1000	
0200	61	68	55	55	239	105	106	92	73	376	615	
0300	60	76	77	76	289	82	85	89	82	338	627	
0400	85	101	190	193	569	80	104	125	139	448	1017	
0500	249	360	568	744	1921	165	201	312	361	1039	2960	
0600	877	1043	1204	1288	4412	413	520	787	814	2534	6946	
0700	1260	1380	1423	1391	5454	1134	1205	1292	1308	4939	10393	
0800	1334	1213	1249	1179	4975	1346	1215	1207	1185	4953	9928	
0900	1168	1197	1216	1292	4873	1068	1036	1073	1044	4221	9094	
1000	1159	1066	1078	1054	4357	1010	945	1005	1012	3972	8329	
1100	889	830	949	933	3601	912	1109	961	1019	4001	7602	
1200	827	918	960	928	3633	1054	1046	998	1006	4104	7737	
1300	898	894	913	890	3595	1001	1025	997	1027	4050	7645	
1400	841	972	1131	1098	4042	1148	1201	1173	1245	4767	8809	
1500	1007	1003	1017	1048	4075	1216	1298	1305	1142	4961	9036	
1600	1002	938	999	1069	4008	1146	1058	775	1035	4014	8022	
1700	981	972	1051	1133	4137	918	800	990	1074	3782	7919	
1800	1054	1088	1245	1199	4586	878	987	977	884	3726	8312	
1900	1156	1173	1065	1008	4402	885	823	793	715	3216	7618	
2000	932	792	771	619	3114	728	734	659	579	2700	5814	
2100	629	612	620	601	2462	562	549	585	549	2245	4707	
2200	561	534	534	505	2134	520	493	453	447	1913	4047	
2300	410	371	372	347	1500	546	515	519	420	2000	3500	
24-Hour Totals:					73538						70014	143552

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	715	5528	730	5161	715	10679
P.M.	1800	4586	1445	5064	1445	9189
Daily	715	5528	730	5161	715	10679
Truck Percentage	3.30		4.68		3.97	

Classification Summary Database																	
Dir	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TotTrk	TotVol
E	152	66330	4571	287	1470	306	43	108	166	6	9	9	23	0	58	2427	73538
W	311	59613	5988	428	1242	538	206	207	366	208	7	9	63	0	828	3274	70014

County: 87
 Station: 0041
 Description: SR 112, E OF NW 2 AVE
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	293	242	188	155	878	385	366	298	227	1276	2154	
0100	155	100	87	88	430	216	173	141	115	645	1075	
0200	78	82	75	71	306	127	120	109	86	442	748	
0300	73	79	65	83	300	91	72	83	69	315	615	
0400	89	120	175	242	626	81	117	121	118	437	1063	
0500	279	337	543	657	1816	168	198	274	350	990	2806	
0600	853	1044	1173	1316	4386	443	560	756	876	2635	7021	
0700	1231	1180	1155	971	4537	1066	1175	1031	894	4166	8703	
0800	1148	1279	1274	1255	4956	1129	1250	1217	1215	4811	9767	
0900	1254	1169	1155	1157	4735	1026	1031	1004	931	3992	8727	
1000	1090	1060	1028	1071	4249	1000	977	978	862	3817	8066	
1100	911	981	1113	1094	4099	833	817	841	764	3255	7354	
1200	926	973	936	960	3795	718	788	709	692	2907	6702	
1300	900	1014	914	1017	3845	712	813	861	819	3205	7050	
1400	963	1083	1163	1145	4354	931	996	1066	945	3938	8292	
1500	1073	1050	1084	981	4188	892	924	956	868	3640	7828	
1600	986	1036	1117	1092	4231	913	867	810	805	3395	7626	
1700	1077	1103	1104	1188	4472	818	854	856	773	3301	7773	
1800	1315	1297	1354	1174	5140	716	788	706	659	2869	8009	
1900	1076	1045	1024	980	4125	686	706	656	567	2615	6740	
2000	931	910	861	704	3406	552	528	490	460	2030	5436	
2100	698	728	647	597	2670	535	503	452	407	1897	4567	
2200	585	731	639	657	2612	390	411	389	370	1560	4172	
2300	468	442	354	356	1620	376	399	400	297	1472	3092	
24-Hour Totals:					75776						59610	135386

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	815	5062	800	4811	815	9770
P.M.	1745	5154	1400	3938	1415	8363
Daily	1745	5154	800	4811	815	9770
Truck Percentage	3.15		5.67		4.26	

Classification Summary Database																	
Dir	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TotTrk	TotVol
E	149	68536	4667	265	1467	290	53	124	136	12	10	7	24	0	36	2388	75776
W	254	46685	4889	384	1162	383	172	194	367	585	4	3	125	0	4403	3379	59610

County: 87
 Station: 0042
 Description: E OF US-1
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	238	287	205	167	897	202	169	128	132	631	1528	
0100	179	149	95	78	501	111	96	90	68	365	866	
0200	49	63	42	49	203	69	72	67	60	268	471	
0300	32	34	52	71	189	47	51	65	55	218	407	
0400	62	100	159	211	532	74	82	109	109	374	906	
0500	205	295	419	510	1429	116	136	158	187	597	2026	
0600	566	758	892	929	3145	199	231	368	334	1132	4277	
0700	875	843	882	902	3502	504	515	566	672	2257	5759	
0800	868	797	786	751	3202	618	619	636	575	2448	5650	
0900	696	667	734	688	2785	560	516	573	534	2183	4968	
1000	704	655	699	557	2615	494	563	508	528	2093	4708	
1100	576	613	578	646	2413	590	554	527	499	2170	4583	
1200	578	607	540	523	2248	566	560	489	520	2135	4383	
1300	562	551	646	580	2339	512	567	602	556	2237	4576	
1400	579	732	737	684	2732	584	657	637	659	2537	5269	
1500	675	661	665	621	2622	714	866	942	905	3427	6049	
1600	679	677	708	721	2785	844	850	892	859	3445	6230	
1700	669	695	659	671	2694	865	949	840	814	3468	6162	
1800	693	691	860	789	3033	631	645	546	478	2300	5333	
1900	690	616	525	462	2293	573	473	567	373	1986	4279	
2000	403	389	371	338	1501	405	381	328	270	1384	2885	
2100	371	336	362	328	1397	243	260	243	250	996	2393	
2200	363	345	376	354	1438	239	261	220	239	959	2397	
2300	227	213	176	167	783	275	312	263	211	1061	1844	
24-Hour Totals:					47278						40671	87949

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	645	3529	745	2545	730	5924
P.M.	1800	3033	1630	3565	1630	6358
Daily	630	3539	1630	3565	1630	6358
Truck Percentage	4.95		3.65		4.35	

Classification Summary Database																	
Dir	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TotTrk	TotVol
E	132	42006	2792	174	581	454	195	618	103	62	1	0	7	146	7	2341	47278
W	111	34952	3253	202	542	159	230	136	87	102	2	0	26	0	869	1486	40671

County: 87
 Station: 0042
 Description: E OF US-1
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	117	107	95	83	402	217	207	175	125	724	1126
0100	59	61	50	51	221	120	110	91	59	380	601
0200	44	46	49	38	177	57	69	74	48	248	425
0300	35	23	46	50	154	43	44	33	47	167	321
0400	50	77	145	172	444	39	63	82	70	254	698
0500	210	301	386	498	1395	85	115	132	179	511	1906
0600	615	737	943	924	3219	214	278	331	422	1245	4464
0700	839	878	817	930	3464	512	617	635	645	2409	5873
0800	861	770	758	728	3117	659	690	669	638	2656	5773
0900	662	693	783	746	2884	594	571	572	576	2313	5197
1000	662	652	611	631	2556	606	569	560	547	2282	4838
1100	606	551	598	531	2286	581	569	615	577	2342	4628
1200	478	541	553	579	2151	578	625	557	547	2307	4458
1300	564	562	567	568	2261	604	646	665	534	2449	4710
1400	552	631	657	673	2513	671	688	770	748	2877	5390
1500	612	655	617	615	2499	784	884	923	874	3465	5964
1600	605	629	668	597	2499	919	922	845	851	3537	6036
1700	582	674	650	717	2623	781	918	904	778	3381	6004
1800	672	722	744	784	2922	645	681	608	593	2527	5449
1900	738	792	613	464	2607	565	647	561	454	2227	4834
2000	441	442	396	413	1692	396	422	319	316	1453	3145
2100	405	410	424	407	1646	314	331	259	256	1160	2806
2200	359	429	381	364	1533	215	308	269	260	1052	2585
2300	242	227	188	182	839	303	345	366	297	1311	2150
24-Hour Totals:	46104					43277					89381

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	715	3486	745	2663	715	6042
P.M.	1800	2922	1530	3638	1530	6104
Daily	630	3584	1530	3638	1530	6104
Truck Percentage	5.38		3.67		4.56	

Classification Summary Database																	
Dir	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TotTrk	TotVol
E	137	40645	2832	172	560	449	186	758	118	56	1	0	9	173	8	2482	46104
W	122	37377	3613	206	590	179	276	120	111	92	0	0	16	0	575	1590	43277

County: 87
 Station: 0043
 Description: SR 112, E OF INTERCOSTAL WATERWAY BRIDGE
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	282	334	242	209	1067	248	221	172	168	809	1876	
0100	206	182	125	97	610	150	118	102	95	465	1075	
0200	74	83	58	69	284	95	77	89	72	333	617	
0300	46	48	66	85	245	57	63	80	67	267	512	
0400	76	113	184	245	618	76	107	120	113	416	1034	
0500	243	335	488	577	1643	125	154	171	198	648	2291	
0600	657	886	1093	1169	3805	231	268	389	402	1290	5095	
0700	1078	1023	1127	1140	4368	548	631	628	755	2562	6930	
0800	1131	1049	1023	965	4168	731	732	723	720	2906	7074	
0900	927	853	938	881	3599	688	623	671	644	2626	6225	
1000	873	844	865	692	3274	602	605	618	632	2457	5731	
1100	715	767	733	793	3008	660	597	617	604	2478	5486	
1200	724	731	689	687	2831	607	640	602	570	2419	5250	
1300	728	754	818	784	3084	603	621	671	620	2515	5599	
1400	749	932	949	871	3501	659	763	765	797	2984	6485	
1500	842	839	834	809	3324	896	1050	1045	1045	4036	7360	
1600	878	860	880	906	3524	1078	970	974	1014	4036	7560	
1700	848	887	861	858	3454	940	1144	1013	1051	4148	7602	
1800	896	884	1079	974	3833	797	803	674	617	2891	6724	
1900	858	802	689	589	2938	701	605	629	461	2396	5334	
2000	537	504	492	457	1990	480	460	371	344	1655	3645	
2100	481	438	457	423	1799	320	316	290	313	1239	3038	
2200	418	387	429	390	1624	295	316	269	301	1181	2805	
2300	269	230	199	194	892	336	375	306	302	1319	2211	
24-Hour Totals:					59483						48076	107559

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	730	4447	745	2941	730	7293
P.M.	1800	3833	1515	4218	1645	7613
Daily	730	4447	1515	4218	1645	7613
Truck Percentage	4.32		3.92		4.14	

Classification Summary Database																	
Dir	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TotTrk	TotVol
E	203	53298	3398	224	716	483	196	625	106	63	1	1	7	146	16	2568	59483
W	199	39405	3956	236	741	170	136	147	174	204	3	2	72	0	2631	1885	48076

County: 87
 Station: 0043
 Description: SR 112, E OF INTERCOSTAL WATERWAY BRIDGE
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	137	116	135	112	500	287	249	185	143	864	1364		
0100	77	62	52	69	260	134	101	102	77	414	674		
0200	47	47	36	46	176	75	74	69	50	268	444		
0300	49	38	60	61	208	49	60	60	61	230	438		
0400	44	105	169	218	536	67	57	72	84	280	816		
0500	216	383	508	577	1684	95	113	150	190	548	2232		
0600	667	915	1113	1173	3868	205	243	377	413	1238	5106		
0700	1034	1085	778	800	3697	562	663	709	783	2717	6414		
0800	938	890	978	931	3737	781	705	753	776	3015	6752		
0900	1023	909	781	572	3285	620	614	687	678	2599	5884		
1000	780	788	752	767	3087	634	666	611	659	2570	5657		
1100	630	701	726	735	2792	585	710	648	660	2603	5395		
1200	728	708	752	735	2923	653	646	621	591	2511	5434		
1300	713	742	794	781	3030	599	590	642	648	2479	5509		
1400	858	838	900	874	3470	729	778	827	867	3201	6671		
1500	800	741	741	551	2833	816	1102	930	1020	3868	6701		
1600	986	795	785	821	3387	1048	1094	1029	841	4012	7399		
1700	750	790	820	832	3192	748	880	961	778	3367	6559		
1800	885	898	927	967	3677	779	746	640	644	2809	6486		
1900	968	822	794	696	3280	604	621	571	467	2263	5543		
2000	599	570	561	512	2242	419	421	381	361	1582	3824		
2100	494	431	468	490	1883	329	304	341	295	1269	3152		
2200	421	448	431	442	1742	312	302	279	284	1177	2919		
2300	289	275	229	204	997	393	354	374	277	1398	2395		
24-Hour Totals:						56486						47282	103768

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	645	4070	745	3022	800	6752
P.M.	1800	3677	1545	4191	1600	7399
Daily	630	4405	1545	4191	1600	7399
Truck Percentage	5.04		4.19		4.65	

Classification Summary Database																	
Dir	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TotTrk	TotVol
E	218	50230	3146	196	705	430	144	876	116	135	2	2	28	212	46	2846	56486
W	175	38719	3989	262	712	227	151	158	203	194	3	1	69	0	2419	1980	47282

County: 87
 Station: 0043
 Description: SR 112, E OF INTERCOSTAL WATERWAY BRIDGE
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	160	143	129	107	539	265	244	222	174	905	1444		
0100	88	85	65	68	306	146	122	114	81	463	769		
0200	60	65	65	47	237	80	92	74	57	303	540		
0300	49	35	54	61	199	53	50	36	60	199	398		
0400	63	104	167	204	538	53	73	79	79	284	822		
0500	242	353	451	577	1623	101	126	151	180	558	2181		
0600	717	850	1145	1162	3874	228	271	362	471	1332	5206		
0700	1040	1069	1060	1169	4338	568	647	687	785	2687	7025		
0800	1102	1009	991	947	4049	783	774	763	790	3110	7159		
0900	886	886	974	938	3684	685	691	659	652	2687	6371		
1000	828	844	792	816	3280	656	652	702	669	2679	5959		
1100	779	701	757	695	2932	633	674	634	720	2661	5593		
1200	642	708	739	697	2786	662	752	639	639	2692	5478		
1300	754	749	772	761	3036	660	702	662	620	2644	5680		
1400	736	854	884	894	3368	722	813	817	780	3132	6500		
1500	828	839	828	814	3309	917	959	1101	1049	4026	7335		
1600	789	835	868	755	3247	1152	1109	1050	978	4289	7536		
1700	771	860	864	900	3395	951	1107	1045	971	4074	7469		
1800	878	938	926	1007	3749	772	843	768	689	3072	6821		
1900	907	1001	768	617	3293	649	729	625	562	2565	5858		
2000	581	576	548	547	2252	440	452	396	405	1693	3945		
2100	530	518	533	527	2108	372	398	320	282	1372	3480		
2200	458	532	502	450	1942	300	334	326	325	1285	3227		
2300	327	292	247	236	1102	325	399	406	322	1452	2554		
24-Hour Totals:						59186						50164	109350

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	715	4400	800	3110	745	7376
P.M.	1800	3749	1530	4411	1530	7677
Daily	630	4416	1530	4411	1530	7677
Truck Percentage	4.61		3.77		4.22	

Classification Summary Database																	
Dir	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TotTrk	TotVol
E	234	52733	3474	238	686	485	186	764	124	56	1	0	13	173	19	2726	59186
W	130	41343	4519	269	751	153	165	147	160	206	1	0	38	0	2282	1890	50164

County: 87
 Station: 0044
 Description: SR 112, E OF BISCAYNE BAY BRIDGE
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	277	315	267	224	1083	295	247	187	194	923	2006		
0100	185	194	138	94	611	159	131	107	93	490	1101		
0200	73	83	65	68	289	103	87	94	76	360	649		
0300	49	47	62	75	233	62	66	77	65	270	503		
0400	70	106	146	223	545	82	116	137	115	450	995		
0500	209	286	403	485	1383	155	171	212	240	778	2161		
0600	542	643	785	857	2827	277	328	449	479	1533	4360		
0700	780	778	835	865	3258	652	805	862	909	3228	6486		
0800	855	715	755	676	3001	910	887	870	876	3543	6544		
0900	683	659	758	668	2768	811	776	778	743	3108	5876		
1000	677	670	710	631	2688	710	735	707	727	2879	5567		
1100	576	609	615	611	2411	825	727	725	746	3023	5434		
1200	620	650	622	595	2487	743	710	703	675	2831	5318		
1300	630	607	658	659	2554	708	729	782	740	2959	5513		
1400	589	698	757	740	2784	805	818	909	912	3444	6228		
1500	667	686	638	671	2662	1059	1213	1154	1054	4480	7142		
1600	665	693	759	702	2819	1155	953	1038	1041	4187	7006		
1700	694	697	698	675	2764	1035	974	1127	971	4107	6871		
1800	688	698	786	836	3008	900	886	772	706	3264	6272		
1900	711	683	625	547	2566	782	712	700	525	2719	5285		
2000	484	470	469	419	1842	541	532	444	389	1906	3748		
2100	444	426	431	381	1682	350	384	341	355	1430	3112		
2200	365	404	384	384	1537	339	367	306	332	1344	2881		
2300	286	226	213	178	903	410	416	362	345	1533	2436		
24-Hour Totals:						48705						54789	103494

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	715	3333	745	3576	730	6838
P.M.	1800	3008	1515	4576	1515	7236
Daily	715	3333	1515	4576	1515	7236
Truck Percentage	8.73		7.58		8.12	

Classification Summary Database																	
Dir	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TotTrk	TotVol
E	943	38542	2410	260	540	868	1603	68	304	418	0	0	191	0	2558	4252	48705
W	228	45770	4622	331	750	733	129	1443	233	208	2	1	43	279	17	4152	54789

County: 87
 Station: 0044
 Description: SR 112, E OF BISCAYNE BAY BRIDGE
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	163	117	121	108	509	297	292	202	156	947	1456	
0100	89	61	60	54	264	151	109	113	86	459	723	
0200	63	50	43	33	189	77	84	75	48	284	473	
0300	48	41	46	62	197	55	65	64	63	247	444	
0400	41	88	139	191	459	68	63	79	84	294	753	
0500	188	302	427	484	1401	112	119	166	222	619	2020	
0600	557	696	829	835	2917	248	295	453	464	1460	4377	
0700	823	765	704	688	2980	694	769	816	949	3228	6208	
0800	772	777	847	780	3176	918	844	888	907	3557	6733	
0900	704	610	659	700	2673	743	779	773	788	3083	5756	
1000	697	648	625	617	2587	746	771	735	751	3003	5590	
1100	536	559	551	638	2284	704	805	785	762	3056	5340	
1200	607	641	616	619	2483	757	775	704	693	2929	5412	
1300	608	628	591	684	2511	723	709	754	762	2948	5459	
1400	641	628	725	770	2764	808	920	1013	960	3701	6465	
1500	639	643	619	529	2430	1034	1138	771	936	3879	6309	
1600	676	659	664	698	2697	1008	1004	989	905	3906	6603	
1700	628	675	668	637	2608	827	933	922	972	3654	6262	
1800	730	730	754	790	3004	868	813	777	737	3195	6199	
1900	776	752	632	641	2801	698	679	640	543	2560	5361	
2000	546	534	502	461	2043	344	288	301	402	1335	3378	
2100	455	420	409	434	1718	384	350	394	348	1476	3194	
2200	402	386	388	417	1593	370	348	309	346	1373	2966	
2300	308	259	230	197	994	452	422	409	320	1603	2597	
24-Hour Totals:					47282						52796	100078

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	800	3176	745	3599	800	6733
P.M.	1800	3004	1430	4145	1430	6922
Daily	630	3252	1430	4145	1430	6922
Truck Percentage	9.02		8.67		8.84	

Classification Summary Database																	
Dir	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TotTrk	TotVol
E	931	37159	2554	268	524	860	1712	94	280	372	1	1	154	0	2372	4266	47282
W	208	43380	4597	357	747	761	128	1571	253	282	5	6	69	401	31	4580	52796

County: 87
 Station: 0044
 Description: SR 112, E OF BISCAYNE BAY BRIDGE
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	175	152	123	109	559	294	272	235	183	984	1543
0100	104	82	71	66	323	155	122	121	83	481	804
0200	50	71	53	57	231	82	91	82	58	313	544
0300	51	39	40	60	190	57	50	39	57	203	393
0400	57	81	143	203	484	58	74	87	80	299	783
0500	215	294	378	507	1394	108	134	162	206	610	2004
0600	550	731	831	887	2999	252	309	442	512	1515	4514
0700	847	812	827	872	3358	637	766	821	876	3100	6458
0800	844	724	742	660	2970	881	832	980	878	3571	6541
0900	698	712	761	771	2942	823	789	746	801	3159	6101
1000	667	685	700	689	2741	751	729	772	769	3021	5762
1100	653	634	588	589	2464	718	770	747	787	3022	5486
1200	560	587	628	572	2347	747	838	774	734	3093	5440
1300	626	645	672	677	2620	769	831	779	733	3112	5732
1400	636	680	731	753	2800	885	930	981	943	3739	6539
1500	713	713	686	629	2741	1061	1129	1136	1055	4381	7122
1600	712	716	733	656	2817	1076	1079	1123	1035	4313	7130
1700	637	754	741	733	2865	1120	1005	1090	997	4212	7077
1800	680	781	741	827	3029	912	974	836	806	3528	6557
1900	778	785	701	575	2839	763	864	714	610	2951	5790
2000	566	547	512	459	2084	528	511	459	464	1962	4046
2100	490	492	444	481	1907	419	464	381	336	1600	3507
2200	424	447	441	455	1767	355	401	354	366	1476	3243
2300	374	308	245	256	1183	406	477	477	385	1745	2928
24-Hour Totals:	49654					56390					106044

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	645	3373	800	3571	745	6751
P.M.	1800	3029	1515	4396	1515	7136
Daily	630	3377	1515	4396	1515	7136
Truck Percentage	9.40		7.30		8.28	

Classification Summary Database																	
Dir	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TotTrk	TotVol
E	955	38918	2860	286	511	803	2104	96	304	403	3	0	158	0	2253	4668	49654
W	219	47120	4922	337	748	737	170	1393	223	196	4	1	48	260	12	4117	56390

County: 87
 Station: 0045
 Description: W OF ALTON RD
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	129	180	131	114	554	127	97	86	96	406	960		
0100	89	99	80	42	310	78	59	53	47	237	547		
0200	29	41	31	29	130	48	39	47	39	173	303		
0300	26	25	31	33	115	27	38	40	43	148	263		
0400	33	35	63	80	211	58	74	75	76	283	494		
0500	92	126	171	235	624	84	83	115	107	389	1013		
0600	249	335	464	553	1601	112	125	166	177	580	2181		
0700	487	461	474	380	1802	282	328	408	455	1473	3275		
0800	420	280	309	257	1266	385	358	368	383	1494	2760		
0900	227	341	386	333	1287	356	334	299	293	1282	2569		
1000	313	312	327	274	1226	317	279	342	311	1249	2475		
1100	279	279	289	273	1120	324	288	303	329	1244	2364		
1200	277	293	262	270	1102	287	274	316	283	1160	2262		
1300	332	292	330	307	1261	271	297	303	282	1153	2414		
1400	295	354	362	333	1344	306	303	340	329	1278	2622		
1500	358	332	321	350	1361	391	464	484	391	1730	3091		
1600	369	359	394	355	1477	480	431	448	432	1791	3268		
1700	346	347	370	383	1446	442	473	398	353	1666	3112		
1800	344	344	426	445	1559	361	277	287	289	1214	2773		
1900	362	329	285	237	1213	316	294	247	217	1074	2287		
2000	222	214	212	191	839	210	206	182	150	748	1587		
2100	207	190	200	173	770	162	134	137	151	584	1354		
2200	189	162	179	179	709	166	127	138	127	558	1267		
2300	133	102	95	78	408	175	179	177	158	689	1097		
24-Hour Totals:						23735						22603	46338

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	645	1975	730	1606	715	3311
P.M.	1800	1559	1515	1819	1600	3268
Daily	645	1975	1515	1819	715	3311
Truck Percentage	6.89		3.35		5.16	

Classification Summary Database																	
Dir	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TotTrk	TotVol
E	83	19885	1959	258	1051	126	79	55	43	0	1	16	7	0	172	1636	23735
W	79	20134	1618	196	457	38	6	20	38	1	0	0	1	0	15	757	22603

County: 87
 Station: 0045
 Description: W OF ALTON RD
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	65	53	49	44	211	120	124	85	68	397	608		
0100	28	26	26	25	105	61	50	52	36	199	304		
0200	19	23	19	16	77	41	38	28	25	132	209		
0300	18	13	23	28	82	22	24	34	22	102	184		
0400	15	45	46	70	176	24	29	30	34	117	293		
0500	63	126	203	209	601	52	46	76	78	252	853		
0600	259	360	465	554	1638	100	120	164	178	562	2200		
0700	425	441	378	373	1617	275	338	361	493	1467	3084		
0800	420	430	437	252	1539	382	357	394	358	1491	3030		
0900	319	388	400	415	1522	330	341	342	302	1315	2837		
1000	424	324	290	303	1341	299	285	305	270	1159	2500		
1100	251	234	266	324	1075	312	334	323	302	1271	2346		
1200	272	289	305	297	1163	311	285	280	272	1148	2311		
1300	290	293	286	361	1230	270	266	290	278	1104	2334		
1400	325	290	367	365	1347	313	329	352	352	1346	2693		
1500	332	302	316	223	1173	350	442	394	412	1598	2771		
1600	389	346	344	343	1422	472	490	461	408	1831	3253		
1700	298	298	363	337	1296	433	436	407	313	1589	2885		
1800	359	387	378	380	1504	355	265	282	250	1152	2656		
1900	413	347	280	310	1350	247	231	206	189	873	2223		
2000	254	231	241	197	923	173	194	171	161	699	1622		
2100	223	177	201	217	818	171	136	154	155	616	1434		
2200	157	181	167	196	701	154	144	115	142	555	1256		
2300	136	127	83	86	432	197	184	165	128	674	1106		
24-Hour Totals:						23343						21649	44992

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	645	1798	745	1626	745	3286
P.M.	1800	1504	1545	1835	1600	3253
Daily	630	1885	1545	1835	745	3286
Truck Percentage	6.37		4.12		5.28	

Classification Summary Database																	
Dir	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TotTrk	TotVol
E	82	19731	1983	253	1040	84	21	40	36	4	2	5	1	0	61	1486	23343
W	74	18935	1646	214	469	85	23	27	55	3	5	9	1	0	103	891	21649

County: 87
 Station: 0045
 Description: W OF ALTON RD
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	56	54	57	41	208	148	129	98	81	456	664		
0100	39	39	29	25	132	76	59	51	38	224	356		
0200	18	29	26	23	96	33	49	39	31	152	248		
0300	23	14	13	30	80	18	25	21	28	92	172		
0400	29	37	52	73	191	32	35	44	33	144	335		
0500	90	120	171	210	591	45	53	71	77	246	837		
0600	259	369	475	595	1698	110	127	188	182	607	2305		
0700	467	442	457	412	1778	266	310	378	443	1397	3175		
0800	347	335	289	253	1224	354	333	406	379	1472	2696		
0900	316	345	374	415	1450	352	308	310	346	1316	2766		
1000	319	327	317	318	1281	343	287	325	310	1265	2546		
1100	312	274	291	274	1151	303	344	298	332	1277	2428		
1200	257	277	288	287	1109	322	314	312	287	1235	2344		
1300	288	291	366	311	1256	311	321	292	286	1210	2466		
1400	287	328	357	386	1358	334	340	340	340	1354	2712		
1500	340	387	335	294	1356	405	406	466	454	1731	3087		
1600	346	362	386	327	1421	434	439	467	394	1734	3155		
1700	290	353	398	379	1420	466	452	409	374	1701	3121		
1800	325	386	358	447	1516	335	335	325	297	1292	2808		
1900	380	408	338	261	1387	299	319	266	198	1082	2469		
2000	238	217	236	202	893	198	166	158	187	709	1602		
2100	233	218	213	223	887	179	173	163	125	640	1527		
2200	201	207	185	243	836	160	148	136	126	570	1406		
2300	144	141	121	112	518	208	192	196	172	768	1286		
24-Hour Totals:						23837						22674	46511

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	645	1961	745	1536	700	3175
P.M.	1800	1516	1545	1794	1545	3182
Daily	630	1979	1545	1794	1545	3182
Truck Percentage	6.70		3.55		5.16	

Classification Summary Database																	
Dir	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TotTrk	TotVol
E	128	20051	1954	280	1019	137	43	49	47	5	2	9	6	0	107	1597	23837
W	53	20080	1730	203	499	32	0	20	44	3	0	0	3	0	7	804	22674

72-Hour Speed Data

County: 87
 Station: 0041
 Description: SR 112, E OF NW 2 AVE
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	224	201	170	164	759	319	252	227	203	1001	1760		
0100	105	135	88	71	399	196	164	159	130	649	1048		
0200	61	68	54	55	238	144	124	108	107	483	721		
0300	59	75	77	76	287	84	97	111	87	379	666		
0400	85	99	189	193	566	121	136	160	143	560	1126		
0500	249	359	566	744	1918	187	223	287	340	1037	2955		
0600	875	1042	1203	1288	4408	398	513	702	736	2349	6757		
0700	1260	1380	1423	1391	5454	996	1072	1104	1248	4420	9874		
0800	1333	1213	1249	1179	4974	1300	1250	1176	1171	4897	9871		
0900	1168	1197	1216	1292	4873	1069	999	979	911	3958	8831		
1000	1159	1066	1078	1053	4356	903	937	934	877	3651	8007		
1100	888	830	949	933	3600	991	935	894	908	3728	7328		
1200	827	918	960	928	3633	921	899	794	810	3424	7057		
1300	898	894	913	890	3595	941	931	947	882	3701	7296		
1400	841	972	1131	1098	4042	1051	1081	1018	1117	4267	8309		
1500	1006	1003	1017	1048	4074	1189	1375	1383	1154	5101	9175		
1600	1002	938	999	1069	4008	1202	1184	1275	1184	4845	8853		
1700	981	972	1051	1133	4137	1218	1206	1177	1069	4670	8807		
1800	1054	1088	1245	1199	4586	1015	977	890	848	3730	8316		
1900	1156	1173	1065	1008	4402	915	885	870	717	3387	7789		
2000	932	791	770	618	3111	757	691	634	512	2594	5705		
2100	629	610	619	601	2459	520	513	461	480	1974	4433		
2200	561	534	534	505	2134	461	481	410	414	1766	3900		
2300	404	371	372	347	1494	451	467	383	367	1668	3162		
24-Hour Totals:						73507						68239	141746

	Peak Volume Information					
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0715	5527	0745	4974	0730	10262
P.M.	1830	4773	1515	5114	1445	9188
Daily	0715	5527	1515	5114	0730	10262

Speed Record Database

Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	3048	2493	992	4424	17643	21347	13931	6284	2354	747	200	44	0	0	0	73507
W	2504	3284	5265	8455	5612	9524	13457	10926	6005	2141	668	398	0	0	0	68239

County: 87
 Station: 0041
 Description: SR 112, E OF NW 2 AVE
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	291	242	188	155	876	335	302	219	190	1046	1922		
0100	154	100	87	88	429	181	141	147	114	583	1012		
0200	78	82	74	70	304	99	84	85	65	333	637		
0300	73	78	65	83	299	62	87	86	77	312	611		
0400	89	120	175	242	626	69	87	103	106	365	991		
0500	279	337	543	657	1816	148	181	268	311	908	2724		
0600	853	1044	1173	1316	4386	385	490	703	780	2358	6744		
0700	1231	1180	1155	971	4537	1019	1081	1104	1217	4421	8958		
0800	1148	1279	1274	1255	4956	1213	1131	1124	1155	4623	9579		
0900	1254	1169	1155	1157	4735	964	935	968	989	3856	8591		
1000	1090	1060	1028	1071	4249	934	877	904	900	3615	7864		
1100	911	980	1112	1094	4097	849	1003	902	933	3687	7784		
1200	924	973	935	960	3792	945	918	910	905	3678	7470		
1300	899	1014	914	1017	3844	865	913	902	920	3600	7444		
1400	962	1082	1163	1144	4351	1050	1098	1073	1106	4327	8678		
1500	1073	1050	1083	980	4186	1102	1206	1150	1050	4508	8694		
1600	986	1036	1117	1092	4231	1069	967	740	989	3765	7996		
1700	1075	1102	1103	1188	4468	882	733	900	981	3496	7964		
1800	1315	1297	1354	1174	5140	797	992	940	800	3529	8669		
1900	1076	1045	1024	979	4124	790	702	707	629	2828	6952		
2000	929	909	861	704	3403	597	589	541	476	2203	5606		
2100	698	728	647	595	2668	477	438	499	462	1876	4544		
2200	584	731	639	657	2611	423	408	349	421	1601	4212		
2300	467	441	354	354	1616	451	444	467	388	1750	3366		
24-Hour Totals:						75744						63268	139012

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0815	5062	0745	4685	0800	9579
P.M.	1745	5154	1445	4564	1430	8917
Daily	1745	5154	0745	4685	0800	9579

Speed Record Database

Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	2311	1491	877	4366	17909	21954	15191	7573	2806	921	274	71	0	0	0	75744
W	7396	2908	3799	5970	5070	8428	12398	9533	5049	1855	533	329	0	0	0	63268

County: 87
 Station: 0041
 Description: SR 112, E OF NW 2 AVE
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total	
0000	288	259	227	220	994	349	305	254	212	1120	2114
0100	144	150	118	112	524	191	156	136	99	582	1106
0200	118	89	74	57	338	122	117	97	74	410	748
0300	72	63	82	89	306	71	71	69	68	279	585
0400	66	122	191	210	589	73	104	106	106	389	978
0500	287	382	531	628	1828	154	178	256	285	873	2701
0600	816	1046	1139	1218	4219	330	492	673	744	2239	6458
0700	1099	1176	1404	1375	5054	918	1009	829	663	3419	8473
0800	1295	1314	1274	1287	5170	831	882	980	1032	3725	8895
0900	1113	1129	1142	1152	4536	929	908	831	782	3450	7986
1000	1194	1040	1048	1096	4378	820	814	765	720	3119	7497
1100	1025	980	975	1023	4003	695	641	656	587	2579	6582
1200	914	973	1010	1014	3911	524	574	518	480	2096	6007
1300	967	980	1051	1031	4029	516	589	633	614	2352	6381
1400	1011	1013	1115	1151	4290	704	795	861	767	3127	7417
1500	976	1028	1106	1025	4135	669	685	733	706	2793	6928
1600	1068	1030	1157	1165	4420	713	635	629	629	2606	7026
1700	1120	1166	1213	1268	4767	650	693	641	617	2601	7368
1800	1237	1226	1256	1098	4817	545	587	540	483	2155	6972
1900	1181	1154	1068	1016	4419	469	544	478	384	1875	6294
2000	852	937	917	741	3447	364	360	313	292	1329	4776
2100	708	734	664	623	2729	301	317	294	276	1188	3917
2200	696	690	638	678	2702	253	279	281	250	1063	3765
2300	538	542	507	434	2021	264	326	295	221	1106	3127
24-Hour Totals:	77626					46475					124101

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0730	5388	0830	3849	0800	8895
P.M.	1745	4987	1400	3127	1400	7417
Daily	0730	5388	0830	3849	0800	8895

Speed Record Database

Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	1413	2064	1143	4281	17328	23002	16263	7848	3024	935	246	79	0	0	0	77626
W	2438	1160	1940	3882	4911	9004	9526	9117	3035	951	281	230	0	0	0	46475

County: 87
 Station: 0042
 Description: E OF US-1
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	237	287	205	167	896	202	169	128	130	629	1525	
0100	179	149	95	78	501	111	96	90	67	364	865	
0200	49	63	42	49	203	69	70	67	59	265	468	
0300	32	34	52	70	188	46	51	65	55	217	405	
0400	62	100	159	211	532	74	82	109	109	374	906	
0500	205	293	419	508	1425	116	136	158	187	597	2022	
0600	565	755	888	908	3116	199	230	367	333	1129	4245	
0700	875	840	861	900	3476	503	514	565	669	2251	5727	
0800	868	794	780	751	3193	618	618	636	572	2444	5637	
0900	690	667	734	687	2778	559	516	573	533	2181	4959	
1000	704	654	695	557	2610	494	562	505	526	2087	4697	
1100	576	613	577	646	2412	589	550	526	496	2161	4573	
1200	578	607	539	523	2247	565	556	487	518	2126	4373	
1300	553	549	646	579	2327	512	566	600	555	2233	4560	
1400	576	732	737	683	2728	581	657	636	658	2532	5260	
1500	675	660	665	621	2621	713	865	938	901	3417	6038	
1600	677	676	706	721	2780	840	846	884	857	3427	6207	
1700	669	695	656	671	2691	863	943	835	809	3450	6141	
1800	693	684	860	789	3026	629	644	541	477	2291	5317	
1900	689	616	525	462	2292	573	473	567	373	1986	4278	
2000	403	389	370	338	1500	404	378	326	270	1378	2878	
2100	371	336	361	328	1396	243	260	241	250	994	2390	
2200	363	345	376	354	1438	239	261	220	239	959	2397	
2300	227	213	176	167	783	273	312	262	209	1056	1839	
24-Hour Totals:					47159						40548	87707

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0630	3511	0745	2541	0730	5893
P.M.	1800	3026	1630	3547	1630	6338
Daily	0630	3511	1630	3547	1630	6338

Speed Record Database

Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	20	24	51	86	264	3038	11130	16895	10226	4127	946	208	144	0	0	47159
W	36	8	14	52	144	2117	7081	11914	10486	5967	1927	511	291	0	0	40548

County: 87
 Station: 0042
 Description: E OF US-1
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	115	101	113	92	421	199	211	153	114	677	1098	
0100	74	49	43	55	221	114	81	88	76	359	580	
0200	37	43	33	44	157	52	66	60	39	217	374	
0300	41	34	54	58	187	35	54	58	43	190	377	
0400	44	93	153	199	489	55	48	61	75	239	728	
0500	193	330	439	489	1451	94	103	138	180	515	1966	
0600	582	769	902	959	3212	209	238	317	364	1128	4340	
0700	814	875	534	588	2811	511	556	552	643	2262	5073	
0800	658	620	692	664	2634	666	620	600	619	2505	5139	
0900	774	701	552	385	2412	538	528	581	553	2200	4612	
1000	607	630	574	573	2384	558	550	541	554	2203	4587	
1100	484	541	540	552	2117	520	621	578	544	2263	4380	
1200	562	562	588	558	2270	582	590	529	501	2202	4472	
1300	512	548	592	585	2237	567	539	569	537	2212	4449	
1400	650	628	687	652	2617	646	703	718	785	2852	5469	
1500	636	575	564	372	2147	739	829	781	827	3176	5323	
1600	803	600	640	630	2673	820	869	615	607	2911	5584	
1700	571	581	673	648	2473	695	715	490	643	2543	5016	
1800	668	666	727	746	2807	642	613	553	538	2346	5153	
1900	767	646	627	536	2576	499	490	487	392	1868	4444	
2000	430	441	435	384	1690	381	343	324	318	1366	3056	
2100	377	322	349	374	1422	282	256	317	266	1121	2543	
2200	330	332	345	373	1380	275	246	229	229	979	2359	
2300	226	211	179	155	771	293	292	287	260	1132	1903	
24-Hour Totals:					43559						39466	83025

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0630	3550	0745	2529	0630	5298
P.M.	1815	2906	1530	3297	1530	5636
Daily	0630	3550	1530	3297	1530	5636

Speed Record Database

Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	2702	1627	458	362	383	2432	8212	13157	9115	3786	970	236	119	0	0	43559
W	1149	375	512	307	399	2828	7060	10712	9066	4952	1484	422	200	0	0	39466

County: 87
 Station: 0042
 Description: E OF US-1
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	117	107	92	83	399	216	206	173	125	720	1119	
0100	59	61	50	50	220	120	110	91	58	379	599	
0200	42	46	49	38	175	57	69	74	48	248	423	
0300	35	23	46	50	154	43	44	32	47	166	320	
0400	50	77	145	172	444	38	63	82	69	252	696	
0500	210	301	385	496	1392	85	115	131	178	509	1901	
0600	614	736	943	885	3178	214	278	331	421	1244	4422	
0700	817	876	801	908	3402	511	617	633	638	2399	5801	
0800	852	769	746	725	3092	624	690	669	637	2620	5712	
0900	662	693	779	743	2877	592	571	570	574	2307	5184	
1000	661	652	611	631	2555	605	568	557	545	2275	4830	
1100	606	545	597	531	2279	580	568	609	572	2329	4608	
1200	478	540	551	574	2143	577	621	555	544	2297	4440	
1300	564	561	567	567	2259	601	646	662	532	2441	4700	
1400	547	630	657	673	2507	670	688	768	747	2873	5380	
1500	610	655	617	615	2497	783	884	916	871	3454	5951	
1600	605	628	664	597	2494	910	914	843	849	3516	6010	
1700	582	673	647	717	2619	779	918	903	776	3376	5995	
1800	667	722	742	777	2908	645	681	608	593	2527	5435	
1900	731	788	610	463	2592	561	647	558	454	2220	4812	
2000	440	441	396	413	1690	396	422	318	313	1449	3139	
2100	405	408	424	407	1644	313	330	259	255	1157	2801	
2200	359	426	272	364	1421	215	308	269	260	1052	2473	
2300	244	228	191	181	844	303	343	365	297	1308	2152	
24-Hour Totals:					45785						43118	88903

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0630	3521	0745	2621	0715	5949
P.M.	1830	3038	1530	3611	1530	6076
Daily	0630	3521	1530	3611	1530	6076

Speed Record Database

Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	286	118	130	204	430	3604	10434	15382	9927	3975	978	204	113	0	0	45785
W	71	98	81	155	288	3521	10030	14379	9151	4002	990	245	107	0	0	43118

County: 87
 Station: 0043
 Description: SR 112, E OF INTERCOSTAL WATERWAY BRIDGE
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	275	328	240	201	1044	246	221	172	167	806	1850	
0100	202	179	120	94	595	149	115	102	95	461	1056	
0200	70	78	56	68	272	95	77	88	72	332	604	
0300	44	47	62	83	236	56	63	80	67	266	502	
0400	74	112	177	242	605	76	106	120	113	415	1020	
0500	233	326	475	568	1602	123	153	171	196	643	2245	
0600	643	857	1077	1112	3689	230	267	388	401	1286	4975	
0700	1044	991	1050	1092	4177	547	629	628	740	2544	6721	
0800	1102	1020	977	933	4032	730	732	723	717	2902	6934	
0900	882	817	909	859	3467	687	623	670	644	2624	6091	
1000	849	826	842	671	3188	600	605	617	629	2451	5639	
1100	704	748	722	785	2959	659	596	617	604	2476	5435	
1200	714	723	677	678	2792	607	640	602	569	2418	5210	
1300	701	738	792	771	3002	603	618	668	620	2509	5511	
1400	720	915	931	843	3409	658	763	765	797	2983	6392	
1500	829	823	817	791	3260	896	1043	1044	1023	4006	7266	
1600	867	834	864	886	3451	1055	959	972	1011	3997	7448	
1700	824	873	839	850	3386	939	1036	1013	992	3980	7366	
1800	875	837	1040	949	3701	788	802	673	606	2869	6570	
1900	838	766	658	570	2832	700	603	628	461	2392	5224	
2000	512	486	470	426	1894	478	454	369	344	1645	3539	
2100	463	418	445	408	1734	320	316	290	313	1239	2973	
2200	414	381	421	384	1600	294	316	268	300	1178	2778	
2300	261	227	196	191	875	335	375	305	302	1317	2192	
24-Hour Totals:					57802						47739	105541

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0730	4264	0745	2925	0745	7116
P.M.	1800	3701	1515	4165	1515	7463
Daily	0730	4264	1515	4165	1515	7463

Speed Record Database

Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	24	34	129	630	264	8870	14075	17763	10493	4186	972	213	149	0	0	57802
W	1036	2459	1831	995	467	1849	4327	7060	8047	9463	6157	2139	1909	0	0	47739

County: 87
 Station: 0043
 Description: SR 112, E OF INTERCOSTAL WATERWAY BRIDGE
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	134	114	131	107	486	287	249	185	143	864	1350	
0100	75	61	49	68	253	134	101	101	77	413	666	
0200	47	46	36	46	175	74	74	69	50	267	442	
0300	49	38	60	59	206	49	59	60	61	229	435	
0400	44	105	166	218	533	66	57	71	84	278	811	
0500	211	372	501	562	1646	94	111	150	190	545	2191	
0600	651	889	1089	1150	3779	205	239	377	413	1234	5013	
0700	993	1039	741	773	3546	560	660	705	777	2702	6248	
0800	898	832	919	869	3518	780	704	752	774	3010	6528	
0900	951	860	718	529	3058	620	611	686	677	2594	5652	
1000	763	773	717	750	3003	632	665	608	658	2563	5566	
1100	613	677	713	718	2721	582	703	642	658	2585	5306	
1200	711	695	736	706	2848	651	646	619	586	2502	5350	
1300	697	713	760	743	2913	597	589	642	638	2466	5379	
1400	811	806	860	844	3321	726	768	825	865	3184	6505	
1500	781	730	713	526	2750	815	1083	916	1010	3824	6574	
1600	966	765	762	806	3299	1026	989	916	841	3772	7071	
1700	720	771	803	803	3097	747	880	961	778	3366	6463	
1800	830	852	885	933	3500	779	746	635	641	2801	6301	
1900	927	796	766	671	3160	602	608	571	465	2246	5406	
2000	574	549	545	480	2148	415	421	375	361	1572	3720	
2100	470	416	434	461	1781	329	304	341	295	1269	3050	
2200	401	411	409	429	1650	311	301	279	282	1173	2823	
2300	279	261	219	195	954	390	350	372	273	1385	2339	
24-Hour Totals:					54345						46844	101189

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0630	4271	0745	3013	0800	6528
P.M.	1815	3597	1515	4035	1600	7071
Daily	0630	4271	1515	4035	1600	7071

Speed Record Database

Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	2979	1841	874	1448	383	7715	10735	13883	9301	3832	991	239	124	0	0	54345
W	2711	3098	1222	601	517	2986	4592	6376	7559	8585	5157	1840	1600	0	0	46844

County: 87
 Station: 0043
 Description: SR 112, E OF INTERCOSTAL WATERWAY BRIDGE
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	155	141	122	102	520	262	243	222	174	901	1421		
0100	88	81	65	63	297	146	122	114	80	462	759		
0200	54	63	64	46	227	79	92	74	57	302	529		
0300	46	35	52	61	194	53	50	35	59	197	391		
0400	62	101	162	201	526	53	73	79	79	284	810		
0500	240	342	438	565	1585	100	126	148	175	549	2134		
0600	698	841	1111	1069	3719	227	270	362	471	1330	5049		
0700	974	1037	987	1084	4082	568	646	687	777	2678	6760		
0800	1045	975	931	909	3860	783	774	762	787	3106	6966		
0900	858	865	933	921	3577	684	691	657	652	2684	6261		
1000	807	819	765	797	3188	656	651	701	667	2675	5863		
1100	749	677	734	672	2832	633	673	633	713	2652	5484		
1200	616	684	702	668	2670	659	742	635	639	2675	5345		
1300	731	728	741	732	2932	660	701	661	620	2642	5574		
1400	710	823	852	871	3256	719	812	816	777	3124	6380		
1500	787	811	806	789	3193	915	959	1098	1042	4014	7207		
1600	776	818	848	737	3179	1144	1080	1029	977	4230	7409		
1700	751	843	835	868	3297	942	1092	1023	928	3985	7282		
1800	834	909	902	957	3602	772	840	752	677	3041	6643		
1900	873	967	731	589	3160	649	729	625	558	2561	5721		
2000	557	554	527	517	2155	438	450	392	405	1685	3840		
2100	504	492	507	508	2011	371	397	320	282	1370	3381		
2200	439	511	361	421	1732	300	334	325	325	1284	3016		
2300	314	281	241	229	1065	325	399	406	322	1452	2517		
24-Hour Totals:						56859						49883	106742

	Peak Volume Information					
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0630	4191	0800	3106	0745	7131
P.M.	1830	3699	1530	4364	1530	7553
Daily	0630	4191	1530	4364	1530	7553

Speed Record Database

Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	317	165	323	1061	430	9774	13107	16157	10160	4041	998	212	114	0	0	56859
W	156	144	292	270	179	3149	7127	10499	10057	9686	5210	1728	1386	0	0	49883

County: 87
 Station: 0044
 Description: SR 112, E OF BISCAYNE BAY BRIDGE
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	277	315	265	224	1081	292	245	187	194	918	1999		
0100	185	194	138	93	610	159	130	107	93	489	1099		
0200	73	83	65	68	289	103	86	94	76	359	648		
0300	49	47	62	75	233	62	66	77	65	270	503		
0400	70	106	146	223	545	81	116	137	115	449	994		
0500	207	286	402	485	1380	155	171	212	239	777	2157		
0600	542	642	780	829	2793	277	328	449	479	1533	4326		
0700	696	768	827	818	3109	651	805	862	909	3227	6336		
0800	808	693	683	655	2839	909	886	869	876	3540	6379		
0900	591	613	729	667	2600	811	776	778	743	3108	5708		
1000	676	667	707	631	2681	710	735	705	727	2877	5558		
1100	576	609	614	609	2408	825	727	725	744	3021	5429		
1200	620	649	620	595	2484	743	710	699	675	2827	5311		
1300	629	607	658	655	2549	706	728	782	740	2956	5505		
1400	589	698	753	740	2780	805	818	908	911	3442	6222		
1500	667	684	636	668	2655	1057	1213	1131	1039	4440	7095		
1600	659	690	753	699	2801	1139	947	994	1038	4118	6919		
1700	692	695	697	673	2757	1026	900	1043	954	3923	6680		
1800	682	691	774	821	2968	897	885	772	700	3254	6222		
1900	707	683	625	546	2561	782	712	698	525	2717	5278		
2000	484	470	468	419	1841	540	532	442	387	1901	3742		
2100	444	426	427	379	1676	350	383	341	354	1428	3104		
2200	365	404	383	384	1536	339	367	305	330	1341	2877		
2300	286	225	213	177	901	409	416	361	343	1529	2430		
24-Hour Totals:						48077						54444	102521

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0715	3221	0745	3573	0730	6712
P.M.	1815	2993	1515	4522	1515	7169
Daily	0715	3221	1515	4522	1515	7169

Speed Record Database

Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	1012	1613	650	904	2084	4041	11123	12844	7201	3931	768	143	1763	0	0	48077
W	1415	1936	2221	1800	964	2595	6290	11967	12505	8512	3018	751	470	0	0	54444

County: 87
 Station: 0044
 Description: SR 112, E OF BISCAYNE BAY BRIDGE
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	163	117	121	106	507	297	288	202	156	943	1450	
0100	89	61	60	54	264	151	109	113	86	459	723	
0200	63	50	42	33	188	77	84	75	48	284	472	
0300	48	41	46	61	196	55	65	64	62	246	442	
0400	41	88	139	191	459	68	63	79	84	294	753	
0500	187	302	427	484	1400	112	118	166	222	618	2018	
0600	557	690	829	813	2889	248	295	453	464	1460	4349	
0700	804	752	704	687	2947	693	769	814	949	3225	6172	
0800	770	752	831	780	3133	918	844	888	906	3556	6689	
0900	695	601	595	638	2529	743	779	773	788	3083	5612	
1000	670	646	625	617	2558	745	769	735	750	2999	5557	
1100	536	558	551	638	2283	701	801	785	762	3049	5332	
1200	604	641	611	619	2475	757	775	703	693	2928	5403	
1300	608	628	590	684	2510	723	709	754	758	2944	5454	
1400	640	628	722	763	2753	808	920	1013	948	3689	6442	
1500	639	642	619	529	2429	1026	1122	771	930	3849	6278	
1600	656	659	663	696	2674	1008	997	989	905	3899	6573	
1700	628	675	668	631	2602	827	932	922	954	3635	6237	
1800	727	726	746	789	2988	865	809	777	735	3186	6174	
1900	769	750	628	641	2788	698	679	639	543	2559	5347	
2000	546	534	502	459	2041	344	287	301	401	1333	3374	
2100	455	420	409	433	1717	384	350	392	348	1474	3191	
2200	402	386	388	417	1593	369	348	309	345	1371	2964	
2300	308	259	229	197	993	452	422	409	320	1603	2596	
24-Hour Totals:					46916						52686	99602

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0630	3198	0745	3599	0800	6689
P.M.	1830	3054	1430	4109	1430	6875
Daily	0630	3198	1430	4109	1430	6875

Speed Record Database

Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	984	1441	503	602	1740	3992	10119	12442	7670	4662	688	155	1918	0	0	46916
W	3359	3093	2460	917	484	2285	6744	11506	11484	7139	2313	594	308	0	0	52686

County: 87
 Station: 0044
 Description: SR 112, E OF BISCAYNE BAY BRIDGE
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	175	152	123	109	559	290	272	235	183	980	1539	
0100	104	82	71	65	322	155	122	121	82	480	802	
0200	50	70	53	57	230	82	91	82	58	313	543	
0300	51	39	40	60	190	57	50	39	56	202	392	
0400	57	81	143	203	484	58	74	87	80	299	783	
0500	215	294	378	507	1394	108	134	162	206	610	2004	
0600	549	731	813	852	2945	252	309	442	512	1515	4460	
0700	838	782	818	850	3288	637	765	821	876	3099	6387	
0800	827	709	714	629	2879	880	832	979	878	3569	6448	
0900	663	710	756	761	2890	822	789	746	801	3158	6048	
1000	665	685	697	685	2732	750	729	768	768	3015	5747	
1100	653	634	588	589	2464	717	769	747	785	3018	5482	
1200	560	587	625	572	2344	747	838	772	734	3091	5435	
1300	624	645	672	677	2618	767	831	779	733	3110	5728	
1400	636	679	730	751	2796	885	929	981	937	3732	6528	
1500	710	713	685	626	2734	1050	1129	1115	1017	4311	7045	
1600	710	716	731	656	2813	1037	1038	1066	1029	4170	6983	
1700	637	754	740	731	2862	1106	982	1039	965	4092	6954	
1800	673	767	727	817	2984	906	968	833	801	3508	6492	
1900	776	776	700	575	2827	761	864	714	610	2949	5776	
2000	566	547	512	459	2084	528	511	458	463	1960	4044	
2100	489	492	444	481	1906	417	464	379	336	1596	3502	
2200	423	429	440	455	1747	355	400	354	365	1474	3221	
2300	374	307	245	256	1182	404	476	477	385	1742	2924	
24-Hour Totals:					49274						55993	105267

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0645	3290	0800	3569	0745	6667
P.M.	1830	3096	1500	4311	1445	7090
Daily	0645	3290	1500	4311	1445	7090

Speed Record Database

Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	986	1399	469	558	1967	4186	10972	12724	8121	4752	648	177	2315	0	0	49274
W	932	1429	1887	2020	1207	4460	9924	13854	11255	6393	1914	471	247	0	0	55993

County: 87
 Station: 0045
 Description: W OF ALTON RD
 Start Date: 10/24/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	129	180	130	114	553	106	86	73	83	348	901		
0100	89	99	79	42	309	67	53	47	43	210	519		
0200	29	41	31	28	129	42	37	40	32	151	280		
0300	26	24	31	33	114	25	32	36	42	135	249		
0400	33	35	63	79	210	50	63	66	68	247	457		
0500	89	125	171	235	620	79	71	106	93	349	969		
0600	248	335	464	553	1600	95	110	140	152	497	2097		
0700	487	461	474	380	1802	252	297	341	372	1262	3064		
0800	420	280	309	257	1266	323	302	310	343	1278	2544		
0900	227	341	386	332	1286	304	298	271	260	1133	2419		
1000	313	312	327	272	1224	276	246	306	282	1110	2334		
1100	279	279	289	272	1119	277	259	261	305	1102	2221		
1200	277	293	261	270	1101	248	242	290	250	1030	2131		
1300	332	291	330	305	1258	243	265	281	252	1041	2299		
1400	294	354	360	333	1341	280	264	294	278	1116	2457		
1500	358	332	320	350	1360	339	415	431	356	1541	2901		
1600	368	358	394	354	1474	450	395	427	398	1670	3144		
1700	346	346	370	383	1445	408	469	365	325	1567	3012		
1800	344	344	426	445	1559	314	242	262	247	1065	2624		
1900	362	329	285	237	1213	284	248	217	190	939	2152		
2000	222	214	211	191	838	187	183	158	131	659	1497		
2100	207	189	200	170	766	137	114	112	127	490	1256		
2200	189	162	176	179	706	145	114	124	113	496	1202		
2300	129	101	95	77	402	150	158	156	141	605	1007		
24-Hour Totals:						23695						20041	43736

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0645	1975	0730	1338	0715	3068
P.M.	1815	1577	1630	1702	1600	3144
Daily	0645	1975	1630	1702	1600	3144

Speed Record Database

Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	1739	389	325	903	2082	4209	5539	4701	2506	950	276	76	0	0	0	23695
W	140	362	2167	6682	6586	2862	926	232	68	10	3	3	0	0	0	20041

County: 87
 Station: 0045
 Description: W OF ALTON RD
 Start Date: 10/25/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total		
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total			
0000	65	52	49	43	209	109	110	79	57	355	564		
0100	28	26	26	25	105	58	43	45	31	177	282		
0200	19	23	18	16	76	32	34	24	21	111	187		
0300	18	13	23	28	82	20	22	31	21	94	176		
0400	15	45	46	69	175	23	25	28	29	105	280		
0500	61	125	203	207	596	46	42	66	68	222	818		
0600	258	359	465	554	1636	90	102	146	140	478	2114		
0700	425	441	378	373	1617	240	293	296	426	1255	2872		
0800	420	430	437	252	1539	323	304	348	304	1279	2818		
0900	319	388	400	415	1522	290	305	292	265	1152	2674		
1000	424	324	290	302	1340	271	248	269	239	1027	2367		
1100	249	233	265	323	1070	284	298	285	268	1135	2205		
1200	272	289	305	297	1163	270	260	247	238	1015	2178		
1300	290	293	286	361	1230	241	235	267	249	992	2222		
1400	325	290	367	365	1347	277	289	314	315	1195	2542		
1500	331	302	316	221	1170	302	394	382	410	1488	2658		
1600	388	346	344	342	1420	466	479	460	408	1813	3233		
1700	298	298	363	337	1296	433	436	399	287	1555	2851		
1800	359	387	378	380	1504	316	249	250	229	1044	2548		
1900	413	347	279	310	1349	216	208	184	166	774	2123		
2000	254	231	241	197	923	158	169	155	145	627	1550		
2100	223	177	201	216	817	154	120	126	135	535	1352		
2200	157	180	167	196	700	134	119	101	130	484	1184		
2300	135	127	83	86	431	177	160	140	113	590	1021		
24-Hour Totals:						23317						19502	42819

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0630	1885	0745	1401	0745	3061
P.M.	1815	1558	1545	1815	1600	3233
Daily	0630	1885	1545	1815	1600	3233

Speed Record Database																
Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	975	322	284	942	2538	4638	5715	4441	2339	818	238	67	0	0	0	23317
W	1459	1126	2263	5787	5418	2436	748	189	58	13	5	0	0	0	0	19502

County: 87
 Station: 0045
 Description: W OF ALTON RD
 Start Date: 10/26/2017
 Start Time: 0000

Time	Direction: E					Direction: W					Combined Total	
	1st	2nd	3rd	4th	Total	1st	2nd	3rd	4th	Total		
0000	56	54	57	41	208	131	116	88	71	406	614	
0100	39	39	29	25	132	67	52	43	30	192	324	
0200	18	29	25	23	95	28	41	35	26	130	225	
0300	22	14	13	30	79	16	21	20	23	80	159	
0400	27	37	50	71	185	27	31	41	29	128	313	
0500	90	119	170	210	589	41	50	66	68	225	814	
0600	257	369	475	595	1696	99	109	160	155	523	2219	
0700	467	442	457	412	1778	230	259	320	370	1179	2957	
0800	347	335	289	253	1224	299	294	352	334	1279	2503	
0900	316	345	374	415	1450	311	273	283	316	1183	2633	
1000	319	327	316	317	1279	306	260	275	273	1114	2393	
1100	312	274	291	274	1151	272	307	267	302	1148	2299	
1200	257	277	288	287	1109	295	269	277	252	1093	2202	
1300	288	291	366	310	1255	267	291	257	245	1060	2315	
1400	285	328	357	386	1356	295	288	297	302	1182	2538	
1500	340	387	335	292	1354	360	364	425	418	1567	2921	
1600	346	362	385	327	1420	392	410	427	355	1584	3004	
1700	290	353	398	379	1420	429	427	369	325	1550	2970	
1800	325	386	358	447	1516	303	299	289	261	1152	2668	
1900	380	408	338	261	1387	265	278	242	176	961	2348	
2000	237	216	236	202	891	170	151	139	159	619	1510	
2100	232	217	213	223	885	155	145	137	109	546	1431	
2200	201	207	185	243	836	141	126	119	113	499	1335	
2300	143	141	121	90	495	183	171	171	148	673	1168	
24-Hour Totals:					23790						20073	43863

Peak Volume Information						
	Direction: E		Direction: W		Combined Directions	
	Hour	Volume	Hour	Volume	Hour	Volume
A.M.	0630	1979	0745	1315	0700	2957
P.M.	1830	1593	1545	1647	1545	3032
Daily	0630	1979	1545	1647	1545	3032

Speed Record Database

Dir	<=20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-80	81-85	>=86	TotVol
E	1680	446	436	1111	2633	4667	5685	4214	1985	670	215	48	0	0	0	23790
W	27	267	2316	7381	6499	2627	750	147	39	14	5	1	0	0	0	20073

FTI Data



COUNTY: 87
 STATION: 2134
 DESCRIPTION: SR 9A/I-95, 200' S NW 151 ST
 START DATE: 07/05/2016
 START TIME: 0000

TIME	DIRECTION: N					DIRECTION: S					COMBINED TOTAL
	1ST	2ND	3RD	4TH	TOTAL	1ST	2ND	3RD	4TH	TOTAL	
0000	922	888	770	660	3240	681	593	582	511	2367	5607
0100	516	519	451	414	1900	449	358	363	349	1519	3419
0200	346	306	297	258	1207	299	259	269	264	1091	2298
0300	267	304	305	269	1145	212	245	249	243	949	2094
0400	244	281	335	337	1197	249	257	368	441	1315	2512
0500	403	402	519	669	1993	439	519	774	1026	2758	4751
0600	774	901	1251	1329	4255	1253	1481	1704	1727	6165	10420
0700	1273	1346	1375	1496	5490	1733	1735	1646	1627	6741	12231
0800	1472	1233	1316	940	4961	1654	1591	1561	1552	6358	11319
0900	993	1273	1403	1469	5138	1505	1501	785	706	4497	9635
1000	1177	1102	1139	1164	4582	698	760	843	1191	3492	8074
1100	1124	1165	1333	1365	4987	1283	1260	1351	1229	5123	10110
1200	1306	1406	1550	1498	5760	1230	1304	1104	1195	4833	10593
1300	1500	1416	1465	1445	5826	1261	1126	1151	1420	4958	10784
1400	1319	1469	1883	1748	6419	1358	1445	1538	1618	5959	12378
1500	1845	1826	1947	1965	7583	1626	1611	1657	1690	6584	14167
1600	1909	1771	1833	1752	7265	1607	1625	1589	1596	6417	13682
1700	1771	1755	1782	1711	7019	1613	1586	1515	1619	6333	13352
1800	1617	1632	1772	1852	6873	1594	1636	1591	1664	6485	13358
1900	1955	2061	1828	1603	7447	1492	1462	1430	1276	5660	13107
2000	1565	1524	1496	1346	5931	1300	1335	1210	1226	5071	11002
2100	1293	1401	1434	1258	5386	1203	1128	1250	1140	4721	10107
2200	1279	1150	1093	1123	4645	1067	923	1114	1073	4177	8822
2300	940	940	981	937	3798	867	792	780	712	3151	6949
24-HOUR TOTALS:	114047					106724					220771

PEAK VOLUME INFORMATION

	DIRECTION: N		DIRECTION: S		COMBINED DIRECTIONS	
	HOUR	VOLUME	HOUR	VOLUME	HOUR	VOLUME
A.M.	715	5689	645	6841	715	12351
P.M.	1515	7647	1500	6584	1515	14212
DAILY	1845	7696	630	6899	1515	14212

COUNTY: 87
 STATION: 2023
 DESCRIPTION: SR 112/AIRPORT EXPWY, 200' E NW 17 AV
 START DATE: 05/24/2016
 START TIME: 0000

TIME	DIRECTION: E					DIRECTION: W					COMBINED TOTAL
	1ST	2ND	3RD	4TH	TOTAL	1ST	2ND	3RD	4TH	TOTAL	
0000	167	172	122	114	575	184	189	152	161	686	1261
0100	119	86	81	65	351	134	123	108	80	445	796
0200	48	67	75	62	252	65	83	92	69	309	561
0300	52	54	45	60	211	63	94	151	132	440	651
0400	78	107	103	136	424	159	176	211	247	793	1217
0500	179	224	274	236	913	272	397	470	465	1604	2517
0600	277	375	538	540	1730	462	739	893	909	3003	4733
0700	822	1062	1142	1246	4272	920	1073	1166	1286	4445	8717
0800	1270	1202	1184	1169	4825	1293	1236	1207	1224	4960	9785
0900	1104	942	856	760	3662	1057	1125	1068	1015	4265	7927
1000	751	699	682	681	2813	918	916	905	941	3680	6493
1100	708	720	778	772	2978	894	991	996	1045	3926	6904
1200	792	808	773	754	3127	1001	1048	977	1031	4057	7184
1300	728	759	719	796	3002	1037	1091	1051	1065	4244	7246
1400	714	758	805	726	3003	1063	1137	1118	1063	4381	7384
1500	782	765	867	854	3268	1189	1210	1154	1193	4746	8014
1600	801	800	816	802	3219	1072	1045	1121	1293	4531	7750
1700	777	812	783	812	3184	1286	1135	1073	993	4487	7671
1800	801	754	736	744	3035	952	928	844	868	3592	6627
1900	785	652	608	564	2609	938	758	705	631	3032	5641
2000	537	455	513	472	1977	595	537	380	412	1924	3901
2100	437	422	426	356	1641	350	362	348	260	1320	2961
2200	330	322	295	244	1191	255	251	242	179	927	2118
2300	226	238	238	271	973	199	168	143	164	674	1647
24-HOUR TOTALS:	53235					66471					119706

PEAK VOLUME INFORMATION

	DIRECTION: E		DIRECTION: W		COMBINED DIRECTIONS	
	HOUR	VOLUME	HOUR	VOLUME	HOUR	VOLUME
A.M.	745	4902	745	5022	745	9924
P.M.	1530	3322	1630	4835	1630	8042
DAILY	745	4902	745	5022	745	9924

Appendix B
Raw Bluetooth Counts
AM Peak Period

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-1 B-2>B-11 B-12	B-1 B-2	B-11 B-12	214	9
B-1 B-2>B-4	B-1 B-2	B-4	152	2.88
B-1 B-2>B-4>B-5>B-8	B-1 B-2	B-8	1	5
B-1 B-2>B-4>B-5>B-9 B-10	B-1 B-2	B-9 B-10	15	6
B-1 B-2>B-4>B-6	B-1 B-2	B-6	78	4
B-1 B-2>B-4>B-6>B-9 B-10	B-1 B-2	B-9 B-10	14	7
B-1 B-2>B-4>B-7>B-9 B-10	B-1 B-2	B-9 B-10	36	6
B-1 B-2>B-4>B-8>B-9 B-10	B-1 B-2	B-9 B-10	15	7
B-1 B-2>B-6>B-5>B-5>B-8	B-1 B-2	B-8	1	13
B-11 B-12>B-1 B-2	B-11 B-12	B-1 B-2	477	17
B-11 B-12>B-4	B-11 B-12	B-4	179	19
B-11 B-12>B-4>B-5>B-8	B-11 B-12	B-8	1	10
B-11 B-12>B-4>B-6	B-11 B-12	B-6	28	20
B-11 B-12>B-4>B-7	B-11 B-12	B-7	3	19
B-11 B-12>B-6	B-11 B-12	B-6	39	21
B-11 B-12>B-6>B-8>B-9 B-10	B-11 B-12	B-9 B-10	5	31
B-3>B-1 B-2	B-3	B-1 B-2	22	2
B-3>B-4>B-1 B-2	B-3	B-1 B-2	12	2
B-4>B-1 B-2	B-4	B-1 B-2	93	2
B-4>B-11 B-12	B-4	B-11 B-12	35	61
B-4>B-5>B-7>B-9 B-10	B-4	B-9 B-10	9	4
B-4>B-5>B-8>B-9 B-10	B-4	B-9 B-10	11	4
B-4>B-6	B-4	B-6	236	1
B-4>B-6>B-7>B-9 B-10	B-4	B-9 B-10	16	4

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-4>B-7>B-9 B-10	B-4	B-9 B-10	85	4
B-4>B-8	B-4	B-8	16	1
B-4>B-8>B-7>B-9 B-10	B-4	B-9 B-10	14	4
B-4>B-8>B-9 B-10	B-4	B-9 B-10	49	4
B-5>B-1 B-2	B-5	B-1 B-2	126	2
B-5>B-11 B-12	B-5	B-11 B-12	29	8
B-5>B-4	B-5	B-4	173	1
B-6>B-5>B-4	B-6	B-4	16	3
B-6>B-8	B-6	B-8	27	0
B-6>B-8>B-9 B-10	B-6	B-9 B-10	133	4
B-7>B-1 B-2	B-7	B-1 B-2	72	2
B-7>B-11 B-12	B-7	B-11 B-12	20	9
B-7>B-4	B-7	B-4	115	1
B-7>B-5	B-7	B-5	84	1
B-7>B-6>B-5	B-7	B-5	1	0
B-8>B-9 B-10	B-8	B-9 B-10	209	3
B-9 B-10>B-4	B-9 B-10	B-4	106	3
B-9 B-10>B-5	B-9 B-10	B-5	91	4
B-9 B-10>B-7	B-9 B-10	B-7	379	2
B-9 B-10>B-7>B-11 B-12	B-9 B-10	B-11 B-12	28	12
B-9 B-10>B-7>B-4	B-9 B-10	B-4	123	3
B-9 B-10>B-7>B-4>B-1 B-2	B-9 B-10	B-1 B-2	44	5
B-9 B-10>B-7>B-4>B-11 B-12	B-9 B-10	B-11 B-12	10	12
B-9 B-10>B-7>B-5	B-9 B-10	B-5	128	3

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-9 B-10>B-7>B-5>B-4>B-1 B-2	B-9 B-10	B-1 B-2	17	5
B-1 B-2>B-11 B-12	B-1 B-2	B-11 B-12	238	9
B-1 B-2>B-4	B-1 B-2	B-4	139	2.98
B-1 B-2>B-4>B-5>B-7>B-9 B-10	B-1 B-2	B-9 B-10	11	6
B-1 B-2>B-4>B-5>B-8	B-1 B-2	B-8	1	8
B-1 B-2>B-4>B-5>B-9 B-10	B-1 B-2	B-9 B-10	25	6
B-1 B-2>B-4>B-6>B-9 B-10	B-1 B-2	B-9 B-10	10	6
B-1 B-2>B-4>B-7>B-9 B-10	B-1 B-2	B-9 B-10	38	6
B-1 B-2>B-4>B-8	B-1 B-2	B-8	6	3
B-1 B-2>B-4>B-8>B-9 B-10	B-1 B-2	B-9 B-10	21	8
B-1 B-2>B-6	B-1 B-2	B-6	84	3
B-11 B-12>B-1 B-2	B-11 B-12	B-1 B-2	578	14
B-11 B-12>B-4	B-11 B-12	B-4	162	18
B-11 B-12>B-4>B-5>B-7>B-8>B-9 B-10	B-11 B-12	B-9 B-10	1	33
B-11 B-12>B-4>B-6	B-11 B-12	B-6	21	23
B-11 B-12>B-4>B-6>B-9 B-10	B-11 B-12	B-9 B-10	6	30
B-11 B-12>B-4>B-8	B-11 B-12	B-8	3	9
B-11 B-12>B-4>B-8>B-9 B-10	B-11 B-12	B-9 B-10	6	28
B-11 B-12>B-6	B-11 B-12	B-6	28	26
B-3>B-1 B-2	B-3	B-1 B-2	20	3
B-3>B-11 B-12	B-3	B-11 B-12	12	10
B-4>B-1 B-2	B-4	B-1 B-2	95	3
B-4>B-11 B-12	B-4	B-11 B-12	33	106
B-4>B-5>B-7>B-9 B-10	B-4	B-9 B-10	12	4

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-4>B-5>B-8>B-9 B-10	B-4	B-9 B-10	8	4
B-4>B-5>B-9 B-10	B-4	B-9 B-10	37	4
B-4>B-6	B-4	B-6	205	1
B-4>B-6>B-7>B-9 B-10	B-4	B-9 B-10	18	4
B-4>B-6>B-9 B-10	B-4	B-9 B-10	38	4
B-4>B-8	B-4	B-8	13	1
B-4>B-8>B-7	B-4	B-7	8	1
B-4>B-8>B-7>B-9 B-10	B-4	B-9 B-10	13	4
B-4>B-8>B-9 B-10	B-4	B-9 B-10	34	4
B-5>B-1 B-2	B-5	B-1 B-2	128	2
B-5>B-11 B-12	B-5	B-11 B-12	21	40
B-5>B-4	B-5	B-4	200	1
B-6>B-5>B-4	B-6	B-4	25	7
B-6>B-8	B-6	B-8	48	0
B-6>B-8>B-9 B-10	B-6	B-9 B-10	137	4
B-6>B-9 B-10	B-6	B-9 B-10	111	4
B-7>B-1 B-2	B-7	B-1 B-2	55	4
B-7>B-11 B-12	B-7	B-11 B-12	18	9
B-7>B-4	B-7	B-4	117	1
B-7>B-5	B-7	B-5	115	1
B-7>B-6	B-7	B-6	12	1
B-7>B-6>B-5	B-7	B-5	2	0
B-8>B-9 B-10	B-8	B-9 B-10	188	3
B-9 B-10>B-4	B-9 B-10	B-4	138	4

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-9 B-10>B-5	B-9 B-10	B-5	110	4
B-9 B-10>B-6	B-9 B-10	B-6	8	3
B-9 B-10>B-7	B-9 B-10	B-7	424	3
B-9 B-10>B-7>B-4	B-9 B-10	B-4	140	4
B-9 B-10>B-7>B-4>B-1 B-2	B-9 B-10	B-1 B-2	49	6
B-9 B-10>B-7>B-5	B-9 B-10	B-5	141	4
B-9 B-10>B-7>B-5>B-1 B-2	B-9 B-10	B-1 B-2	18	7
B-9 B-10>B-7>B-5>B-4>B-1 B-2	B-9 B-10	B-1 B-2	24	7
B-9 B-10>B-7>B-6	B-9 B-10	B-6	11	3
B-1 B-2>B-11 B-12	B-1 B-2	B-11 B-12	213	8
B-1 B-2>B-4	B-1 B-2	B-4	150	2.07
B-1 B-2>B-4>B-5>B-8	B-1 B-2	B-8	1	3
B-1 B-2>B-4>B-5>B-9 B-10	B-1 B-2	B-9 B-10	15	7
B-1 B-2>B-4>B-6	B-1 B-2	B-6	74	3
B-1 B-2>B-4>B-6>B-7>B-8	B-1 B-2	B-8	1	3
B-1 B-2>B-4>B-6>B-9 B-10	B-1 B-2	B-9 B-10	15	6
B-1 B-2>B-4>B-7>B-9 B-10	B-1 B-2	B-9 B-10	43	7
B-1 B-2>B-4>B-8	B-1 B-2	B-8	4	3
B-1 B-2>B-4>B-8>B-9 B-10	B-1 B-2	B-9 B-10	15	6
B-1 B-2>B-5>B-8	B-1 B-2	B-8	1	2
B-1 B-2>B-6	B-1 B-2	B-6	88	3
B-1 B-2>B-8	B-1 B-2	B-8	7	3
B-11 B-12>B-1 B-2	B-11 B-12	B-1 B-2	458	17
B-11 B-12>B-4	B-11 B-12	B-4	171	22

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-11 B-12>B-4>B-3>B-6>B-8>B-9 B-10	B-11 B-12	B-9 B-10	1	54
B-11 B-12>B-4>B-3>B-7>B-8>B-9 B-10	B-11 B-12	B-9 B-10	1	36
B-11 B-12>B-4>B-5>B-6>B-8>B-7>B-9 B-	B-11 B-12	B-9 B-10	1	30
B-11 B-12>B-4>B-5>B-8	B-11 B-12	B-8	1	10
B-11 B-12>B-4>B-5>B-8>B-7>B-9 B-10	B-11 B-12	B-9 B-10	2	31
B-11 B-12>B-4>B-5>B-8>B-9 B-10	B-11 B-12	B-9 B-10	1	35
B-11 B-12>B-4>B-6	B-11 B-12	B-6	21	23
B-11 B-12>B-4>B-6>B-7>B-9 B-10	B-11 B-12	B-9 B-10	8	18
B-11 B-12>B-4>B-6>B-8>B-9 B-10	B-11 B-12	B-9 B-10	5	30
B-11 B-12>B-4>B-7>B-9 B-10	B-11 B-12	B-9 B-10	18	28
B-11 B-12>B-4>B-8	B-11 B-12	B-8	2	20
B-11 B-12>B-5>B-7>B-9 B-10	B-11 B-12	B-9 B-10	2	60
B-11 B-12>B-6	B-11 B-12	B-6	25	24
B-11 B-12>B-6>B-7>B-9 B-10	B-11 B-12	B-9 B-10	3	61
B-11 B-12>B-8>B-9 B-10	B-11 B-12	B-9 B-10	9	31
B-3>B-1 B-2	B-3	B-1 B-2	21	2
B-4>B-1 B-2	B-4	B-1 B-2	73	2
B-4>B-11 B-12	B-4	B-11 B-12	32	110
B-4>B-5>B-6	B-4	B-6	10	1
B-4>B-5>B-7>B-9 B-10	B-4	B-9 B-10	14	4
B-4>B-5>B-8>B-9 B-10	B-4	B-9 B-10	10	4
B-4>B-5>B-9 B-10	B-4	B-9 B-10	34	4
B-4>B-6	B-4	B-6	204	1
B-4>B-6>B-7>B-9 B-10	B-4	B-9 B-10	24	4

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-4>B-6>B-9 B-10	B-4	B-9 B-10	44	5
B-4>B-8	B-4	B-8	26	1
B-4>B-8>B-7>B-9 B-10	B-4	B-9 B-10	20	5
B-4>B-8>B-9 B-10	B-4	B-9 B-10	58	5
B-5>B-1 B-2	B-5	B-1 B-2	132	2
B-5>B-11 B-12	B-5	B-11 B-12	26	13
B-5>B-4	B-5	B-4	156	1
B-6>B-5>B-4	B-6	B-4	16	4
B-6>B-8	B-6	B-8	45	0
B-6>B-8>B-9 B-10	B-6	B-9 B-10	116	4
B-7>B-1 B-2	B-7	B-1 B-2	78	2
B-7>B-11 B-12	B-7	B-11 B-12	15	11
B-7>B-4	B-7	B-4	110	1
B-7>B-5	B-7	B-5	98	1
B-7>B-6>B-5	B-7	B-5	2	0
B-8>B-9 B-10	B-8	B-9 B-10	201	4
B-9 B-10>B-4	B-9 B-10	B-4	101	4
B-9 B-10>B-5	B-9 B-10	B-5	104	3
B-9 B-10>B-6	B-9 B-10	B-6	10	40
B-9 B-10>B-7	B-9 B-10	B-7	428	3
B-9 B-10>B-7>B-4	B-9 B-10	B-4	137	3
B-9 B-10>B-7>B-4>B-1 B-2	B-9 B-10	B-1 B-2	42	5
B-9 B-10>B-7>B-5	B-9 B-10	B-5	160	3
B-9 B-10>B-7>B-5>B-1 B-2	B-9 B-10	B-1 B-2	15	5

Raw Bluetooth Counts

PM Peak Period



Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-1 B-2>B-11 B-12	B-1 B-2	B-11 B-12	481	41.17
B-1 B-2>B-4	B-1 B-2	B-4	262	1.95
B-1 B-2>B-4>B-5>B-8	B-1 B-2	B-8	1	3.92
B-1 B-2>B-4>B-6	B-1 B-2	B-6	155	3.02
B-1 B-2>B-4>B-8	B-1 B-2	B-8	3	2.50
B-1 B-2>B-4>B-8>B-7	B-1 B-2	B-7	1	4.62
B-1 B-2>B-4>B-9 B-10	B-1 B-2	B-9 B-10	181	5.40
B-1 B-2>B-5>B-7	B-1 B-2	B-7	1	2.67
B-1 B-2>B-6	B-1 B-2	B-6	184	3.03
B-11 B-12>B-1 B-2	B-11 B-12	B-1 B-2	1,032	10.28
B-11 B-12>B-4	B-11 B-12	B-4	324	10.40
B-11 B-12>B-4>B-5>B-6>B-7	B-11 B-12	B-7	1	11.93
B-11 B-12>B-4>B-5>B-8>B-7	B-11 B-12	B-7	1	12.65
B-11 B-12>B-4>B-6	B-11 B-12	B-6	51	12.65
B-11 B-12>B-4>B-9 B-10	B-11 B-12	B-9 B-10	119	13.30
B-11 B-12>B-6	B-11 B-12	B-6	80	11.98
B-3>B-1 B-2	B-3	B-1 B-2	20	5.56
B-3>B-11 B-12	B-3	B-11 B-12	20	279.25
B-4>B-1 B-2>B-4>B-3>B-1 B-2>B-4>B-9 B-10	B-4	B-9 B-10	1	597.93
B-4>B-1 B-2>B-4>B-3>B-4>B-7>B-9 B-10	B-4	B-9 B-10	1	288.75
B-4>B-1 B-2>B-4>B-6>B-5>B-9 B-10	B-4	B-9 B-10	1	337.70
B-4>B-1 B-2>B-4>B-9 B-10	B-4	B-9 B-10	1	650.38
B-4>B-1 B-2>B-9 B-10	B-4	B-9 B-10	4	520.36

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-4>B-1 B-2>B-9 B-10>B-5>B-1 B-2>B-4>B-9 B-10	B-4	B-9 B-10	1	399.23
B-4>B-1 B-2>B-9 B-10>B-7>B-4>B-8>B-9 B-10	B-4	B-9 B-10	1	566.53
B-4>B-11 B-12>B-4>B-5>B-9 B-10	B-4	B-9 B-10	1	503.40
B-4>B-11 B-12>B-4>B-9 B-10>B-7>B-11 B-12>B-4>B-9 B-10	B-4	B-9 B-10	1	632.27
B-4>B-11 B-12>B-6>B-8>B-9 B-10	B-4	B-9 B-10	1	474.85
B-4>B-11 B-12>B-6>B-9 B-10	B-4	B-9 B-10	1	348.90
B-4>B-11 B-12>B-9 B-10	B-4	B-9 B-10	2	542.75
B-4>B-3>B-3>B-11 B-12	B-4	B-11 B-12	1	46.10
B-4>B-3>B-4>B-11 B-12	B-4	B-11 B-12	1	56.62
B-4>B-5>B-6>B-7>B-5>B-4>B-1 B-2>B-7>B-5>B-9 B-10	B-4	B-9 B-10	1	411.95
B-4>B-5>B-6>B-7>B-9 B-10	B-4	B-9 B-10	4	3.51
B-4>B-5>B-6>B-9 B-10	B-4	B-9 B-10	6	3.56
B-4>B-5>B-7>B-9 B-10	B-4	B-9 B-10	10	3.68
B-4>B-5>B-8>B-7>B-9 B-10	B-4	B-9 B-10	2	3.83
B-4>B-5>B-8>B-9 B-10	B-4	B-9 B-10	3	3.23
B-4>B-5>B-9 B-10	B-4	B-9 B-10	23	3.63
B-4>B-5>B-9 B-10>B-6>B-8>B-9 B-10	B-4	B-9 B-10	1	37.78
B-4>B-5>B-9 B-10>B-7>B-5>B-4>B-5>B-9 B-10	B-4	B-9 B-10	1	659.53
B-4>B-6	B-4	B-6	340	1.01
B-4>B-6>B-4>B-9 B-10	B-4	B-9 B-10	1	387.97
B-4>B-6>B-5>B-5>B-11 B-12	B-4	B-11 B-12	1	36.35

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-4>B-6>B-5>B-6>B-9 B-10	B-4	B-9 B-10	2	448.84
B-4>B-6>B-5>B-9 B-10	B-4	B-9 B-10	1	343.53
B-4>B-6>B-6>B-7>B-9 B-10	B-4	B-9 B-10	1	1,333.98
B-4>B-6>B-7>B-9 B-10	B-4	B-9 B-10	7	3.97
B-4>B-6>B-8>B-7>B-9 B-10	B-4	B-9 B-10	2	3.74
B-4>B-6>B-8>B-9 B-10	B-4	B-9 B-10	9	3.65
B-4>B-6>B-8>B-9 B-10>B-8>B-9 B-10	B-4	B-9 B-10	1	582.47
B-4>B-6>B-9 B-10	B-4	B-9 B-10	29	3.68
B-4>B-6>B-9 B-10>B-6>B-8>B-9 B-10	B-4	B-9 B-10	1	65.37
B-4>B-7>B-6>B-9 B-10	B-4	B-9 B-10	1	116.55
B-4>B-7>B-8>B-9 B-10	B-4	B-9 B-10	1	3.72
B-4>B-7>B-9 B-10	B-4	B-9 B-10	71	3.58
B-4>B-8	B-4	B-8	31	0.93
B-4>B-8>B-4>B-7>B-9 B-10	B-4	B-9 B-10	1	493.58
B-4>B-8>B-7>B-9 B-10	B-4	B-9 B-10	10	3.58
B-4>B-8>B-7>B-9 B-10>B-7>B-5>B-8>B-9 B-10	B-4	B-9 B-10	1	592.12
B-4>B-8>B-7>B-9 B-10>B-9 B-10>B-7>B-4>B-11 B-12>B-4>B-8>B-7>B-9 B-10	B-4	B-9 B-10	1	480.28
B-4>B-8>B-9 B-10	B-4	B-9 B-10	54	3.73
B-4>B-8>B-9 B-10>B-11 B-12>B-9 B-10	B-4	B-9 B-10	1	311.43
B-4>B-8>B-9 B-10>B-4>B-1 B-2>B-4>B-9 B-10	B-4	B-9 B-10	1	259.58
B-4>B-8>B-9 B-10>B-6>B-9 B-10	B-4	B-9 B-10	1	544.37
B-4>B-9 B-10>B-1 B-2>B-9 B-10	B-4	B-9 B-10	1	297.28
B-4>B-9 B-10>B-4>B-6>B-8>B-9 B-10	B-4	B-9 B-10	1	312.12

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-4>B-9 B-10>B-4>B-6>B-9 B-10	B-4	B-9 B-10	2	336.28
B-4>B-9 B-10>B-4>B-8>B-9 B-10	B-4	B-9 B-10	1	424.00
B-4>B-9 B-10>B-4>B-9 B-10	B-4	B-9 B-10	2	328.97
B-4>B-9 B-10>B-6>B-8>B-9 B-10	B-4	B-9 B-10	2	194.44
B-4>B-9 B-10>B-6>B-9 B-10	B-4	B-9 B-10	1	343.37
B-4>B-9 B-10>B-7>B-1 B-2>B-4>B-9 B-10	B-4	B-9 B-10	1	393.68
B-4>B-9 B-10>B-7>B-4>B-1 B-2>B-9 B-10	B-4	B-9 B-10	1	452.87
B-4>B-9 B-10>B-7>B-5>B-4>B-9 B-10	B-4	B-9 B-10	1	307.10
B-4>B-9 B-10>B-7>B-5>B-9 B-10	B-4	B-9 B-10	1	224.83
B-4>B-9 B-10>B-7>B-6>B-1 B-2>B-4>B-9 B-10	B-4	B-9 B-10	1	458.08
B-4>B-9 B-10>B-7>B-9 B-10	B-4	B-9 B-10	4	406.87
B-4>B-9 B-10>B-7>B-9 B-10>B-4>B-9 B-10	B-4	B-9 B-10	1	184.13
B-4>B-9 B-10>B-9 B-10>B-7>B-4>B-9 B-10	B-4	B-9 B-10	1	128.93
B-5>B-4	B-5	B-4	196	2.21
B-5>B-4>B-1 B-2	B-5	B-1 B-2	32	7.14
B-5>B-4>B-11 B-12	B-5	B-11 B-12	13	45.73
B-6>B-4>B-11 B-12	B-6	B-11 B-12	1	47.03
B-6>B-8	B-6	B-8	49	0.45
B-6>B-9 B-10	B-6	B-9 B-10	141	3.18
B-7>B-1 B-2	B-7	B-1 B-2	134	7.78
B-7>B-11 B-12	B-7	B-11 B-12	55	295.92
B-7>B-4	B-7	B-4	190	2.68
B-7>B-4>B-11 B-12	B-7	B-11 B-12	14	43.04
B-7>B-5	B-7	B-5	165	0.93

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-7>B-6	B-7	B-6	28	110.43
B-8>B-7>B-9 B-10	B-8	B-9 B-10	28	2.89
B-8>B-9 B-10	B-8	B-9 B-10	194	2.70
B-9 B-10>B-1 B-2	B-9 B-10	B-1 B-2	265	13.82
B-9 B-10>B-4	B-9 B-10	B-4	312	8.30
B-9 B-10>B-5	B-9 B-10	B-5	245	6.15
B-9 B-10>B-7	B-9 B-10	B-7	1,208	4.39
B-9 B-10>B-7>B-11 B-12	B-9 B-10	B-11 B-12	74	72.97
B-9 B-10>B-7>B-3>B-4>B-11 B-12	B-9 B-10	B-11 B-12	4	30.98
B-9 B-10>B-7>B-4	B-9 B-10	B-4	316	7.88
B-9 B-10>B-7>B-4>B-11 B-12	B-9 B-10	B-11 B-12	28	37.94
B-9 B-10>B-7>B-5	B-9 B-10	B-5	415	6.13
B-9 B-10>B-7>B-5>B-11 B-12	B-9 B-10	B-11 B-12	20	45.29
B-9 B-10>B-7>B-5>B-4>B-11 B-12	B-9 B-10	B-11 B-12	16	57.88
B-1 B-2>B-11 B-12	B-1 B-2	B-11 B-12	598	47.72
B-1 B-2>B-4	B-1 B-2	B-4	252	1.58
B-1 B-2>B-4>B-6	B-1 B-2	B-6	133	2.70
B-1 B-2>B-4>B-6>B-8	B-1 B-2	B-8	2	2.23
B-1 B-2>B-4>B-8	B-1 B-2	B-8	4	2.54
B-1 B-2>B-4>B-8>B-7	B-1 B-2	B-7	1	2.60
B-1 B-2>B-4>B-9 B-10	B-1 B-2	B-9 B-10	148	5.01
B-1 B-2>B-5>B-8	B-1 B-2	B-8	1	2.45
B-1 B-2>B-6	B-1 B-2	B-6	224	3.63
B-1 B-2>B-8	B-1 B-2	B-8	9	2.82

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-11 B-12>B-1 B-2	B-11 B-12	B-1 B-2	1,069	8.98
B-11 B-12>B-4	B-11 B-12	B-4	360	9.03
B-11 B-12>B-4>B-6	B-11 B-12	B-6	57	12.18
B-11 B-12>B-4>B-9 B-10	B-11 B-12	B-9 B-10	118	12.50
B-11 B-12>B-6	B-11 B-12	B-6	83	11.68
B-3>B-1 B-2	B-3	B-1 B-2	23	4.25
B-3>B-11 B-12	B-3	B-11 B-12	21	29.37
B-4>B-1 B-2>B-11 B-12>B-9 B-10	B-4	B-9 B-10	1	1,104.13
B-4>B-1 B-2>B-4>B-11 B-12>B-4>B-9 B-10	B-4	B-9 B-10	1	672.37
B-4>B-1 B-2>B-4>B-7>B-9 B-10	B-4	B-9 B-10	1	384.88
B-4>B-1 B-2>B-4>B-9 B-10	B-4	B-9 B-10	5	508.03
B-4>B-1 B-2>B-6>B-4>B-6>B-7>B-9 B-10	B-4	B-9 B-10	1	405.17
B-4>B-1 B-2>B-6>B-9 B-10	B-4	B-9 B-10	2	461.12
B-4>B-1 B-2>B-7>B-9 B-10	B-4	B-9 B-10	1	432.97
B-4>B-1 B-2>B-9 B-10	B-4	B-9 B-10	6	522.22
B-4>B-1 B-2>B-9 B-10>B-5>B-9 B-10	B-4	B-9 B-10	1	348.97
B-4>B-11 B-12>B-4>B-9 B-10	B-4	B-9 B-10	1	346.78
B-4>B-11 B-12>B-7>B-9 B-10	B-4	B-9 B-10	1	167.47
B-4>B-11 B-12>B-9 B-10	B-4	B-9 B-10	4	434.63
B-4>B-5>B-4>B-9 B-10	B-4	B-9 B-10	1	371.48
B-4>B-5>B-4>B-9 B-10>B-7>B-9 B-10>B-7>B-9 B-10	B-4	B-9 B-10	1	527.73
B-4>B-5>B-6>B-7>B-8>B-9 B-10	B-4	B-9 B-10	1	3.08
B-4>B-5>B-6>B-8>B-7>B-9 B-10	B-4	B-9 B-10	1	3.80

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-4>B-5>B-6>B-8>B-9 B-10	B-4	B-9 B-10	2	3.71
B-4>B-5>B-6>B-9 B-10	B-4	B-9 B-10	4	3.28
B-4>B-5>B-7>B-9 B-10	B-4	B-9 B-10	5	3.23
B-4>B-5>B-8>B-4>B-9 B-10	B-4	B-9 B-10	1	252.40
B-4>B-5>B-8>B-7>B-9 B-10	B-4	B-9 B-10	4	3.85
B-4>B-5>B-8>B-9 B-10	B-4	B-9 B-10	3	3.48
B-4>B-5>B-9 B-10	B-4	B-9 B-10	26	3.51
B-4>B-6	B-4	B-6	340	1.98
B-4>B-6>B-1 B-2>B-9 B-10	B-4	B-9 B-10	1	416.00
B-4>B-6>B-4>B-5>B-9 B-10	B-4	B-9 B-10	1	172.20
B-4>B-6>B-5>B-4>B-7>B-4>B-7>B-9 B-10>B-7>B-6>B-9 B-10	B-4	B-9 B-10	1	446.15
B-4>B-6>B-5>B-9 B-10	B-4	B-9 B-10	1	1,177.57
B-4>B-6>B-7>B-8>B-9 B-10	B-4	B-9 B-10	1	3.67
B-4>B-6>B-7>B-8>B-9 B-10>B-11 B-12>B-4>B-5>B-8>B-7>B-9 B-10	B-4	B-9 B-10	1	144.85
B-4>B-6>B-7>B-9 B-10	B-4	B-9 B-10	6	9.10
B-4>B-6>B-7>B-9 B-10>B-11 B-12>B-4>B-9 B-10>B-5>B-9 B-10>B-7>B-11 B-12>B-9 B-10	B-4	B-9 B-10	1	593.63
B-4>B-6>B-8>B-7>B-9 B-10	B-4	B-9 B-10	7	3.47
B-4>B-6>B-8>B-9 B-10	B-4	B-9 B-10	13	23.87
B-4>B-6>B-9 B-10	B-4	B-9 B-10	34	3.82
B-4>B-6>B-9 B-10>B-11 B-12>B-5>B-6>B-9 B-10	B-4	B-9 B-10	1	336.42

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-4>B-6>B-9 B-10>B-5>B-9 B-10	B-4	B-9 B-10	1	106.52
B-4>B-6>B-9 B-10>B-6>B-9 B-10	B-4	B-9 B-10	1	165.22
B-4>B-6>B-9 B-10>B-7>B-6>B-8>B-9 B-10	B-4	B-9 B-10	1	392.72
B-4>B-7>B-6>B-9 B-10	B-4	B-9 B-10	1	334.37
B-4>B-7>B-8>B-9 B-10	B-4	B-9 B-10	1	3.47
B-4>B-7>B-9 B-10	B-4	B-9 B-10	68	3.38
B-4>B-7>B-9 B-10>B-4>B-5>B-9 B-10	B-4	B-9 B-10	1	651.83
B-4>B-7>B-9 B-10>B-4>B-9 B-10	B-4	B-9 B-10	2	949.65
B-4>B-7>B-9 B-10>B-7>B-5>B-5>B-4>B-9 B-10	B-4	B-9 B-10	1	437.18
B-4>B-7>B-9 B-10>B-7>B-5>B-9 B-10	B-4	B-9 B-10	1	194.73
B-4>B-7>B-9 B-10>B-7>B-6>B-8>B-9 B-10	B-4	B-9 B-10	1	448.88
B-4>B-8	B-4	B-8	31	1.07
B-4>B-8>B-7>B-9 B-10	B-4	B-9 B-10	7	3.73
B-4>B-8>B-9 B-10	B-4	B-9 B-10	42	3.52
B-4>B-8>B-9 B-10>B-8>B-9 B-10	B-4	B-9 B-10	1	580.77
B-4>B-9 B-10>B-1 B-2>B-6>B-8>B-9 B-10	B-4	B-9 B-10	1	168.52
B-4>B-9 B-10>B-1 B-2>B-9 B-10	B-4	B-9 B-10	2	678.46
B-4>B-9 B-10>B-11 B-12>B-1 B-2>B-9 B-10	B-4	B-9 B-10	1	547.12
B-4>B-9 B-10>B-11 B-12>B-9 B-10	B-4	B-9 B-10	4	492.28
B-4>B-9 B-10>B-4>B-5>B-9 B-10	B-4	B-9 B-10	1	58.30
B-4>B-9 B-10>B-4>B-9 B-10	B-4	B-9 B-10	3	442.87
B-4>B-9 B-10>B-6>B-8>B-9 B-10	B-4	B-9 B-10	1	398.92
B-4>B-9 B-10>B-6>B-9 B-10	B-4	B-9 B-10	1	539.00
B-4>B-9 B-10>B-7>B-1 B-2>B-4>B-9 B-10	B-4	B-9 B-10	1	369.72

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-4>B-9 B-10>B-7>B-1 B-2>B-9 B-10>B-5>B-6>B-9 B-10	B-4	B-9 B-10	1	1,255.63
B-4>B-9 B-10>B-7>B-11 B-12>B-4>B-9 B-10	B-4	B-9 B-10	1	503.38
B-4>B-9 B-10>B-7>B-4>B-1 B-2>B-4>B-9 B-10	B-4	B-9 B-10	1	168.80
B-4>B-9 B-10>B-7>B-4>B-7>B-9 B-10	B-4	B-9 B-10	1	517.63
B-4>B-9 B-10>B-7>B-4>B-9 B-10	B-4	B-9 B-10	3	254.88
B-4>B-9 B-10>B-7>B-4>B-9 B-10>B-7>B-1 B-2>B-7>B-9 B-10>B-7>B-4>B-9 B-10	B-4	B-9 B-10	1	2,039.72
B-4>B-9 B-10>B-7>B-5>B-1 B-2>B-4>B-8>B-9 B-10	B-4	B-9 B-10	1	496.43
B-4>B-9 B-10>B-7>B-5>B-6>B-9 B-10	B-4	B-9 B-10	2	395.14
B-4>B-9 B-10>B-7>B-6>B-1 B-2>B-4>B-9 B-10	B-4	B-9 B-10	1	324.78
B-4>B-9 B-10>B-7>B-8>B-9 B-10	B-4	B-9 B-10	1	369.95
B-4>B-9 B-10>B-7>B-9 B-10	B-4	B-9 B-10	1	378.38
B-4>B-9 B-10>B-8>B-9 B-10	B-4	B-9 B-10	2	264.03
B-5>B-3	B-5	B-3	1	0.93
B-5>B-4	B-5	B-4	164	0.98
B-5>B-4>B-1 B-2	B-5	B-1 B-2	25	4.78
B-5>B-4>B-11 B-12	B-5	B-11 B-12	16	41.26
B-6>B-8	B-6	B-8	56	0.45
B-6>B-9 B-10	B-6	B-9 B-10	148	3.05
B-7>B-1 B-2	B-7	B-1 B-2	146	5.01
B-7>B-11 B-12	B-7	B-11 B-12	58	232.72
B-7>B-4	B-7	B-4	177	1.05

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-7>B-4>B-11 B-12	B-7	B-11 B-12	20	103.35
B-7>B-5	B-7	B-5	194	0.75
B-7>B-5>B-11 B-12	B-7	B-11 B-12	14	245.76
B-7>B-6	B-7	B-6	24	182.40
B-8>B-7>B-9 B-10	B-8	B-9 B-10	17	2.72
B-8>B-9 B-10	B-8	B-9 B-10	199	2.65
B-9 B-10>B-1 B-2	B-9 B-10	B-1 B-2	313	8.75
B-9 B-10>B-4	B-9 B-10	B-4	228	4.43
B-9 B-10>B-4>B-11 B-12	B-9 B-10	B-11 B-12	16	32.88
B-9 B-10>B-5	B-9 B-10	B-5	219	4.08
B-9 B-10>B-5>B-11 B-12	B-9 B-10	B-11 B-12	17	286.50
B-9 B-10>B-5>B-4>B-11 B-12	B-9 B-10	B-11 B-12	6	27.91
B-9 B-10>B-7	B-9 B-10	B-7	1,204	3.22
B-9 B-10>B-7>B-11 B-12	B-9 B-10	B-11 B-12	70	49.28
B-9 B-10>B-7>B-4	B-9 B-10	B-4	254	4.25
B-9 B-10>B-7>B-4>B-11 B-12	B-9 B-10	B-11 B-12	33	25.22
B-9 B-10>B-7>B-5	B-9 B-10	B-5	315	3.87
B-9 B-10>B-7>B-5>B-11 B-12	B-9 B-10	B-11 B-12	23	33.93
B-9 B-10>B-7>B-5>B-4>B-11 B-12	B-9 B-10	B-11 B-12	12	34.98
B-9 B-10>B-7>B-6>B-11 B-12	B-9 B-10	B-11 B-12	4	33.76
B-1 B-2>B-4	B-1 B-2	B-4	262	1.67
B-1 B-2>B-4>B-8	B-1 B-2	B-8	6	2.23
B-1 B-2>B-4>B-9 B-10	B-1 B-2	B-9 B-10	165	5.00
B-11 B-12>B-1 B-2	B-11 B-12	B-1 B-2	1,112	10.04

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-11 B-12>B-1 B-2>B-8	B-11 B-12	B-8	1	11.27
B-11 B-12>B-4	B-11 B-12	B-4	425	10.62
B-11 B-12>B-4>B-5>B-8	B-11 B-12	B-8	1	10.40
B-11 B-12>B-4>B-6	B-11 B-12	B-6	58	13.59
B-11 B-12>B-4>B-6>B-7	B-11 B-12	B-7	1	12.20
B-11 B-12>B-4>B-8	B-11 B-12	B-8	5	13.02
B-11 B-12>B-4>B-9 B-10	B-11 B-12	B-9 B-10	149	14.27
B-11 B-12>B-6	B-11 B-12	B-6	80	14.28
B-3>B-1 B-2	B-3	B-1 B-2	17	2.93
B-3>B-11 B-12	B-3	B-11 B-12	23	100.80
B-4>B-1 B-2>B-4>B-5>B-9 B-10	B-4	B-9 B-10	1	587.28
B-4>B-1 B-2>B-4>B-6>B-9 B-10	B-4	B-9 B-10	1	78.62
B-4>B-1 B-2>B-4>B-7>B-9 B-10>B-6>B-1 B-2>B-4>B-7>B-9 B-10	B-4	B-9 B-10	1	370.03
B-4>B-1 B-2>B-4>B-9 B-10	B-4	B-9 B-10	3	445.03
B-4>B-1 B-2>B-5>B-9 B-10	B-4	B-9 B-10	1	255.50
B-4>B-1 B-2>B-6>B-8>B-9 B-10	B-4	B-9 B-10	1	542.98
B-4>B-1 B-2>B-7>B-9 B-10	B-4	B-9 B-10	1	272.42
B-4>B-1 B-2>B-9 B-10	B-4	B-9 B-10	9	334.77
B-4>B-1 B-2>B-9 B-10>B-7>B-4>B-7>B-4>B-6>B-4>B-7>B-4>B-9 B-10	B-4	B-9 B-10	1	2,416.15
B-4>B-11 B-12>B-1 B-2>B-4>B-9 B-10	B-4	B-9 B-10	1	1,003.40
B-4>B-11 B-12>B-4>B-5>B-6>B-8>B-7>B-9 B-10	B-4	B-9 B-10	1	30.90
B-4>B-11 B-12>B-4>B-6>B-9 B-10	B-4	B-9 B-10	1	543.23

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-4>B-11 B-12>B-4>B-9 B-10	B-4	B-9 B-10	3	487.22
B-4>B-11 B-12>B-8>B-9 B-10	B-4	B-9 B-10	2	368.23
B-4>B-11 B-12>B-9 B-10	B-4	B-9 B-10	3	584.03
B-4>B-3>B-8>B-9 B-10	B-4	B-9 B-10	1	32.18
B-4>B-5>B-1 B-2>B-4>B-6>B-9 B-10	B-4	B-9 B-10	1	427.35
B-4>B-5>B-4>B-9 B-10	B-4	B-9 B-10	1	602.53
B-4>B-5>B-6>B-6>B-8>B-9 B-10	B-4	B-9 B-10	1	62.43
B-4>B-5>B-6>B-7>B-9 B-10	B-4	B-9 B-10	1	4.12
B-4>B-5>B-6>B-8>B-7>B-9 B-10	B-4	B-9 B-10	1	4.20
B-4>B-5>B-6>B-8>B-7>B-9 B-10>B-7>B-5>B-1 B-2>B-4>B-5>B-7>B-9 B-10	B-4	B-9 B-10	1	507.52
B-4>B-5>B-6>B-8>B-9 B-10	B-4	B-9 B-10	2	3.60
B-4>B-5>B-6>B-9 B-10	B-4	B-9 B-10	8	3.57
B-4>B-5>B-7>B-9 B-10	B-4	B-9 B-10	5	46.98
B-4>B-5>B-7>B-9 B-10>B-6>B-8>B-7>B-9 B-10	B-4	B-9 B-10	1	110.23
B-4>B-5>B-8>B-7>B-9 B-10	B-4	B-9 B-10	2	3.54
B-4>B-5>B-8>B-9 B-10	B-4	B-9 B-10	4	3.39
B-4>B-5>B-9 B-10	B-4	B-9 B-10	20	3.36
B-4>B-6	B-4	B-6	329	2.08
B-4>B-6>B-1 B-2>B-9 B-10>B-7>B-11 B-12>B-9 B-10	B-4	B-9 B-10	1	200.07
B-4>B-6>B-4>B-7>B-9 B-10	B-4	B-9 B-10	1	721.62
B-4>B-6>B-4>B-9 B-10	B-4	B-9 B-10	2	423.86
B-4>B-6>B-5>B-9 B-10	B-4	B-9 B-10	2	631.48

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-4>B-6>B-6>B-8>B-9 B-10	B-4	B-9 B-10	1	48.10
B-4>B-6>B-7>B-6>B-9 B-10	B-4	B-9 B-10	1	33.95
B-4>B-6>B-7>B-9 B-10	B-4	B-9 B-10	9	3.57
B-4>B-6>B-8	B-4	B-8	8	21.28
B-4>B-6>B-8>B-7>B-9 B-10	B-4	B-9 B-10	8	11.97
B-4>B-6>B-8>B-9 B-10	B-4	B-9 B-10	11	4.28
B-4>B-6>B-9 B-10	B-4	B-9 B-10	41	3.77
B-4>B-7>B-11 B-12>B-9 B-10	B-4	B-9 B-10	1	829.68
B-4>B-7>B-4>B-6>B-9 B-10	B-4	B-9 B-10	1	545.07
B-4>B-7>B-4>B-9 B-10	B-4	B-9 B-10	1	433.13
B-4>B-7>B-5>B-6>B-9 B-10	B-4	B-9 B-10	1	116.68
B-4>B-7>B-9 B-10	B-4	B-9 B-10	58	3.56
B-4>B-7>B-9 B-10>B-7>B-4>B-6>B-8>B-9 B-10	B-4	B-9 B-10	1	510.22
B-4>B-8	B-4	B-8	38	0.98
B-4>B-8>B-11 B-12>B-9 B-10	B-4	B-9 B-10	1	132.97
B-4>B-8>B-7>B-9 B-10	B-4	B-9 B-10	4	3.72
B-4>B-8>B-7>B-9 B-10>B-1 B-2>B-5>B-8>B-9 B-10	B-4	B-9 B-10	1	360.88
B-4>B-8>B-7>B-9 B-10>B-6>B-8>B-9 B-10	B-4	B-9 B-10	1	239.42
B-4>B-8>B-9 B-10	B-4	B-9 B-10	45	3.75
B-4>B-8>B-9 B-10>B-7>B-11 B-12>B-6>B-9 B-10	B-4	B-9 B-10	1	522.53
B-4>B-8>B-9 B-10>B-7>B-9 B-10	B-4	B-9 B-10	1	469.98
B-4>B-9 B-10>B-1 B-2>B-9 B-10	B-4	B-9 B-10	1	907.70

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-4>B-9 B-10>B-1 B-2>B-9 B-10>B-11 B-12>B-9 B-10	B-4	B-9 B-10	1	714.65
B-4>B-9 B-10>B-11 B-12>B-1 B-2>B-4>B-6>B-8>B-9 B-10	B-4	B-9 B-10	1	652.57
B-4>B-9 B-10>B-11 B-12>B-4>B-8>B-9 B-10	B-4	B-9 B-10	1	404.98
B-4>B-9 B-10>B-4>B-8>B-9 B-10	B-4	B-9 B-10	2	397.10
B-4>B-9 B-10>B-4>B-9 B-10	B-4	B-9 B-10	6	215.23
B-4>B-9 B-10>B-5>B-8>B-9 B-10	B-4	B-9 B-10	1	612.85
B-4>B-9 B-10>B-5>B-9 B-10	B-4	B-9 B-10	1	219.90
B-4>B-9 B-10>B-6>B-6>B-4>B-9 B-10	B-4	B-9 B-10	1	393.32
B-4>B-9 B-10>B-6>B-9 B-10	B-4	B-9 B-10	3	317.97
B-4>B-9 B-10>B-7>B-1 B-2>B-9 B-10	B-4	B-9 B-10	2	622.96
B-4>B-9 B-10>B-7>B-5>B-8>B-9 B-10	B-4	B-9 B-10	1	500.65
B-4>B-9 B-10>B-7>B-5>B-9 B-10	B-4	B-9 B-10	1	205.63
B-4>B-9 B-10>B-7>B-9 B-10	B-4	B-9 B-10	4	340.33
B-4>B-9 B-10>B-8>B-9 B-10	B-4	B-9 B-10	1	405.35
B-4>B-9 B-10>B-9 B-10>B-7>B-11 B-12>B-5>B-8>B-9 B-10	B-4	B-9 B-10	1	559.45
B-5>B-4	B-5	B-4	170	3.13
B-5>B-4>B-1 B-2	B-5	B-1 B-2	34	5.73
B-5>B-4>B-11 B-12	B-5	B-11 B-12	15	42.35
B-6>B-8	B-6	B-8	52	0.44
B-6>B-9 B-10	B-6	B-9 B-10	142	3.15
B-7>B-1 B-2	B-7	B-1 B-2	170	5.73

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-7>B-11 B-12	B-7	B-11 B-12	68	138.62
B-7>B-4	B-7	B-4	195	2.47
B-7>B-5	B-7	B-5	171	0.80
B-7>B-6	B-7	B-6	32	57.97
B-8>B-7>B-9 B-10	B-8	B-9 B-10	25	2.75
B-8>B-9 B-10	B-8	B-9 B-10	185	2.68
B-9 B-10>B-1 B-2	B-9 B-10	B-1 B-2	321	11.70
B-9 B-10>B-4	B-9 B-10	B-4	337	7.32
B-9 B-10>B-5	B-9 B-10	B-5	286	5.88
B-9 B-10>B-5>B-11 B-12	B-9 B-10	B-11 B-12	17	126.75
B-9 B-10>B-7	B-9 B-10	B-7	1,315	4.87
B-9 B-10>B-7>B-11 B-12	B-9 B-10	B-11 B-12	73	82.90
B-9 B-10>B-7>B-3>B-11 B-12	B-9 B-10	B-11 B-12	4	34.04
B-9 B-10>B-7>B-3>B-4>B-11 B-12	B-9 B-10	B-11 B-12	4	32.05
B-9 B-10>B-7>B-4	B-9 B-10	B-4	320	6.56
B-9 B-10>B-7>B-4>B-11 B-12	B-9 B-10	B-11 B-12	26	39.56
B-9 B-10>B-7>B-5	B-9 B-10	B-5	349	5.45
B-9 B-10>B-7>B-5>B-11 B-12	B-9 B-10	B-11 B-12	19	160.63
B-9 B-10>B-7>B-5>B-3>B-4>B-11 B-12	B-9 B-10	B-11 B-12	5	50.17
B-9 B-10>B-7>B-5>B-4>B-11 B-12	B-9 B-10	B-11 B-12	19	49.78
B-9 B-10>B-7>B-6>B-11 B-12	B-9 B-10	B-11 B-12	4	60.28

Appendix C

Bluetooth Matrix Adjustment Procedure

AM Peak Period

Table A1 - Raw OD from appendix B													
Origin	Destination												
		B-1	B-2	B-3	B-4	B-5	B-6	B-7	B-8	B-9	B-10	B-11	B-12
WB I-195 Mainline @ NW 12th Ave	B-1		n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 Mainline @ NW 12th Ave	B-2	n/a		n/a	441	n/a	324	n/a	n/a	n/a	273	665	n/a
WB I-195 On-Ramp from N Miami Ave	B-3	75	n/a		n/a	n/a	n/a	n/a	n/a	n/a	n/a	12	n/a
EB I-195 Off-Ramp to N Miami Ave	B-4	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 On-Ramp from US-1	B-5	386	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	76	n/a
EB I-195 Off-Ramp to US-1	B-6	n/a	n/a	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 Off-Ramp to US-1	B-7	n/a	n/a	n/a	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a
EB I-195 On-Ramp From US-1	B-8	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a	598	n/a	n/a
WB I-195 Mainline @ West of On-Ramp from Alton Road	B-9	209	n/a	n/a	n/a	n/a	n/a	1,231	n/a		n/a	38	n/a
EB I-195 Mainline @ West of Off-Ramp to Alton Road	B-10	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a	n/a
NB I-95 Mainline near GGI	B-11	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a
SB I-95 Mainline near GGI	B-12	1,513	n/a	n/a	512	n/a	162	n/a	n/a	n/a	69	n/a	

Table A1 Explanation: Numbers in Table A1 are based on raw Bluetooth counts (Appendix B – AM Peak). This should be considered that Appendix B shows different records for each pair based on average travel time. Total amount of the counts for each pair is the actual match paired. As an example, filtering the raw data for AM peak period for Start Location “B-9_B-10” and End Location “B-1_B-2” will result in the records shown next page. Total count is 209 which shows number of match paired devices between stations B-9_B-10 to B-1_B-2. Based on the direction and locations, this number should be inserted in Cell “B-9 / B-1”. Also, n/a shows there is not possibility of any movements between the pairs.

Sequence	Start Location	End Location	Count
B-9 B-10>B-7>B-4>B-1 B-2	B-9 B-10	B-1 B-2	44
B-9 B-10>B-7>B-5>B-4>B-1 B-2	B-9 B-10	B-1 B-2	17
B-9 B-10>B-7>B-4>B-1 B-2	B-9 B-10	B-1 B-2	49
B-9 B-10>B-7>B-5>B-1 B-2	B-9 B-10	B-1 B-2	18
B-9 B-10>B-7>B-5>B-4>B-1 B-2	B-9 B-10	B-1 B-2	24
B-9 B-10>B-7>B-4>B-1 B-2	B-9 B-10	B-1 B-2	42
B-9 B-10>B-7>B-5>B-1 B-2	B-9 B-10	B-1 B-2	15

The reason that station B-1_B-2 is shown with double alphabetical IDs (B-1_B-2) is that the distance between locations B-1 and B-2 are less than 300 feet; consequently one Bluetooth device is able to capture both locations and directions. As a result, one device is implemented, but for simplicity and also presentation of O-D matrices, each direction is shown with separate ID. By that, B-1 in the O-D matrix is showing northbound and B-2 is showing southbound. Similar explanation is valid for B-9_B-10 and also B-11_B-12. For this example, vehicles are going from location B-9_B-10 to B-1_B-2 which is northbound, so the captures should be located in the matrix to represent movement from B-9 to B-1.

Table A2 - Bluetooth Capture Rate									
Station		Day_1_Bluetooth_Count	Day_2_Bluetooth_Count	Day_3_Bluetooth_Count	Average_Bluetooth_Count_3_Days	Machine Count Source	Machine Count	Bluetooth Capture Rate	Directional Distribution
WB I-195 Mainline @ NW 12th Ave	B-1	8,891	9,059	9,195	9,048	FTI station, 872023	31,162	0.29	0.46
EB I-195 Mainline @ NW 12th Ave	B-2	8,891	9,059	9,195	9,048	FTI station, 872023	31,162	0.29	0.54
WB I-195 On-Ramp from N Miami Ave	B-3	482	480	480	481	v-30 Ramp	4,215	0.11	
EB I-195 Off-Ramp to N Miami Ave	B-4	7,970	7,963	7,902	7,945	Class 1, EB Only	19,143	0.42	
WB I-195 On-Ramp from US-1	B-5	3,831	4,210	4,089	4,043	v-15 Ramp + C2 WB	11,205	0.36	
EB I-195 Off-Ramp to US-1	B-6	3,053	2,979	3,089	3,040	Class2 (EB)+v-14	15,030	0.20	
WB I-195 Off-Ramp to US-1	B-7	4,185	4,654	4,819	4,553	Class2 (WB)+v-13	10,762	0.42	
EB I-195 On-Ramp From US-1	B-8	1,361	1,324	1,522	1,402	v12	2,986	0.47	
WB I-195 Mainline @ West of On-Ramp from Alton Road	B-9	8,992	9,032	9,330	9,118	CLass4	23,318	0.39	0.49
EB I-195 Mainline @ West of Off-Ramp to Alton Road	B-10	8,992	9,032	9,330	9,118	CLass4	23,318	0.39	0.51
NB I-95 Mainline near GGI	B-11	13,434	13,407	12,522	13,121	FTI station 872134	43,605	0.30	0.46
SB I-95 Mainline near GGI	B-12	13,434	13,407	12,522	13,121	FTI station 872134	43,605	0.30	0.54

Table A2 Explanation: Three first columns show how many Bluetooth devices were captured at each station and for each day of data collection for the AM peak period (6:00 AM to 10:00 AM). This is an output of data collection. As an example, for device B-3 and for second day of data collection, there are a total of 480 captured devices as can be seen in the figure (right). Three days counts were averaged in the fourth column of Table A2.

5	10/25/2017	6:00	12
6	10/25/2017	6:15	21
7	10/25/2017	6:30	23
8	10/25/2017	6:45	26
9	10/25/2017	7:00	28
0	10/25/2017	7:15	27
1	10/25/2017	7:30	23
2	10/25/2017	7:45	41
3	10/25/2017	8:00	41
4	10/25/2017	8:15	34
5	10/25/2017	8:30	33
6	10/25/2017	8:45	35
7	10/25/2017	9:00	37
8	10/25/2017	9:15	41
9	10/25/2017	9:30	27
0	10/25/2017	9:45	31
		Sum	480

The goal is to find what percent of actual volume is captured at the station. So the number should be compared with traffic counts at same location and hours. For this example, Station 30 of volume data collection represents the similar traffic that is captured by device B-3. Raw data for each station is provided in Appendix A of data collection report. A summary of AM peak period (6:00 AM to 9:00 AM) for station v-30 can be viewed in the figure (right) and the average traffic for three days of data collection.

Time	Day 1	Day 2	Day 3	
6:00:00 AM	658	662	730	
7:00:00 AM	1,299	1,333	1,272	
8:00:00 AM	1,349	1,231	1,289	
9:00:00 AM	924	1,017	882	
Total (4 hours)	4,230	4,243	4,173	
				Average 4,215

Next column is showing Bluetooth capture rate which is computed by dividing average Bluetooth captures to actual volume of three days, as far as B-3 device example for the AM peak:

$$\frac{480}{4215} = 0.11$$

As explained before, there is one device implemented for three locations of below:

- ✓ B-1 – B-2
- ✓ B-9 – B10
- ✓ B-11 – B-12

Time	Northbound	Southbound	Both Directions
6:00:00 AM	4,255	6,165	10,420
7:00:00 AM	5,490	6,741	12,231
8:00:00 AM	4,961	6,358	11,319
9:00:00 AM	5,138	4,497	9,635
Total (4 hours)	19,844	23,761	43,605
Directional Distribution	46%	54%	

Consequently, the Bluetooth captures of them are the same. Capture rate and directional distribution is derived from FTI stations at those locations. As an example and for station B-11_B-12, FTI Station 872134 is used to estimate capture rate and directional distribution as figure (left):



Table A3 – Assumptions in order to provide the O/Ds based on on-ramps and off-ramps. This table estimates what paired match counts or captures are related to the ramps and what percentage is related to the mainline.					
Origin	Destination	Mainline Source/Volume	Ramp Source/Volume		
B02	B04	Class_1_EB 19,143	V-31 (Ramp) 3,735	Ramp Ratio 0.195	Exiting Matched Counts 86
B02	B06	Class_2_EB 12,157	V-14 (Ramp) 2,873	Ramp Ratio 0.19	Exiting Matched Counts 62
	B05	Class_2_WB 8,249	V-15 (Ramp) 2,956	Ramp Ratio 0.26	Ramp Captures 1,067
B05	B01	Class_2_WB 8,249	V-15 (Ramp) 2,956	Ramp Ratio 0.264	Exiting Matched Counts 102
B05	B11	Class_2_WB 8,249	V-15 (Ramp) 2,956	Ramp Ratio 0.26	Exiting Matched Counts 20
B09	B07	Class_2_WB 8,249	V-13 (Ramp) 2,513	Ramp Ratio 0.23	Exiting Matched Counts 287
B12	B04	Class_1_EB 19,143	V-31 (Ramp) 3,735	Ramp Ratio 0.20	Exiting Matched Counts 100
B12	B06	Class_2_EB 12,157	V-14 (Ramp) 2,873	Ramp Ratio 0.19	Exiting Matched Counts 31

Table A3 Explanation: Target of study is to explore percentage of traffic exit any of the ramps (i.e. B-3 to B-7). However, when Bluetooth device is installed at a ramp, it captures a radius of about 300 feet which covers mainline too. As a result, match paired of Table A1 should be adjusted based on volume ratio of ramp and mainline. For example, in the AM peak period there are 441 match paired between B-2 to B-4. Volume counts at ramp B-4 and mainline shows ratio of traffic exiting ramp B-4 to mainline traffic at the ramp location is 0.195 (=3,735 / 19,143).

Table A4 - Bluetooth Sample Trips														
Origin	Total Trips at Origin	Destination												
		B-1	B-2	B-3	B-4	B-5	B-6	B-7	B-8	B-9	B-10	B-11	B-12	
WB I-195 Mainline @ NW 12th Ave	B-1	4,841	0	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 Mainline @ NW 12th Ave	B-2	481	n/a	0	n/a	86	n/a	62	n/a	n/a	n/a	273	665	n/a
WB I-195 On-Ramp from N Miami Ave	B-3	1,067	75	n/a	0	n/a	n/a	n/a	n/a	n/a	n/a	n/a	12	n/a
EB I-195 Off-Ramp to N Miami Ave	B-4	1,402	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 On-Ramp from US-1	B-5	4,443	102	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a	20	n/a
EB I-195 Off-Ramp to US-1	B-6	7,150	n/a	n/a	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 Off-Ramp to US-1	B-7	1,402	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a
EB I-195 On-Ramp From US-1	B-8	4,443	n/a	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a	598	n/a	n/a
WB I-195 Mainline @ West of On-Ramp from Alton Road	B-9	1,402	209	n/a	n/a	n/a	n/a	n/a	287	n/a	0	n/a	38	n/a
EB I-195 Mainline @ West of Off-Ramp to Alton Road	B-10	7,150	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a	n/a
NB I-95 Mainline near GGI	B-11	4,841	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a
SB I-95 Mainline near GGI	B-12	481	1513	n/a	n/a	512	n/a	162	n/a	n/a	n/a	69	n/a	0

Table A4 Explanation: As mentioned, the adjustments shown in Table A3 are applied to raw data on Table A1 to form this table. As An example a factor of 0.195 is applied to B-2 to B-4 (= 441×0.195=86). The movements which are mentioned in Table A3 needed the adjustment based on data analysis. The total trips at origin are actually the average Bluetooth counts (Table A2) multiplied by directional distribution of the station (if applicable) multiplied by adjustment factor of Table A3 if applicable. For instance, for B-5:

$$4,043 \text{ (Average Bluetooth captures at station B-5)} \times 0.266 \text{ (Ramp capture from Table A3)} = 1,067$$

Or for B-2:

$$9,048 \text{ (Average Bluetooth captures at station B-5)} \times 0.54 \text{ (Directional distribution from Table A2)} = 4,841$$

Table A5 - O/Ds													
		Destination											
Origin		B-1	B-2	B-3	B-4	B-5	B-6	B-7	B-8	B-9	B-10	B-11	B-12
WB I-195 Mainline @ NW 12th Ave	B-1	0	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 Mainline @ NW 12th Ave	B-2	n/a	0	n/a	1.78%	n/a	1.28%	n/a	n/a	n/a	5.64%	13.74%	n/a
WB I-195 On-Ramp from N Miami Ave	B-3	15.60%	n/a	0	n/a	n/a	n/a	n/a	n/a	n/a	n/a	2.50%	n/a
EB I-195 Off-Ramp to N Miami Ave	B-4	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 On-Ramp from US-1	B-5	9.55%	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a	1.88%	n/a
EB I-195 Off-Ramp to US-1	B-6	n/a	n/a	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 Off-Ramp to US-1	B-7	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a
EB I-195 On-Ramp From US-1	B-8	n/a	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a	100.0%	n/a	n/a
WB I-195 Mainline @ West of On-Ramp from Alton Road	B-9	4.70%	n/a	n/a	n/a	n/a	n/a	6.47%	n/a	0	n/a	0.86%	n/a
EB I-195 Mainline @ West of Off-Ramp to Alton Road	B-10	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a	n/a
NB I-95 Mainline near GGI	B-11	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a
SB I-95 Mainline near GGI	B-12	21.16%	n/a	n/a	7.16%	n/a	2.27%	n/a	n/a	n/a	0.97%	n/a	0

Table A5 Explanation: the O-D matrix is estimated by dividing the cell values in Table A4 to total volume at origin. As can be seen, summation of each row is less than 100%. The reason is that study area is not closed and there are trips which are not captured (i.e. trips to I-95 southbound).

Table A6 - Travel Time													
Origin		Destination											
		B-1	B-2	B-3	B-4	B-5	B-6	B-7	B-8	B-9	B-10	B-11	B-12
WB I-195 Mainline @ NW 12th Ave	B-1		n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 Mainline @ NW 12th Ave	B-2	n/a		n/a	158	n/a	188	n/a	n/a	n/a	400	526	n/a
WB I-195 On-Ramp from N Miami Ave	B-3	122	n/a		n/a	n/a	n/a	n/a	n/a	n/a	n/a	614	n/a
EB I-195 Off-Ramp to N Miami Ave	B-4	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 On-Ramp from US-1	B-5	134	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	624	n/a
EB I-195 Off-Ramp to US-1	B-6	n/a	n/a	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 Off-Ramp to US-1	B-7	n/a	n/a	n/a	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a
EB I-195 On-Ramp From US-1	B-8	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a	203	n/a	n/a
WB I-195 Mainline @ West of On-Ramp from Alton Road	B-9	325	n/a	n/a	n/a	n/a	n/a	150	n/a		n/a	724	n/a
EB I-195 Mainline @ West of Off-Ramp to Alton Road	B-10	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a	n/a
NB I-95 Mainline near GGI	B-11	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a
SB I-95 Mainline near GGI	B-12	949	n/a	n/a	1,192	n/a	1,350	n/a	n/a	n/a	1,855	n/a	

Table A6 Explanation: This table uses the raw Bluetooth data travel times (Appendix B) to estimate average travel time. Raw data provides median travel time with counts and the weight average is calculated as below (result is multiplied by 60 to convert minutes to seconds). Example is provided for movement B-2 to B-4:

$$TT = \frac{\sum Count \times TT}{\sum Count} = \frac{152 \times 2.88 + 139 \times 2.98 + 150 \times 2.07}{152 + 139 + 150} \times 60 = 158$$

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-1 B-2>B-4	B-1 B-2	B-4	152	2.88
B-1 B-2>B-4	B-1 B-2	B-4	139	2.98
B-1 B-2>B-4	B-1 B-2	B-4	150	2.07

Bluetooth Matrix Adjustment Procedure

PM Peak Period



Origin	Destination												
		B-1	B-2	B-3	B-4	B-5	B-6	B-7	B-8	B-9	B-10	B-11	B-12
WB I-195 Mainline @ NW 12th Ave	B-1		n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 Mainline @ NW 12th Ave	B-2	n/a		n/a	776	n/a	696	n/a	n/a	n/a	494	1079	n/a
WB I-195 On-Ramp from N Miami Ave	B-3	60	n/a		n/a	n/a	n/a	n/a	n/a	n/a	n/a	64	n/a
EB I-195 Off-Ramp to N Miami Ave	B-4	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 On-Ramp from US-1	B-5	91	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	44	n/a
EB I-195 Off-Ramp to US-1	B-6	n/a	n/a	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 Off-Ramp to US-1	B-7	n/a	n/a	n/a	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a
EB I-195 On-Ramp From US-1	B-8	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a	648	n/a	n/a
WB I-195 Mainline @ West of On-Ramp from Alton Road	B-9	899	n/a	n/a	n/a	n/a	n/a	3727	n/a		n/a	494	n/a
EB I-195 Mainline @ West of Off-Ramp to Alton Road	B-10	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a	n/a
NB I-95 Mainline near GGI	B-11	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a
SB I-95 Mainline near GGI	B-12	3213	n/a	n/a	1109	n/a	409	n/a	n/a	n/a	386	n/a	

Table B1 Explanation: Numbers in Table B1 are based on raw Bluetooth counts (Appendix B - PM Peak). This should be considered that Appendix B shows different records for each pair based on average travel time. Total amount of the counts for each pair is the actual match paired. As an example, filtering the raw data for PM peak period for Start Location “B-9_B-10” and End Location “B-1_B-2” will result in the records shown next page. Total count is 899 which shows number of match paired devices between stations B-9_B-10 to B-1_B-2. Based on the direction and locations, this number should be inserted in Cell “B-9 / B-1”. Also, n/a shows there is not possibility of any movements between the pairs.

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-9 B-10>B-1 B-2	B-9 B-10	B-1 B-2	265	13.82
B-9 B-10>B-1 B-2	B-9 B-10	B-1 B-2	313	8.75
B-9 B-10>B-1 B-2	B-9 B-10	B-1 B-2	321	11.70

It is explained in the AM data processing sections that why some stations are presented with double alphabetical IDs (i.e. B-1_B-2).

Table B2 - Bluetooth Capture Rate									
Station		Day_1_Bluetooth_Count	Day_2_Bluetooth_Count	Day_3_Bluetooth_Count	Average_Bluetooth_Count_3_Days	Machine Count Source	Machine Count	Bluetooth Capture Rate	Directional Distribution
WB I-195 Mainline @ NW 12th Ave	B-1	13,881	13,636	14,079	13,865	FTI station, 872023	35,703	0.39	0.57
EB I-195 Mainline @ NW 12th Ave	B-2	13,881	13,636	14,079	13,865	FTI station, 872023	35,703	0.39	0.43
WB I-195 On-Ramp from N Miami Ave	B-3	628	688	682	666	v-30 Ramp	5,052	0.136	
EB I-195 Off-Ramp to N Miami Ave	B-4	10,534	10,273	10,927	10,578	Class 1, EB Only	21,433	0.49	
WB I-195 On-Ramp from US-1	B-5	4,993	4,871	5,097	4,987	v-15 Ramp + C2 WB	16,917	0.29	
EB I-195 Off-Ramp to US-1	B-6	4,534	4,737	4,705	4,659	Class2 (EB)+v-14	16,671	0.28	
WB I-195 Off-Ramp to US-1	B-7	7,312	7,469	7,842	7,541	Class2 (WB)+v-13	19,316	0.39	
EB I-195 On-Ramp From US-1	B-8	1,801	1,813	1,803	1,806	v12	4,453	0.41	
WB I-195 Mainline @ West of On-Ramp from Alton Road	B-9	13,278	13,310	14,056	13,548	CLass4	32,328	0.42	0.57
EB I-195 Mainline @ West of Off-Ramp to Alton Road	B-10	13,278	13,310	14,056	13,548	CLass4	32,328	0.42	0.43
NB I-95 Mainline near GGI	B-11	18,265	21,200	21,322	20,262	fti station	67,666	0.30	0.53
SB I-95 Mainline near GGI	B-12	18,265	21,200	21,322	20,262	fti station	67,666	0.30	0.47

Table B2 Explanation: Three first columns show how many Bluetooth devices were captures at each station and for each day of data collection for the PM peak period (3:00 PM to 8:00 PM). This is an output of data collection. As an example, for device B-3 and for second day of data collection, there are a total of 688 captured devices as can be seen in the figure (right). Three days counts were averaged in the fourth column of Table B2.

The goal is to find what percent of actual volume is captured at the station. So the number should be compared with traffic counts at same location and hours. For this example, Station 30 of volume data collection represents the similar traffic that is captured by device B-3. Raw data for each station is provided in Appendix A of data collection report. A summary of PM peak period (3:00 PM to 8:00 PM) for station v-30 can be viewed in the figure below and the average traffic for three days of data collection.

10/25/2017	15:00	38
10/25/2017	15:15	42
10/25/2017	15:30	46
10/25/2017	15:45	39
10/25/2017	16:00	37
10/25/2017	16:15	32
10/25/2017	16:30	32
10/25/2017	16:45	48
10/25/2017	17:00	32
10/25/2017	17:15	29
10/25/2017	17:30	41
10/25/2017	17:45	35
10/25/2017	18:00	32
10/25/2017	18:15	38
10/25/2017	18:30	32
10/25/2017	18:45	25
10/25/2017	19:00	29
10/25/2017	19:15	32
10/25/2017	19:30	27
10/25/2017	19:45	22
		Sum
		688

Time	Day 1	Day 2	Day 3	
3:00:00 PM	1,212	1,174	1186	
4:00:00 PM	1,003	958	976	
5:00:00 PM	1,035	961	919	
6:00:00 PM	1,070	895	966	
7:00:00 PM	907	910	983	Average
Total (5 hours)	5,227	4,898	5,030	5,052

Next column is showing Bluetooth capture rate which is computed by dividing average Bluetooth captures to actual volume of three days, as far as B-3 device example for the PM peak:

$$\frac{688}{5052} = 0.136$$

As explained before, there is one device implemented for three locations of below:

- ✓ B-1 – B-2
- ✓ B-9 – B10
- ✓ B-11 – B-12

Time	Northbound	Southbound	Both Directions
3:00:00 PM	7,583	6,584	14,167
4:00:00 PM	7,265	6,417	13,682
5:00:00 PM	7,019	6,333	13,352
6:00:00 PM	6,873	6,485	13,358
7:00:00 PM	7,447	5,660	13,107
Total (5 hours)	36,187	31,479	67,666
Directional Distribution	53%	47%	

Consequently, the Bluetooth captures of them are the same. Capture rate and directional distribution is derived from FTI stations at those locations. As an example and for station B-11_B-12, FTI Station 872134 is used to estimate capture rate and directional distribution as shown in the figure (left):

Table B3 – Assumptions in order to provide the O/Ds based on on-ramps and off-ramps. This table estimates what paired match counts or captures are related to the ramps and what percentage is related to the mainline.						
Origin	Destination	Mainline Source/Volume	Ramp Source/Volume			
B02	B04	Class_1_EB 21,433	V-31 (Ramp) 5,415	Ramp Ratio 0.253	Exiting Mached Counts 196	
B02	B06	Class_2_EB 13,092	V-14 (Ramp) 3,579	Ramp Ratio 0.21	Exiting Mached Counts 149	
	B05	Class_2_WB 8,249	Class_2_WB 14,247	V-15 (Ramp) 2,670	Ramp Ratio 0.16	
B05	B01	Class_2_WB 14,247	V-15 (Ramp) 2,670	Ramp Ratio 0.16	Exiting Mached Counts 14	
B05	B11	Class_2_WB 14,247	V-15 (Ramp) 5,415	Ramp Ratio 0.16	Exiting Mached Counts 7	
B09	B07	Class_2_WB 14,247	V-13 (Ramp) 5,069	Ramp Ratio 0.26	Exiting Mached Counts 978	
B12	B04	Class_1_EB 21,433	V-31 (Ramp) 5,415	Ramp Ratio 0.25	Exiting Mached Counts 280	
B12	B06	Class_2_EB 13,092	V-14 (Ramp) 3,579	Ramp Ratio 0.21	Exiting Mached Counts 88	

Table B3 Explanation: Target of study is to explore percentage of traffic exit any of the ramps (i.e. B-3 to B-7). However, when Bluetooth device is installed at a ramp, it captures a radius of about 300 feet which covers mainline too. As a result, match paired of Table A1 should be adjusted based on volume ratio of ramp and mainline. For example, in the PM peak period there are 776 match paired between B-2 to B-4. Volume counts at ramp B-4 and mainline shows ratio of traffic exiting ramp B-4 to mainline traffic at the ramp location is 0.253 ($=5,415 / 21,443$).

Table B4 - Bluetooth Sample Trips														
Origin	Total Trips at Origin	Destination												
		B-1	B-2	B-3	B-4	B-5	B-6	B-7	B-8	B-9	B-10	B-11	B-12	
WB I-195 Mainline @ NW 12th Ave	B-1	0	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 Mainline @ NW 12th Ave	B-2	5,948	n/a	0	n/a	196	n/a	149	n/a	n/a	n/a	494	1079	n/a
WB I-195 On-Ramp from N Miami Ave	B-3	666	60	n/a	0	n/a	n/a	n/a	n/a	n/a	n/a	n/a	64	n/a
EB I-195 Off-Ramp to N Miami Ave	B-4		n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 On-Ramp from US-1	B-5	787	14	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a	7	n/a
EB I-195 Off-Ramp to US-1	B-6		n/a	n/a	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 Off-Ramp to US-1	B-7		n/a	n/a	n/a	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a
EB I-195 On-Ramp From US-1	B-8	1,806	n/a	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a	648	n/a	n/a
WB I-195 Mainline @ West of On-Ramp from Alton Road	B-9	7,730	899	n/a	n/a	n/a	n/a	n/a	978	n/a	0	n/a	494	n/a
EB I-195 Mainline @ West of Off-Ramp to Alton Road	B-10		n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a	n/a
NB I-95 Mainline near GGI	B-11		n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a
SB I-95 Mainline near GGI	B-12	9,426	3213	n/a	n/a	110 9	n/a	409	n/a	n/a	n/a	386	n/a	0

Table B4 Explanation: As mentioned, the adjustments shown in Table B3 are applied to raw data on Table B1 to form this table. As An example a factor of 0.253 is applied to B-2 to B-4 (= 776×0.253=196). The movements which are mentioned in Table B3 needed the adjustment based on data analysis. The total trips at origin are actually the average Bluetooth counts (Table B2) multiplied by directional distribution of the station (if applicable) multiplied by adjustment factor of Table B3 if applicable. For instance, for B-5:

$$4,987 \text{ (Average Bluetooth captures at station B-5)} \times 0.16 \text{ (Ramp capture from Table A3)} = 787$$

Or for B-2:

$$13,865 \text{ (Average Bluetooth captures at station B-5)} \times 0.43 \text{ (Directional distribution from Table B2)} = 5,948$$

Table B5 - O/Ds													
		Destination											
Origin		B-1	B-2	B-3	B-4	B-5	B-6	B-7	B-8	B-9	B-10	B-11	B-12
WB I-195 Mainline @ NW 12th Ave	B-1	0	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 Mainline @ NW 12th Ave	B-2	n/a	0	n/a	3.30%	n/a	2.51%	n/a	n/a	n/a	8.31%	18.14%	n/a
WB I-195 On-Ramp from N Miami Ave	B-3	9.01%	n/a	0	n/a	n/a	n/a	n/a	n/a	n/a	n/a	9.61%	n/a
EB I-195 Off-Ramp to N Miami Ave	B-4	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 On-Ramp from US-1	B-5	1.82%	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a	0.88%	n/a
EB I-195 Off-Ramp to US-1	B-6	n/a	n/a	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 Off-Ramp to US-1	B-7	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a	n/a	n/a	n/a	n/a
EB I-195 On-Ramp From US-1	B-8	n/a	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a	100.0%	n/a	n/a
WB I-195 Mainline @ West of On-Ramp from Alton Road	B-9	11.63%	n/a	n/a	n/a	n/a	n/a	12.65%	n/a	0	n/a	6.39%	n/a
EB I-195 Mainline @ West of Off-Ramp to Alton Road	B-10	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a	n/a
NB I-95 Mainline near GGI	B-11	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	0	n/a
SB I-95 Mainline near GGI	B-12	34.09%	n/a	n/a	11.76%	n/a	4.34%	n/a	n/a	n/a	4.09%	n/a	0

Table B5 Explanation: the O-D matrix is estimated by dividing the cell values in Table B4 to total volume at origin. As can be seen, summation of each row is less than 100%. The reason is that study area is not closed and there are trips which are not captured (i.e. trips to I-95 southbound).

Table B6 - Travel Time													
		Destination											
Origin		B-1	B-2	B-3	B-4	B-5	B-6	B-7	B-8	B-9	B-10	B-11	B-12
WB I-195 Mainline @ NW 12th Ave	B-1		n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
EB I-195 Mainline @ NW 12th Ave	B-2	n/a		n/a	104	n/a	189	n/a	n/a	n/a	309	2,688	n/a
WB I-195 On-Ramp from N Miami Ave	B-3	259	n/a		n/a	n/a	n/a	n/a	n/a	n/a	n/a	1,792	n/a
EB I-195 Off-Ramp to N Miami Ave	B-4	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 On-Ramp from US-1	B-5	358	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	2,577	n/a
EB I-195 Off-Ramp to US-1	B-6	n/a	n/a	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a	n/a
WB I-195 Off-Ramp to US-1	B-7	n/a	n/a	n/a	n/a	n/a	n/a		n/a	n/a	n/a	n/a	n/a
EB I-195 On-Ramp From US-1	B-8	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a	161	n/a	n/a
WB I-195 Mainline @ West of On-Ramp from Alton Road	B-9	678	n/a	n/a	n/a	n/a	n/a	251	n/a		n/a	4,080	n/a
EB I-195 Mainline @ West of Off-Ramp to Alton Road	B-10	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a	n/a
NB I-95 Mainline near GGI	B-11	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a		n/a
SB I-95 Mainline near GGI	B-12	586	n/a	n/a	602	n/a	763	n/a	n/a	n/a	806	n/a	

Table B6 Explanation: This table uses the raw Bluetooth data travel times (Appendix B) to estimate average travel time. Raw data provides median travel time with counts and the weight average is calculated as below and (result is multiplied by 60 to convert minutes to seconds). Example is provided for movement B-2 to B-4:

$$TT = \frac{\sum Count \times TT}{\sum Count} = \frac{262 \times 1.95 + 252 \times 1.58 + 262 \times 1.67}{262 + 252 + 262} \times 60 = 104$$

Sequence	Start Location	End Location	Count	Median Travel Time (mins)
B-1 B-2>B-4	B-1 B-2	B-4	262	1.95
B-1 B-2>B-4	B-1 B-2	B-4	252	1.58
B-1 B-2>B-4	B-1 B-2	B-4	262	1.67

I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

Travel Time Reports for study: I-195 EASTBOUND AM

<u>Report Name</u>	<u>Page</u>
Study Summary	2
Overall Output Statistics	3
Detailed Stats By Run - Travel Time.....	4
Detailed Stats By Run - Stops	5
Detailed Stats By Run - Average Speed	6
Detailed Stats By Run - Total Delay.....	7
Detailed Stats By Run - Time Less Than0 MPH	8
Detailed Stats By Run - Time Less Than5 MPH	9
Detailed Stats By Run - Time Less Than20 MPH	10
Speed/Distance Plot of All Runs	11
Time/Space Trajectory of All Runs.....	12
Speed Profile (Distance vs Speed) for RUN 2 EB AM 2-14-2018-R001	13
Speed Profile (Distance vs Speed) for RUN 3 EB AM 2-14-2018-R001	17
Speed Profile (Distance vs Speed) for RUN 5 EB AM 2-15-2018-R001	21
Speed Profile (Distance vs Speed) for RUN 6 EB AM 2-15-2018-R001	25
Speed Profile (Distance vs Speed) for RUN 7 EB AM 2-15-2018-R001	29
Speed Profile (Distance vs Speed) for RUN 8 EB AM 2-15-2018-R001	33
Speed Profile (Time vs Speed) for RUN 2 EB AM 2-14-2018-R001	37
Speed Profile (Time vs Speed) for RUN 3 EB AM 2-14-2018-R001	38
Speed Profile (Time vs Speed) for RUN 5 EB AM 2-15-2018-R001	39
Speed Profile (Time vs Speed) for RUN 6 EB AM 2-15-2018-R001	40
Speed Profile (Time vs Speed) for RUN 7 EB AM 2-15-2018-R001	41
Speed Profile (Time vs Speed) for RUN 8 EB AM 2-15-2018-R001	42

I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 1

Study Summary Runs Used in This Study

Run Title	Start Date	Start Time	Length	Before/After	Run Type
RUN 2 EB AM 2-14-2018-R001	02/14/18	07:38:56	27616	Before	Secondary
RUN 3 EB AM 2-14-2018-R001	02/14/18	08:01:58	27520	Before	Secondary
RUN 5 EB AM 2-15-2018-R001	02/14/18	07:03:47	27531	Before	Secondary
RUN 6 EB AM 2-15-2018-R001	02/14/18	07:25:12	27643	Before	Secondary
RUN 7 EB AM 2-15-2018-R001	02/14/18	07:46:25	27475	Before	Secondary
RUN 8 EB AM 2-15-2018-R001	02/14/18	08:08:05	27509	Before	Secondary

Notes:

Node Info

#	Length	Name
1	0	NW 13th Avenue
2	6966	I-95
3	2653	NW 1st Avenue
4	1426	Biscayne Blvd
5	1343	NE 36th Street
6	2539	Bridge 870301 end
7	7561	Bridge 870302 Begin
8	2111	Physical Gore Off Ramp
9	2910	Alton RD

Length of Study Route = 27,509 feet.

I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 2

Overall Output Statistics

Node #	Length (ft)	Node Name	Travel Time	# of Stops	Avg Speed (MPH)	Total Delay	Time <= 0 MPH	Time <= 5 MPH	Time <= 20 MPH
1	0	NW 13th Avenue							
2	6966	I-95	173.7	3.0	27.3	61.3	3.8	16.7	86.7
3	2653	NW 1st Avenue	43.0	0.0	42.1	5.2	0.0	0.0	3.5
4	1426	Biscayne Blvd	17.8	0.0	54.5	0.0	0.0	0.0	0.0
5	1343	NE 36th Street	17.0	0.0	53.9	0.0	0.0	0.0	0.0
6	2539	Bridge 870301 end	35.5	0.0	48.8	0.0	0.0	0.0	0.0
7	7561	Bridge 870302 Begin	132.2	0.5	39.0	18.2	1.7	3.2	23.2
8	2111	Physical Gore Off Ramp	49.8	0.5	28.9	15.3	2.2	3.3	18.2
9	2910	Alton RD	150.7	3.0	13.2	100.8	43.0	56.0	110.5
Total	27,509		619.7	7	30.3	200.8	50.7	79.2	242

Stats based on 6 runs.

Stops based on a Stop Speed of 5 MPH.

Total Delay based on a Normal Speed of 0 MPH.

I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 3

Travel Time

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	NW 13th Avenue						
2	6966	I-95	99	209	101	117	221	295
3	2653	NW 1st Avenue	36	77	35	35	36	39
4	1426	Biscayne Blvd	19	18	17	17	17	19
5	1343	NE 36th Street	17	16	18	17	17	17
6	2539	Bridge 870301 end	43	35	34	32	33	36
7	7561	Bridge 870302 Begin	106	98	208	126	96	159
8	2111	Physical Gore Off Ramp	33	36	40	36	30	124
9	2910	Alton RD	180	183	60	110	156	215
Total	27,509		533	672	513	490	606	904

Run # 1 = RUN 2 EB AM 2-14-2018-R001

Run # 2 = RUN 3 EB AM 2-14-2018-R001

Run # 3 = RUN 5 EB AM 2-15-2018-R001

Run # 4 = RUN 6 EB AM 2-15-2018-R001

Run # 5 = RUN 7 EB AM 2-15-2018-R001

Run # 6 = RUN 8 EB AM 2-15-2018-R001

I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 4

Number of Stops

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	NW 13th Avenue						
2	6966	I-95	0	5	0	0	6	7
3	2653	NW 1st Avenue	0	0	0	0	0	0
4	1426	Biscayne Blvd	0	0	0	0	0	0
5	1343	NE 36th Street	0	0	0	0	0	0
6	2539	Bridge 870301 end	0	0	0	0	0	0
7	7561	Bridge 870302 Begin	0	0	1	0	0	2
8	2111	Physical Gore Off Ramp	0	0	0	0	0	3
9	2910	Alton RD	2	4	0	1	4	7
Total	27,509		2	9	1	1	10	19

Stops based on a Stop Speed of 5 MPH.

Run # 1 = RUN 2 EB AM 2-14-2018-R001

Run # 2 = RUN 3 EB AM 2-14-2018-R001

Run # 3 = RUN 5 EB AM 2-15-2018-R001

Run # 4 = RUN 6 EB AM 2-15-2018-R001

Run # 5 = RUN 7 EB AM 2-15-2018-R001

Run # 6 = RUN 8 EB AM 2-15-2018-R001

I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 5

Average Speed (MPH)

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	NW 13th Avenue	0.0	0.0	0.0	0.0	0.0	0.0
2	6966	I-95	48.0	22.8	47.2	40.6	21.6	16.1
3	2653	NW 1st Avenue	50.7	23.7	52.2	53.0	50.9	46.4
4	1426	Biscayne Blvd	50.9	54.1	55.9	55.2	56.0	51.2
5	1343	NE 36th Street	54.7	55.0	52.5	54.6	55.4	54.0
6	2539	Bridge 870301 end	39.7	50.7	50.3	53.2	51.8	48.0
7	7561	Bridge 870302 Begin	48.7	52.3	24.8	41.1	53.5	32.4
8	2111	Physical Gore Off Ramp	43.8	40.1	35.9	39.8	47.9	11.6
9	2910	Alton RD	10.9	10.7	33.3	18.0	12.5	9.2
Total	27,509		35.2	27.9	36.6	38.3	30.9	20.7

Run # 1 = RUN 2 EB AM 2-14-2018-R001

Run # 2 = RUN 3 EB AM 2-14-2018-R001

Run # 3 = RUN 5 EB AM 2-15-2018-R001

Run # 4 = RUN 6 EB AM 2-15-2018-R001

Run # 5 = RUN 7 EB AM 2-15-2018-R001

Run # 6 = RUN 8 EB AM 2-15-2018-R001

I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 6

Total Delay

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	NW 13th Avenue	0	0	0	0	0	0
2	6966	I-95	0	90	0	0	102	176
3	2653	NW 1st Avenue	0	31	0	0	0	0
4	1426	Biscayne Blvd	0	0	0	0	0	0
5	1343	NE 36th Street	0	0	0	0	0	0
6	2539	Bridge 870301 end	0	0	0	0	0	0
7	7561	Bridge 870302 Begin	0	0	79	0	0	30
8	2111	Physical Gore Off Ramp	0	0	4	0	0	88
9	2910	Alton RD	130	133	10	60	107	165
Total	27,509		130	254	93	60	209	459

Total Delay based on a Normal Speed of 0 MPH.

Run # 1 = RUN 2 EB AM 2-14-2018-R001

Run # 2 = RUN 3 EB AM 2-14-2018-R001

Run # 3 = RUN 5 EB AM 2-15-2018-R001

Run # 4 = RUN 6 EB AM 2-15-2018-R001

Run # 5 = RUN 7 EB AM 2-15-2018-R001

Run # 6 = RUN 8 EB AM 2-15-2018-R001

I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 7

Time Below

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	NW 13th Avenue	0	0	0	0	0	0
2	6966	I-95	0	4	0	0	11	8
3	2653	NW 1st Avenue	0	0	0	0	0	0
4	1426	Biscayne Blvd	0	0	0	0	0	0
5	1343	NE 36th Street	0	0	0	0	0	0
6	2539	Bridge 870301 end	0	0	0	0	0	0
7	7561	Bridge 870302 Begin	0	0	3	0	0	7
8	2111	Physical Gore Off Ramp	0	0	0	0	0	13
9	2910	Alton RD	65	65	0	40	28	60
Total	27,509		65	69	3	40	39	88

Run # 1 = RUN 2 EB AM 2-14-2018-R001

Run # 2 = RUN 3 EB AM 2-14-2018-R001

Run # 3 = RUN 5 EB AM 2-15-2018-R001

Run # 4 = RUN 6 EB AM 2-15-2018-R001

Run # 5 = RUN 7 EB AM 2-15-2018-R001

Run # 6 = RUN 8 EB AM 2-15-2018-R001

I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 8

Time Below

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	NW 13th Avenue	0	0	0	0	0	0
2	6966	I-95	0	15	0	0	34	51
3	2653	NW 1st Avenue	0	0	0	0	0	0
4	1426	Biscayne Blvd	0	0	0	0	0	0
5	1343	NE 36th Street	0	0	0	0	0	0
6	2539	Bridge 870301 end	0	0	0	0	0	0
7	7561	Bridge 870302 Begin	0	0	6	0	0	13
8	2111	Physical Gore Off Ramp	0	0	0	0	0	20
9	2910	Alton RD	79	79	0	43	52	83
Total	27,509		79	94	6	43	86	167

Run # 1 = RUN 2 EB AM 2-14-2018-R001

Run # 2 = RUN 3 EB AM 2-14-2018-R001

Run # 3 = RUN 5 EB AM 2-15-2018-R001

Run # 4 = RUN 6 EB AM 2-15-2018-R001

Run # 5 = RUN 7 EB AM 2-15-2018-R001

Run # 6 = RUN 8 EB AM 2-15-2018-R001

I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 9

Time Below

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	NW 13th Avenue	0	0	0	0	0	0
2	6966	I-95	0	125	0	15	140	240
3	2653	NW 1st Avenue	0	21	0	0	0	0
4	1426	Biscayne Blvd	0	0	0	0	0	0
5	1343	NE 36th Street	0	0	0	0	0	0
6	2539	Bridge 870301 end	0	0	0	0	0	0
7	7561	Bridge 870302 Begin	0	0	83	0	0	56
8	2111	Physical Gore Off Ramp	0	0	0	0	0	109
9	2910	Alton RD	154	139	0	59	122	189
Total	27,509		154	285	83	74	262	594

Run # 1 = RUN 2 EB AM 2-14-2018-R001

Run # 2 = RUN 3 EB AM 2-14-2018-R001

Run # 3 = RUN 5 EB AM 2-15-2018-R001

Run # 4 = RUN 6 EB AM 2-15-2018-R001

Run # 5 = RUN 7 EB AM 2-15-2018-R001

Run # 6 = RUN 8 EB AM 2-15-2018-R001

I-195 EASTBOUND (AM)

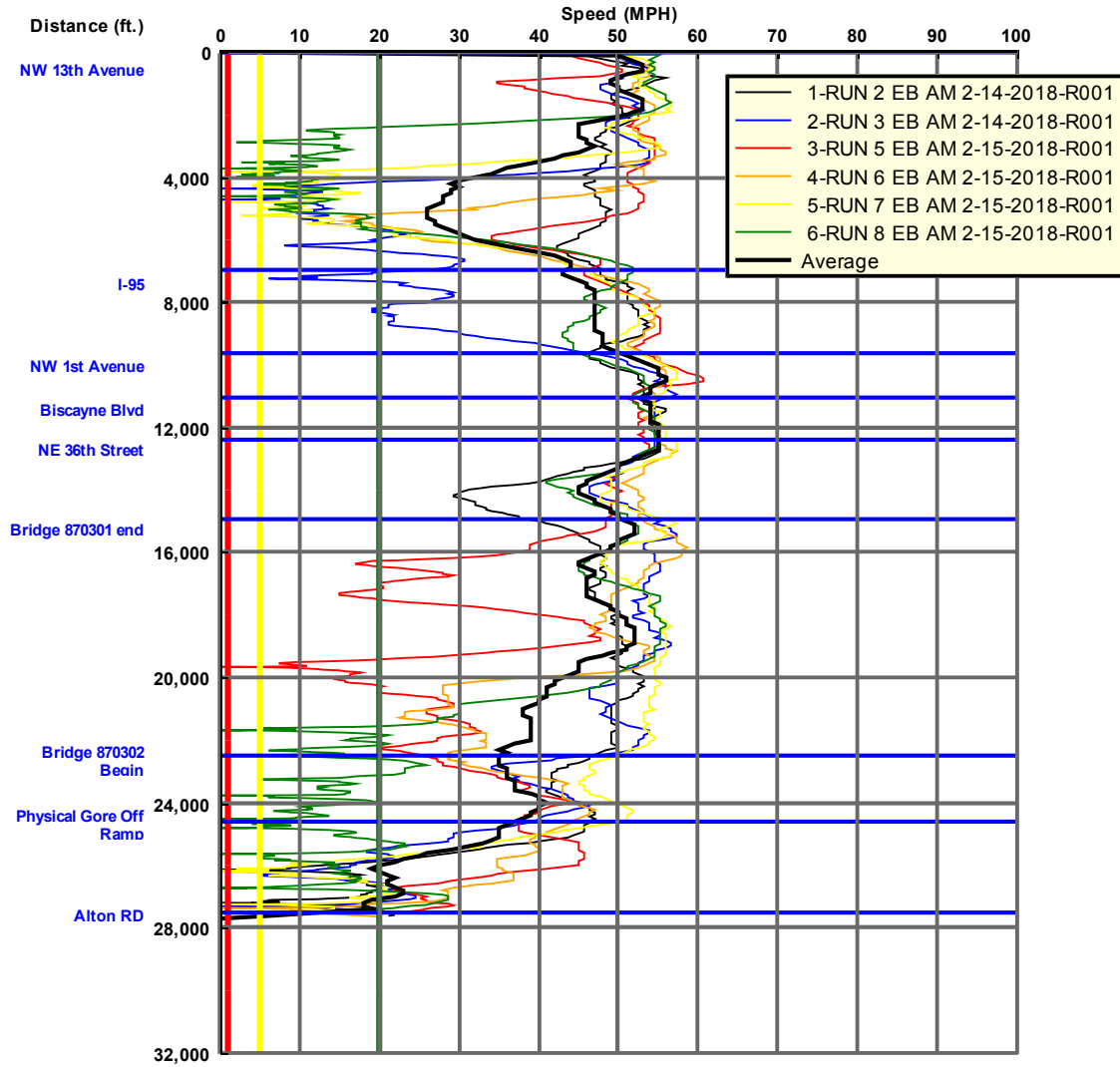
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 10

Speed/Distance Profiles of All Runs



I-195 EASTBOUND (AM)

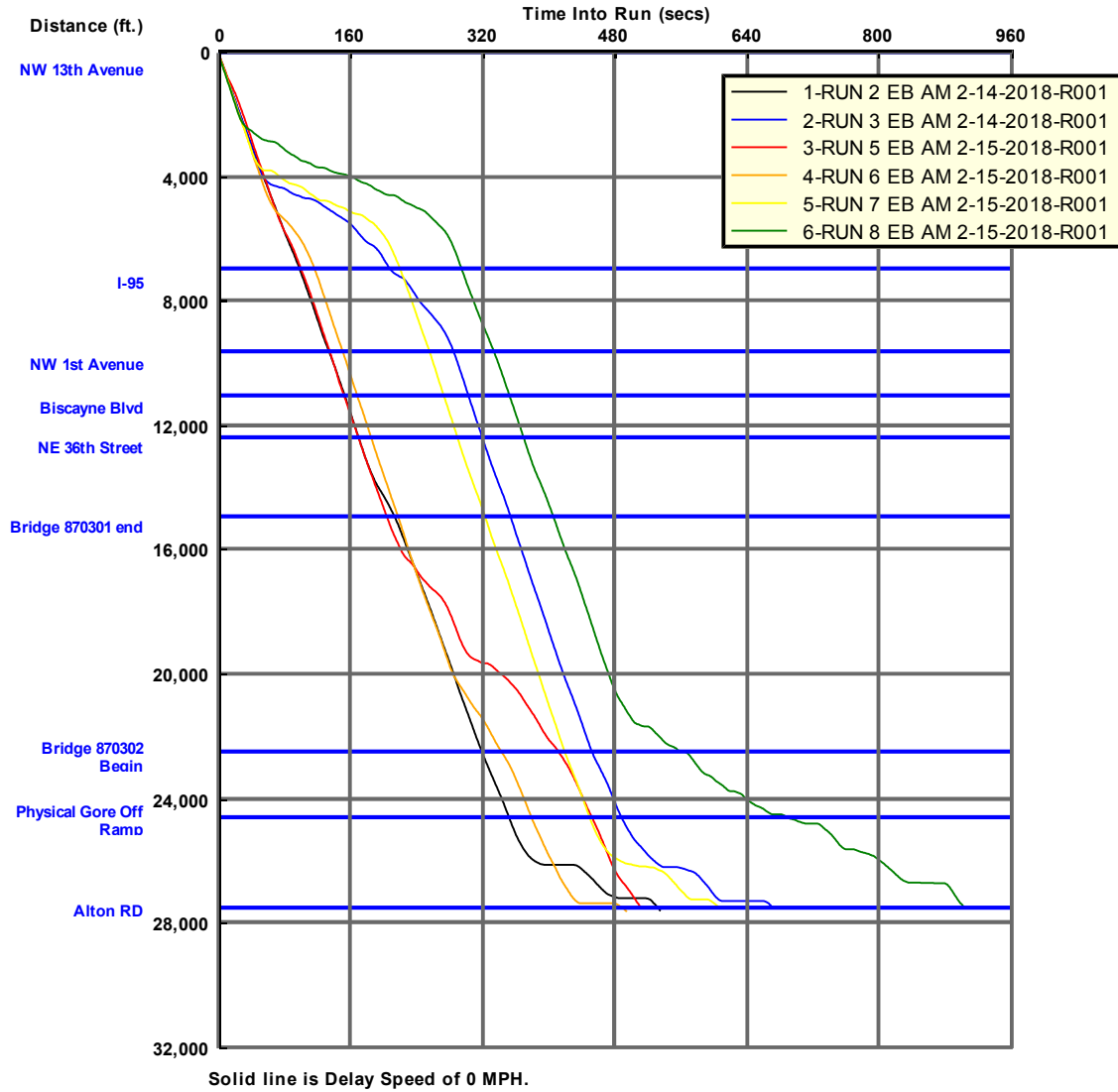
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 11

Space/Time Trajectory of All Runs



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

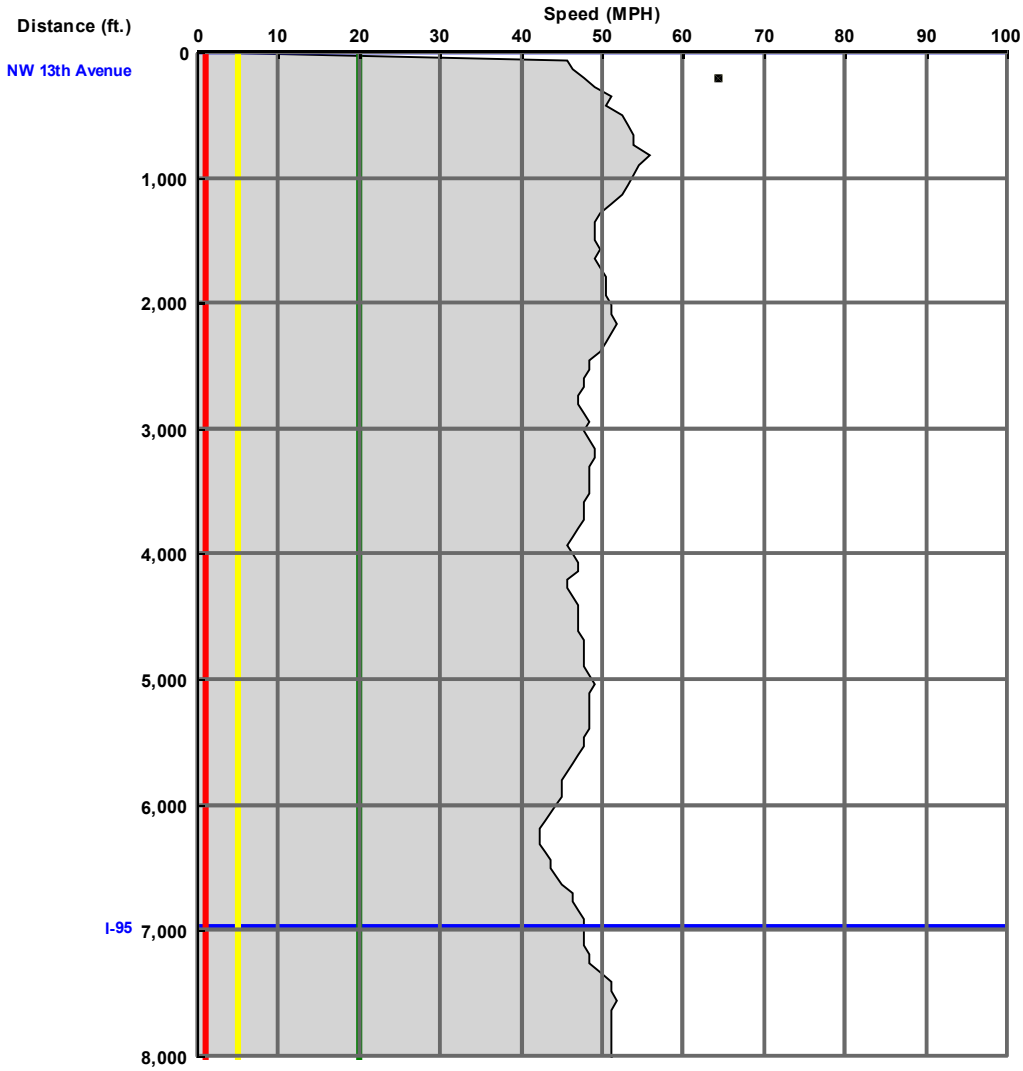
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 12

Speed Profile

Run: RUN 2 EB AM 2-14-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

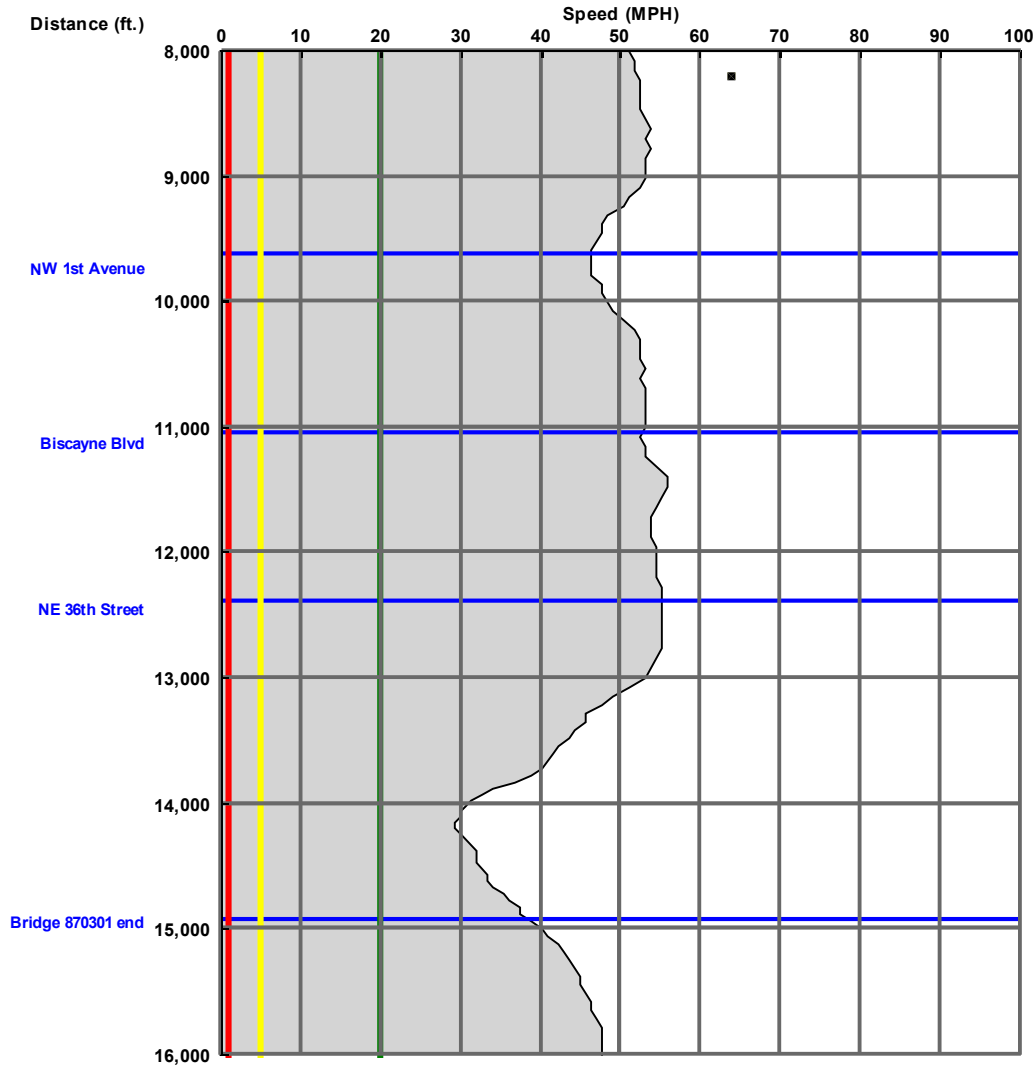
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 13

Speed Profile

Run: RUN 2 EB AM 2-14-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

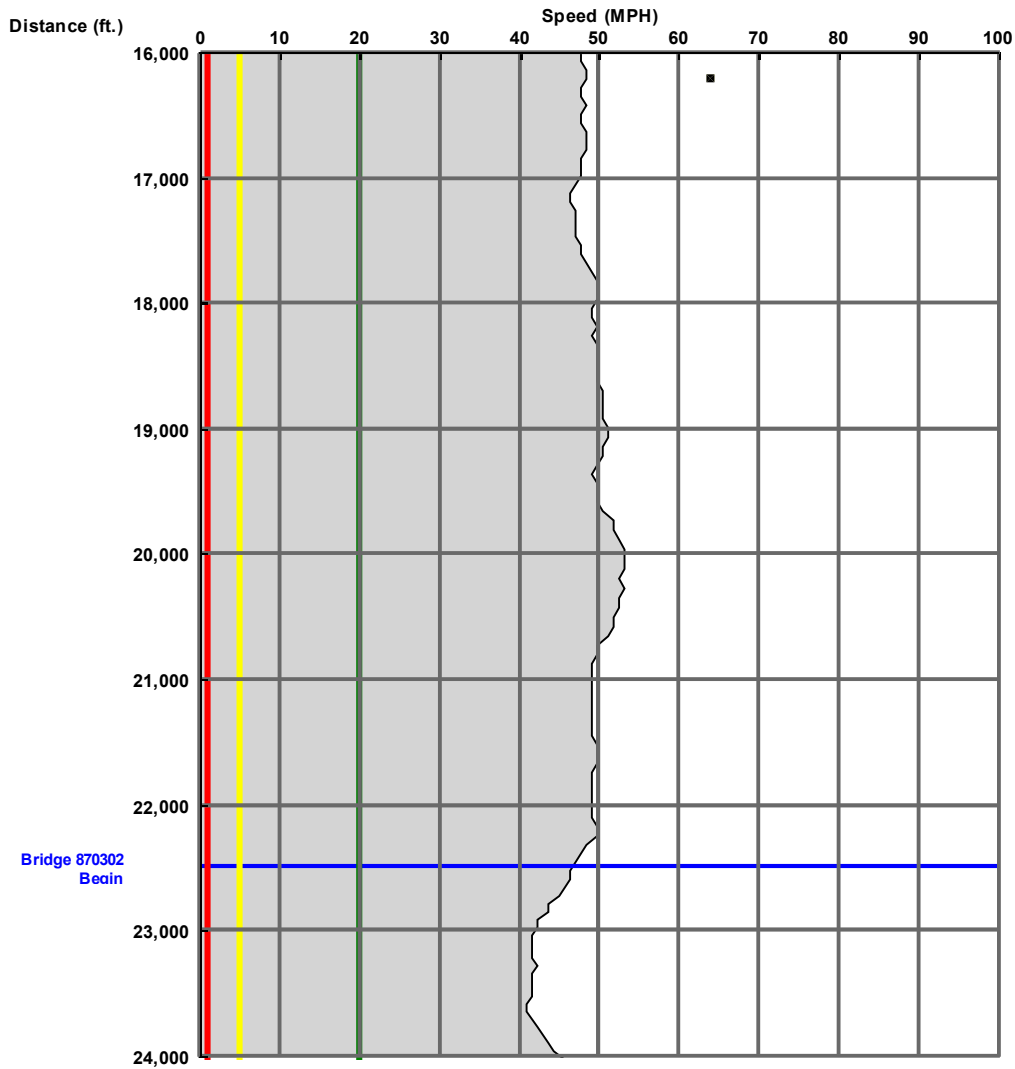
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 14

Speed Profile

Run: RUN 2 EB AM 2-14-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

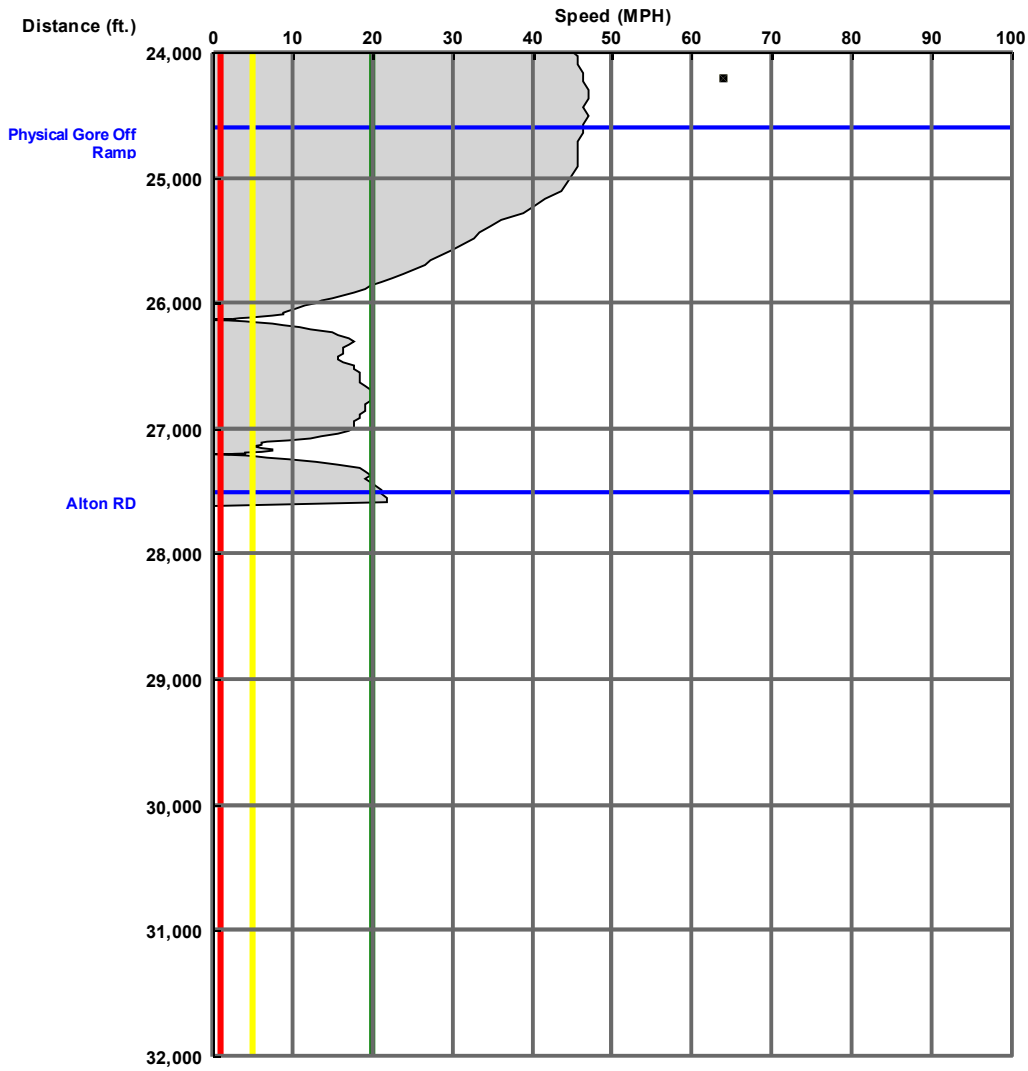
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 15

Speed Profile

Run: RUN 2 EB AM 2-14-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

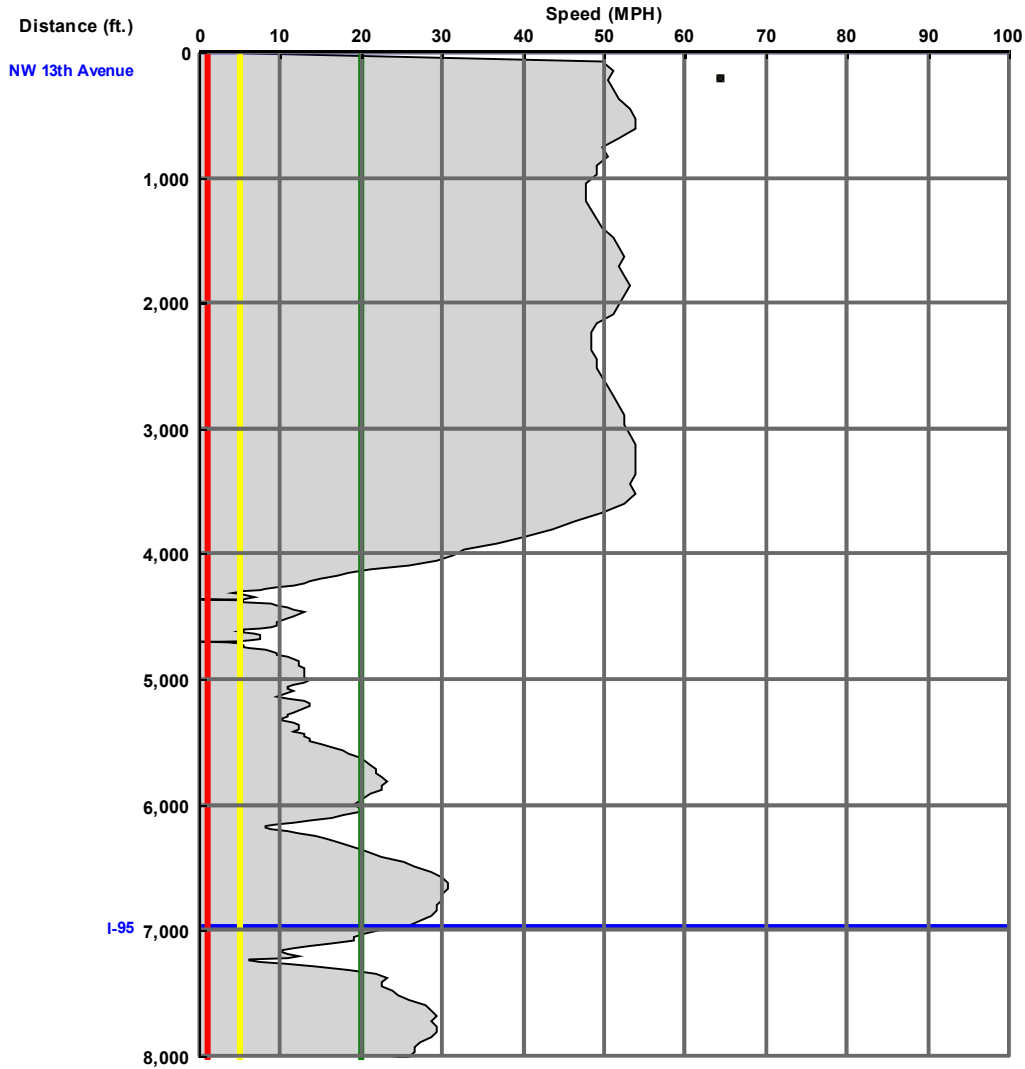
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 16

Speed Profile

Run: RUN 3 EB AM 2-14-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

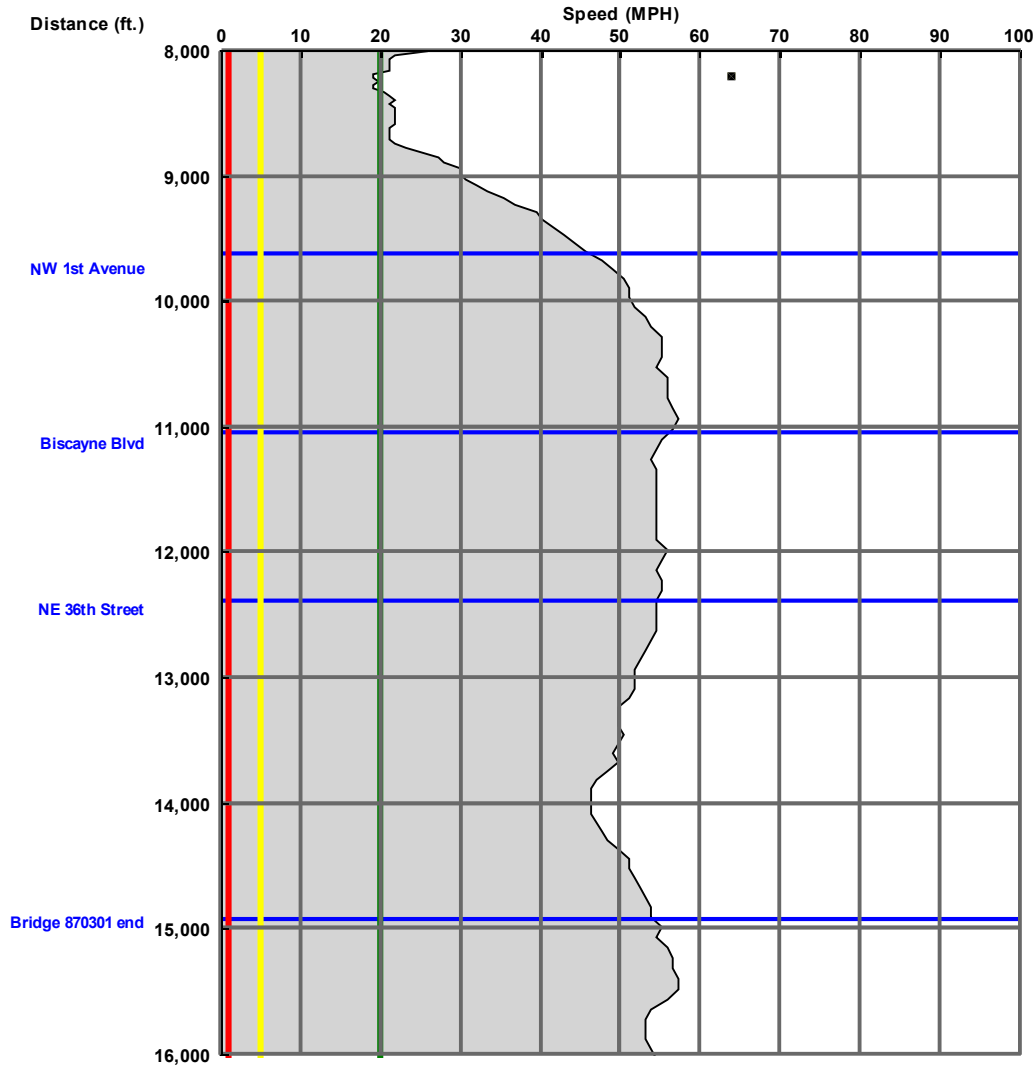
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 17

Speed Profile

Run: RUN 3 EB AM 2-14-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

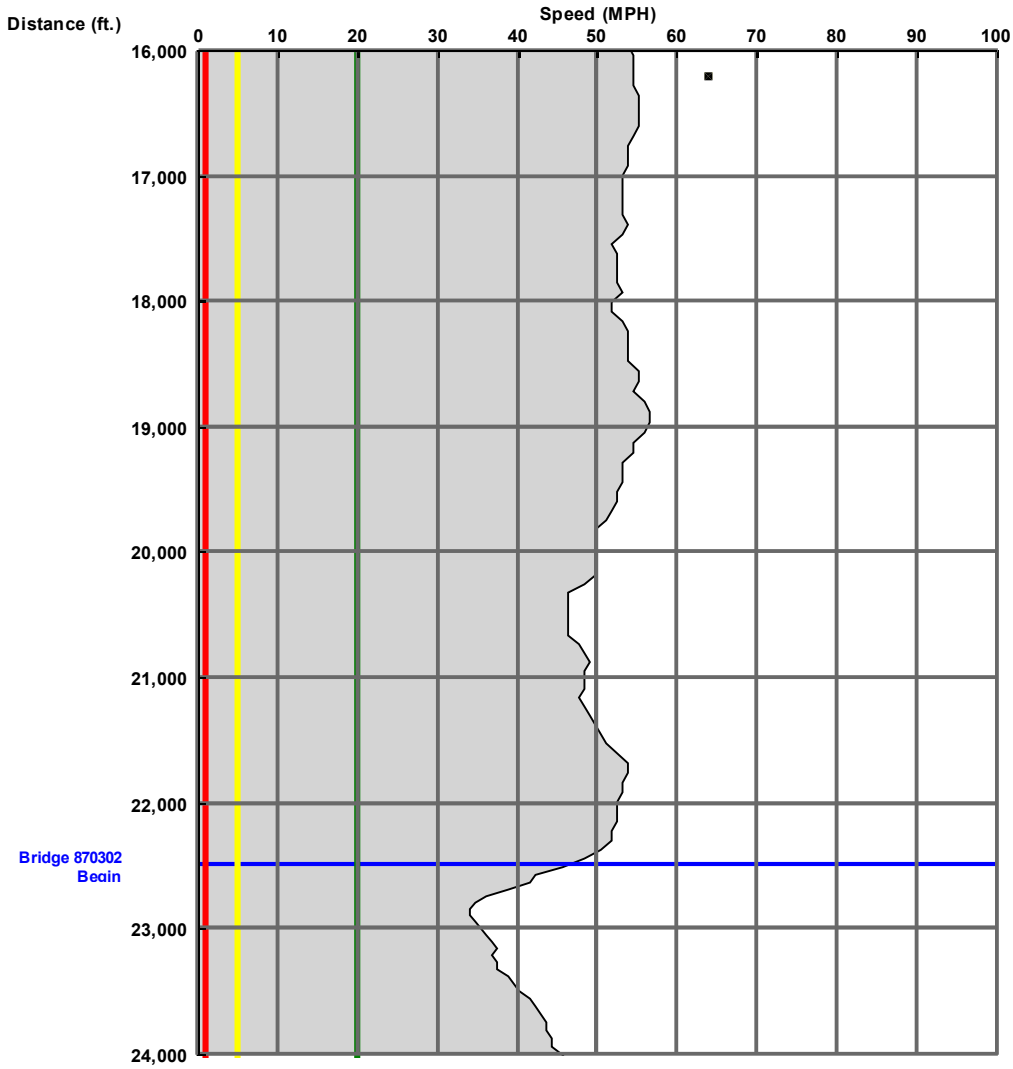
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 18

Speed Profile

Run: RUN 3 EB AM 2-14-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

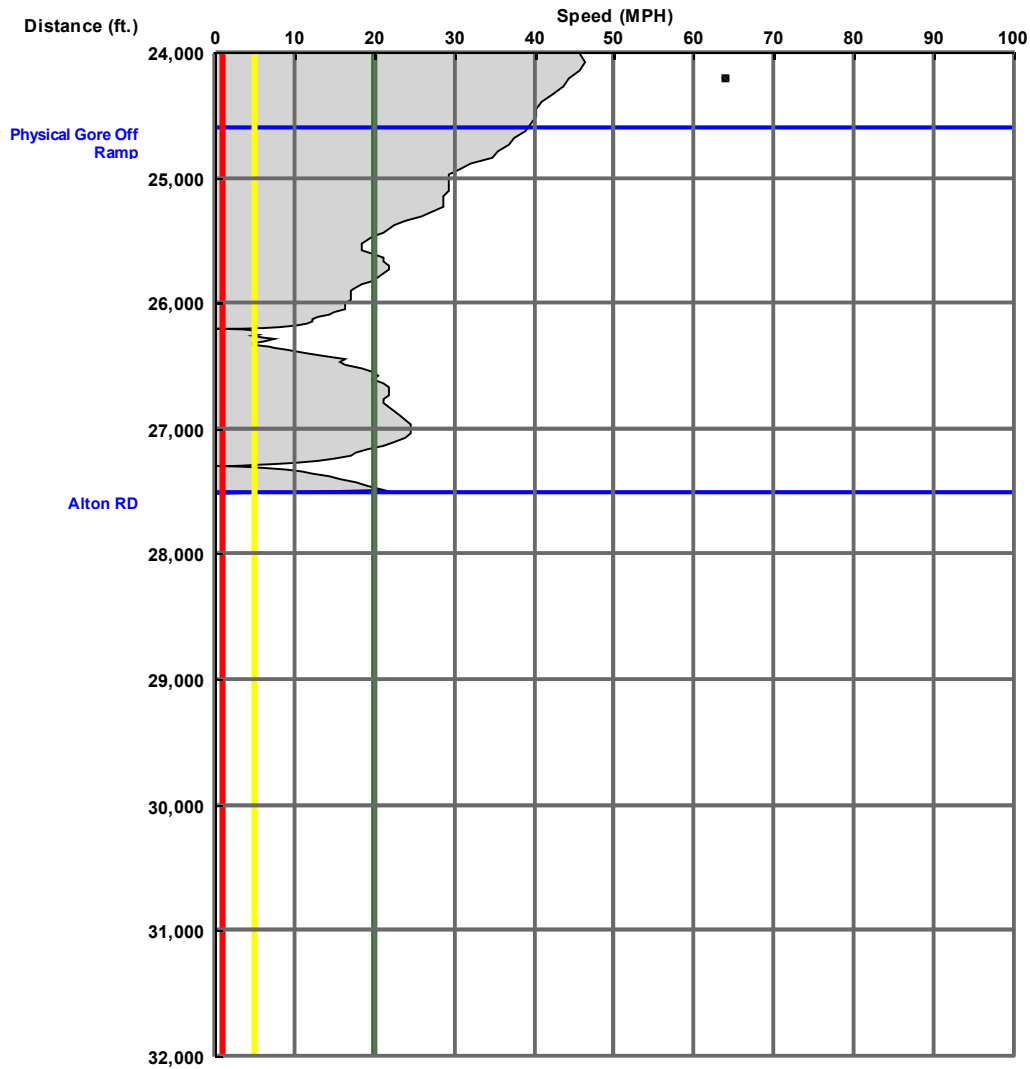
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 19

Speed Profile

Run: RUN 3 EB AM 2-14-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

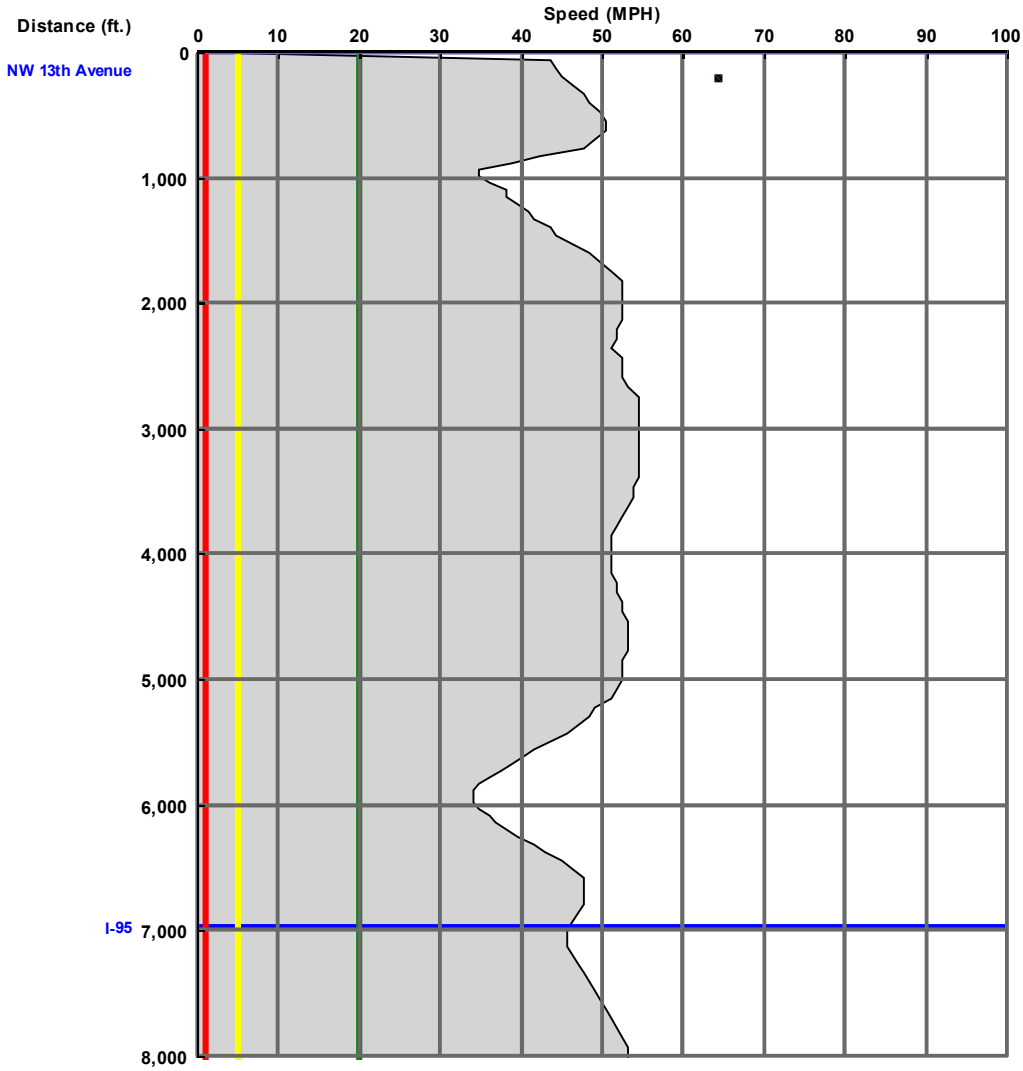
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 20

Speed Profile

Run: RUN 5 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

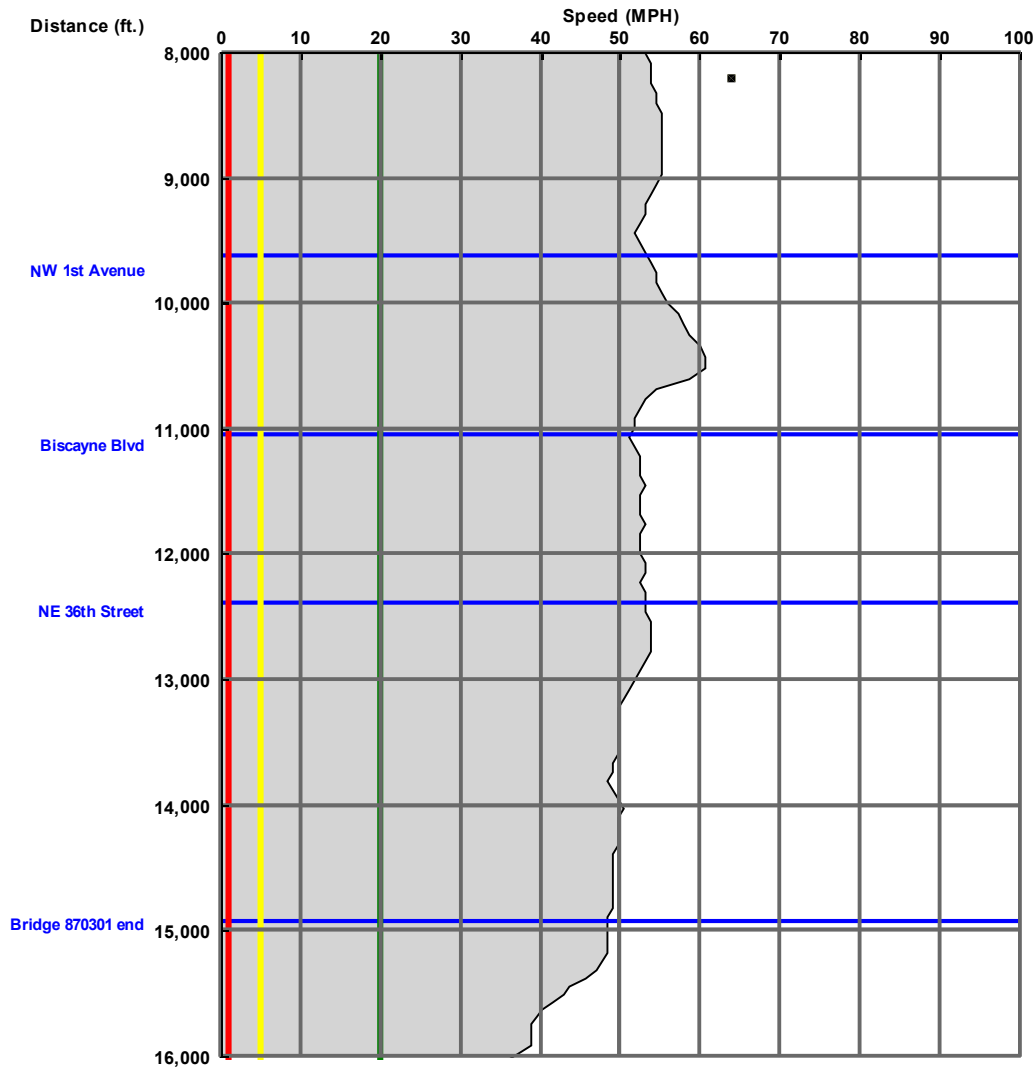
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 21

Speed Profile

Run: RUN 5 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

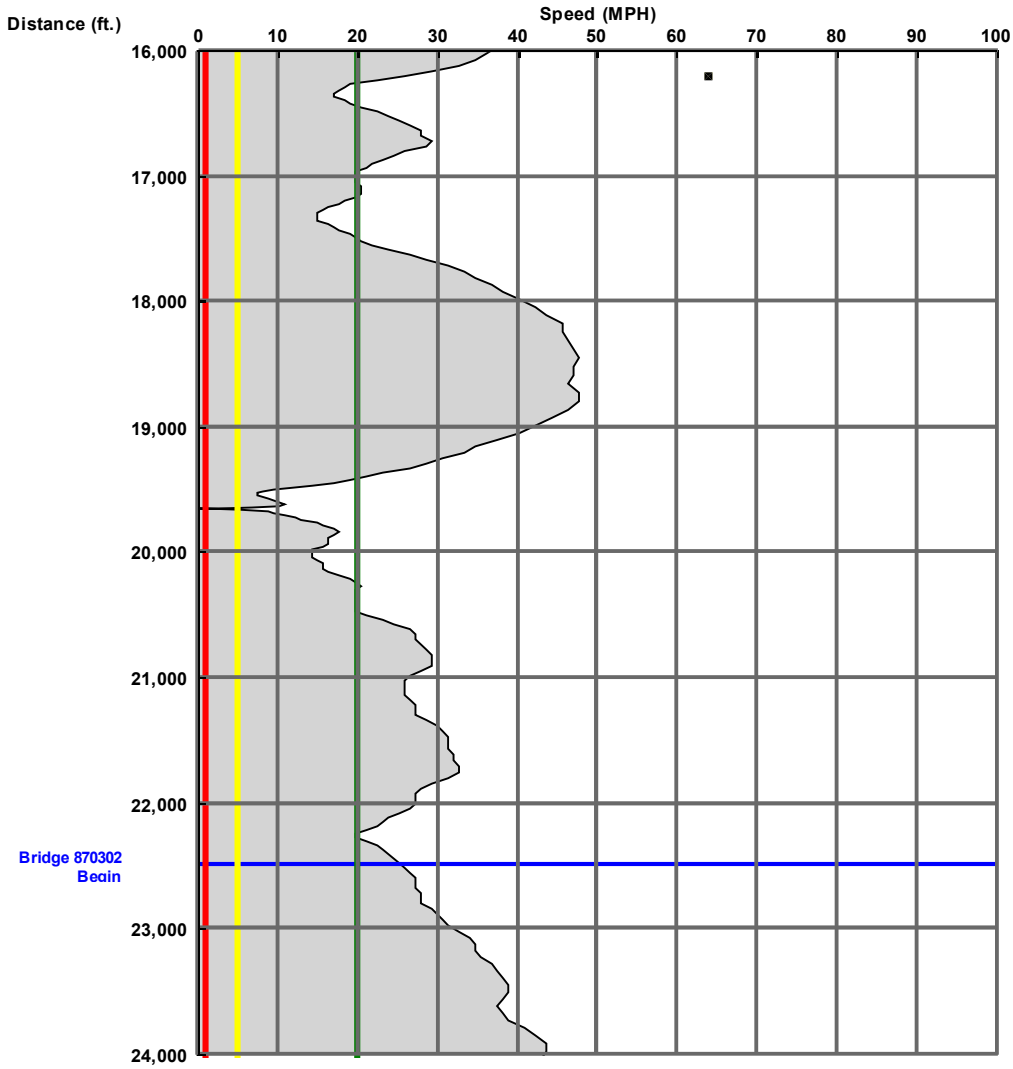
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 22

Speed Profile

Run: RUN 5 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

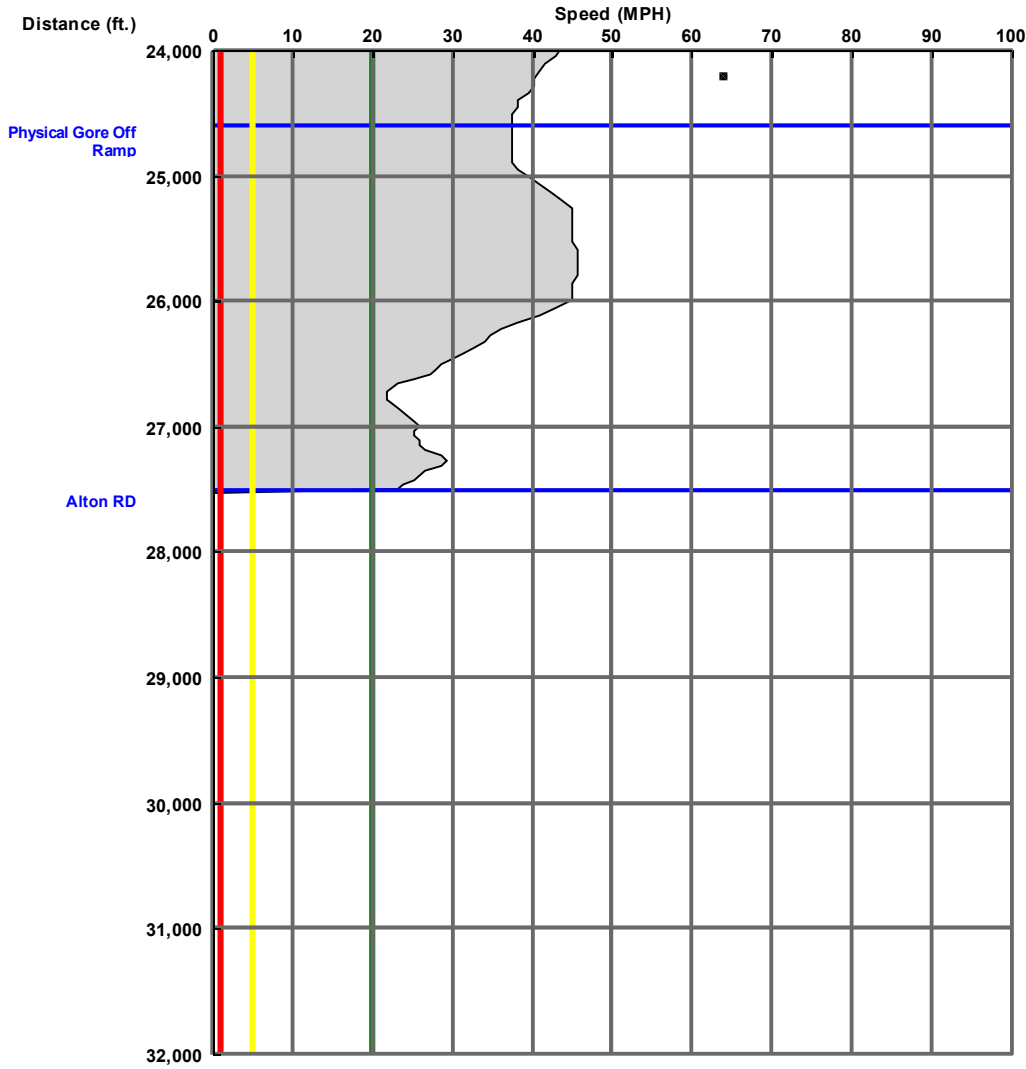
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 23

Speed Profile

Run: RUN 5 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

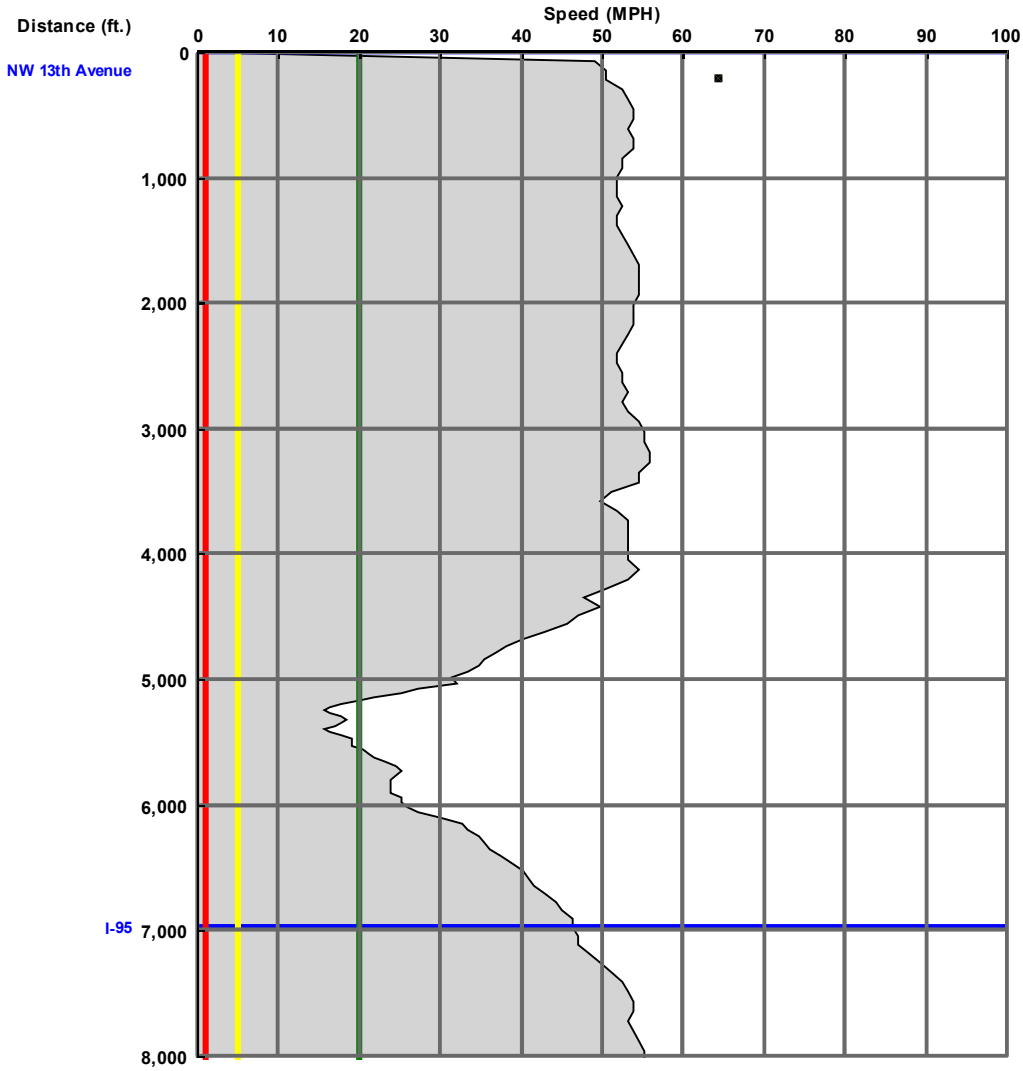
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 24

Speed Profile

Run: RUN 6 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

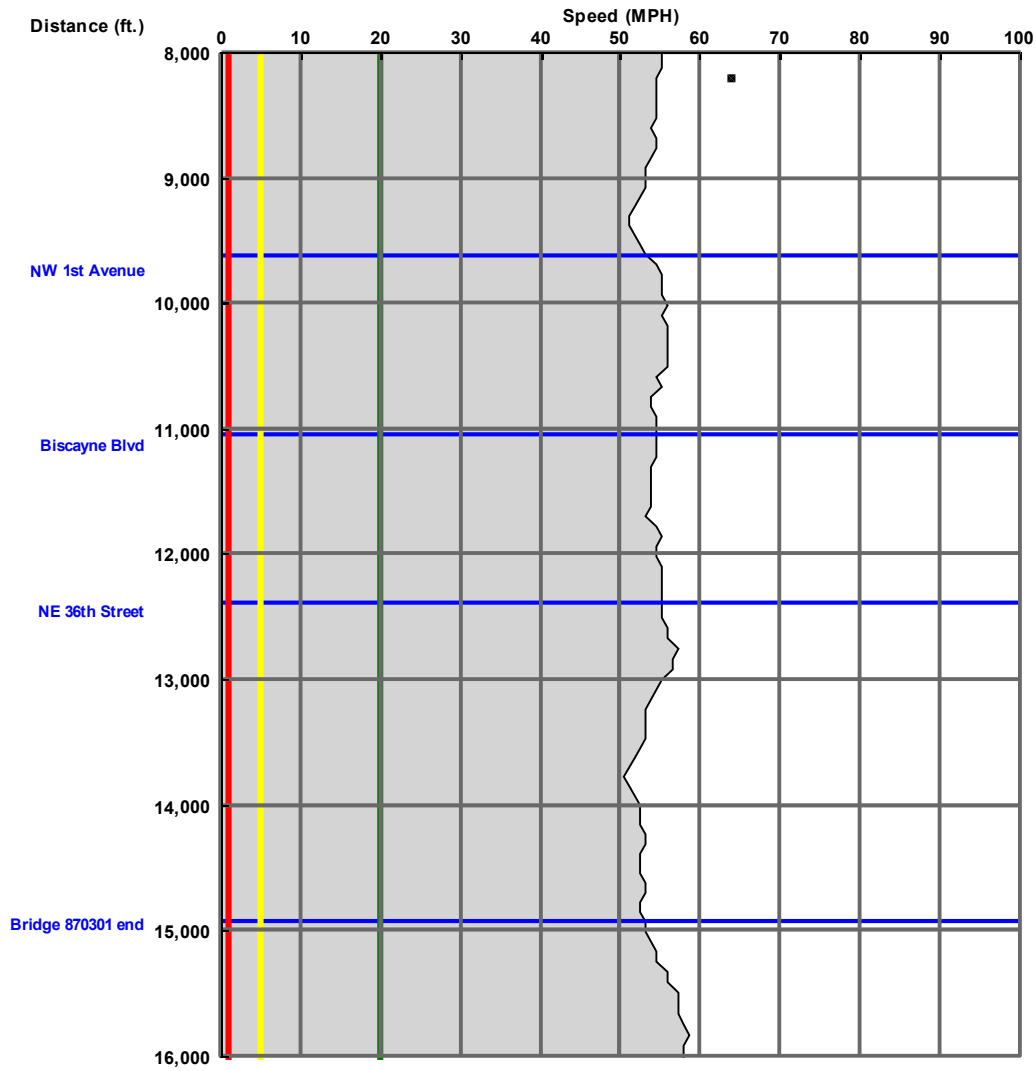
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 25

Speed Profile

Run: RUN 6 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

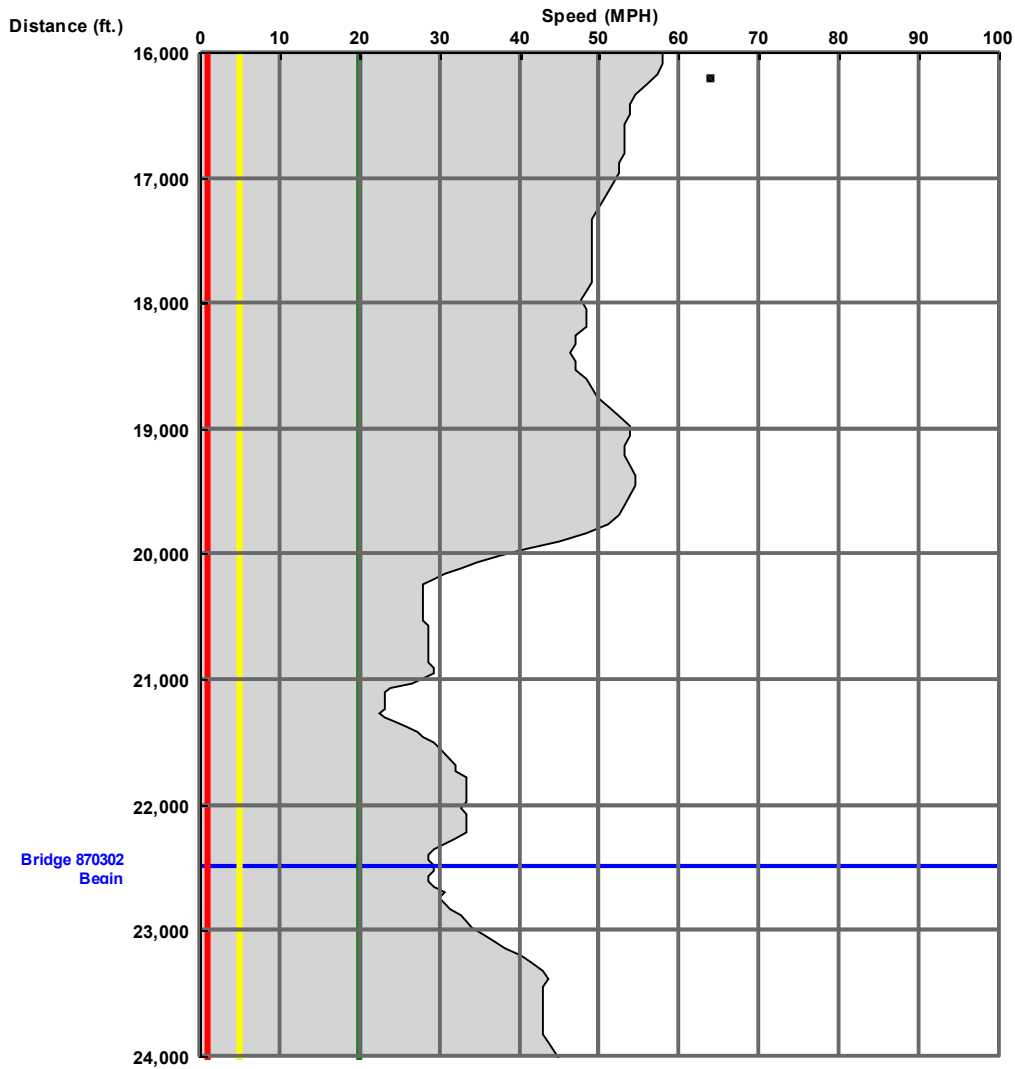
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 26

Speed Profile

Run: RUN 6 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

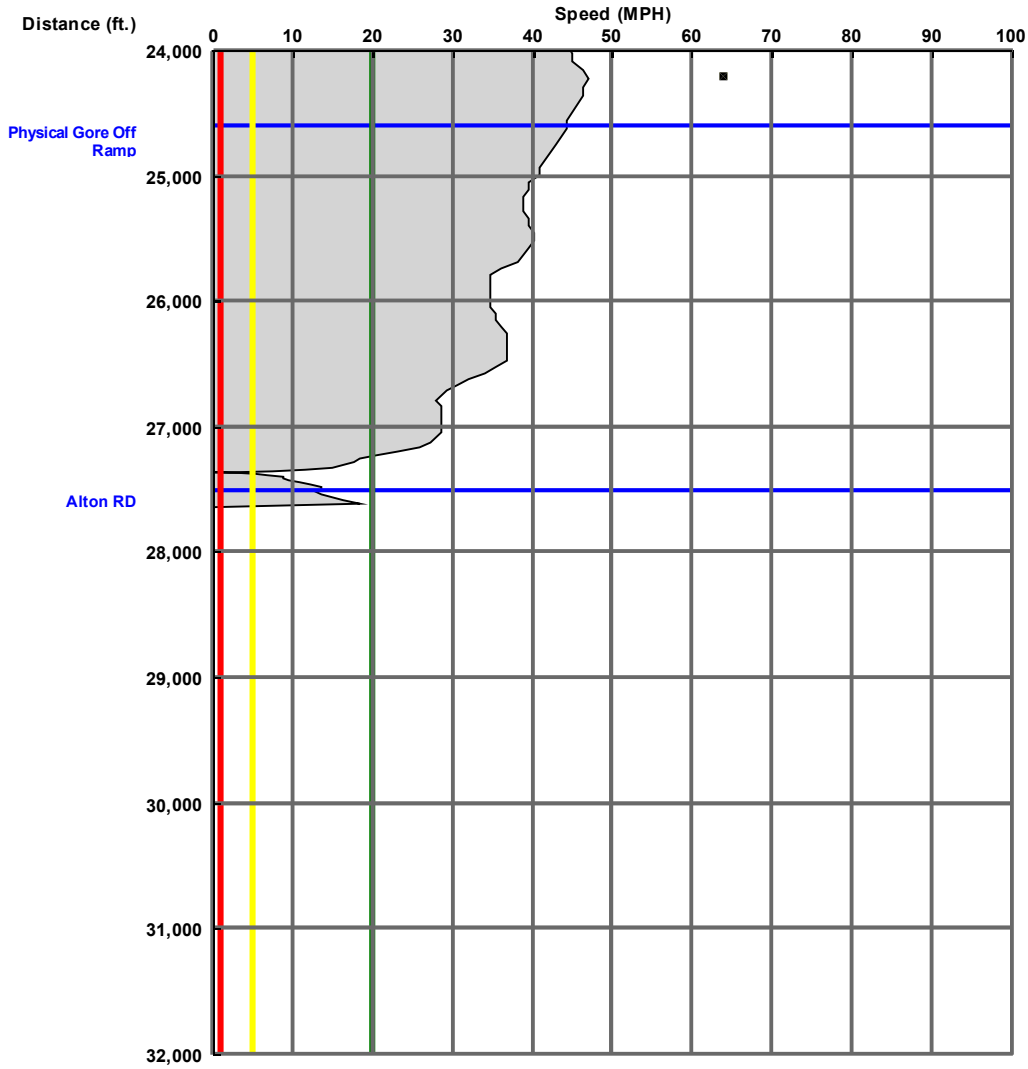
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 27

Speed Profile

Run: RUN 6 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

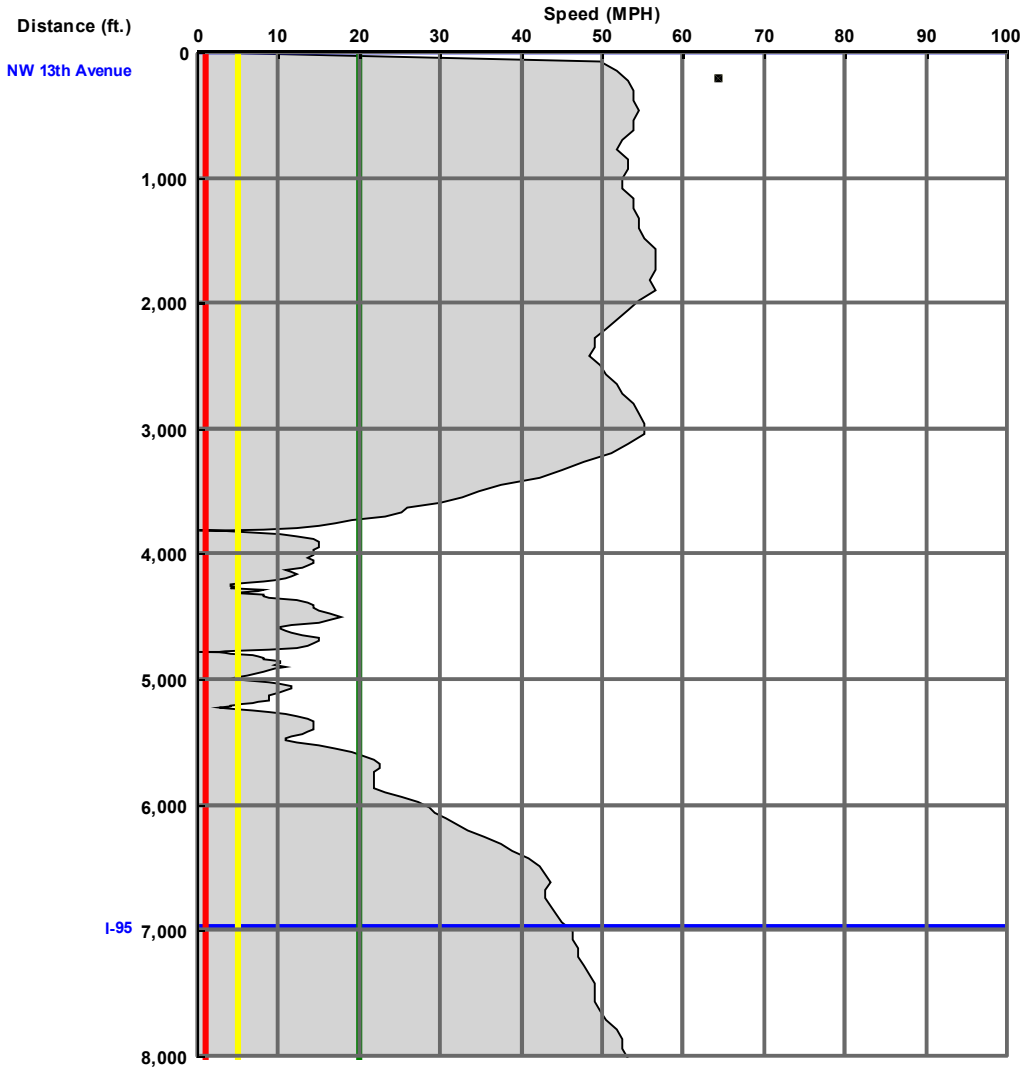
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 28

Speed Profile

Run: RUN 7 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

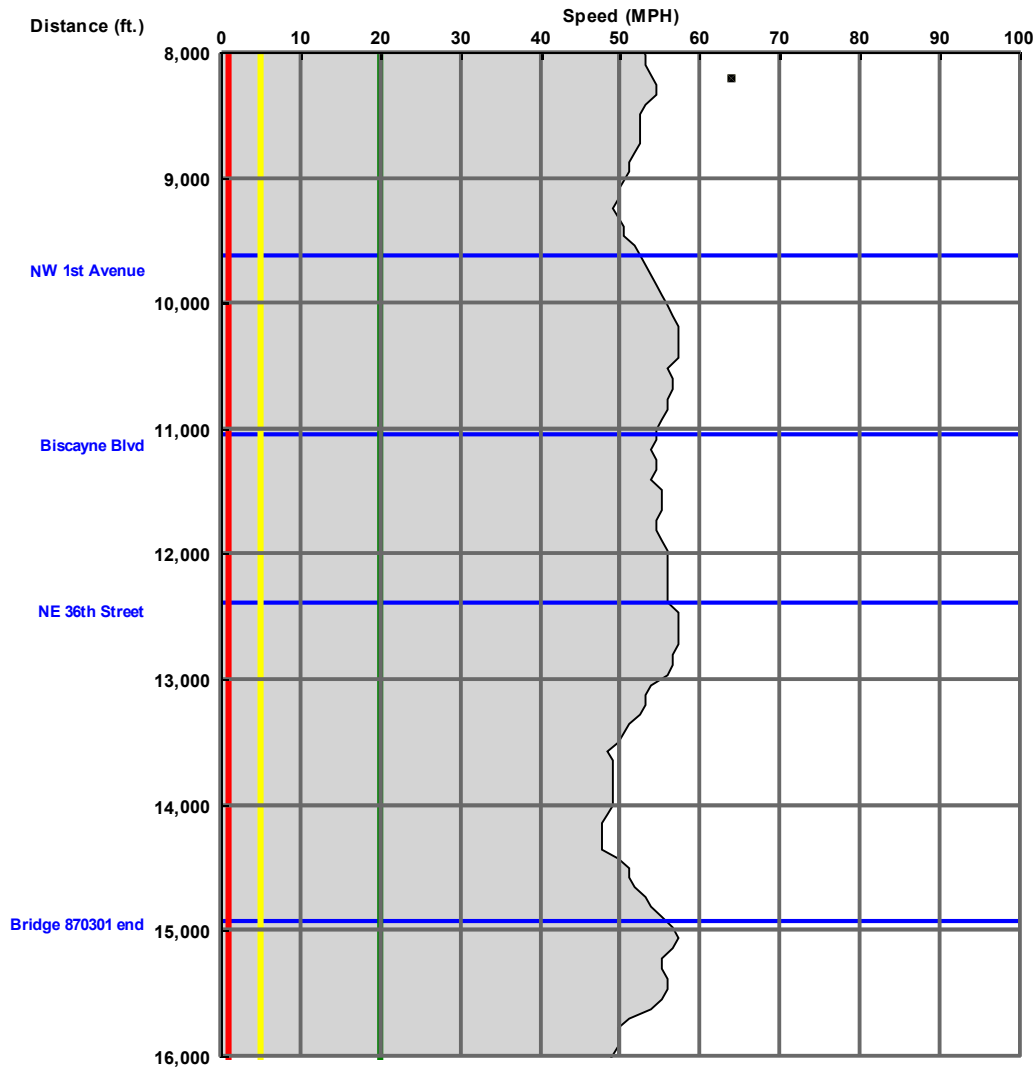
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 29

Speed Profile

Run: RUN 7 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

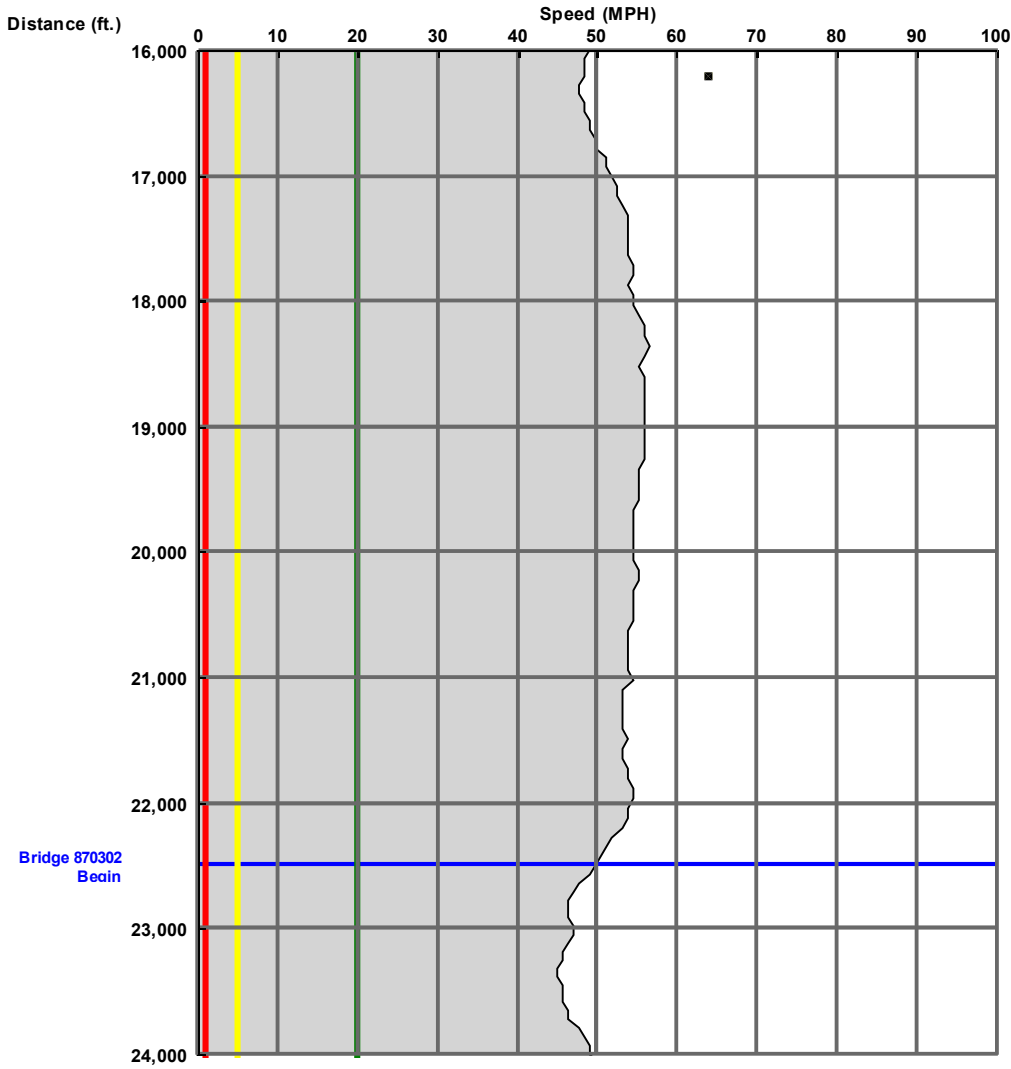
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 30

Speed Profile

Run: RUN 7 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

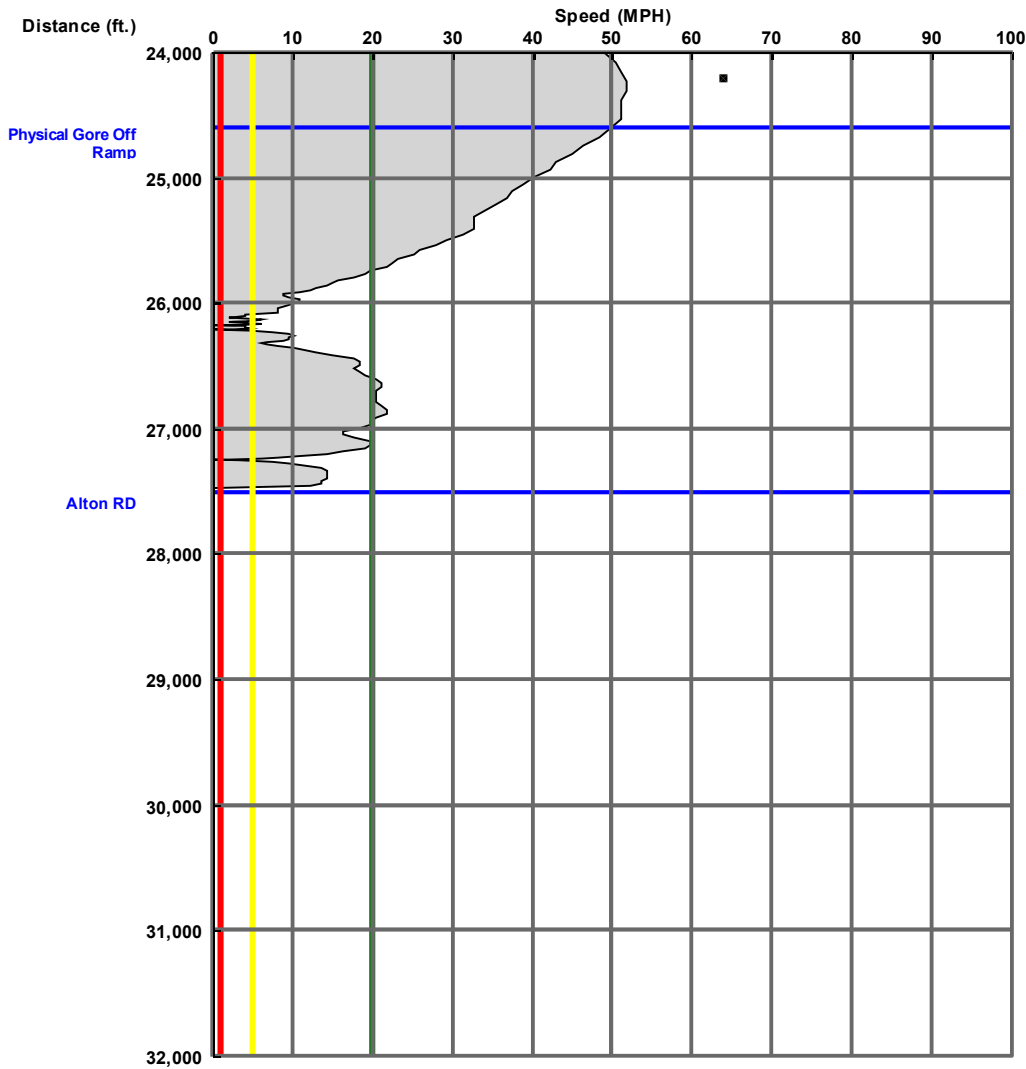
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 31

Speed Profile

Run: RUN 7 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

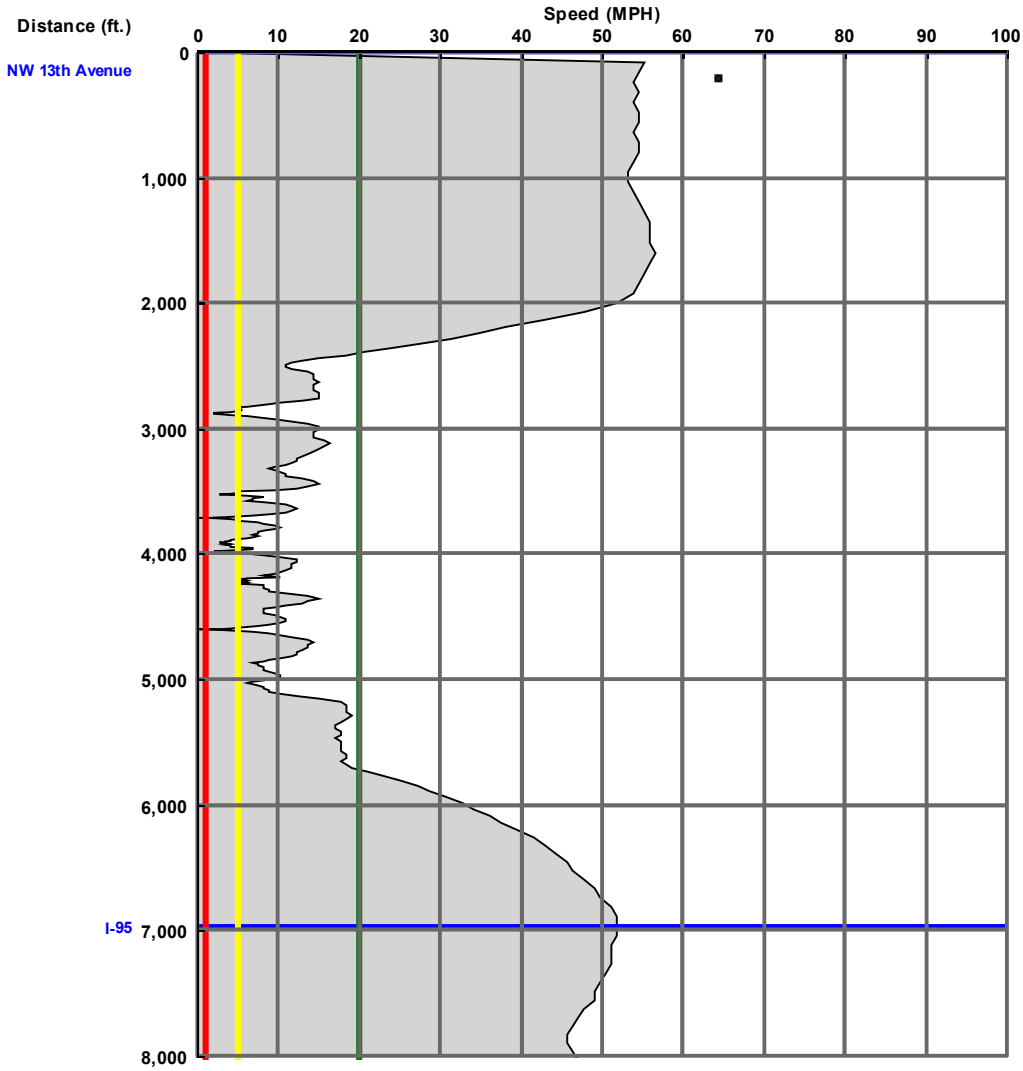
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 32

Speed Profile

Run: RUN 8 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

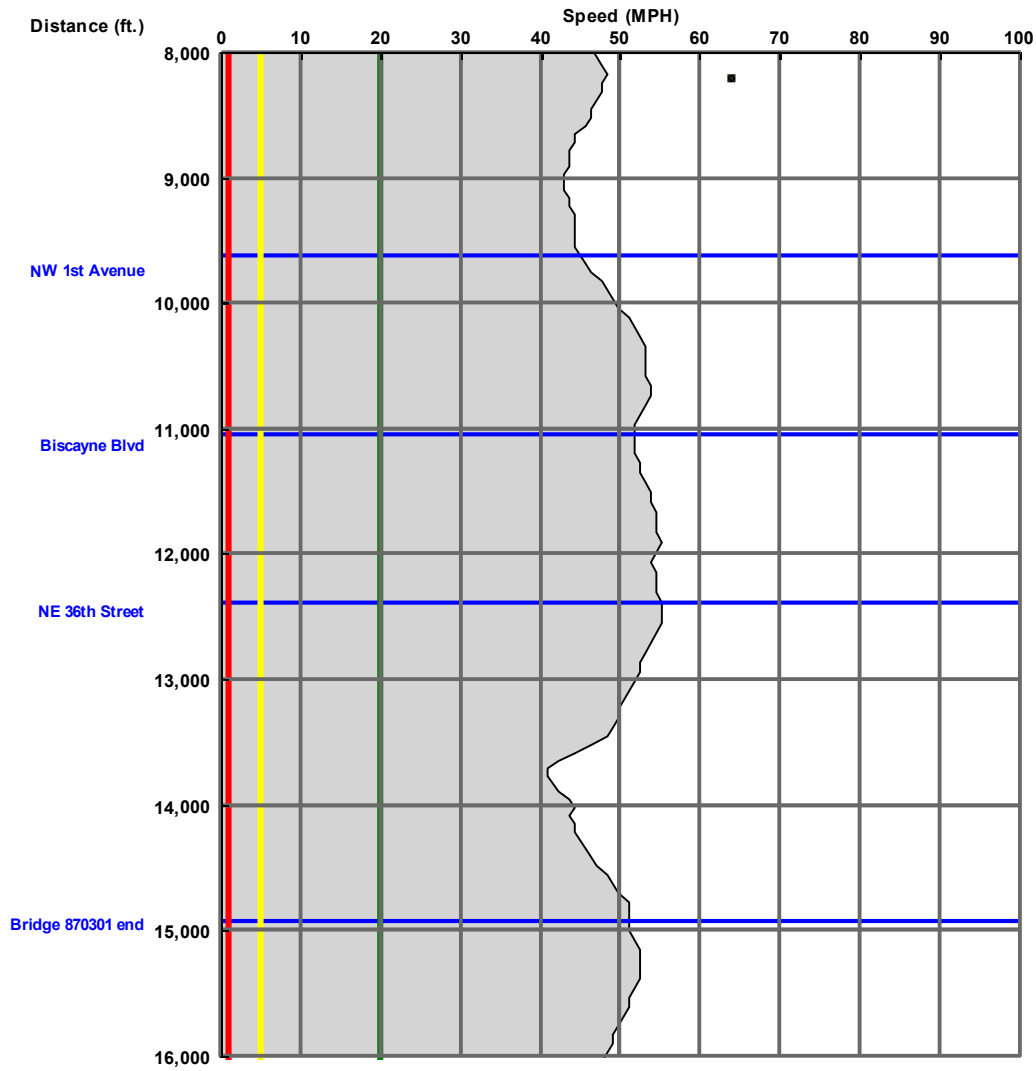
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 33

Speed Profile

Run: RUN 8 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

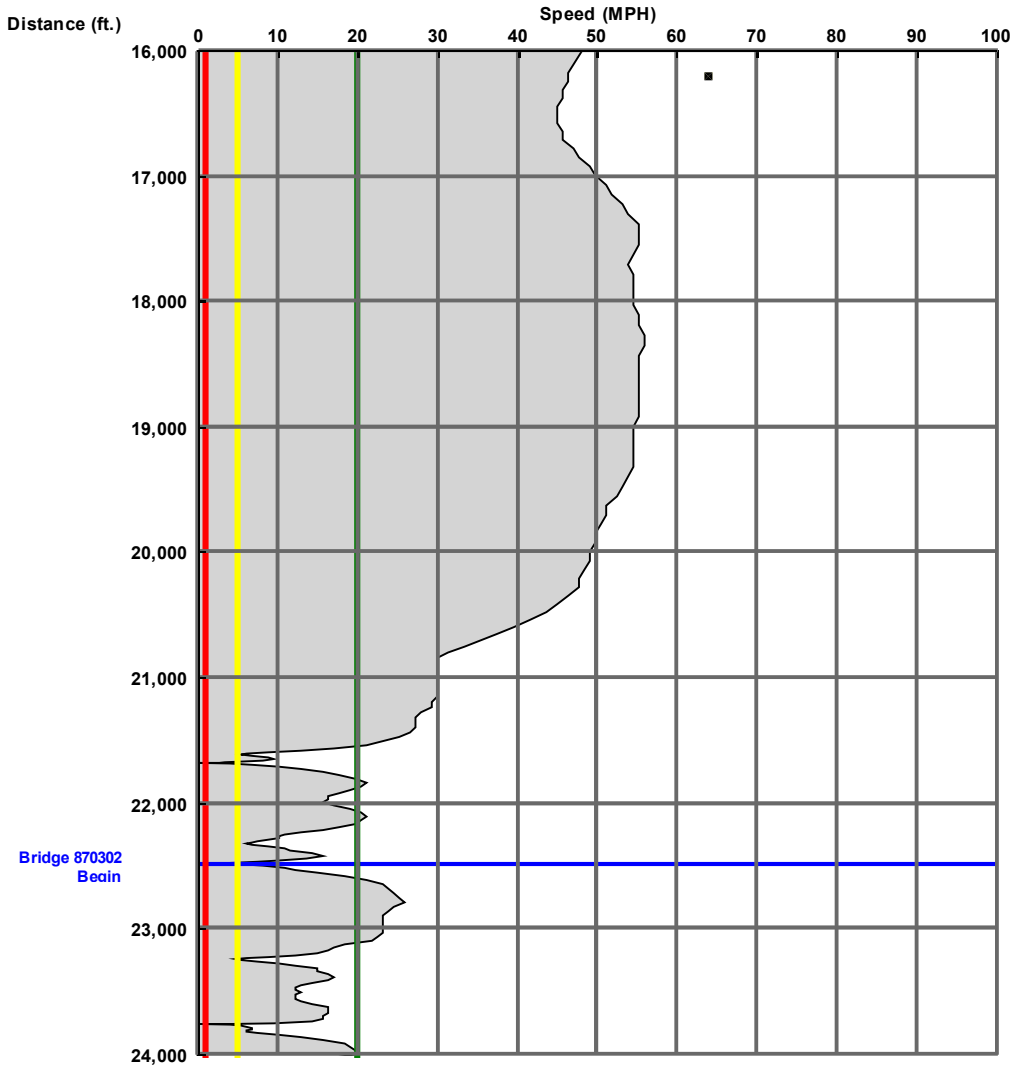
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 34

Speed Profile

Run: RUN 8 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

Ten and Two - Travel Time Data

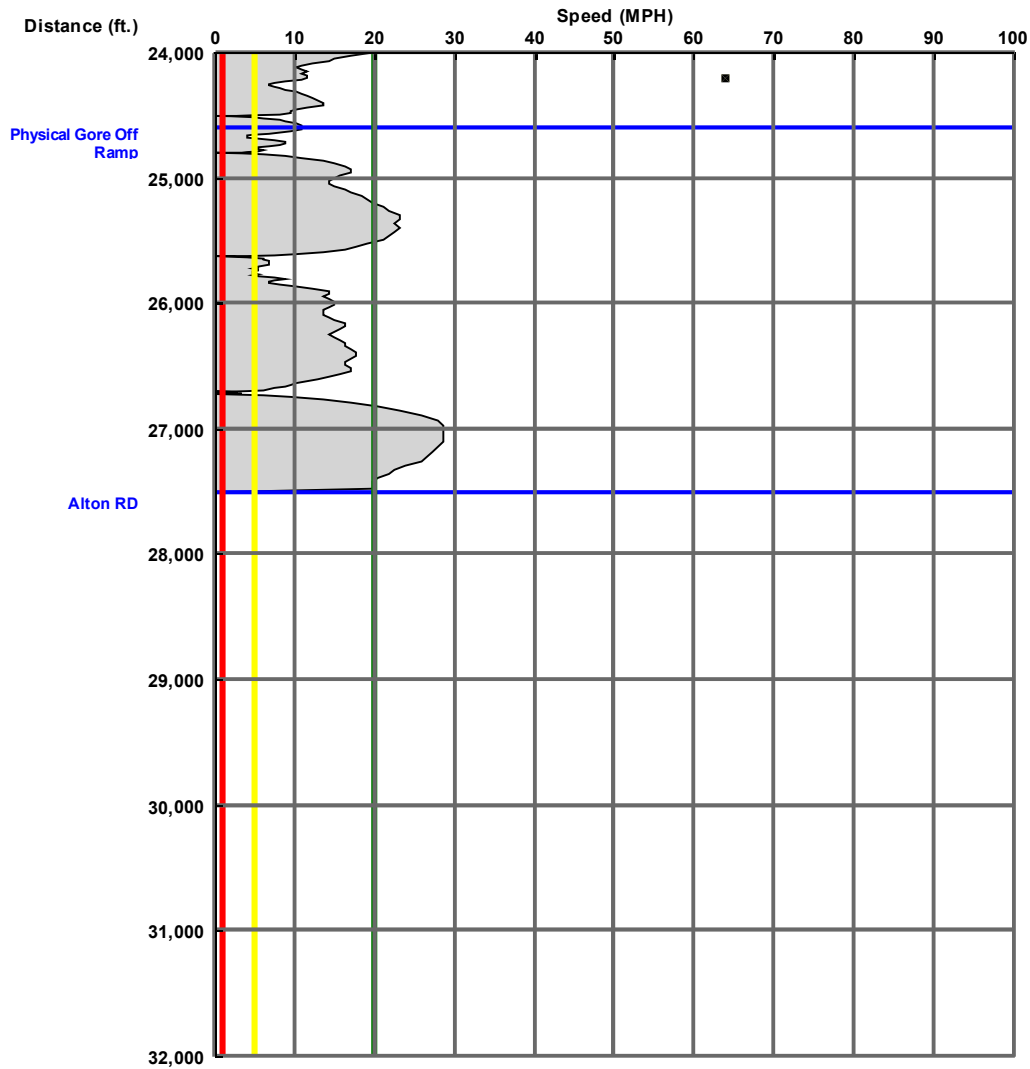
Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 35

Speed Profile

Run: RUN 8 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

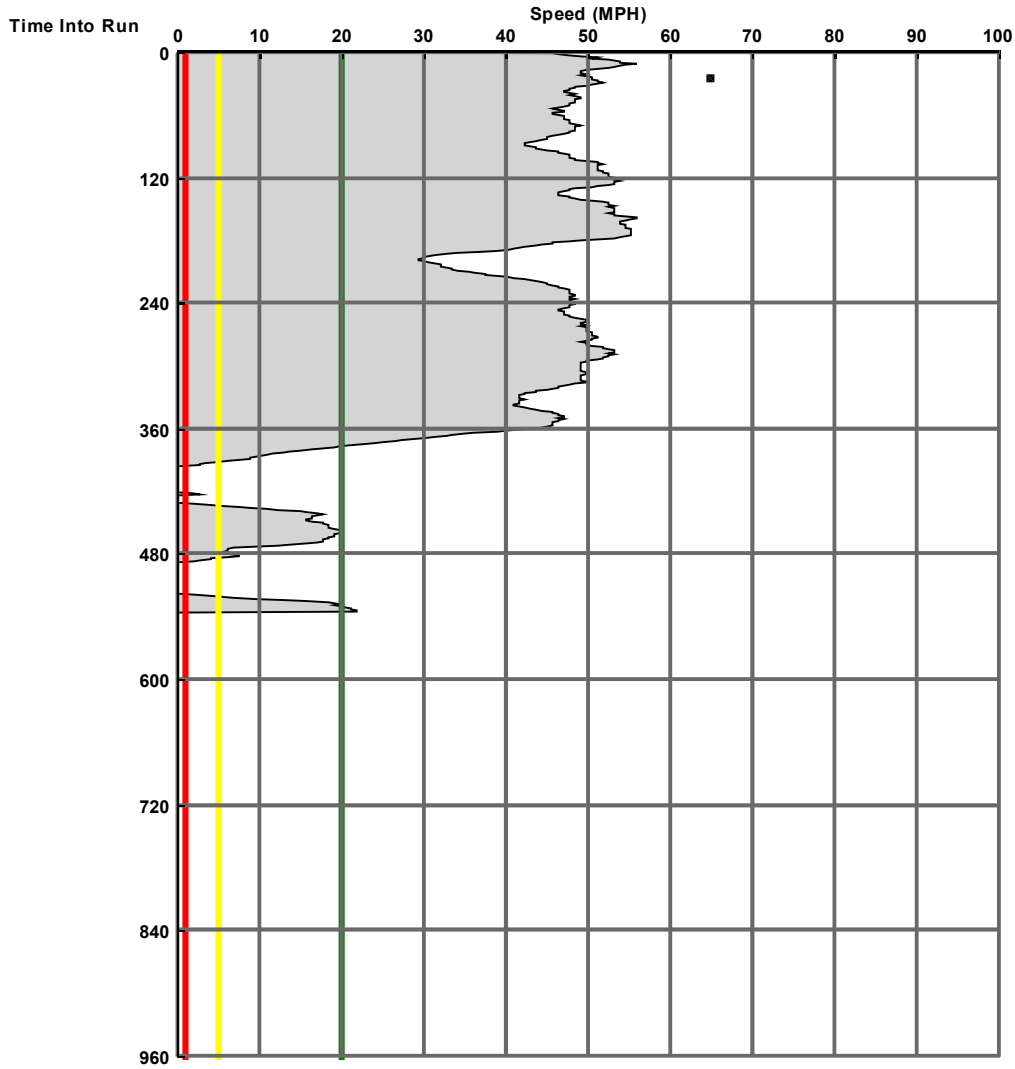
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 36

Time Based Speed Profile Run: RUN 2 EB AM 2-14-2018-R001



I-195 EASTBOUND (AM)

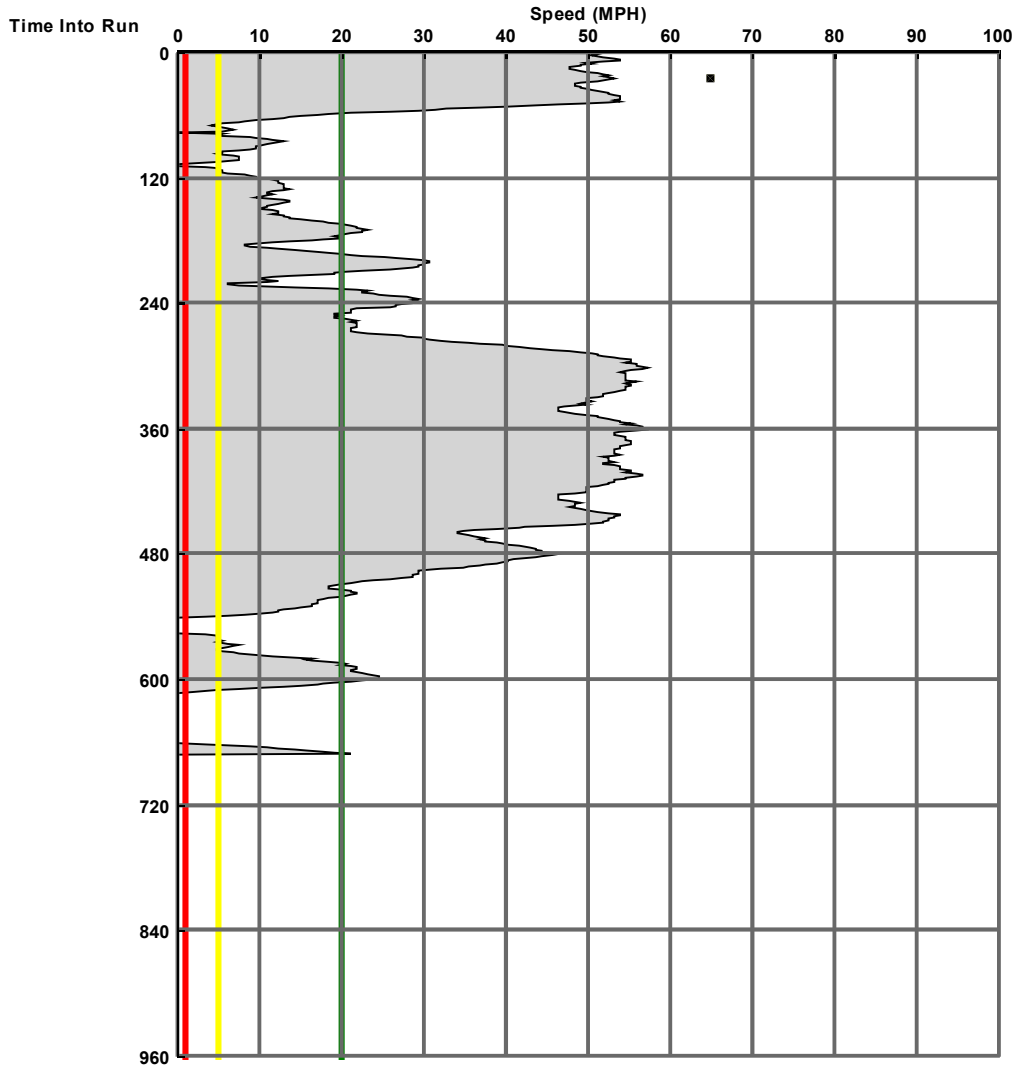
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 37

Time Based Speed Profile Run: RUN 3 EB AM 2-14-2018-R001



I-195 EASTBOUND (AM)

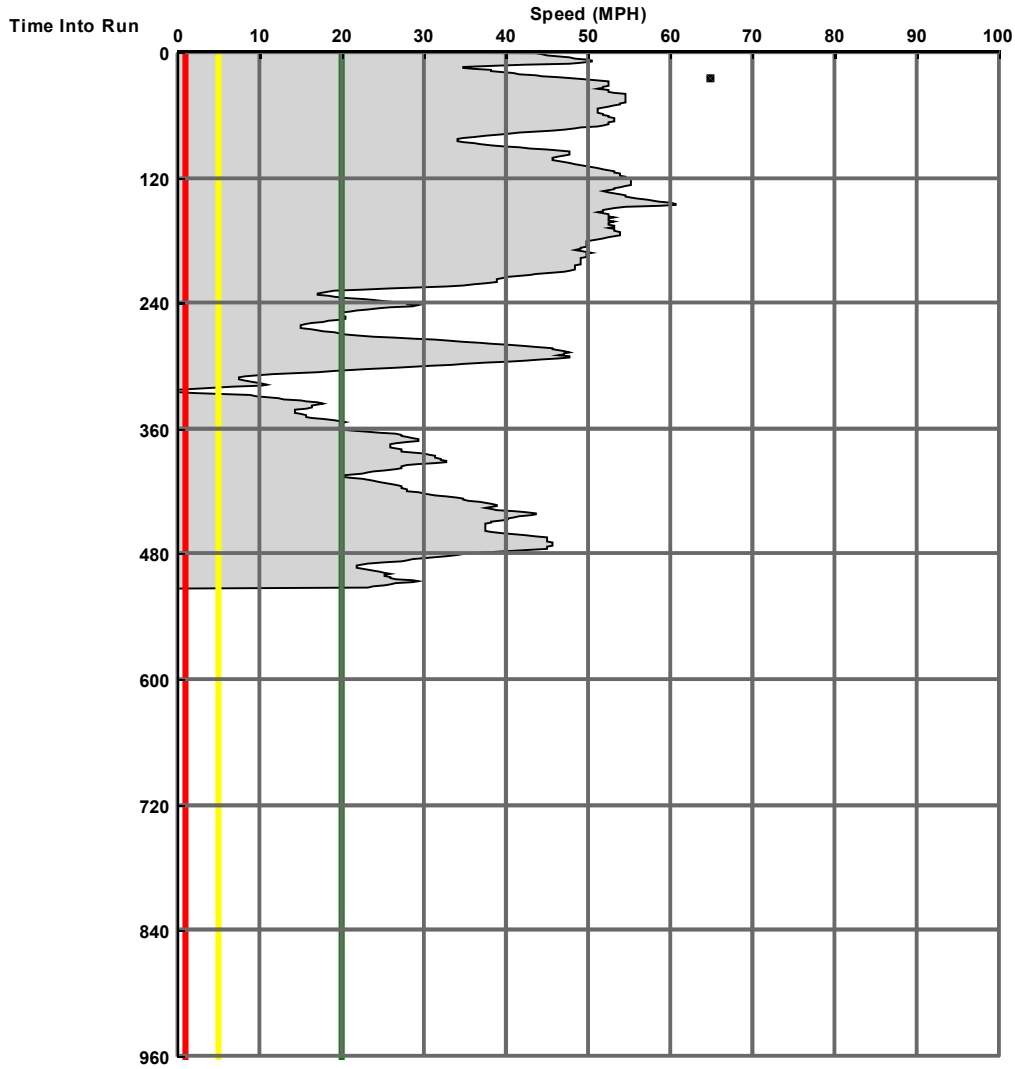
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 38

Time Based Speed Profile Run: RUN 5 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

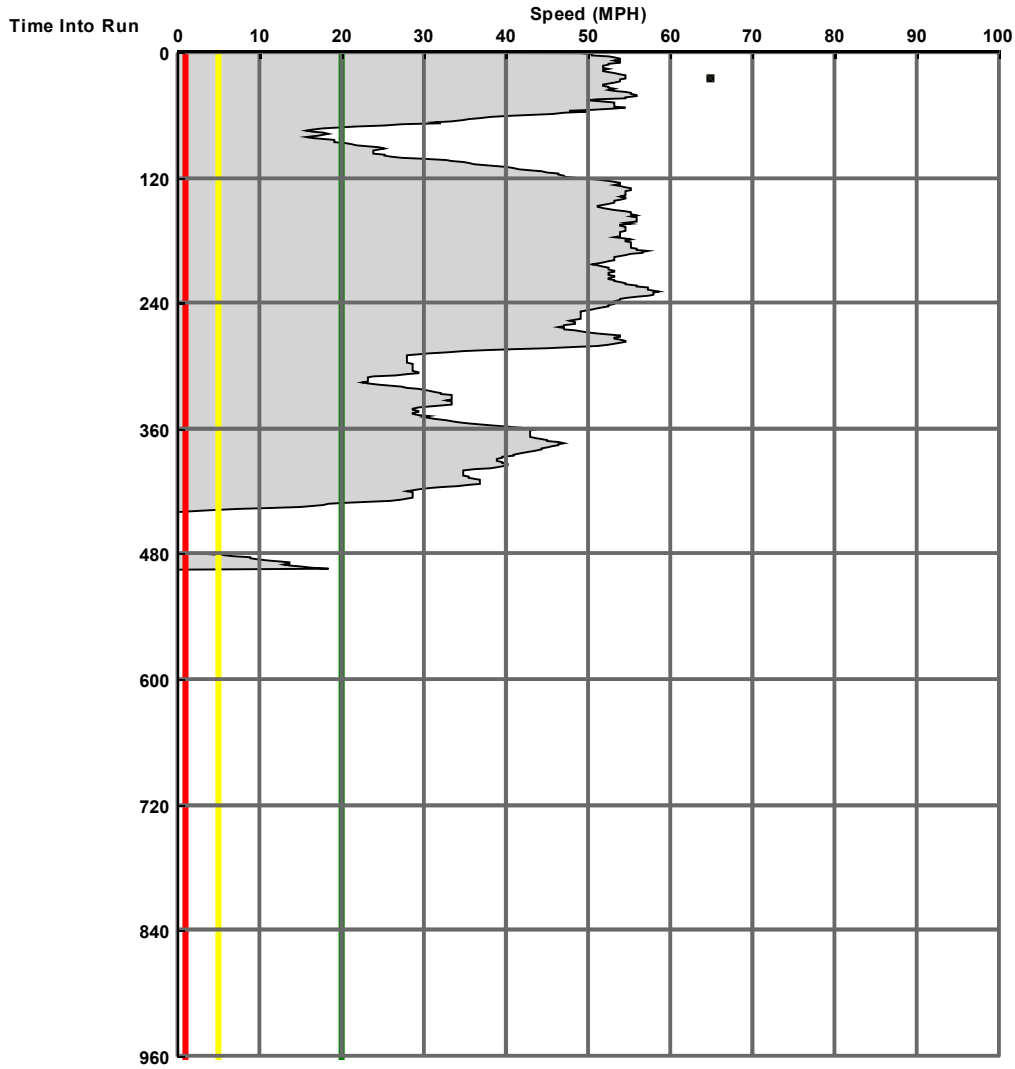
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 39

Time Based Speed Profile Run: RUN 6 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

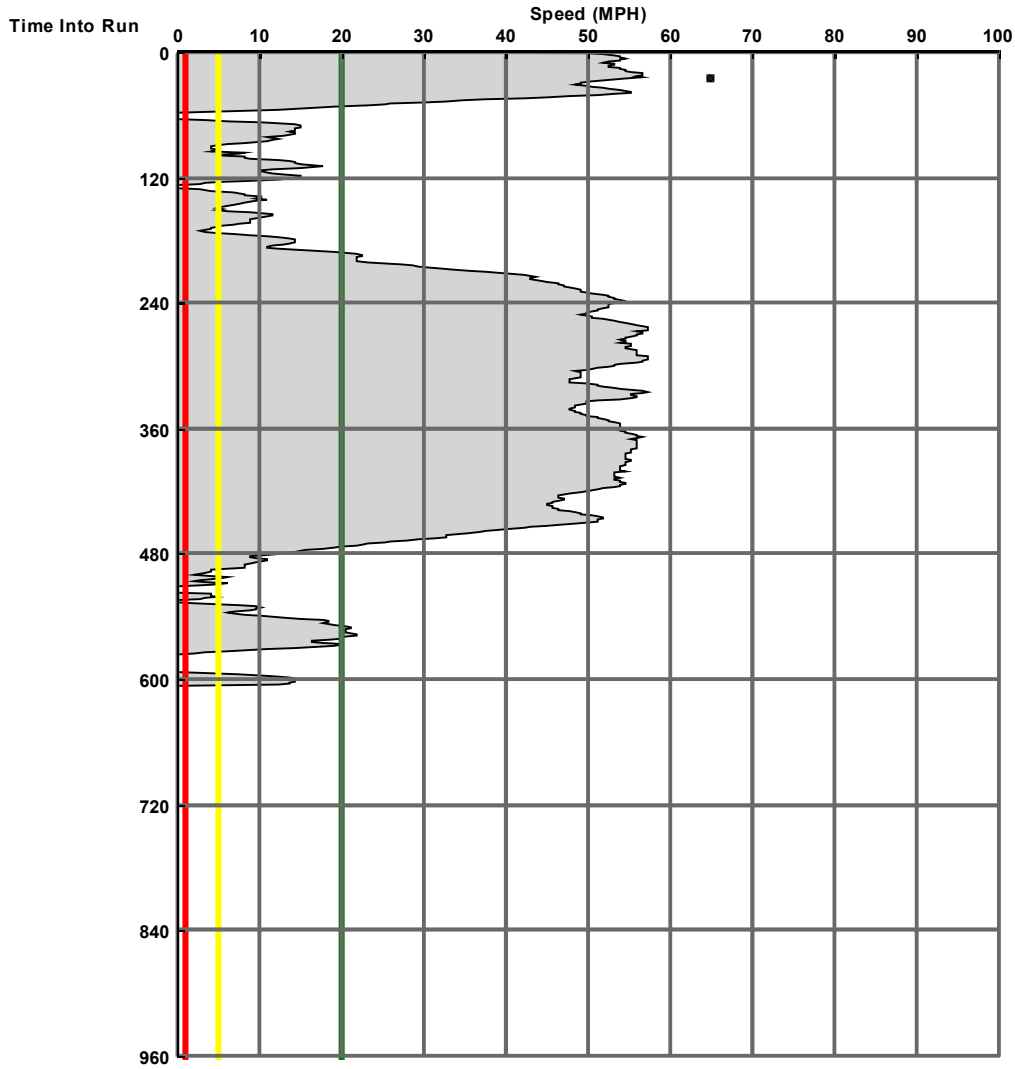
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 40

Time Based Speed Profile Run: RUN 7 EB AM 2-15-2018-R001



I-195 EASTBOUND (AM)

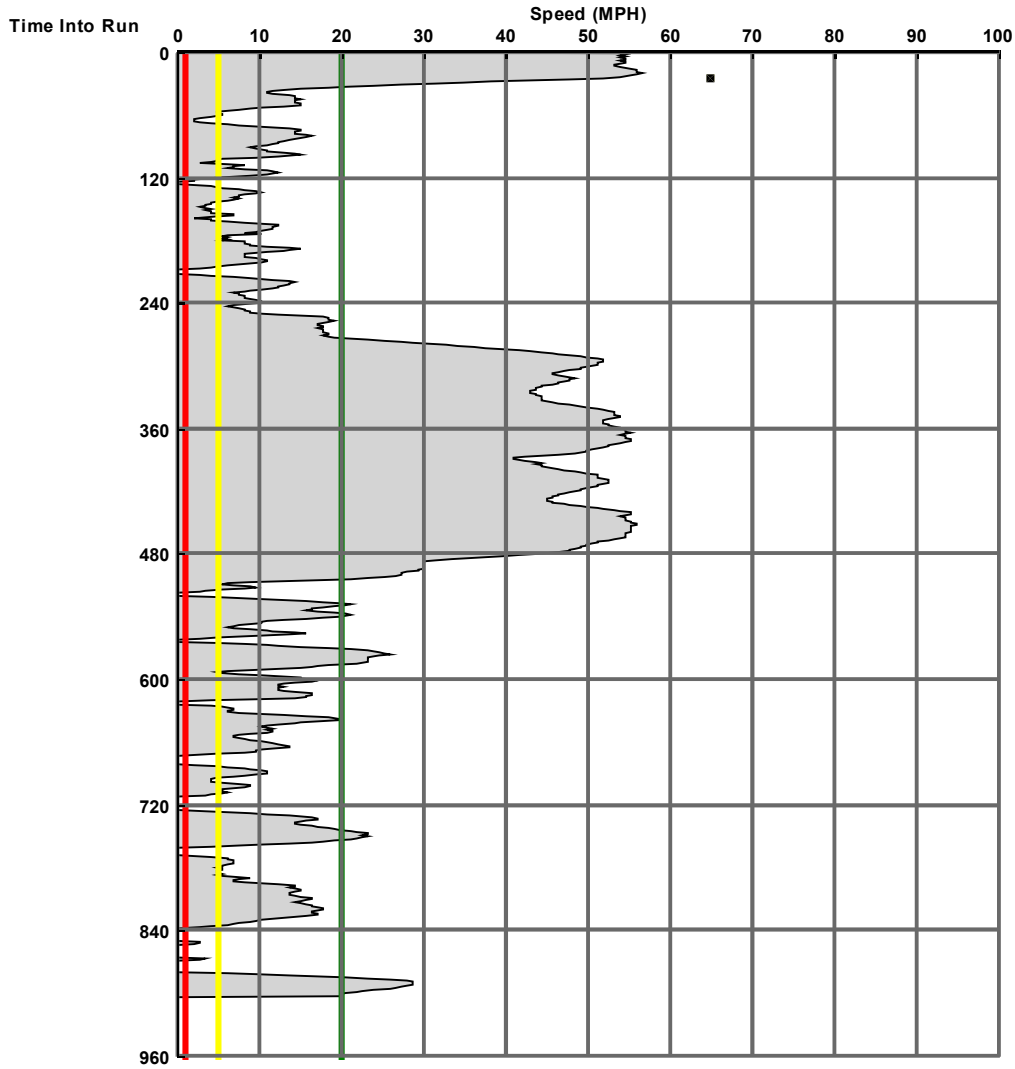
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND AM

Study Date: 3/21/2018

Page No: 41

Time Based Speed Profile Run: RUN 8 EB AM 2-15-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

Travel Time Reports for study: I-195 EASTBOUND PM

<u>Report Name</u>	<u>Page</u>
Study Summary	2
Overall Output Statistics	3
Detailed Stats By Run - Travel Time.....	4
Detailed Stats By Run - Stops	5
Detailed Stats By Run - Average Speed	6
Detailed Stats By Run - Total Delay.....	7
Detailed Stats By Run - Time Less Than0 MPH	8
Detailed Stats By Run - Time Less Than5 MPH	9
Detailed Stats By Run - Time Less Than20 MPH	10
Speed/Distance Plot of All Runs	11
Time/Space Trajectory of All Runs.....	12
Speed Profile (Distance vs Speed) for RUN 1 EB PM 2-14-2018-R001	13
Speed Profile (Distance vs Speed) for RUN 2 EB PM 2-14-2018-R001	17
Speed Profile (Distance vs Speed) for RUN 3 EB PM 2-14-2018-R001	21
Speed Profile (Distance vs Speed) for RUN 4 EB PM 2-14-2018-R001	25
Speed Profile (Distance vs Speed) for RUN 6 EB PM 2-14-2018-R001	29
Speed Profile (Distance vs Speed) for RUN 7 EB PM 2-15-2018-R001	33
Speed Profile (Time vs Speed) for RUN 1 EB PM 2-14-2018-R001	37
Speed Profile (Time vs Speed) for RUN 2 EB PM 2-14-2018-R001	38
Speed Profile (Time vs Speed) for RUN 3 EB PM 2-14-2018-R001	39
Speed Profile (Time vs Speed) for RUN 4 EB PM 2-14-2018-R001	40
Speed Profile (Time vs Speed) for RUN 6 EB PM 2-14-2018-R001	41
Speed Profile (Time vs Speed) for RUN 7 EB PM 2-15-2018-R001	42

I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 1

Study Summary Runs Used in This Study

Run Title	Start Date	Start Time	Length	Before/After	Run Type
RUN 1 EB PM 2-14-2018-R001	02/14/18	14:59:54	27481	Before	Secondary
RUN 2 EB PM 2-14-2018-R001	02/14/18	15:24:23	27754	Before	Secondary
RUN 3 EB PM 2-14-2018-R001	02/14/18	15:58:17	27773	Before	Secondary
RUN 4 EB PM 2-14-2018-R001	02/14/18	16:28:45	27689	Before	Secondary
RUN 6 EB PM 2-14-2018-R001	02/14/18	17:37:52	27769	Before	Secondary
RUN 7 EB PM 2-15-2018-R001	02/14/18	14:58:38	27231	Before	Secondary

Notes:

Node Info

#	Length	Name
1	0	NW 13th Avenue
2	6966	I-95
3	2653	NW 1st Avenue
4	1426	Biscayne Blvd
5	1343	NE 36th Street
6	2539	Bridge 870301 end
7	7561	Bridge 870302 Begin
8	2111	Physical Gore Off Ramp
9	2910	Alton RD

Length of Study Route = 27,509 feet.

I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 2

Overall Output Statistics

Node #	Length (ft)	Node Name	Travel Time	# of Stops	Avg Speed (MPH)	Total Delay	Time <= 0 MPH	Time <= 5 MPH	Time <= 20 MPH
1	0	NW 13th Avenue							
2	6966	I-95	100.7	0.0	47.2	0.0	0.0	0.0	1.2
3	2653	NW 1st Avenue	44.7	0.0	40.5	6.0	0.0	0.0	6.3
4	1426	Biscayne Blvd	19.0	0.0	51.2	0.0	0.0	0.0	0.0
5	1343	NE 36th Street	17.5	0.0	52.3	0.0	0.0	0.0	0.0
6	2539	Bridge 870301 end	33.8	0.0	51.2	0.0	0.0	0.0	0.0
7	7561	Bridge 870302 Begin	95.3	0.0	54.1	0.0	0.0	0.0	0.0
8	2111	Physical Gore Off Ramp	29.3	0.0	49.1	0.0	0.0	0.0	0.0
9	2910	Alton RD	152.7	2.7	13.0	103.7	46.3	67.0	120.5
Total	27,509		493.0	2.7	38.0	109.7	46.3	67	128

Stats based on 6 runs.

Stops based on a Stop Speed of 5 MPH.

Total Delay based on a Normal Speed of 40 MPH.

I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 3

Travel Time

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	NW 13th Avenue						
2	6966	I-95	113	92	109	100	98	92
3	2653	NW 1st Avenue	79	34	37	35	48	35
4	1426	Biscayne Blvd	20	18	18	20	20	18
5	1343	NE 36th Street	17	16	17	21	17	17
6	2539	Bridge 870301 end	35	33	34	34	34	33
7	7561	Bridge 870302 Begin	95	96	93	93	101	94
8	2111	Physical Gore Off Ramp	29	29	29	27	32	30
9	2910	Alton RD	160	270	60	84	67	275
Total	27,509		548	588	397	414	417	594

Run # 1 = RUN 1 EB PM 2-14-2018-R001

Run # 2 = RUN 2 EB PM 2-14-2018-R001

Run # 3 = RUN 3 EB PM 2-14-2018-R001

Run # 4 = RUN 4 EB PM 2-14-2018-R001

Run # 5 = RUN 6 EB PM 2-14-2018-R001

Run # 6 = RUN 7 EB PM 2-15-2018-R001

I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 4

Number of Stops

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	NW 13th Avenue						
2	6966	I-95	0	0	0	0	0	0
3	2653	NW 1st Avenue	0	0	0	0	0	0
4	1426	Biscayne Blvd	0	0	0	0	0	0
5	1343	NE 36th Street	0	0	0	0	0	0
6	2539	Bridge 870301 end	0	0	0	0	0	0
7	7561	Bridge 870302 Begin	0	0	0	0	0	0
8	2111	Physical Gore Off Ramp	0	0	0	0	0	0
9	2910	Alton RD	2	4	1	1	1	7
Total	27,509		2	4	1	1	1	7

Stops based on a Stop Speed of 5 MPH.

Run # 1 = RUN 1 EB PM 2-14-2018-R001

Run # 2 = RUN 2 EB PM 2-14-2018-R001

Run # 3 = RUN 3 EB PM 2-14-2018-R001

Run # 4 = RUN 4 EB PM 2-14-2018-R001

Run # 5 = RUN 6 EB PM 2-14-2018-R001

Run # 6 = RUN 7 EB PM 2-15-2018-R001

I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 5

Average Speed (MPH)

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	NW 13th Avenue	0.0	0.0	0.0	0.0	0.0	0.0
2	6966	I-95	42.1	51.9	43.8	47.6	48.9	51.8
3	2653	NW 1st Avenue	22.7	53.5	49.2	51.3	37.5	52.1
4	1426	Biscayne Blvd	48.8	54.4	53.8	50.1	48.9	54.0
5	1343	NE 36th Street	54.9	54.4	53.6	43.5	52.9	54.1
6	2539	Bridge 870301 end	50.3	52.4	50.5	51.1	51.3	52.6
7	7561	Bridge 870302 Begin	54.2	53.7	55.4	55.3	50.9	54.4
8	2111	Physical Gore Off Ramp	48.8	49.9	51.2	53.3	44.0	49.0
9	2910	Alton RD	12.1	7.3	32.4	23.5	29.7	6.4
Total	27,509		34.2	31.9	47.3	45.3	45.0	31.3

Run # 1 = RUN 1 EB PM 2-14-2018-R001

Run # 2 = RUN 2 EB PM 2-14-2018-R001

Run # 3 = RUN 3 EB PM 2-14-2018-R001

Run # 4 = RUN 4 EB PM 2-14-2018-R001

Run # 5 = RUN 6 EB PM 2-14-2018-R001

Run # 6 = RUN 7 EB PM 2-15-2018-R001

I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 6

Total Delay

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	NW 13th Avenue	0	0	0	0	0	0
2	6966	I-95	0	0	0	0	0	0
3	2653	NW 1st Avenue	34	0	0	0	2	0
4	1426	Biscayne Blvd	0	0	0	0	0	0
5	1343	NE 36th Street	0	0	0	0	0	0
6	2539	Bridge 870301 end	0	0	0	0	0	0
7	7561	Bridge 870302 Begin	0	0	0	0	0	0
8	2111	Physical Gore Off Ramp	0	0	0	0	0	0
9	2910	Alton RD	111	220	10	34	17	230
Total	27,509		145	220	10	34	19	230

Total Delay based on a Normal Speed of 40 MPH.

Run # 1 = RUN 1 EB PM 2-14-2018-R001

Run # 2 = RUN 2 EB PM 2-14-2018-R001

Run # 3 = RUN 3 EB PM 2-14-2018-R001

Run # 4 = RUN 4 EB PM 2-14-2018-R001

Run # 5 = RUN 6 EB PM 2-14-2018-R001

Run # 6 = RUN 7 EB PM 2-15-2018-R001

I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 7

Time Below

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	NW 13th Avenue	0	0	0	0	0	0
2	6966	I-95	0	0	0	0	0	0
3	2653	NW 1st Avenue	0	0	0	0	0	0
4	1426	Biscayne Blvd	0	0	0	0	0	0
5	1343	NE 36th Street	0	0	0	0	0	0
6	2539	Bridge 870301 end	0	0	0	0	0	0
7	7561	Bridge 870302 Begin	0	0	0	0	0	0
8	2111	Physical Gore Off Ramp	0	0	0	0	0	0
9	2910	Alton RD	36	111	1	20	0	110
Total	27,509		36	111	1	20	0	110

Run # 1 = RUN 1 EB PM 2-14-2018-R001

Run # 2 = RUN 2 EB PM 2-14-2018-R001

Run # 3 = RUN 3 EB PM 2-14-2018-R001

Run # 4 = RUN 4 EB PM 2-14-2018-R001

Run # 5 = RUN 6 EB PM 2-14-2018-R001

Run # 6 = RUN 7 EB PM 2-15-2018-R001

I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 8

Time Below

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	NW 13th Avenue	0	0	0	0	0	0
2	6966	I-95	0	0	0	0	0	0
3	2653	NW 1st Avenue	0	0	0	0	0	0
4	1426	Biscayne Blvd	0	0	0	0	0	0
5	1343	NE 36th Street	0	0	0	0	0	0
6	2539	Bridge 870301 end	0	0	0	0	0	0
7	7561	Bridge 870302 Begin	0	0	0	0	0	0
8	2111	Physical Gore Off Ramp	0	0	0	0	0	0
9	2910	Alton RD	56	182	4	22	1	137
Total	27,509		56	182	4	22	1	137

Run # 1 = RUN 1 EB PM 2-14-2018-R001

Run # 2 = RUN 2 EB PM 2-14-2018-R001

Run # 3 = RUN 3 EB PM 2-14-2018-R001

Run # 4 = RUN 4 EB PM 2-14-2018-R001

Run # 5 = RUN 6 EB PM 2-14-2018-R001

Run # 6 = RUN 7 EB PM 2-15-2018-R001

I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 9

Time Below

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	NW 13th Avenue	0	0	0	0	0	0
2	6966	I-95	7	0	0	0	0	0
3	2653	NW 1st Avenue	38	0	0	0	0	0
4	1426	Biscayne Blvd	0	0	0	0	0	0
5	1343	NE 36th Street	0	0	0	0	0	0
6	2539	Bridge 870301 end	0	0	0	0	0	0
7	7561	Bridge 870302 Begin	0	0	0	0	0	0
8	2111	Physical Gore Off Ramp	0	0	0	0	0	0
9	2910	Alton RD	131	248	19	42	21	262
Total	27,509		176	248	19	42	21	262

Run # 1 = RUN 1 EB PM 2-14-2018-R001

Run # 2 = RUN 2 EB PM 2-14-2018-R001

Run # 3 = RUN 3 EB PM 2-14-2018-R001

Run # 4 = RUN 4 EB PM 2-14-2018-R001

Run # 5 = RUN 6 EB PM 2-14-2018-R001

Run # 6 = RUN 7 EB PM 2-15-2018-R001

I-195 EASTBOUND (PM)

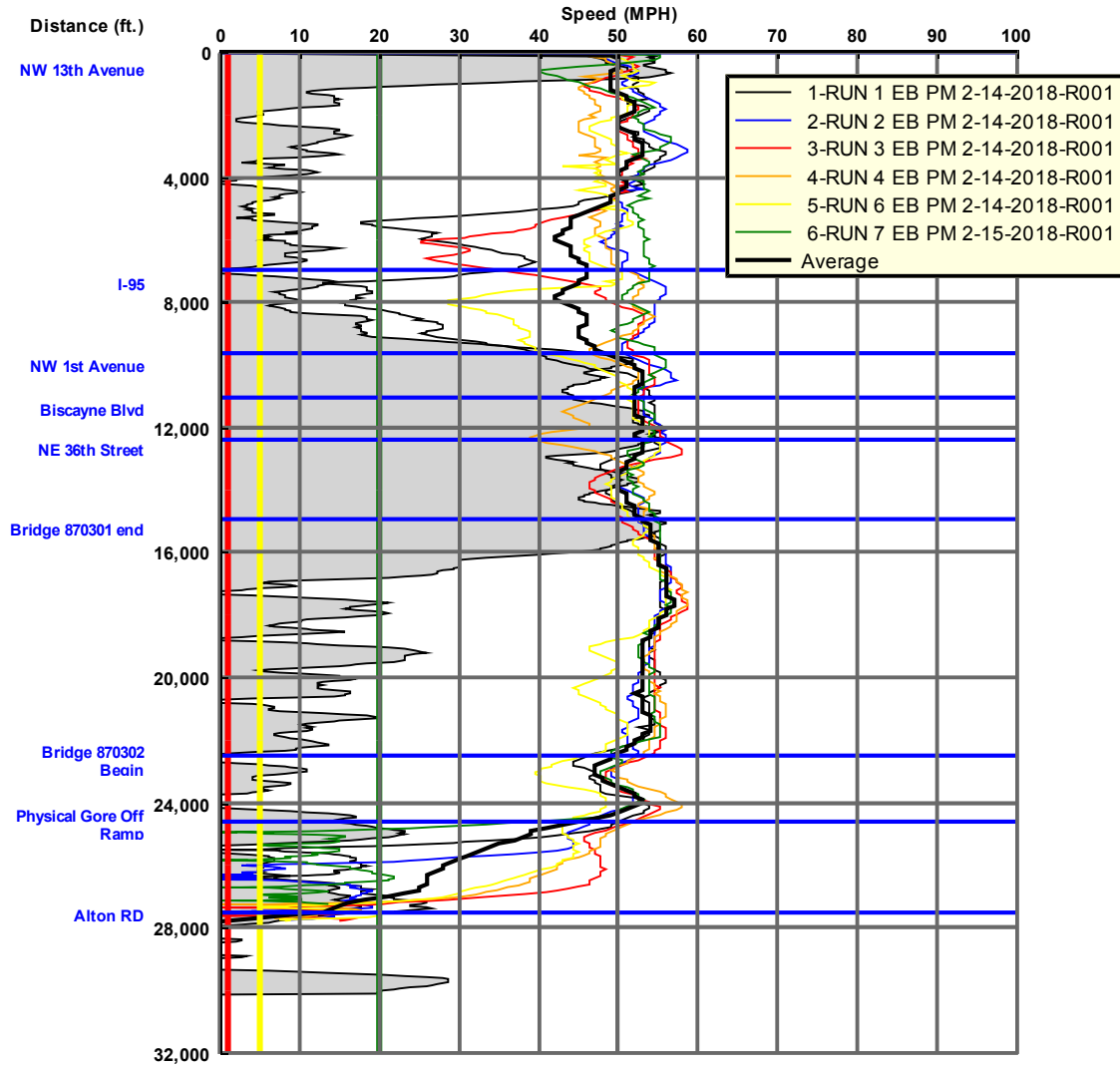
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 10

Speed/Distance Profiles of All Runs



I-195 EASTBOUND (PM)

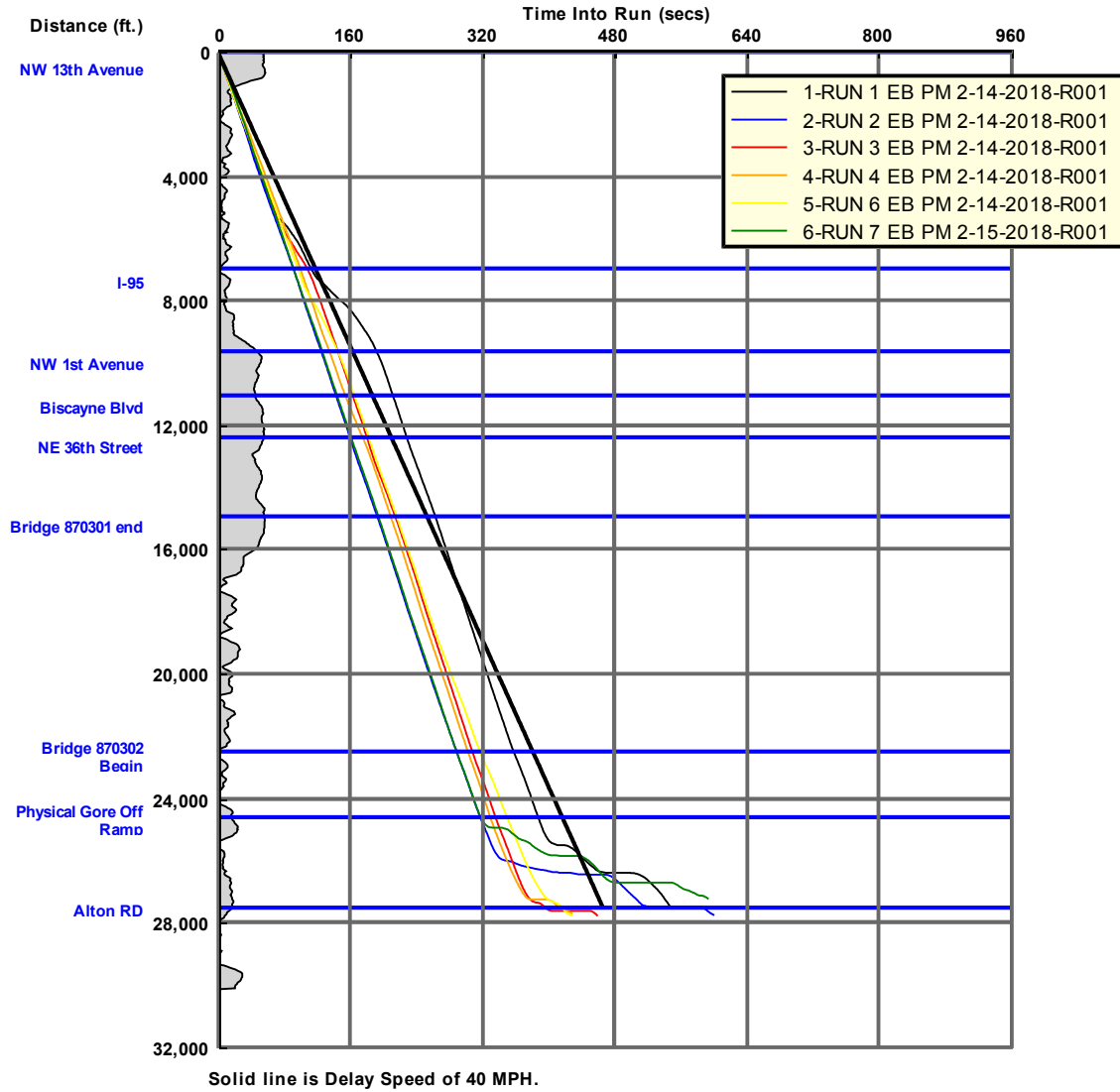
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 11

Space/Time Trajectory of All Runs



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

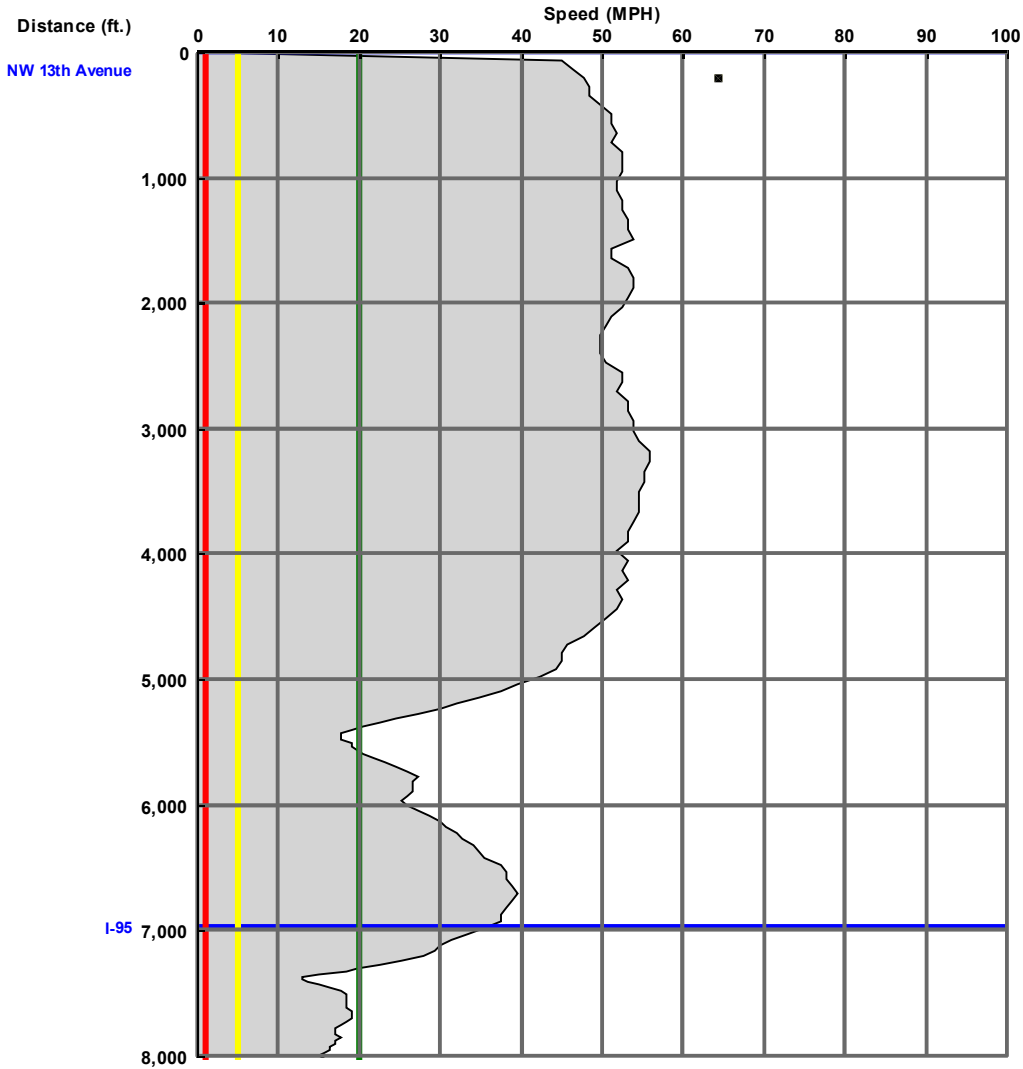
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 12

Speed Profile

Run: RUN 1 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

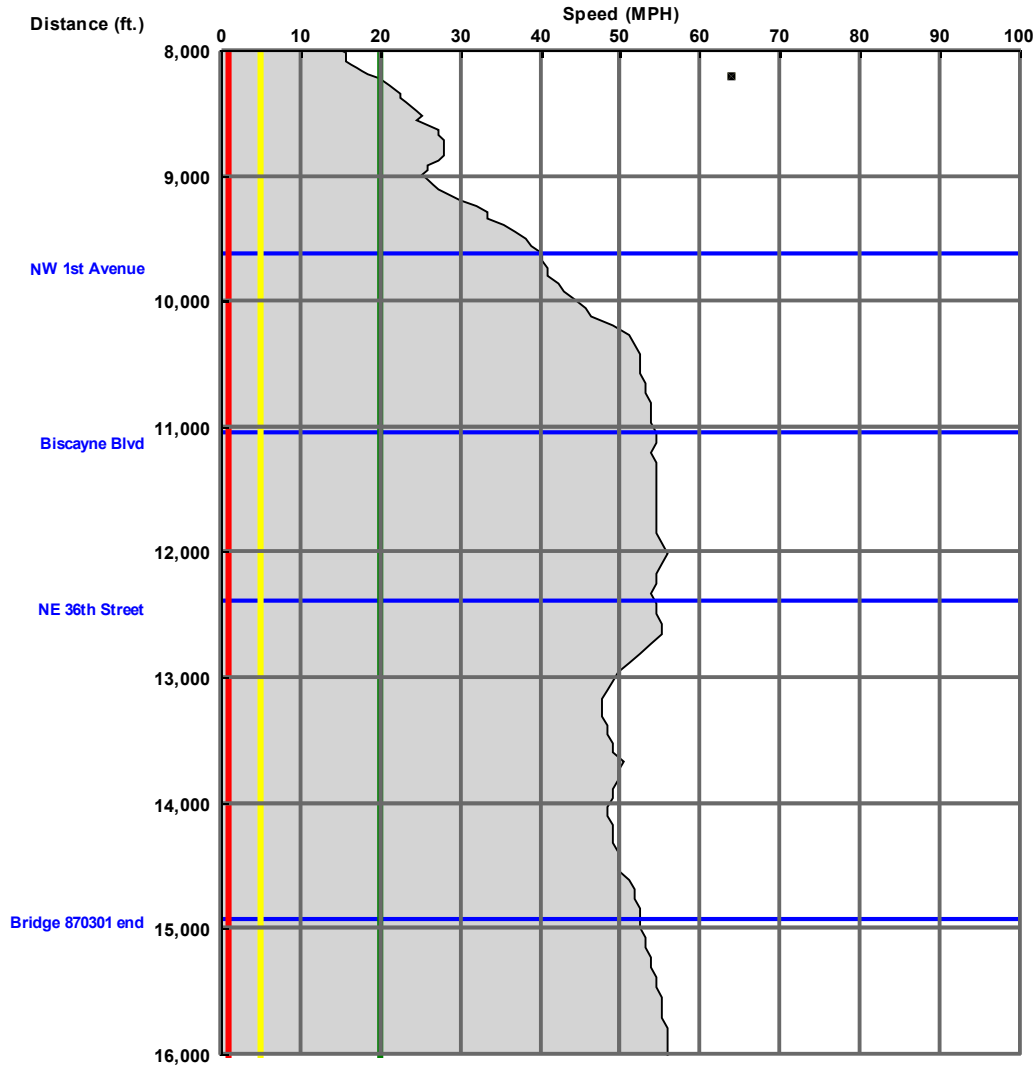
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 13

Speed Profile

Run: RUN 1 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

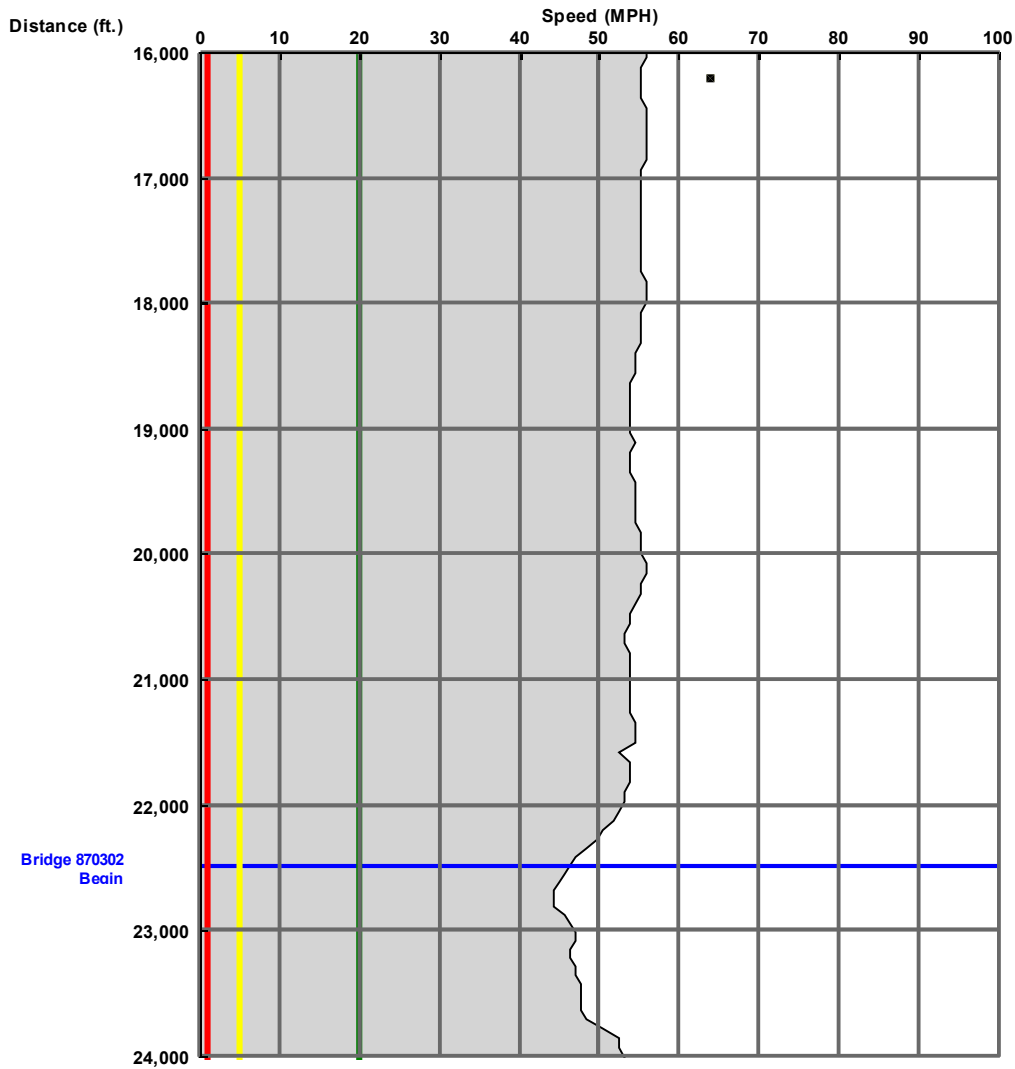
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 14

Speed Profile

Run: RUN 1 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

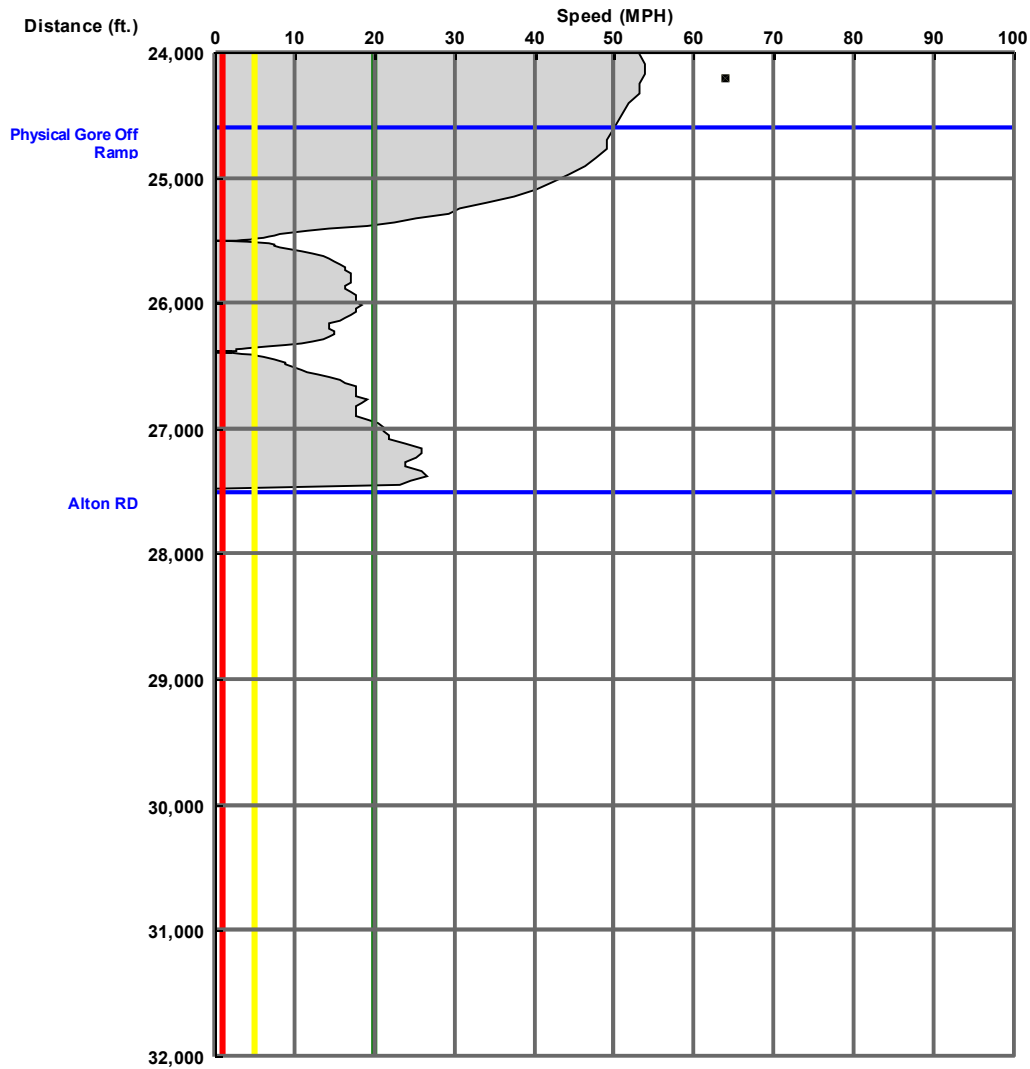
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 15

Speed Profile

Run: RUN 1 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

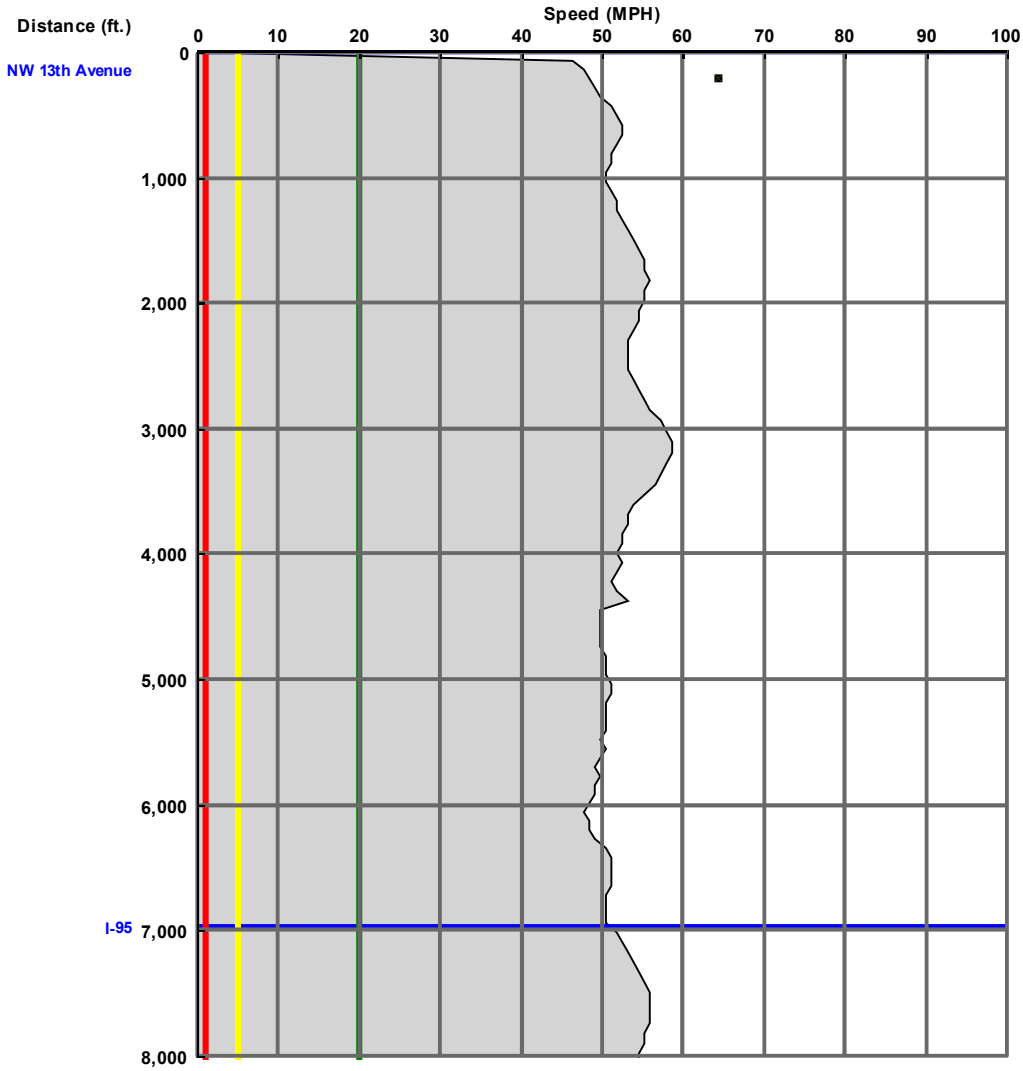
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 16

Speed Profile

Run: RUN 2 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

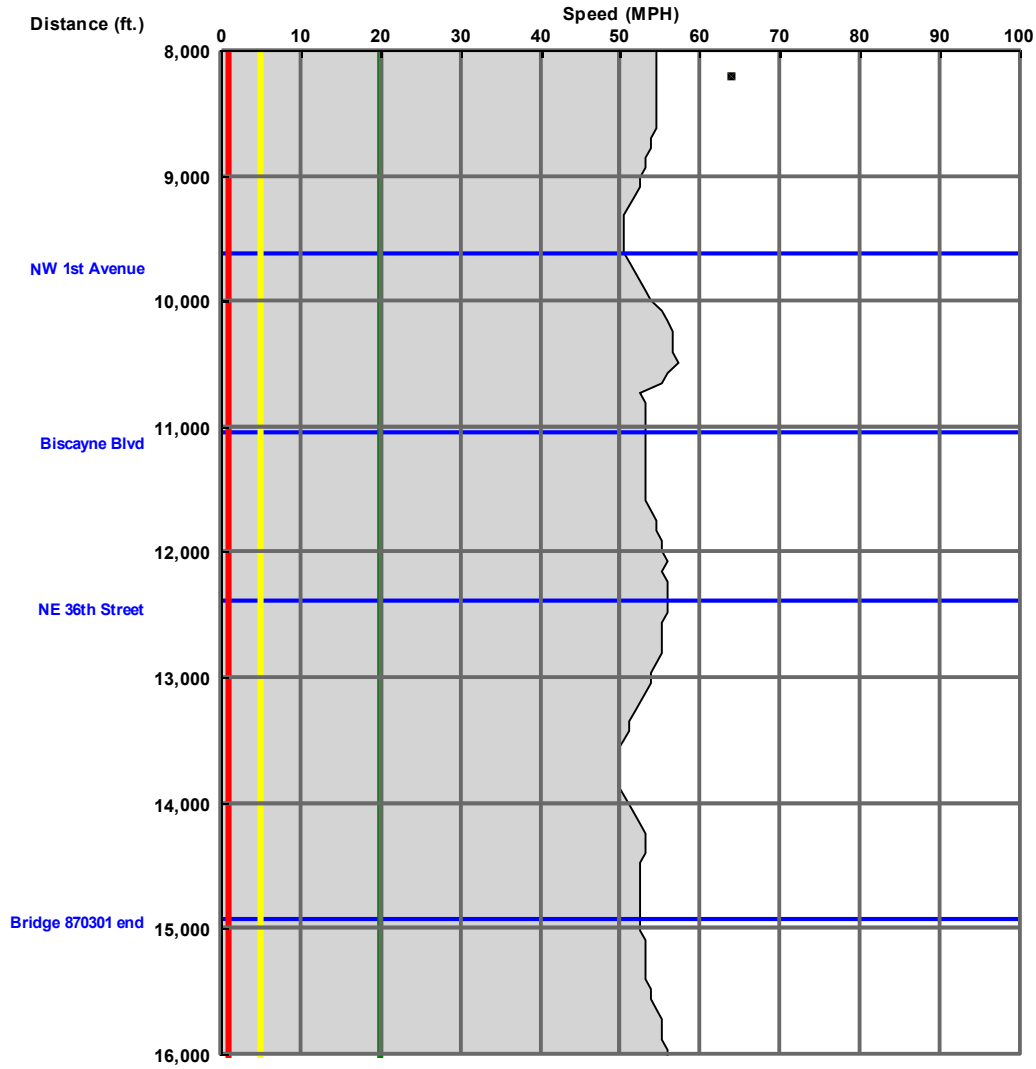
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 17

Speed Profile

Run: RUN 2 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

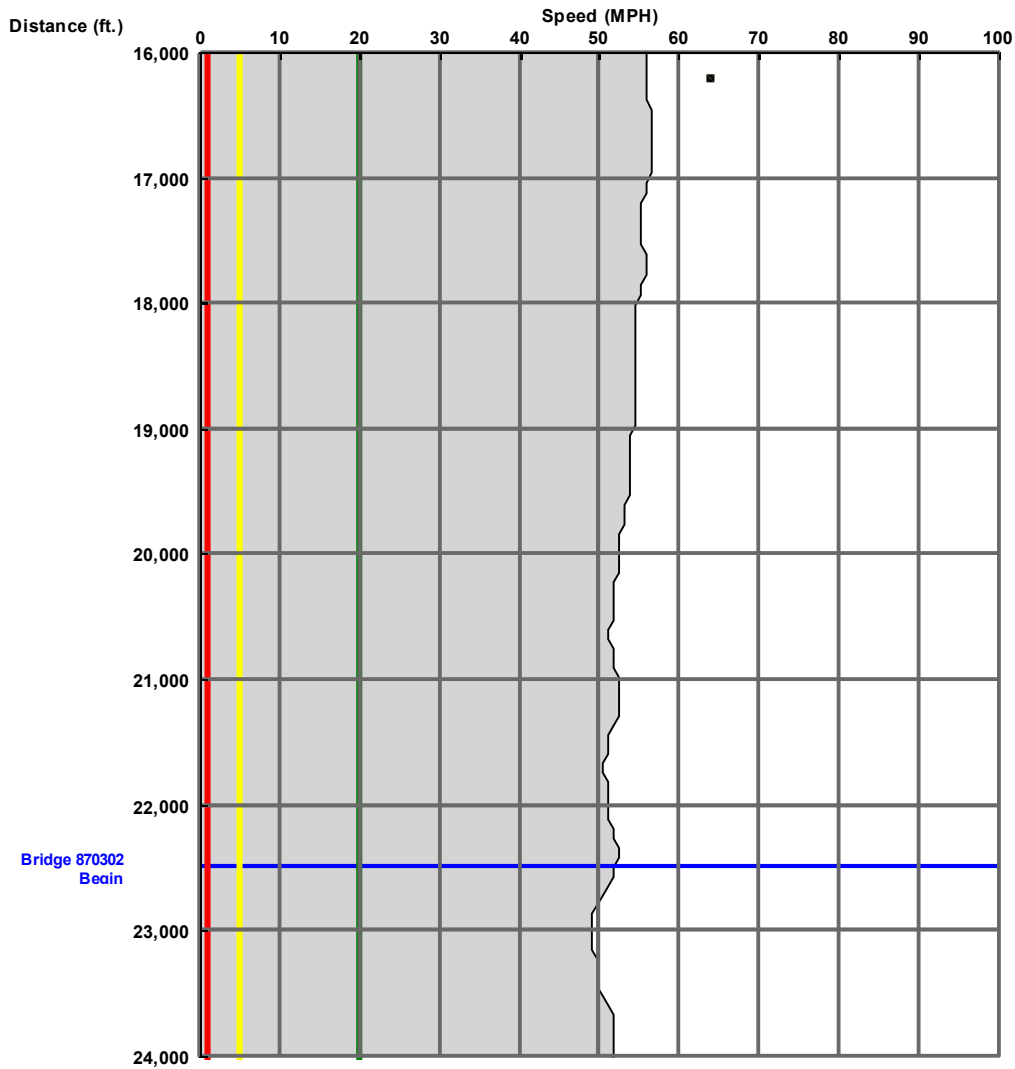
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 18

Speed Profile

Run: RUN 2 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

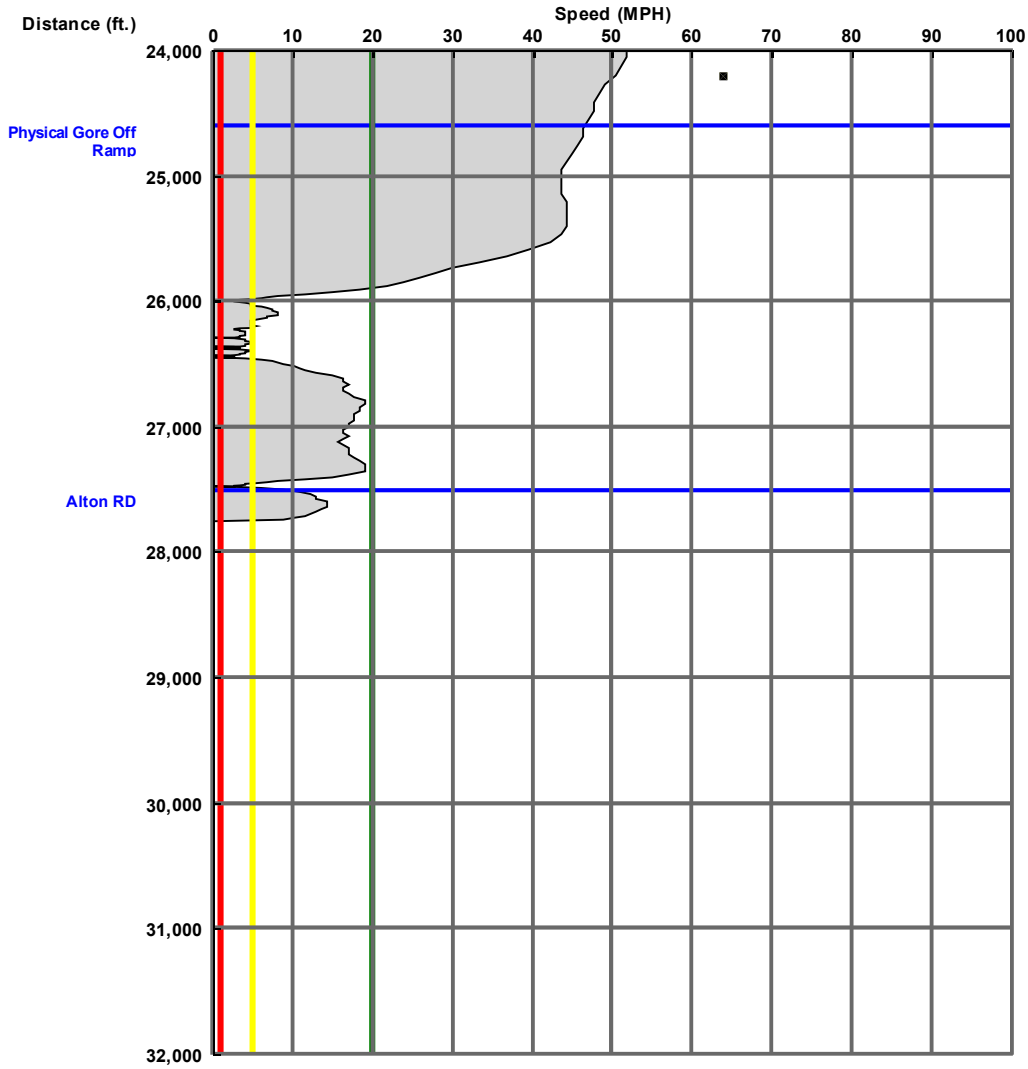
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 19

Speed Profile

Run: RUN 2 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

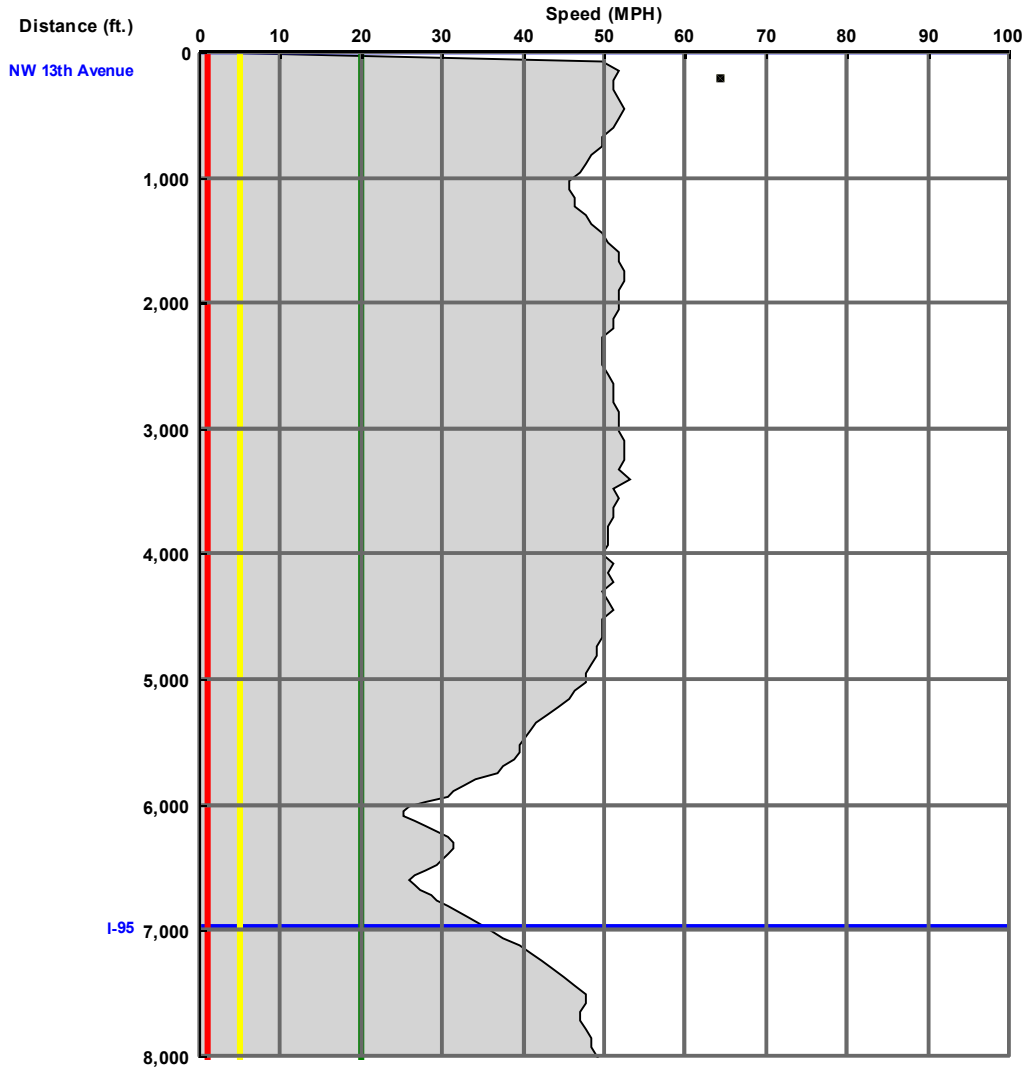
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 20

Speed Profile

Run: RUN 3 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

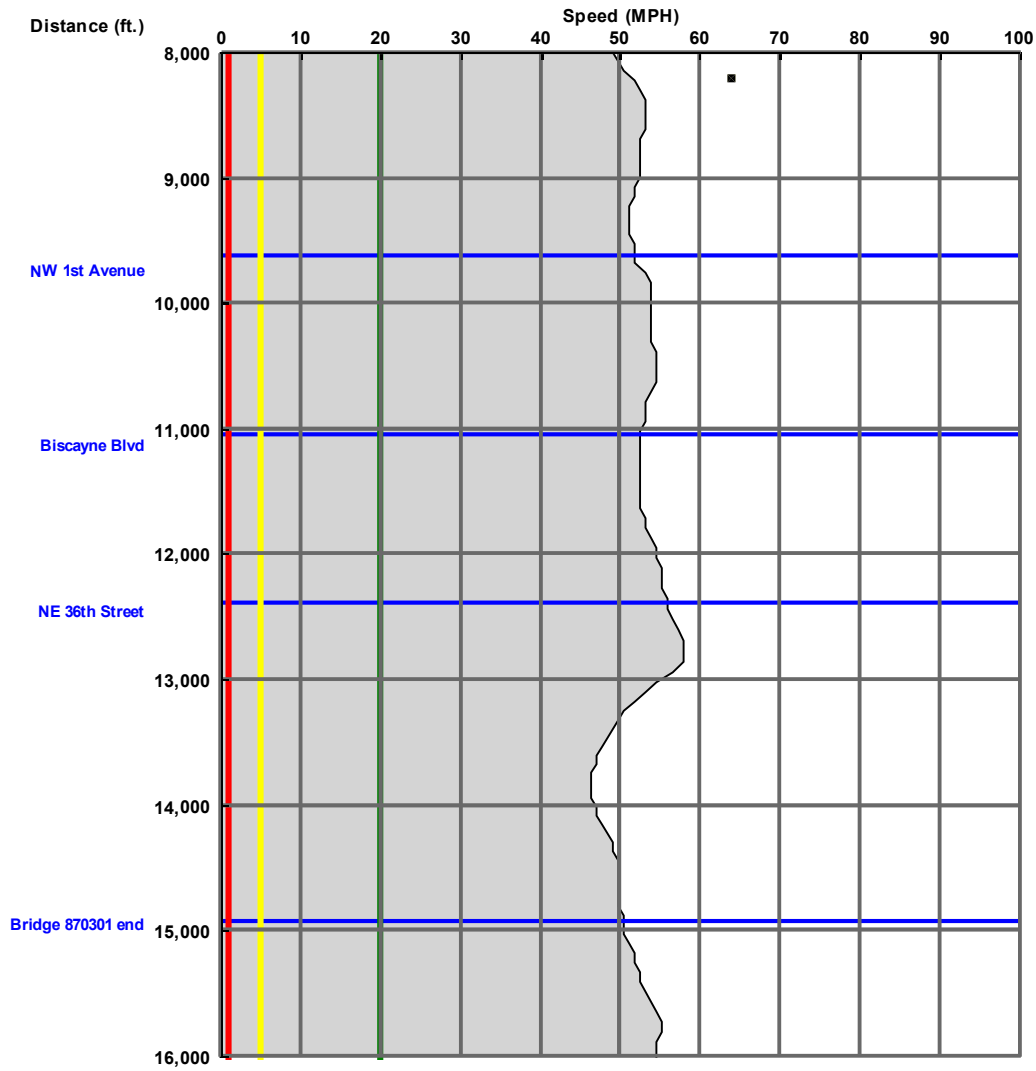
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 21

Speed Profile

Run: RUN 3 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

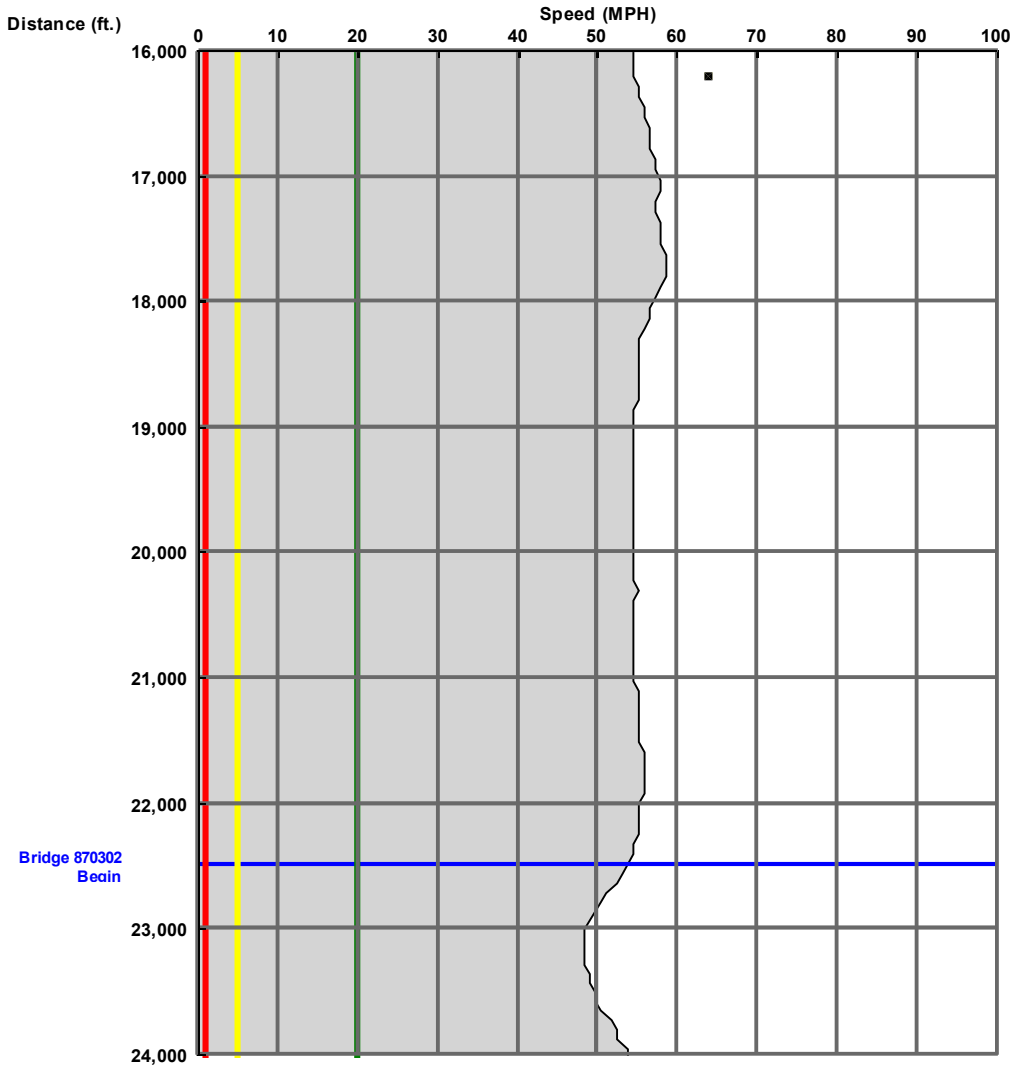
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 22

Speed Profile

Run: RUN 3 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

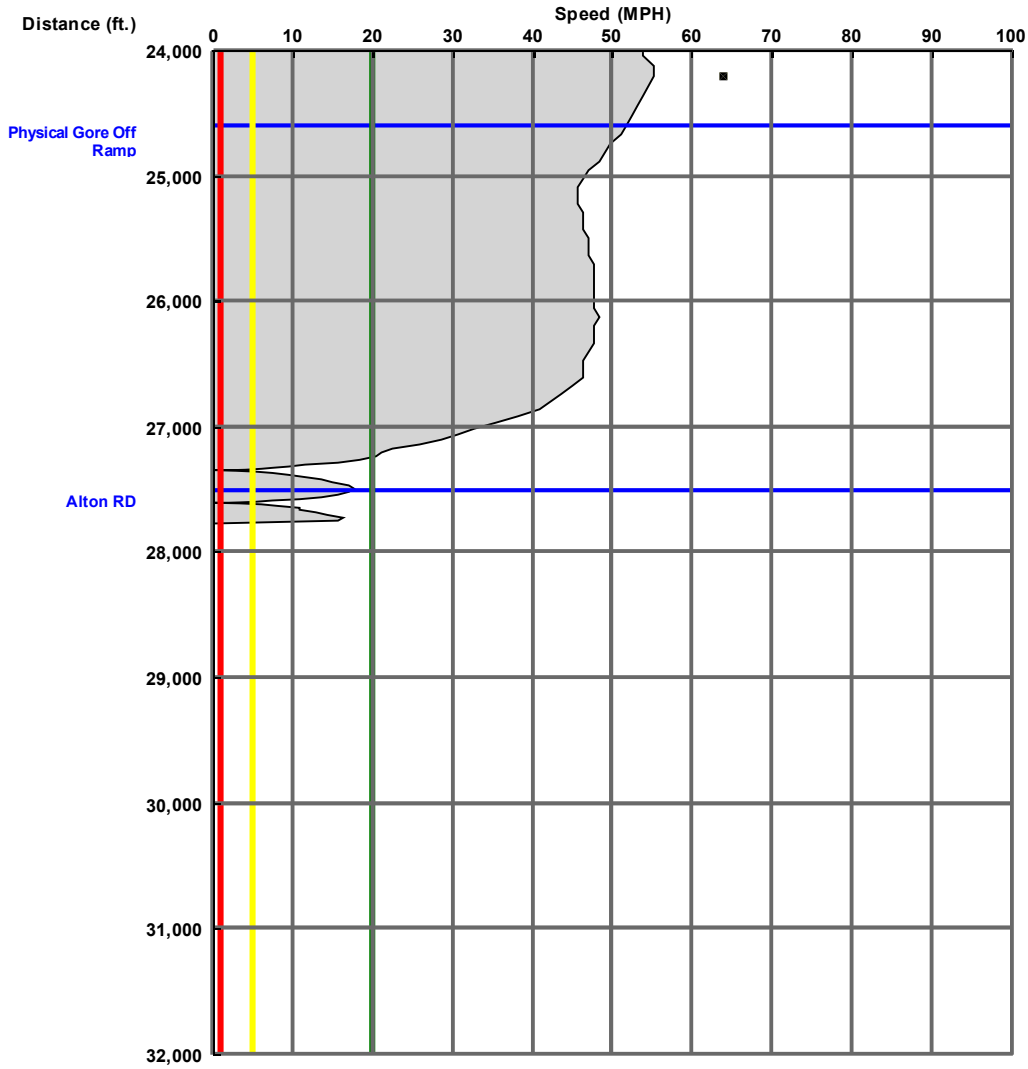
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 23

Speed Profile

Run: RUN 3 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

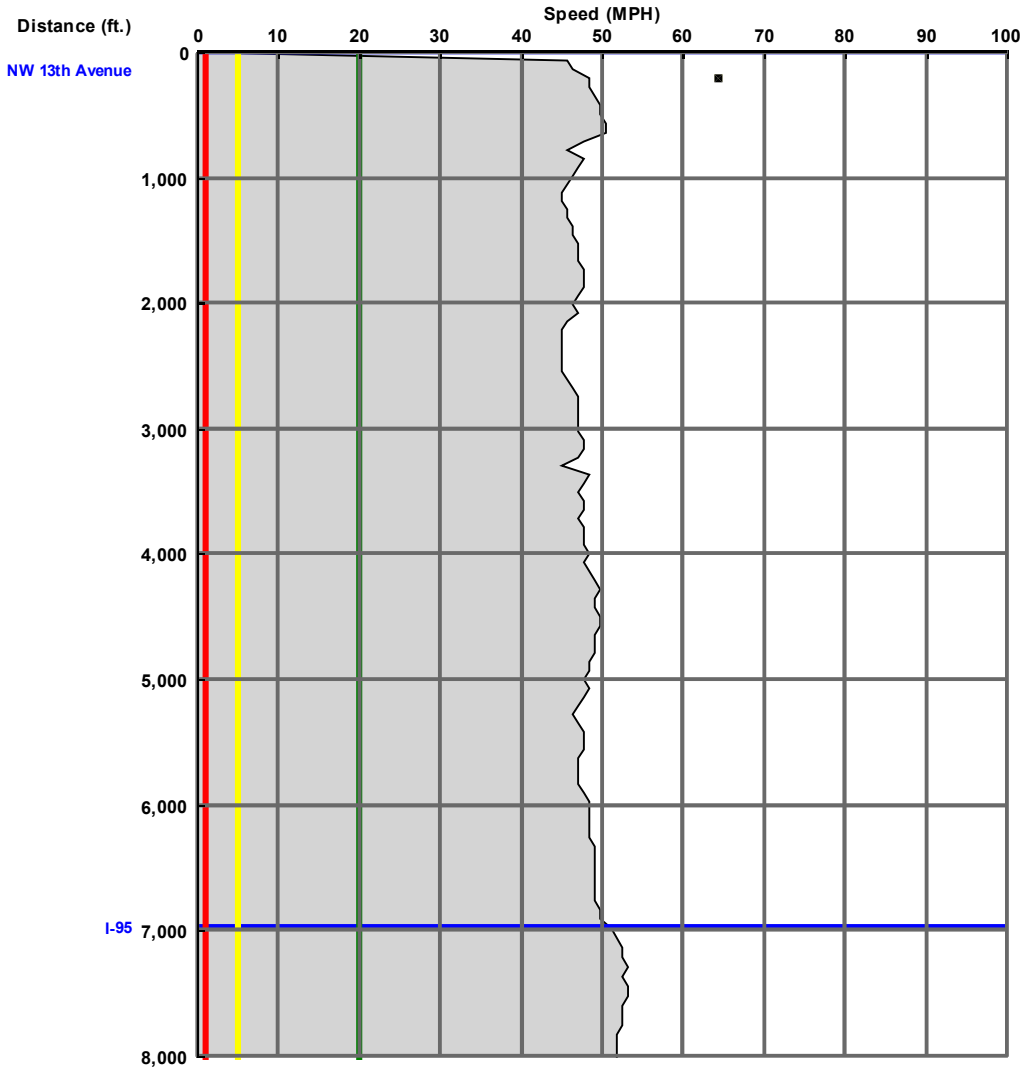
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 24

Speed Profile

Run: RUN 4 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

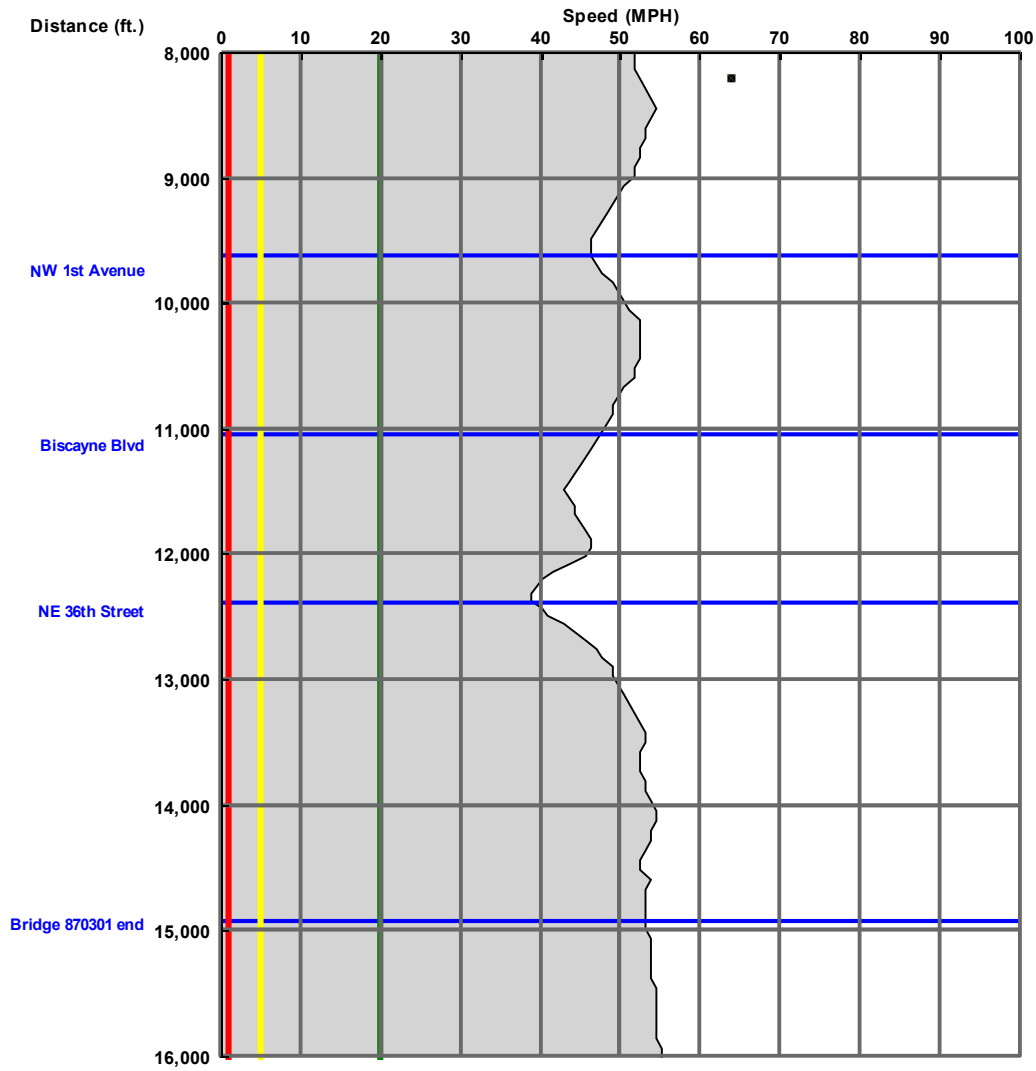
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 25

Speed Profile

Run: RUN 4 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

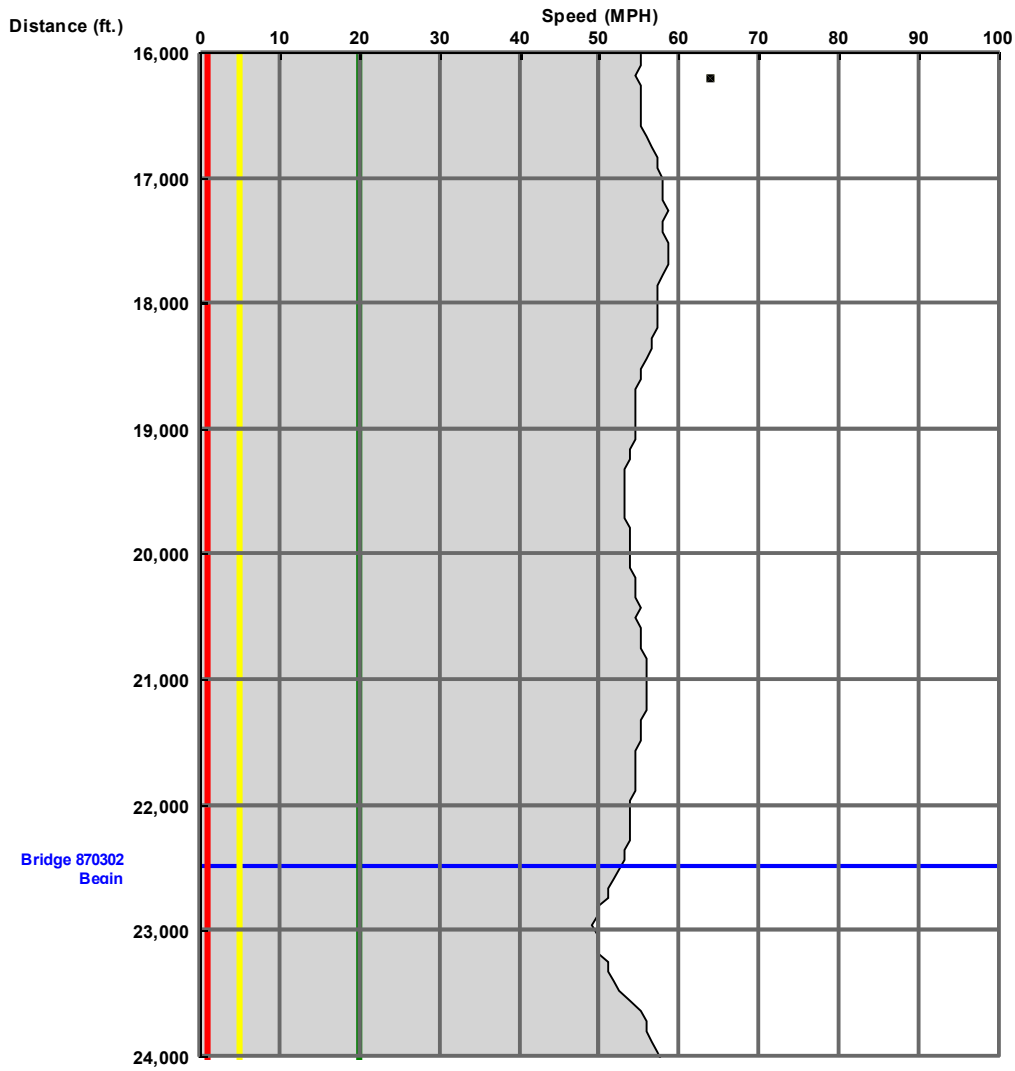
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 26

Speed Profile

Run: RUN 4 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

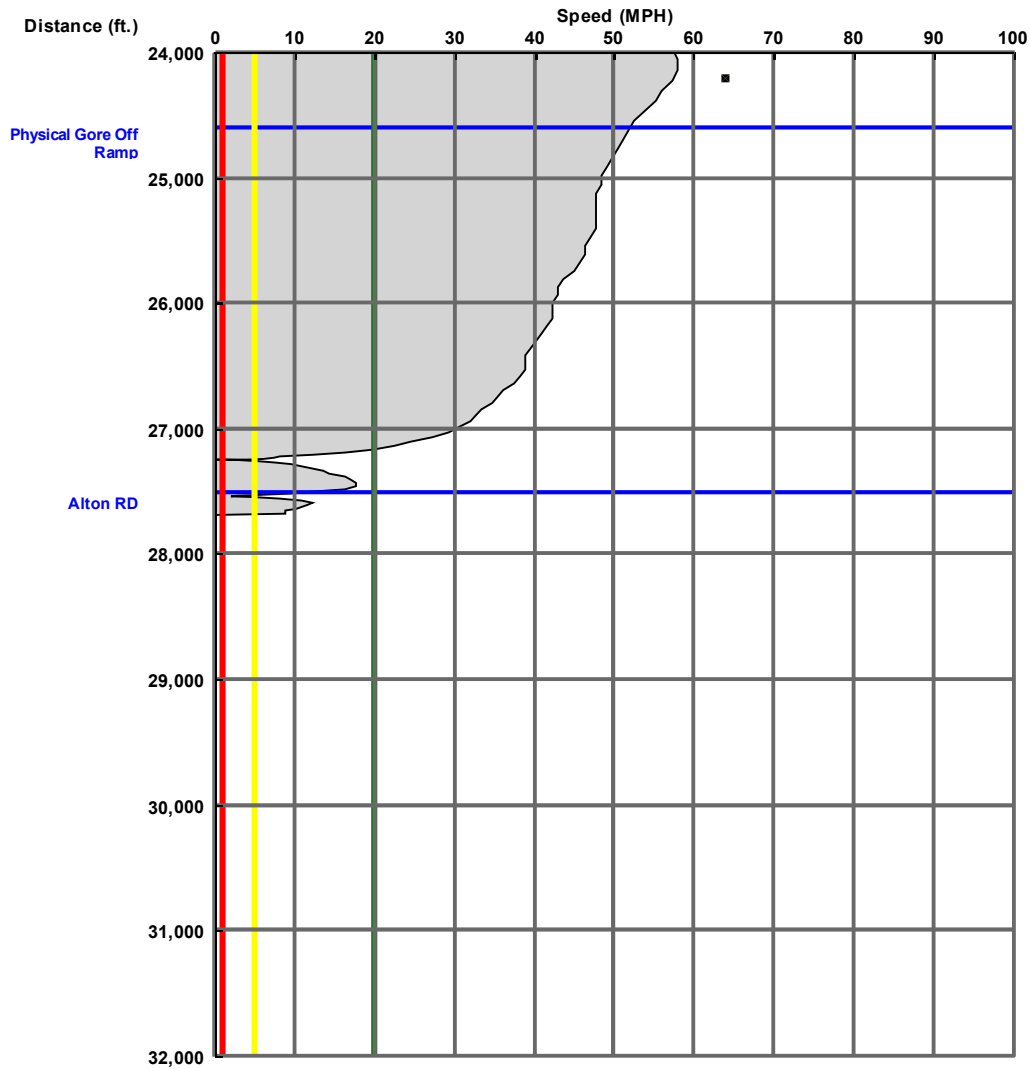
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 27

Speed Profile

Run: RUN 4 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

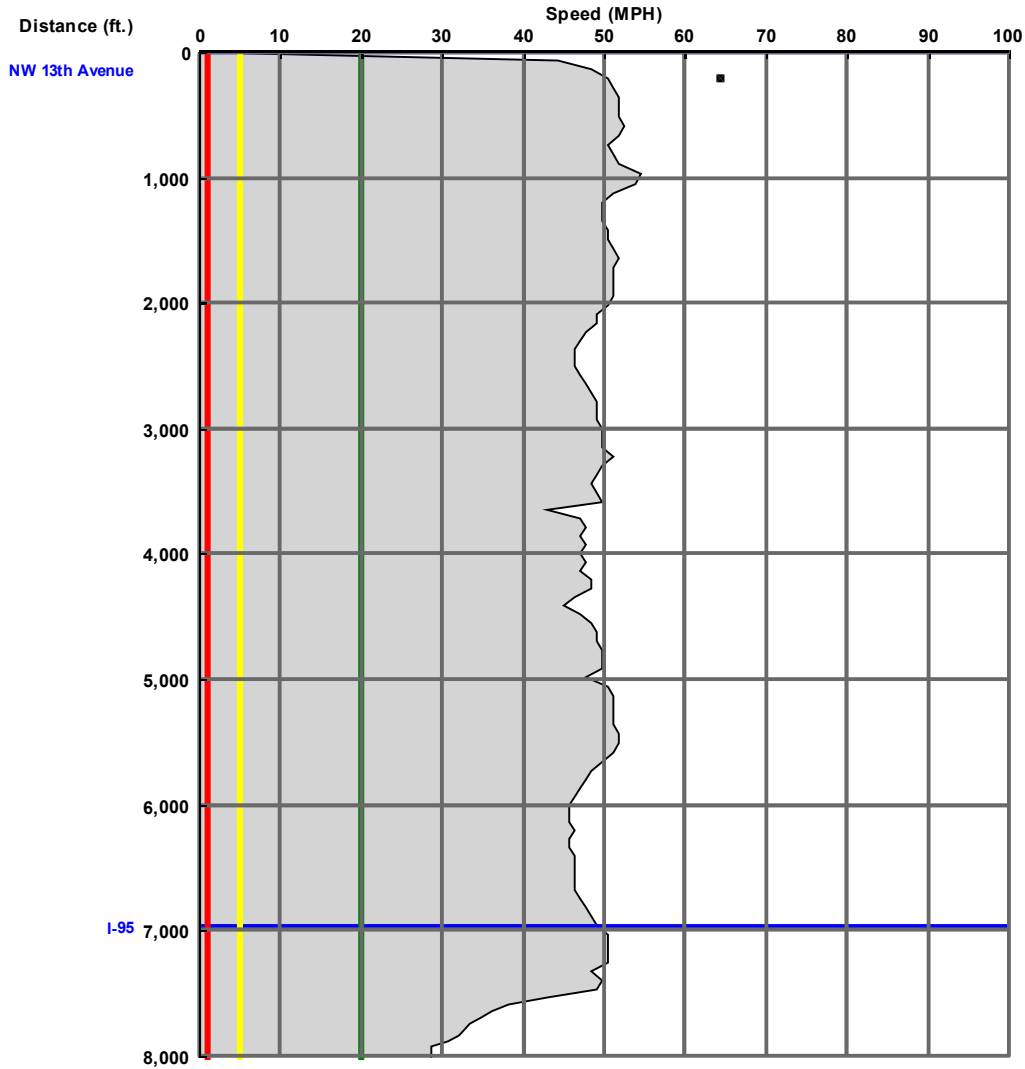
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 28

Speed Profile

Run: RUN 6 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

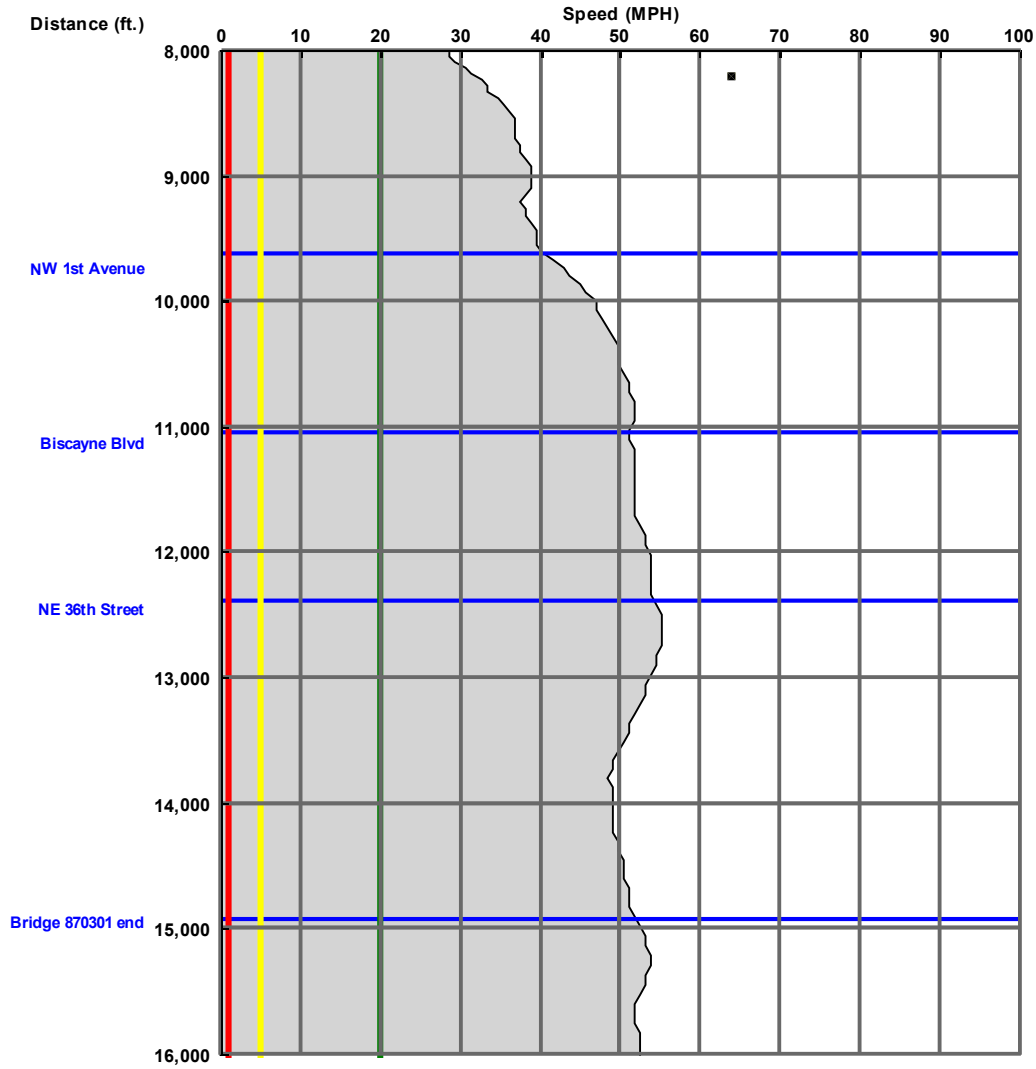
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 29

Speed Profile

Run: RUN 6 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

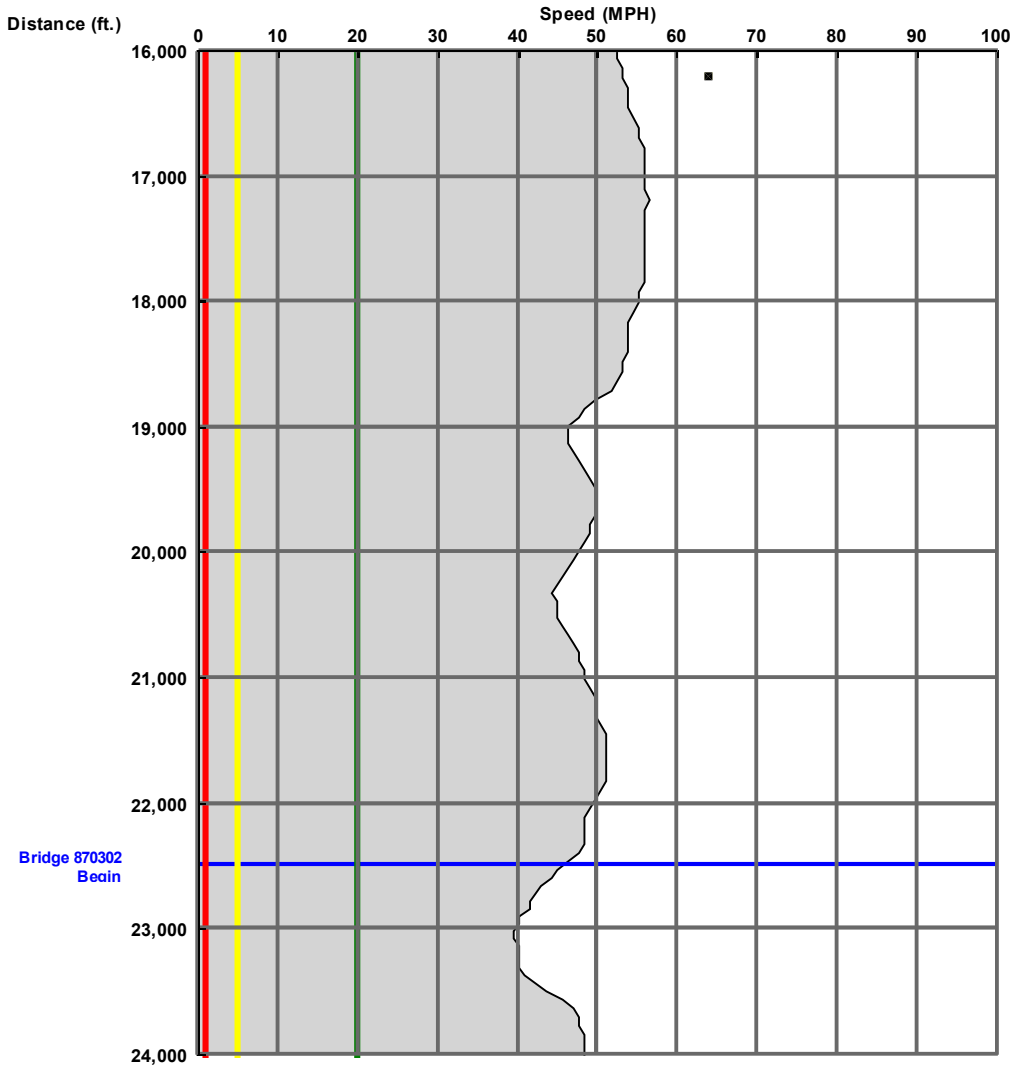
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 30

Speed Profile

Run: RUN 6 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

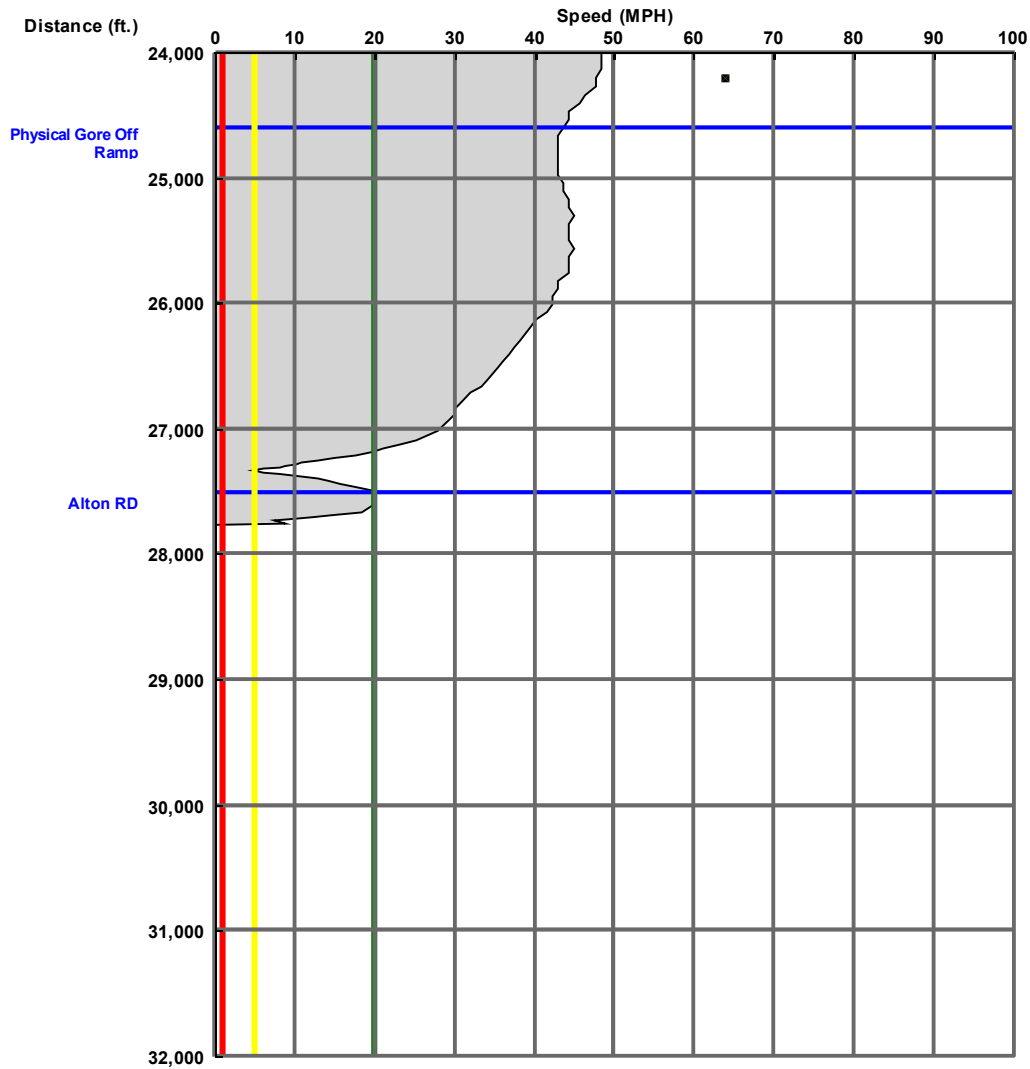
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 31

Speed Profile

Run: RUN 6 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

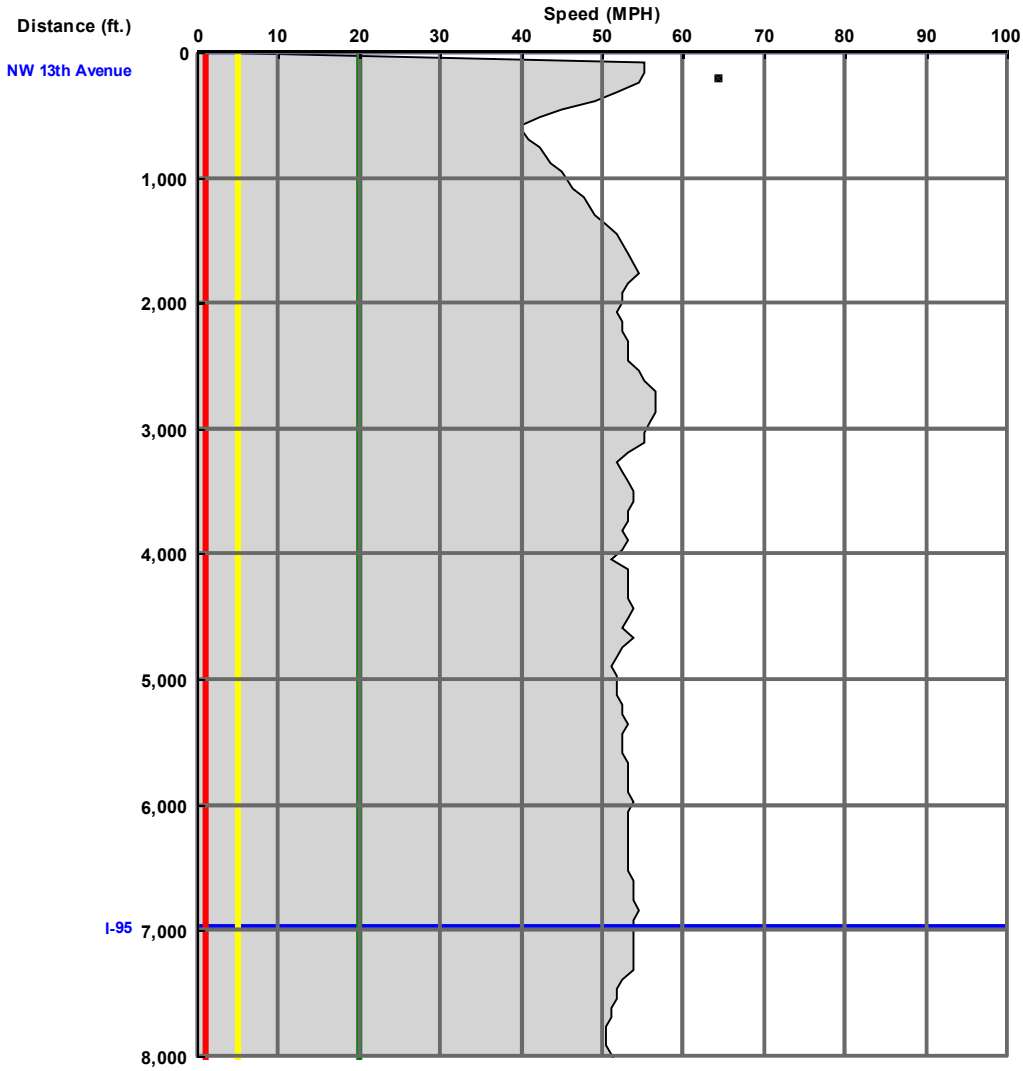
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 32

Speed Profile

Run: RUN 7 EB PM 2-15-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

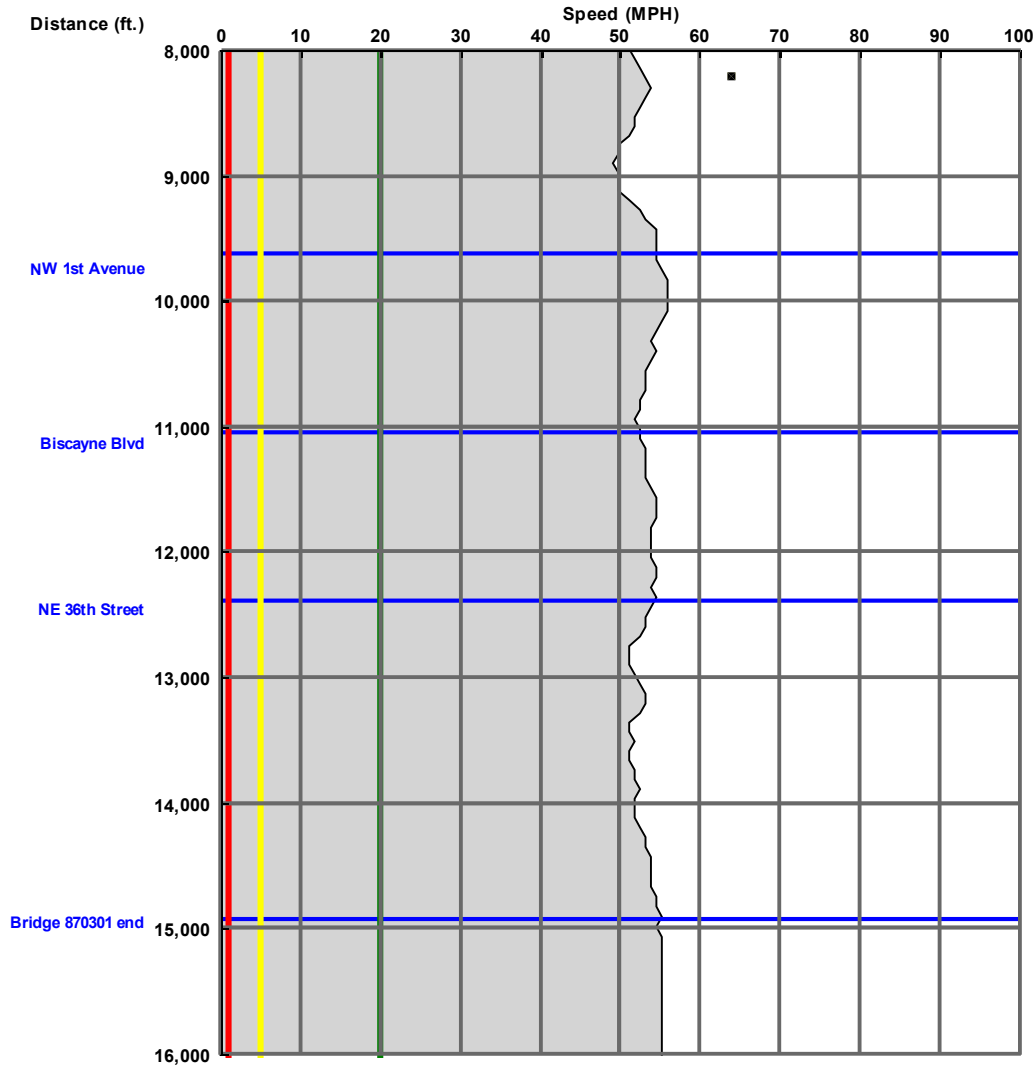
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 33

Speed Profile

Run: RUN 7 EB PM 2-15-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

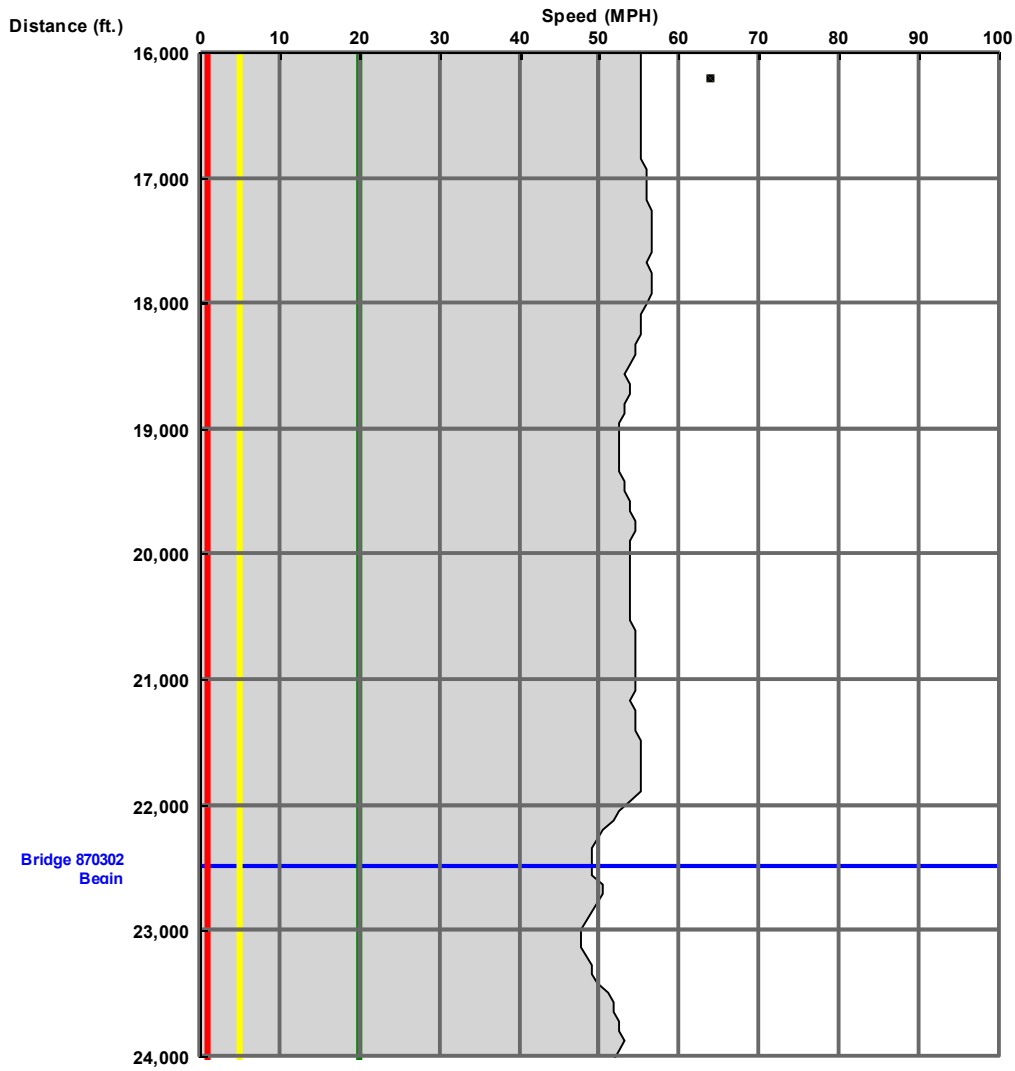
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 34

Speed Profile

Run: RUN 7 EB PM 2-15-2018-R001



I-195 EASTBOUND (PM)

Ten and Two - Travel Time Data

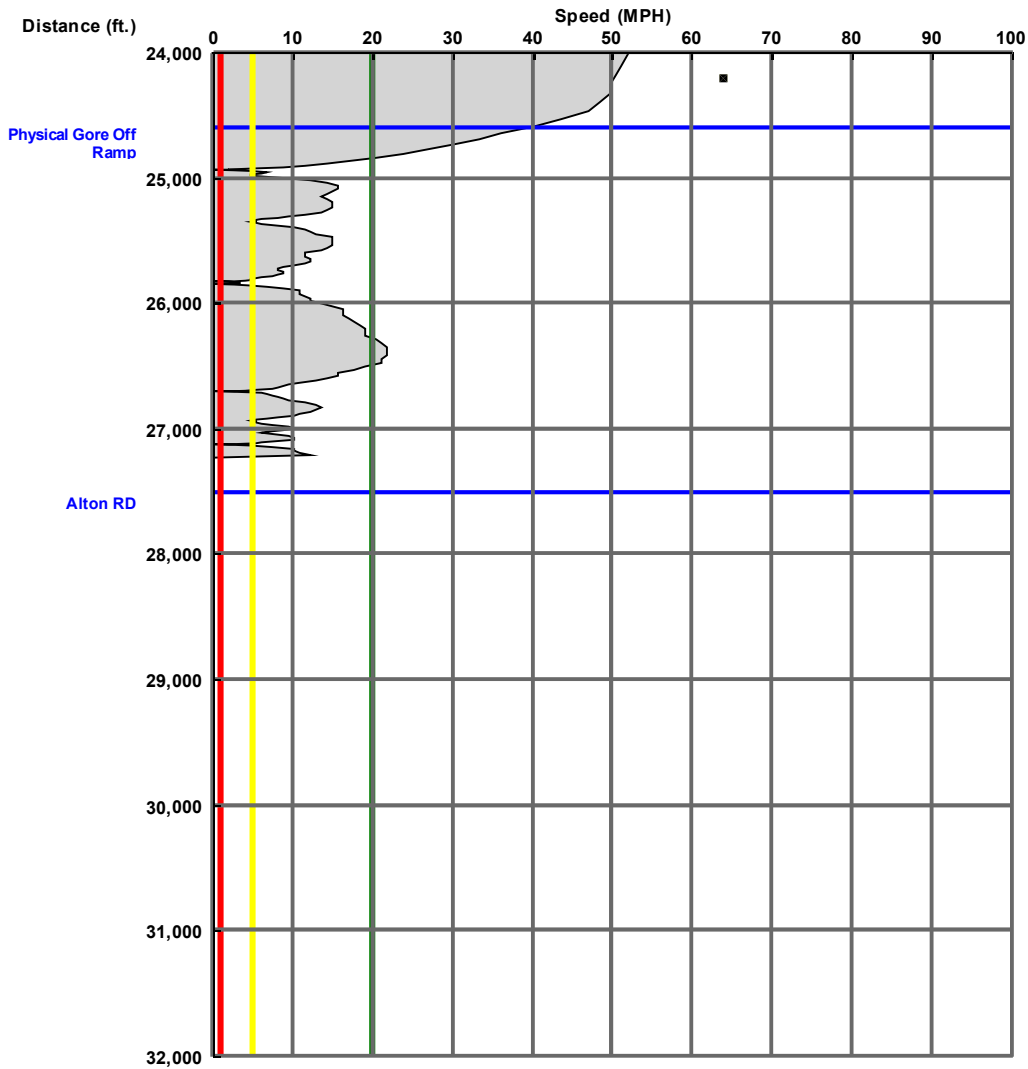
Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 35

Speed Profile

Run: RUN 7 EB PM 2-15-2018-R001



I-195 EASTBOUND (PM)

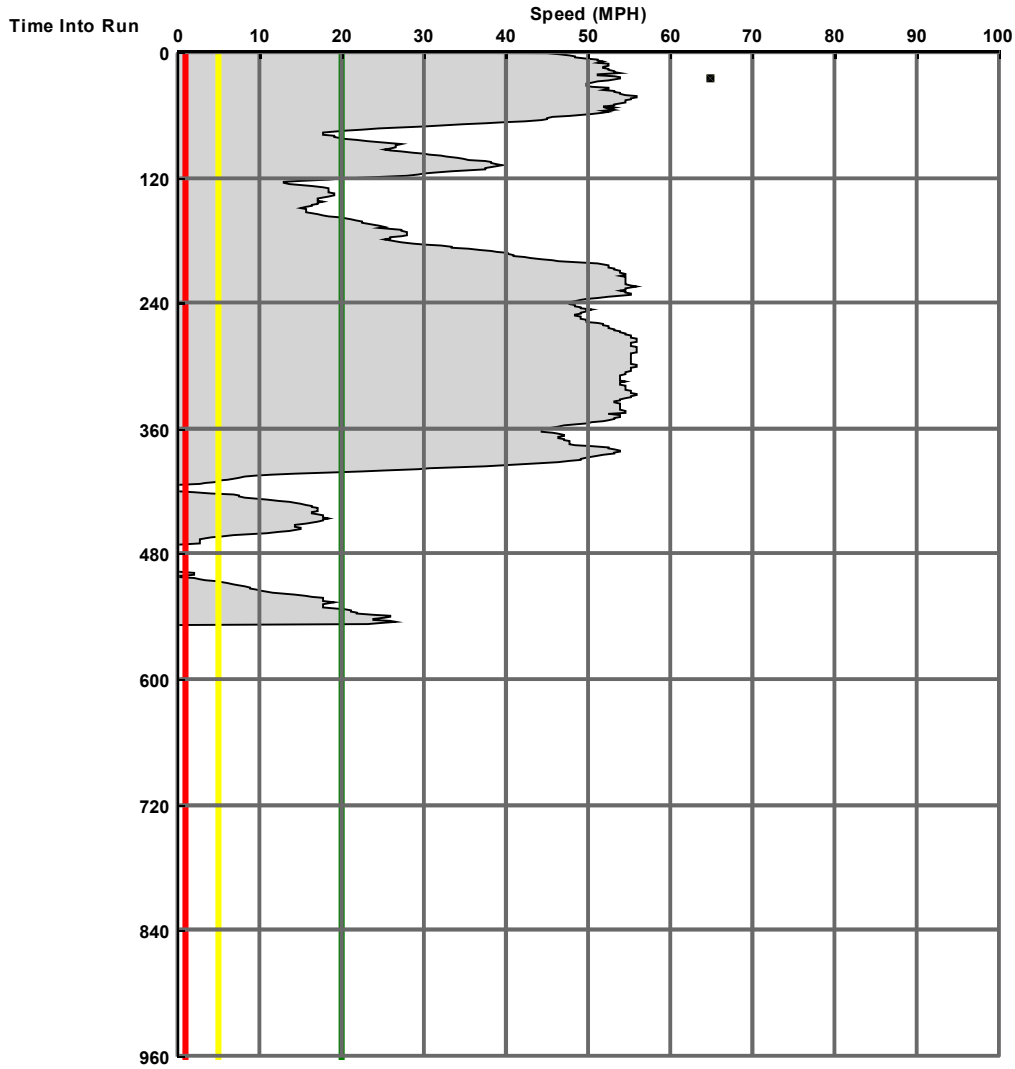
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 36

Time Based Speed Profile Run: RUN 1 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

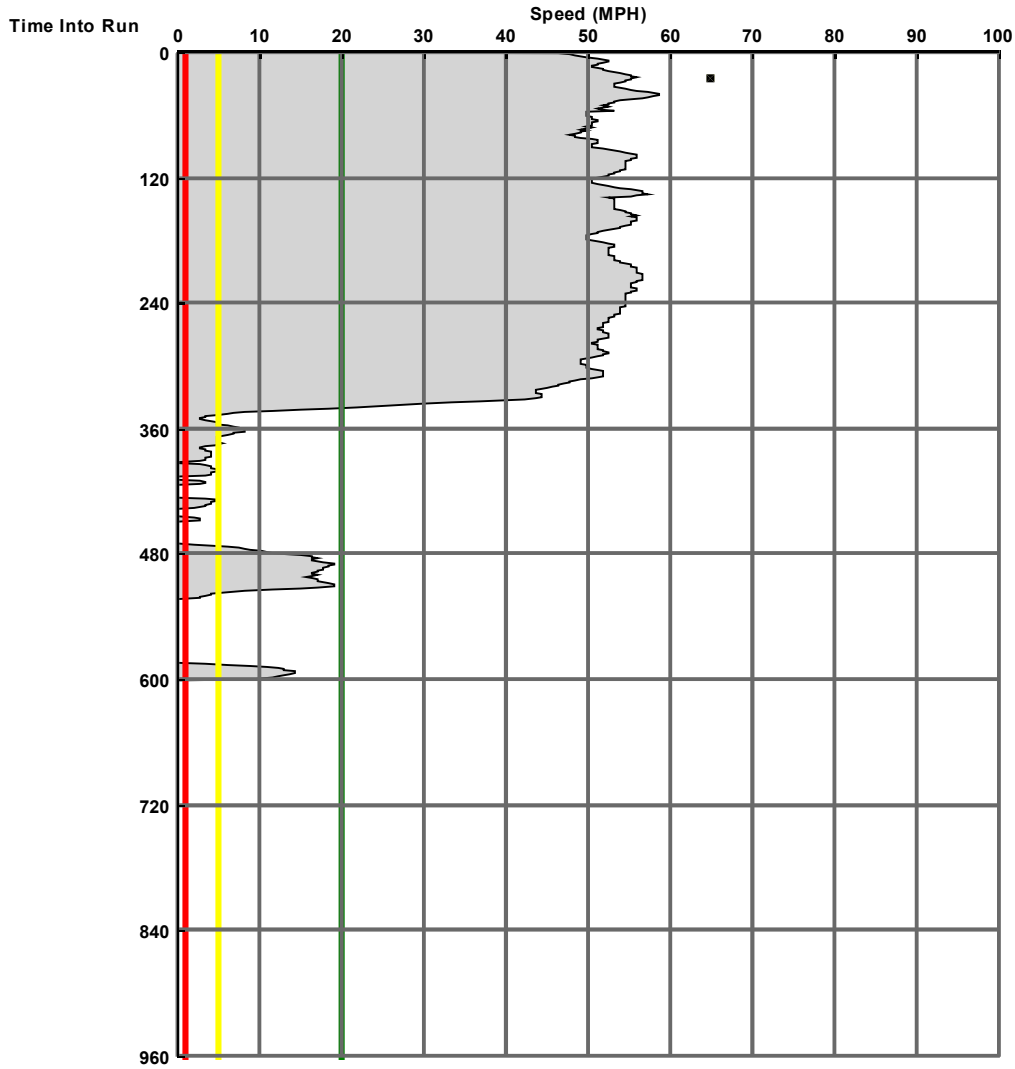
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 37

Time Based Speed Profile Run: RUN 2 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

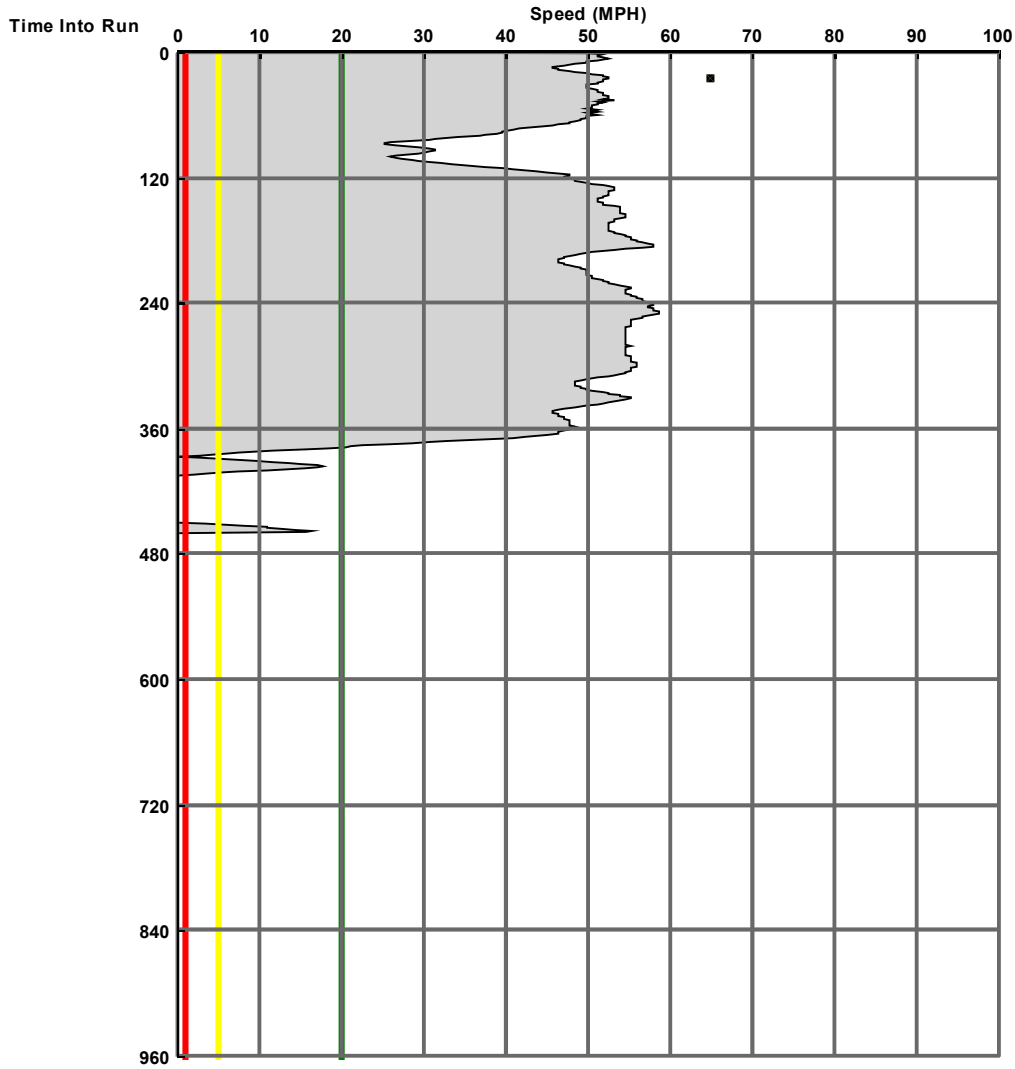
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 38

Time Based Speed Profile Run: RUN 3 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

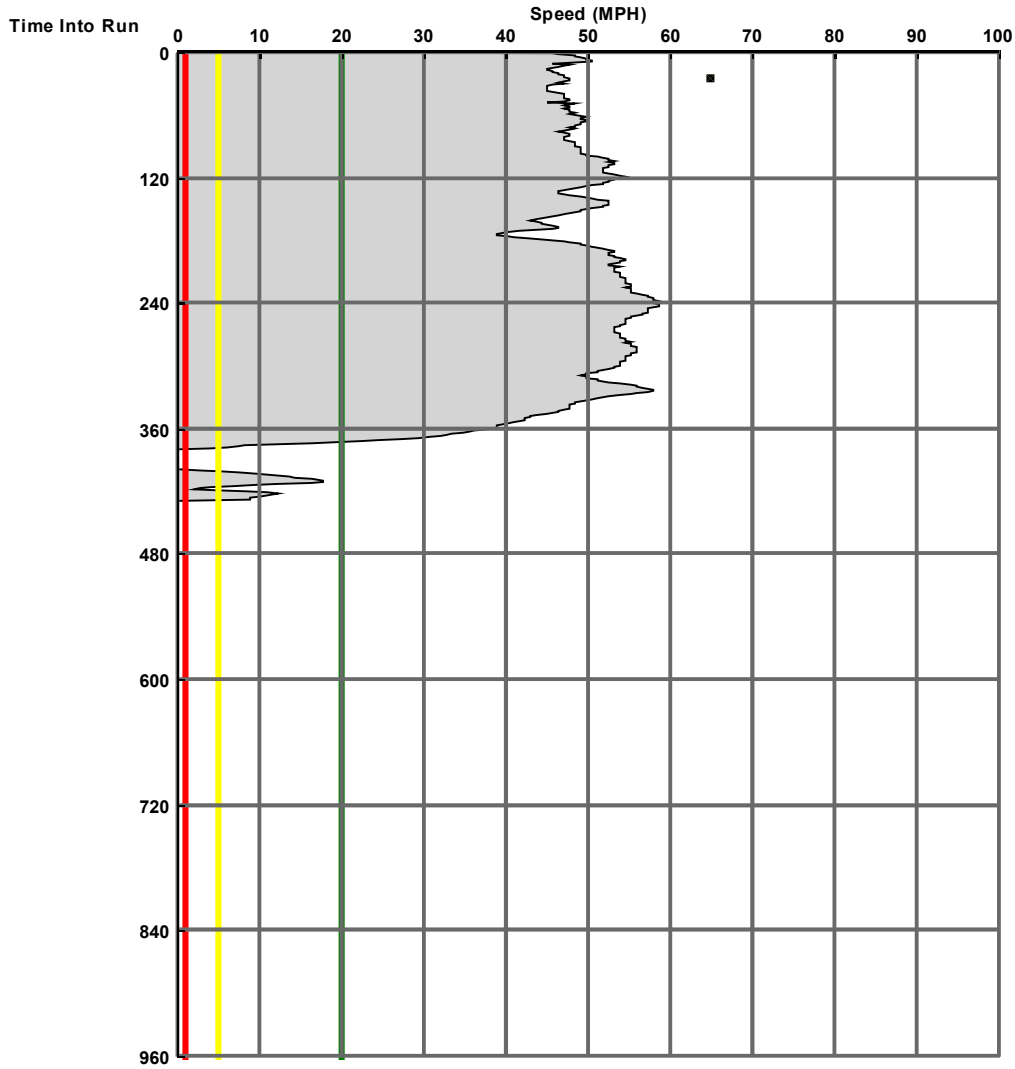
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 39

Time Based Speed Profile Run: RUN 4 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

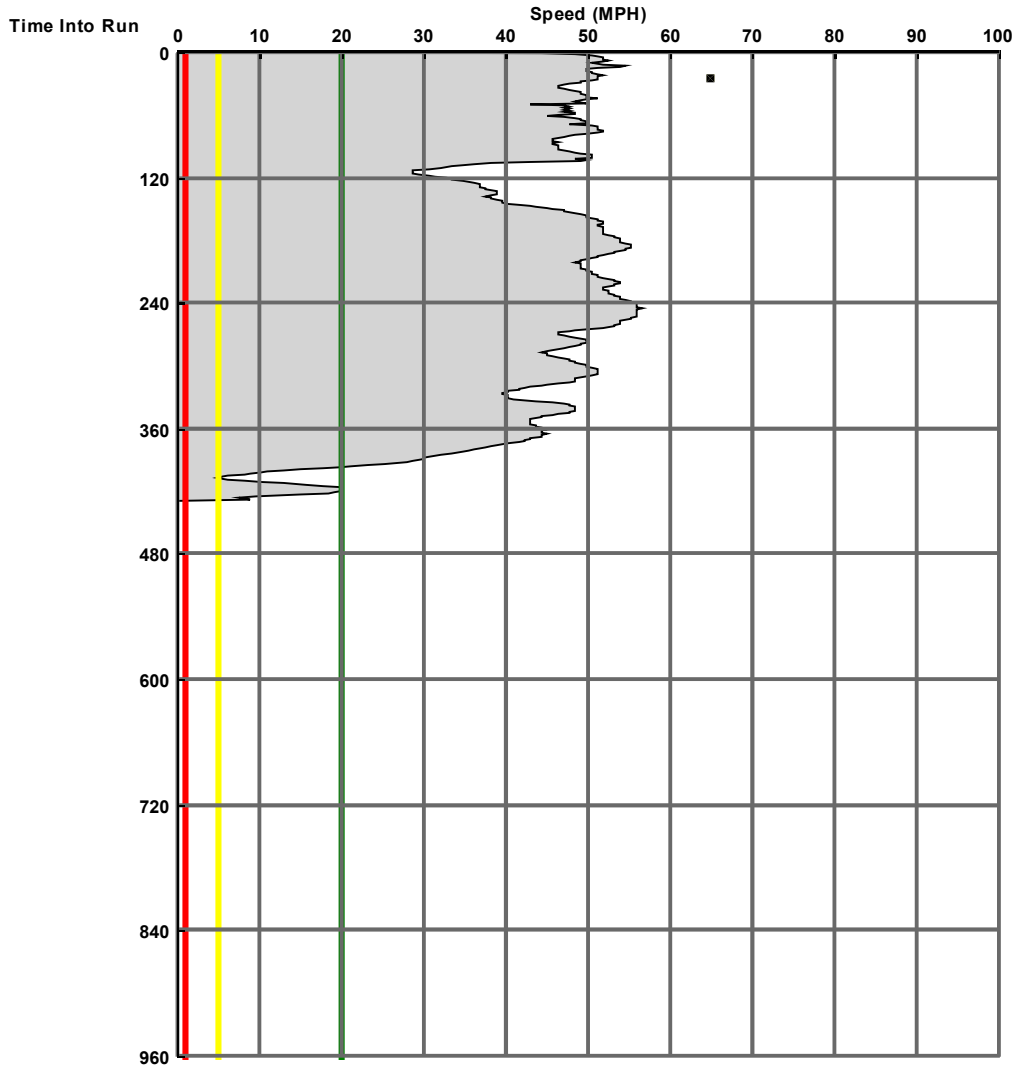
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 40

Time Based Speed Profile Run: RUN 6 EB PM 2-14-2018-R001



I-195 EASTBOUND (PM)

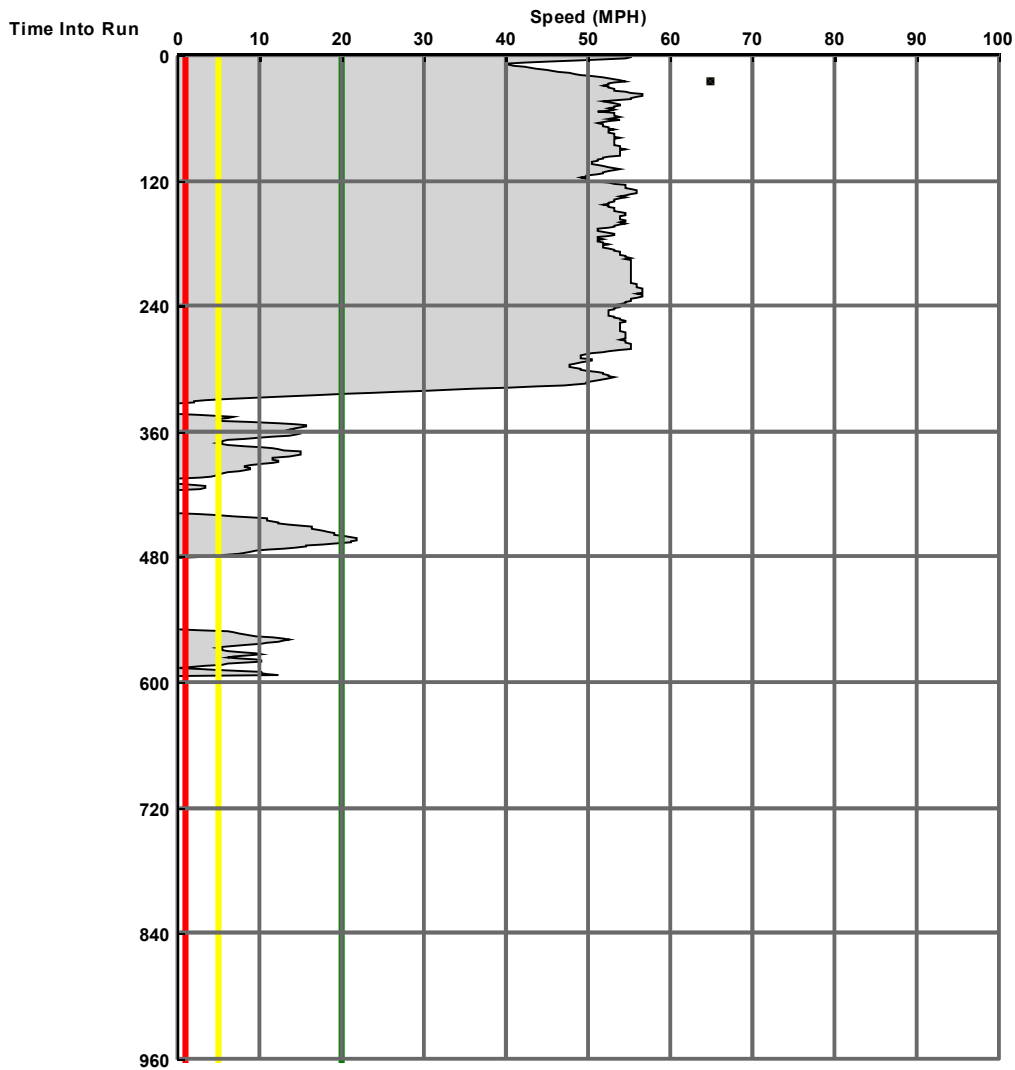
Ten and Two - Travel Time Data

Study Name: I-195 EASTBOUND PM

Study Date: 3/21/2018

Page No: 41

Time Based Speed Profile Run: RUN 7 EB PM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

Travel Time Reports for study: I-195 Westbound AM

<u>Report Name</u>	<u>Page</u>
Study Summary	2
Overall Output Statistics	3
Detailed Stats By Run - Travel Time.....	4
Detailed Stats By Run - Stops	5
Detailed Stats By Run - Average Speed	6
Detailed Stats By Run - Total Delay.....	7
Detailed Stats By Run - Time Less Than0 MPH	8
Detailed Stats By Run - Time Less Than5 MPH	9
Detailed Stats By Run - Time Less Than20 MPH	10
Speed/Distance Plot of All Runs	11
Time/Space Trajectory of All Runs.....	12
Speed Profile (Distance vs Speed) for RUN 2 WB AM 2-14-2018-R001	13
Speed Profile (Distance vs Speed) for RUN 3 WB AM 2-14-2018-R001	17
Speed Profile (Distance vs Speed) for RUN 4 WB AM 2-14-2018-R001	21
Speed Profile (Distance vs Speed) for RUN 5 WB AM 2-15-2018-R001	25
Speed Profile (Distance vs Speed) for RUN 6 WB AM 2-15-2018-R001	29
Speed Profile (Distance vs Speed) for RUN 7 WB AM 2-15-2018-R001	33
Speed Profile (Time vs Speed) for RUN 2 WB AM 2-14-2018-R001.....	37
Speed Profile (Time vs Speed) for RUN 3 WB AM 2-14-2018-R001.....	38
Speed Profile (Time vs Speed) for RUN 4 WB AM 2-14-2018-R001.....	39
Speed Profile (Time vs Speed) for RUN 5 WB AM 2-15-2018-R001.....	40
Speed Profile (Time vs Speed) for RUN 6 WB AM 2-15-2018-R001.....	41
Speed Profile (Time vs Speed) for RUN 7 WB AM 2-15-2018-R001.....	42

I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 1

Study Summary Runs Used in This Study

Run Title	Start Date	Start Time	Length	Before/After	Run Type
RUN 2 WB AM 2-14-2018-R001	02/14/18	07:49:42	27257	Before	Secondary
RUN 3 WB AM 2-14-2018-R001	02/14/18	08:15:37	27281	Before	Secondary
RUN 4 WB AM 2-14-2018-R001	02/14/18	08:46:04	27374	Before	Secondary
RUN 5 WB AM 2-15-2018-R001	02/14/18	07:13:00	27367	Before	Secondary
RUN 6 WB AM 2-15-2018-R001	02/14/18	07:34:17	27364	Before	Secondary
RUN 7 WB AM 2-15-2018-R001	02/14/18	07:57:21	27379	Before	Secondary

Notes:

Node Info

#	Length	Name
1	0	Alton RD
2	2622	Physical Gore Off Ramp
3	2339	Bridge 870302 end
4	7565	Bridge 870301 Begin
5	2710	NE 36th Street
6	2572	Biscayne Blvd
7	2630	NW 1st Avenue
8	2843	I-95
9	3811	NW 13th Avenue

Length of Study Route = 27,092 feet.

I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 2

Overall Output Statistics

Node #	Length (ft)	Node Name	Travel Time	# of Stops	Avg Speed (MPH)	Total Delay	Time <= 0 MPH	Time <= 5 MPH	Time <= 20 MPH
1	0	Alton RD							
2	2622	Physical Gore Off Ramp	41.5	0.0	43.1	0.0	0.0	0.0	0.8
3	2339	Bridge 870302 end	30.5	0.0	52.3	0.0	0.0	0.0	0.0
4	7565	Bridge 870301 Begin	95.2	0.0	54.2	0.0	0.0	0.0	0.0
5	2710	NE 36th Street	35.8	0.0	51.6	0.0	0.0	0.0	0.0
6	2572	Biscayne Blvd	32.7	0.0	53.7	0.0	0.0	0.0	0.0
7	2630	NW 1st Avenue	34.5	0.0	52.0	0.0	0.0	0.0	0.0
8	2843	I-95	38.7	0.0	50.1	0.0	0.0	0.0	0.0
9	3811	NW 13th Avenue	51.7	0.0	50.3	0.0	0.0	0.0	0.0
Total	27,092		360.5	0	51.2	0	0	0	0.8

Stats based on 6 runs.

Stops based on a Stop Speed of 5 MPH.

Total Delay based on a Normal Speed of 0 MPH.

I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 3

Travel Time

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	Alton RD						
2	2622	Physical Gore Off Ramp	44	41	45	40	40	39
3	2339	Bridge 870302 end	31	30	32	30	30	30
4	7565	Bridge 870301 Begin	94	95	96	94	96	96
5	2710	NE 36th Street	35	35	36	35	38	36
6	2572	Biscayne Blvd	33	33	32	32	33	33
7	2630	NW 1st Avenue	34	36	35	34	34	34
8	2843	I-95	36	46	38	36	35	41
9	3811	NW 13th Avenue	52	50	54	49	56	49
Total	27,092		359	366	368	350	362	358

Run # 1 = RUN 2 WB AM 2-14-2018-R001

Run # 2 = RUN 3 WB AM 2-14-2018-R001

Run # 3 = RUN 4 WB AM 2-14-2018-R001

Run # 4 = RUN 5 WB AM 2-15-2018-R001

Run # 5 = RUN 6 WB AM 2-15-2018-R001

Run # 6 = RUN 7 WB AM 2-15-2018-R001

I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 4

Number of Stops

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	Alton RD						
2	2622	Physical Gore Off Ramp	0	0	0	0	0	0
3	2339	Bridge 870302 end	0	0	0	0	0	0
4	7565	Bridge 870301 Begin	0	0	0	0	0	0
5	2710	NE 36th Street	0	0	0	0	0	0
6	2572	Biscayne Blvd	0	0	0	0	0	0
7	2630	NW 1st Avenue	0	0	0	0	0	0
8	2843	I-95	0	0	0	0	0	0
9	3811	NW 13th Avenue	0	0	0	0	0	0
Total	27,092		0	0	0	0	0	0

Stops based on a Stop Speed of 5 MPH.

Run # 1 = RUN 2 WB AM 2-14-2018-R001

Run # 2 = RUN 3 WB AM 2-14-2018-R001

Run # 3 = RUN 4 WB AM 2-14-2018-R001

Run # 4 = RUN 5 WB AM 2-15-2018-R001

Run # 5 = RUN 6 WB AM 2-15-2018-R001

Run # 6 = RUN 7 WB AM 2-15-2018-R001

I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 5

Average Speed (MPH)

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	Alton RD	0.0	0.0	0.0	0.0	0.0	0.0
2	2622	Physical Gore Off Ramp	40.6	44.0	39.7	45.9	45.5	46.1
3	2339	Bridge 870302 end	52.7	53.4	50.9	52.5	52.7	53.0
4	7565	Bridge 870301 Begin	54.6	54.4	53.5	54.8	53.7	54.0
5	2710	NE 36th Street	52.7	53.1	52.3	53.7	48.5	51.3
6	2572	Biscayne Blvd	53.6	53.5	53.2	54.0	53.7	53.3
7	2630	NW 1st Avenue	53.2	49.3	51.9	53.1	52.8	52.3
8	2843	I-95	53.3	41.9	50.4	54.6	55.3	47.2
9	3811	NW 13th Avenue	49.9	52.2	48.3	52.6	46.6	53.3
Total	27,092		51.5	50.6	50.2	52.9	51.1	51.7

Run # 1 = RUN 2 WB AM 2-14-2018-R001

Run # 2 = RUN 3 WB AM 2-14-2018-R001

Run # 3 = RUN 4 WB AM 2-14-2018-R001

Run # 4 = RUN 5 WB AM 2-15-2018-R001

Run # 5 = RUN 6 WB AM 2-15-2018-R001

Run # 6 = RUN 7 WB AM 2-15-2018-R001

I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 6

Total Delay

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	Alton RD	0	0	0	0	0	0
2	2622	Physical Gore Off Ramp	0	0	0	0	0	0
3	2339	Bridge 870302 end	0	0	0	0	0	0
4	7565	Bridge 870301 Begin	0	0	0	0	0	0
5	2710	NE 36th Street	0	0	0	0	0	0
6	2572	Biscayne Blvd	0	0	0	0	0	0
7	2630	NW 1st Avenue	0	0	0	0	0	0
8	2843	I-95	0	0	0	0	0	0
9	3811	NW 13th Avenue	0	0	0	0	0	0
Total	27,092		0	0	0	0	0	0

Total Delay based on a Normal Speed of 0 MPH.

Run # 1 = RUN 2 WB AM 2-14-2018-R001

Run # 2 = RUN 3 WB AM 2-14-2018-R001

Run # 3 = RUN 4 WB AM 2-14-2018-R001

Run # 4 = RUN 5 WB AM 2-15-2018-R001

Run # 5 = RUN 6 WB AM 2-15-2018-R001

Run # 6 = RUN 7 WB AM 2-15-2018-R001

I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 7

Time Below

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	Alton RD	0	0	0	0	0	0
2	2622	Physical Gore Off Ramp	0	0	0	0	0	0
3	2339	Bridge 870302 end	0	0	0	0	0	0
4	7565	Bridge 870301 Begin	0	0	0	0	0	0
5	2710	NE 36th Street	0	0	0	0	0	0
6	2572	Biscayne Blvd	0	0	0	0	0	0
7	2630	NW 1st Avenue	0	0	0	0	0	0
8	2843	I-95	0	0	0	0	0	0
9	3811	NW 13th Avenue	0	0	0	0	0	0
Total	27,092		0	0	0	0	0	0

Run # 1 = RUN 2 WB AM 2-14-2018-R001

Run # 2 = RUN 3 WB AM 2-14-2018-R001

Run # 3 = RUN 4 WB AM 2-14-2018-R001

Run # 4 = RUN 5 WB AM 2-15-2018-R001

Run # 5 = RUN 6 WB AM 2-15-2018-R001

Run # 6 = RUN 7 WB AM 2-15-2018-R001

I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 8

Time Below

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	Alton RD	0	0	0	0	0	0
2	2622	Physical Gore Off Ramp	0	0	0	0	0	0
3	2339	Bridge 870302 end	0	0	0	0	0	0
4	7565	Bridge 870301 Begin	0	0	0	0	0	0
5	2710	NE 36th Street	0	0	0	0	0	0
6	2572	Biscayne Blvd	0	0	0	0	0	0
7	2630	NW 1st Avenue	0	0	0	0	0	0
8	2843	I-95	0	0	0	0	0	0
9	3811	NW 13th Avenue	0	0	0	0	0	0
Total	27,092		0	0	0	0	0	0

Run # 1 = RUN 2 WB AM 2-14-2018-R001

Run # 2 = RUN 3 WB AM 2-14-2018-R001

Run # 3 = RUN 4 WB AM 2-14-2018-R001

Run # 4 = RUN 5 WB AM 2-15-2018-R001

Run # 5 = RUN 6 WB AM 2-15-2018-R001

Run # 6 = RUN 7 WB AM 2-15-2018-R001

I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 9

Time Below

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	Alton RD	0	0	0	0	0	0
2	2622	Physical Gore Off Ramp	2	0	3	0	0	0
3	2339	Bridge 870302 end	0	0	0	0	0	0
4	7565	Bridge 870301 Begin	0	0	0	0	0	0
5	2710	NE 36th Street	0	0	0	0	0	0
6	2572	Biscayne Blvd	0	0	0	0	0	0
7	2630	NW 1st Avenue	0	0	0	0	0	0
8	2843	I-95	0	0	0	0	0	0
9	3811	NW 13th Avenue	0	0	0	0	0	0
Total	27,092		2	0	3	0	0	0

Run # 1 = RUN 2 WB AM 2-14-2018-R001

Run # 2 = RUN 3 WB AM 2-14-2018-R001

Run # 3 = RUN 4 WB AM 2-14-2018-R001

Run # 4 = RUN 5 WB AM 2-15-2018-R001

Run # 5 = RUN 6 WB AM 2-15-2018-R001

Run # 6 = RUN 7 WB AM 2-15-2018-R001

I-195 WESTBOUND (AM)

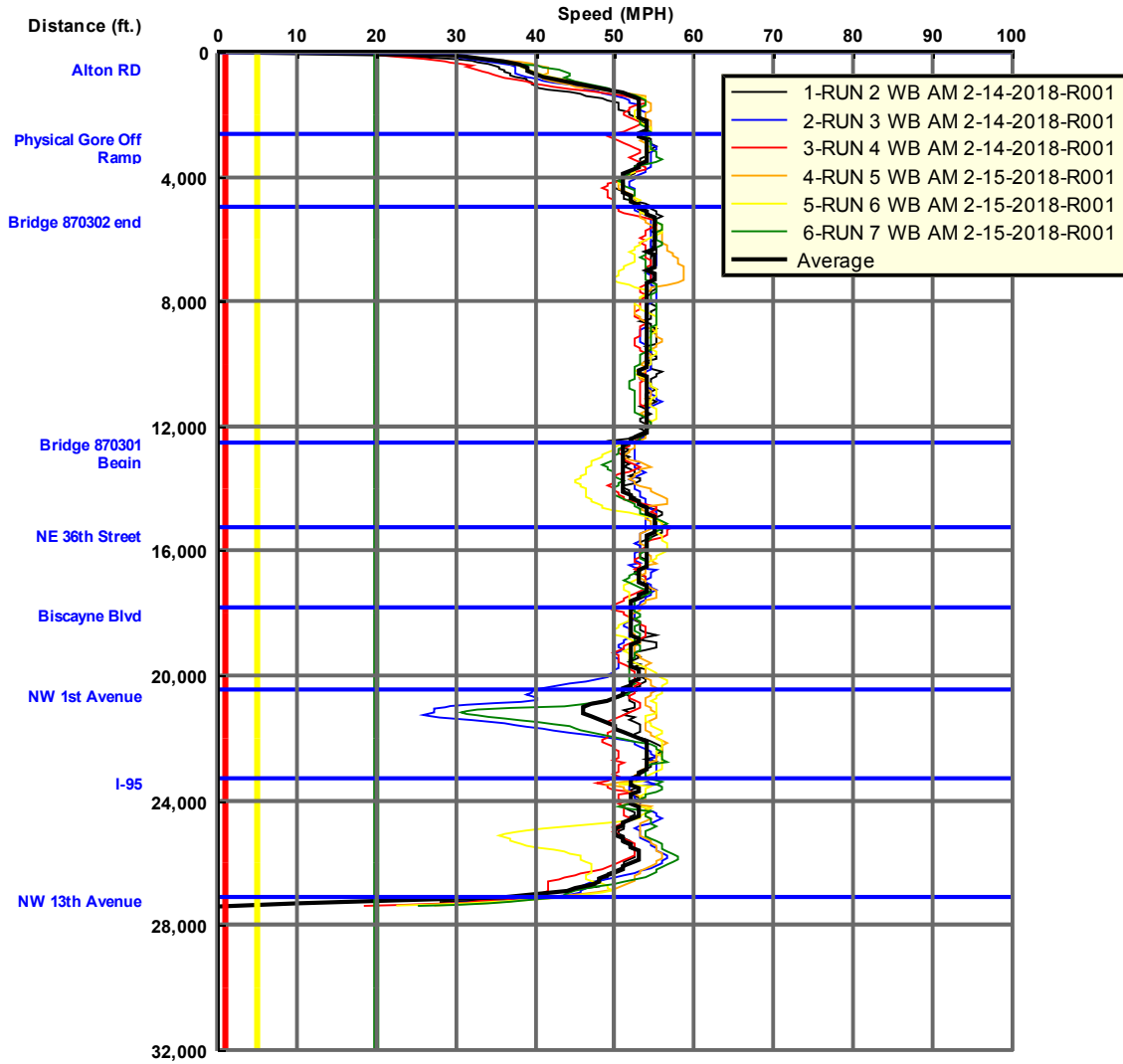
Ten and Two - Travel Time Data

Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 10

Speed/Distance Profiles of All Runs



I-195 WESTBOUND (AM)

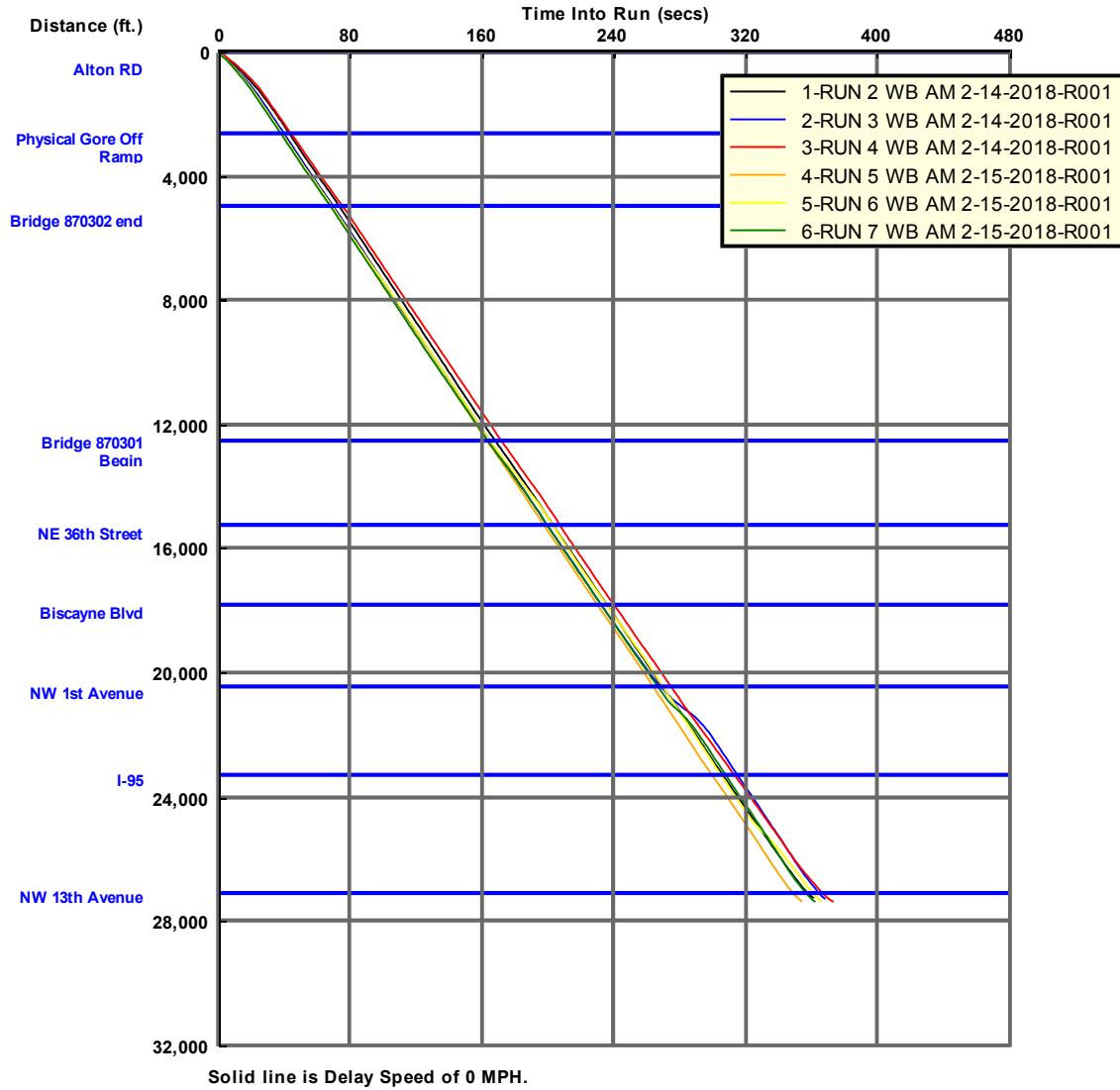
Ten and Two - Travel Time Data

Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 11

Space/Time Trajectory of All Runs



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

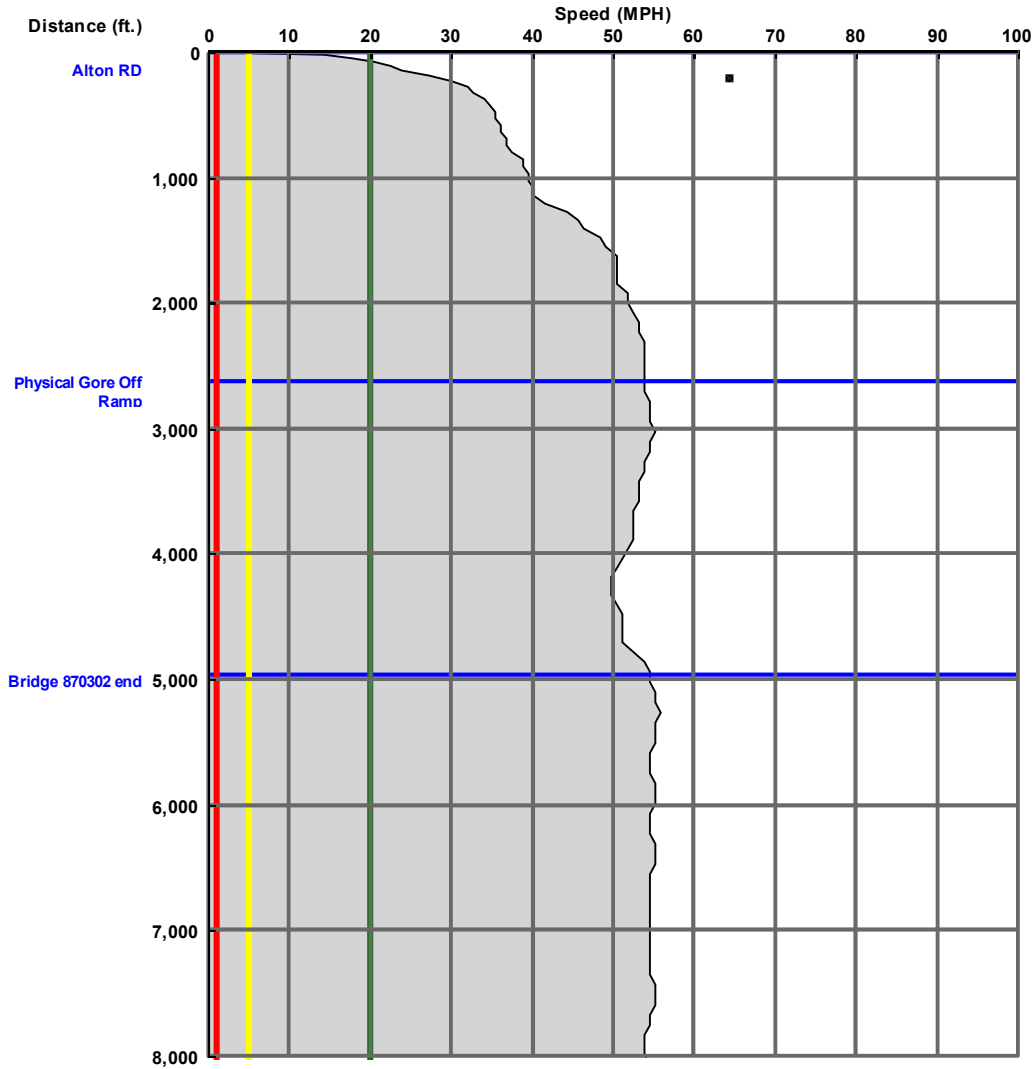
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 12

Speed Profile

Run: RUN 2 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

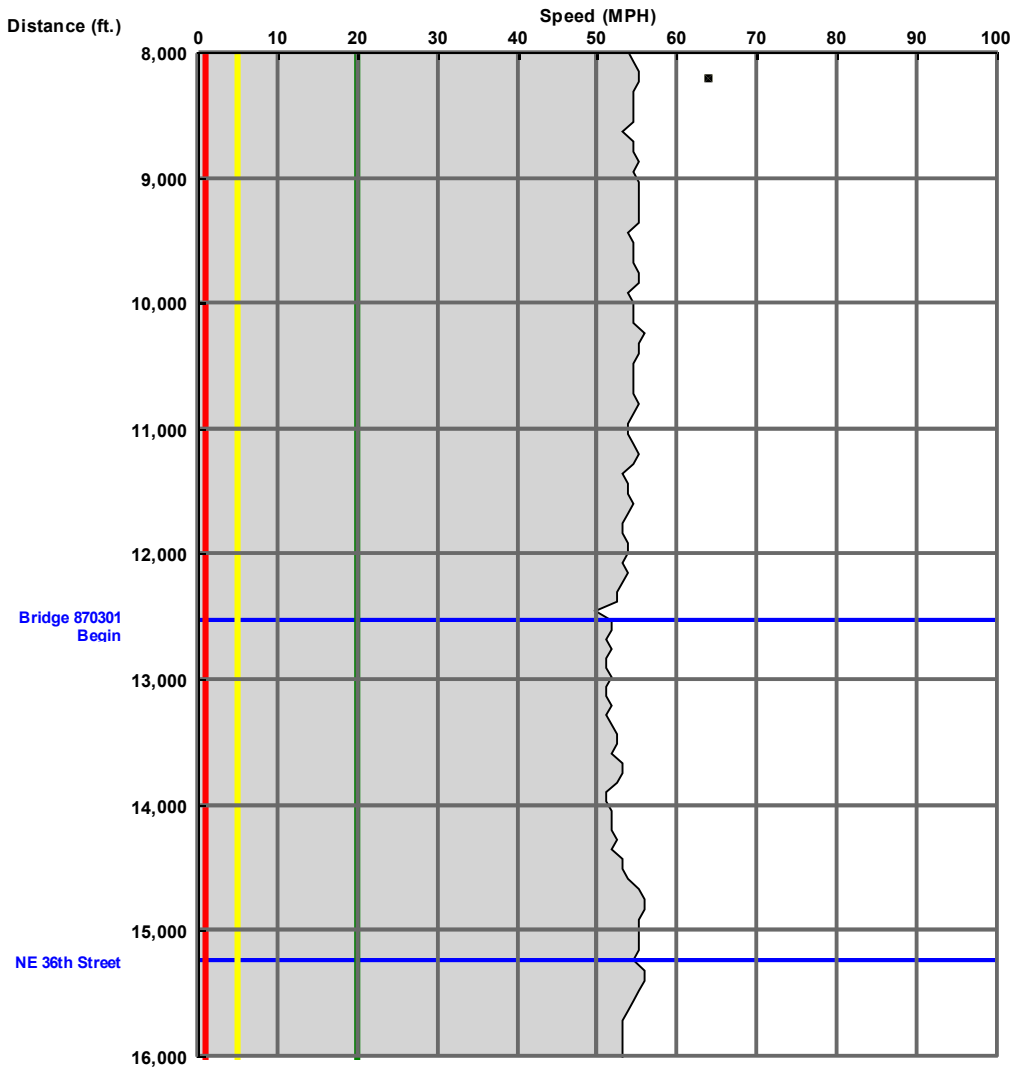
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 13

Speed Profile

Run: RUN 2 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

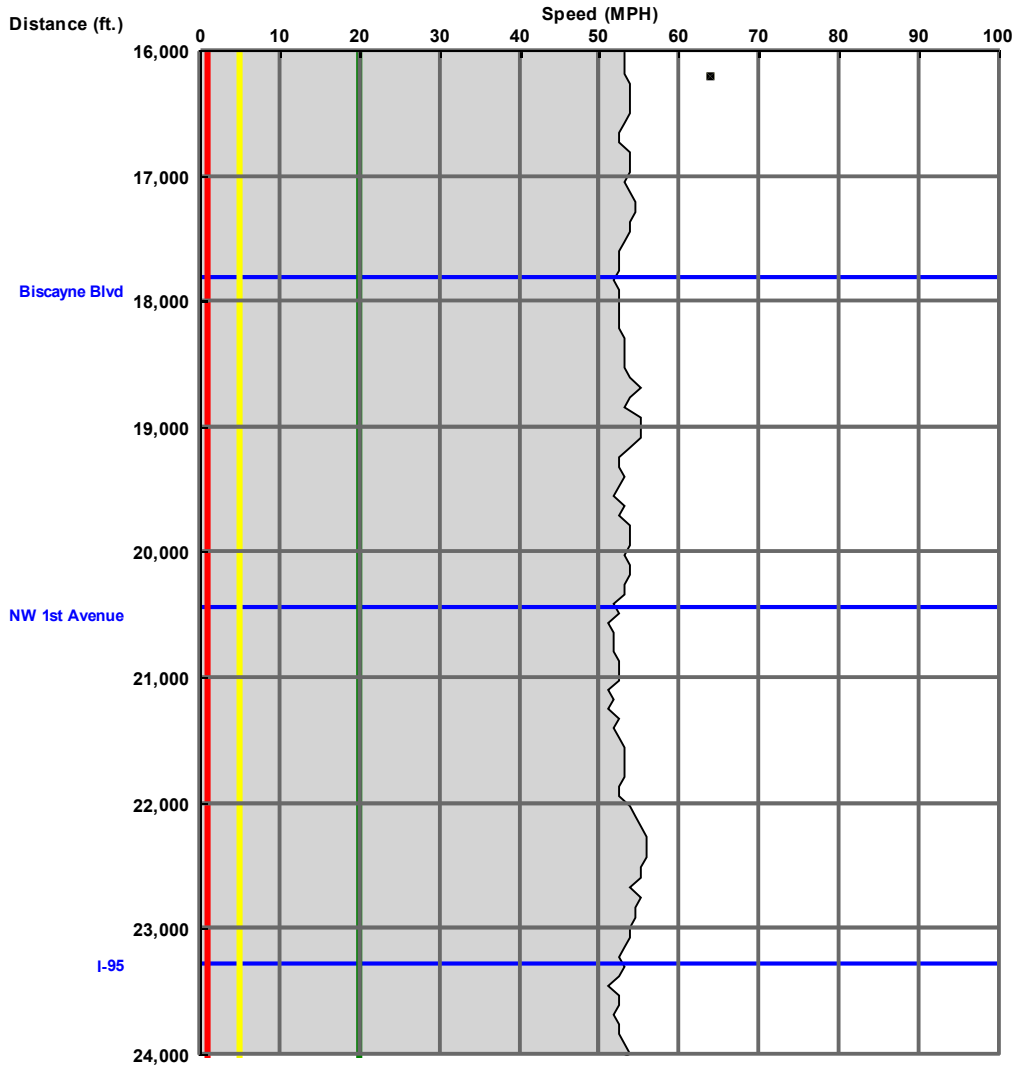
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 14

Speed Profile

Run: RUN 2 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

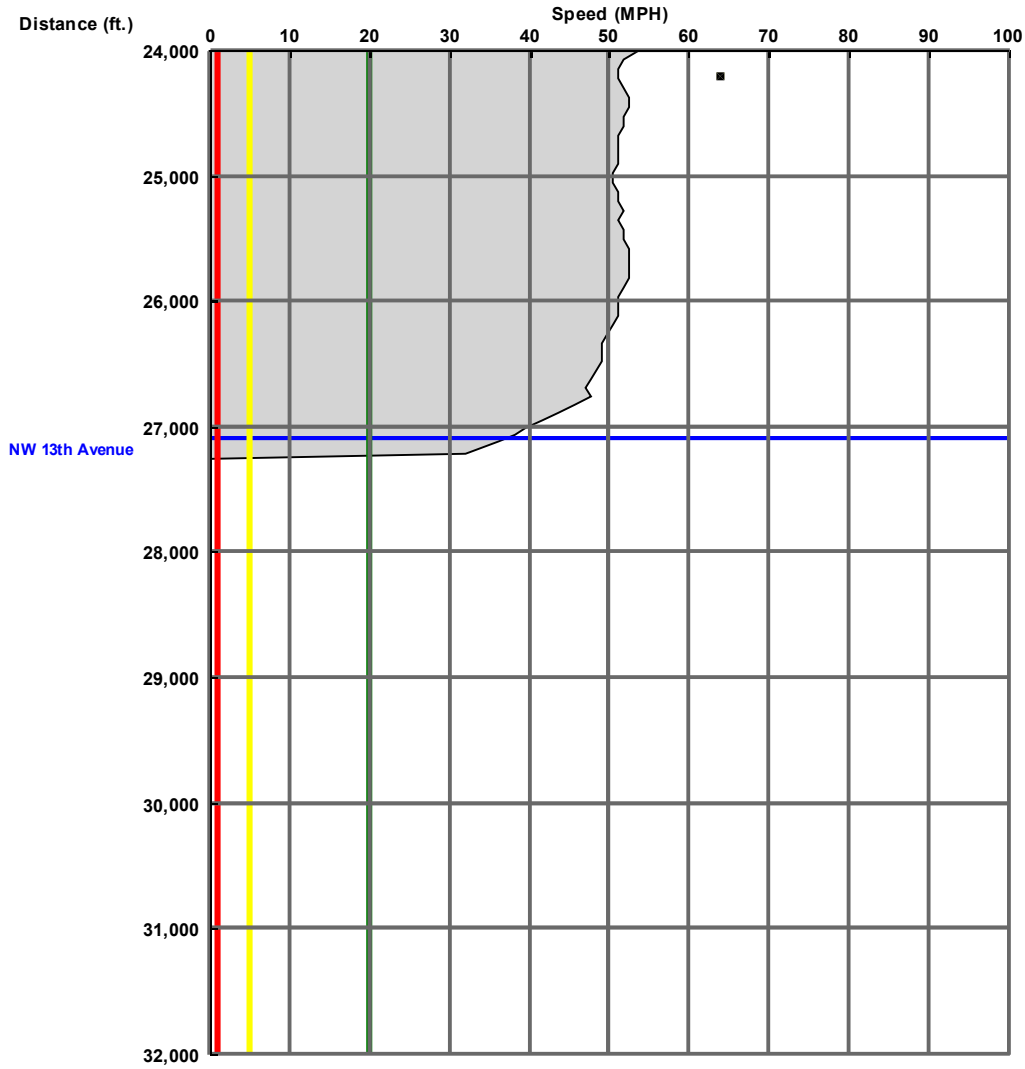
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 15

Speed Profile

Run: RUN 2 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

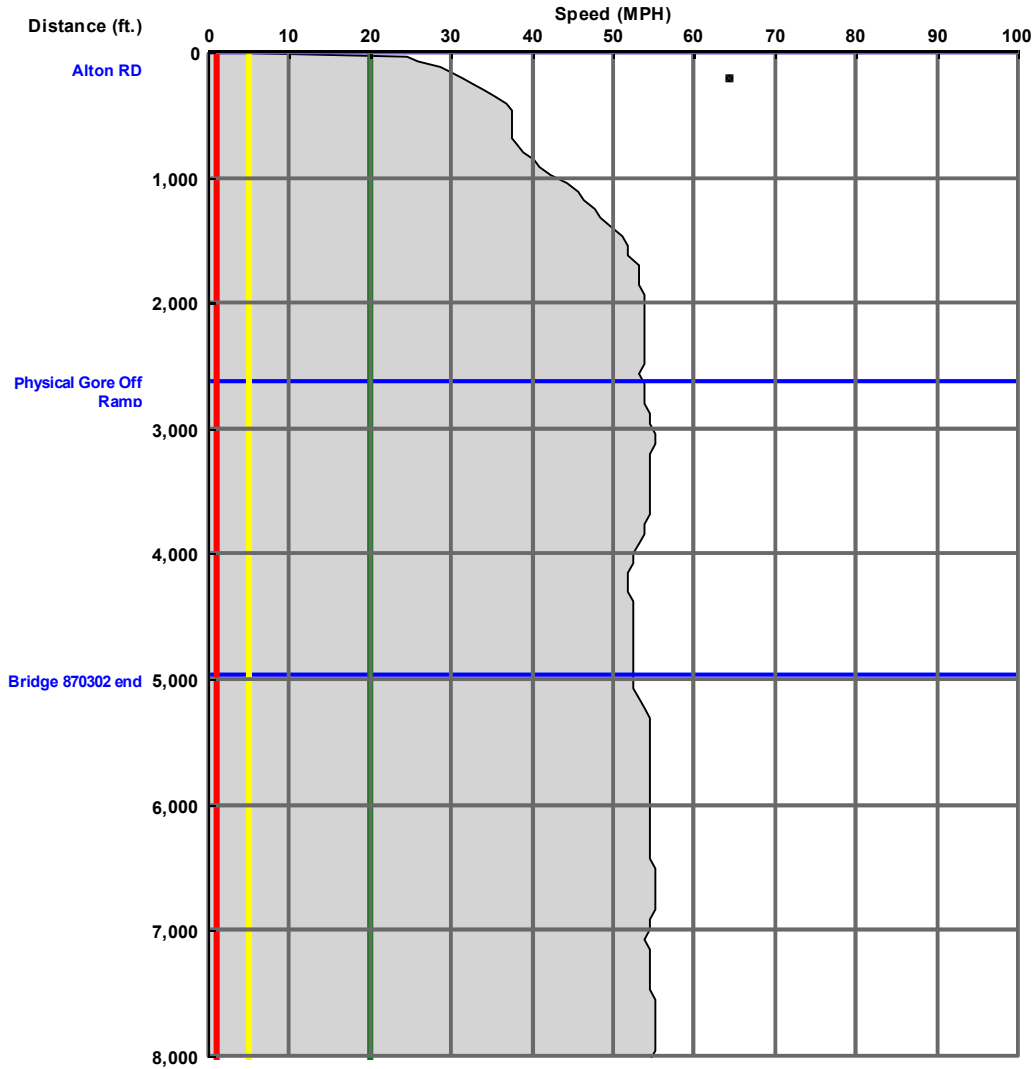
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 16

Speed Profile

Run: RUN 3 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

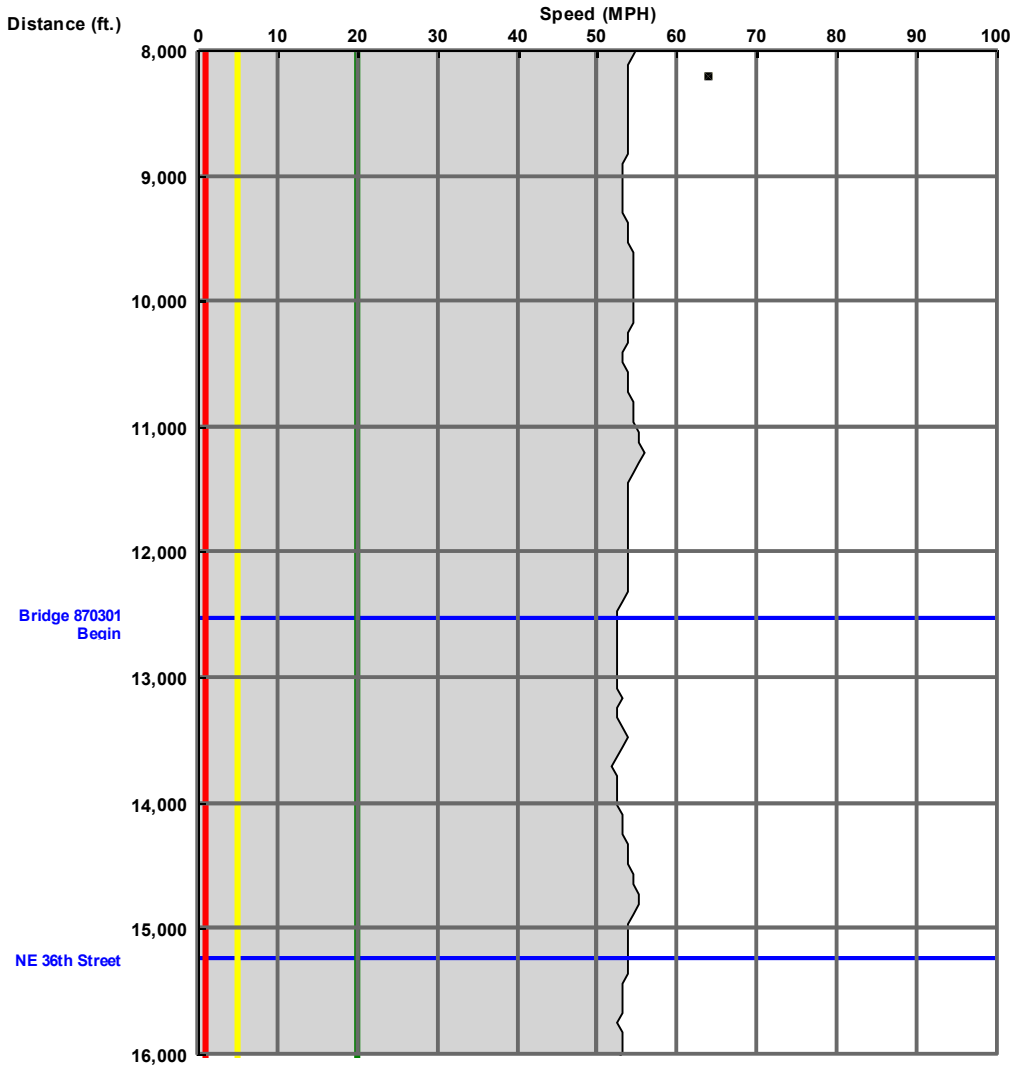
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 17

Speed Profile

Run: RUN 3 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

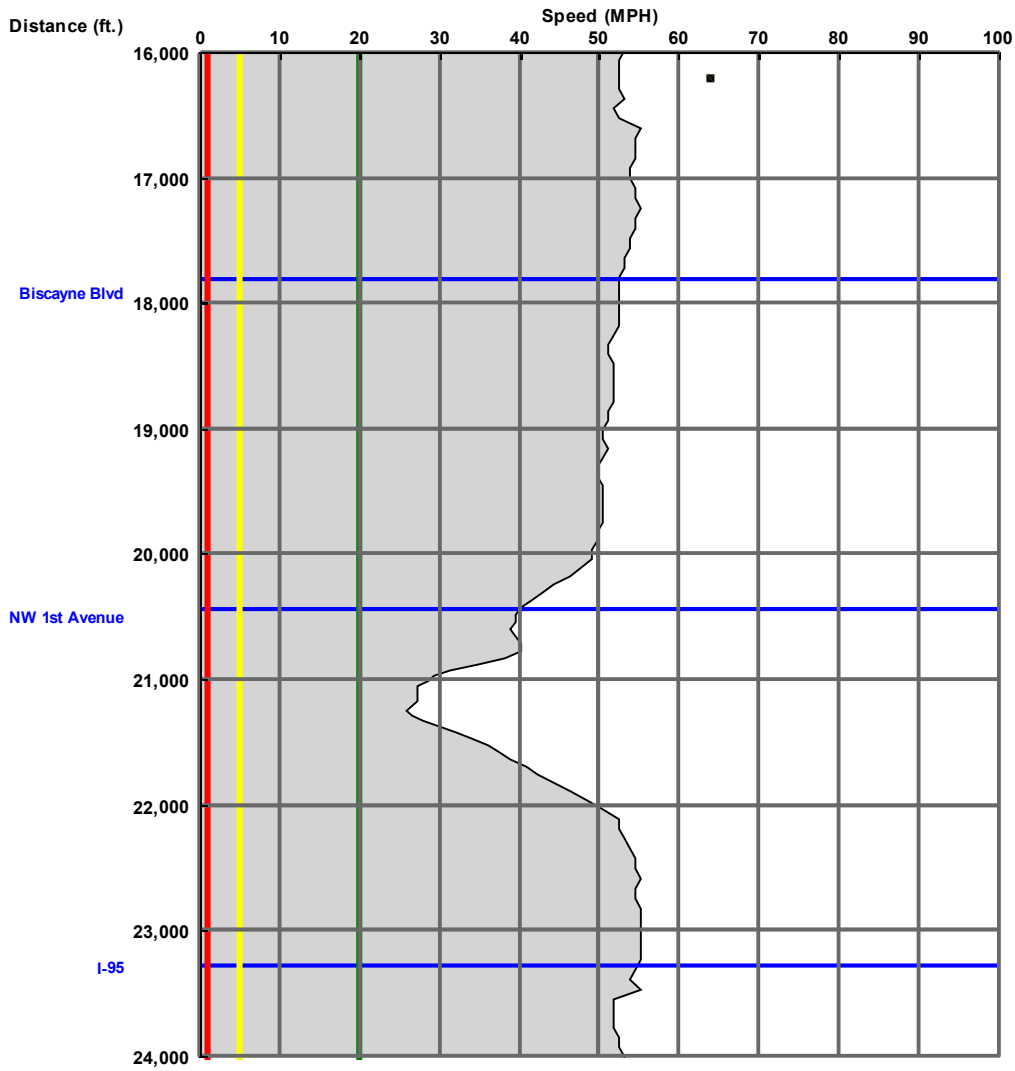
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 18

Speed Profile

Run: RUN 3 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

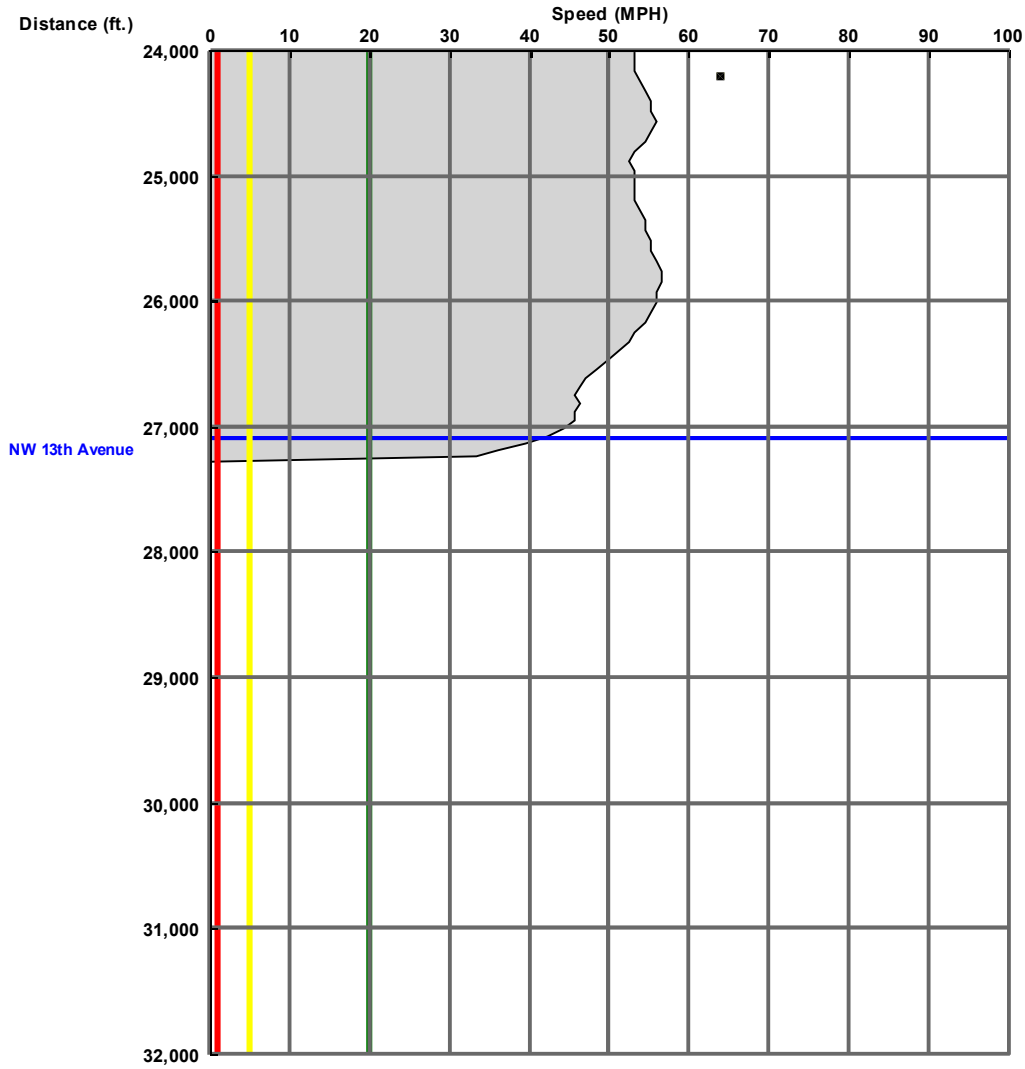
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 19

Speed Profile

Run: RUN 3 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

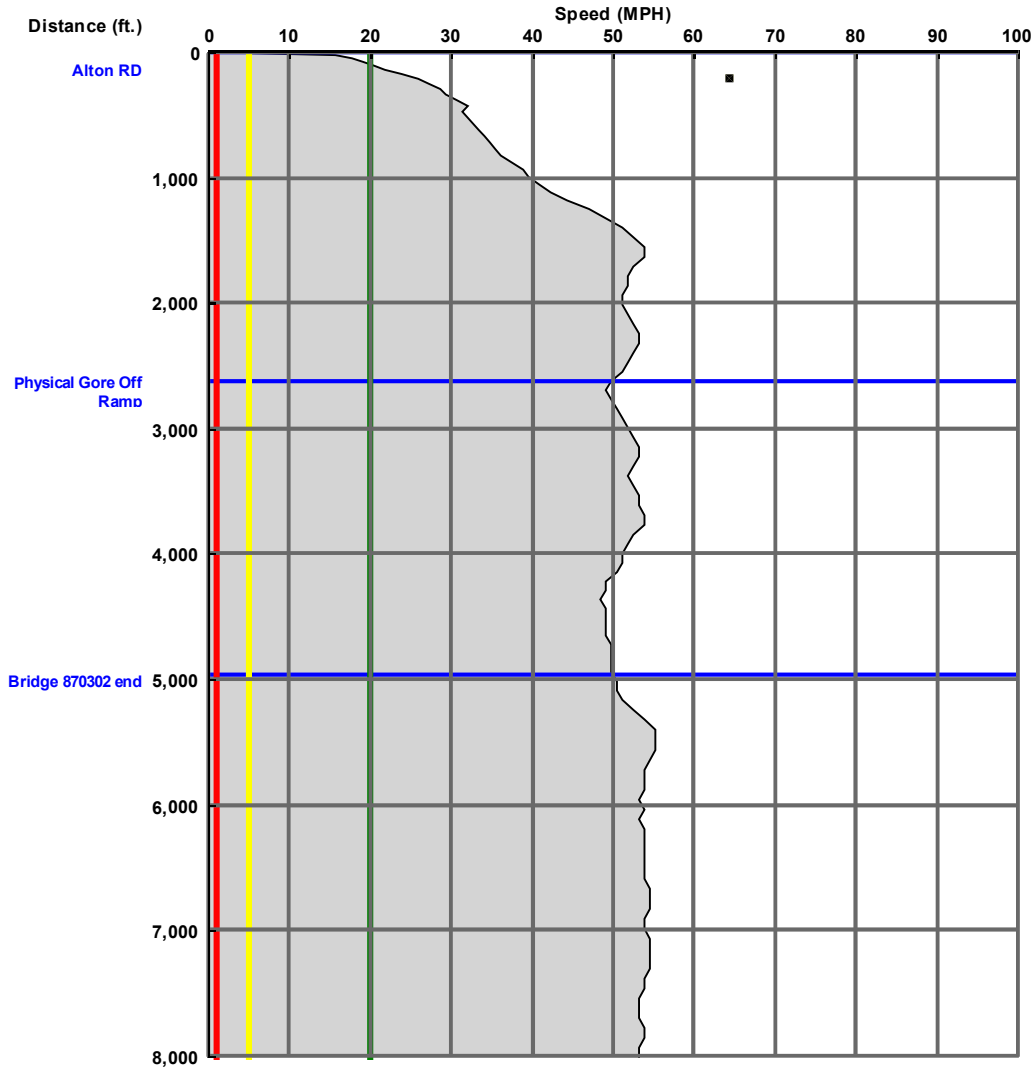
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 20

Speed Profile

Run: RUN 4 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

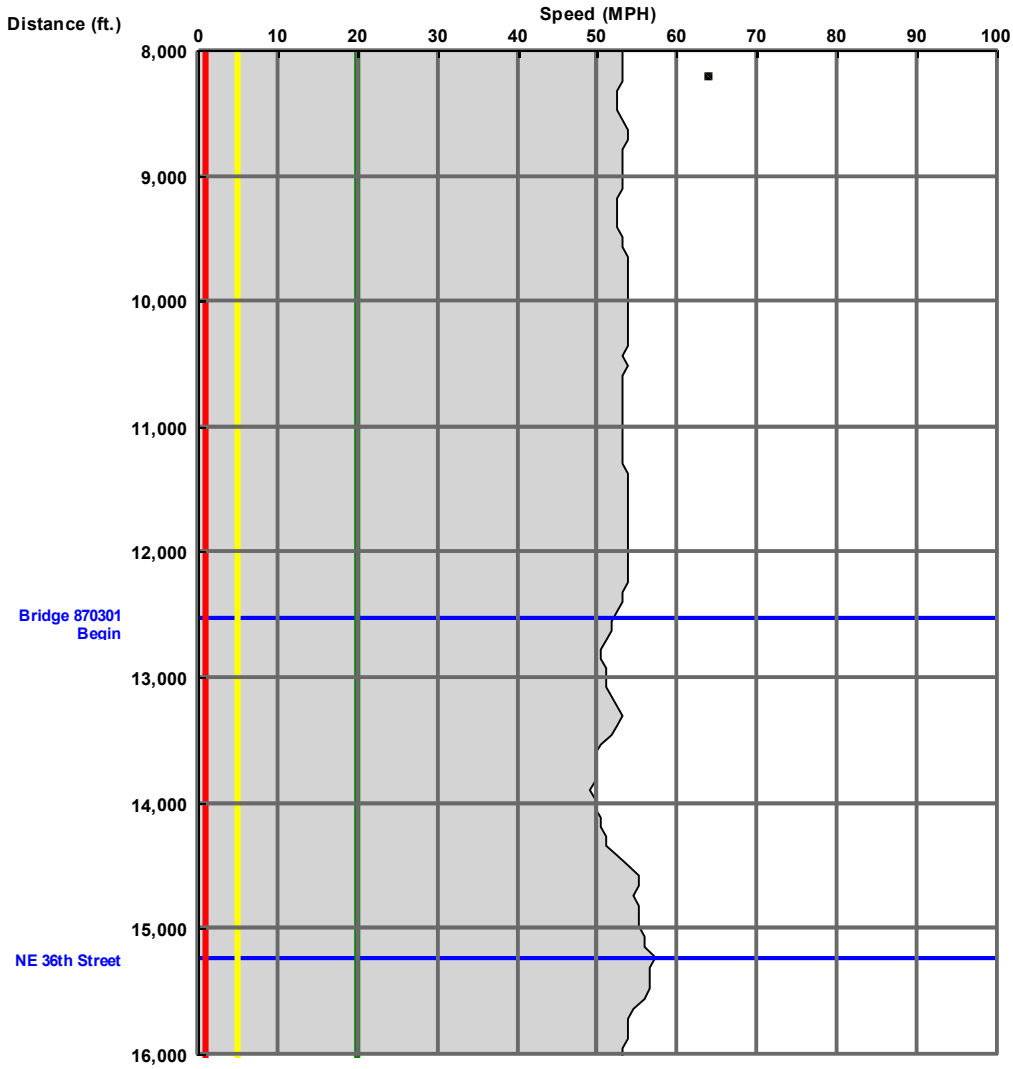
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 21

Speed Profile

Run: RUN 4 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

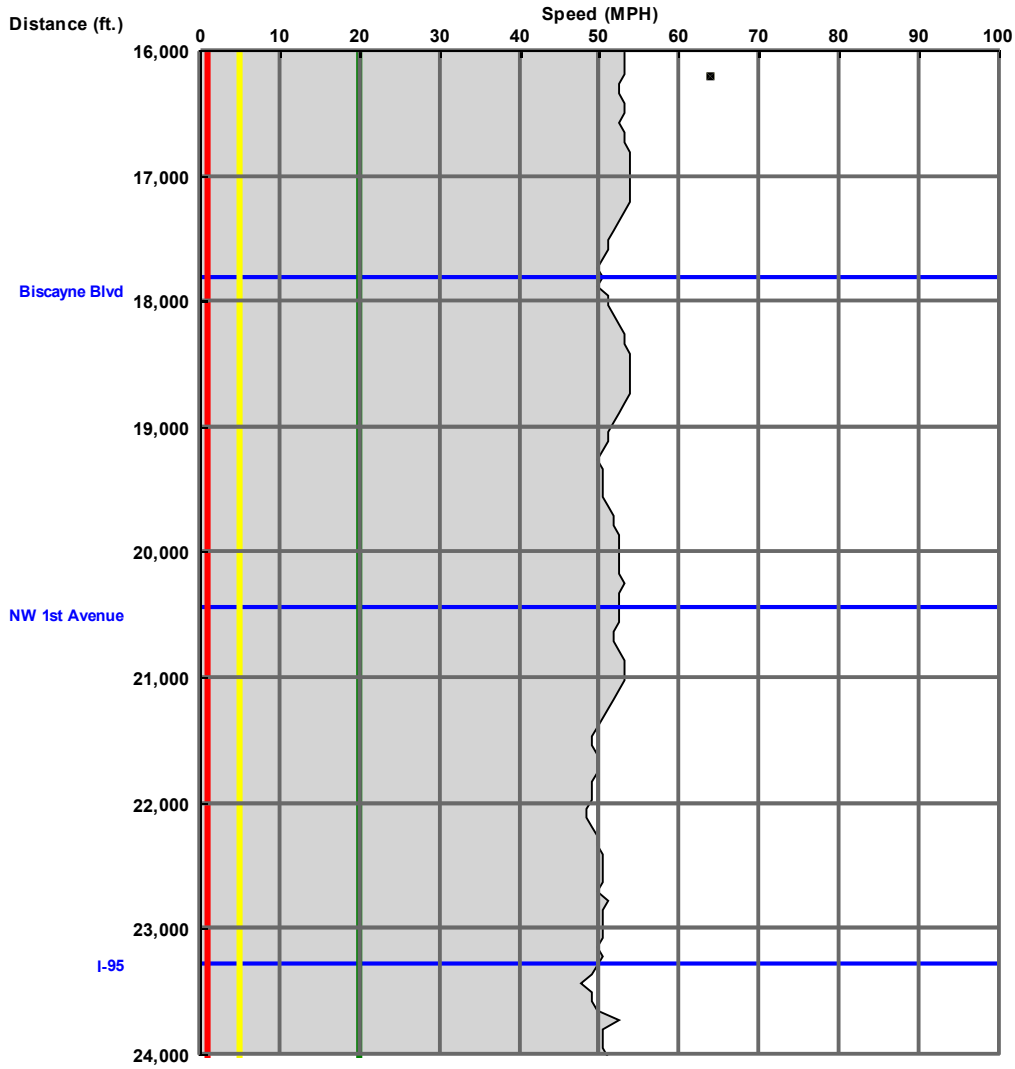
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 22

Speed Profile

Run: RUN 4 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

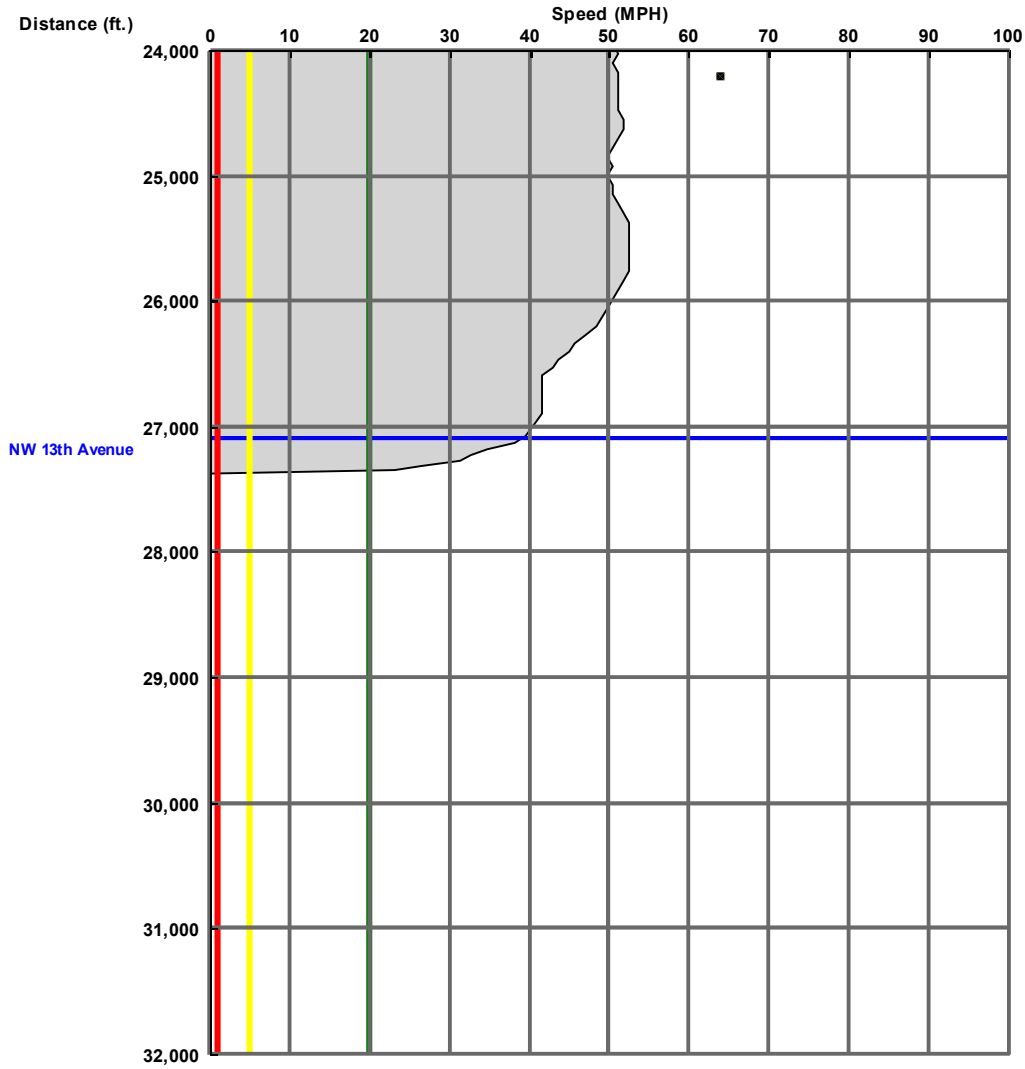
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 23

Speed Profile

Run: RUN 4 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

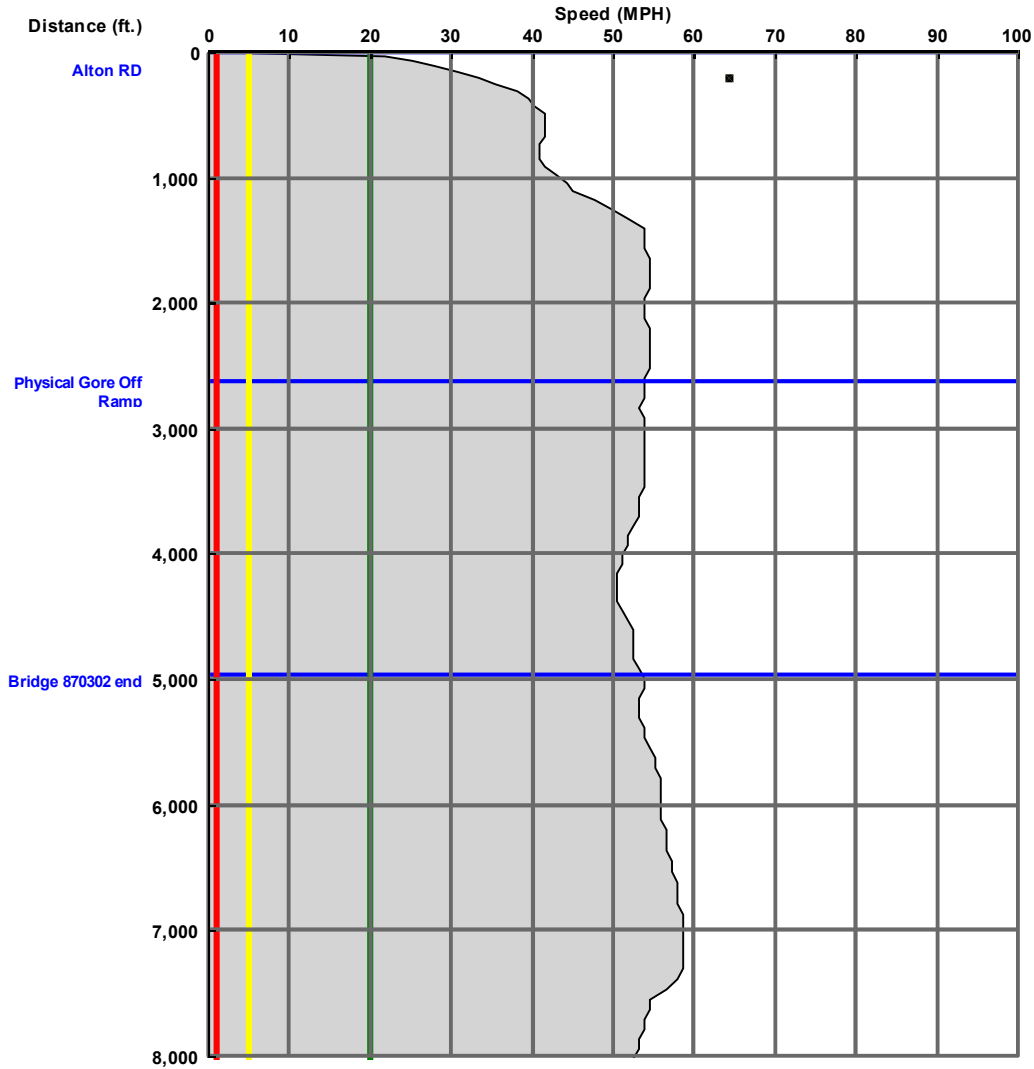
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 24

Speed Profile

Run: RUN 5 WB AM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

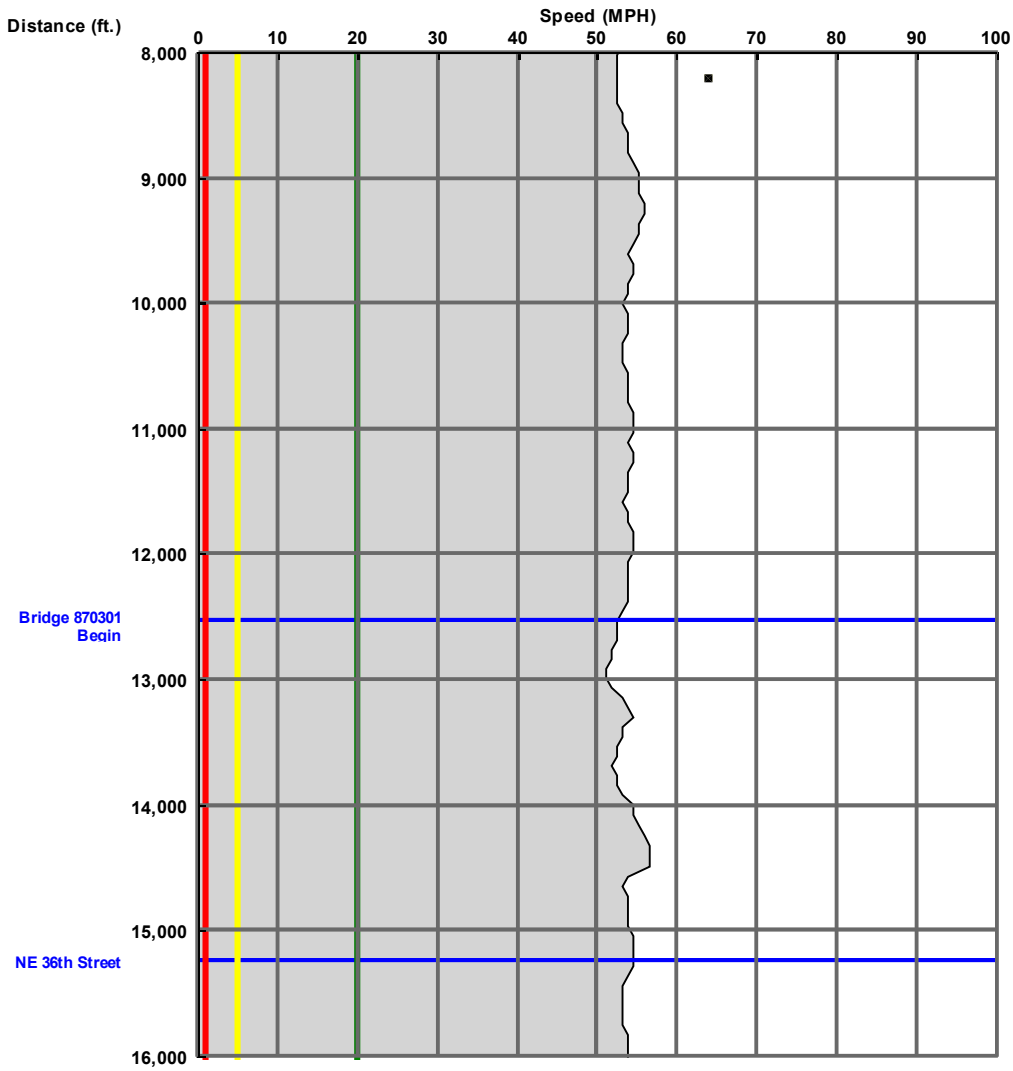
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 25

Speed Profile

Run: RUN 5 WB AM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

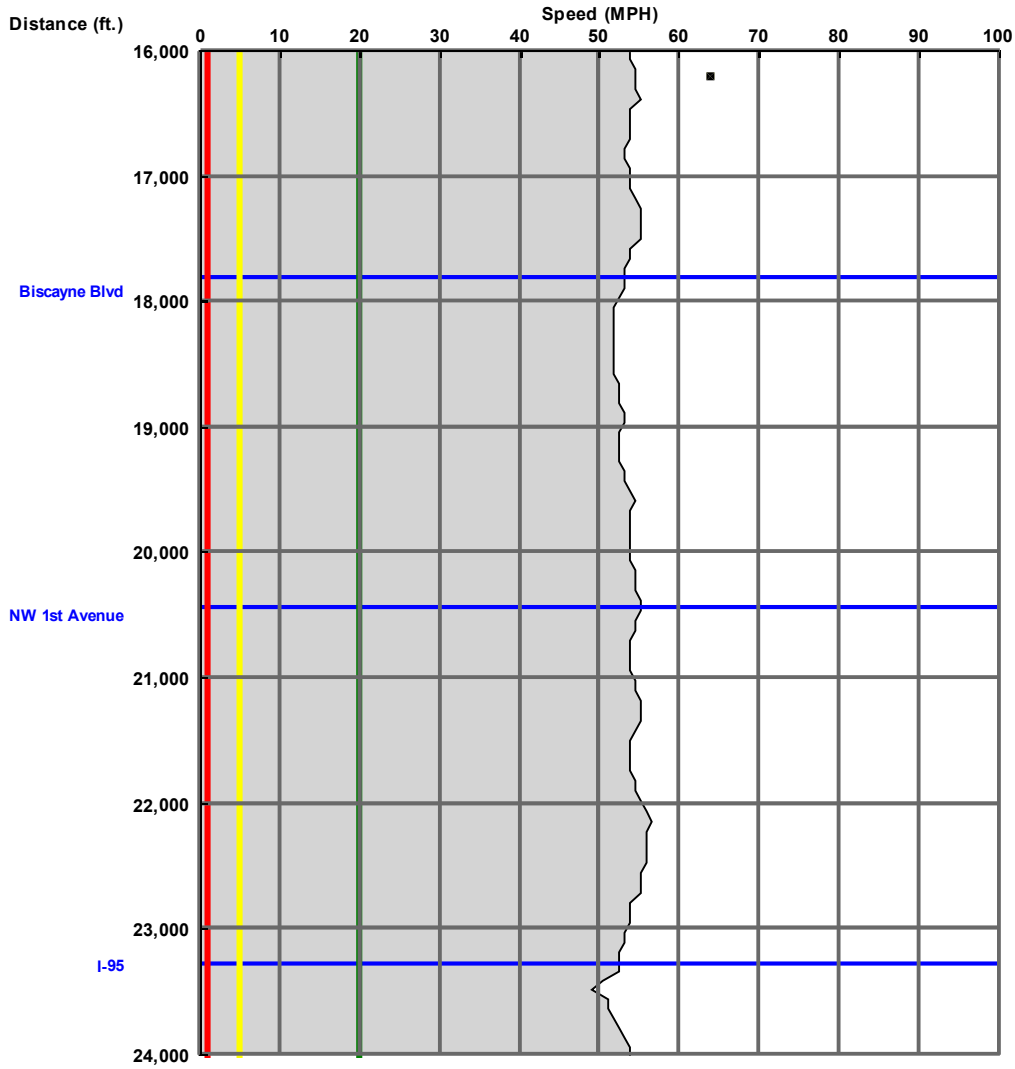
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 26

Speed Profile

Run: RUN 5 WB AM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

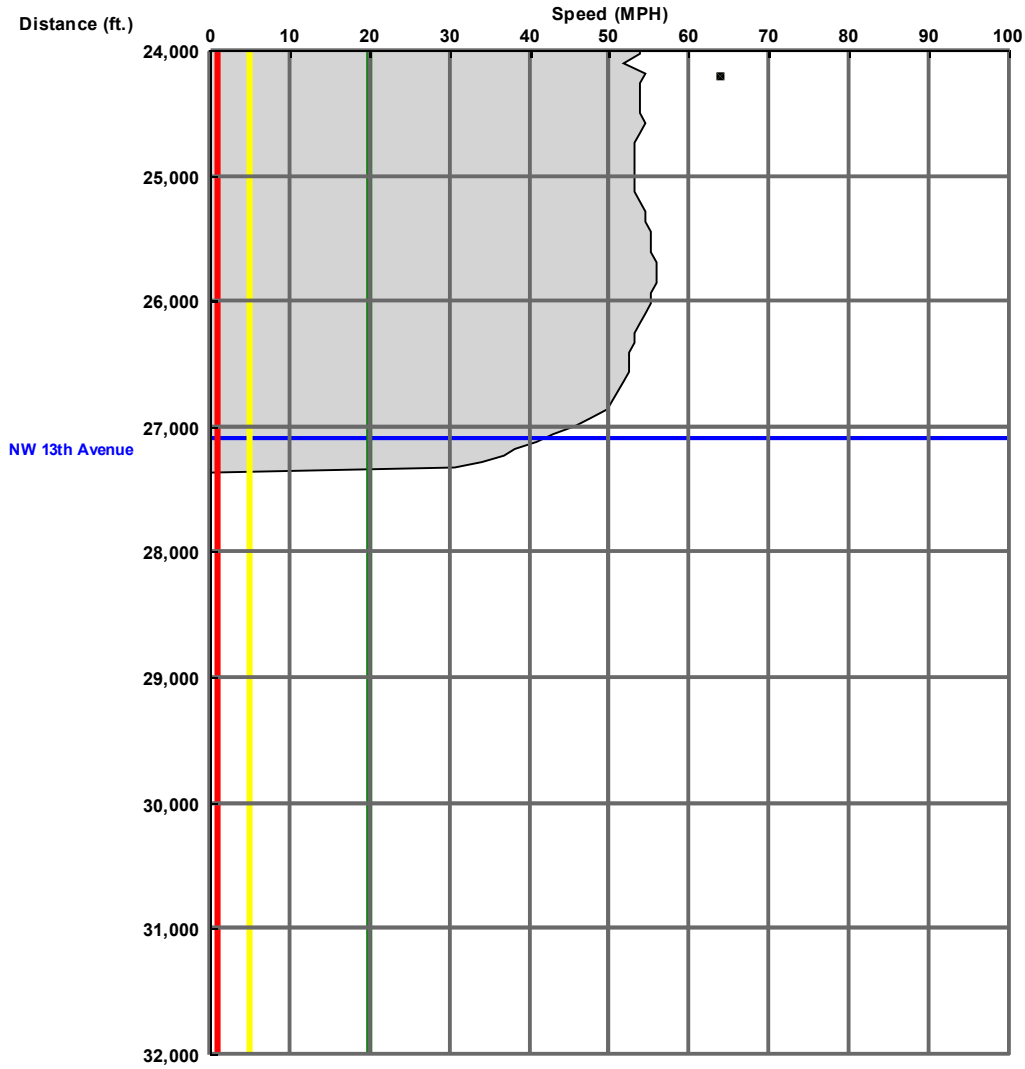
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 27

Speed Profile

Run: RUN 5 WB AM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

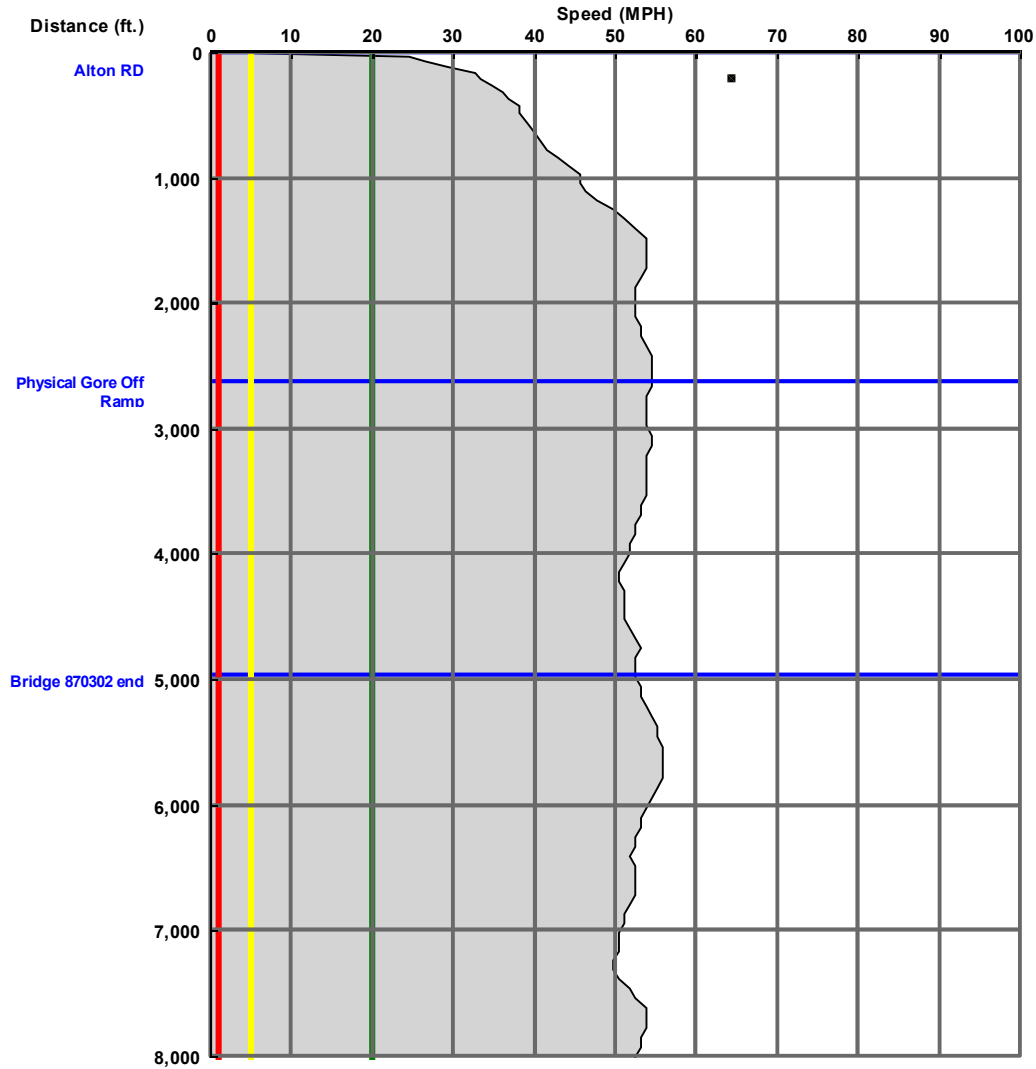
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 28

Speed Profile

Run: RUN 6 WB AM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

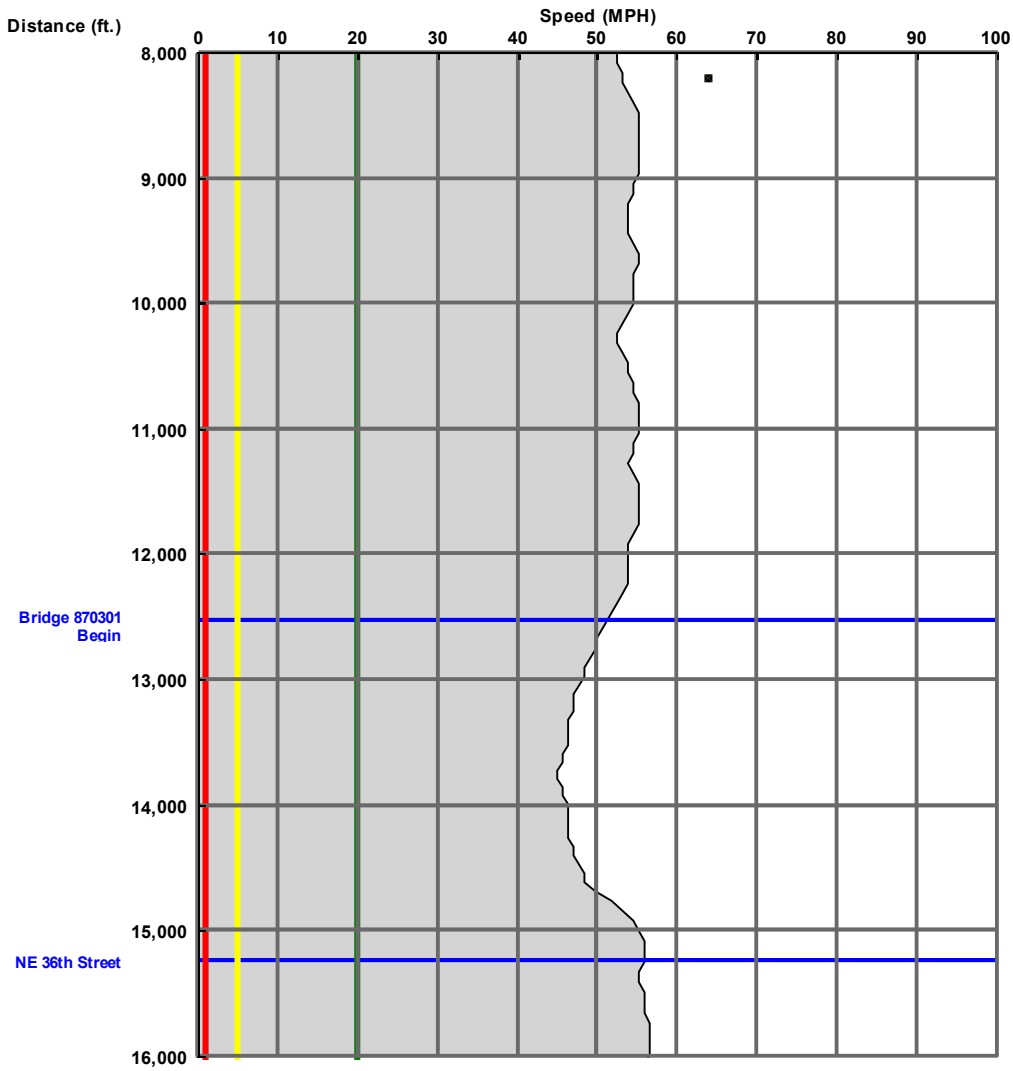
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 29

Speed Profile

Run: RUN 6 WB AM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

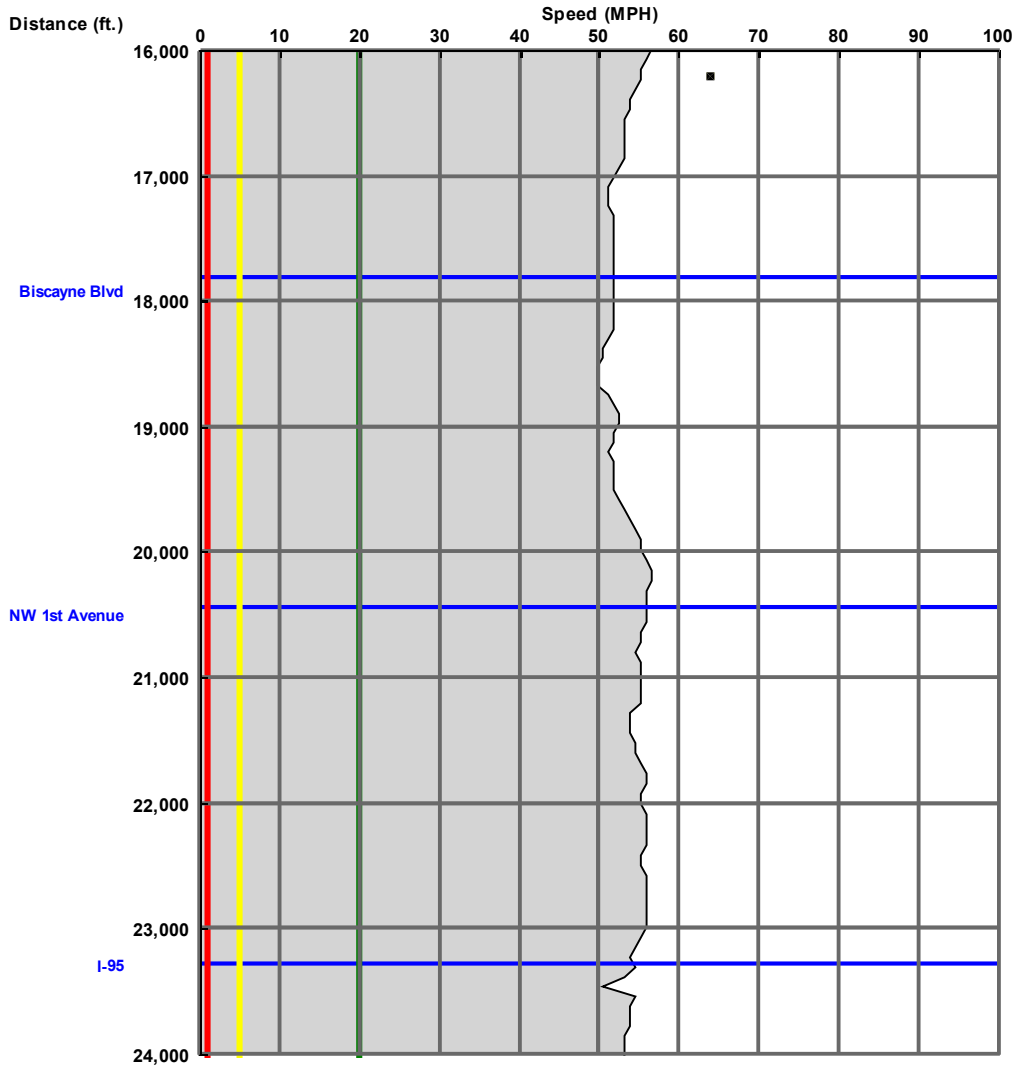
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 30

Speed Profile

Run: RUN 6 WB AM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

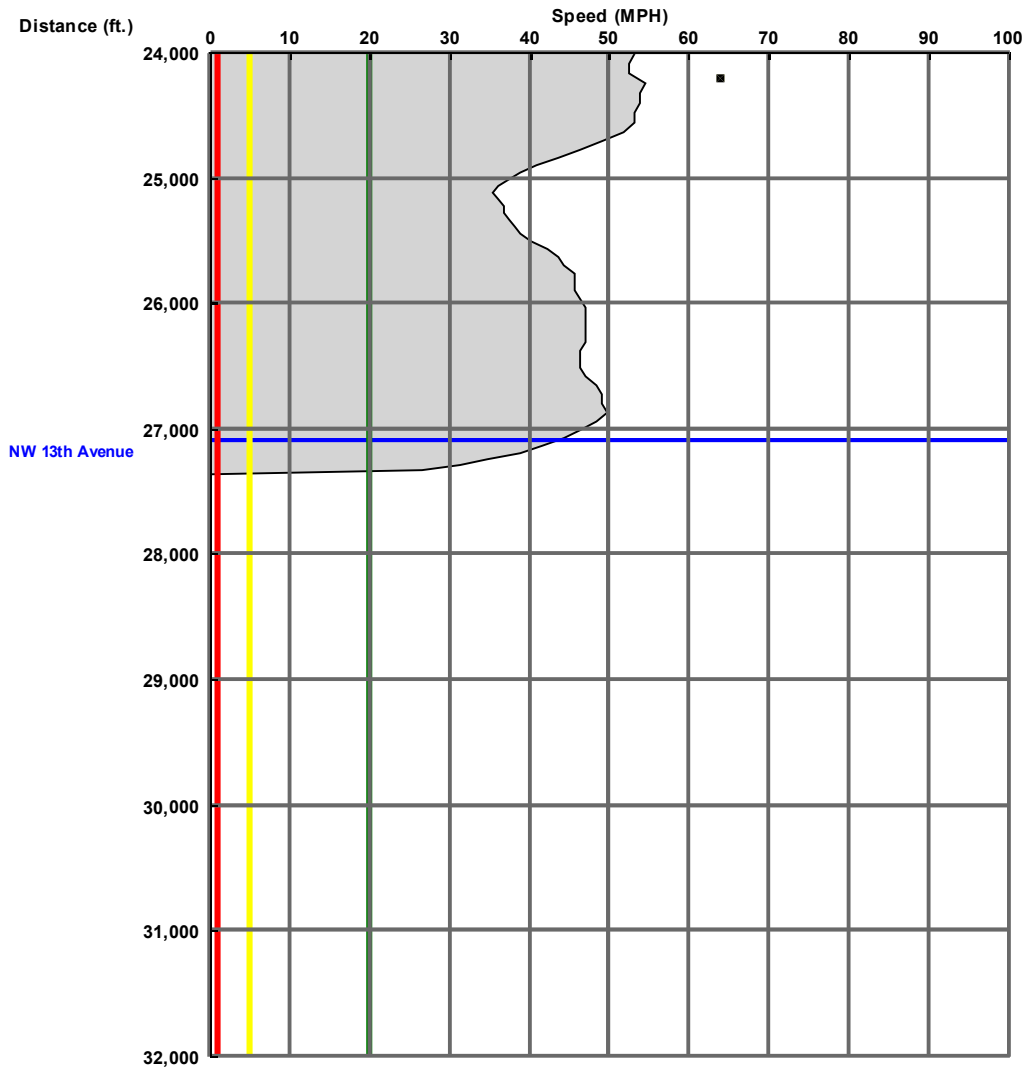
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 31

Speed Profile

Run: RUN 6 WB AM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

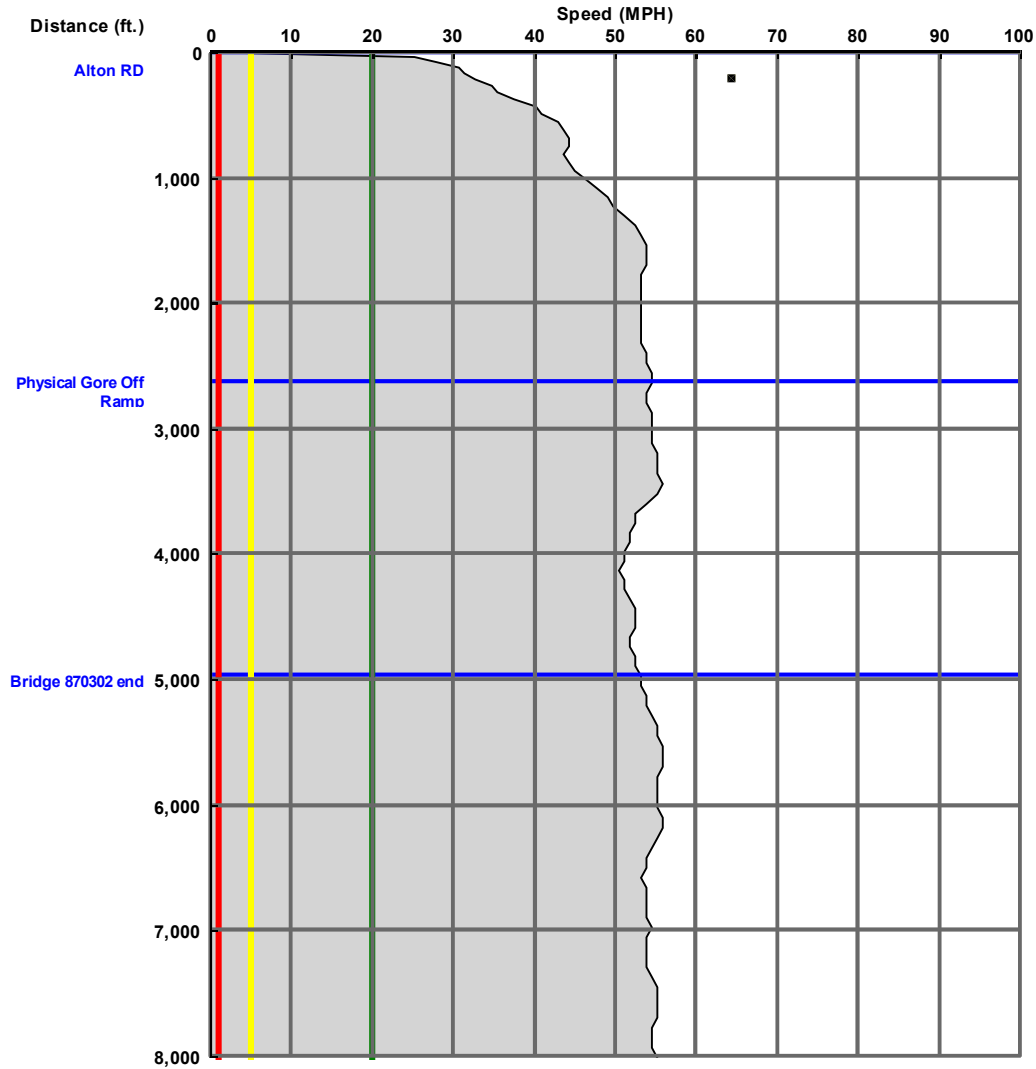
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 32

Speed Profile

Run: RUN 7 WB AM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

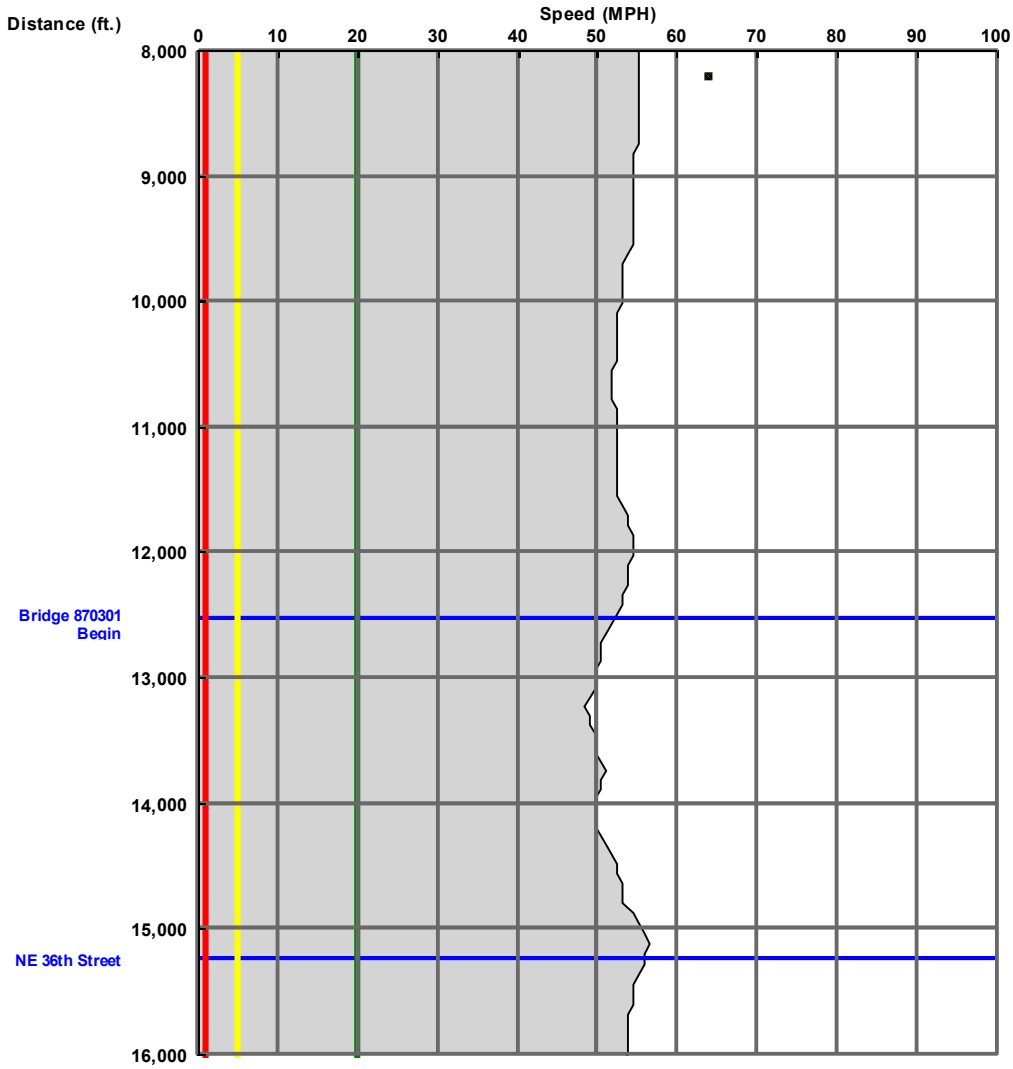
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 33

Speed Profile

Run: RUN 7 WB AM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

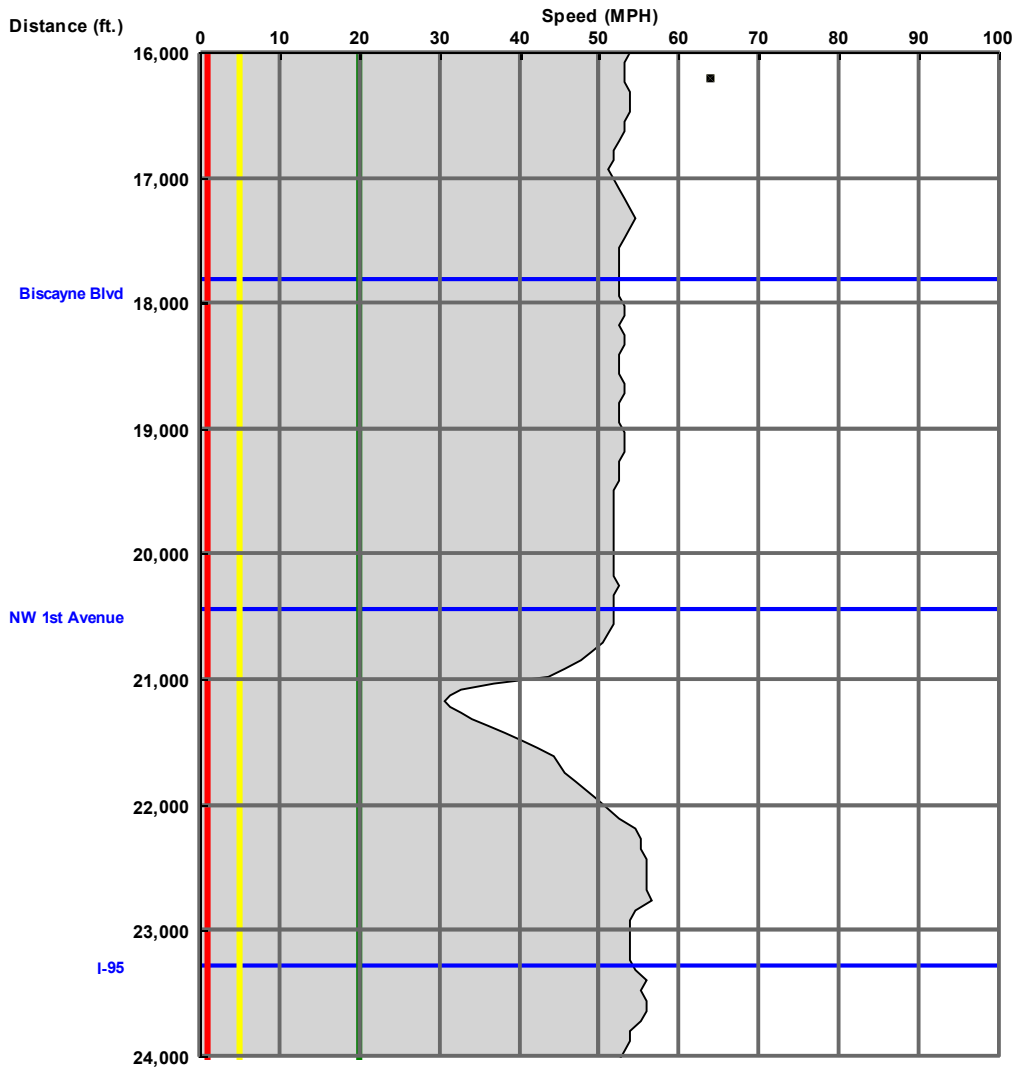
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 34

Speed Profile

Run: RUN 7 WB AM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

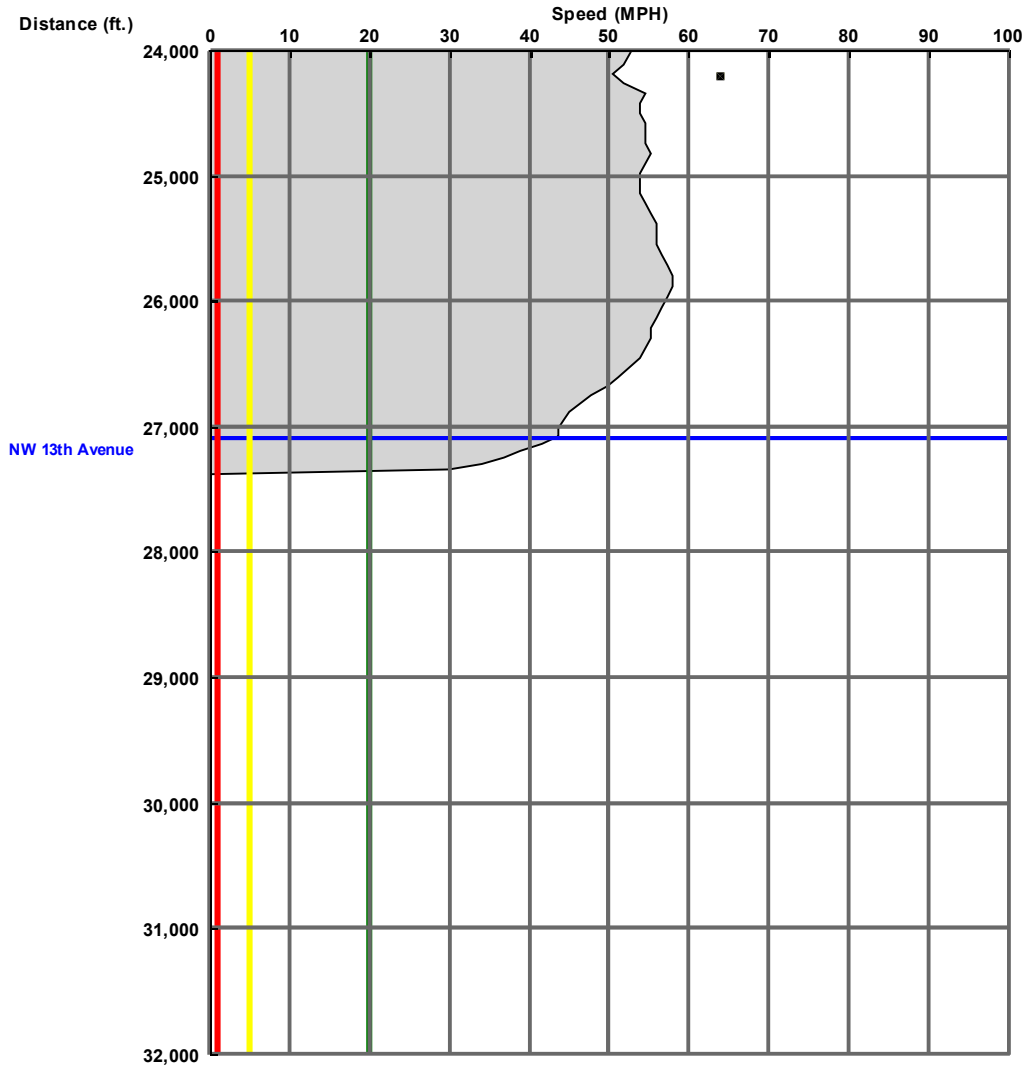
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 35

Speed Profile

Run: RUN 7 WB AM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

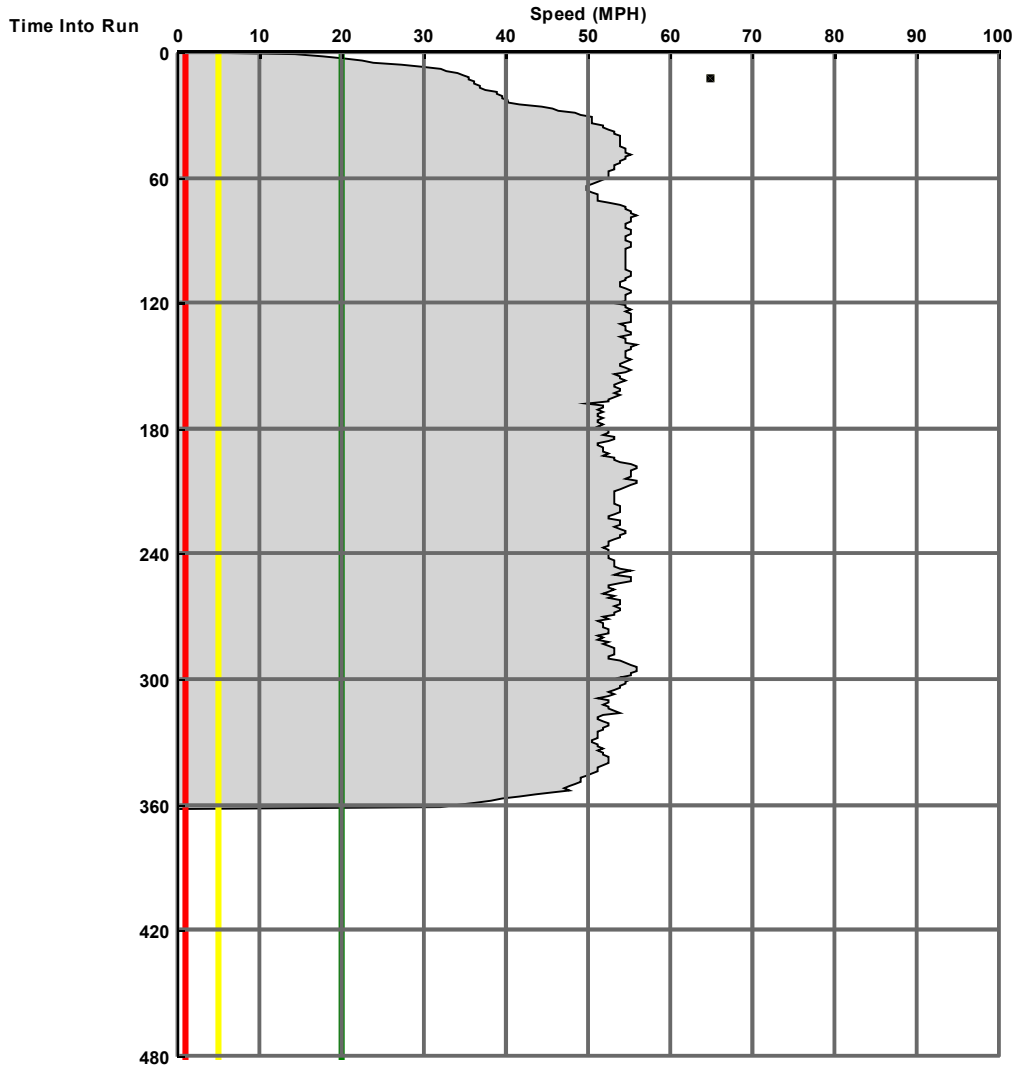
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 36

Time Based Speed Profile

Run: RUN 2 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

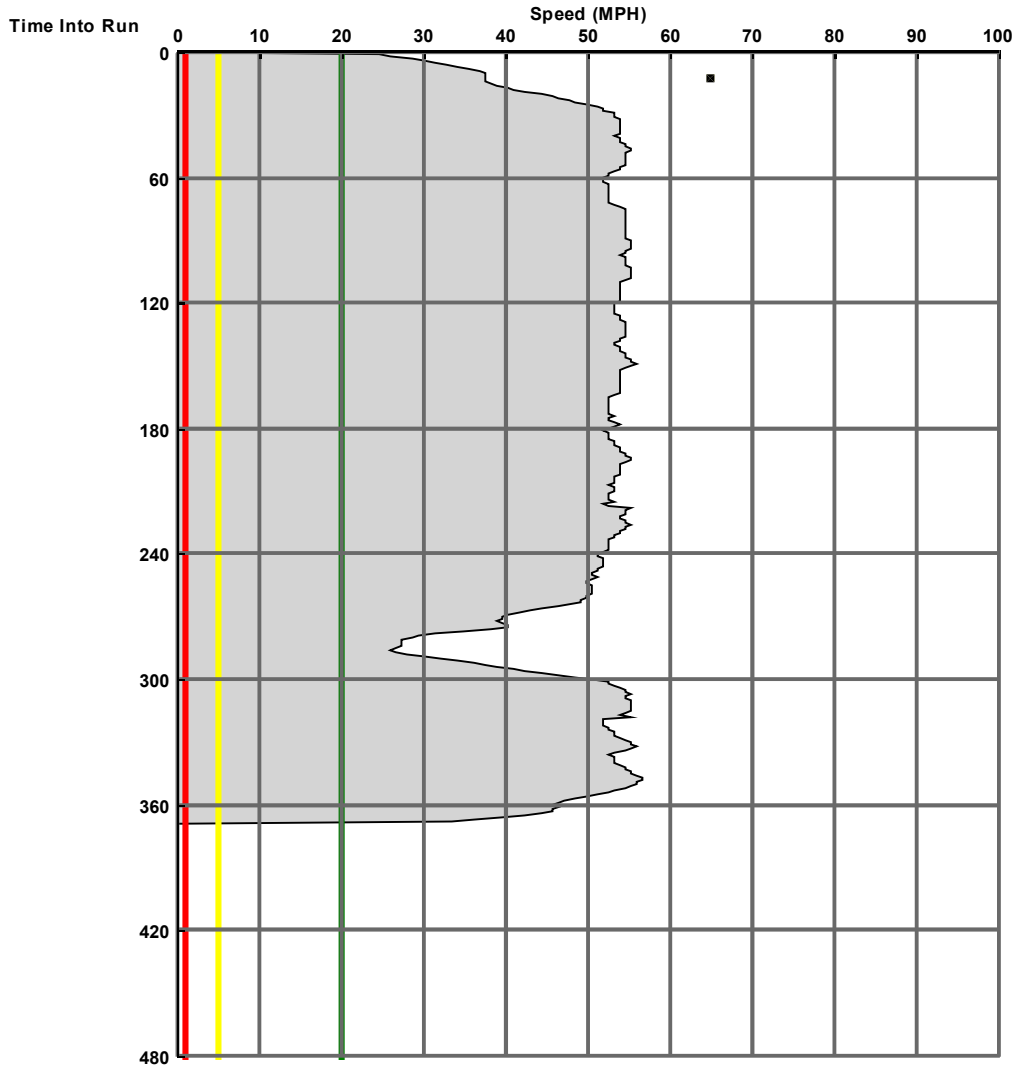
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 37

Time Based Speed Profile

Run: RUN 3 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

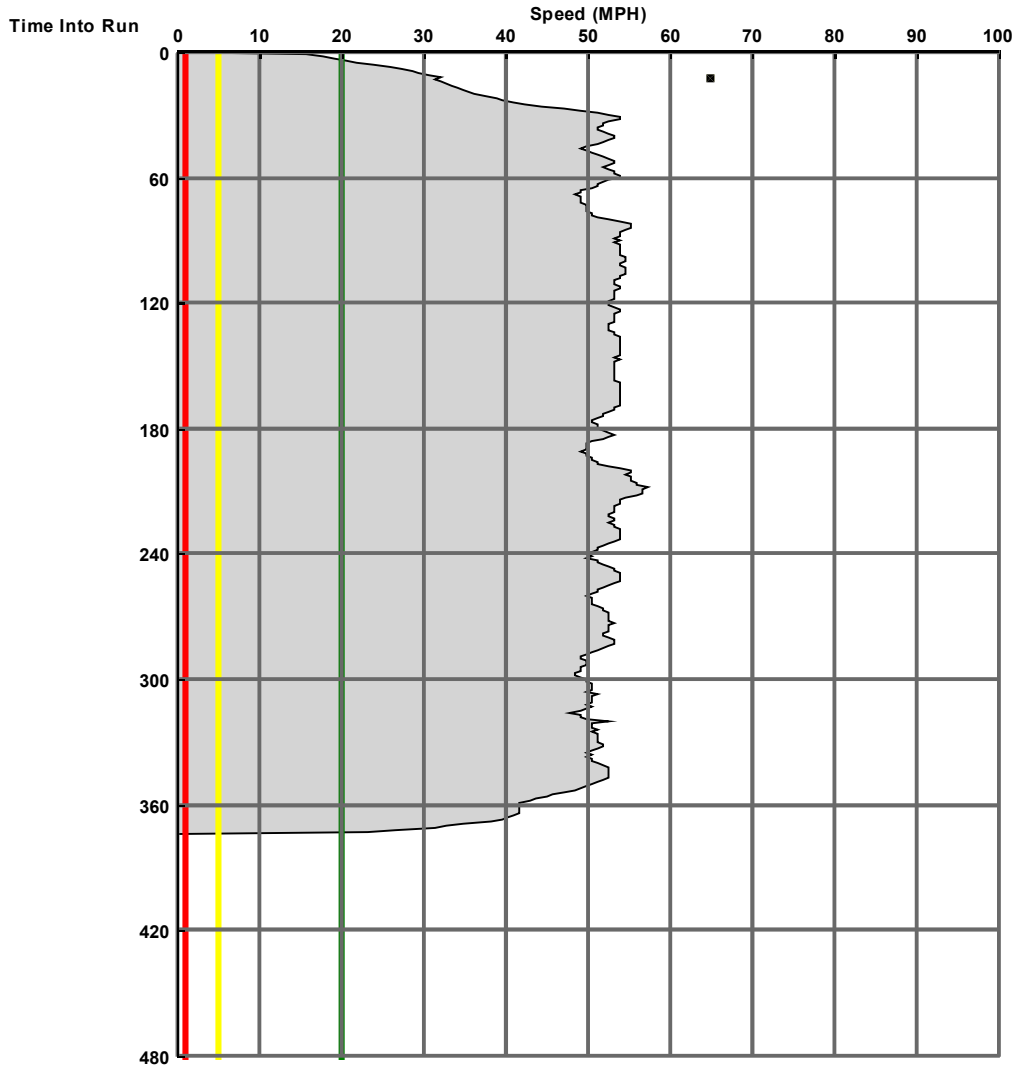
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 38

Time Based Speed Profile

Run: RUN 4 WB AM 2-14-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

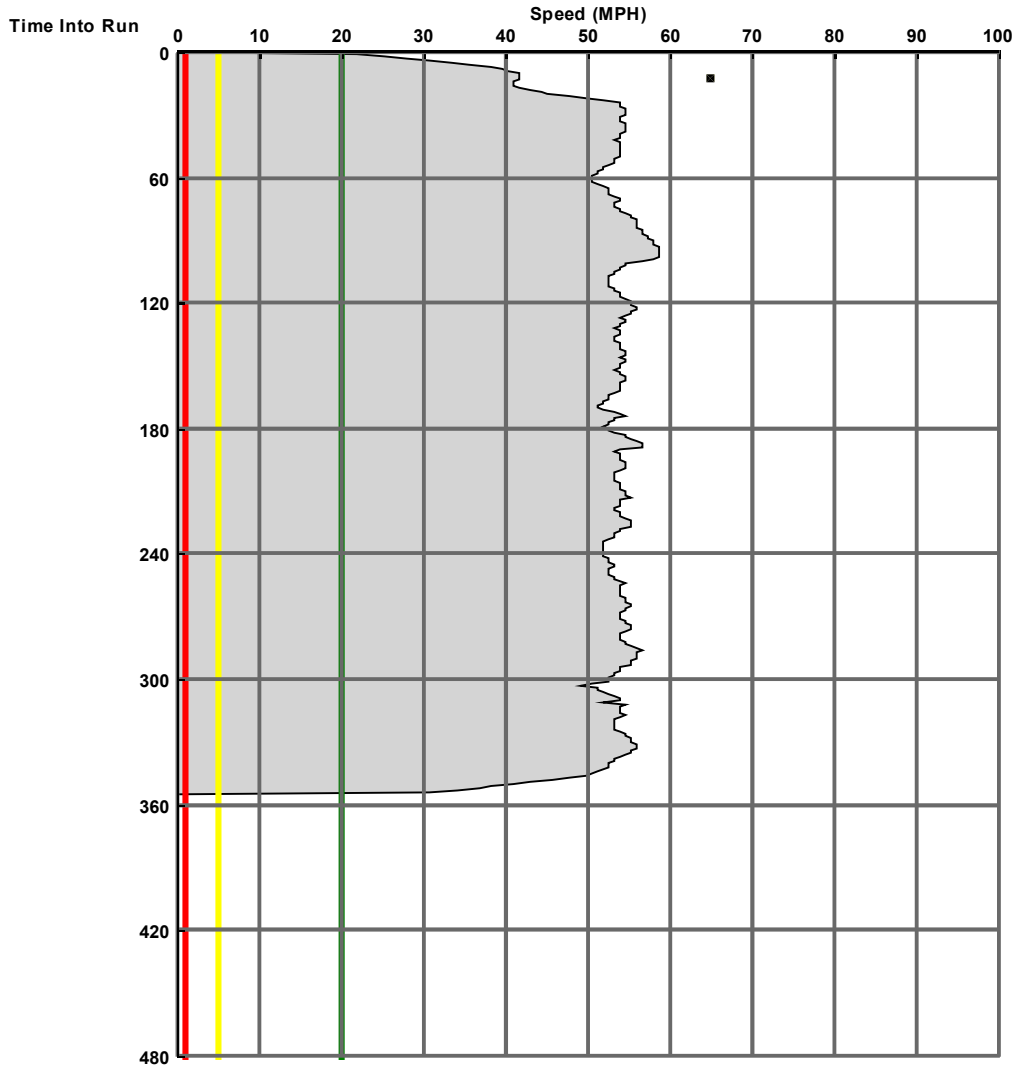
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 39

Time Based Speed Profile

Run: RUN 5 WB AM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

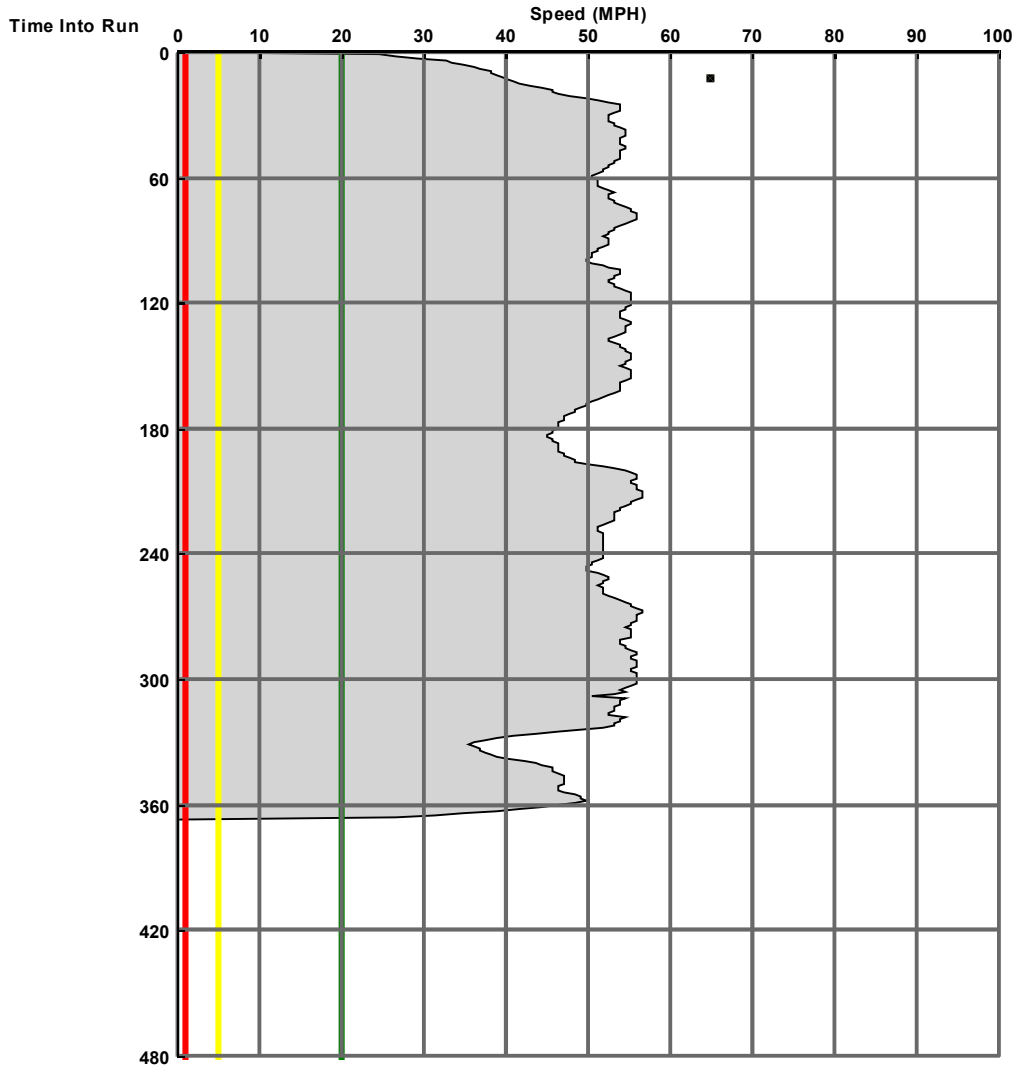
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 40

Time Based Speed Profile

Run: RUN 6 WB AM 2-15-2018-R001



I-195 WESTBOUND (AM)

Ten and Two - Travel Time Data

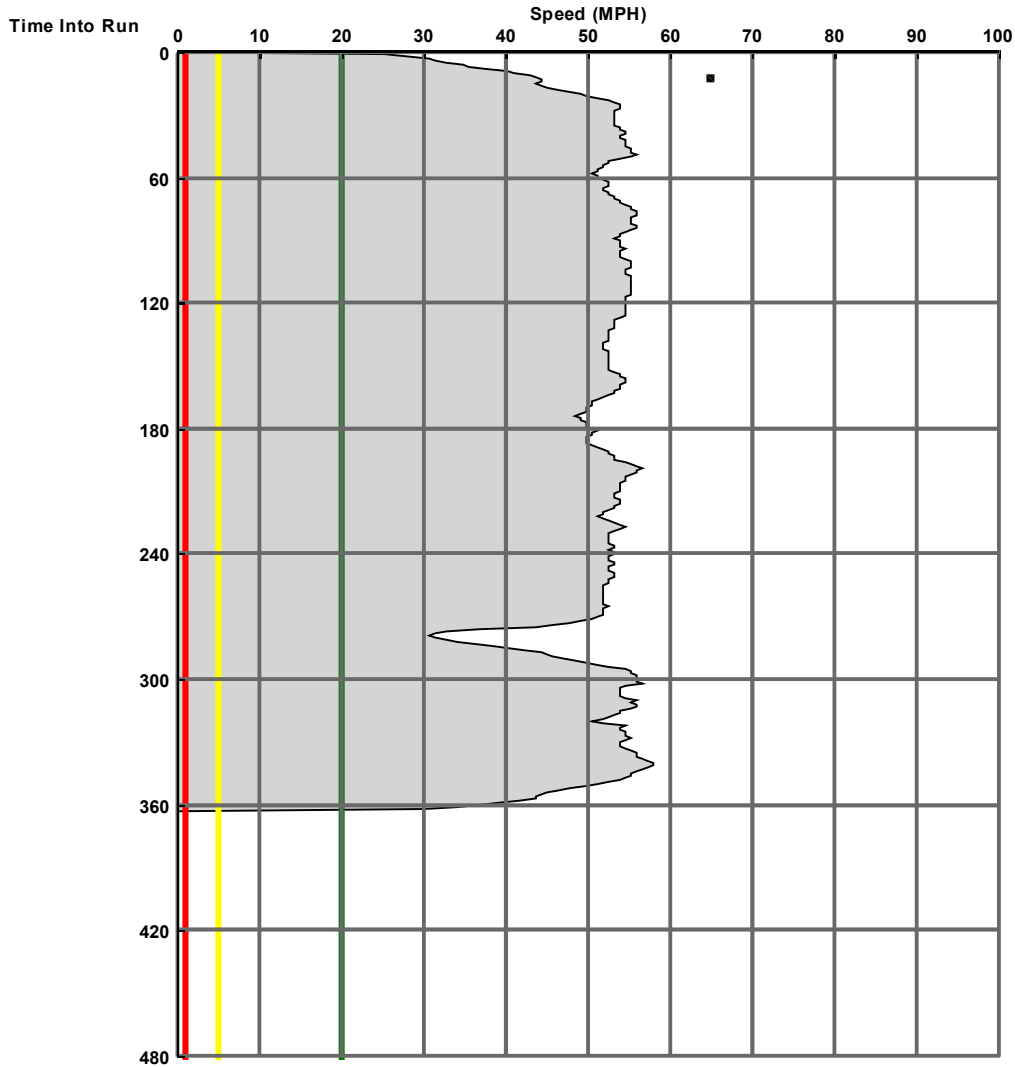
Study Name: I-195 Westbound AM

Study Date: 3/21/2018

Page No: 41

Time Based Speed Profile

Run: RUN 7 WB AM 2-15-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

Travel Time Reports for study: I-195 Westbound PM

<u>Report Name</u>	<u>Page</u>
Study Summary	2
Overall Output Statistics	3
Detailed Stats By Run - Travel Time.....	4
Detailed Stats By Run - Stops	5
Detailed Stats By Run - Average Speed	6
Detailed Stats By Run - Total Delay.....	7
Detailed Stats By Run - Time Less Than0 MPH	8
Detailed Stats By Run - Time Less Than5 MPH	9
Detailed Stats By Run - Time Less Than20 MPH	10
Speed/Distance Plot of All Runs	11
Time/Space Trajectory of All Runs.....	12
Speed Profile (Distance vs Speed) for RUN 1 WB PM 2-14-2018-R001	13
Speed Profile (Distance vs Speed) for RUN 3 WB PM 2-14-2018-R001	17
Speed Profile (Distance vs Speed) for RUN 4 WB PM 2-14-2018-R001	21
Speed Profile (Distance vs Speed) for RUN 5 WB PM 2-14-2018-R001	25
Speed Profile (Distance vs Speed) for RUN 6 WB PM 2-14-2018-R001	29
Speed Profile (Distance vs Speed) for RUN 7 WB PM 2-15-2018-R001	33
Speed Profile (Time vs Speed) for RUN 1 WB PM 2-14-2018-R001.....	37
Speed Profile (Time vs Speed) for RUN 3 WB PM 2-14-2018-R001.....	38
Speed Profile (Time vs Speed) for RUN 4 WB PM 2-14-2018-R001.....	39
Speed Profile (Time vs Speed) for RUN 5 WB PM 2-14-2018-R001.....	40
Speed Profile (Time vs Speed) for RUN 6 WB PM 2-14-2018-R001.....	41
Speed Profile (Time vs Speed) for RUN 7 WB PM 2-15-2018-R001.....	42

I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 42

Study Summary Runs Used in This Study

Run Title	Start Date	Start Time	Length	Before/After	Run Type
RUN 1 WB PM 2-14-2018-R001	02/14/18	15:09:37	27372	Before	Secondary
RUN 3 WB PM 2-14-2018-R001	02/14/18	16:08:59	27171	Before	Secondary
RUN 4 WB PM 2-14-2018-R001	02/14/18	16:38:16	27337	Before	Secondary
RUN 5 WB PM 2-14-2018-R001	02/14/18	17:13:55	27195	Before	Secondary
RUN 6 WB PM 2-14-2018-R001	02/14/18	17:46:13	27194	Before	Secondary
RUN 7 WB PM 2-15-2018-R001	02/14/18	15:11:33	27236	Before	Secondary

Notes:

Node Info

#	Length	Name
1	0	Alton RD
2	2622	Physical Gore Off Ramp
3	2339	Bridge 870302 end
4	7565	Bridge 870301 Begin
5	2710	NE 36th Street
6	2572	Biscayne Blvd
7	2630	NW 1st Avenue
8	2843	I-95
9	3811	NW 13th Avenue

Length of Study Route = 27,092 feet.

I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 43

Overall Output Statistics

Node #	Length (ft)	Node Name	Travel Time	# of Stops	Avg Speed (MPH)	Total Delay	Time <= 0 MPH	Time <= 5 MPH	Time <= 20 MPH
1	0	Alton RD							
2	2622	Physical Gore Off Ramp	89.7	0.7	19.9	44.3	5.5	7.7	54.3
3	2339	Bridge 870302 end	54.5	0.3	29.3	17.0	3.5	5.3	10.5
4	7565	Bridge 870301 Begin	161.8	0.5	31.9	37.2	6.7	8.2	33.3
5	2710	NE 36th Street	52.5	0.0	35.2	6.7	0.0	0.0	2.2
6	2572	Biscayne Blvd	68.7	0.5	25.5	29.5	0.0	1.7	41.0
7	2630	NW 1st Avenue	79.5	0.5	22.6	36.5	0.0	1.7	47.0
8	2843	I-95	102.7	1.8	18.9	55.8	3.3	14.8	69.2
9	3811	NW 13th Avenue	110.3	1.2	23.6	46.7	0.5	3.5	58.3
Total	27,092		719.7	5.5	25.7	273.7	19.5	42.8	315.8

Stats based on 6 runs.

Stops based on a Stop Speed of 5 MPH.

Total Delay based on a Normal Speed of 0 MPH.

I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 44

Travel Time

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	Alton RD						
2	2622	Physical Gore Off Ramp	80	105	100	106	97	50
3	2339	Bridge 870302 end	29	56	55	63	89	35
4	7565	Bridge 870301 Begin	170	188	201	165	145	102
5	2710	NE 36th Street	52	48	52	71	50	42
6	2572	Biscayne Blvd	32	36	60	123	126	35
7	2630	NW 1st Avenue	51	41	113	102	133	37
8	2843	I-95	54	79	57	179	210	37
9	3811	NW 13th Avenue	91	131	98	136	150	56
Total	27,092		559	684	736	945	1000	394

Run # 1 = RUN 1 WB PM 2-14-2018-R001

Run # 2 = RUN 3 WB PM 2-14-2018-R001

Run # 3 = RUN 4 WB PM 2-14-2018-R001

Run # 4 = RUN 5 WB PM 2-14-2018-R001

Run # 5 = RUN 6 WB PM 2-14-2018-R001

Run # 6 = RUN 7 WB PM 2-15-2018-R001

I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 45

Number of Stops

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	Alton RD						
2	2622	Physical Gore Off Ramp	0	0	1	2	0	1
3	2339	Bridge 870302 end	0	0	0	0	2	0
4	7565	Bridge 870301 Begin	2	1	0	0	0	0
5	2710	NE 36th Street	0	0	0	0	0	0
6	2572	Biscayne Blvd	0	0	0	2	1	0
7	2630	NW 1st Avenue	0	0	0	0	3	0
8	2843	I-95	0	1	0	4	6	0
9	3811	NW 13th Avenue	0	2	0	1	4	0
Total	27,092		2	4	1	9	16	1

Stops based on a Stop Speed of 5 MPH.

Run # 1 = RUN 1 WB PM 2-14-2018-R001

Run # 2 = RUN 3 WB PM 2-14-2018-R001

Run # 3 = RUN 4 WB PM 2-14-2018-R001

Run # 4 = RUN 5 WB PM 2-14-2018-R001

Run # 5 = RUN 6 WB PM 2-14-2018-R001

Run # 6 = RUN 7 WB PM 2-15-2018-R001

I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 46

Average Speed (MPH)

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	Alton RD	0.0	0.0	0.0	0.0	0.0	0.0
2	2622	Physical Gore Off Ramp	22.7	17.2	17.9	17.0	18.6	36.8
3	2339	Bridge 870302 end	54.1	28.6	29.5	25.3	17.9	44.9
4	7565	Bridge 870301 Begin	30.4	27.3	25.6	31.4	35.5	50.7
5	2710	NE 36th Street	36.3	38.5	35.5	25.8	36.8	44.1
6	2572	Biscayne Blvd	53.6	49.6	29.1	14.3	14.0	48.7
7	2630	NW 1st Avenue	35.1	43.7	15.9	17.5	13.4	49.4
8	2843	I-95	36.2	24.3	34.4	10.8	9.2	51.5
9	3811	NW 13th Avenue	28.4	20.0	26.3	19.2	17.4	46.6
Total	27,092		33.1	27.1	25.1	19.6	18.5	46.9

Run # 1 = RUN 1 WB PM 2-14-2018-R001

Run # 2 = RUN 3 WB PM 2-14-2018-R001

Run # 3 = RUN 4 WB PM 2-14-2018-R001

Run # 4 = RUN 5 WB PM 2-14-2018-R001

Run # 5 = RUN 6 WB PM 2-14-2018-R001

Run # 6 = RUN 7 WB PM 2-15-2018-R001

I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 47

Total Delay

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	Alton RD	0	0	0	0	0	0
2	2622	Physical Gore Off Ramp	34	60	55	61	52	4
3	2339	Bridge 870302 end	0	16	14	23	49	0
4	7565	Bridge 870301 Begin	41	59	72	35	16	0
5	2710	NE 36th Street	5	2	5	24	4	0
6	2572	Biscayne Blvd	0	0	16	79	82	0
7	2630	NW 1st Avenue	6	0	68	57	88	0
8	2843	I-95	5	30	8	130	162	0
9	3811	NW 13th Avenue	26	65	33	71	85	0
Total	27,092		117	232	271	480	538	4

Total Delay based on a Normal Speed of 0 MPH.

Run # 1 = RUN 1 WB PM 2-14-2018-R001

Run # 2 = RUN 3 WB PM 2-14-2018-R001

Run # 3 = RUN 4 WB PM 2-14-2018-R001

Run # 4 = RUN 5 WB PM 2-14-2018-R001

Run # 5 = RUN 6 WB PM 2-14-2018-R001

Run # 6 = RUN 7 WB PM 2-15-2018-R001

I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 48

Time Below

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	Alton RD	0	0	0	0	0	0
2	2622	Physical Gore Off Ramp	29	0	0	4	0	0
3	2339	Bridge 870302 end	0	0	0	0	21	0
4	7565	Bridge 870301 Begin	14	26	0	0	0	0
5	2710	NE 36th Street	0	0	0	0	0	0
6	2572	Biscayne Blvd	0	0	0	0	0	0
7	2630	NW 1st Avenue	0	0	0	0	0	0
8	2843	I-95	0	0	0	8	12	0
9	3811	NW 13th Avenue	0	0	0	3	0	0
Total	27,092		43	26	0	15	33	0

Run # 1 = RUN 1 WB PM 2-14-2018-R001

Run # 2 = RUN 3 WB PM 2-14-2018-R001

Run # 3 = RUN 4 WB PM 2-14-2018-R001

Run # 4 = RUN 5 WB PM 2-14-2018-R001

Run # 5 = RUN 6 WB PM 2-14-2018-R001

Run # 6 = RUN 7 WB PM 2-15-2018-R001

I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 49

Time Below

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	Alton RD	0	0	0	0	0	0
2	2622	Physical Gore Off Ramp	29	0	2	11	0	4
3	2339	Bridge 870302 end	0	0	0	0	32	0
4	7565	Bridge 870301 Begin	20	29	0	0	0	0
5	2710	NE 36th Street	0	0	0	0	0	0
6	2572	Biscayne Blvd	0	0	0	8	2	0
7	2630	NW 1st Avenue	0	0	0	0	10	0
8	2843	I-95	0	4	0	39	46	0
9	3811	NW 13th Avenue	0	3	0	8	10	0
Total	27,092		49	36	2	66	100	4

Run # 1 = RUN 1 WB PM 2-14-2018-R001

Run # 2 = RUN 3 WB PM 2-14-2018-R001

Run # 3 = RUN 4 WB PM 2-14-2018-R001

Run # 4 = RUN 5 WB PM 2-14-2018-R001

Run # 5 = RUN 6 WB PM 2-14-2018-R001

Run # 6 = RUN 7 WB PM 2-15-2018-R001

I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 50

Time Below

Node #	Length	Node Name	Run # 1	Run # 2	Run # 3	Run # 4	Run # 5	Run # 6
1	0	Alton RD	0	0	0	0	0	0
2	2622	Physical Gore Off Ramp	40	85	62	66	63	10
3	2339	Bridge 870302 end	0	5	0	12	46	0
4	7565	Bridge 870301 Begin	38	55	65	23	19	0
5	2710	NE 36th Street	0	0	0	13	0	0
6	2572	Biscayne Blvd	0	0	19	106	121	0
7	2630	NW 1st Avenue	0	0	108	68	106	0
8	2843	I-95	0	34	0	174	207	0
9	3811	NW 13th Avenue	27	87	36	89	111	0
Total	27,092		105	266	290	551	673	10

Run # 1 = RUN 1 WB PM 2-14-2018-R001

Run # 2 = RUN 3 WB PM 2-14-2018-R001

Run # 3 = RUN 4 WB PM 2-14-2018-R001

Run # 4 = RUN 5 WB PM 2-14-2018-R001

Run # 5 = RUN 6 WB PM 2-14-2018-R001

Run # 6 = RUN 7 WB PM 2-15-2018-R001

I-195 WESTBOUND (PM)

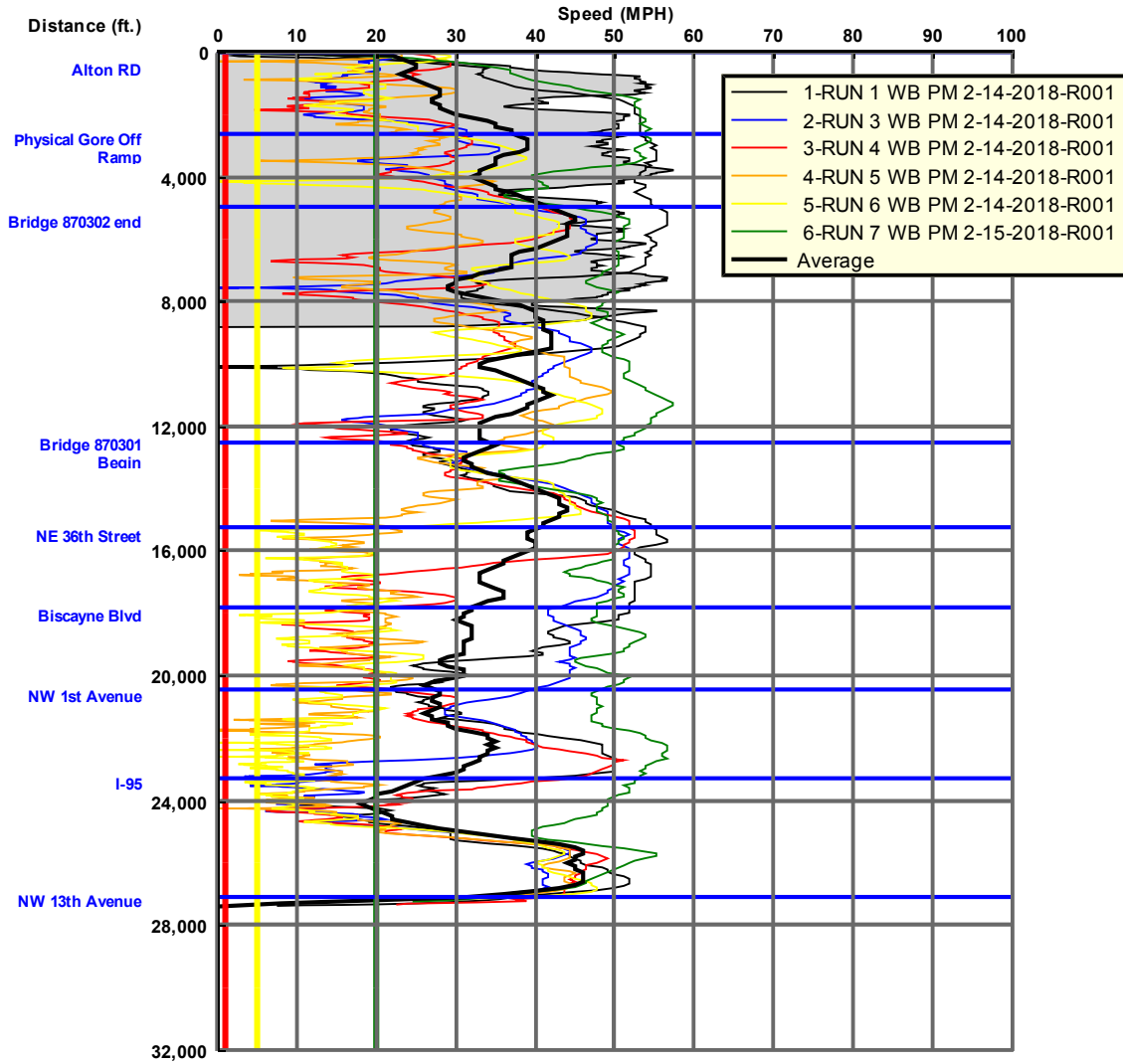
Ten and Two - Travel Time Data

Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 51

Speed/Distance Profiles of All Runs



I-195 WESTBOUND (PM)

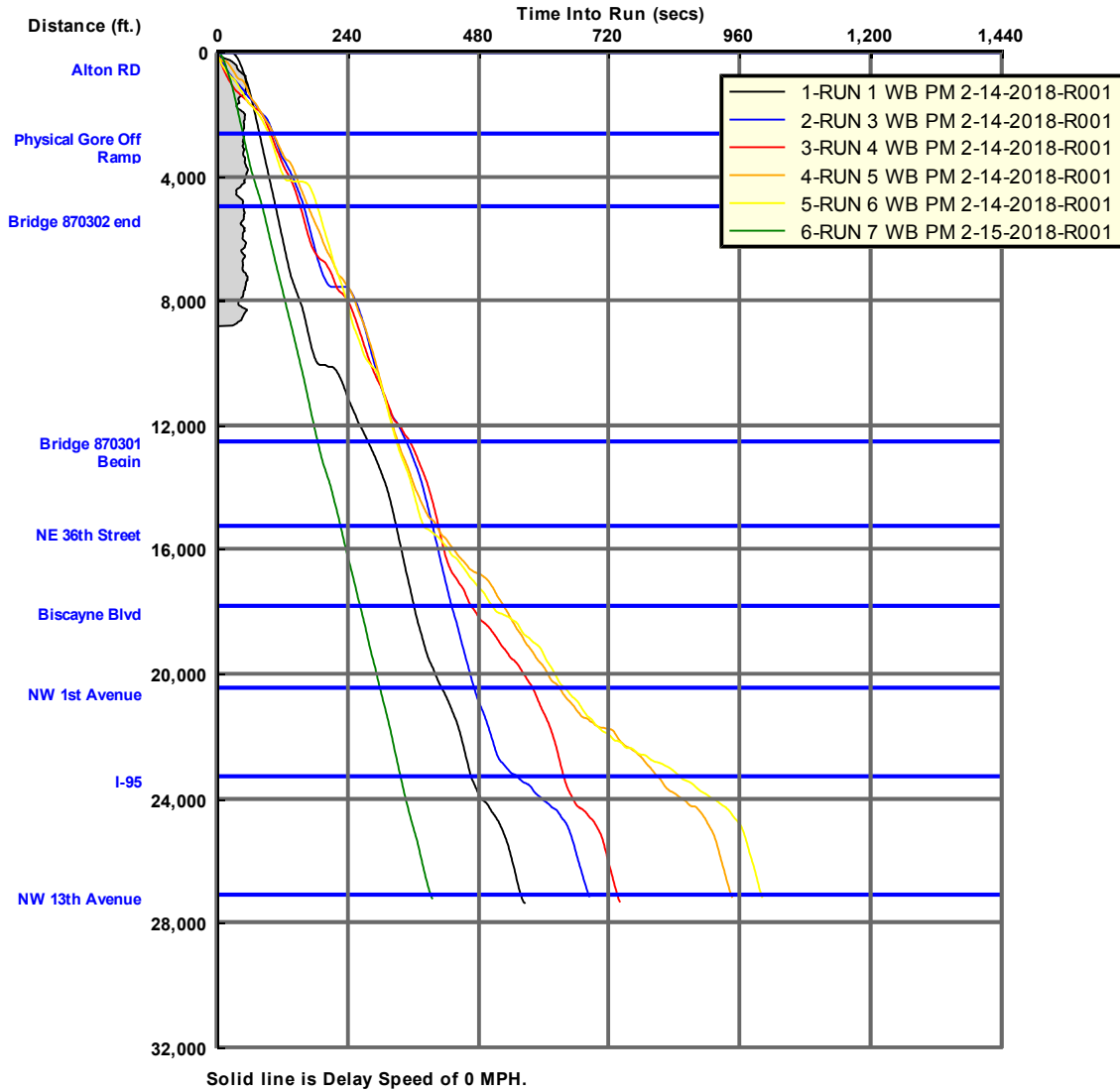
Ten and Two - Travel Time Data

Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 52

Space/Time Trajectory of All Runs



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

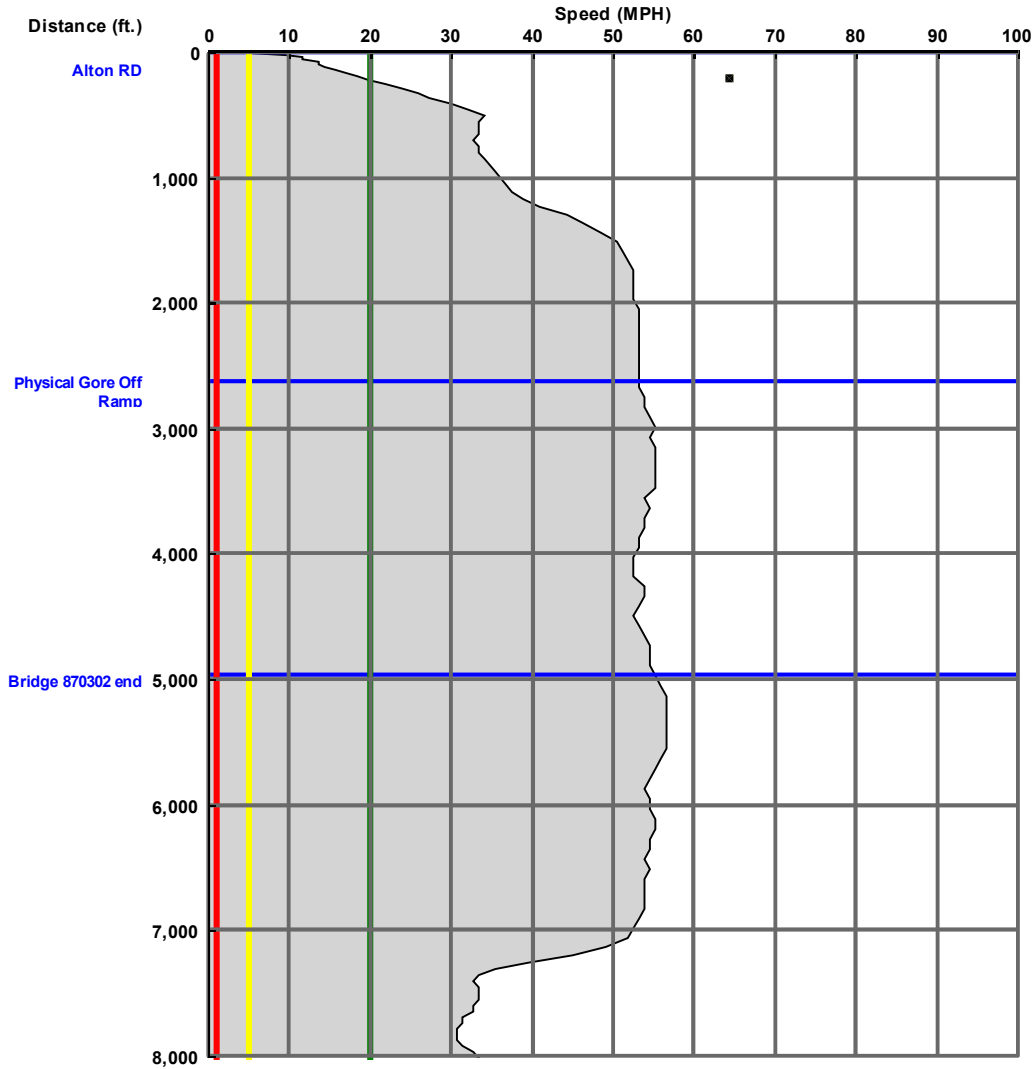
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 53

Speed Profile

Run: RUN 1 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

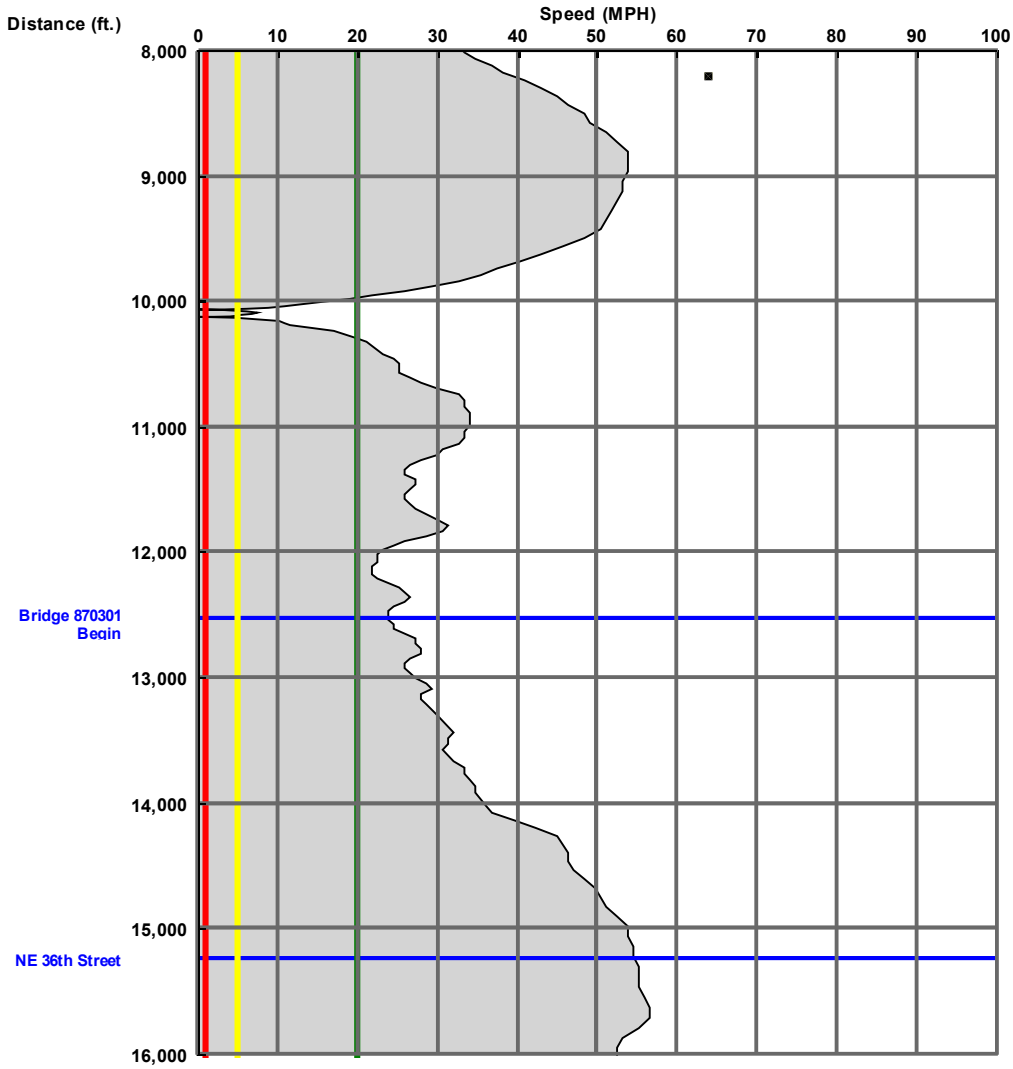
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 54

Speed Profile

Run: RUN 1 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

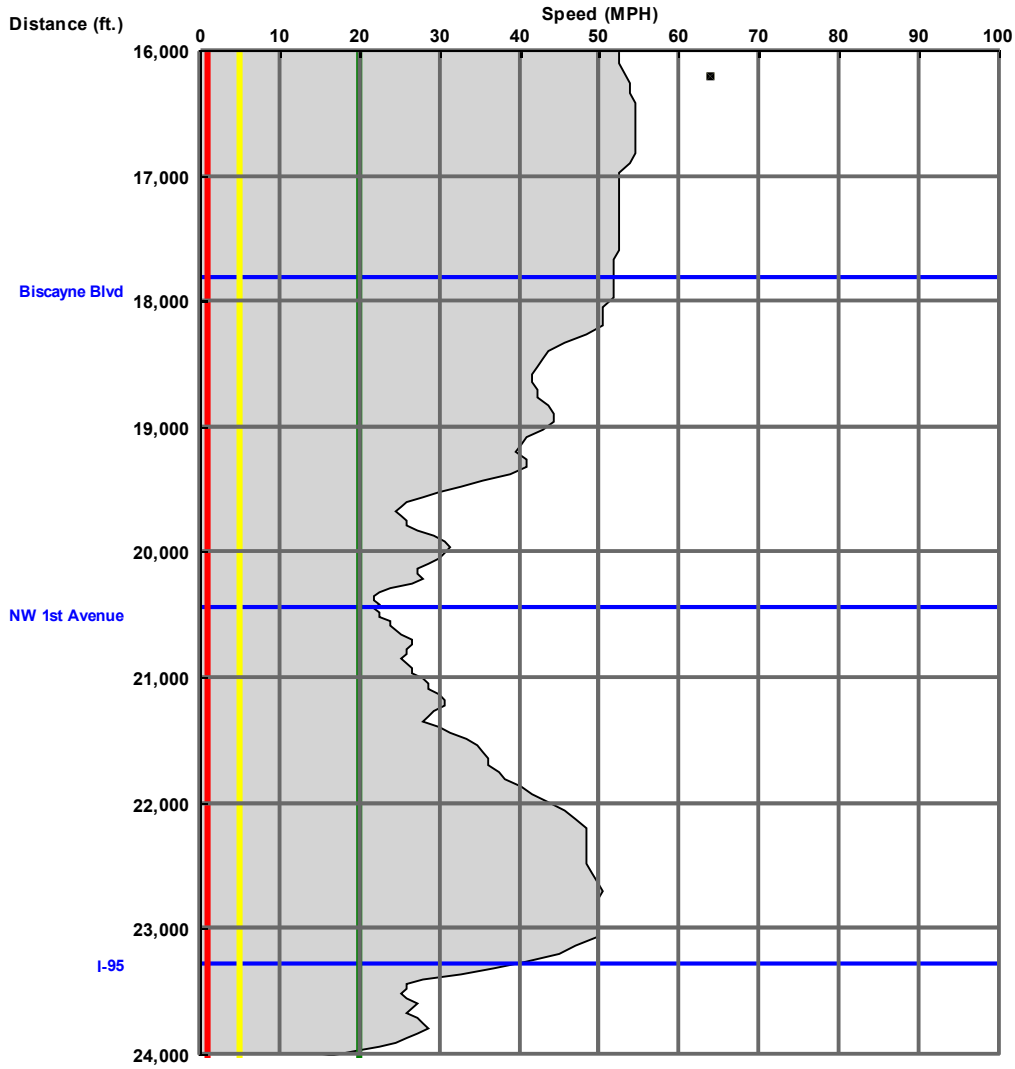
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 55

Speed Profile

Run: RUN 1 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

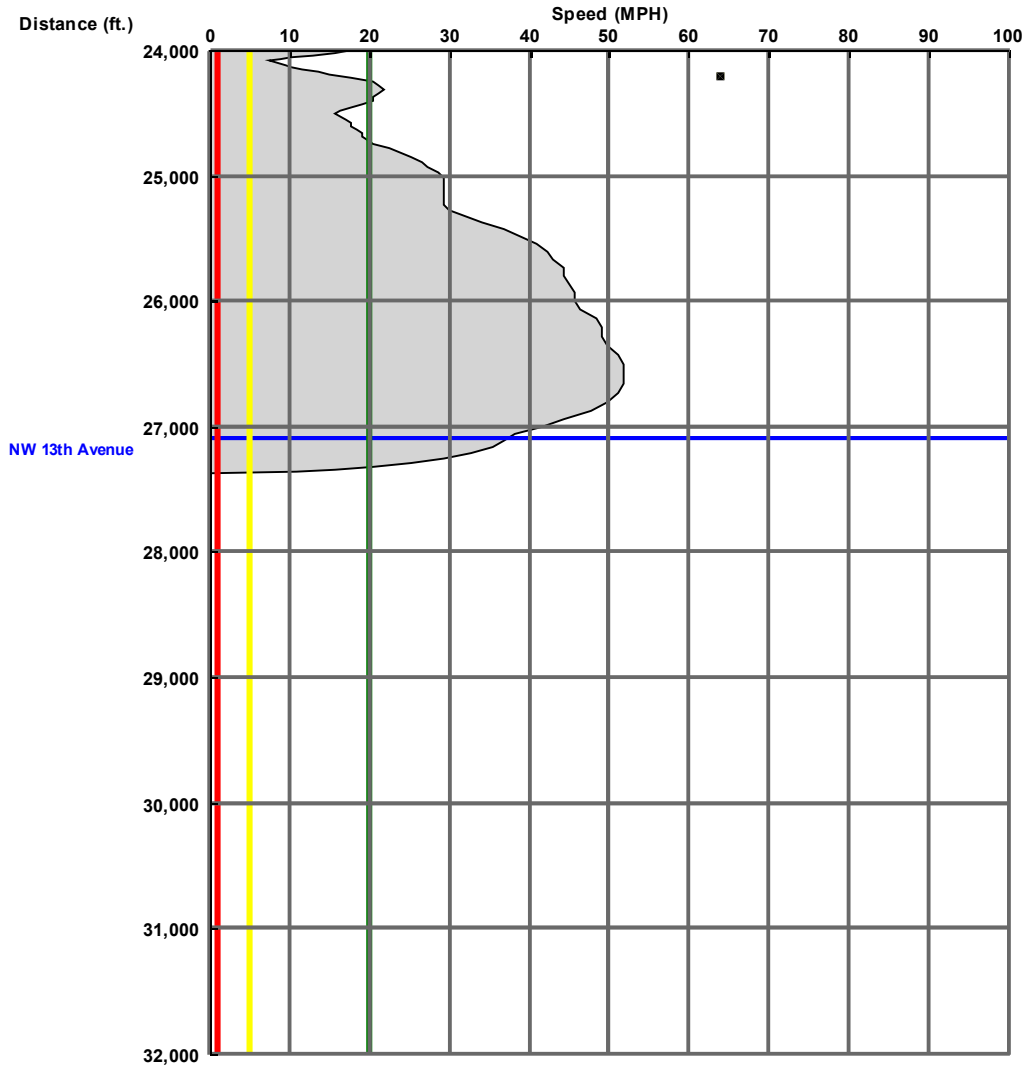
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 56

Speed Profile

Run: RUN 1 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

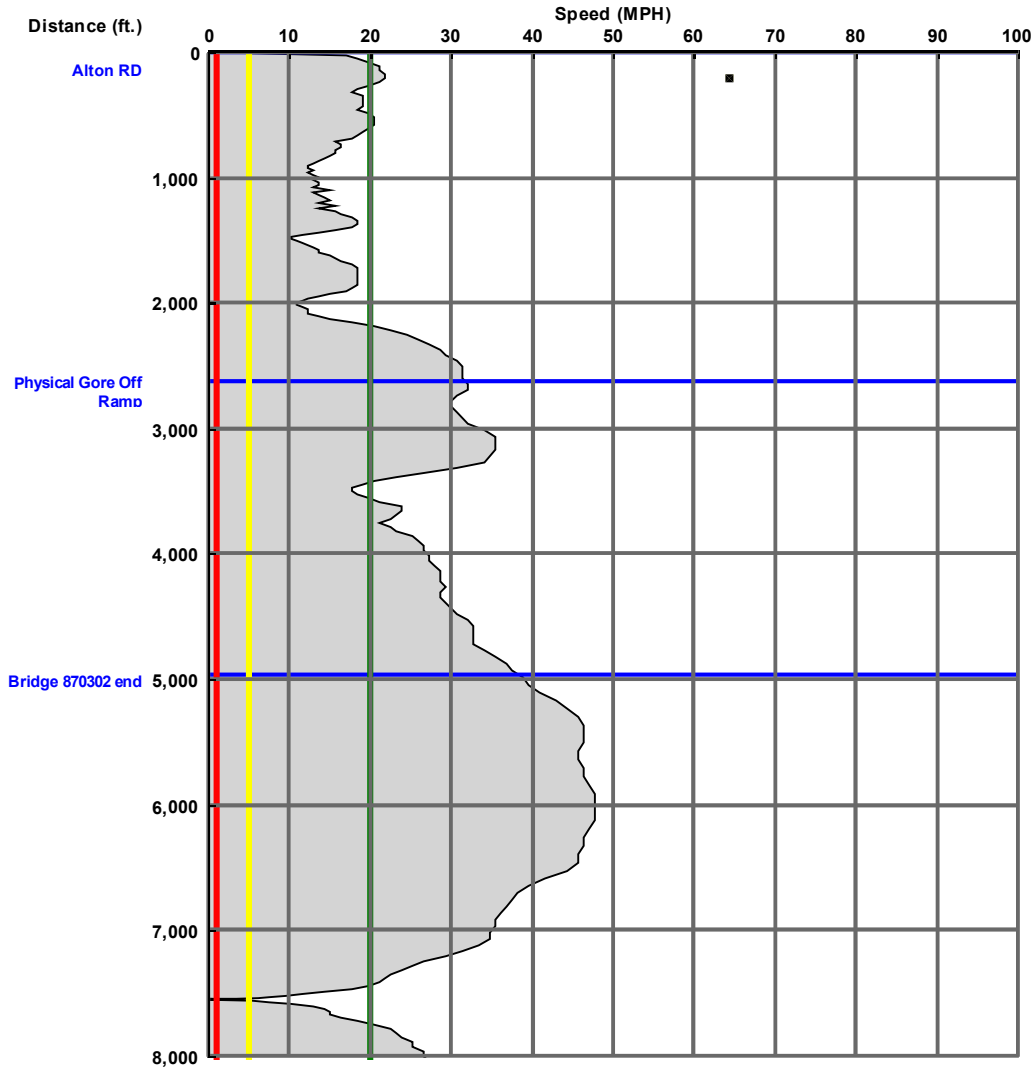
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 57

Speed Profile

Run: RUN 3 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

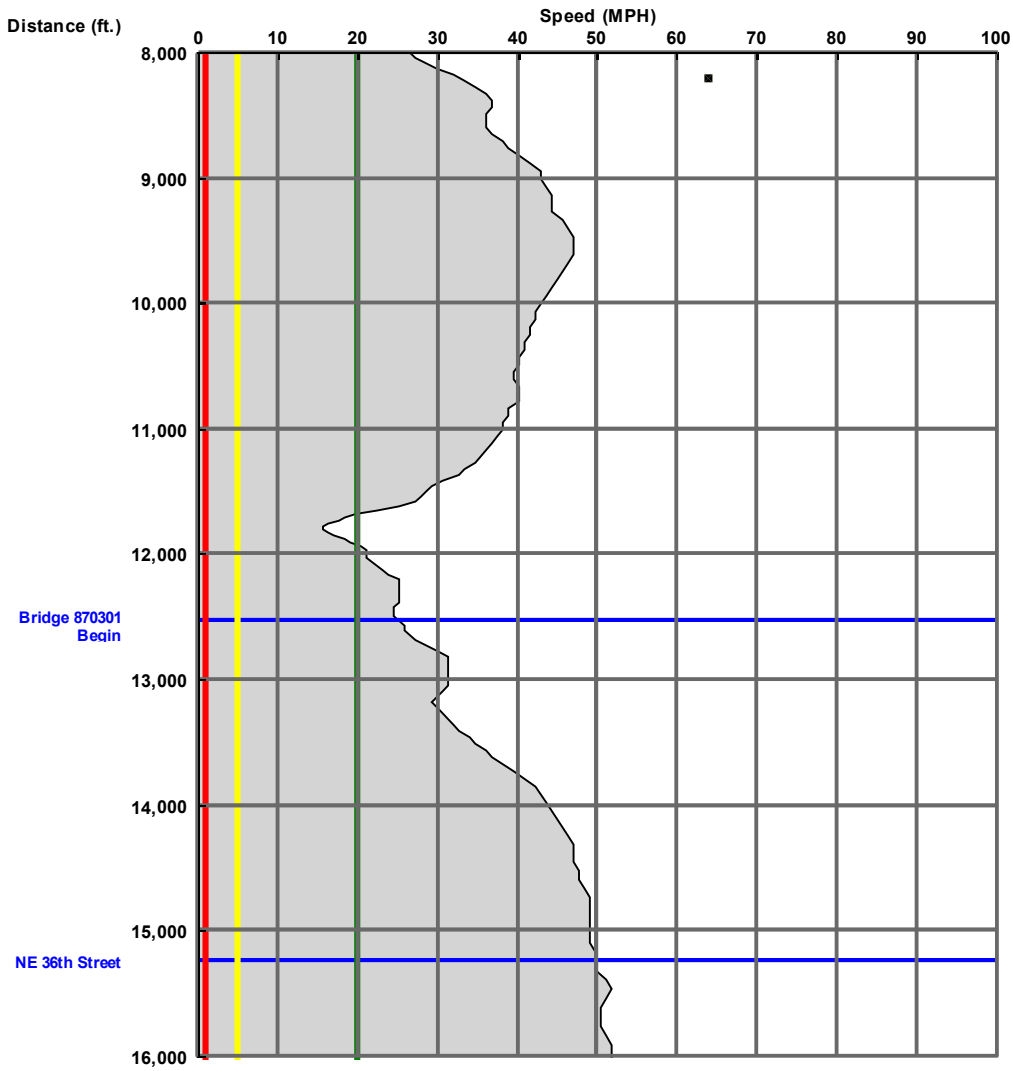
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 58

Speed Profile

Run: RUN 3 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

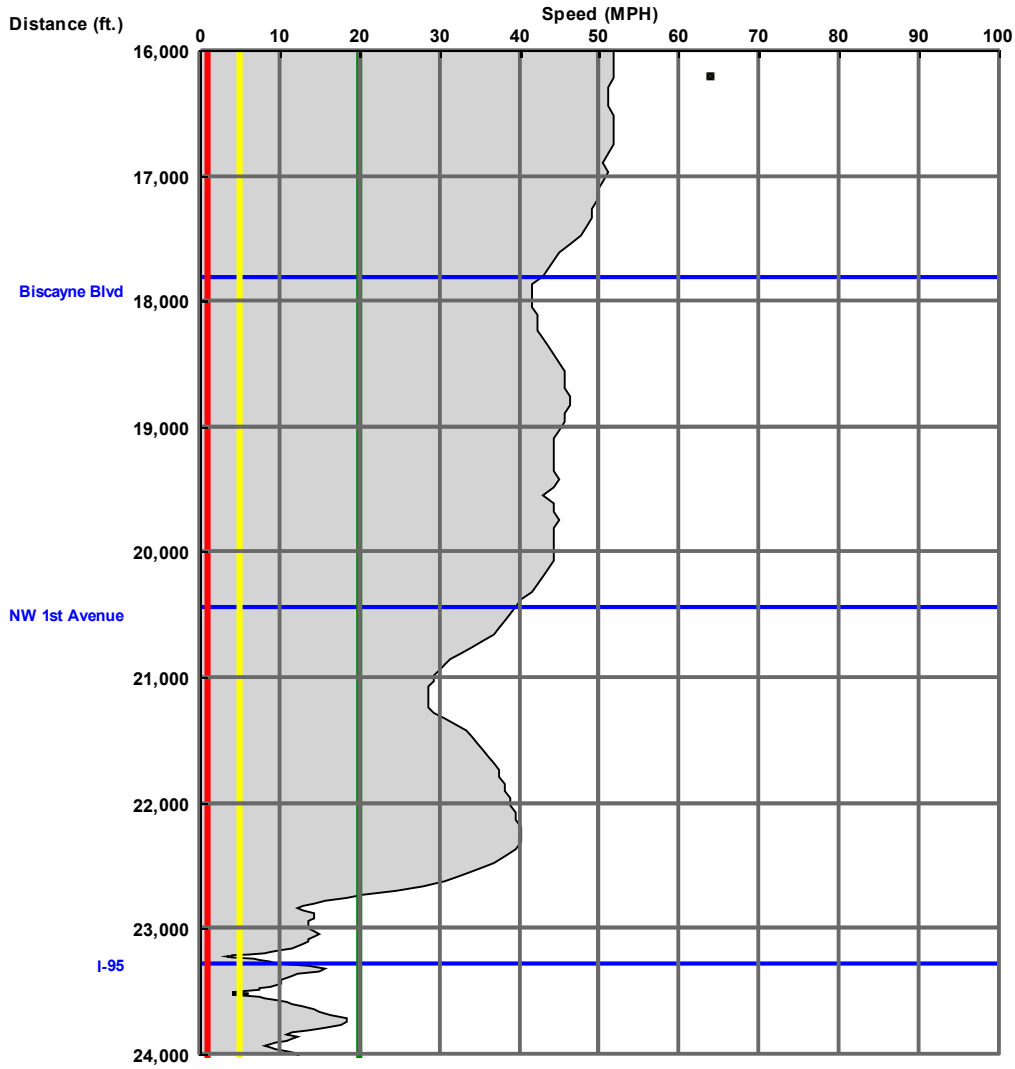
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 59

Speed Profile

Run: RUN 3 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

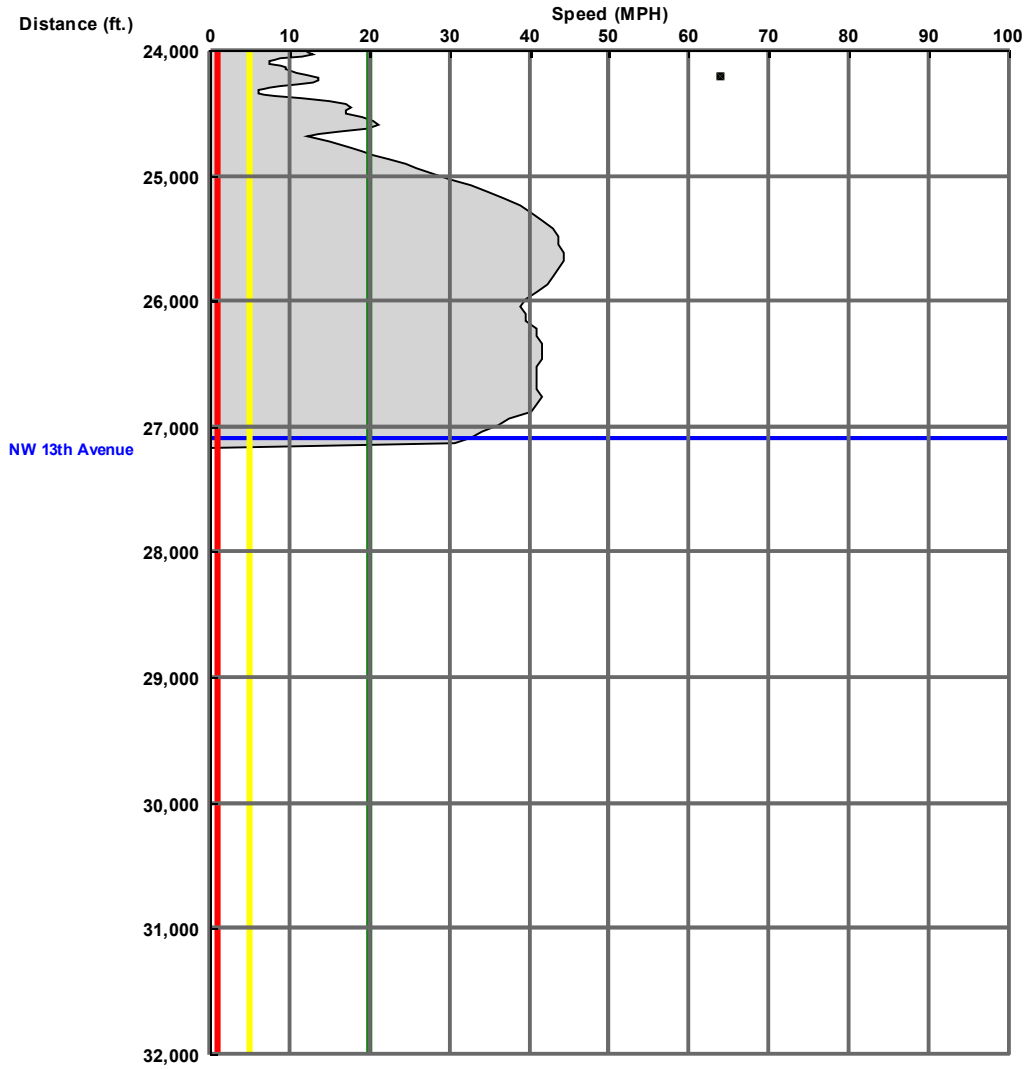
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 60

Speed Profile

Run: RUN 3 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

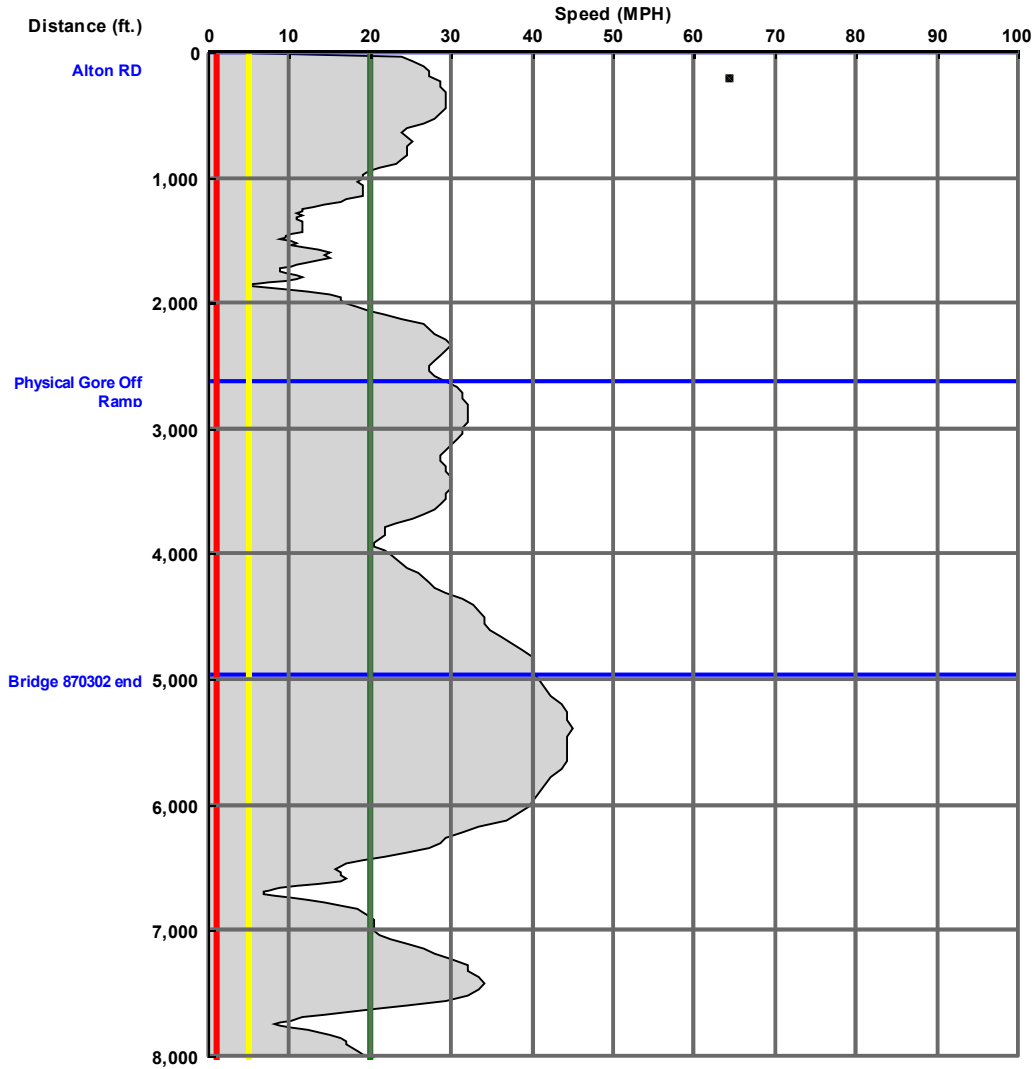
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 61

Speed Profile

Run: RUN 4 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

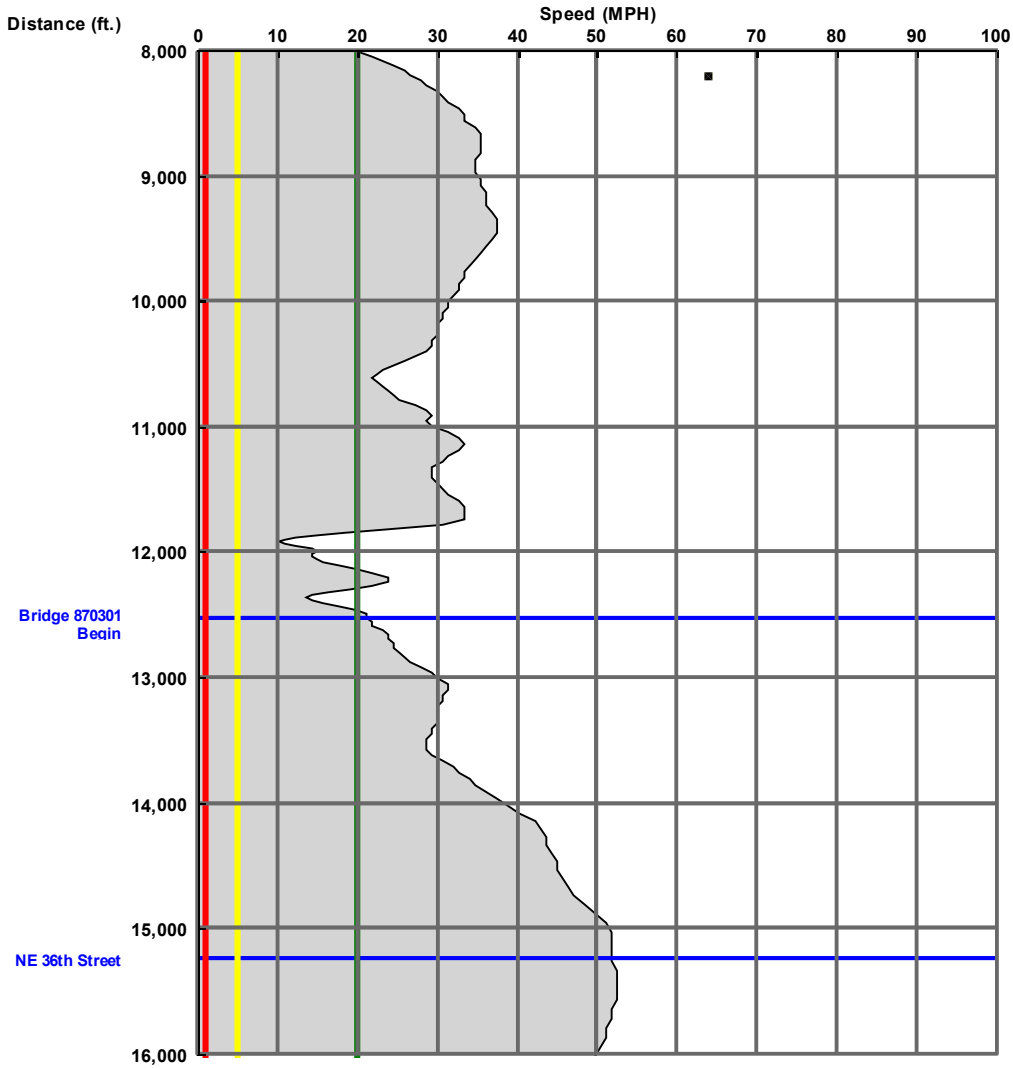
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 62

Speed Profile

Run: RUN 4 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

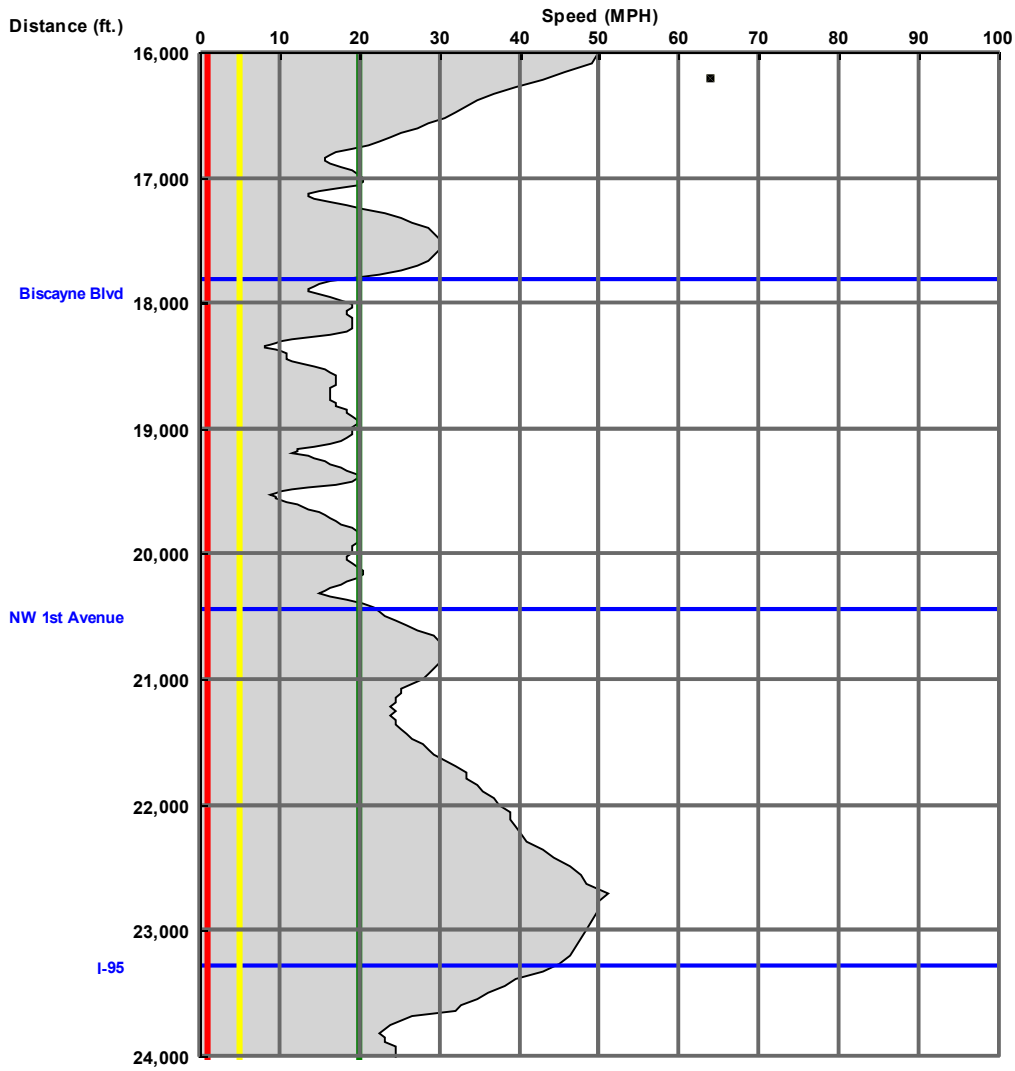
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 63

Speed Profile

Run: RUN 4 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

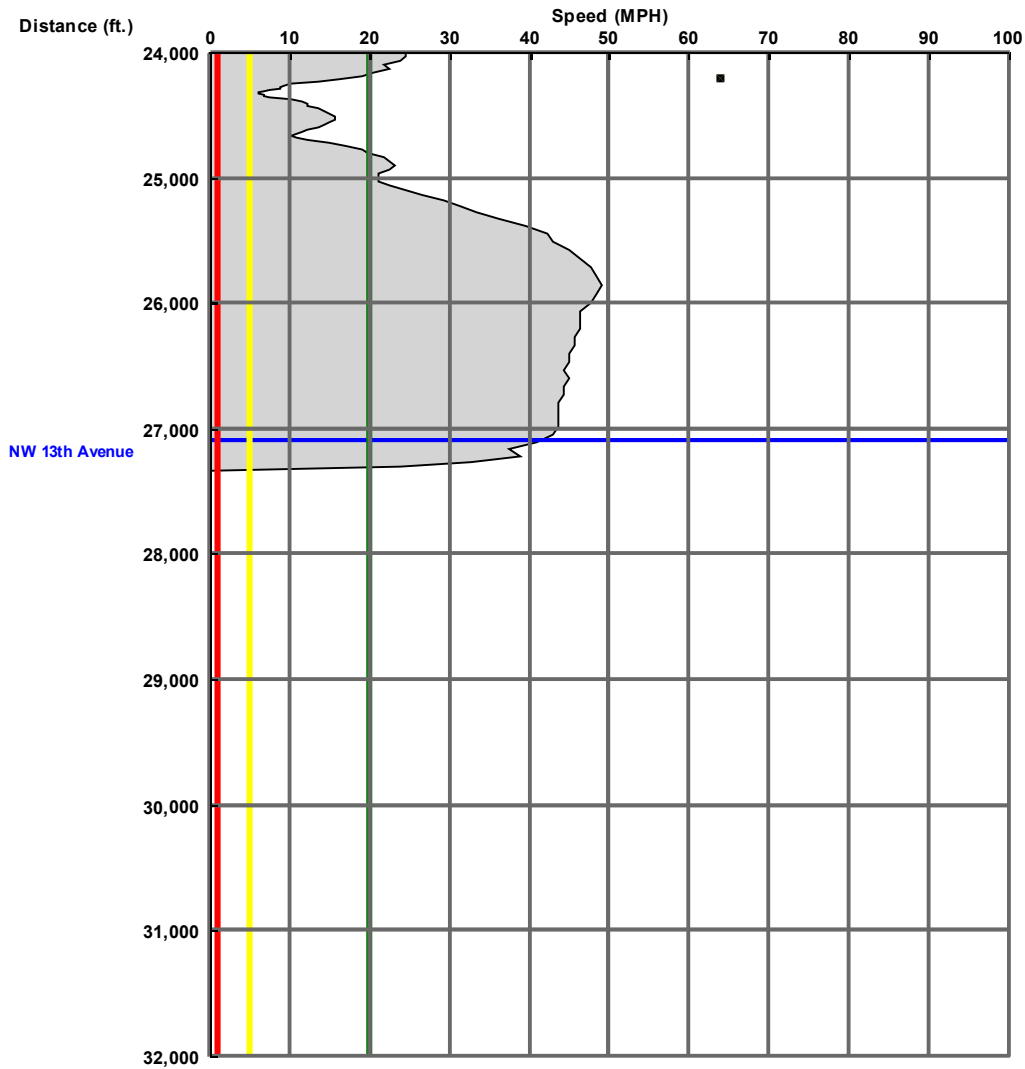
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 64

Speed Profile

Run: RUN 4 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

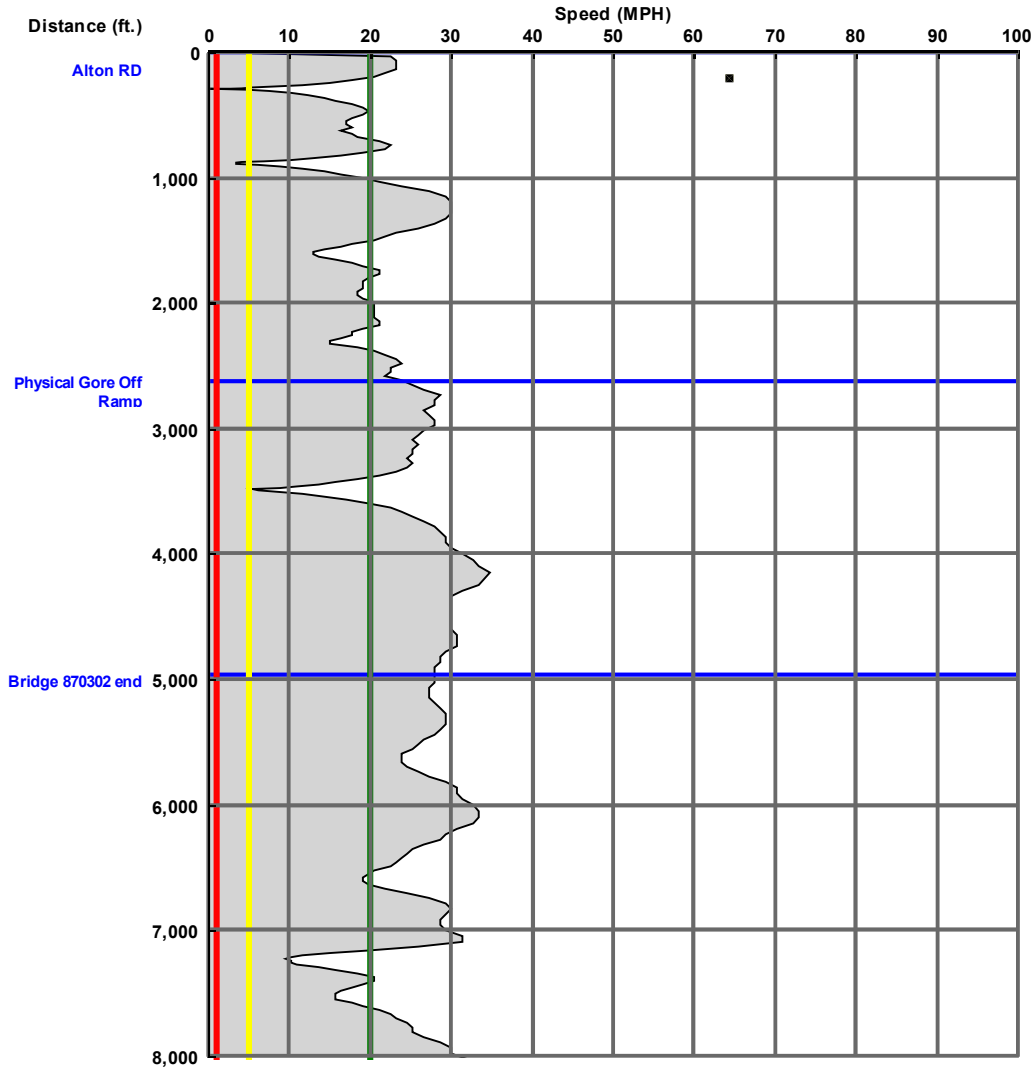
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 65

Speed Profile

Run: RUN 5 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

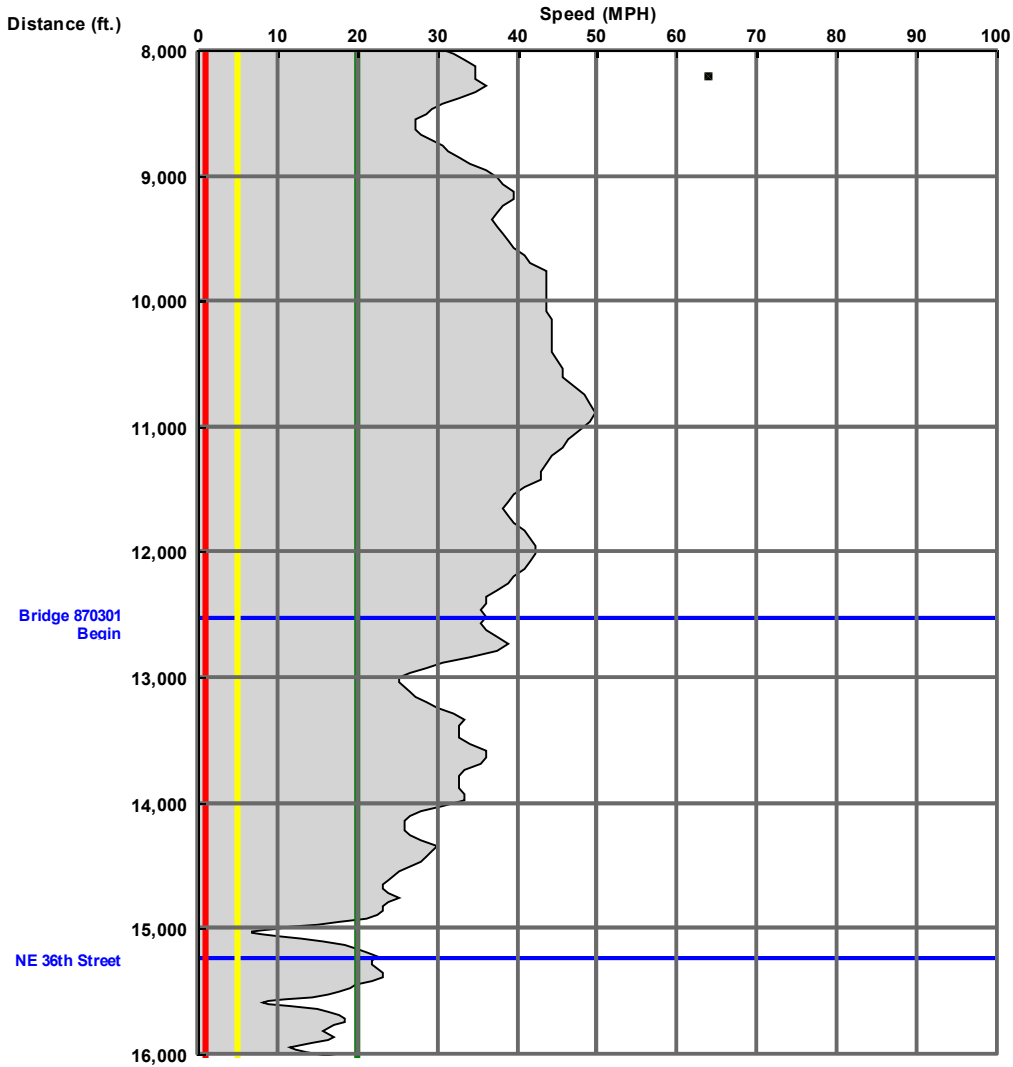
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 66

Speed Profile

Run: RUN 5 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

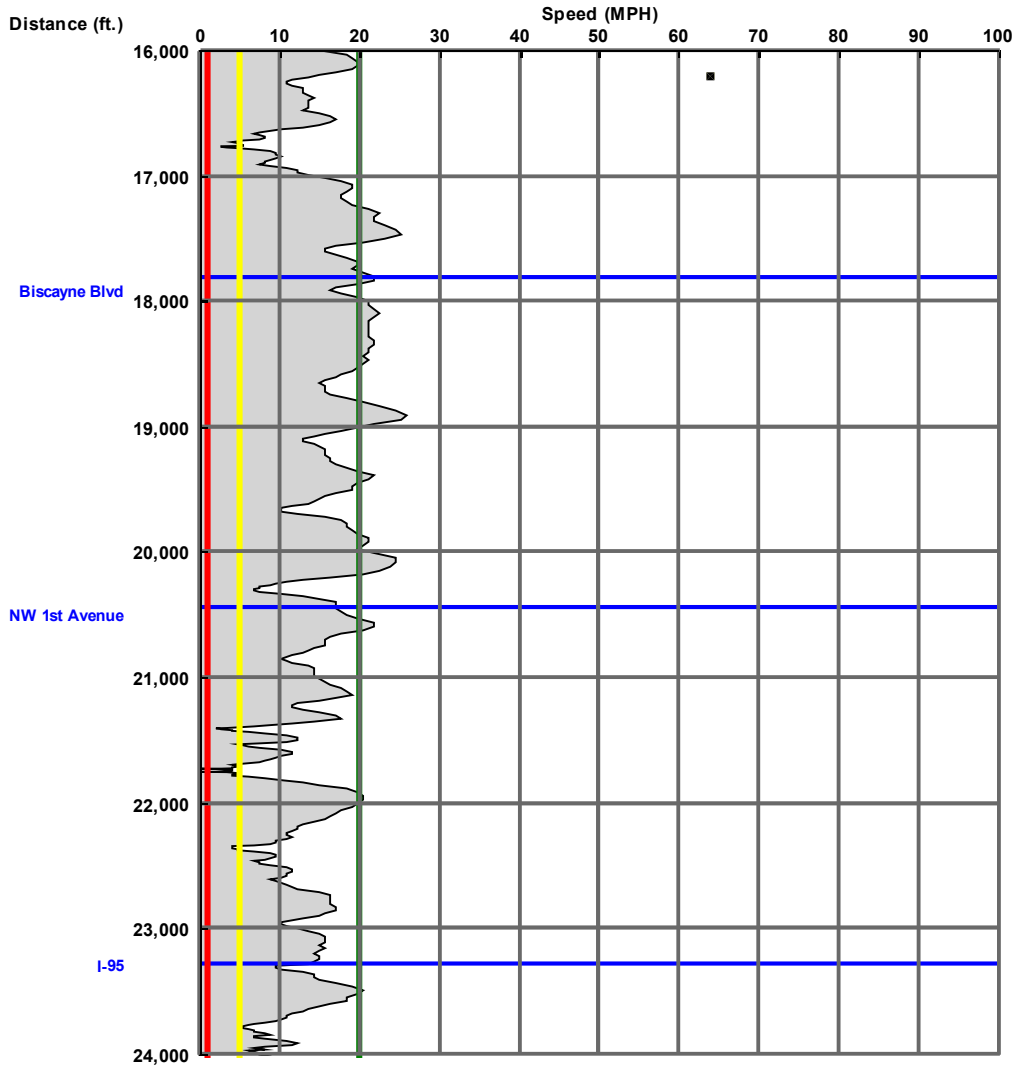
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 67

Speed Profile

Run: RUN 5 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

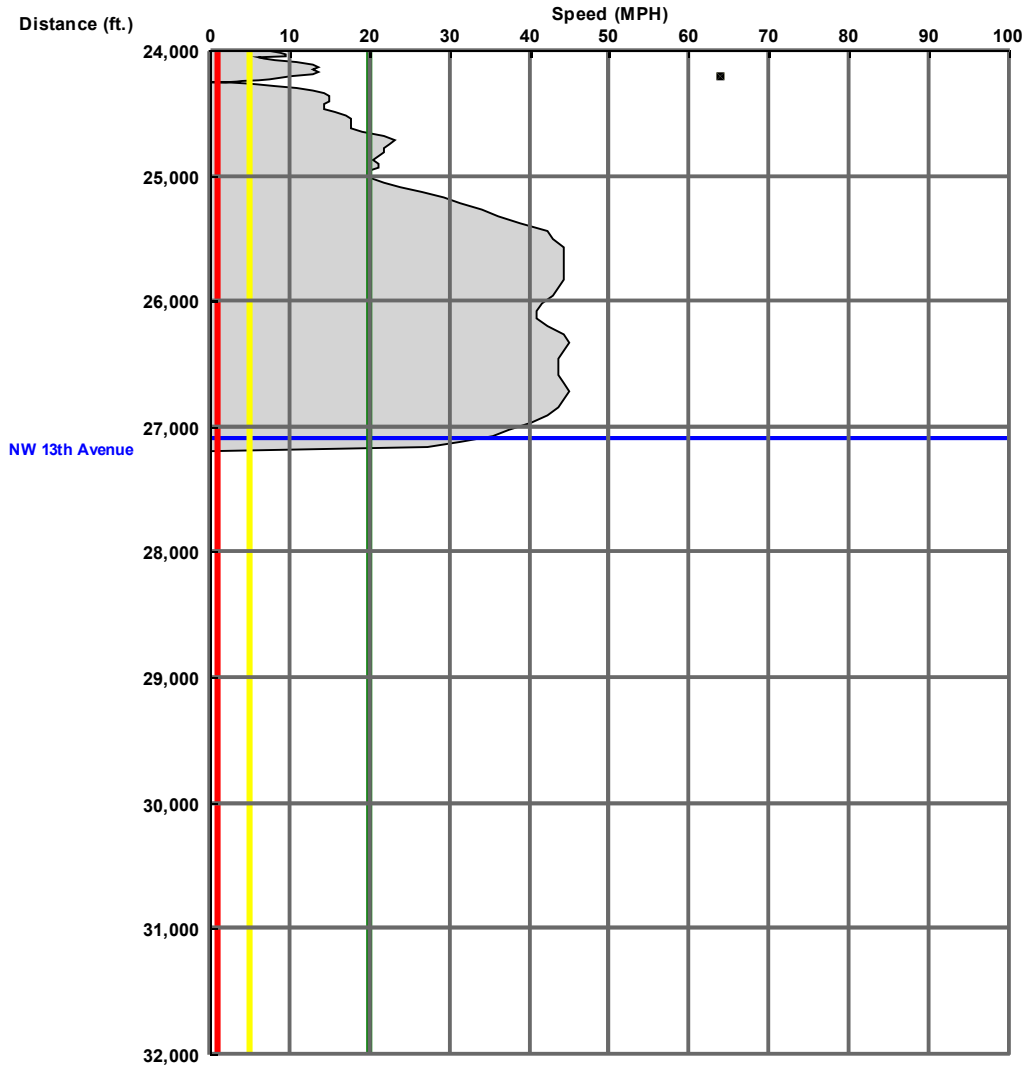
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 68

Speed Profile

Run: RUN 5 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

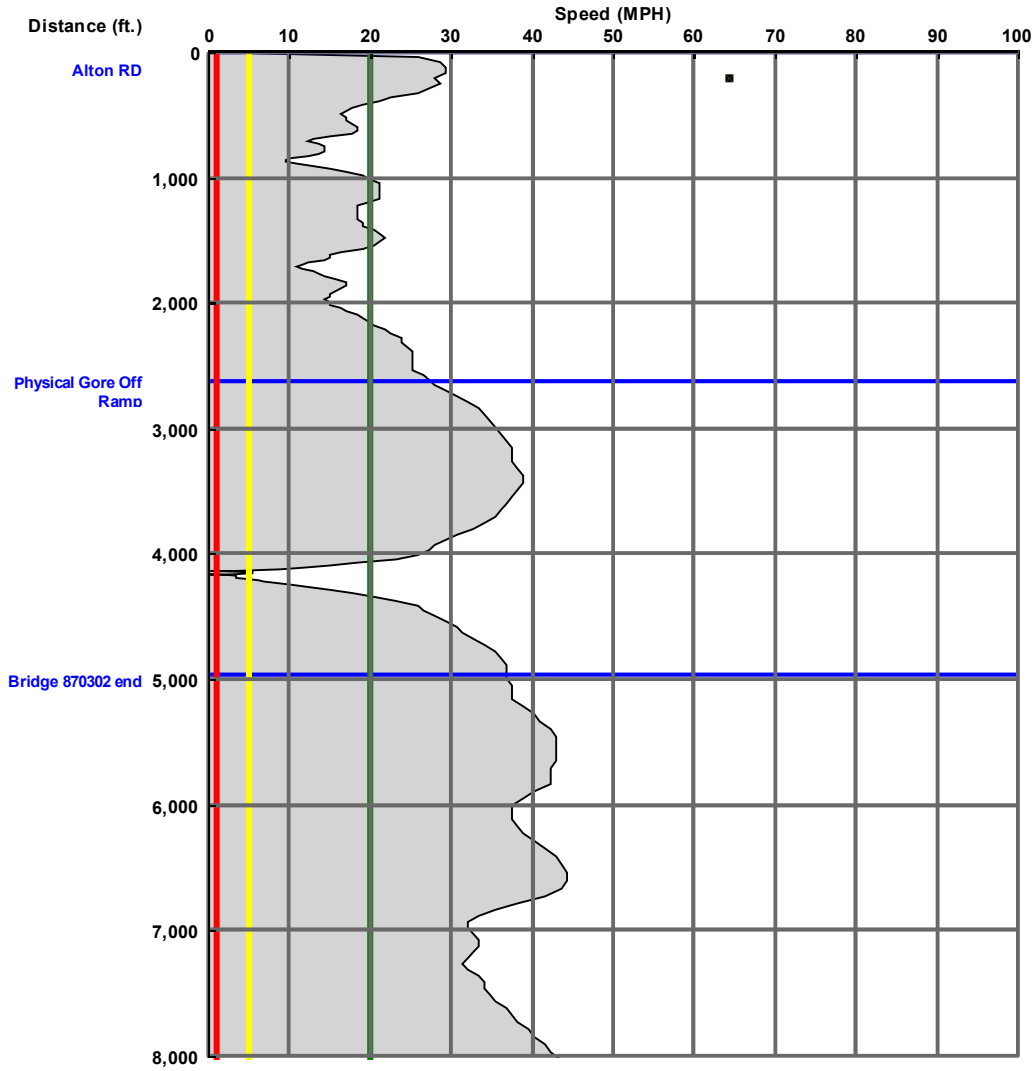
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 69

Speed Profile

Run: RUN 6 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

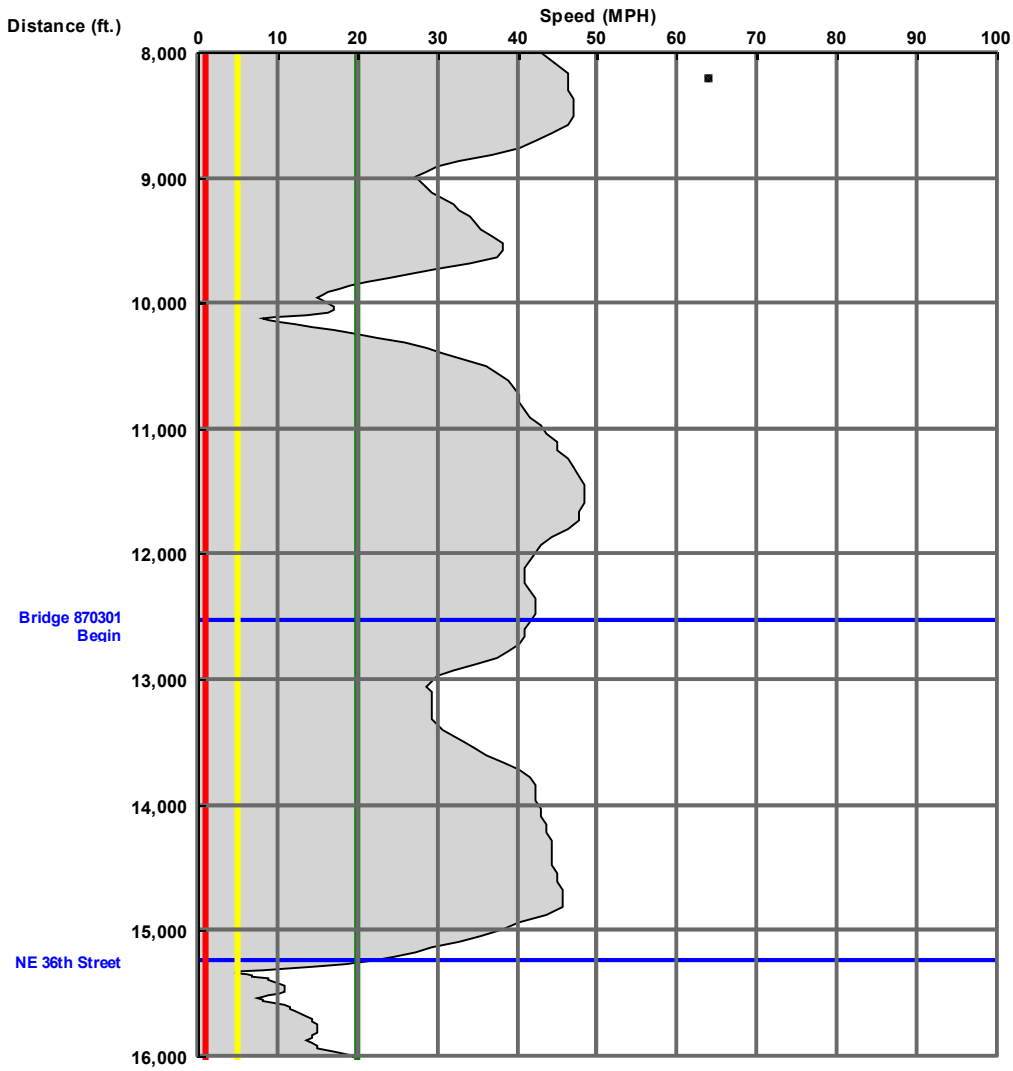
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 70

Speed Profile

Run: RUN 6 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

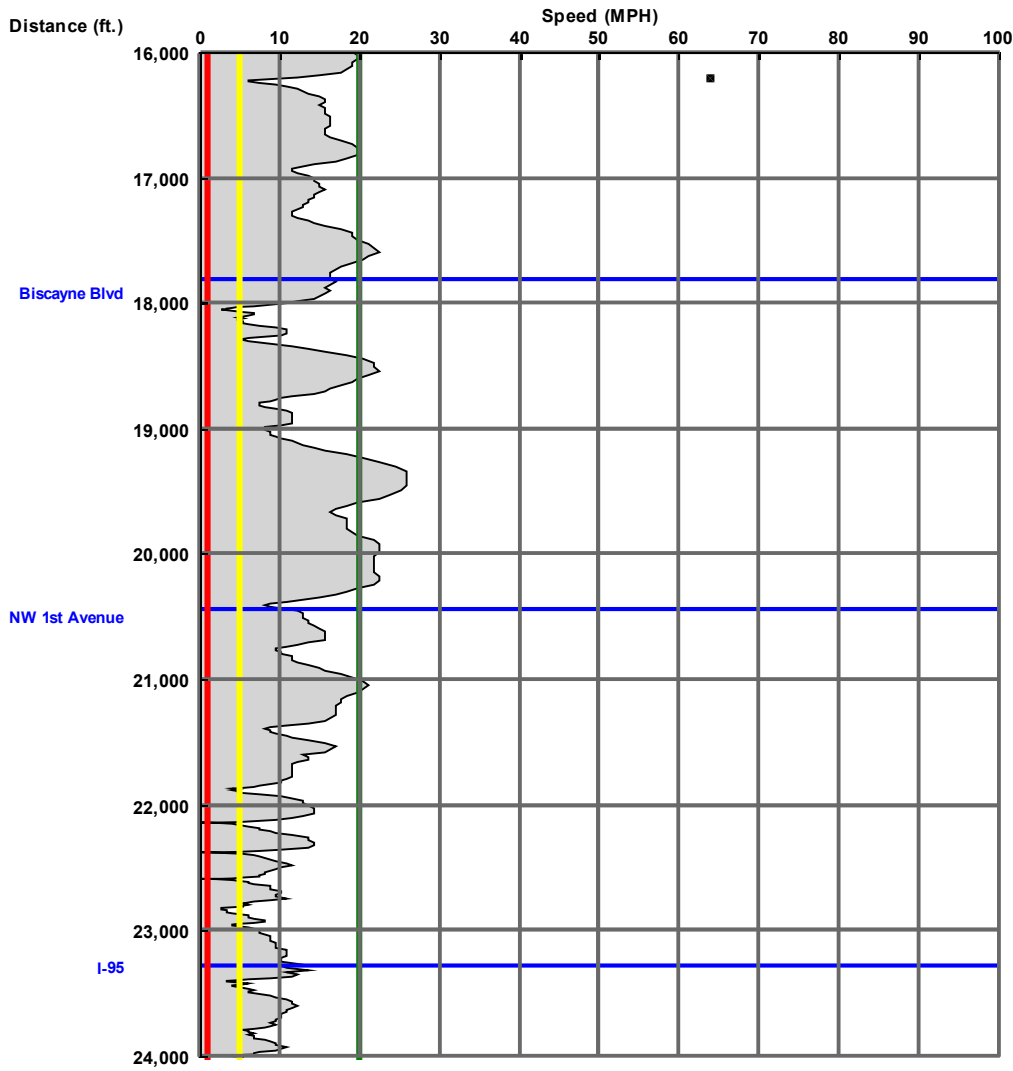
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 71

Speed Profile

Run: RUN 6 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

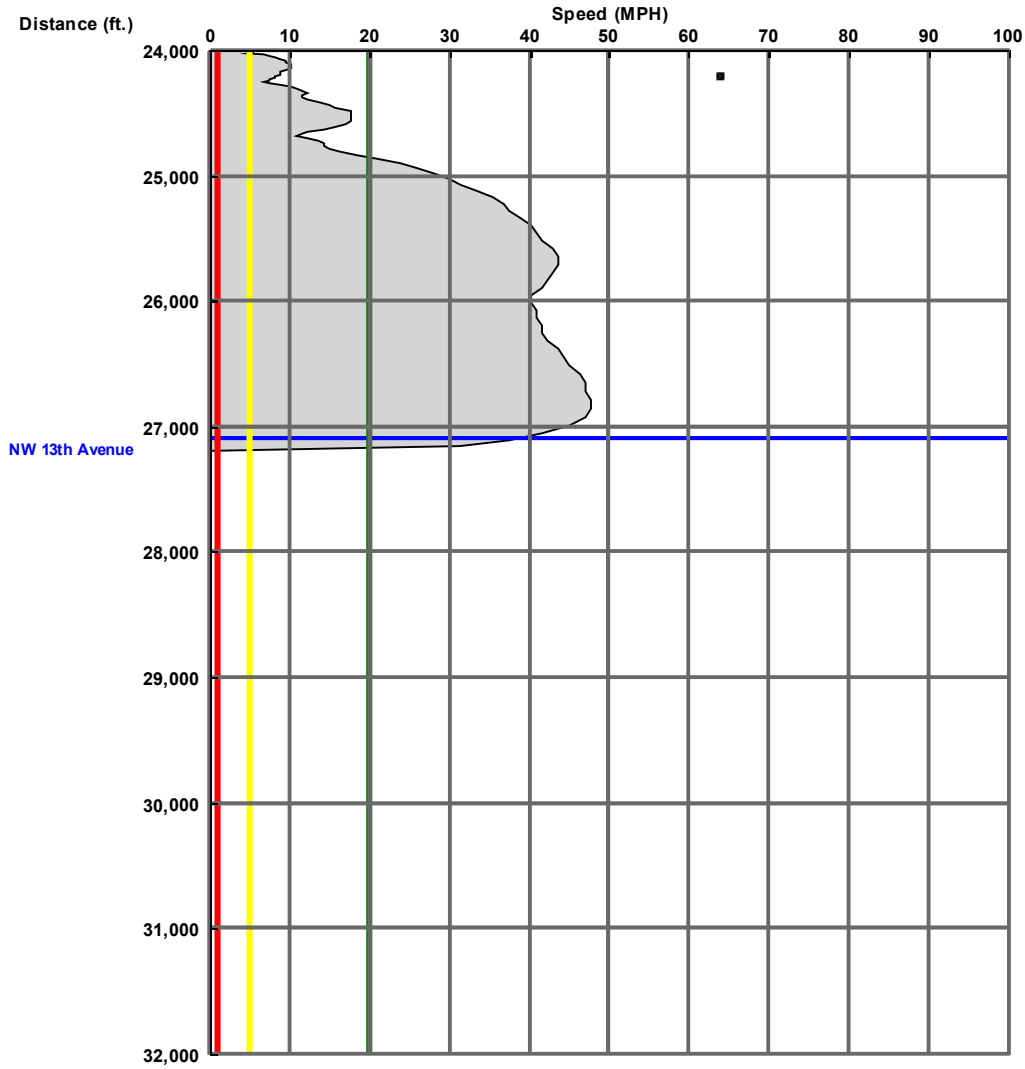
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 72

Speed Profile

Run: RUN 6 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

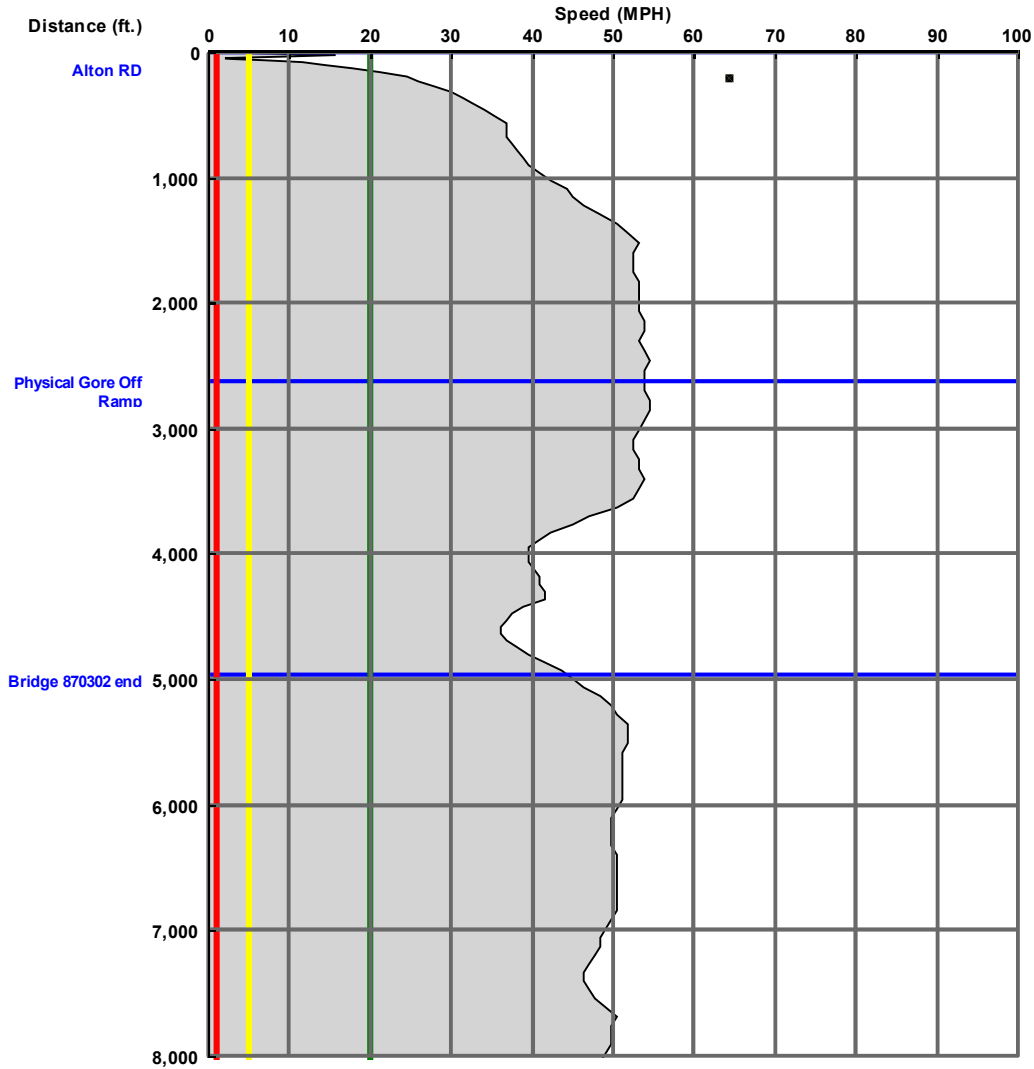
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 73

Speed Profile

Run: RUN 7 WB PM 2-15-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

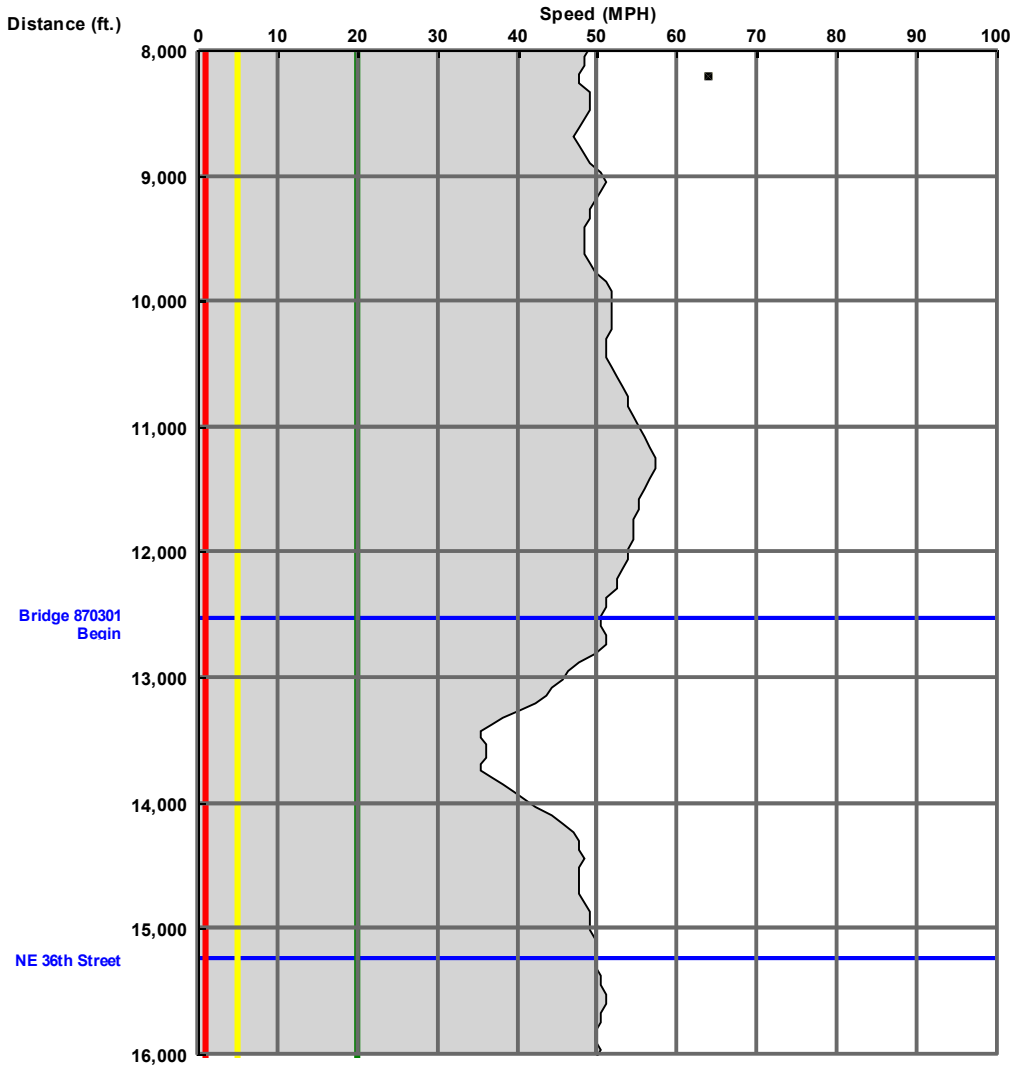
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 74

Speed Profile

Run: RUN 7 WB PM 2-15-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

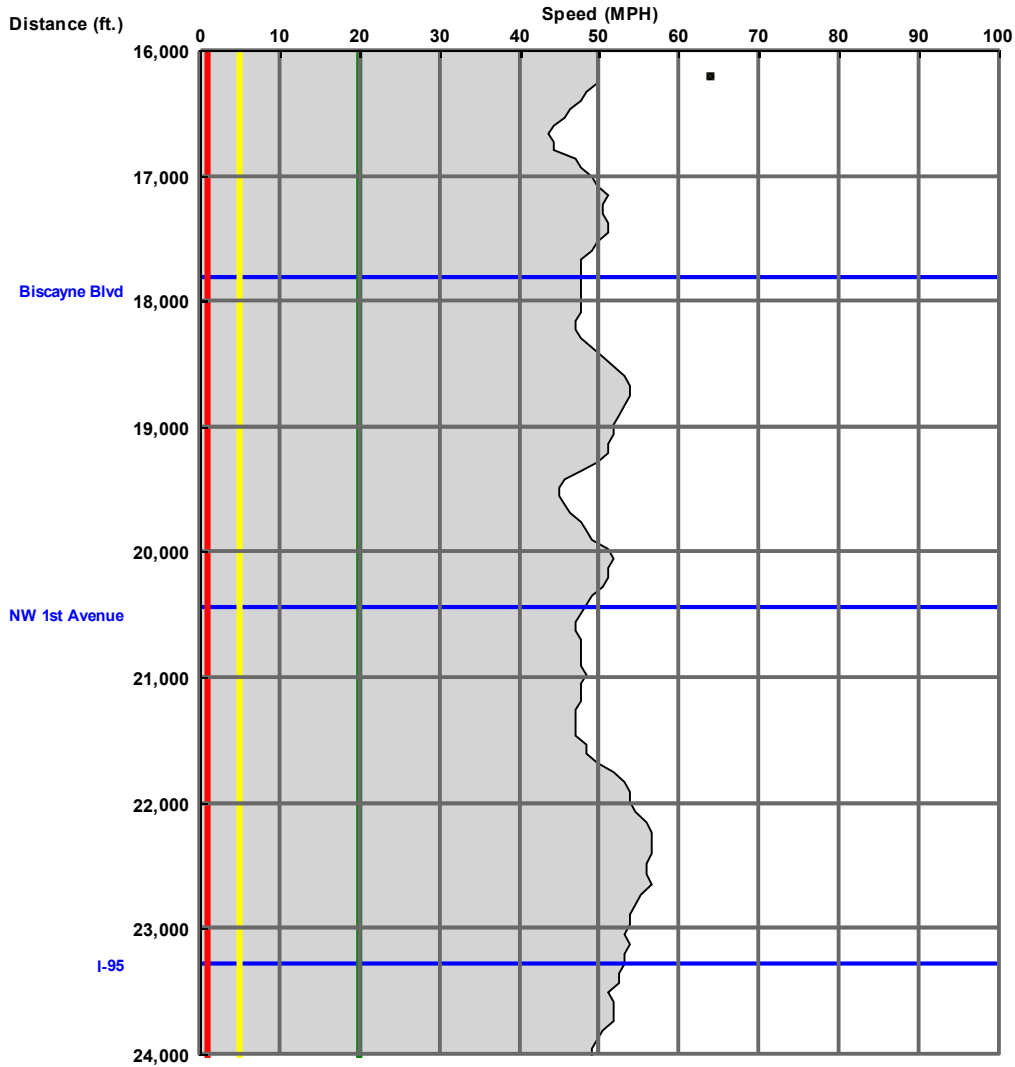
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 75

Speed Profile

Run: RUN 7 WB PM 2-15-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

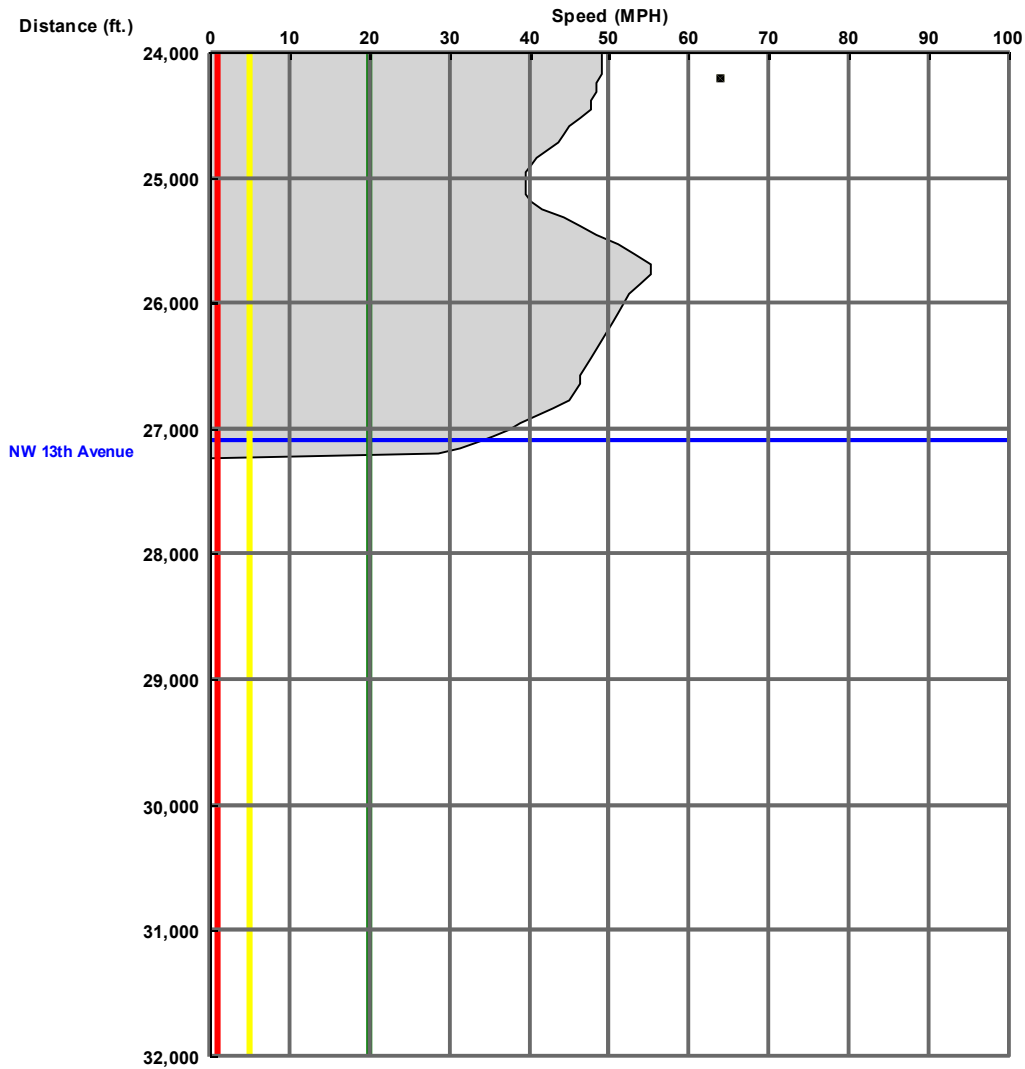
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 76

Speed Profile

Run: RUN 7 WB PM 2-15-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

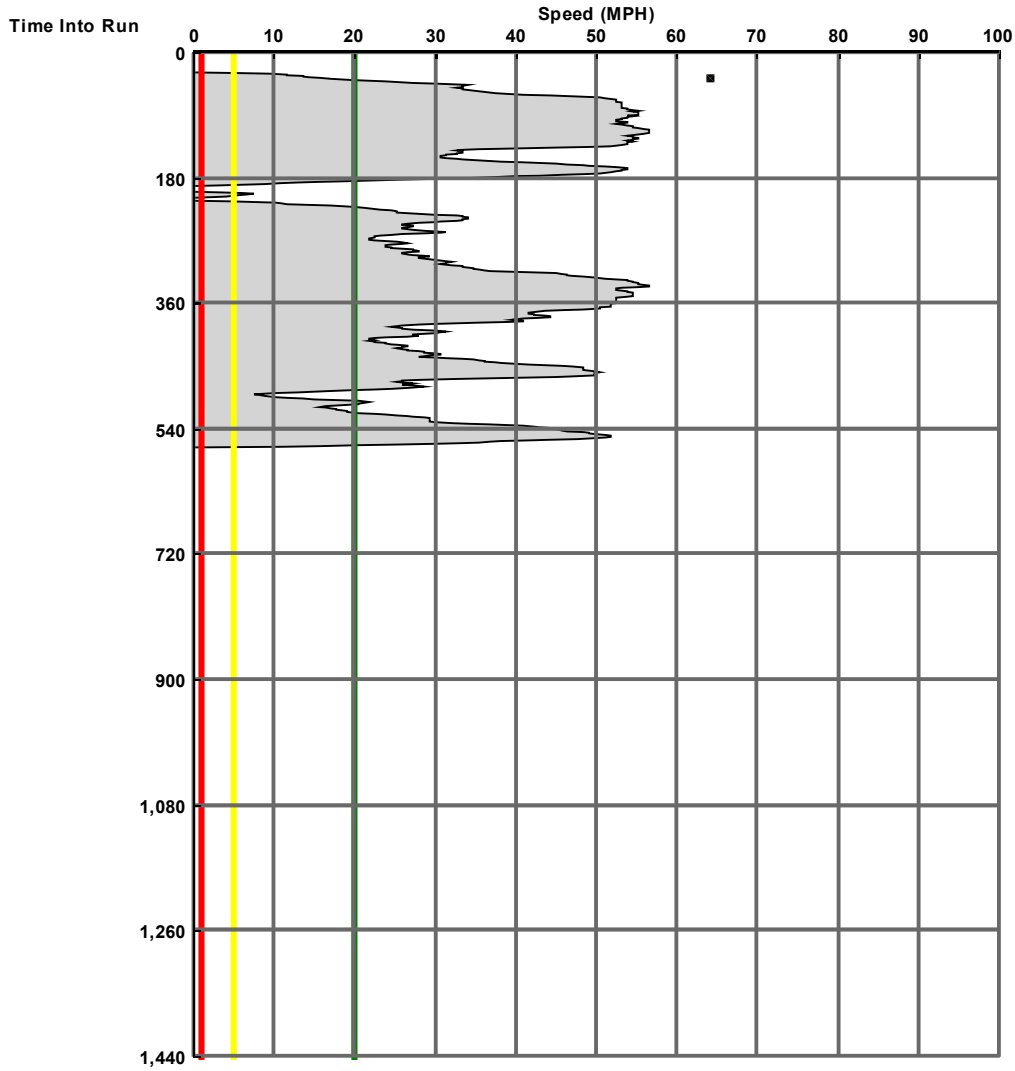
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 77

Time Based Speed Profile

Run: RUN 1 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

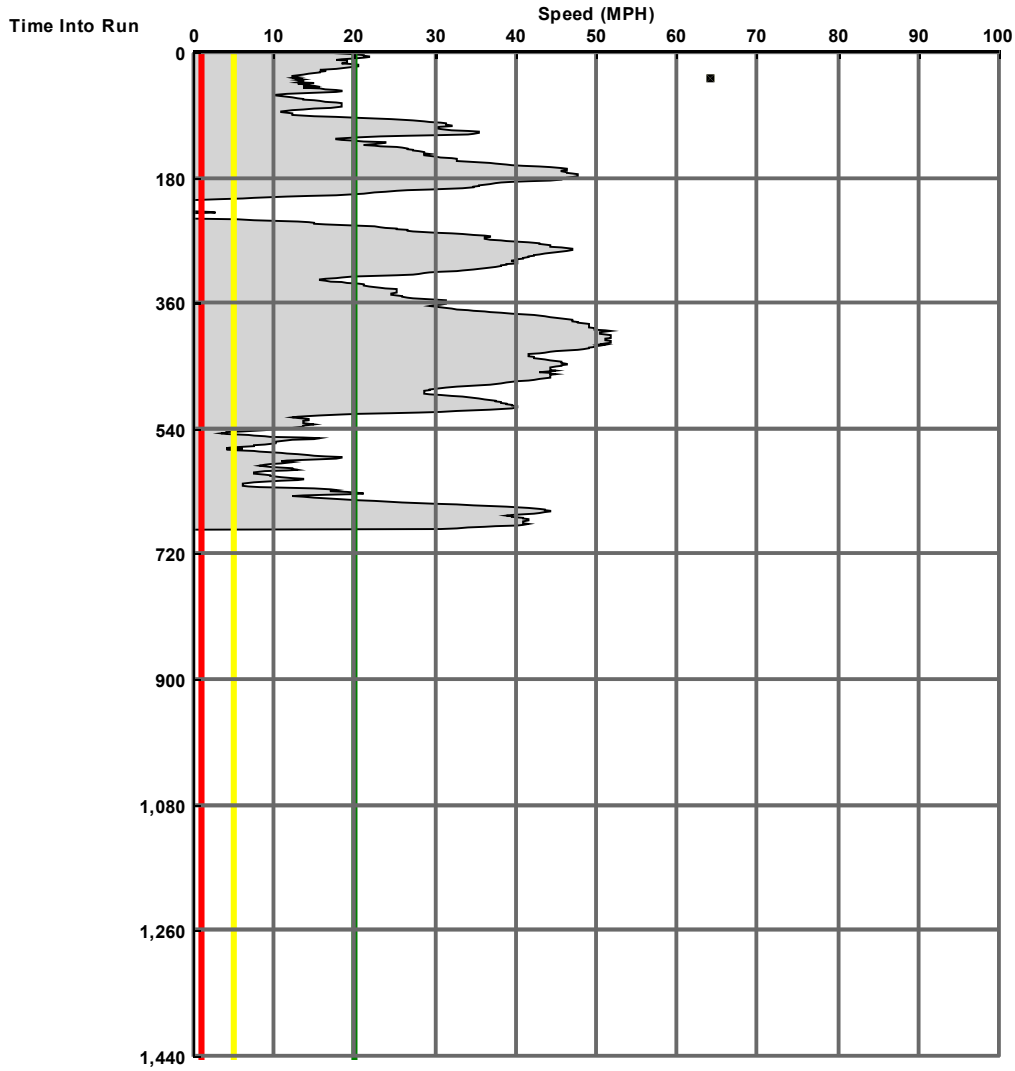
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 78

Time Based Speed Profile

Run: RUN 3 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

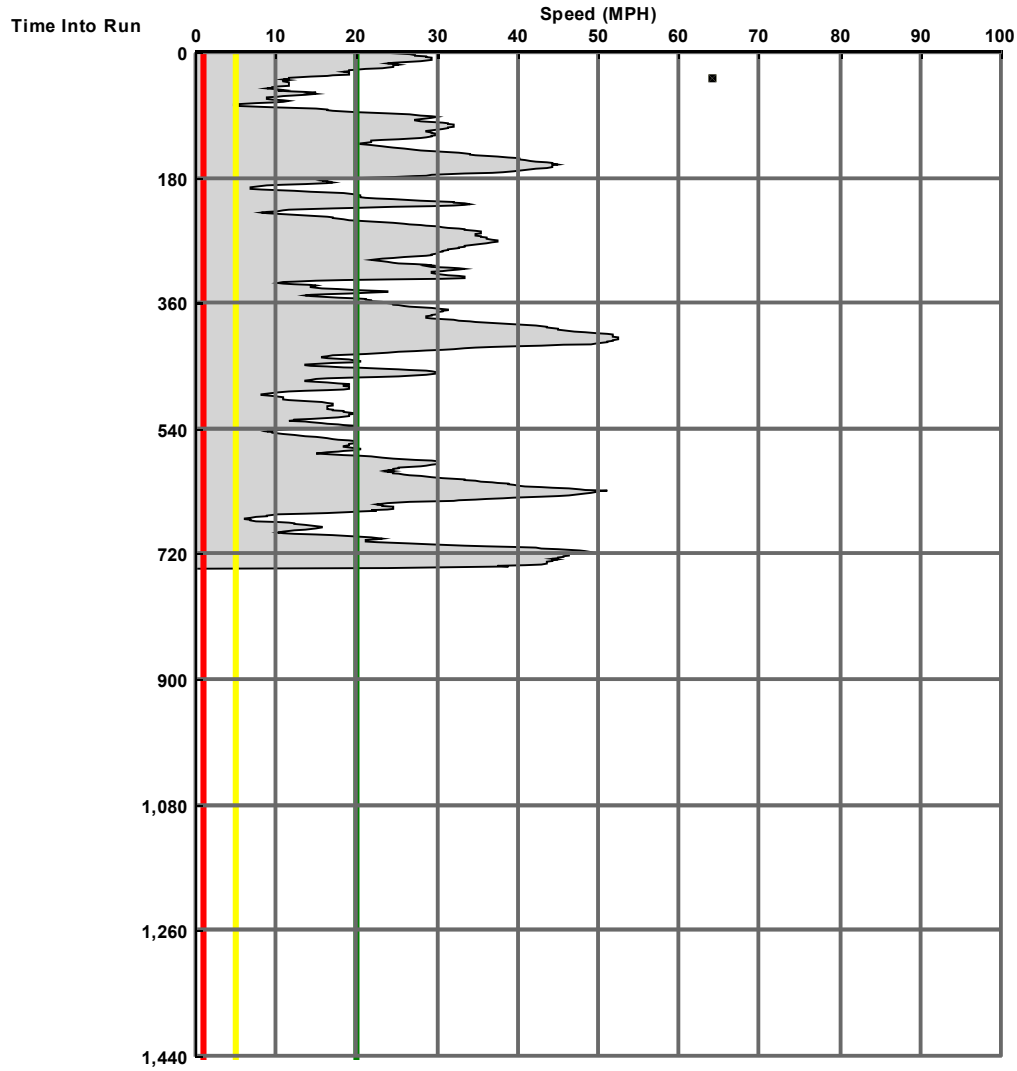
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 79

Time Based Speed Profile

Run: RUN 4 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

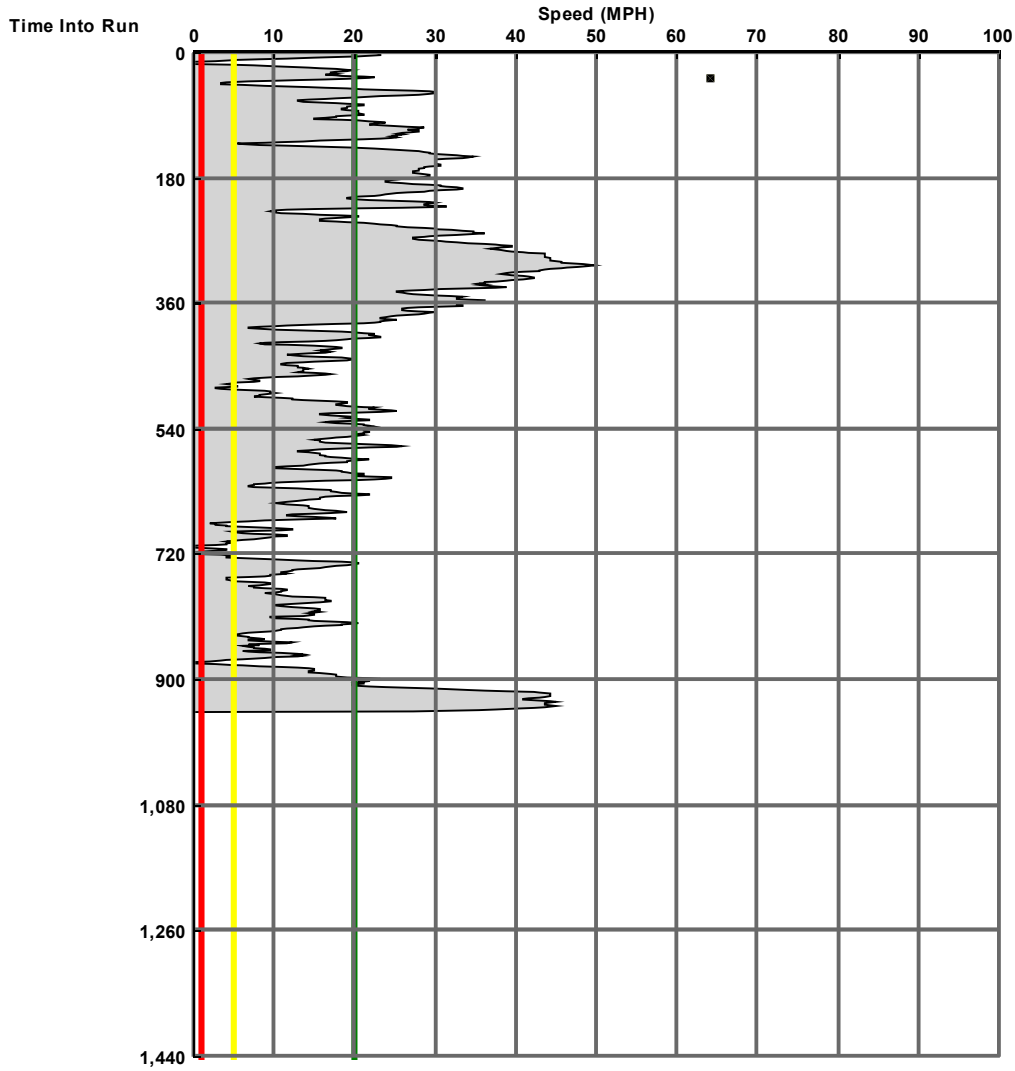
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 80

Time Based Speed Profile

Run: RUN 5 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

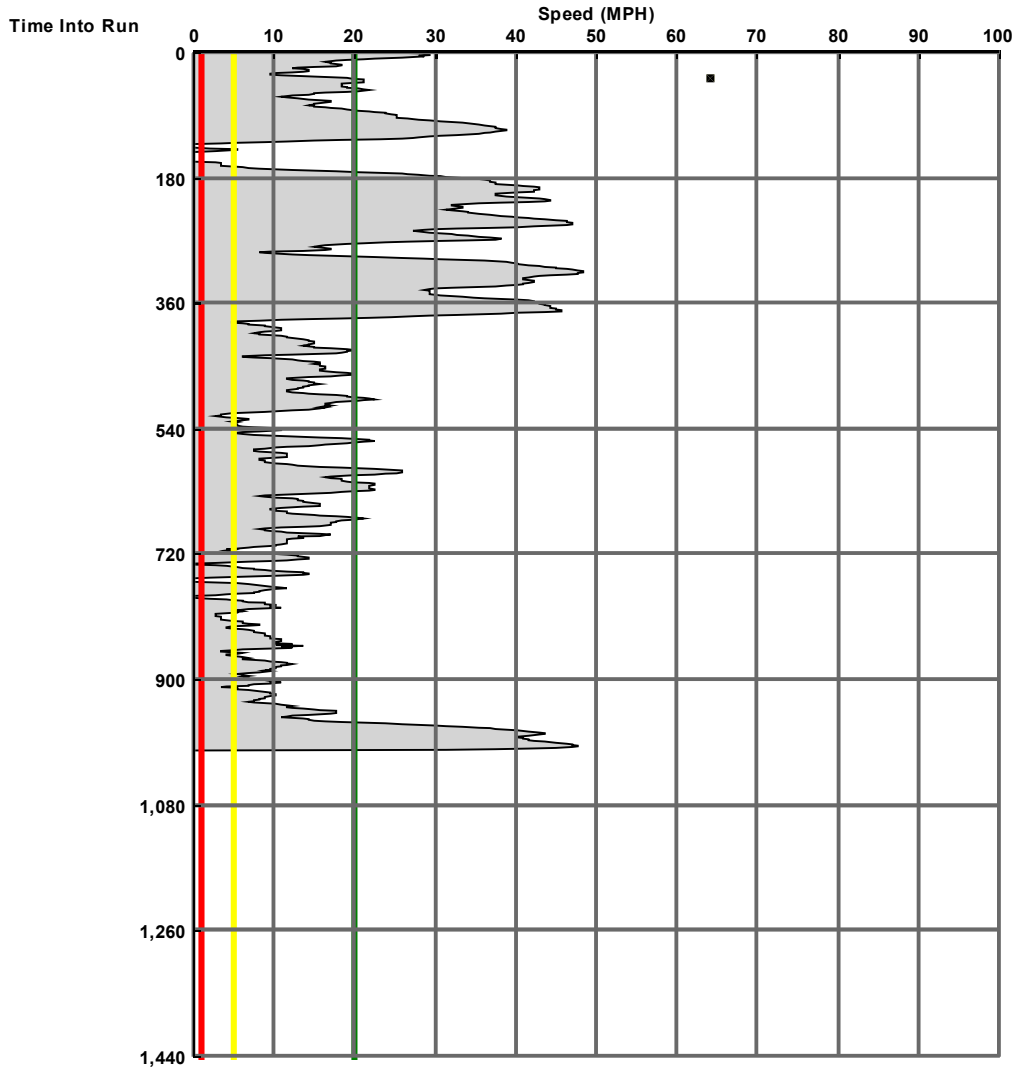
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 81

Time Based Speed Profile

Run: RUN 6 WB PM 2-14-2018-R001



I-195 WESTBOUND (PM)

Ten and Two - Travel Time Data

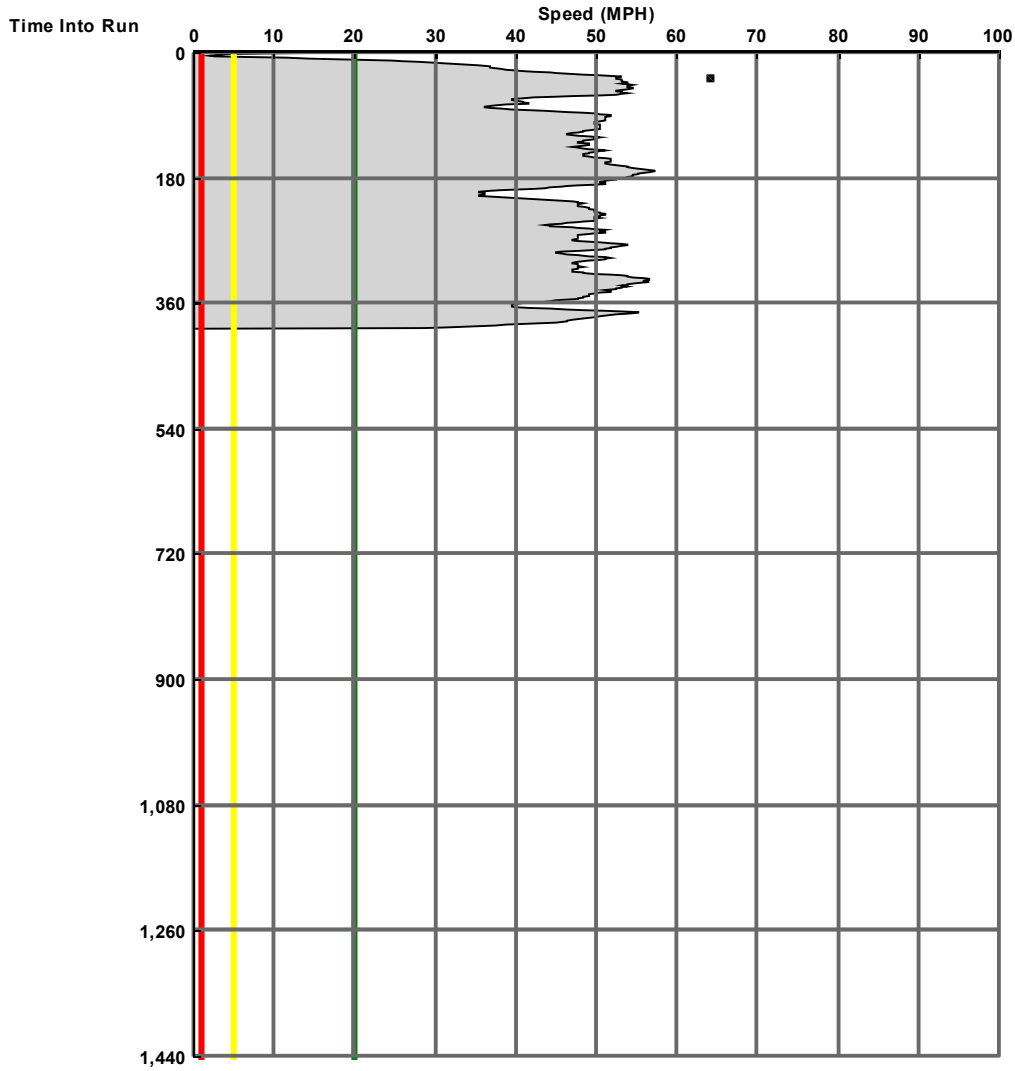
Study Name: I-195 Westbound PM

Study Date: 3/21/2018

Page No: 82

Time Based Speed Profile

Run: RUN 7 WB PM 2-15-2018-R001



PEAK SPREADING REVIEW

Lorin Brissett

Subject: FW: I-195 CPS - (Cost for 1 hour of Additional TMC Data)
Attachments: Volume_Profile_Summary_Peak_Spreading_Assessment_w_attachments.pdf

From: Jeffries, Ken [<mailto:Ken.Jeffries@dot.state.fl.us>]
Sent: Tuesday, September 26, 2017 9:02 AM
To: Lorin Brissett <lbrissett@bcceng.com>
Cc: Steinmiller, Phil <Phil.Steinmiller@dot.state.fl.us>; Jose Munoz <jmunoz@bcceng.com>; Sung-Ryong Han <shan@bcceng.com>
Subject: RE: I-195 Planning Study - Peak Hour Spread

Yes, let me know the cost.

Kenneth Jeffries
Transportation Planner
Planning and Environmental Management Office
Florida Department of Transportation, District 6
Adam Leigh Cann Building
1000 NW 111th Avenue, Room 6111
Miami, Florida 33172

Phone: (305) 470-5445, Fax: (305) 470-5205
E-mail: Ken.Jeffries@dot.state.fl.us

From: Lorin Brissett [<mailto:lbrissett@bcceng.com>]
Sent: Monday, September 25, 2017 2:39 PM
To: Jeffries, Ken
Cc: Steinmiller, Phil; Jose Munoz; Sung-Ryong Han
Subject: RE: I-195 Planning Study - Peak Hour Spread

Ken,

Please see attached for peak spreading assessment we've performed using 2016 FTI Counts for three mainline FDOT count stations on I-195.

Based on the FTI data for the mainline, the morning peak seems fairly typical (i.e., 7:00 AM to 9:00 AM) but the PM peak seems a little more spread out. Therefore, for the peak hour analyses, you could consider having us process 2 hours in the AM Peak and 3 hours in the PM peak based on the FTI data. We could ask Caltran for a quote for just adding the additional hour to the PM peak alone and keeping the AM peak at 2 hours?

Please advise.

Lorin R.C. Brissett, P.E
BCC Engineering, Inc.
t. (954) 928-1828
m.(954)372-0236



FDOT Florida Traffic Online (2016)

Zoom to

State Extent

Florida Counties

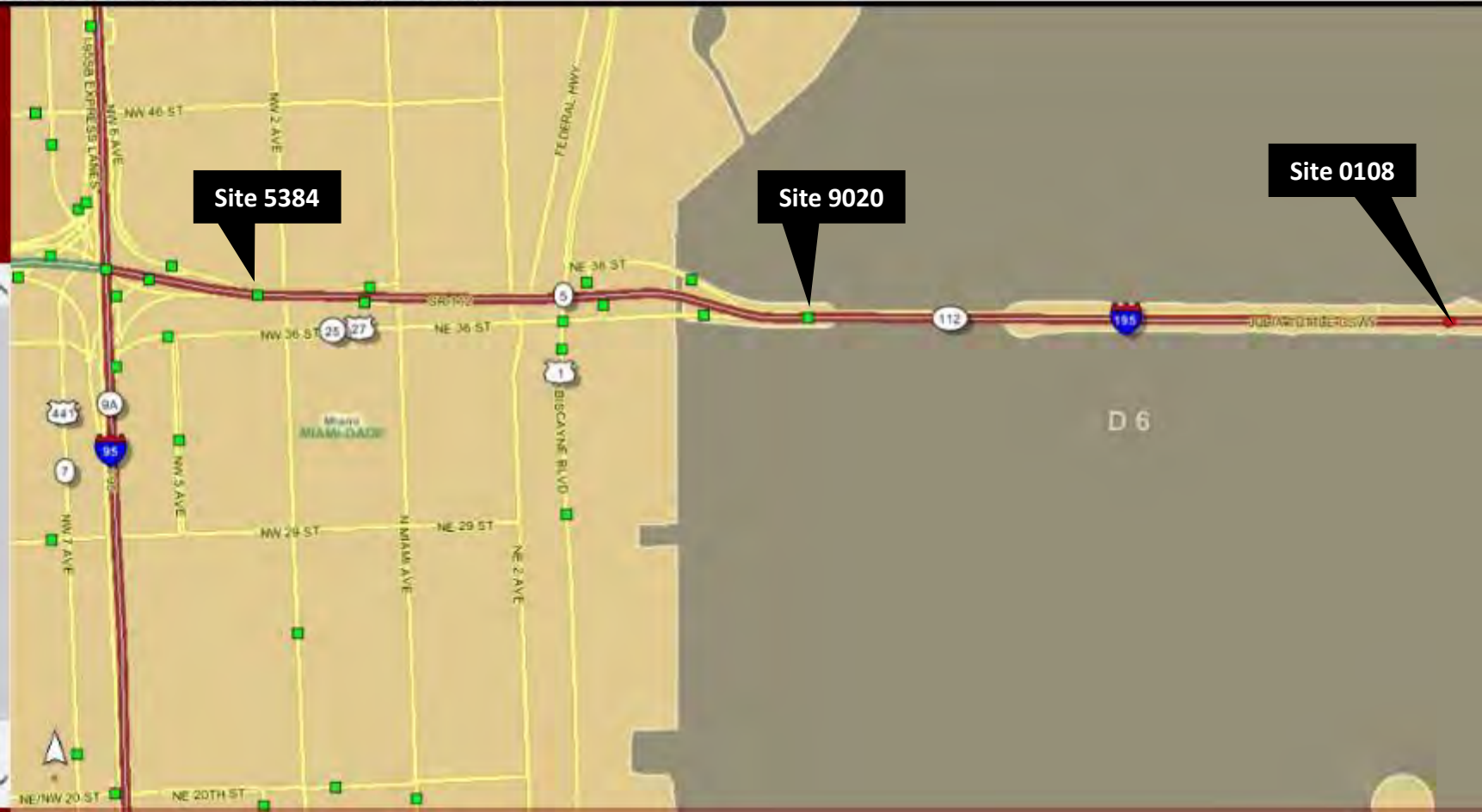
Zoom to a county

Florida Cities

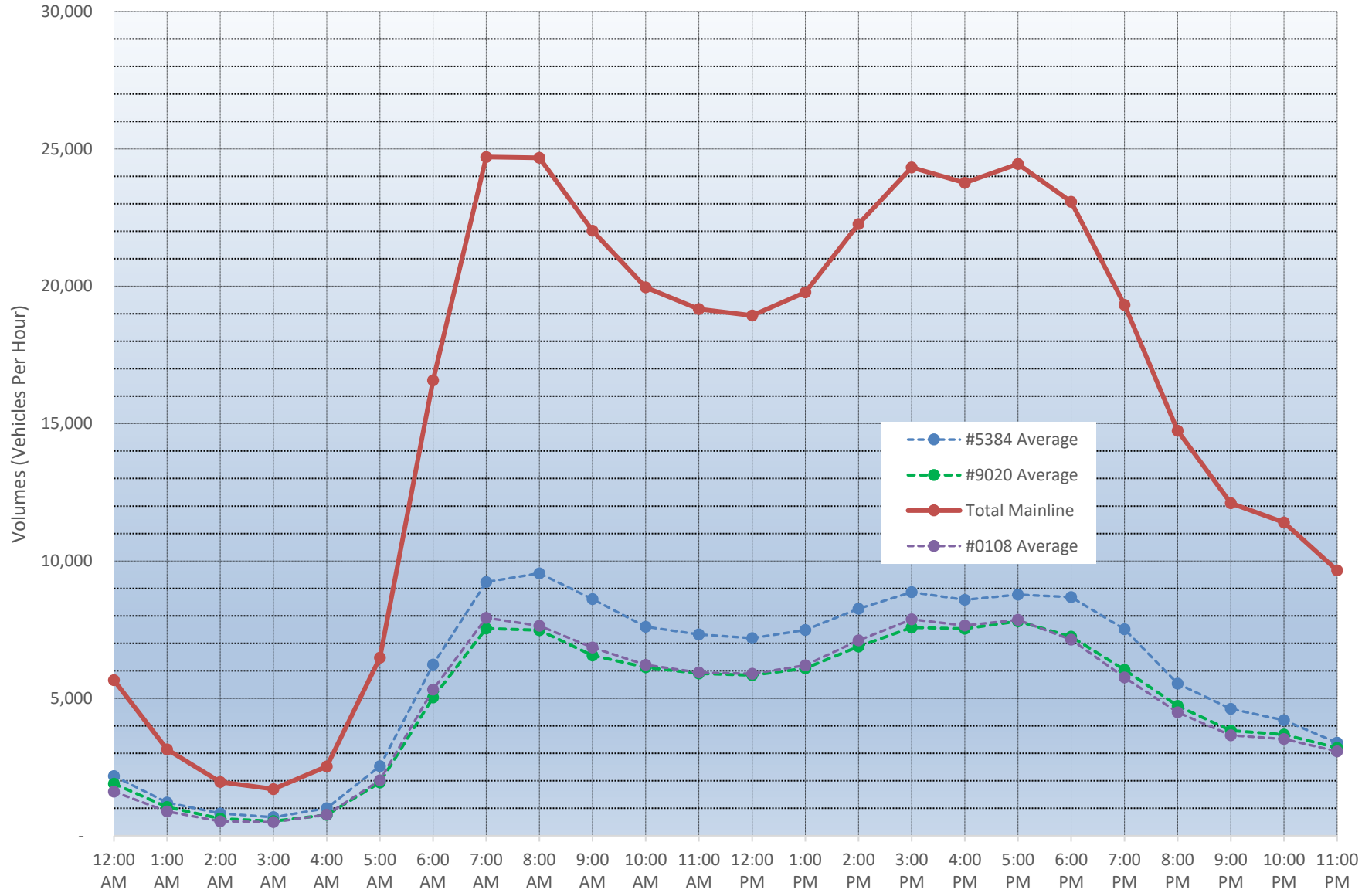
Zoom to a city

LEGEND

- Portable Traffic Monitoring Sites
- Telemetered Traffic Monitoring Sites
- Toll Roads
- Interstates
- Roads
- Rivers
- Lakes
- County Lines
- Cities and Towns
- FDOT Urban Areas
- County Boundaries



Hourly Volume Profile Mainline I-195 Based on FDOT 2016 FTI Data



I-195 Corridor Planning Study
Hourly Volume Profile on Mainline I-195 Based on FDOT 2016 FTI Data

Hour	Station #5384				Station #9020				Station #0108				Total Mainline ¹
	9/27/2016	9/28/2016	9/29/2016	#5384 Average	8/23/2016	8/24/2016	8/25/2016	#9020 Average	8/23/2016	8/24/2016	8/25/2016	#0108 Average	
12:00 AM	2,287	2,114	2,106	2,169	2,237	1,673	1,761	1,890	1,512	1,590	1,697	1,600	5,659
1:00 AM	1,371	1,104	1,160	1,212	1,259	857	1,032	1,049	834	811	1,006	884	3,145
2:00 AM	910	796	722	809	768	496	609	624	489	462	612	521	1,955
3:00 AM	709	633	679	674	591	406	590	529	480	405	600	495	1,698
4:00 AM	1,040	982	983	1,002	776	709	791	759	781	701	811	764	2,525
5:00 AM	2,672	2,487	2,423	2,527	1,979	1,932	1,902	1,938	2,044	1,989	2,025	2,019	6,484
6:00 AM	6,589	6,104	5,988	6,227	5,093	5,125	4,876	5,031	5,245	5,395	5,325	5,322	16,580
7:00 AM	9,615	8,953	9,139	9,236	7,543	7,651	7,423	7,539	7,831	7,954	8,005	7,930	24,705
8:00 AM	10,058	9,494	9,106	9,553	7,434	7,682	7,316	7,477	7,548	7,699	7,675	7,641	24,671
9:00 AM	8,659	8,786	8,396	8,614	6,657	6,640	6,378	6,558	6,935	6,886	6,726	6,849	22,021
10:00 AM	7,443	7,586	7,799	7,609	6,199	6,272	5,926	6,132	6,224	6,267	6,173	6,221	19,963
11:00 AM	7,176	7,530	7,263	7,323	5,923	5,898	5,894	5,905	5,853	5,925	6,042	5,940	19,168
12:00 PM	7,251	7,107	7,229	7,196	6,020	5,746	5,761	5,842	5,964	5,806	5,915	5,895	18,933
1:00 PM	7,620	7,450	7,384	7,485	6,110	6,156	6,003	6,090	6,174	6,175	6,263	6,204	19,778
2:00 PM	8,413	8,232	8,150	8,265	6,864	6,925	6,859	6,883	7,016	7,106	7,213	7,112	22,259
3:00 PM	9,085	8,759	8,763	8,869	7,552	7,608	7,581	7,580	7,718	7,926	7,979	7,874	24,324
4:00 PM	8,258	8,953	8,543	8,585	7,197	7,228	8,171	7,532	7,464	7,472	8,010	7,649	23,765
5:00 PM	8,828	9,057	8,452	8,779	7,658	7,756	8,007	7,807	7,978	7,831	7,771	7,860	24,446
6:00 PM	8,621	8,915	8,507	8,681	7,150	7,113	7,473	7,245	7,169	7,219	7,038	7,142	23,068
7:00 PM	7,749	7,381	7,422	7,517	5,736	5,678	6,700	6,038	5,661	5,616	6,019	5,765	19,321
8:00 PM	5,316	5,664	5,615	5,532	4,445	4,463	5,260	4,723	4,431	4,433	4,608	4,491	14,745
9:00 PM	4,395	4,728	4,736	4,620	3,491	3,599	4,401	3,830	3,437	3,548	3,973	3,653	12,103
10:00 PM	4,184	4,248	4,178	4,203	3,415	3,498	4,123	3,679	3,400	3,437	3,715	3,517	11,399
11:00 PM	3,622	3,422	3,112	3,385	2,865	3,072	3,666	3,201	2,804	3,033	3,387	3,075	9,661

Notes:

1. Total mainline is the sum of the averages at each countsite.

#5384

COUNTY: 87
 STATION: 5384
 DESCRIPTION: SR-112/AIRPORT EXPY. 200' W BR. OVER NW 2 AVE
 START DATE: 09/28/2016
 START TIME: 0000

TIME	DIRECTION: E					DIRECTION: W					COMBINED TOTAL
	1ST	2ND	3RD	4TH	TOTAL	1ST	2ND	3RD	4TH	TOTAL	
0000	236	213	171	184	804	454	353	286	217	1310	2114
0100	124	128	87	81	420	199	167	164	154	684	1104
0200	68	70	68	59	265	146	146	133	106	531	796
0300	44	54	64	65	227	100	120	98	88	406	633
0400	78	106	148	178	510	90	113	141	128	472	982
0500	211	322	469	645	1647	136	169	229	306	840	2487
0600	799	973	1111	1216	4099	326	432	580	667	2005	6104
0700	1216	1242	1255	1241	4954	849	987	1029	1134	3999	8953
0800	1289	1225	1188	1115	4817	1121	1234	1197	1125	4677	9494
0900	1234	1217	1096	1210	4757	1058	997	1008	966	4029	8786
1000	1067	1030	972	826	3895	924	926	911	930	3691	7586
1100	858	911	938	941	3648	939	956	968	1019	3882	7530
1200	867	789	831	870	3357	971	945	895	939	3750	7107
1300	843	838	870	927	3478	931	1050	978	1013	3972	7450
1400	866	904	1073	1007	3850	980	1104	1184	1114	4382	8232
1500	958	911	915	996	3780	1234	1204	1260	1281	4979	8759
1600	961	948	1032	961	3902	1256	1374	1243	1178	5051	8953
1700	1098	1082	1106	1084	4370	1161	1035	1203	1288	4687	9057
1800	1178	1223	1177	1042	4620	1161	1190	1015	929	4295	8915
1900	1042	969	893	886	3790	928	973	878	812	3591	7381
2000	762	668	659	643	2732	770	785	698	679	2932	5664
2100	650	655	581	557	2443	607	625	538	515	2285	4728
2200	562	541	498	566	2167	552	530	527	472	2081	4248
2300	424	357	319	310	1410	542	529	506	435	2012	3422
24-HOUR TOTALS:	69942					70543					140485

PEAK VOLUME INFORMATION

	DIRECTION: E		DIRECTION: W		COMBINED DIRECTIONS	
	HOUR	VOLUME	HOUR	VOLUME	HOUR	VOLUME
A.M.	715	5027	745	4686	745	9629
P.M.	1745	4662	1530	5171	1730	9433
DAILY	715	5027	1530	5171	745	9629

TRUCK PERCENTAGE 5.00 4.09 4.55

CLASSIFICATION SUMMARY DATABASE

DIR	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TOTTRK	TOTVOL
E	369	57076	8998	585	1324	539	372	266	275	137	1	0	0	0	0	3499	69942
W	136	57498	10022	507	1287	598	114	196	164	21	0	0	0	0	0	2887	70543

COUNTY: 87
 STATION: 5384
 DESCRIPTION: SR-112/AIRPORT EXPY. 200' W BR. OVER NW 2 AVE
 START DATE: 09/29/2016
 START TIME: 0000

TIME	DIRECTION: E					DIRECTION: W					COMBINED TOTAL
	1ST	2ND	3RD	4TH	TOTAL	1ST	2ND	3RD	4TH	TOTAL	
0000	300	262	214	194	970	346	332	265	193	1136	2106
0100	149	152	107	104	512	185	177	142	144	648	1160
0200	84	71	72	69	296	126	111	105	84	426	722
0300	54	55	77	68	254	119	106	99	101	425	679
0400	65	114	141	178	498	104	107	130	144	485	983
0500	215	334	459	631	1639	150	167	211	256	784	2423
0600	776	912	1086	1183	3957	307	423	594	707	2031	5988
0700	1210	1313	1298	1296	5117	868	1039	1103	1012	4022	9139
0800	1212	1185	1155	1166	4718	1110	1104	1102	1072	4388	9106
0900	1159	1205	1119	1255	4738	959	955	859	885	3658	8396
1000	1195	1102	1091	1045	4433	813	841	844	868	3366	7799
1100	927	911	947	961	3746	824	894	908	891	3517	7263
1200	902	891	857	926	3576	863	936	950	904	3653	7229
1300	872	890	921	927	3610	932	925	919	998	3774	7384
1400	933	952	1018	1005	3908	994	1082	1093	1073	4242	8150
1500	1029	1012	1038	1001	4080	1112	1229	1160	1182	4683	8763
1600	1031	996	1003	1025	4055	1120	1085	1161	1122	4488	8543
1700	1007	1053	1118	1079	4257	1133	1084	985	993	4195	8452
1800	1217	1185	1148	1186	4736	931	909	994	937	3771	8507
1900	1105	1025	988	940	4058	869	873	839	783	3364	7422
2000	806	697	703	620	2826	768	681	690	650	2789	5615
2100	632	632	613	569	2446	628	570	581	511	2290	4736
2200	561	561	608	538	2268	498	504	483	425	1910	4178
2300	458	396	353	323	1530	347	390	437	408	1582	3112
24-HOUR TOTALS:	72228					65627					137855

PEAK VOLUME INFORMATION

	DIRECTION: E		DIRECTION: W		COMBINED DIRECTIONS	
	HOUR	VOLUME	HOUR	VOLUME	HOUR	VOLUME
A.M.	715	5119	800	4388	715	9383
P.M.	1800	4736	1515	4691	1515	8773
DAILY	715	5119	1515	4691	715	9383

TRUCK PERCENTAGE 5.07 4.08 4.60

CLASSIFICATION SUMMARY DATABASE

DIR	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TOTTRK	TOTVOL
E	315	58755	9499	653	1446	730	177	272	237	142	2	0	0	0	0	3659	72228
W	172	53104	9675	510	1179	533	94	197	146	17	0	0	0	0	0	2676	65627

#9020

COUNTY: 87
 STATION: 9020
 DESCRIPTION: SR 112/I-195/JULIA TUTTLE CSWY, 2900' E US-1 @R108
 START DATE: 08/23/2016
 START TIME: 0000

TIME	DIRECTION: E					DIRECTION: W					COMBINED TOTAL	
	1ST	2ND	3RD	4TH	TOTAL	1ST	2ND	3RD	4TH	TOTAL		
0000	348	302	293	288	1231	337	249	214	206	1006	2237	
0100	240	195	163	115	713	170	146	126	104	546	1259	
0200	91	117	111	113	432	91	85	81	79	336	768	
0300	96	51	62	69	278	71	76	78	88	313	591	
0400	60	80	123	151	414	86	71	97	108	362	776	
0500	149	276	402	567	1394	125	122	154	184	585	1979	
0600	616	854	1032	1083	3585	248	287	445	528	1508	5093	
0700	975	1041	1109	1125	4250	720	797	910	866	3293	7543	
0800	1118	949	817	796	3680	934	944	957	919	3754	7434	
0900	856	947	822	855	3480	828	841	739	769	3177	6657	
1000	793	825	873	791	3282	746	761	650	760	2917	6199	
1100	703	724	688	707	2822	736	821	774	770	3101	5923	
1200	637	697	760	747	2841	799	794	757	829	3179	6020	
1300	712	735	793	780	3020	743	808	754	785	3090	6110	
1400	712	821	899	857	3289	880	838	905	952	3575	6864	
1500	800	728	778	819	3125	1001	1102	1182	1142	4427	7552	
1600	681	725	765	741	2912	1062	1063	1069	1091	4285	7197	
1700	784	823	872	841	3320	1090	1110	1089	1049	4338	7658	
1800	856	966	886	865	3573	990	965	854	768	3577	7150	
1900	874	768	725	658	3025	763	688	702	558	2711	5736	
2000	601	609	554	523	2287	605	606	489	458	2158	4445	
2100	493	460	468	387	1808	440	428	446	369	1683	3491	
2200	461	484	490	458	1893	361	430	352	379	1522	3415	
2300	352	284	271	249	1156	414	473	466	356	1709	2865	
24-HOUR TOTALS:					57810						57152	114962

PEAK VOLUME INFORMATION

	DIRECTION: E		DIRECTION: W		COMBINED DIRECTIONS	
	HOUR	VOLUME	HOUR	VOLUME	HOUR	VOLUME
A.M.	715	4393	800	3754	730	7955
P.M.	1800	3573	1515	4488	1700	7658
DAILY	715	4393	1515	4488	730	7955

TRUCK PERCENTAGE 2.93 2.99 2.96

CLASSIFICATION SUMMARY DATABASE

DIR	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TOTTRK	TOTVOL
E	259	50339	5520	273	711	188	302	107	90	17	3	1	0	0	0	1692	57810
W	245	48895	6303	293	681	208	324	100	82	20	0	1	0	0	0	1709	57152

COUNTY: 87
 STATION: 9020
 DESCRIPTION: SR 112/I-195/JULIA TUTTLE CSWY, 2900' E US-1 @R108
 START DATE: 08/24/2016
 START TIME: 0000

TIME	DIRECTION: E					DIRECTION: W					COMBINED TOTAL	
	1ST	2ND	3RD	4TH	TOTAL	1ST	2ND	3RD	4TH	TOTAL		
0000	212	161	153	113	639	312	284	250	188	1034	1673	
0100	109	70	75	64	318	188	143	105	103	539	857	
0200	62	58	41	54	215	74	87	64	56	281	496	
0300	51	48	49	45	193	51	51	61	50	213	406	
0400	59	60	124	147	390	71	84	70	94	319	709	
0500	172	231	394	560	1357	106	126	159	184	575	1932	
0600	615	851	1064	1136	3666	256	288	413	502	1459	5125	
0700	1092	1041	1159	1163	4455	717	774	871	834	3196	7651	
0800	1173	1022	953	811	3959	893	939	932	959	3723	7682	
0900	901	789	857	883	3430	856	822	784	748	3210	6640	
1000	803	895	764	715	3177	755	734	842	764	3095	6272	
1100	660	661	693	740	2754	698	830	825	791	3144	5898	
1200	631	713	636	697	2677	745	788	754	782	3069	5746	
1300	688	764	748	793	2993	868	764	779	752	3163	6156	
1400	772	801	863	814	3250	830	850	1017	978	3675	6925	
1500	819	784	788	746	3137	1086	1163	1140	1082	4471	7608	
1600	775	703	699	802	2979	1065	1067	1051	1066	4249	7228	
1700	804	828	846	888	3366	1205	1139	1102	944	4390	7756	
1800	915	972	928	911	3726	935	895	801	756	3387	7113	
1900	915	724	710	661	3010	705	693	651	619	2668	5678	
2000	588	557	610	560	2315	546	568	530	504	2148	4463	
2100	424	494	512	473	1903	442	453	418	383	1696	3599	
2200	463	510	532	431	1936	438	410	354	360	1562	3498	
2300	372	335	283	282	1272	433	478	469	420	1800	3072	
24-HOUR TOTALS:					57117						57066	114183

	PEAK VOLUME INFORMATION					
	DIRECTION: E		DIRECTION: W		COMBINED DIRECTIONS	
	HOUR	VOLUME	HOUR	VOLUME	HOUR	VOLUME
A.M.	715	4536	800	3723	730	8054
P.M.	1800	3726	1645	4512	1645	7792
DAILY	715	4536	1645	4512	730	8054
TRUCK PERCENTAGE	2.74		2.85		2.80	

CLASSIFICATION SUMMARY DATABASE																	
DIR	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TOTTRK	TOTVOL
E	188	49387	5978	295	639	132	285	102	88	19	3	1	0	0	0	1564	57117
W	217	48696	6525	293	649	186	312	94	72	21	1	0	0	0	0	1628	57066

COUNTY: 87
 STATION: 9020
 DESCRIPTION: SR 112/I-195/JULIA TUTTLE CSWY, 2900' E US-1 @R108
 START DATE: 08/25/2016
 START TIME: 0000

TIME	DIRECTION: E					DIRECTION: W					COMBINED TOTAL	
	1ST	2ND	3RD	4TH	TOTAL	1ST	2ND	3RD	4TH	TOTAL		
0000	213	210	131	140	694	338	303	212	214	1067	1761	
0100	126	120	106	82	434	186	145	142	125	598	1032	
0200	90	57	45	61	253	107	85	90	74	356	609	
0300	57	52	67	53	229	83	91	84	103	361	590	
0400	59	64	127	153	403	107	99	105	77	388	791	
0500	158	237	383	517	1295	129	155	148	175	607	1902	
0600	567	769	987	1106	3429	247	273	430	497	1447	4876	
0700	968	1023	1136	1028	4155	685	816	848	919	3268	7423	
0800	1012	902	824	912	3650	906	916	932	912	3666	7316	
0900	827	740	773	825	3165	798	838	796	781	3213	6378	
1000	757	700	735	710	2902	675	839	745	765	3024	5926	
1100	763	698	718	732	2911	741	773	697	772	2983	5894	
1200	625	621	669	701	2616	806	807	782	750	3145	5761	
1300	666	657	754	738	2815	771	840	805	772	3188	6003	
1400	774	815	885	835	3309	808	884	928	930	3550	6859	
1500	791	690	780	785	3046	1070	1072	1172	1221	4535	7581	
1600	868	846	866	989	3569	1158	1149	1143	1152	4602	8171	
1700	886	807	889	932	3514	1082	1087	1173	1151	4493	8007	
1800	966	916	998	1023	3903	972	986	847	765	3570	7473	
1900	1127	964	933	863	3887	735	713	687	678	2813	6700	
2000	821	717	726	698	2962	626	565	570	537	2298	5260	
2100	685	678	627	573	2563	484	471	442	441	1838	4401	
2200	537	636	685	660	2518	407	399	400	399	1605	4123	
2300	483	497	487	452	1919	387	428	481	451	1747	3666	
24-HOUR TOTALS:					60141						58362	118503

	PEAK VOLUME INFORMATION					
	DIRECTION: E		DIRECTION: W		COMBINED DIRECTIONS	
	HOUR	VOLUME	HOUR	VOLUME	HOUR	VOLUME
A.M.	645	4233	745	3673	715	7688
P.M.	1800	3903	1530	4700	1600	8171
DAILY	645	4233	1530	4700	1600	8171
TRUCK PERCENTAGE	3.43		2.89		3.16	

CLASSIFICATION SUMMARY DATABASE																	
DIR	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TOTTRK	TOTVOL
E	190	51703	6184	274	677	701	244	94	62	11	1	0	0	0	0	2064	60141
W	215	50185	6276	323	711	198	265	105	70	14	0	0	0	0	0	1686	58362

#01080

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
JANUARY 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: E LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1	F	1730	2423	1904	1303	916	1024	1595	1424	1522	1578	1941	2432	2710	3379	3529	3454	3421	3264	2843	2478	2082	2013	2425	1648	53038H
2	A	1149	682	444	367	490	933	1691	1804	2222	2347	2562	3081	3608	3715	3751	3854	3570	3512	3316	2942	2640	2429	2690	2178	55977N
3	S	1681	1060	651	548	559	910	1559	1484	1842	1948	2193	2549	2863	3278	3707	3307	3188	3097	2774	1791	2182	1839	1740	1187	47937N
4	M	764	463	289	247	451	1380	3667	4533	3928	3575	3053	2937	2916	3185	3795	3509	3624	3948	4224	3320	2174	1954	1825	1583	61344A
5	T	723	392	218	224	445	1348	3684	4599	4084	3691	3185	2929	2935	3129	3673	3620	3587	3756	3915	3116	2430	1954	1873	1162	60672N
6	W	646	336	181	225	414	1336	3515	4478	4276	3765	3056	2899	2956	3204	3511	3565	3832	3830	3833	3616	2484	2410	2133	1297	61798N
7	R	756	443	257	244	440	1379	3667	4524	4183	4009	3427	3289	3257	3388	3893	3473	3636	3588	3895	3663	2607	2161	2078	1505	63762N
8	F	912	456	284	270	455	1343	3752	4578	4360	4009	3371	3272	3398	3425	3984	3935	3924	4200	4284	3646	2680	2454	2355	1800	67147N
9	A	1430	893	545	464	504	937	2027	2210	2650	2729	2751	3021	3347	3495	4026	3688	3457	3310	3214	2870	2768	2626	2665	2239	57866N
10	S	1744	1035	697	514	539	872	1483	1568	1944	2242	2503	2853	2876	3163	3579	3431	3339	3274	3191	2591	2341	1998	1824	1291	50892N
11	M	765	426	263	268	482	1378	3685	4676	4261	3761	3269	3143	3078	3503	3618	3689	3600	3719	3935	3294	2309	1888	1707	1138	61855N
12	T	674	376	222	194	450	1278	3169	3816	3637	4391	3662	2897	3066	3059	3644	3420	3387	3864	3935	3333	2361	1900	1798	1105	59638N
13	W	672	361	181	198	433	1449	3771	4182	4091	3819	3501	3242	3192	3323	3832	3593	3639	3540	4042	3334	2314	2169	1958	1302	62138N
14	R	796	422	250	266	478	1406	3814	4153	4075	3939	3304	3074	2958	3099	3791	3573	3419	3559	3576	3610	2677	2251	2005	1494	61989N
15	F	902	487	317	289	479	1370	3733	4634	4129	3395	3319	3044	2912	3301	3551	3345	3562	3870	3890	3276	2665	2524	2590	1990	63574N
16	A	1403	947	581	539	551	907	2038	2156	2633	2619	2810	3051	3370	3750	4233	4063	3914	3497	3372	3114	2782	2538	2560	2170	59598N
17	S	1829	1326	782	574	630	928	1584	1642	1784	1952	2124	2400	2994	3047	3751	3516	3242	3152	3019	2621	2300	2113	1943	1570	50823N
18	M	1042	630	402	327	528	1349	3337	4171	3689	3336	3155	3127	3359	3392	3672	3562	3644	3635	3537	2780	2118	1988	1857	1220	59857H
19	T	717	360	214	220	494	1519	3833	4704	4232	4055	3397	2953	2879	3208	3648	3482	3627	3938	4235	3627	2451	2046	1639	1227	62705N
20	W	597	350	207	204	447	1410	3315	3204	2537	2783	3915	3150	3046	3227	3682	3584	3472	3599	3929	3560	2503	2287	1905	1340	58253A
21	R	838	420	256	248	527	1467	3818	4781	3981	4026	3491	3202	3264	3315	3953	3569	3833	3902	4149	3538	2629	2393	2181	1508	65289N
22	F	994	581	335	313	478	1415	3300	4304	3662	3747	3026	2700	2910	3216	3271	2981	3970	3906	3723	3158	2697	2448	2570	1905	61610A
23	A	1277	846	523	439	523	935	1958	2530	3036	2861	2670	2692	2969	3396	3858	3738	4154	3878	3874	3128	2754	2557	2671	2155	59422A
24	S	1687	1573	775	735	806	1733	2385	2026	2341	2584	2661	2700	2859	3035	3696	3655	3559	3332	3253	2842	2119	1934	1634	1169	55093S
25	M	832	867	321	254	482	1465	3616	4546	4191	3723	3284	3129	3180	3213	3836	3804	3677	3667	4077	3341	2289	2044	1864	1192	62894A
26	T	768	346	218	221	508	1455	3811	4493	3838	4023	3619	3171	3308	3289	3767	3634	3507	3590	3923	3402	2314	2005	1849	1359	62418N
27	W	593	315	247	205	523	1502	3868	4573	3912	3602	3630	3077	3108	3310	3577	3533	3776	3978	3788	3106	2226	2058	1867	1228	61602N
28	R	723	406	246	223	467	1367	3789	4524	3795	3592	2931	2982	3004	2997	3547	3571	3544	3644	3875	3114	2347	2181	2205	1642	60716N
29	F	873	523	326	256	496	1447	3757	4698	4176	3844	3573	3358	3390	3520	3706	3832	3908	3878	3850	3624	2884	2511	2516	2078	67024N
30	A	1475	921	571	479	486	942	1941	2231	2620	2620	2493	2760	2895	3329	3882	3782	3614	3723	3596	3122	2506	2518	2500	2203	57209N
31	S	1531	1076	713	562	620	973	1630	1633	1958	2041	2517	2883	3090	3419	3674	3638	3560	3600	3043	2549	2296	2182	1805	1267	52260N

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WEEKDAY AVERAGE = 61842 SATURDAY AVERAGE = 58014 SUNDAY AVERAGE = 51401 NUMBER OF GOOD DAYS 31 TOTAL MONTHLY COUNT = 1846400
MONTHLY AVERAGE = 59804

COMMENTS:

"B"=====> BAD DAY 1/1: NEW YEAR'S DAY; 1/18: MLK JR DAY
"N"=====> NORMAL DAY 1/4-8: PUBLIC SCHOOLS; 1/4-11: COLLEGES/UNIVERSITIES - WINTER BREAK ENDS
"A"=====> ATYPICAL DAY 1/11: NATL CHAMPIONSHIP COLLEGE FOOTBALL GAME - ALABAMA VS. CLEMSON (8:30PM EST)
"H"=====> ATYPICAL DAY (HOLIDAY) 1/24: 2016 14TH ANNUAL MIAMI MARATHON
"S"=====> ATYPICAL DAY (SPECIAL EVENT)

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
JANUARY 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: W LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1 F	2105	3484	3136	2794	1983	1256	1105	1283	1175	1284	1588	2051	2282	2352	2661	3145	3420	3780	4506	3863	2757	2102	1980	2147	58239H
2 A	1569	1132	729	659	749	770	803	1444	1534	2023	2234	2693	2912	2816	3082	3512	3563	3955	4016	3522	2785	2121	2121	2378	53122N
3 S	2092	1505	1027	894	966	970	855	1359	1371	1663	2428	2877	3049	2706	2837	3452	3417	3583	3171	2268	1907	1579	1491	1690	49157N
4 M	1012	616	409	385	544	884	1581	3787	4182	3168	3253	3432	3438	3332	3910	4007	4510	4650	4656	3288	2037	1636	1523	1816	62056N
5 T	1317	599	341	313	396	669	1584	3586	3821	3272	3235	3385	3607	3411	3871	4061	4232	4726	3803	2687	2009	1655	1497	1710	59787N
6 W	1020	506	303	260	320	613	1482	3566	4074	3305	3169	3403	3516	3348	4206	4664	3118	4799	4131	2705	2132	1781	1773	1874	60068A
7 R	1265	628	367	349	475	613	1538	3655	3828	3314	3315	3475	3424	3526	3982	4090	3942	4376	4257	3859	2332	1851	1785	2102	62348A
8 F	1526	710	448	405	474	733	1455	3473	3971	3340	3506	3664	3621	3568	4143	4621	4485	4567	4347	3255	2377	2045	1971	2527	65232N
9 A	2510	1847	1147	850	751	717	815	1644	1763	2198	2423	2740	2953	3141	3364	3859	3890	3812	3674	3248	2390	2034	2114	2453	56337A
10 S	2088	1604	1067	883	806	803	767	1216	1372	1810	2298	2605	3413	3112	3190	3656	3513	3624	3481	2685	2070	1646	1537	1760	51006N
11 M	1151	575	407	375	546	822	1667	3701	4169	3473	3262	3301	3597	3302	3918	4714	4783	4630	3579	2676	1844	1630	1466	1685	61273N
12 T	1076	546	320	262	369	700	1516	3628	3954	3643	3655	3589	3677	3365	3970	4621	4721	4665	3559	2607	1997	1636	1484	2046	61606N
13 W	1103	453	294	229	341	699	1547	3652	4080	3442	3259	3393	3503	3556	4119	4872	4885	4164	4110	2633	2112	1823	1670	1891	61830N
14 R	1381	643	428	360	425	665	1532	3572	3937	3372	3248	3322	3621	3570	4024	4755	4083	4298	4180	2626	2250	1712	1669	1866	61539N
15 F	1215	679	355	353	496	694	1405	3456	3867	3237	3174	3533	3571	3680	3963	4042	4148	4283	3460	2716	2269	1837	1973	2362	60768N
16 A	2475	1784	993	689	720	674	841	1477	1685	2191	2498	2740	3040	3096	3229	3728	3849	4126	4018	3210	2597	2290	2293	2555	56798A
17 S	2163	1626	1133	911	854	927	804	1250	1258	1691	2222	2802	2997	2899	2944	3431	3388	3541	3372	2793	2257	1736	1682	1956	50637N
18 M	1399	788	520	511	638	896	1284	2376	2949	2884	3431	3773	4040	3950	3620	4833	4890	4801	3938	2802	1998	1728	1594	1850	61493H
19 T	1137	565	340	281	426	718	1542	3725	4140	3431	3215	3477	3430	3517	3977	4888	4823	4336	4098	3054	2222	1795	1987	2213	63337A
20 W																									51400B
21 R	1265	593	391	286	450	746	1503	3615	4132	3352	3296	3503	3581	3526	4036	4946	4675	4706	4125	3058	2371	2048	1394	1306	62904N
22 F	1303	696	464	387	483	714	1489	3401	3745	3218	3480	3482	3559	3476	3778	3643	3566	2728	2663	3178	2164	1855	1897	2040	57409A
23 A	1688	1089	808	571	704	663	848	1507	1796	2270	2768	3066	3096	3126	3280	3860	3401	3680	3415	3147	2491	2109	2064	2468	53915N
24 S	2036	1491	1108	833	1083	1636	1646	1929	2306	2869	3729	3630	3168	3070	3030	3354	3150	3512	3530	2901	2193	1810	1990	2026	58030S
25 M	1192	658	391	378	596	852	1593	3420	3949	3348	3399	3276	3455	3485	3834	4721	4560	4453	4084	2817	2065	1622	1502	1798	61448N
26 T	1107	477	263	270	439	678	1562	3732	4070	3403	3174	3398	3291	3233	3850	4919	4768	4642	3973	3031	2028	1665	1542	1994	61509N
27 W	1121	523	324	244	364	677	1569	3588	4098	3443	3161	3526	3524	3465	4058	4724	4715	4272	3773	2577	2070	1655	1451	1779	60701N
28 R	1061	525	331	330	381	641	1536	3642	3995	3239	3057	3179	3204	3345	3744	3882	3981	4167	3700	2832	2282	1653	1651	1839	58197N
29 F	1273	623	442	364	468	753	1471	3561	3905	3437	3290	3609	3673	3626	4232	4810	4521	4606	3948	3043	2386	2093	2032	2264	64430N
30 A	1693	1209	958	840	705	697	839	1533	1849	2168	2416	2787	2929	2783	3054	3502	3776	3619	3004	3198	2435	1975	2514	2445	52928N
31 S	2065	1377	1110	876	899	868	851	1351	1424	1889	2438	2699	3034	2884	3109	3618	3438	3743	3484	2747	2496	2226	1799	1972	52397A

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WEEKDAY AVERAGE = 61291 SATURDAY AVERAGE = 54620 SUNDAY AVERAGE = 52245 NUMBER OF GOOD DAYS 30 TOTAL MONTHLY COUNT = 1760501
MONTHLY AVERAGE = 59046

COMMENTS:

"B"=====> BAD DAY
"N"=====> NORMAL DAY
"A"=====> ATYPICAL DAY
"H"=====> ATYPICAL DAY (HOLIDAY)
"S"=====> ATYPICAL DAY (SPECIAL EVENT)

1/1: NEW YEAR'S DAY; 1/18: MLK JR DAY
1/4-8: PUBLIC SCHOOLS; 1/4-11: COLLEGES/UNIVERSITIES - WINTER BREAK ENDS
1/11: NATL CHAMPIONSHIP COLLEGE FOOTBALL GAME - ALABAMA VS. CLEMSON (8:30PM EST)
1/24: 2016 14TH ANNUAL MIAMI MARATHON

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
FEBRUARY 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: E LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1	M	797	492	247	261	502	1432	3389	4127	3812	3505	3585	3017	2945	3104	3641	3660	3483	3839	3908	2949	2222	1857	1674	1034	59482N
2	T	621	326	180	218	481	1438	3803	4778	3940	3995	3169	3158	3350	3484	3880	3475	3392	3625	3886	3238	2344	2037	1802	1082	61702N
3	W	635	310	204	209	468	1445	3844	4632	4031	4013	3505	3044	3290	3272	3718	3715	3642	4005	4037	3096	2311	2240	2034	1382	63082N
4	R	680	404	276	237	494	1421	3812	4525	4129	4029	3485	3184	3331	3256	3715	3512	3782	3802	3929	3619	2765	2423	2103	1587	64500N
5	F	875	493	318	226	515	1435	3636	4640	4315	3983	3493	3350	3277	3521	3919	3865	3612	3876	4067	3524	2761	2689	2354	1996	66740N
6	A	1510	847	597	483	540	955	2081	2299	2581	2533	2470	2681	2878	3221	3802	3669	3521	3714	3305	3188	2714	3001	2513	1999	57102N
7	S	1491	945	668	512	474	841	1535	1612	1873	2037	2281	2534	2733	2819	3458	3379	3388	3567	3111	2123	1702	1838	1938	1648	48507S
8	M	738	390	213	239	504	1406	3750	4397	4336	3765	3548	3201	3403	3347	3781	3298	3505	3730	4122	3293	2223	1924	1777	1099	61989N
9	T	597	348	201	205	489	1405	3849	4716	4045	3965	3316	3081	3100	3264	3798	3557	3389	3196	3602	3462	2463	2026	2135	1546	61755N
10	W	673	297	217	216	445	1349	3851	4887	3989	4014	3483	3150	3643	3637	3959	3692	3796	3832	4122	3590	2637	2476	2027	1406	65388N
11	R	750	400	245	220	460	1471	3819	4786	4091	3750	3999	3606	3240	3613	4011	3757	3779	2348	4227	3736	3545	2548	2382	1646	66429A
12	F	1039	562	325	289	512	1426	3832	4435	3623	3483	4048	3732	3857	3986	4267	3749	3857	3841	4000	3563	3010	2601	2590	1964	68591N
13	A	1434	919	546	487	535	919	2048	2328	2751	2837	2700	3004	3220	3723	4123	3695	3655	3558	3332	3436	3040	2794	2755	2161	60000N
14	S	1653	1144	708	570	613	884	1620	1649	2063	2489	2611	3011	3270	3542	3500	3701	3984	3936	3701	3375	2787	2336	2082	1529	56758S
15	M	961	609	352	322	529	1339	3446	4067	3776	3369	3155	3108	3217	3199	3813	3352	3714	3688	3356	2672	2191	1847	1924	1151	59157N
16	T	651	386	248	243	519	1479	3696	4549	3655	3175	3004	2857	3048	3223	3658	3342	3713	3630	4077	3254	2423	2014	1863	1285	59992N
17	W	763	378	233	217	466	1391	3876	4786	4041	3914	3519	3433	3441	3486	3659	3762	3280	3616	3561	3721	2443	2375	2154	1593	64108N
18	R	859	422	248	238	467	1358	3892	4696	3976	4057	3801	3675	3487	3651	3905	3832	3683	3853	3981	3129	2749	2341	2313	1571	66184N
19	F	975	535	293	257	511	1468	3990	4794	4026	3891	3797	3601	3507	3728	3855	3880	3839	4112	4214	3859	3065	2566	2617	1969	69349N
20	A	1369	868	569	461	548	971	2164	2351	2896	3112	3590	3144	2963	3282	3980	3734	4182	3966	3599	3463	2698	2569	2856	2312	61647A
21	S	1747	1155	748	628	612	949	1681	1689	2005	2152	2520	2865	3175	3564	3974	3643	3663	3207	3144	2744	2201	2091	1832	1261	53250N
22	M	747	515	277	252	510	1441	3789	4569	4219	3708	3512	3469	3411	3376	3725	3544	3545	3643	3868	3354	2308	2068	2114	1338	63302N
23	T	687	364	226	195	466	1434	3932	4763	4011	3995	3334	3088	3024	3368	3745	3294	3277	3588	3914	3240	2393	2014	1997	1208	61557N
24	W	611	362	231	196	451	1409	3785	4568	4232	4092	3406	3236	3396	3491	3745	3842	3558	3791	3797	3444	2339	2223	2463	1540	64208S
25	R	853	415	290	245	530	1448	3855	4608	3688	3862	3606	3090	3555	3467	4025	4018	3778	3809	4129	3834	2736	2471	2436	1711	66459S
26	F	954	518	315	315	513	1406	3750	4687	4230	3683	3739	3682	3438	3329	3528	3842	4359	4519	4431	3733	2881	2736	2744	2190	69522S
27	A	1604	974	643	506	551	920	2038	2235	2720	2781	2765	3083	3418	3598	4088	4032	4023	3811	3706	3207	2537	2555	2763	2421	60979N
28	S	1740	1079	698	603	666	982	1659	1697	2111	2272	2544	2965	3274	3460	3950	3515	3511	3754	3154	2985	2213	2024	1786	1250	53892N
29	M	849	471	253	260	534	1412	3859	4663	4136	3950	3344	3125	2901	3287	3756	3363	3408	3544	3667	3246	2128	1955	1752	1163	61026N

WEEKDAY AVERAGE = 64177 SATURDAY AVERAGE = 59932 SUNDAY AVERAGE = 53102 NUMBER OF GOOD DAYS 29 TOTAL MONTHLY COUNT = 1796657
MONTHLY AVERAGE = 61988

COMMENTS:

- "B"=====> BAD DAY
 - "N"=====> NORMAL DAY
 - "A"=====> ATYPICAL DAY
 - "H"=====> ATYPICAL DAY (HOLIDAY)
 - "S"=====> ATYPICAL DAY (SPECIAL EVENT)
- 2/7: SUPERBOWL 50 - CAROLINA VS DENVER - SANTA CLARA CA - 6:30PM
2/14: VALENTINE'S DAY; 2/15: PRESIDENT'S DAY
2/24-28: SOUTH BEACH FOOD AND WINE FESTIVAL

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
FEBRUARY 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: W LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY																				
1	M	1182	550	429	400	662	851	1572	3644	3923	3209	3227	3474	3441	3291	3895	4325	4440	4395	3492	2642	1936	1553	1453	1706	59692N																				
2	T	1078	470	308	296	373	732	1546	3664	3987	3389	3099	3231	3110	3419	3997	4550	4271	4475	4256	2902	2071	1721	1664	1986	60595N																				
3	W	1097	484	294	212	300	609	1584	3642	3957	3359	3182	3477	3526	3354	3950	4628	4651	4567	2937	1748	2547	1804	1697	2003	59609A																				
4	R	1344	589	441	351	415	725	1607	3689	3889	3377	3196	3482	3463	3427	3972	4806	4700	4729	3931	3017	2300	1967	1774	2037	63228N																				
5	F	1363	708	455	396	441	704	1485	3310	3556	3283	3300	3610	3898	3819	4206	4901	4422	4169	4058	2867	2264	2020	1914	2201	63350N																				
6	A	1822	1209	824	704	684	779	899	1663	1925	2388	2603	2768	3063	2826	3056	3601	3529	3629	3213	2769	2317	2053	1969	2434	52727N																				
7	S	2228	1354	990	767	763	763	804	1287	1394	1811	2442	2862	3077	3040	2956	3336	3278	3399	3053	2204	1686	1542	1801	1975	48812N																				
8	M	1026	442	323	324	445	803	1544	3620	3946	3368	3259	3296	3352	3459	3955	4025	3648	3597	4697	3718	2185	1580	1472	1645	59729A																				
9	T	913	462	303	245	361	665	1566	3733	4035	3382	3187	3359	3575	3369	4009	4711	4558	3539	3781	3063	2117	1771	1591	1755	60050N																				
10	W	980	459	297	232	318	654	1538	3637	4134	3412	3378	3463	3470	3617	4192	4876	4645	4414	4238	2913	2403	1838	1762	1867	62737N																				
11	R	1145	532	369	316	430	666	1635	3639	4035	3485	3182	3386	3594	3428	4010	4916	4059	4072	4432	3247	2563	2102	2244	2356	63843N																				
12	F	1491	743	499	413	498	748	1489	3373	3755	3292	3320	3538	3502	3615	4103	4633	4435	4116	4329	3688	2546	2183	2043	2377	64729N																				
13	A	1927	1395	909	785	633	705	883	1568	1922	2351	2614	2835	2992	3825	4072	4864	4583	4294	3656	3487	2631	2231	2425	2562	60149A																				
14	S	2242	1572	1123	878	850	850	747	1323	1436	1954	2461	2845	3179	2926	2926	3424	3378	3588	3510	3275	2384	2115	2186	2248	53420S																				
15	M	1723	984	547	443	581	852	1263	2526	3011	3139	3423	3654	3648	3855	3853	4658	4541	4256	3483	2560	2030	1590	1463	1642	59725A																				
16	T	1021	509	333	295	419	777	1502	3269	3387	3287	2938	3068	3178	3170	3723	4627	4372	4584	3677	2830	2120	1766	1502	1774	58128N																				
17	W	1007	507	282	227	357	696	1532	3780	3986	3502	3133	3190	3240	3504	3772	3618	4104	4298	4321	4167	2379	1912	1641	1772	60927A																				
18	R	1174	580	407	380	448	695	1634	3633	4019	3480	3260	3474	3706	3681	4220	4797	4575	4569	4594	3265	2451	2102	1787	2008	64939N																				
19	F	1339	763	413	399	532	802	1596	3684	3974	3474	3543	3737	3867	3889	4214	4441	3564	4629	4417	3418	2430	2132	2143	2456	65856N																				
20	A	2119	1195	884	744	864	849	883	1773	1931	2424	2676	3141	3177	3083	3240	3747	3794	3766	3448	2863	2424	2157	2616	2637	56435A																				
21	S	2275	1548	1079	893	915	943	886	1406	1553	1858	2448	2756	2908	2977	2926	3322	3468	3700	3677	3249	2389	1730	1623	1793	52322N																				
22	M	1076	563	337	355	496	918	1660	3780	4107	3313	3304	3280	3379	3327	4006	4708	4713	4607	4216	3001	2075	1562	1403	1720	61906N																				
23	T	1158	560	349	341	431	755	1675	3687	3864	3017	3025	3284	3397	3332	4094	4841	3995	4240	3833	2893	2057	1621	1539	1712	59700N																				
24	W	995	449	270	245	332	719	1621	3682	3928	3277	3258	3360	3269	3325	4272	4710	4438	4235	4189	2874	2156	1794	1731	1889	61018N																				
25	R	1104	623	406	363	414	709	1586	3700	3809	3396	3310	3555	3644	3461	4035	4727	4510	4015	4198	3111	2550	2058	2050	2238	63572N																				
26	F	1394	830	501	403	471	774	1555	3564	3745	3313	3295	3580	3569	3427	4050	4709	4083	3754	4454	3481	2686	2278	2481	2687	65084S																				
27	A	1985	1430	1086	893	886	742	923	1630	1946	2368	2711	2902	3076	2960	3216	4072	3946	3913	4020	3611	2638	2180	2419	2949	58502S																				
28	S	2116	1490	1116	915	960	963	889	1403	1509	1962	2720	2897	3167	3103	3064	3506	3504	4176	4230	3227	2185	2019	1633	1735	54489A																				
29	M	1145	606	409	378	580	974	1657	3717	4006	3232	3449	3491	3315	3219	3902	4726	4687	4516	3510	2819	1853	1563	1507	1678	60939N																				
WEEKDAY AVERAGE =		61948					SATURDAY AVERAGE =					56953					SUNDAY AVERAGE =					52261					NUMBER OF GOOD DAYS					29					TOTAL MONTHLY COUNT =					1736212				
MONTHLY AVERAGE =		59851																																												

COMMENTS:

"B"=====> BAD DAY
 "N"=====> NORMAL DAY
 "A"=====> ATYPICAL DAY
 "H"=====> ATYPICAL DAY (HOLIDAY)
 "S"=====> ATYPICAL DAY (SPECIAL EVENT)

2/7: SUPERBOWL 50 - CAROLINA VS DENVER - SANTA CLARA CA - 6:30PM
 2/14: VALENTINE'S DAY; 2/15: PRESIDENT'S DAY
 2/24-28: SOUTH BEACH FOOD AND WINE FESTIVAL

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
MARCH 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: E LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY							
1	T	670	327	236	212	460	1416	3882	4647	4064	3766	3247	3060	2955	3213	3604	3413	3411	3403	3739	3099	2327	2003	2247	1202	60603N							
2	W	636	338	215	206	467	1402	3911	4701	3915	3891	3438	3244	3149	3396	3798	3443	3569	3394	3598	3392	2385	2170	1947	1365	61970N							
3	R	804	429	300	263	449	1448	3839	4776	4054	3839	3366	3233	3324	3382	3915	3704	3661	3700	3786	3333	2483	2370	2316	1617	64391N							
4	F	936	534	331	294	521	1429	3858	4799	4121	3870	3492	3487	3660	3654	3712	3746	3989	3723	3831	3371	2712	2686	2631	2032	67419N							
5	A	1477	935	587	510	562	988	2375	2563	2560	2536	2595	2855	3169	3651	3884	3923	3699	3511	3491	3009	2786	2582	2415	2224	58887N							
6	S	1684	1216	779	644	702	1082	1556	1559	2228	2614	3043	3137	3396	3736	3445	3549	3582	3102	3042	2684	2353	2261	1834	1315	54543N							
7	M	838	522	323	268	564	1480	3677	4680	4071	3722	3321	3033	3277	3330	3732	3554	3400	3549	3873	3091	2300	1982	1826	1284	61697N							
8	T	724	392	253	257	511	1441	3709	4679	3931	3867	3318	3193	3163	3392	3702	3575	3527	3525	3971	3209	2336	2116	1952	1242	61985N							
9	W	696	415	245	247	503	1394	3720	4400	3898	3408	3180	3445	3533	3628	3848	3804	3791	3742	3931	3439	2615	2306	2348	1615	64151N							
10	R	950	563	410	321	484	1491	3816	4683	3830	3967	3628	3450	3489	3671	3909	3881	3835	3972	3963	3418	3054	2566	2422	1859	67632N							
11	F	1127	687	444	371	584	1399	3969	4682	4030	4003	3736	3530	3672	3740	3779	3775	4167	4099	4217	3734	3067	2837	2715	2215	70579N							
12	A	1690	1190	886	697	712	1072	2105	2325	2525	2606	2857	3030	3158	3632	4180	3914	3792	3731	3400	3346	2855	2726	3140	2581	62150N							
13	S																									55439B							
14	M	1083	645	439	353	599	1564	3649	4384	4013	3990	3639	3315	3401	3555	3860	3643	3701	3697	3877	3169	2630	2249	2468	1492	65415A							
15	T	1004	547	376	328	554	1502	3647	4280	3982	3985	3489	3260	3346	3552	4055	3480	3606	3728	3831	3554	2971	2404	2355	1991	65827A							
16	W	1154	595	409	363	565	1516	3715	4516	4024	3911	3557	3499	3504	3937	4052	4016	3930	3917	4151	3829	2965	2720	2493	1950	69288A							
17	R	1218	691	451	391	625	1535	3685	4579	3604	3633	3848	3698	3695	4179	4217	4266	4168	4049	4040	3699	3391	3031	3141	2285	72119A							
18	F	1700	1061	777	757	783	1639	3658	4411	4351	4154	3852	3782	4000	4093	3598	3385	4161	3145	4535	3579	2935	2974	3015	2519	72864S							
19	A	2620	1831	993	754	753	1127	2049	2188	2512	2613	2866	3155	3577	3830	4265	4164	4063	3400	3777	3259	2822	2778	3402	3002	65800S							
20	S	2612	1986	1062	895	859	1151	1650	1564	2022	2065	2448	3261	3630	3963	3877	4106	3840	3523	3323	2565	2364	2431	2230	2101	59528S							
21	M	1560	799	529	474	666	1623	3640	4481	3950	3349	3274	3340	3653	3665	4044	3825	3640	3990	4289	3408	2768	2303	2022	1430	66722A							
22	T	836	462	300	271	524	1468	3533	4445	4086	3473	3211	3093	3116	3412	3607	3332	3503	3675	3902	3456	2758	2327	2177	1421	62388N							
23	W	839	443	317	243	449	1388	3487	4012	4089	3546	3257	3436	3868	3865	4100	3409	3513	3620	4018	3660	3213	2811	2555	1813	65951N							
24	R	1038	638	394	326	475	1365	3413	4203	3870	3471	3262	3378	3592	3500	3897	3721	3657	3704	4047	2967	2743	2745	2554	1839	64799N							
25	F	1168	737	503	392	487	1392	3333	4043	3579	3143	3015	3211	2413	4085	3766	3782	3500	3613	3223	2793	2702	2399	2763	2462	62504A							
26	A	1769	1083	697	558	596	993	1838	1969	2306	2493	2654	3109	3530	3982	3984	3691	3724	3239	3322	2827	2790	2648	2886	2243	58931N							
27	S	1746	1226	802	617	625	853	1570	1526	1848	2018	2449	2630	3010	3224	3679	3371	3047	2835	2929	2484	2376	2124	1921	1412	50322N							
28	M	884	529	306	277	491	1374	3616	4216	4033	3492	3279	3105	3192	3315	3299	3570	3472	3715	3643	2861	2391	2153	2416	1296	60925A							
29	T	783	410	270	220	448	1411	3672	4453	4066	3643	3350	3139	3015	3239	3433	3188	3125	3338	3805	2904	2353	2242	2108	1307	59922N							
30	W	739	432	222	202	427	1386	3637	4567	4160	3581	3619	3086	3046	3273	3618	3651	3492	3575	3640	3140	2641	2256	2111	1518	62019N							
31	R	929	506	308	270	422	1437	3624	4515	3985	3739	3312	3224	3209	3459	3937	3727	3609	3542	3936	3107	2818	2594	2429	1895	64533N							
WEEKDAY AVERAGE =		65110				SATURDAY AVERAGE =				61442				SUNDAY AVERAGE =				54798				NUMBER OF GOOD DAYS				30		TOTAL MONTHLY COUNT =				1905864	
MONTHLY AVERAGE =		63113																															

COMMENTS:

"B"=====> BAD DAY
 "N"=====> NORMAL DAY
 "A"=====> ATYPICAL DAY
 "H"=====> ATYPICAL DAY (HOLIDAY)
 "S"=====> ATYPICAL DAY (SPECIAL EVENT)

3/13: DAYLIGHT SAVINGS TIME BEGINS
 3/18-20: ULTRA MUSIC FESTIVAL (DOWNTOWN MIAMI)
 SPRING BREAK: 3/1-19 - COLLEGES; 3/21-25 - PUBLIC SCHOOLS
 3/17: ST PATRICK'S DAY; 3/27: EASTER SUNDAY

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
MARCH 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: W LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY																				
1	T	1021	499	329	270	404	750	1582	3597	3948	3315	3153	3238	3191	3241	3845	4860	4610	3615	3798	2849	1985	1568	1516	1704	58888N																				
2	W	976	456	271	243	325	704	1588	3680	3964	3173	3155	3232	3482	3334	4179	4614	4049	4165	3682	2936	2114	1765	1673	1885	59645N																				
3	R	1065	607	426	341	446	703	1591	3648	3800	3330	3144	3409	3511	3539	4117	4957	4520	4452	3653	3004	2304	1841	1794	1922	62124N																				
4	F	1280	768	460	349	486	758	1513	3489	3763	3377	3311	3524	3559	3749	4192	4766	4478	4541	3724	3035	2420	2288	2229	2331	64390N																				
5	A	1784	1168	894	781	792	820	929	1657	1880	2242	2601	3210	3086	2957	3395	3565	3555	3382	3148	2944	2733	2277	2208	2629	54637N																				
6	S	2120	1616	1130	983	1076	1017	951	1458	1592	1961	2498	2875	3104	2942	3045	3861	4054	4341	4218	3733	2572	1915	1745	1818	56625N																				
7	M	1103	671	485	418	641	911	1677	3627	3913	3387	3119	3396	3362	3412	3988	4620	4710	4539	3551	2843	2020	1640	1422	1668	61123N																				
8	T	1004	540	373	358	478	767	1622	3599	3826	3203	3287	3412	3384	3265	3954	4881	4677	4414	3585	3041	2074	1784	1679	1809	61016N																				
9	W	1093	590	386	275	369	685	1613	3550	3743	3333	3189	3350	3477	3312	4017	4785	4601	4445	3843	3169	2266	1882	1786	2035	61794N																				
10	R	1305	717	552	493	614	804	1682	3644	4027	3508	3391	3499	3627	3476	4015	4818	4635	4483	4180	3830	2641	2164	2078	2200	66383N																				
11	F	1875	1009	635	518	614	887	1617	3571	3827	3331	3251	3443	3587	3536	4171	4480	4544	4776	4169	3267	2677	2309	2381	2678	67153A																				
12	A	2135	1548	1074	1034	982	922	1041	1758	1905	2321	2569	2896	2918	2802	3042	3603	3406	3623	3739	3543	3046	2481	2478	2915	57781A																				
13	S																									56737B																				
14	M	1362	818	585	485	686	1058	1550	3437	3943	3734	3641	3327	3477	3501	3882	4746	4597	4631	3906	3152	2422	1846	1750	1925	64461N																				
15	T	1208	790	490	458	544	804	1614	3471	3827	3373	3162	3357	3330	3392	4028	4116	3990	3955	3960	3060	2750	2143	2021	2245	62088A																				
16	W	1440	658	462	370	478	911	1567	3401	4046	3409	3326	3448	3419	3405	4215	4846	4791	4464	3888	3361	2843	2228	2224	2826	66026A																				
17	R	1475	874	549	519	605	965	1628	3360	3873	3416	3301	3423	3659	3535	4082	4658	4584	4595	4329	3806	4040	3631	2636	2663	70206A																				
18	F	2046	1292	955	887	915	1069	1640	3209	3702	3246	3285	3460	3609	3540	3836	4836	4495	4503	3787	3657	3006	2622	2740	2959	69296S																				
19	A	3346	2659	1458	1112	1055	1144	1060	1595	1776	2155	2466	2768	2899	2972	3172	3687	3677	3874	3711	3460	3133	2649	2902	3154	61884S																				
20	S	2780	2271	1638	1383	1236	1304	1112	1405	1443	1770	2330	2756	2986	2904	3164	3634	3675	4008	4016	3285	2695	2072	2034	2332	58233S																				
21	M	1797	1173	867	756	849	1202	1608	3096	3627	3317	3276	3432	3403	3595	3821	4789	4745	4668	3568	2995	2467	1946	1755	2080	64832A																				
22	T	1280	715	461	400	518	750	1370	3033	3545	3325	3172	3442	3515	3400	3701	4728	4438	4393	3453	3032	2448	1835	1782	1918	60654N																				
23	W	1303	717	391	323	425	697	1347	2861	3644	3312	3407	3802	3378	3510	3810	4436	4379	4390	3951	3161	2594	2098	1981	2498	62415N																				
24	R	1985	900	567	485	536	750	1298	2709	3346	3298	3397	3611	3526	3494	3819	4732	4743	4338	3428	2983	2248	2094	1915	2134	62336A																				
25	F	1566	954	708	546	560	796	1289	2271	2900	3012	3179	3437	3512	3498	3910	4653	3764	4121	3426	2969	1805	2270	2141	2195	59482A																				
26	A	1847	1491	1088	991	894	847	940	1448	1750	2139	2436	2686	2717	2737	2952	3496	3370	3673	3544	3606	3172	2449	2336	2633	55242A																				
27	S	2321	1640	1228	961	872	913	820	1284	1413	1763	2317	2552	2879	2838	2738	3137	3084	3383	3388	3253	2823	1838	1582	1762	50789N																				
28	M	1149	631	394	359	567	851	1501	3230	3916	3385	3329	3271	3334	3172	3847	4790	4138	4693	3915	2909	2143	1628	1595	1788	60535N																				
29	T	1070	639	386	319	407	751	1538	3295	3920	3341	3182	3417	3396	3318	4015	4702	3984	4376	3285	2736	2115	1719	1515	1723	59149N																				
30	W	1113	604	352	217	340	685	1459	3279	3804	3480	3334	3402	3683	3461	4099	4724	4689	4287	3411	2807	2268	1755	1584	1782	60619N																				
31	R	1160	623	427	369	459	714	1503	3281	3805	3441	3374	3198	3457	3516	3901	4661	4138	4548	3644	3084	2536	2015	1844	2103	61801N																				
WEEKDAY AVERAGE =		62969					SATURDAY AVERAGE =					57386					SUNDAY AVERAGE =					55216					NUMBER OF GOOD DAYS					30					TOTAL MONTHLY COUNT =					1841607				
MONTHLY AVERAGE =		61064																																												

COMMENTS:

"B"=====> BAD DAY
 "N"=====> NORMAL DAY
 "A"=====> ATYPICAL DAY
 "H"=====> ATYPICAL DAY (HOLIDAY)
 "S"=====> ATYPICAL DAY (SPECIAL EVENT)

3/13: DAYLIGHT SAVINGS TIME BEGINS
 3/18-20: ULTRA MUSIC FESTIVAL (DOWNTOWN MIAMI)
 SPRING BREAK: 3/1-19 - COLLEGES; 3/21-25 - PUBLIC SCHOOLS
 3/17: ST PATRICK'S DAY; 3/27: EASTER SUNDAY

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
APRIL 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: E LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY			
1	F	1132	621	383	358	481	1408	3585	4497	4142	3844	3477	3417	3574	3661	3825	3786	3523	3766	3979	3458	2976	2854	2792	2188	67727N			
2	A	1618	977	661	569	572	996	2009	2373	2984	2841	2709	2860	3170	3509	3796	3824	3673	3355	3355	2680	2549	2512	2759	2295	58646N			
3	S	1686	1178	795	635	620	1010	1456	1372	1600	1502	2022	2923	2900	3343	3328	3431	3411	3114	3157	2513	2302	2024	1786	1420	49528N			
4	M	832	499	310	289	520	1453	3635	4615	4015	3590	3320	3012	2821	3048	3524	3564	3511	3518	3675	2854	2300	1973	1748	1134	59760N			
5	T	650	352	274	236	458	1481	3690	4223	3545	3722	3522	3084	3058	3195	3619	3241	3360	3324	3607	3280	2296	1878	2070	1345	59510N			
6	W	738	347	225	229	455	1418	3637	4732	3850	3646	3309	2932	2943	3265	3614	3462	3570	3604	3781	3042	2497	2341	2070	1300	61007N			
7	R	783	439	285	287	458	1451	3790	4697	3856	3893	3310	3352	3337	3370	3660	3508	3435	3634	3931	3107	2686	2390	2371	1901	63931N			
8	F	1081	640	364	365	488	1429	3665	4291	4244	3801	3659	3517	3714	3748	3984	3926	3751	3885	3697	3464	2916	2723	2515	2162	68029N			
9	A	1667	950	673	488	517	982	2039	2356	2584	2661	2931	3199	3551	3889	2047	2656	3966	3805	3593	3120	2633	2603	2757	2362	58029A			
10	S	2089	1292	845	676	656	914	1623	1754	2247	2891	3308	3359	3526	3597	3654	3706	3283	3195	2776	2466	2296	2022	1759	1376	55310N			
11	M	822	463	306	304	518	1475	3707	4659	3595	3844	3372	3088	3257	3366	3822	3283	3382	3374	3708	2934	2229	1961	1726	1171	60366N			
12	T	622	332	239	250	477	1482	3649	4449	4109	3723	3252	3013	2988	3227	3480	3252	3231	3540	3667	2845	2349	2000	1872	1230	59278N			
13	W	613	313	225	197	440	1506	3790	4679	3888	3736	3215	2882	3053	3427	3198	3263	3125	3276	3666	2926	2339	2120	1956	1350	59183N			
14	R	793	482	258	245	448	1457	3815	4739	3942	3908	3337	3143	3239	3436	3658	3419	3615	3443	3781	3091	2534	2345	2249	1700	63077N			
15	F	999	510	333	273	517	1482	3712	4605	4027	3803	3487	3222	3286	3590	3928	3733	3467	3494	3444	2046	2575	2231	2559	2162	63485N			
16	A	1334	891	557	480	511	970	2047	2163	2495	2531	2695	3002	3550	4032	4172	3890	3816	3513	3142	3033	2621	2584	2631	2201	58861N			
17	S	1557	1064	727	645	663	933	1634	1649	2098	2089	2476	2735	3066	3198	3656	3447	3304	3196	2959	2609	2449	2121	1854	1345	51474N			
18	M	819	458	294	267	523	1505	3769	4803	4037	3955	3505	3278	3072	3281	3738	3435	3493	3424	3684	2953	2198	1905	1772	1114	61282N			
19	T	659	337	201	214	479	1438	3863	4820	3762	4142	3638	3376	3225	3263	3713	3320	3426	3477	3695	3377	2371	2054	1945	1279	62074N			
20	W	733	339	219	210	399	1448	3764	4551	4109	3611	2715	3269	3067	3392	3543	3478	3441	3446	3733	3191	2398	1944	2300	1435	60735N			
21	R	933	470	303	222	414	1434	3785	4724	4122	3661	3385	3165	3056	3312	3546	3478	3557	3543	3732	3253	2557	2420	2154	1616	62842N			
22	F	1026	562	356	287	456	1447	3710	4634	3964	3750	3378	3231	3166	3412	3945	3771	3653	3695	3940	3506	2983	2837	2732	2022	66463N			
23	A	1443	922	614	442	499	930	1969	2080	2356	2539	2458	2798	3017	3368	3839	3752	3400	3457	3159	2826	2599	2600	2640	2142	55849N			
24	S	1646	1275	845	662	643	884	1691	1733	2153	2250	2591	3153	3325	3757	4000	3630	3205	2811	2781	2746	2168	2102	1888	1403	53342N			
25	M	867	499	327	261	571	1466	3695	4547	4158	2961	3374	3070	2861	3207	3601	3413	3392	3570	3740	2852	2161	1873	1841	1221	59528N			
26	T	717	404	256	242	478	1436	3856	4639	3925	3740	3252	3098	3024	3266	3589	3369	3389	3608	3880	3056	2407	2063	1930	1288	60912N			
27	W	742	421	277	228	448	1390	3744	4699	3986	3612	3341	3080	3092	3369	3721	3418	3342	3666	3650	3426	2871	2185	2299	2066	63073A			
28	R	1281	587	341	282	467	1448	3805	4573	4162	3562	3169	3356	3159	3248	3389	3313	3893	3636	3730	2863	2393	2359	2136	1647	62799A			
29	F	1023	611	416	310	525	1429	3774	4611	4079	3580	3407	3202	3261	3403	3751	3436	3559	3648	3657	3167	2767	2461	2407	2193	64677N			
30	A	1595	994	694	540	595	944	2090	2163	2465	2492	2719	2752	3226	3583	3843	3680	3494	3165	3008	2843	2588	2595	2591	2213	56872N			
WEEKDAY AVERAGE =		62183				SATURDAY AVERAGE =				57651				SUNDAY AVERAGE =				52414				NUMBER OF GOOD DAYS		30		TOTAL MONTHLY COUNT =		1807649	
MONTHLY AVERAGE =		60140																											

COMMENTS:

- "B"=====> BAD DAY
- "N"=====> NORMAL DAY
- "A"=====> ATYPICAL DAY
- "H"=====> ATYPICAL DAY (HOLIDAY)
- "S"=====> ATYPICAL DAY (SPECIAL EVENT)

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
APRIL 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: W LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

Table with columns: DY D, 1-24, DAILY. Rows represent hourly traffic counts for days 1 through 30, including weekly averages and monthly totals.

COMMENTS:
4/18: FEDERAL INCOME TAX DAY

"B"=====> BAD DAY
"N"=====> NORMAL DAY
"A"=====> ATYPICAL DAY
"H"=====> ATYPICAL DAY (HOLIDAY)
"S"=====> ATYPICAL DAY (SPECIAL EVENT)

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
MAY 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: E LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY	
1	S	1675	1170	775	676	686	970	1731	1650	2054	2260	2637	2978	3372	3665	3707	3748	3460	3122	2948	2534	2162	2091	1819	1384	53274N	
2	M	814	533	304	276	569	1557	3730	4788	3892	3565	3393	3041	3009	3286	3572	3317	3424	3678	3624	2677	2220	1894	1807	1238	60208N	
3	T	673	404	266	241	497	1444	3925	4782	3973	3782	3187	2826	2812	3094	3418	3292	2844	3389	3621	2959	2218	1918	1896	1236	58697N	
4	W	687	393	206	202	446	1479	3817	4704	3998	3772	3257	2914	2646	2771	2955	2851	3099	3271	3275	2947	2254	1923	1932	1300	57099N	
5	R	776	435	324	302	486	1436	3957	4801	4013	3797	3528	3233	3193	3317	3847	3504	3250	3711	3851	3176	2747	2446	2281	1725	64136N	
6	F	1228	651	447	375	501	1460	3904	4764	4233	3518	3365	3313	3250	3458	3840	3537	3255	3600	3802	3201	2892	2729	2656	2170	66149N	
7	A	1713	1130	713	540	588	952	2111	2222	2484	2623	2831	2955	3344	3602	4094	3493	3634	3523	3221	2684	2694	2645	2544	2303	58643N	
8	S	1686	1160	950	589	590	868	1596	1624	1987	2233	2703	3208	3576	3748	3856	3490	3038	2849	2934	2556	2251	2090	1756	1330	52668N	
9	M	763	431	300	304	486	1458	3647	4688	4127	3654	3196	2961	3048	3355	3549	3407	3378	3457	3675	2927	2146	1921	1754	1633	60265A	
10	T	832	364	241	243	477	1484	3858	4355	3812	3917	3464	3052	2943	3205	3462	3237	3324	3441	3579	2985	2245	2066	1916	1310	59812N	
11	W	682	325	215	206	427	1450	3818	4805	4051	3699	3475	3100	3079	3395	3653	3516	3400	3212	3701	3086	2252	2114	1963	1394	61018N	
12	R	899	477	288	267	492	1380	3931	4731	4029	3820	3482	3261	3109	3512	3794	3457	3524	3598	3944	3136	2629	2315	2460	1709	64244N	
13	F	1008	582	352	306	522	1468	3925	4695	4178	3810	3455	3456	3502	3775	3853	3797	3512	3627	3792	3308	2743	2539	2567	2745	67517N	
14	A	1678	1049	711	587	618	951	2138	2142	2564	2722	2824	2981	2943	3639	3708	3584	3349	3209	3056	3057	2613	2458	2600	2385	57566N	
15	S	1886	1368	895	749	759	948	1615	1716	2051	2450	2793	3159	3549	3736	4118	3778	3128	2887	2819	2491	2178	2064	1858	1354	54349N	
16	M	912	553	321	320	563	1474	3875	4652	4079	3642	3181	3028	3037	3300	3537	3359	3375	3482	3653	2742	2173	1918	1755	1168	60099N	
17	T	698	353	274	223	493	1500	3909	4752	3943	3742	3328	2792	2867	3067	3456	3130	3242	3252	3627	2896	2010	1965	1818	1130	58467N	
18	W	626	353	239	232	422	1426	3446	4407	3969	3838	3067	2785	3022	3283	3399	2998	3243	3339	3679	2862	2210	2171	2030	1307	58353N	
19	R	833	435	237	246	463	1384	3634	4391	3946	3739	3173	3007	2849	3197	3893	3323	3416	3715	3782	3417	2675	2245	2223	1619	61842N	
20	F	982	564	341	353	522	1421	3814	4627	4214	3781	3547	3348	3249	3512	3714	3596	3274	3561	3742	2986	2661	2708	2595	2138	65250N	
21	A	1645	1051	708	569	616	970	2173	2190	2395	2715	2663	2932	3046	3336	3778	3724	2950	3111	3116	2966	2727	2914	2957	2481	57733N	
22	S	1952	1230	868	700	722	888	1559	1636	1981	2330	2534	2966	3395	3761	3912	3592	3311	2858	2807	2417	2207	1991	1818	1378	52813N	
23	M	855	494	309	295	584	1442	3557	4803	3855	3662	3175	2788	2864	3171	3493	3050	3254	3323	3697	2773	2081	1911	1753	1200	58389N	
24	T	700	392	244	273	513	1488	3905	4603	4011	3688	3105	2871	2690	2978	3418	3002	3144	2419	3939	2622	2164	1976	1798	1064	57007A	
25	W	660	354	235	250	466	1348	3904	4692	3988	3570	3394	2926	2944	3214	3528	3292	3359	3366	3797	2912	2340	2138	2064	1395	60136N	
26	R	895	478	334	328	705	1709	4096	4654	3968	3671	3216	3139	3035	3337	3730	3242	3329	3733	3956	3064	2409	2380	2208	1618	63234A	
27	F																										57005B
28	A																										43889B
29	S																										44578B
30	M																										44374B
31	T	857	460	374	349	566	1630	3535	4583	3896	3688	3087	3003	2954	3122	3386	3075	3218	3360	3666	2825	2125	1894	1796	1110	58559A	

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WEEKDAY AVERAGE = 61414 SATURDAY AVERAGE = 57981 SUNDAY AVERAGE = 53276 NUMBER OF GOOD DAYS 27 TOTAL MONTHLY COUNT = 1607527

MONTHLY AVERAGE = 59761

COMMENTS:
5/8: MOTHER'S DAY; 5/30: MEMORIAL DAY

"B"=====> BAD DAY
"N"=====> NORMAL DAY
"A"=====> ATYPICAL DAY
"H"=====> ATYPICAL DAY (HOLIDAY)
"S"=====> ATYPICAL DAY (SPECIAL EVENT)

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
MAY 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: W LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1	S	2269	1595	1248	991	1030	1079	915	1402	1440	1825	2326	2838	3146	2746	2850	3342	3377	3674	3601	3432	3101	2062	1637	1818	53744N
2	M	1163	627	444	415	611	947	1615	3511	3744	3271	2992	3269	3260	3297	3833	4588	4564	4456	3602	2816	2097	1603	1457	1713	59895N
3	T	1030	573	336	320	393	734	1536	3546	3680	3221	3200	3248	3229	3235	3899	4731	3811	4373	3548	2536	2044	1654	1451	1737	58065N
4	W	1097	516	320	223	305	659	1524	3421	3640	3325	3103	3330	3667	3447	3515	3610	3874	3681	3196	2554	2028	1633	1455	1742	55865N
5	R	1126	584	457	358	417	656	1541	3507	3719	3329	3046	3182	3294	3269	3938	4704	4394	4619	3969	3159	2579	1937	1863	2110	61757N
6	F	1591	897	616	493	499	735	1467	3391	3625	3249	3107	3261	3421	3634	4170	4719	3318	4531	4024	3287	2623	2184	2099	2419	63360N
7	A	1985	1464	970	822	796	783	934	1582	1871	2271	2442	2620	2987	2937	3288	3913	3773	3681	3460	3098	3083	2611	2515	2701	56587N
8	S	2304	1638	1141	935	933	930	888	1308	1489	1786	2244	2768	2891	2840	2964	3428	3388	3598	4021	3583	3067	2127	1807	1808	53886N
9	M	1068	573	402	335	507	819	1561	3455	3561	3001	3238	3173	3117	3282	3978	4686	4577	4521	3673	3049	2151	1656	1471	1799	59653N
10	T	1166	547	359	310	368	649	1611	3459	3678	3171	3036	3111	3367	3320	3966	4683	4462	4503	3721	2998	2187	1801	1620	1794	59887N
11	W	1121	512	296	216	316	636	1546	3340	3789	3255	3110	3207	3272	3451	3959	4718	4101	4713	3696	2951	2256	1862	1662	1971	59956N
12	R	1204	602	405	404	460	672	1538	3454	3789	3233	3177	3333	3378	3406	4148	4819	4078	4709	3678	3202	2443	1971	1864	2111	62078N
13	F	1458	846	513	468	481	739	1450	3360	3624	3251	3225	3421	3588	3610	4168	4536	4462	3219	4061	3325	2694	2235	2042	2463	63239N
14	A	2044	1378	1005	859	884	822	920	1505	1868	2104	2411	2653	2766	2891	2964	3671	3516	3554	3372	3193	3094	2491	2217	2787	54969N
15	S	2515	1742	1182	1039	1129	1106	867	1370	1402	1714	2161	2569	2928	2855	3037	3447	3353	3699	3790	3705	3035	2008	1844	1804	54301N
16	M	1164	678	448	450	620	921	1607	3533	3682	3210	3049	3263	3193	3203	3845	4462	2897	3253	4018	2781	1982	1520	1381	1660	56820A
17	T	1109	547	361	322	459	754	1555	3418	3661	3223	3069	3164	3241	3224	3891	4789	4327	4431	3335	2651	2014	1538	1496	1669	58248N
18	W	1066	500	288	231	377	625	1489	3330	3797	3302	3090	3272	3458	3468	4192	4023	3799	4216	3217	2647	2029	1659	1596	1848	57519N
19	R	1153	607	402	380	435	678	1436	3378	3556	3128	3142	3375	3342	3254	4064	4750	4679	4433	3554	3025	2350	1976	1929	2117	61143N
20	F																									54862B
21	A	1963	1315	970	810	851	794	943	1485	1796	2091	2536	2806	2783	2792	3132	3553	3388	3936	3230	2758	2628	2204	2183	2781	53728N
22	S	2931	1857	1250	1084	1081	1126	890	1363	1389	1769	2159	2660	3038	2694	2934	3588	3413	3666	3717	3371	2803	2080	1662	1829	54354N
23	M	1122	689	446	392	607	889	1580	3526	3905	2778	2885	3263	3301	3209	3919	4379	3669	4362	3332	2620	2005	1560	1372	1655	57465N
24	T	1052	547	337	295	446	691	1566	3496	3750	3234	2829	3044	3198	3159	3621	4210	4209	4410	3446	2641	1832	1649	1530	1648	56840N
25	W	1057	526	326	245	353	682	1479	3397	3717	3258	2919	3191	3188	3266	4022	4687	4709	4370	3544	2860	2301	1846	1828	1939	59710N
26	R	1223	644	462	447	484	761	1506	3414	3687	3256	3082	3107	3344	3328	3852	4674	4182	4580	3676	3094	2577	2483	2245	2421	62529A
27	F	1737	950	629	520	572	738	1490	3284	3393	3194	3149	3304	3548	3566	4419	4586	4338	3737	3077	2725	2156	1917	1778	2092	60899A
28	A	1872	1595	1116	902	875	938	949	1471	1624	1980	2246	2580	2723	2588	2589	2765	2483	2612	2708	2631	2569	2130	1974	2235	48155N
29	S	2279	1751	1392	1149	1274	1299	972	1311	1279	1502	1792	2111	2422	2298	2578	2936	2785	3072	3182	3133	2794	2369	2037	2140	49857A
30	M	1807	1365	1016	863	964	1072	1020	1629	1587	1677	2079	2513	2564	2638	2876	3240	3212	3194	2970	2822	2154	1660	1537	1765	48224H
31	T	1208	733	557	468	595	909	1549	3455	3738	3237	3054	3242	3283	3346	3843	4710	4681	4402	3306	2664	1995	1571	1483	1641	59670A

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WEEKDAY AVERAGE = 59518 SATURDAY AVERAGE = 53360 SUNDAY AVERAGE = 53228 NUMBER OF GOOD DAYS 30 TOTAL MONTHLY COUNT = 1722403

MONTHLY AVERAGE = 57740

COMMENTS:
5/8: MOTHER'S DAY; 5/30: MEMORIAL DAY

"B"=====> BAD DAY
"N"=====> NORMAL DAY
"A"=====> ATYPICAL DAY
"H"=====> ATYPICAL DAY (HOLIDAY)
"S"=====> ATYPICAL DAY (SPECIAL EVENT)

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
JUNE 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: E LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1	W	622	336	237	211	459	1346	3766	4564	4018	3628	3366	3027	2936	3277	3535	3124	3192	3328	3704	3062	2367	2157	1859	1327	59448N
2	R	819	431	297	278	462	1395	3877	4710	4028	3693	3339	3143	3040	3272	3610	3323	3054	3115	3882	3146	2403	2176	1996	1388	60877N
3	F	971	520	357	329	444	1425	3855	4377	4158	3751	3465	3191	3343	3533	3630	3417	1933	2399	2944	3059	2580	2312	2482	2053	60528S
4	A	1574	958	650	508	546	897	2083	2210	2445	2609	2606	2792	3010	3400	3666	3506	3405	3168	3231	2759	2404	2480	2584	2046	55537N
5	S	1613	1085	786	625	589	849	1598	1565	1870	2092	2264	2572	2808	2924	3387	3151	2994	2595	2683	2230	1913	1838	1708	1222	46961N
6	M	706	517	314	319	487	1373	3725	4591	4052	3506	3155	2868	2894	3162	3292	2940	2892	3224	3899	2593	1977	1867	1685	1079	57117N
7	T	731	400	236	221	435	1353	3684	4738	4053	3611	3197	2867	3058	3142	3351	2900	2949	3228	3240	2861	1981	1958	1747	1110	57051N
8	W	659	372	235	240	416	1343	3773	4779	3961	3498	3073	2837	2836	2900	3293	2700	2803	3151	3488	2968	2156	1915	1637	1156	56189N
9	R	796	449	284	224	453	1369	3660	4617	4088	3542	3073	3062	2929	3169	3483	3081	3036	3310	3562	2972	2167	2179	2100	1272	58877N
10	F	935	483	345	312	424	1342	3673	4457	4064	3769	3555	3228	3161	3171	3130	2956	3254	3501	3527	3169	2424	2350	2270	1651	61151N
11	A	1367	947	598	512	464	904	2054	2167	2466	2536	2592	2777	2876	3101	3383	2827	2804	2788	2805	2491	2277	2211	2294	1978	51219N
12	S	1414	1073	745	614	642	825	1553	1531	1798	2194	2538	2661	2876	3006	3261	2885	2624	2699	2600	2286	1906	1816	1591	1108	46246N
13	M	780	417	311	287	475	1344	3578	4303	3901	3431	3004	2796	2797	2861	3346	3021	2993	3135	3428	2561	1931	1782	1751	1049	55282N
14	T	713	409	232	218	431	1421	3682	4273	4109	3594	3208	2725	2896	2976	3290	2918	3004	3249	3498	2730	2169	1761	1796	1120	56422N
15	W	689	364	236	226	416	1289	3624	3844	3961	3836	3233	2887	2835	2932	3450	2958	3058	3338	3734	2820	2237	2062	2044	1384	57457N
16	R	778	457	273	255	404	1387	3608	4549	4020	3772	3246	2945	2817	2935	3145	3151	3170	3308	3619	3059	2507	2105	2124	1453	59087N
17	F	1093	612	337	331	472	1405	3629	4427	4107	3786	3342	3202	3104	3233	3489	3427	3062	3464	3350	3012	2389	2270	2410	1821	61774N
18	A	1425	939	631	496	524	878	2008	2055	2390	2460	2557	2685	2754	2999	3488	3256	3101	2602	2443	2474	2138	2133	2228	1859	50523N
19	S	1573	1001	641	577	563	841	1425	1461	1745	1935	2140	2455	2690	2918	3307	3008	2724	2539	2441	2180	2013	1754	1576	1474	44981N
20	M	805	479	272	264	446	1317	3448	4292	3880	3512	2990	2865	2723	2923	3243	2982	3039	3378	3514	2796	2237	2037	1956	1213	56611N
21	T	695	401	238	207	488	1374	3668	4342	3882	3560	3098	2493	2439	2499	3128	2886	2914	3256	3399	2678	2074	1678	1668	1190	54255N
22	W	655	372	249	208	422	1314	3582	4399	4125	3603	3180	2887	2887	3078	3128	3274	3058	3393	3667	3093	2320	1939	1911	1309	58053N
23	R	885	468	285	246	419	1348	3583	4352	4103	3750	3310	2821	3058	3183	3425	3260	3153	3329	3785	3033	2345	2044	2258	1576	60019N
24	F	986	613	344	318	488	1300	3541	4402	4244	3756	3365	3068	3114	3510	3204	2722	3300	3782	3455	3107	2588	2429	2589	1934	62159N
25	A	1492	910	647	520	542	843	1976	2069	2345	2462	2650	2856	2933	3262	3813	3442	3321	2975	2953	2783	2544	2454	2510	2038	54340N
26	S	1503	1089	713	634	638	824	1545	1489	1898	1989	2460	2862	3050	3223	3509	3346	3088	2880	2724	2265	1843	1615	1648	1457	48292N
27	M	925	538	295	260	485	1366	3576	4286	4020	3723	3001	2868	2404	2743	3172	2783	2865	3306	3568	1997	2048	1828	1711	1220	54988A
28	T	733	385	263	219	423	1356	3618	4337	4096	3589	3189	2661	2618	2831	3206	3062	3090	3176	3623	2898	2221	1853	1936	1166	56549N
29	W																									51680B
30	R	858	444	308	244	475	1323	3556	4244	4124	3751	3305	2927	2954	3193	3546	3183	3147	3410	3798	3277	2489	2285	2060	1568	60469N

WEEKDAY AVERAGE = 58225 SATURDAY AVERAGE = 52905 SUNDAY AVERAGE = 46620 NUMBER OF GOOD DAYS 29 TOTAL MONTHLY COUNT = 1622462
MONTHLY AVERAGE = 55807

COMMENTS:

"B"=====> BAD DAY
"N"=====> NORMAL DAY
"A"=====> ATYPICAL DAY
"H"=====> ATYPICAL DAY (HOLIDAY)
"S"=====> ATYPICAL DAY (SPECIAL EVENT)
6/9: END OF 2015-16 PUBLIC SCHOOL YEAR
6/19: FATHER'S DAY

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
JUNE 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: W LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1	W	1033	484	285	276	371	611	1484	3346	3915	3143	2993	3113	3114	3091	4037	4568	4490	4380	3519	2794	2064	1754	1685	1842	58392N
2	R	1141	642	441	446	436	625	1475	3358	3506	3105	3137	3308	3261	3319	3936	4796	4495	3863	3683	2747	2123	1807	1702	1857	59209N
3	F	1356	720	475	368	465	699	1498	3332	3556	3117	3096	3417	3498	3495	4291	4756	2703	2733	3070	2696	2332	1870	1907	2282	57732S
4	A	1759	1287	963	791	746	763	901	1558	1702	2144	2416	2592	2701	2719	2919	3481	3287	3271	3293	3029	2649	2342	2644	2622	52579N
5	S	2132	1547	995	863	904	933	853	1367	1377	1624	2171	2570	2817	2618	2741	3238	3288	3435	3148	2771	2179	1552	1477	1659	48259N
6	M	1057	628	373	363	505	784	1566	3310	3697	3252	3048	3145	3091	3137	3496	4324	2945	4751	3275	2599	1998	1503	1418	1489	55754A
7	T	1077	524	329	299	365	587	1502	3190	3546	3331	2841	3129	3200	2990	3453	4203	4318	4340	3488	2647	1854	1659	1506	1607	55985N
8	W	1062	520	289	209	308	623	1410	3142	3523	3011	3109	2893	3021	3015	3616	3893	4259	3976	3486	2607	1988	1525	1440	1794	54719N
9	R	1135	552	383	333	439	664	1401	2998	3684	3205	2915	3216	3349	3198	3755	4725	4479	4385	3331	2574	2009	1714	1700	1763	57907N
10	F	1204	696	425	415	480	664	1336	2856	3393	3183	3104	3341	3442	3515	4184	3768	4034	4393	3141	2654	2218	1852	2013	2142	58453N
11	A	1792	1126	836	733	705	684	835	1533	1638	1971	2164	2391	2517	2689	2966	3531	3441	3608	2912	2522	2074	1965	2051	2342	49026N
12	S	1822	1308	1023	826	911	933	810	1234	1249	1521	1968	2223	2632	2690	2651	3181	3127	3281	3059	2874	2654	2069	1617	1765	47428N
13	M	1093	599	422	359	543	776	1385	2781	3479	2931	2806	2954	2982	2976	3475	4408	4557	4224	3379	2503	1947	1574	1499	1507	55159N
14	T	1048	493	340	306	444	676	1352	2832	3545	3045	2914	3102	2965	3029	3534	4492	4638	4332	3364	2706	2105	1759	1464	1721	56206N
15	W	1042	527	278	246	325	562	1366	2844	3406	3195	3029	3075	3105	3131	3693	4523	4439	3830	3447	2710	2151	1742	1575	2019	56260N
16	R	1180	622	396	340	405	675	1322	2830	3615	3199	2991	3008	3288	3232	3613	4690	4573	4404	3342	2673	2280	1781	1545	1860	57864N
17	F	1357	782	503	424	492	689	1322	2727	3363	3179	3009	3300	3262	3405	3912	4557	3893	3803	3436	2618	2048	1799	1840	2175	57895N
18	A	1736	1261	883	800	779	680	858	1464	1652	1983	2288	2340	2665	2667	2904	3441	3158	3073	3179	2692	2078	1853	1729	2331	48494N
19	S	1910	1312	923	810	886	903	788	1256	1283	1553	2000	2446	2714	2626	2588	3090	2939	3463	3068	3309	2241	1623	1334	1900	46965A
20	M	1038	538	344	369	500	785	1439	2876	3615	3112	2782	3000	3071	2935	3571	4458	4581	4293	3328	2587	2008	1611	1489	1735	56065N
21	T	1301	573	357	315	412	649	1369	2797	3508	3110	2959	3085	2956	2995	3429	4296	4285	3959	3032	2280	1829	1477	1376	1637	53986N
22	W	938	457	283	241	312	577	1332	2754	3524	3253	2931	2937	3045	2949	3606	4565	4520	4441	3510	2825	2207	1725	1580	2183	56695N
23	R	1184	633	412	383	400	629	1359	2683	3626	3239	3036	3093	3185	3135	3630	4632	4091	4468	3527	2822	2258	1914	1726	1882	57947N
24	F	1354	841	508	426	472	690	1350	2700	3377	3242	3054	3184	3386	3273	3828	4538	4526	4442	3413	3033	2370	2097	1988	2229	60321N
25	A	1804	1284	921	790	724	782	852	1474	1654	2107	2145	2470	2676	2747	2807	3317	3192	3148	3014	3107	2842	2465	2278	2538	51138N
26	S	2181	1419	1057	937	888	903	814	1235	1335	1620	2052	2461	2788	2638	2711	3425	3069	3512	3636	3462	2398	1752	1726	2637	50656A
27	M	1598	700	405	356	508	814	1405	2832	3553	3186	2973	3188	3069	3029	3420	4082	4210	4254	3253	2591	1943	1551	1468	1631	56019A
28	T	997	485	342	297	419	638	1350	2840	3602	3031	2907	2953	3058	3091	3435	4603	4529	4269	3307	2620	2127	1824	1488	1713	55925N
29	W	987	515	285	230	344	629	1280	2703	3641	3285	2944	2990	3088	3119	3647	4597	4521	4304	3524	2738	2139	1713	1618	1865	56706N
30	R	1088	613	437	409	456	642	1330	2702	3579	3165	2890	3190	3224	3300	3561	4665	4085	4532	3606	2755	2253	1862	1778	1979	58101N

WEEKDAY AVERAGE = 56927 SATURDAY AVERAGE = 50309 SUNDAY AVERAGE = 48327 NUMBER OF GOOD DAYS 30 TOTAL MONTHLY COUNT = 1647845
MONTHLY AVERAGE = 54753

COMMENTS:

"B"=====> BAD DAY
"N"=====> NORMAL DAY
"A"=====> ATYPICAL DAY
"H"=====> ATYPICAL DAY (HOLIDAY)
"S"=====> ATYPICAL DAY (SPECIAL EVENT)
6/9: END OF 2015-16 PUBLIC SCHOOL YEAR
6/19: FATHER'S DAY

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
JULY 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: E LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1	F	1069	559	369	284	470	1333	3514	4513	4203	3571	3426	3175	3221	3320	3575	3542	3522	3523	3377	3003	2666	2545	2432	2001	63213N
2	A	1596	950	659	546	576	851	1841	2121	2689	3049	3505	3474	3581	3779	4083	3725	3059	3007	3104	2886	2509	2545	2721	2642	59498A
3	S	1870	1199	738	606	615	792	1526	1566	2208	2670	3092	3310	3902	3787	4204	3797	3255	2929	2883	2697	2406	2235	2309	2099	56695A
4	M	1455	971	664	462	509	892	1848	1817	2034	2445	2693	2952	3136	3442	3663	3269	3215	2717	2675	2359	1927	2307	2049	1469	50970H
5	T	873	484	336	307	542	1375	3516	4304	3554	3331	3199	2730	2861	2500	2546	2591	2379	2812	3119	2655	2045	2002	1853	1163	53077A
6	W	654	357	250	185	388	1291	3578	4230	4212	3540	3226	2833	2899	3067	3348	3019	3066	3299	3766	2866	2331	1989	1969	1334	57697N
7	R	751	442	288	258	435	1284	3589	4310	4161	3670	3310	2993	3014	2977	3478	3182	3087	3449	3778	3084	2393	2259	2085	1520	59797N
8	F	948	516	338	266	475	1342	3623	4337	4214	3672	3217	3084	3044	3239	3707	3370	3465	3474	3554	2960	2430	2430	2543	1836	62084N
9	A	1271	927	624	508	501	907	2004	2066	2388	2342	2451	2746	2853	3295	3716	3507	3291	2997	2915	2699	2592	2507	2455	2000	53562N
10	S	1637	1115	757	605	605	832	1507	1567	1888	2038	2284	2600	2839	3015	3579	3289	2835	2620	2790	2293	2080	1911	1798	1311	47795N
11	M	797	420	309	315	474	1493	3529	4247	4116	3650	3001	2858	2956	2970	3249	3089	3049	3337	3526	2695	2246	1833	1930	1146	57235N
12	T	749	378	236	188	448	1363	3673	4325	4131	3737	3111	2750	2757	3095	3425	2985	2874	3123	3906	3178	2250	1975	1821	1211	57689N
13	W	646	442	274	200	419	1321	3582	4368	4110	3584	3073	2833	3033	2951	3423	3165	3092	3484	4012	3236	2331	2138	1963	1397	59077N
14	R	837	531	337	265	438	1335	3640	4456	4133	3816	3233	3065	3200	3302	3672	3383	3286	3605	4094	3337	2767	2581	2367	1757	63437N
15	F	1169	651	402	327	511	1335	3597	4358	4096	3903	3111	2926	3250	3545	3759	3760	3672	4090	4255	3541	2976	2673	2522	1820	66249N
16	A	1351	871	573	471	517	898	2003	2093	2677	2555	2635	2763	2968	3378	3846	3609	3605	3289	3225	2806	2569	2689	2589	2196	56176N
17	S	1640	1174	731	633	608	839	1520	1547	2025	2140	2397	2632	2993	3126	3556	3531	3291	3150	2998	2653	2225	2124	1957	1374	50864N
18	M	872	517	315	277	513	1429	3409	4017	4208	3772	3304	3071	3009	3116	3348	3058	3185	3376	3864	2997	2216	1975	1922	1301	59071N
19	T	776	456	272	251	469	1405	3603	4332	4251	3808	3270	2891	2921	2997	3432	3196	3174	3490	3821	3022	2488	2275	2136	1338	60074N
20	W	766	403	271	234	426	1366	3504	4405	4151	3683	3266	2939	2929	3045	3472	3166	3198	3388	3814	3035	2476	2195	2045	1482	59659N
21	R	967	549	323	270	409	1191	3357	4265	4141	3535	3159	2698	2055	2945	3327	3352	3331	3580	3956	3096	2501	2317	2192	1597	59113N
22	F	994	513	370	291	437	1359	3550	4172	4036	3868	3315	3158	3227	3271	3681	3384	3091	3429	3620	3106	2626	2633	2567	2058	62756N
23	A	1468	990	613	516	560	853	1939	2022	2423	2444	2506	2869	2986	3268	3559	3443	3199	3021	3085	2800	2548	2434	2599	1997	54142N
24	S	1647	1101	733	608	596	753	1464	1476	1775	1812	2160	2367	2463	2751	3383	3140	3009	2920	2771	2448	2171	1973	1900	1274	46695N
25	M	829	559	332	310	536	1420	3498	4151	3897	3672	3041	2786	2912	3039	3317	3183	3128	3429	3497	2899	2124	1848	1958	1234	57599N
26	T	732	444	266	231	434	1348	3536	4270	3964	3813	3126	2910	2751	2959	3557	3134	2897	3479	3940	2911	2377	2131	2103	1269	58582N
27	W	749	389	307	219	432	1354	3405	4273	4173	3626	3191	2780	2957	3073	3448	3162	3129	3594	4157	3203	2457	2226	1991	1480	59775N
28	R	961	541	366	296	450	1312	3555	4348	4254	3788	3295	3017	2859	3206	3342	3276	3178	3424	3803	3067	2525	2282	2375	1669	61189N
29	F	1132	644	425	335	490	1439	3488	4288	4305	3736	3454	3152	3105	3473	3811	3514	3558	3483	3630	3060	2820	2504	2610	2160	64616N
30	A	1535	953	672	528	567	920	1972	2127	2329	2387	2547	2761	3056	3316	3744	3696	3343	3084	2905	2672	2612	2497	2725	2261	55209N
31	S	1859	1189	793	599	636	808	1498	1594	1871	2080	2351	2654	2912	3054	3512	3158	3156	2921	2815	2279	2252	2002	2033	1425	49451N

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WEEKDAY AVERAGE = 59459 SATURDAY AVERAGE = 55717 SUNDAY AVERAGE = 50300 NUMBER OF GOOD DAYS 31 TOTAL MONTHLY COUNT = 1783046
MONTHLY AVERAGE = 57616

COMMENTS:

"B"=====> BAD DAY
"N"=====> NORMAL DAY
"A"=====> ATYPICAL DAY
"H"=====> ATYPICAL DAY (HOLIDAY)
"S"=====> ATYPICAL DAY (SPECIAL EVENT)
7/4: INDEPENDENCE DAY

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
JULY 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: W LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1	F	1707	784	474	499	504	671	1258	2627	3333	3213	3050	3155	3530	3440	3972	4229	3668	4291	3390	2813	2686	2101	1944	2286	59625A
2	A	2116	1402	922	904	750	733	829	1339	1542	2020	2327	2654	2734	2767	2914	3439	3355	3400	3548	3536	3271	2605	2543	2655	54305N
3	S	2320	1680	1344	907	952	951	757	1223	1202	1552	2035	2378	2478	2466	2770	3260	3501	3691	3668	3463	3493	2834	2305	2490	53720A
4	M	1973	1379	885	731	743	878	786	1268	1333	1569	1794	2369	2404	2588	2570	2852	3062	3263	3187	2971	2423	2863	3968	3074	50933H
5	T																									48798B
6	W	1076	516	323	250	341	628	1340	2723	3569	3210	3001	3038	3187	3010	3445	4570	4492	4235	3491	2757	2275	1865	1564	1745	56651N
7	R	1127	561	366	349	415	614	1285	2653	3617	3277	3020	3135	3159	3130	3586	4550	4361	4017	3483	2964	2335	1905	1749	1867	57525N
8	F	1342	762	468	355	480	628	1237	2636	3424	3229	3100	3146	3299	3257	3679	4566	4671	4269	3285	2773	2483	2095	1878	2217	59279N
9	A	1754	1168	843	727	719	698	875	1453	1661	2045	2185	2524	2680	2633	2927	3712	3242	3225	3119	3110	3037	2717	2246	2404	51704N
10	S	2004	1532	1110	939	973	898	781	1264	1307	1476	2050	2383	2647	2596	2780	2990	2797	3027	3287	3122	2854	2252	1752	1845	48666N
11	M	1152	660	389	414	481	767	1358	2915	3563	3169	2886	2939	2910	2918	3447	4575	4522	4169	3408	2640	2143	1671	1415	1577	56088N
12	T	1078	524	351	300	389	617	1305	2801	3675	3255	2822	3039	2928	3006	3515	4580	4033	4168	3997	2734	2164	1880	1585	1733	56479N
13	W	1121	605	350	242	327	604	1314	2815	3702	3201	2909	3038	3136	2988	3585	4584	4705	4287	3404	2780	2340	1815	1787	2005	57644N
14	R	1228	693	494	371	466	650	1369	2772	3621	3262	2986	3039	3279	3322	3751	4522	4551	4465	3690	3036	2741	2229	2073	2181	60791N
15	F	1564	919	589	469	563	697	1261	2769	3424	3377	3220	3247	3701	3373	3785	4631	4697	4458	3518	3070	2725	2246	2304	2437	63044N
16	A	1983	1285	954	796	661	764	956	1521	1718	2122	2299	2519	2834	2601	2895	3379	3257	3310	3578	3647	2944	2572	2336	2668	53599N
17	S	2253	1627	1175	973	1008	1008	874	1324	1395	1656	2053	2497	2599	2559	2709	3142	3025	3306	3534	3176	3173	2314	1946	1961	51287N
18	M	1368	757	468	424	566	871	1387	2812	3739	3283	2984	3255	3221	3251	3759	4615	4495	4512	3498	2742	2300	1778	1554	1718	59357N
19	T	1122	594	379	330	456	677	1297	2761	3687	3231	3122	3235	3188	3205	3622	4471	3639	4841	3552	2887	2255	1922	1728	1984	58185N
20	W	1376	611	371	274	350	638	1341	2797	3765	3380	3226	3136	3237	3256	3518	4675	4374	4250	3504	2782	2293	1852	1815	1903	58724N
21	R	1204	641	444	444	482	688	1300	2652	3605	3324	3198	3004	2987	3247	3481	4767	4477	4233	3442	2871	2349	1914	1762	1957	58473N
22	F	1353	782	491	411	464	679	1340	2661	3467	3256	3097	3295	3312	3292	3794	4285	3612	4609	3599	2649	2274	2092	1934	2267	59015N
23	A	1832	1274	1000	774	689	785	907	1410	1674	2016	2331	2538	2659	2573	2776	3443	3271	3368	3182	3080	2840	2464	2116	2460	51462N
24	S	1987	1529	1110	975	938	891	788	1262	1297	1603	2084	2830	3146	2620	2572	3067	2830	2963	2994	2595	2634	2092	1787	1813	48407N
25	M	1110	650	444	421	575	814	1355	2782	3687	3098	2973	3205	3169	3009	3383	4569	4583	4253	3362	2671	2268	1694	1511	1684	57270N
26	T	1075	535	382	311	382	619	1307	2729	3591	3288	2975	3086	3217	3246	3454	4672	3806	4413	2900	2777	2276	2016	1804	1782	56643N
27	W	1012	561	330	274	332	617	1247	2697	3517	3371	3160	3185	3198	3152	3654	4619	4488	4304	3497	2830	2352	1946	1690	1806	57839N
28	R	1206	671	440	375	485	617	1309	2739	3625	3282	3026	3216	3316	3314	3618	4730	4681	4655	3468	2869	2467	1929	1782	1997	59817N
29	F	1454	866	506	393	523	734	1210	2709	3502	3252	3062	3354	3503	3398	3839	4567	4541	4370	3509	2868	1947	2328	2001	2281	60717N
30	A	1904	1358	999	971	829	751	871	1464	1727	1941	2256	2451	2747	2576	2947	3268	3317	3443	3554	3170	3135	2745	2246	2460	53130N
31	S	2087	1661	1176	927	1023	907	817	1291	1302	1645	2156	2534	2670	2737	2796	3175	3365	3425	3292	3297	2947	2383	1792	1877	51282N

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WEEKDAY AVERAGE = 58043 SATURDAY AVERAGE = 52840 SUNDAY AVERAGE = 50672 NUMBER OF GOOD DAYS 30 TOTAL MONTHLY COUNT = 1681661
MONTHLY AVERAGE = 56247

COMMENTS:

- "B"=====> BAD DAY
- "N"=====> NORMAL DAY
- "A"=====> ATYPICAL DAY
- "H"=====> ATYPICAL DAY (HOLIDAY)
- "S"=====> ATYPICAL DAY (SPECIAL EVENT)

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
AUGUST 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: E LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

Table with columns: DY D, 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, DAILY. Rows represent hourly traffic counts for days 1 through 31.

WEEKDAY AVERAGE = 58722 SATURDAY AVERAGE = 52291 SUNDAY AVERAGE = 45374 NUMBER OF GOOD DAYS 30 TOTAL MONTHLY COUNT = 1677001
MONTHLY AVERAGE = 55897

COMMENTS:

- "B"=====> BAD DAY
"N"=====> NORMAL DAY
"A"=====> ATYPICAL DAY
"H"=====> ATYPICAL DAY (HOLIDAY)
"S"=====> ATYPICAL DAY (SPECIAL EVENT)

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
AUGUST 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: W LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY											
1	M	1177	730	478	426	577	822	1396	2718	3545	3290	3021	3223	3400	3230	3630	4653	4766	4699	3689	2831	2221	1650	1519	1690	59381N											
2	T	1072	499	313	304	384	587	1291	2629	3433	3191	3026	3217	3230	3124	3675	4757	4767	4573	3650	2421	1782	1418	1374	1774	56491N											
3	W	1017	528	305	249	310	553	1257	2564	3555	3219	3150	3210	3286	3237	3706	4498	4553	4508	3670	2905	2190	1870	1656	1842	57838N											
4	R	1248	662	406	398	497	602	1260	2638	3501	3226	3091	3181	3264	3318	3705	4697	4504	4518	3624	3016	2542	1993	1803	2048	59742N											
5	F	1395	827	524	457	522	681	1295	2589	3477	3265	3133	3295	3491	3237	3797	4659	4067	3904	3256	2893	2658	2133	2136	2207	59898N											
6	A	1859	1363	934	821	790	711	864	1441	1597	2055	2336	2699	2736	2542	2758	3261	3000	3257	3441	3282	2369	1914	1894	2266	50190N											
7	S	1845	1426	1119	868	909	878	805	1218	1240	1608	2044	2399	2707	2641	2663	3300	3518	4098	3309	2513	2065	1655	1520	1721	48069N											
8	M	1088	619	377	335	481	767	1273	2677	3436	3141	3053	3175	3263	3040	3536	4129	3596	3694	3101	2416	1730	1413	1240	1474	53054N											
9	T	926	550	300	336	380	600	1210	2709	3461	3110	2949	2895	3090	3145	3619	4071	4379	4080	3256	2556	1856	1608	1504	1730	54320N											
10	W	1043	525	312	252	296	562	1280	2596	3433	3169	3011	2983	3109	3081	3458	4337	4513	4345	3408	2680	2288	1770	1630	1891	55972N											
11	R	1232	638	451	399	455	599	1225	2590	3503	3255	3125	3177	3309	3107	3696	4751	4577	4382	3438	2807	2291	1936	1721	2064	58728N											
12	F	1331	724	516	401	531	673	1241	2568	3187	3148	3236	3228	3190	3364	3702	4490	4506	3890	3566	2834	2347	2015	1855	2265	58808N											
13	A	1873	1302	979	758	668	720	834	1496	1640	1975	2364	2517	2658	2602	2818	3293	3106	3563	3400	3449	3112	2520	2093	2537	52277N											
14	S	1961	1449	1051	811	828	823	707	1191	1186	1513	2013	2366	2839	2678	2511	2934	2927	3074	3078	2852	2704	2240	1661	1732	47129N											
15	M	1162	694	455	395	524	787	1357	2793	3527	3177	3134	3102	3182	3112	3368	4563	4436	4133	3524	2525	2163	1623	1359	1709	56804N											
16	T	1073	603	373	289	385	665	1200	2351	2785	2592	2592	3261	3142	3186	3631	4400	4439	4364	3402	2723	2138	1737	1569	1772	54672N											
17	W	1096	572	296	243	285	605	1338	2687	3509	3076	2855	3172	3098	3010	3334	4580	4505	4293	3509	2664	2197	1703	1575	1880	56082N											
18	R	1219	655	489	419	435	653	1343	2743	3513	3226	2936	3105	3268	3043	3547	4756	4392	4521	3647	2864	2311	1958	1810	2072	58925N											
19	F	1626	840	506	410	491	709	1340	2702	3306	3235	3103	3294	3384	3378	3693	4726	4587	4229	3358	2773	2469	2059	1875	2224	60317N											
20	A	1845	1311	910	761	771	717	860	1407	1767	2049	2244	2527	2782	2730	2745	3392	3034	3029	3124	3010	3004	2271	2212	2610	51112N											
21	S	2035	1561	1133	920	896	928	753	1290	1248	1635	1988	2461	2584	2600	2733	3172	2985	3214	3164	3139	2571	1882	1732	1940	48564N											
22	M	1072	617	356	323	541	861	1691	3228	3709	3209	3029	3140	3164	3163	3630	4495	4415	4219	3351	2714	2124	1633	1397	1625	57706N											
23	T	931	464	284	259	361	624	1597	3353	3808	3153	2927	3057	3149	3165	3683	4627	4514	4598	3500	2682	2118	1642	1519	1684	57699N											
24	W	971	501	260	211	308	616	1565	3296	3766	3214	3110	3162	3135	3167	3800	4759	4468	4391	3378	2600	2107	1654	1511	1784	57734N											
25	R	991	564	349	365	378	626	1555	3432	3725	3197	3036	2971	3132	3204	3678	4678	4726	4521	3491	2633	2135	1866	1593	1892	58738N											
26	F	1219	698	439	385	466	664	1456	3218	3625	3081	2962	3224	3430	3348	3877	4583	4616	4009	3426	2990	2351	1985	1834	2078	59964N											
27	A	1641	1173	826	698	711	697	818	1423	1644	1960	2262	2503	2710	2517	2551	3021	2882	3017	2692	2522	2341	2039	2034	2393	47075N											
28	S	1886	1353	1077	875	887	772	739	1176	1235	1460	1917	2394	2563	2532	2488	2957	2733	2902	2734	2590	2168	1588	1449	1580	44055N											
29	M	1010	496	379	316	472	802	1597	3205	3512	3048	2915	3137	3153	3064	3531	4668	4524	4117	3197	2533	1829	1428	1286	1505	55724N											
30	T	895	480	319	281	423	634	1444	3059	3527	3145	2830	3005	3136	2974	3507	3866	4101	4238	2977	2427	1820	1397	1292	1854	53631N											
31	W	988	454	237	207	363	624	1567	3280	3838	2998	2920	2990	3164	3110	3852	4650	4494	4382	3282	2650	2029	1560	1481	1618	56738N											
WEEKDAY AVERAGE =		57510				SATURDAY AVERAGE =				50164				SUNDAY AVERAGE =				46954				NUMBER OF GOOD DAYS				31				TOTAL MONTHLY COUNT =				1707437			
MONTHLY AVERAGE =		54953																																			

COMMENTS:

"B"=====> BAD DAY
8/22-29: UNIVERSITIES AND COLLEGES BEGIN 2016 FALL SEMESTER
"N"=====> NORMAL DAY
8/22: PUBLIC SCHOOLS OPEN FOR 2016-17 SCHOOL YEAR
"A"=====> ATYPICAL DAY
"H"=====> ATYPICAL DAY (HOLIDAY)
"S"=====> ATYPICAL DAY (SPECIAL EVENT)

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
SEPTEMBER 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: E LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1	R																									51698B
2	F																									53576B
3	A																									37324B
4	S																									36125B
5	M																									36552B
6	T	573	324	202	190	484	1485	3329	3819	3720	3722	3119	2774	2734	2940	3464	3119	3085	3174	3699	3039	2007	1761	1625	888	55276N
7	W																									53316B
8	R																									53225B
9	F	832	399	294	263	454	1373	3650	4553	3851	3842	2920	2772	2795	3125	3550	3238	3365	3595	3613	3124	2760	2527	2385	1871	61151N
10	A																									45823B
11	S	1713	1046	669	540	580	773	1507	1396	1586	1538	1868	1991	2614	3050	3432	3013	2670	2483	2509	2217	1938	1731	1513	1046	43423N
12	M																									54130B
13	T																									55126B
14	W	540	294	211	215	417	1415	3701	4442	4175	3671	3143	2721	2784	3098	3421	3112	3322	3653	3878	2932	2402	2004	1794	1164	58509N
15	R	727	400	280	223	471	1468	3746	4377	4004	3826	3272	2819	2786	3075	3474	3240	3336	3624	3750	3302	2519	2184	1988	1384	60275N
16	F	811	415	262	257	481	1436	3766	4380	4062	3755	3296	2970	3156	2565	2538	3323	3137	3727	3230	3301	3086	2701	2508	1973	61136N
17	A																									53145B
18	S																									41012B
19	M																									52967B
20	T	561	287	214	181	497	1493	3726	4731	4161	3648	3170	2864	2867	3026	3112	3007	3192	3379	3691	3045	2059	1902	1692	1034	57539N
21	W	582	283	207	189	397	1472	3665	4407	4205	3731	2794	2793	2862	3076	3232	3143	3063	3567	3775	3179	2411	1852	1816	1184	57885N
22	R	630	324	244	192	467	1393	3610	4485	3654	3273	3294	2822	2732	2946	3493	3122	3240	3723	3639	3013	2281	2061	2016	1327	57981N
23	F																									60140B
24	A																									47908B
25	S	1458	947	677	527	534	811	1242	1157	1552	1501	1617	1883	2547	2899	3120	2766	2833	2810	2759	2333	2025	1749	1617	1094	42458N
26	M	680	375	279	259	518	1446	3673	4472	3796	3741	3101	2807	2726	2939	3275	3088	3067	3466	3623	2759	1932	1554	1525	1149	56250N
27	T	553	323	175	206	456	1536	3660	4416	3882	3695	3053	3008	2749	2799	3211	2851	2949	3206	3638	2812	2111	1757	1661	1055	55762N
28	W																									51857B
29	R																									56218B
30	F	817	464	285	259	467	1438	3707	4571	3919	3798	3356	2969	2681	3141	3387	3357	3524	2578	3110	3264	2484	2185	2226	1589	59576A

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WEEKDAY AVERAGE = 58078 SATURDAY AVERAGE = 0 SUNDAY AVERAGE = 42941 NUMBER OF GOOD DAYS 13 TOTAL MONTHLY COUNT = 727221

MONTHLY AVERAGE = 55555

COMMENTS:
9/5: LABOR DAY

"B"=====> BAD DAY
 "N"=====> NORMAL DAY
 "A"=====> ATYPICAL DAY
 "H"=====> ATYPICAL DAY (HOLIDAY)
 "S"=====> ATYPICAL DAY (SPECIAL EVENT)

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
SEPTEMBER 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: W LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1	R	980	540	402	403	413	621	1486	3371	3754	3118	2901	2936	3196	3119	3753	4743	4616	4470	3294	2722	2170	1794	1624	1795	58221N
2	F	1271	654	426	419	502	685	1429	3193	3471	2993	3085	3307	3359	3424	4016	4225	4399	3826	3104	2780	2266	2113	2094	2322	59363N
3	A	1769	1322	991	901	770	668	857	1281	1630	1886	2041	2459	2576	2482	2622	3251	3063	3202	3026	2700	2517	2254	2292	2522	49082N
4	S	2035	1528	1097	827	805	910	800	1152	1207	1468	1843	2156	2392	2509	2647	2981	3077	3209	3216	3258	3156	2329	2674	2420	49696A
5	M	1708	1042	783	651	777	879	769	1417	1443	1792	2093	2491	2632	2658	3079	3558	2964	2911	2580	2343	1787	1352	1241	1501	44451H
6	T	900	461	285	286	427	760	1603	3475	3764	3178	2865	2873	3052	3052	3718	4748	4445	4266	3401	2508	1790	1583	1335	1621	56396N
7	W	833	403	242	181	311	605	1565	3389	3835	3136	2872	3018	3102	3015	3767	4686	4473	3828	3502	2868	2030	1680	1522	1687	56550N
8	R	991	534	346	312	398	633	1620	3410	3721	3156	2946	3098	3133	3107	3592	4671	4679	4384	3459	2774	2156	1643	1566	1749	58078N
9	F	1098	602	367	335	468	622	1516	3249	3652	3109	3092	3346	3304	3158	3882	4594	4560	4641	3354	2864	2186	1867	1858	2200	59924N
10	A	2030	1118	811	693	713	707	860	1491	1613	2055	2333	2487	2699	2539	2712	3471	3309	3102	3058	2894	2803	2334	2226	2544	50602N
11	S	2113	1359	930	858	843	853	712	1219	1247	1555	2053	2321	2606	3094	2883	3192	2976	3144	2981	2476	2177	1592	1493	1673	46350N
12	M	916	489	303	301	452	782	1623	3455	3733	3189	2882	3031	3119	3175	3805	4594	4558	4393	3436	2688	2025	1512	1430	1591	57482N
13	T	794	413	231	214	388	626	1515	3427	3801	2965	2898	3334	3202	3043	3649	4608	4598	4316	3299	2671	2027	1576	1621	1946	57162N
14	W	956	385	258	213	303	607	1544	3429	3811	3157	2907	2922	3144	3091	3742	4653	4525	4451	3356	2965	2152	1870	1617	1807	57865N
15	R	1041	526	366	335	448	650	1612	3474	3877	3177	2880	3148	3161	3027	3797	4681	4416	4526	3802	2907	2226	1857	1842	2052	59828N
16	F	1257	628	444	342	455	653	1526	3309	3809	3115	3076	3262	3424	3255	3942	4567	4159	4105	3736	3241	2548	2157	2203	2603	61816N
17	A	2109	1313	986	831	704	688	877	1467	1770	2110	2347	2534	2692	2750	2911	3213	2490	2698	2993	3179	2948	2333	2504	2999	51446N
18	S	2200	1544	1084	884	805	787	758	1171	1289	1677	2176	2564	2638	2626	2755	3322	3517	4004	4584	4329	3503	1961	1647	1753	53578A
19	M	1013	515	336	336	473	819	1635	3496	3854	3067	2949	3007	3102	2990	3787	4691	4539	4446	3460	2529	1834	1511	1384	1529	57302N
20	T	854	461	270	251	318	694	1612	3417	3779	3273	3076	3162	3285	3335	3893	4682	4655	4432	3281	2624	1859	1578	1530	1871	58192N
21	W	896	416	251	200	304	660	1588	3415	3744	3223	2920	2995	3116	3124	3858	4554	4418	4368	3491	2729	2107	1654	1723	1986	57740N
22	R	1000	473	297	315	379	672	1532	3408	3784	3128	3024	3047	3227	3214	3677	4690	4595	4331	3448	2709	2104	1763	1636	1777	58230N
23	F	1210	612	405	374	457	658	1465	3299	3720	3142	3173	3210	3171	3377	3922	4734	4568	4137	3275	2870	2189	1727	1849	2165	59709N
24	A	1665	1219	839	714	657	645	880	1457	1755	2029	2408	2497	2605	2563	2828	3419	2979	3186	2814	2469	2108	1852	1945	2206	47739N
25	S	1829	1288	986	811	799	692	747	1091	1312	1612	1826	2497	2720	2472	2734	3123	2884	3044	2930	2600	2057	1550	1503	1802	44909N
26	M	1015	492	348	329	489	743	1642	3407	3425	3275	3032	3046	3056	3153	3787	4625	4686	4594	3598	2586	1749	1337	1339	1551	57304N
27	T	797	398	273	261	353	635	1537	3457	3766	3151	2999	3184	3264	3396	3798	3766	3810	4212	3560	2555	1991	1596	1409	1556	55724N
28	W	858	445	237	208	303	597	1616	3393	3943	3124	2856	2955	3126	3000	3285	3754	4497	4434	3460	2911	2170	1671	1554	1879	56276N
29	R	1288	580	375	327	415	592	1603	3370	3715	3032	3023	3236	3162	3264	3709	4106	4467	4333	3537	2702	2084	1634	1536	1871	57961N
30	F	1277	626	380	355	437	646	1589	3304	3660	3184	3115	3274	3271	3351	3994	4736	4623	3913	3201	2389	2191	1833	1794	2005	59148N

WEEKDAY AVERAGE = 57314 SATURDAY AVERAGE = 49717 SUNDAY AVERAGE = 48633 NUMBER OF GOOD DAYS 30 TOTAL MONTHLY COUNT = 1658124
MONTHLY AVERAGE = 54988

COMMENTS:

"B"=====> BAD DAY
"N"=====> NORMAL DAY
"A"=====> ATYPICAL DAY
"H"=====> ATYPICAL DAY (HOLIDAY)
"S"=====> ATYPICAL DAY (SPECIAL EVENT)

9/5: LABOR DAY

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
OCTOBER 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: E LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1	A	1283	837	574	460	437	868	1924	1908	2223	2115	2262	2368	2450	2772	3259	3102	2881	2763	2803	2517	2453	2292	2376	1782	48709N
2	S	1339	1033	647	532	534	761	1076	1514	1684	1749	1994	2279	2395	2623	2912	2674	2523	2486	2543	2238	1945	1713	1637	1106	41937N
3	M	729	418	250	204	445	1340	3494	4264	3902	3213	2851	2572	2524	2715	3140	2768	2909	3305	3436	2621	1948	1509	1562	999	53118N
4	T	522	270	162	171	397	1362	3711	4354	4088	3597	3037	2633	2497	2625	3158	2762	3000	3085	3462	2669	2105	1681	1660	1028	54036N
5	W																									43010B
6	R																									12328B
7	F																									32918B
8	A																									34941B
9	S																									32024B
10	M																									40822B
11	T																									49214B
12	W	621	341	222	204	407	1408	3551	4223	3882	3395	2987	2856	2643	2784	3287	2875	2922	3091	3322	2900	2090	1995	1800	1116	54922N
13	R	687	391	217	213	429	1428	3796	4722	3928	3996	3520	3040	2808	3147	3643	3390	3413	3707	3951	3411	2428	2253	2115	1494	62127N
14	F	857	423	324	264	515	1473	3821	4583	4309	3859	3455	3181	3110	3093	3746	3495	3451	3706	3760	3226	2671	2461	2370	1835	63988N
15	A	1350	866	614	521	566	975	2137	2222	2501	2505	2610	2679	2532	2967	3337	3113	3034	3086	3106	2490	2345	2210	2342	1927	52035N
16	S	1448	961	615	451	483	875	1628	1484	1923	2098	2301	2430	2558	2642	2940	2775	2828	2929	2830	2371	2065	1929	1712	1134	45410N
17	M	733	447	300	273	486	1509	3669	4273	4123	3662	3365	3005	2830	2880	3528	3051	3129	3437	3691	2848	2081	1773	1672	1043	57808N
18	T	609	288	192	197	419	1497	3628	4438	3860	3514	2845	2963	2698	2864	3296	3062	3070	3266	3703	2975	2208	1893	1910	1132	56527N
19	W	595	293	209	213	432	1509	3825	4491	4025	3720	3315	2967	2892	3092	3495	3206	3153	3472	3886	3140	2327	1902	1660	1282	59101N
20	R	735	410	239	236	495	1546	3772	4612	3924	3703	3231	2965	2934	3061	3440	2673	3158	2806	3507	3150	2332	2182	2151	1391	58653N
21	F	845	450	269	266	522	1550	3754	4582	4054	3818	3386	3193	3013	3288	3522	3444	3484	3476	3943	3338	2612	2324	2444	1980	63557N
22	A	1447	875	548	496	512	982	2079	2208	2418	2427	2486	2637	2823	3255	3739	3377	3297	3142	2965	2816	2565	2414	2460	2048	54016N
23	S	1593	1038	738	566	542	826	1457	1369	1675	1839	2156	2340	2635	2839	3747	3272	3433	2954	2820	2321	1982	1642	1507	1093	46384N
24	M	572	357	247	239	500	1502	3662	4444	3891	3571	2977	2844	2730	2944	3304	3165	3056	3327	3604	2751	2089	1713	1618	1022	56129N
25	T	528	296	205	184	442	1483	3756	4443	4143	3624	3044	2656	2579	2803	3520	3077	3076	3314	3831	3294	2377	1790	1855	1197	57517N
26	W	525	286	176	214	466	1478	3688	4634	4091	3653	3293	2892	2784	3088	3477	3222	3279	3381	3685	3260	2378	1945	1956	1259	59110N
27	R	642	337	212	228	471	1457	3735	4535	4083	3697	3204	2901	2773	3080	2728	3311	3381	3506	3592	3196	2388	2116	2023	1294	58890N
28	F	827	459	316	269	476	1401	3405	4146	4143	3618	3447	3110	3031	3228	3745	3371	3625	3837	3820	3213	2637	2404	2548	2076	63152N
29	A	1601	1070	637	572	573	950	2079	1952	2332	2164	2285	2232	2509	2736	3059	3010	2858	2858	2424	3262	2558	2485	2455	2227	50888N
30	S	1756	1275	904	741	679	914	1480	1343	1664	1727	1793	2081	2262	2366	2976	2756	2658	2622	2657	2274	2087	1934	1796	1134	43879N
31	M	858	549	310	306	564	1465	3643	4247	4025	3610	3015	2664	2722	2834	3252	3096	3213	3580	3848	3121	2745	2530	2347	1634	60178S

WEEKDAY AVERAGE = 58800 SATURDAY AVERAGE = 51412 SUNDAY AVERAGE = 44403 NUMBER OF GOOD DAYS 24 TOTAL MONTHLY COUNT = 1322071
MONTHLY AVERAGE = 55688

COMMENTS:
10/5 - 10/11: HURRICANE MATTHEW IMPACTS TRAFFIC
10/31: HALLOWEEN

"B"=====> BAD DAY
"N"=====> NORMAL DAY
"A"=====> ATYPICAL DAY
"H"=====> ATYPICAL DAY (HOLIDAY)
"S"=====> ATYPICAL DAY (SPECIAL EVENT)

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
OCTOBER 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: W LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1	A	1638	1087	820	689	631	630	808	1391	1560	1955	2380	2660	2553	2523	2709	3242	2835	2814	2751	2643	2197	1967	1940	2249	46672N
2	S	1809	1383	979	826	842	744	679	1156	1214	1491	1904	2289	2512	2485	2501	2917	2718	2907	2821	2512	1910	1589	1590	1644	43422N
3	M	931	502	315	289	444	599	1381	2672	3241	2935	2750	3070	2890	2973	3589	4619	4494	4364	3180	2568	1804	1307	1322	1592	53831N
4	T	794	401	221	184	283	579	1471	3304	3634	3014	2715	2882	2990	3178	3566	4641	4530	4429	3377	2615	1941	1560	1556	1527	55392N
5	W	864	398	275	202	306	639	1527	3271	3757	3129	2869	2913	2898	3368	4198	4577	3957	3688	2828	2216	1557	1192	1085	1267	52981A
6	R																									16689B
7	F																									41757B
8	A	1624	1063	756	682	660	632	818	1387	1543	1807	2194	2307	2458	2540	2635	3256	2978	3030	2825	2754	2165	1796	1855	2125	45890N
9	S	1879	1311	952	831	794	688	689	1127	1181	1491	1914	2361	2694	2443	2647	2871	2988	3067	2920	2868	2063	1448	1408	1618	44253N
10	M	1045	625	428	362	459	798	1454	3190	3602	3102	2983	2972	3180	3089	3746	4775	4718	4513	3368	2722	1794	1473	1378	2095	57871A
11	T	979	469	315	285	374	684	1552	3351	3775	3278	2964	3224	3133	3119	3653	4851	4675	4543	3372	2708	1844	1495	1576	2107	58326N
12	W	964	508	330	282	349	611	1344	2731	3237	2843	2881	2950	2968	3052	3562	4470	4575	4261	3308	2590	2030	1710	1434	1652	54642N
13	R	918	515	340	346	395	641	1575	3499	3771	3133	3064	3185	3296	3246	3701	4692	4614	4666	3580	3019	2281	1922	1708	1886	59993N
14	F	1265	700	449	369	451	642	1483	3245	3712	3212	3133	3236	3406	3314	3904	4849	4778	4455	3666	3271	2326	2061	1998	2224	62149N
15	A	1597	1212	889	807	743	706	932	1563	1700	2147	2379	2692	2874	3108	2922	3406	2983	3202	3029	2593	2462	1970	1979	2150	50045N
16	S	1743	1277	942	715	833	786	761	1241	1326	1678	2368	2781	2960	2476	2580	3234	2888	2992	2915	2470	1990	1526	1460	1768	45710N
17	M	1012	541	390	331	500	944	1544	3396	3685	3262	2998	3173	3168	3137	3693	4659	4760	4408	3395	2767	2010	1500	2067	2626	59966A
18	T	1252	503	334	277	330	677	1557	3289	3601	3083	3021	2886	3019	3002	3673	4625	4594	4452	3534	2772	2041	1638	1596	1717	57473A
19	W	978	516	240	236	332	610	1555	3329	3752	3215	3061	3267	3288	3185	4025	4749	4586	4590	3601	2836	2080	1627	1509	1825	58992N
20	R	1094	546	370	337	424	650	1622	3454	3719	3387	3167	3232	3305	3294	3771	3165	2828	4133	3438	2907	2112	1593	1653	1883	56084A
21	F	1265	597	396	381	438	711	1566	3395	3680	3334	3249	3331	3439	1854	3393	4785	4696	4513	3637	2925	2378	1998	1957	2190	60108A
22	A	1713	1209	801	697	669	690	925	1652	1862	2132	2445	2582	2693	2624	3000	3578	3115	3339	3166	3114	2496	2059	1982	2474	51017N
23	S	2016	1371	955	805	786	837	757	1238	1297	1739	2293	2607	2619	2537	2517	3123	2990	3065	3157	2740	1924	1536	1333	1548	45790N
24	M	948	496	323	303	467	724	1564	3214	3582	3142	3052	3122	3081	3005	3687	4789	4521	4351	3349	2460	1781	1364	1370	1545	56240N
25	T	945	446	260	252	318	627	1529	3280	3454	3118	2928	3019	2912	2999	3566	4749	4597	4554	3611	2740	1917	1512	1497	1773	56603N
26	W	988	441	233	226	280	670	1525	3376	3803	3255	3114	3239	3124	3207	3931	4719	4798	4603	3662	2642	2094	1524	1456	1639	58549N
27	R	965	472	318	325	388	646	1555	3401	3699	3404	3283	3555	3385	3406	3776	4748	4780	4485	2235	1886	2093	1789	1590	1748	57932A
28	F	1128	590	386	396	443	728	1423	3105	3602	3243	3341	3568	3405	3389	3939	4838	4584	4158	3405	2770	2114	1846	1970	2255	60626N
29	A	1804	1372	971	835	763	726	850	1457	1671	2035	2470	2812	2856	2693	2865	3228	2748	2872	2648	2384	2014	1688	2004	2542	48308N
30	S	1961	1477	1219	1093	1012	929	792	1134	1243	1581	2095	2480	2688	2521	2483	2920	2787	2862	2527	2234	1830	1530	1385	1597	44380N
31	M	1062	539	393	408	505	829	1561	3284	3527	3232	3002	3050	3056	3063	3608	4762	4731	4176	3124	2426	1985	1756	1803	2180	58062S

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WEEKDAY AVERAGE = 57880 SATURDAY AVERAGE = 48386 SUNDAY AVERAGE = 44711 NUMBER OF GOOD DAYS 29 TOTAL MONTHLY COUNT = 1561307

MONTHLY AVERAGE = 54642

COMMENTS:

- "B"=====> BAD DAY
 - "N"=====> NORMAL DAY
 - "A"=====> ATYPICAL DAY
 - "H"=====> ATYPICAL DAY (HOLIDAY)
 - "S"=====> ATYPICAL DAY (SPECIAL EVENT)
- 10/5 - 10/11: HURRICANE MATTHEW IMPACTS TRAFFIC
10/31: HALLOWEEN

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
NOVEMBER 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: E LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1	T	945	643	454	382	571	1487	3645	4574	4033	3169	3207	2732	2729	2940	3379	3145	3205	3352	3777	2981	2293	1869	1903	1197	58612A
2	W	594	292	192	172	438	1443	3687	4611	4005	3714	3255	2891	3044	2958	3383	3178	3238	3436	3642	3457	2920	2021	1921	1199	59691N
3	R	757	419	255	258	484	1440	3800	4434	4083	3567	3227	3339	2883	3139	3625	3415	3375	3663	3815	3700	2679	2227	2210	1483	62277N
4	F	873	493	290	275	475	1496	3777	4631	4069	3757	3391	3301	3327	3411	3745	3406	3522	3783	3736	3662	2944	2483	2420	1759	65026A
5	A	1239	834	569	470	524	973	1824	2400	2394	2416	2514	2641	2583	2736	3171	3051	3064	3026	3082	2847	2423	2311	2502	2057	51651N
6	S	1652	700	526	486	559	893	1731	1702	2063	2026	2392	2696	2661	2643	3011	2887	2833	3064	3093	2312	1971	1734	1620	1036	46291A
7	M	625	340	203	241	493	1567	3856	4548	3885	3502	3221	2423	2986	3053	3468	3215	3300	3674	3942	3058	2036	1774	1707	1052	58169N
8	T	553	282	214	206	434	1565	3707	4557	3949	3748	3175	3022	2851	2937	3444	3092	3243	3678	4005	3140	2137	1820	1644	1047	58450N
9	W	650	284	224	230	450	1505	3851	4763	4038	3789	3251	2890	2840	3275	3467	3428	3282	3563	3822	3341	2181	1927	1683	1147	59881N
10	R	682	384	235	210	452	1514	3891	4765	4124	3724	3633	3113	2865	3207	3668	3253	3311	3508	3623	3408	2689	2313	2096	1678	62346N
11	F	1002	500	324	310	472	1435	3622	4353	3991	3460	3403	3329	3396	3628	3856	3858	3698	3695	3653	3925	2554	3496	2783	1830	66573A
12	A	1306	846	580	444	552	952	2155	2374	2569	2567	2682	2659	2766	3031	3811	3969	3892	3833	3872	2999	2380	2310	2612	2058	57219N
13	S	1521	1074	728	581	607	909	1706	1639	1933	2049	2459	2780	2750	2946	3324	3062	2913	2958	3191	2377	2060	1792	1656	1115	48130N
14	M	609	394	256	290	591	1539	3932	4719	3980	3787	3491	2958	2853	2964	3477	3316	3331	3341	3935	3276	2294	1955	1788	1002	60078N
15	T	557	317	206	194	452	1501	3854	4493	3986	3920	3304	2943	2816	2946	3448	3104	3281	3218	3813	3324	2095	1888	1935	1173	58768N
16	W	548	314	161	202	400	1463	3912	4692	3894	3772	3685	3113	2857	2923	3507	3294	3462	3521	3861	3417	2400	2048	1939	1172	60557N
17	R	701	401	260	202	483	1512	3909	4710	4074	3647	3358	3007	3047	3260	3641	3447	3405	3673	4052	3350	2446	2150	2393	1580	62708N
18	F	938	478	289	252	470	1503	3900	4297	4057	3743	3439	3210	3115	3702	3836	3355	3890	3913	3712	3663	3341	2411	2371	2039	65924N
19	A	1522	903	580	512	484	1008	2242	2285	2651	2466	2450	2554	2855	3107	3603	3543	3413	3336	3514	3372	3031	2541	2613	2196	56781N
20	S	1848	1181	705	557	554	908	1784	1666	1991	2091	2239	2565	2797	2895	3314	3065	2897	2881	2867	2344	2050	1832	1714	1077	47822N
21	M	709	416	251	238	481	1431	3853	4647	4008	3837	3320	2983	3077	3140	3521	3171	3237	3518	3711	3082	2197	1811	1701	1092	59432N
22	T	593	337	218	174	446	1373	3842	4759	4025	3747	3485	3233	2945	3242	3491	3252	3219	3353	3766	3225	2221	1951	1853	1217	59967N
23	W	631	397	288	202	409	1360	3596	4503	4254	3679	3319	3056	3063	3225	3553	3080	3087	3416	3118	2701	2199	2129	2127	1546	58938A
24	R	1189	733	473	413	438	840	1686	1707	1609	1601	1728	1885	2064	2192	2700	2277	2256	2060	2130	1811	1807	1957	2152	1650	39358H
25	F	1192	715	417	295	406	996	2407	2816	2511	2288	2409	2739	2695	2913	3578	3212	3212	2940	2783	2451	2123	2119	2202	1851	51270A
26	A	1355	800	513	500	511	914	2012	2086	2332	2179	2323	2461	2636	2807	3295	3301	3170	3116	3000	2664	2506	2358	2565	2144	51548N
27	S	1477	983	692	589	572	943	1686	1663	1950	1975	2180	2545	2782	2876	3263	2927	2930	2747	2877	2246	1866	1785	1621	1159	46334N
28	M	655	412	280	210	542	1493	3787	4589	4037	3901	3389	3110	2954	3320	3794	3482	3482	3715	3981	3231	2262	1949	1999	1203	61777N
29	T	696	421	228	219	457	1433	3806	4727	3997	3983	3538	3301	3188	3360	3929	3542	3513	3581	3708	3387	2919	2494	2313	1807	64547A
30	W	1022	503	334	259	469	1490	3884	4785	3844	4116	3890	3608	3365	3550	4008	3855	3892	3720	3938	3604	2768	2416	2305	1828	67453A

WEEKDAY AVERAGE = 60021 SATURDAY AVERAGE = 54300 SUNDAY AVERAGE = 47144 NUMBER OF GOOD DAYS 30 TOTAL MONTHLY COUNT = 1727578
MONTHLY AVERAGE = 57364

COMMENTS:

"B"=====> BAD DAY 11/6: DAYLIGHT SAVING TIME ENDS
"N"=====> NORMAL DAY 11/8: PRESIDENTIAL ELECTION STATEWIDE
"A"=====> ATYPICAL DAY 11/11: VETERANS DAY
"H"=====> ATYPICAL DAY (HOLIDAY) 11/24: THANKSGIVING
"S"=====> ATYPICAL DAY (SPECIAL EVENT)

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
NOVEMBER 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: W LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY																				
1	T	1600	1111	947	598	597	815	1581	3294	3644	3153	3000	2924	3127	2978	3721	4795	4651	4438	3660	2826	1953	1485	1419	1577	59894A																				
2	W	925	456	243	215	271	614	1511	3374	3757	3312	2966	3118	3092	3227	3229	2724	4370	4722	3731	2844	2247	1790	1601	1875	56214A																				
3	R	1113	572	325	303	367	638	1558	3374	3713	3255	3229	3313	3357	3274	3673	4876	4516	4596	3511	2865	2097	1869	1840	2225	60459N																				
4	F	1314	693	434	371	411	690	1458	3289	3561	3352	3431	3490	3646	3506	3939	4520	3738	4161	4144	3087	2350	2028	1991	2272	61876N																				
5	A	1807	1161	800	644	725	697	887	1629	1796	2188	2570	2828	3027	2985	2992	3394	3062	3155	2904	2754	2163	1753	1917	2399	50237N																				
6	S	1975	1025	797	705	780	690	857	1403	1661	2030	2655	3055	3099	2645	2696	3076	2734	3238	2991	2462	1819	1520	1315	1515	46743A																				
7	M	877	482	291	277	469	756	1693	3686	3812	3405	3139	3350	3250	3136	3714	4653	4769	4539	3468	2559	1801	1413	1395	1490	58424N																				
8	T	821	474	249	245	360	689	1583	3295	3708	3108	3023	3169	3248	3374	3779	4617	4510	4614	3376	2583	1862	1571	1459	1754	57471N																				
9	W	1324	566	327	274	313	671	1597	3417	3867	3271	3109	3152	3292	3156	3774	4329	4116	4649	3715	2625	1969	1644	1794	1641	58592N																				
10	R	1059	496	348	341	355	630	1596	3524	3715	3764	3214	3241	3204	3329	3759	4837	4650	3749	4133	2761	2094	1808	1722	2179	60508N																				
11	F	1338	708	454	385	490	712	1284	2782	3294	3317	3230	3364	3507	3392	3916	4741	4454	4368	3583	3633	2375	1922	2028	2438	61715N																				
12	A	1820	1298	827	683	742	672	929	1587	1903	2255	2513	2809	2873	2715	2973	3549	3437	3449	3357	2870	2160	2027	2277	2453	52178N																				
13	S	2093	1510	1102	852	951	973	841	1379	1429	1850	2306	2674	2922	2682	2943	3296	3150	3347	3424	2509	1801	1478	1510	2038	49060N																				
14	M	1115	530	314	338	527	849	1690	3637	3918	3318	3153	3278	3250	3152	3807	4804	4693	4252	3659	2583	1893	1608	1494	1743	59605N																				
15	T	1176	503	305	287	340	643	1631	3592	3815	3375	3045	3196	3289	3328	3744	4435	3984	3900	4203	2961	2020	1540	1461	1696	58469N																				
16	W	827	410	264	171	335	630	1605	3544	3876	3256	3028	2991	3441	3383	4201	4721	4526	4640	3781	2710	2014	1793	1606	2078	59831N																				
17	R	1116	595	349	339	438	645	1699	3604	3595	3268	3283	3316	3323	3325	3888	4785	4709	4550	4048	2893	2156	1817	1696	1910	61347N																				
18	F	1177	683	469	442	449	729	1551	3470	3612	3424	3340	3609	3567	3385	4132	4281	4408	4586	4159	2785	2217	2094	2064	2375	63008N																				
19	A	1771	1184	890	705	690	710	968	1676	1992	2281	2740	2924	3014	2900	3168	3661	3503	3771	3539	2689	2199	2080	2373	2652	54080N																				
20	S	2302	1607	1049	947	906	826	825	1404	1550	1892	2386	2743	2880	2683	2733	3316	3224	3433	3437	2425	1895	1546	1412	1529	48950N																				
21	M	926	494	342	334	526	753	1622	3612	3828	3404	3279	3329	3301	3207	3812	4738	4306	4165	4046	2601	2050	1432	1426	1539	59072N																				
22	T	846	435	265	263	336	645	1611	3374	3733	3212	3149	3380	3425	3346	3813	4644	4196	4507	3867	2686	2256	1618	1617	1694	58918N																				
23	W	986	497	324	231	302	625	1358	2901	3303	3132	3285	3371	3691	3651	4130	3333	3611	4115	2886	2485	1975	1637	1693	1931	55453A																				
24	R	1292	806	625	632	584	613	725	1217	1318	1557	1767	2099	2282	2358	2579	2996	2753	2722	2239	1800	1551	1502	1546	1747	39310H																				
25	F	1021	537	372	272	355	542	856	1665	1887	2207	2314	2477	2524	2673	3064	3612	3573	3404	3143	2480	1979	1769	1884	2022	46632A																				
26	A	1564	1085	820	721	716	638	842	1378	1587	1895	2314	2366	2861	2625	2862	3298	3150	3496	3505	2882	2254	1903	2065	2324	49151N																				
27	S	1907	1342	1133	882	873	767	830	1273	1318	1749	2396	2840	2848	2591	2521	2925	3052	3425	3429	2521	2082	1631	1475	1662	47472N																				
28	M	932	548	340	316	514	826	1655	3513	3666	3345	3256	3388	3387	3360	3992	4891	4621	4479	3765	3025	2296	1803	1701	1796	61415N																				
29	T	1053	701	392	293	386	683	1610	3545	3844	3407	3089	3343	3458	3435	3824	4766	4615	4269	3306	2939	2623	2112	2097	2379	62169A																				
30	W	1596	909	627	441	437	646	1628	3531	3976	3664	3193	3387	3540	3393	4078	4680	4657	4124	4041	3567	2813	2549	2267	2397	66141A																				
WEEKDAY AVERAGE =		58395					SATURDAY AVERAGE =					51412					SUNDAY AVERAGE =					48056					NUMBER OF GOOD DAYS					30					TOTAL MONTHLY COUNT =					1684394				
MONTHLY AVERAGE =		55920																																												

COMMENTS:

"B"=====> BAD DAY 11/6: DAYLIGHT SAVING TIME ENDS
"N"=====> NORMAL DAY 11/8: PRESIDENTIAL ELECTION STATEWIDE
"A"=====> ATYPICAL DAY 11/11: VETERANS DAY
"H"=====> ATYPICAL DAY (HOLIDAY) 11/24: THANKSGIVING
"S"=====> ATYPICAL DAY (SPECIAL EVENT)

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
DECEMBER 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: E LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1	R	1167	699	468	370	495	1433	3504	4549	3833	4037	3867	3478	3698	3751	3681	3753	3631	3816	3722	3754	2958	2716	2637	2068	68085A
2	F	1583	821	661	593	690	1552	3538	4398	4017	3907	4085	3961	3931	3920	4053	3981	3863	3633	3782	3629	3155	2926	2804	2395	71878A
3	A	1941	1377	1126	897	937	1194	2140	2396	2684	2739	3100	3279	3517	3702	3909	3980	3626	3895	3669	3496	2866	2707	2686	2431	64294A
4	S	2005	1566	1080	1005	870	1168	1912	1831	2153	2303	2665	3138	3650	3668	4053	3926	3704	3361	3147	2682	2257	2001	1868	1394	57407A
5	M	1003	665	504	469	663	1710	3858	4620	4114	3721	3408	3065	3049	3282	3687	3287	3450	3408	3877	3053	2101	1708	1564	1352	61618A
6	T	589	366	231	193	506	1518	3765	4706	4088	3937	2979	2789	2845	3237	3566	3218	3306	3369	3642	3044	2193	1890	1954	1128	59059N
7	W	598	325	185	198	453	1444	3750	4727	4073	3755	3292	2886	2839	3162	3537	3189	3332	3508	3353	3230	2152	2005	2007	1262	59262N
8	R	654	381	255	235	482	1431	3702	4774	4059	3625	3301	2989	2979	3205	3345	3107	3145	3472	3415	3114	2350	2038	2118	1370	59546N
9	F	810	491	300	262	467	1533	2822	4495	3816	3913	3299	3060	2898	3537	3867	3957	3872	3803	3484	3438	2915	2347	2293	1721	63400N
10	A	1335	846	570	443	516	879	1937	1992	2235	2009	1901	2214	2280	2615	3003	2854	2933	2982	2901	2771	2303	2213	2294	1992	48018N
11	S	1454	1025	640	530	529	894	1560	1487	1795	1821	1948	2153	2358	2548	2803	2659	2700	2740	2719	2330	2041	1692	1628	1070	43124N
12	M	696	449	246	269	550	1542	3698	4510	4031	3860	3115	2736	2870	3167	3597	3346	3389	3543	3643	3033	2040	1684	1823	1081	58918N
13	T	590	323	207	199	442	1533	3699	4596	4219	3930	3281	3115	2917	3065	3333	2905	3023	3192	3658	3102	2284	1984	1972	1145	58714N
14	W	658	305	228	217	455	1433	3732	4609	3992	3822	3268	2877	2917	3162	3634	3281	3300	3491	3684	3254	2362	2151	1980	1361	60173N
15	R	832	444	290	274	487	1445	3599	4560	4004	3692	3352	3066	2894	3275	3605	3405	3535	3684	3883	3391	2472	2161	2260	1574	62184N
16	F	954	512	348	298	450	1432	3678	4728	4258	3866	3290	3085	3222	3460	3844	3716	4101	3513	3670	3342	3080	2434	2493	2218	65992N
17	A	1563	944	651	466	541	891	1905	2108	2424	2323	2561	2638	2872	3075	3472	3303	3466	3479	3195	3281	2815	2512	2472	2126	55083N
18	S	1644	1213	781	617	579	898	1529	1527	1824	1935	2308	2582	2691	2930	3355	3202	3012	2885	2856	2349	2182	1192	1241	1218	46550A
19	M	837	525	320	273	505	1476	3672	4561	4111	3429	3069	2998	3132	3207	3411	3338	3292	3517	3972	2863	2213	1856	1821	1231	59629N
20	T	681	443	267	194	468	1472	3627	4768	3994	3645	3191	2882	2823	3092	3473	3367	3170	3448	3622	3254	2423	2114	2179	1503	60100N
21	W	800	381	277	221	428	1472	3676	4689	3909	3540	2927	2989	2919	3060	3528	3415	3538	3595	3732	3442	2363	2145	2008	1320	60374A
22	R	949	556	321	290	460	1107	3000	4577	3747	3368	2624	3127	3139	3195	3438	3279	3295	3453	3625	2784	2474	2214	2188	1915	59125A
23	F	1026	581	363	325	472	1331	3451	3897	3849	3109	3031	2958	3258	3373	3896	3745	3628	3230	2972	2544	2219	2100	2108	1668	59134A
24	A	1223	745	536	445	465	920	1717	1642	1928	2047	2150	2319	2225	2505	2717	2619	2472	2324	2388	2032	1892	1650	2056	1801	42818A
25	S	1466	1183	694	454	386	628	1247	1111	1249	1390	1530	1947	2210	2455	2957	2840	2753	2580	2603	2118	1744	1845	1879	1272	40541H
26	M	894	529	327	262	377	894	2015	2120	2073	2143	2396	2653	3049	3227	3645	2197	2868	3272	2946	2303	2011	1859	1984	1311	47355A
27	T	890	499	314	257	434	1398	3290	3966	3633	3129	2977	3257	3474	3723	3912	3504	3439	3665	3532	2793	2400	2148	2556	1425	60615A
28	W	844	477	297	261	414	1393	3447	4099	3727	2912	3266	3602	3473	3735	3917	4052	3871	3945	3736	3011	2354	2283	2317	1732	63165A
29	R	1013	570	357	332	493	1358	3324	4031	3812	3228	3211	3629	3636	3829	4087	3998	4017	4140	3571	3075	2675	2415	2602	1988	65391A
30	F	1169	699	470	388	578	1386	2793	3495	3601	3181	3266	3480	3680	3807	3731	4028	4262	3932	3721	3128	2738	2560	2659	2142	64894A
31	A	1741	1280	724	523	574	1014	1687	1816	2072	2237	2422	2789	3194	3222	3807	3568	3375	3107	2952	2789	2720	2886	3171	1764	55434A

WEEKDAY AVERAGE = 61034 SATURDAY AVERAGE = 53129 SUNDAY AVERAGE = 46906 NUMBER OF GOOD DAYS 31 TOTAL MONTHLY COUNT = 1801880
MONTHLY AVERAGE = 57887

COMMENTS:
12/25: CHRISTMAS DAY

"B"=====> BAD DAY
"N"=====> NORMAL DAY
"A"=====> ATYPICAL DAY
"H"=====> ATYPICAL DAY (HOLIDAY)
"S"=====> ATYPICAL DAY (SPECIAL EVENT)

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

DATE 03/16/17

FLORIDA DEPARTMENT OF TRANSPORTATION
TRAFFIC COUNTS
HOURLY CONTINUOUS COUNTS FINAL REPORT
DECEMBER 2016

COUNTY NAME: MIAMI-DADE STATION: 0108 DIRECTION: W LANE: 0
DESCRIPTION: SR-112/I-195, 1 MI E OF SR-5/US-1, DADE CO.
LOCATION: COUNTY 87 SECTION 004 SUBSECTION 000 MILEPOST 3.536 ROUTES: I-195 SR-112

DY	D	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	DAILY
1	R	1973	1200	775	606	653	786	1535	3545	3701	3173	3108	3364	3530	3621	3947	4603	4578	4057	3857	3081	2972	2741	2424	2628	66458A
2	F	2286	1563	1078	963	851	930	1669	3338	3664	3249	3276	3483	3468	3674	4144	4790	4432	4326	3934	3116	2922	2776	2607	2917	69456A
3	A	2507	2031	1756	1425	1230	1112	1139	1891	1869	2237	2698	2826	3048	3137	3273	3961	3658	4011	3990	3802	3262	2730	2650	2915	63158A
4	S	2523	2088	1627	1269	1305	1170	1114	1557	1591	1959	2514	2799	2997	3058	3244	3710	3763	4263	4306	3477	2570	1907	1717	1906	58434A
5	M	1276	865	608	552	718	1157	1777	3611	3977	3387	3224	3438	3463	3419	3978	4689	4513	4355	3843	2681	2024	1500	1354	1541	61950A
6	T	955	502	339	242	370	708	1605	3575	3846	3341	3125	3126	3311	3259	4009	4776	4541	4445	3500	2632	2045	1532	1549	1672	59005N
7	W	909	469	271	222	316	669	1558	3445	3897	3375	2971	3128	3204	3188	3958	4870	4659	4432	3428	2564	2133	1542	1735	1719	58662N
8	R	1027	485	339	322	417	685	1610	3482	3641	3386	3142	3347	3345	3237	3775	3967	3947	4206	3762	2553	2062	1757	1618	1801	57913N
9	F	1104	589	400	335	456	687	1479	3321	3638	3247	3332	3382	3487	3442	4253	4621	4567	4365	3442	2705	2185	1997	1968	2247	61249N
10	A	2137	1143	782	670	618	628	832	1525	1790	1979	2332	2647	2797	2624	2654	3058	2824	2854	2489	2221	1881	1797	1993	2251	46526N
11	S	1984	1344	902	707	838	1079	826	1269	1356	1715	2356	2732	2957	2569	2499	2821	2633	2810	2542	1990	1731	1485	1446	1567	44158N
12	M	1066	784	378	323	520	851	1644	3619	3925	3164	3135	3262	3358	3354	3786	4771	4712	4599	3568	2528	1806	1500	1479	1586	59718N
13	T	913	425	302	256	368	669	1582	3526	3867	3192	3042	3241	3252	3215	3791	3579	3503	4087	4044	3078	2128	1715	1603	1736	57114A
14	W	1173	508	309	244	354	636	1591	3373	3815	3257	3022	3213	3089	3229	3961	4794	4730	4446	3540	2628	2058	1713	1665	1917	59265N
15	R	1095	604	436	372	460	665	1543	3356	3730	3303	2966	3091	3267	3357	3860	4868	4914	4530	3640	2674	2193	1841	1911	2005	60681N
16	F	1393	744	498	419	459	686	1441	3235	3574	3140	3271	3340	3468	3454	4111	4885	4731	2126	3393	3301	2361	2098	2042	2526	60696A
17	A	1996	1424	930	769	755	659	864	1412	1737	2167	2447	2785	2913	2867	2914	3390	3220	3488	3230	2712	2296	2145	2226	2556	51902N
18	S	2363	1422	1088	877	812	805	798	1136	1309	1738	2283	2547	2896	2715	2759	3317	3336	3638	3645	2552	1899	1735	1542	1705	48917N
19	M	1040	576	397	310	512	815	1553	3183	3417	3195	3252	3204	3303	3316	3806	4481	4657	4630	3836	2672	1943	1722	1585	1772	59177N
20	T	1042	601	336	297	360	653	1470	3228	3655	3304	3273	3359	3344	3238	3804	4805	4926	4483	3767	2759	2128	1840	1685	1900	60257N
21	W	1117	566	331	254	343	652	1462	3191	3573	3330	3267	3403	3440	3431	3997	4710	4859	4525	3930	2873	2184	1768	1778	2100	61084N
22	R	1208	622	434	410	440	645	1419	2934	3378	3289	3159	3473	3450	3409	3945	4441	4601	4551	3835	2899	2225	1867	1790	2144	60568N
23	F	1332	696	485	421	496	650	1211	2503	2966	3048	3362	3559	3726	3823	4057	4643	4248	3687	3154	2518	2093	1753	1891	2089	58411N
24	A	1512	1008	714	568	641	625	715	1256	1441	1699	2144	2299	2391	2254	2482	2923	2807	2928	3005	2464	1945	1449	1521	1886	42677A
25	S	1422	1169	660	567	496	447	525	954	890	1152	1374	1633	1980	2038	2087	2566	2645	3016	3066	2316	1897	1621	1522	1726	37769H
26	M	1081	639	398	358	389	521	775	1345	1598	2051	2389	2641	2581	2548	2855	3279	3311	3522	3285	2687	2045	1724	1730	1862	45614A
27	T	1202	745	400	354	401	536	1141	2119	2601	2851	2964	3119	3168	3087	3609	4603	4651	4607	4555	3367	2380	1883	2033	2116	58492A
28	W	1324	705	394	315	367	576	1173	2243	2744	2973	3101	3261	3343	3221	3656	4880	4916	4608	4368	3293	2576	2015	2019	2281	60352A
29	R	1566	938	546	512	574	682	1162	2138	2695	2735	2999	3203	3265	3336	3854	4734	4636	4368	4374	3773	2562	2220	2141	2407	61420A
30	F	1711	1054	706	648	668	755	1154	2173	2540	2781	3136	3481	3639	3799	3990	4616	4586	4402	3794	3187	2391	2334	2277	2390	62212A
31	A	2046	1480	1084	834	803	760	754	1315	1466	1696	2125	2420	2620	2578	2666	3225	3278	3418	3266	2767	2370	2068	1950	1875	48864N

WEEKDAY AVERAGE = 59797 SATURDAY AVERAGE = 50625 SUNDAY AVERAGE = 47320 NUMBER OF GOOD DAYS 31 TOTAL MONTHLY COUNT = 1762159
MONTHLY AVERAGE = 56704

COMMENTS:
12/25: CHRISTMAS DAY

"B"=====> BAD DAY
"N"=====> NORMAL DAY
"A"=====> ATYPICAL DAY
"H"=====> ATYPICAL DAY (HOLIDAY)
"S"=====> ATYPICAL DAY (SPECIAL EVENT)

NOTE: ATYPICAL DAYS HAVE COUNTS THAT ARE HIGHER OR LOWER THAN NORMAL, BUT STILL REASONABLE, AND NO LOCAL SPECIAL EVENTS ARE KNOWN.

TRAFFIC SIGNAL TIMING PLANS








TOD Schedule Report
for 2047: N Miami Av&N 36 St

Print Date:
11/20/2017

Print Time:
1:51 PM

<u>Asset</u>	<u>Intersection</u>	<u>TOD Schedule</u>	<u>Op Mode</u>	<u>Plan #</u>	<u>Cycle</u>	<u>Offset</u>	<u>TOD Setting</u>	<u>Active PhaseBank</u>	<u>Active Maximum</u>
2047	N Miami Av&N 36 St	DOW-2		N/A	0	0	N/A	0	Max 0

Splits

<u>PH 1</u>	<u>PH 2</u>	<u>PH 3</u>	<u>PH 4</u>	<u>PH 5</u>	<u>PH 6</u>	<u>PH 7</u>	<u>PH 8</u>
EBL	WBT	SBL	NBT	WBL	EBT	-	SBT
0	0	0	0	0	0	0	0
							

AM

PM

Active Phase Bank: Phase Bank 1

Phase	<u>Walk</u>			<u>Don't Walk</u>			<u>Min Initial</u>			<u>Veh Ext</u>			<u>Max Limit</u>			<u>Max 2</u>			<u>Yellow</u>	<u>Red</u>
	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3		
1 EBL	0	0	0	0	0	0	7	7	7	2	2	2	10	10	10	26	26	26	3.7	2.6
2 WBT	7	7	7	19	19	19	16	16	16	1	1	1	32	32	32	0	40	40	4	2.6
3 SBL	0	0	0	0	0	0	7	7	7	2	2	2	10	10	10	26	26	26	3.7	2.3
4 NBT	7	7	7	19	19	19	16	16	16	2.5	-2.5	-2.5	32	32	32	110	96	96	4	2.3
5 WBL	0	0	0	0	0	0	7	7	7	2	2	2	10	10	10	26	26	26	3.7	2.6
6 EBT	7	7	7	19	19	19	16	16	16	1	1	1	32	32	32	0	40	40	4	2.6
7 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8 SBT	7	7	7	19	19	19	16	16	16	2.5	-2.5	-2.5	32	32	32	110	96	96	4	2.3

Last In Service Date: unknown

<u>Permitted Phases</u>	
	12345678
Default	123456-8
External Permit 0	-----
External Permit 1	12-456-8
External Permit 2	-2-4-6-8

<u>Current TOD Schedule</u>	<u>Plan</u>	<u>Cycle</u>	<u>Green Time</u>								<u>Ring Offset</u>	<u>Offset</u>
			1 EBL	2 WBT	3 SBL	4 NBT	5 WBL	6 EBT	7 -	8 SBT		
	3	180	20	30	18	87	20	30	0	110	0	140
	5	120	12	35	8	40	12	35	0	54	0	102
	9	130	12	42	12	39	12	42	0	57	0	90
	12	150	12	35	12	66	12	35	0	84	0	26
	13	110	9	39	8	29	9	39	0	43	0	52
	14	90	9	29	8	19	9	29	0	33	0	2
	18	80	7	4	8	36	7	4	0	50	0	0
	19	90	9	27	9	20	9	27	0	34	0	24
	20	110	12	34	12	27	12	34	0	45	0	96
	21	120	16	37	12	30	16	37	0	48	0	102
	22	100	10	35	10	20	10	35	0	36	0	2

<u>Local TOD Schedule</u>		
<u>Time</u>	<u>Plan</u>	<u>DOW</u>
0000	Free	Su M T W Th F S
0600	19	Su S
0600	3	M T W Th F
1000	5	M T W Th F
1000	20	Su S
1200	9	M T W Th F
1300	21	Su S
1500	12	M T W Th F
1800	22	Su S
1900	13	M T W Th F S
2200	14	M T W Th F
2200	Free	Su S

TOD Schedule Report
for 2047: N Miami Av&N 36 St

Print Date:
11/20/2017

Print Time:
1:51 PM

Current Time of Day Function				Local Time of Day Function				* Settings
<u>Time</u>	<u>Function</u>	<u>Settings *</u>	<u>Day of Week</u>	<u>Time</u>	<u>Function</u>	<u>Settings *</u>	<u>Day of Week</u>	
0000	TOD OUTPUTS	-----	SuM T W ThF S	0000	TOD OUTPUTS	-----	SuM T W ThF S	Blank - FREE - Phase Bank 1, Max 1
								Blank - Plan - Phase Bank 1, Max 2
								1 - Phase Bank 2, Max 1
								2 - Phase Bank 2, Max 2
								3 - Phase Bank 3, Max 1
								4 - Phase Bank 3, Max 2
								5 - EXTERNAL PERMIT 1
								6 - EXTERNAL PERMIT 2
								7 - X-PED OMIT
								8 - TBA

No Calendar Defined/Enabled







TOD Schedule Report
for 2097: US 1&NE 36 St

Print Date:
11/20/2017

Print Time:
1:59 PM

<u>Asset</u>	<u>Intersection</u>	<u>TOD Schedule</u>	<u>Op Mode</u>	<u>Plan #</u>	<u>Cycle</u>	<u>Offset</u>	<u>TOD Setting</u>	<u>Active PhaseBank</u>	<u>Active Maximum</u>
2097	US 1&NE 36 St	DOW-2		N/A	0	0	N/A	0	Max 0

Splits

<u>PH 1</u>	<u>PH 2</u>	<u>PH 3</u>	<u>PH 4</u>	<u>PH 5</u>	<u>PH 6</u>	<u>PH 7</u>	<u>PH 8</u>
NBL	SBT	EBT	WBT	SBL	NBT	-	-
0	0	0	0	0	0	0	0
							

Active Phase Bank: Phase Bank 1

<u>Phase</u>	<u>Walk</u>			<u>Don't Walk</u>			<u>Min Initial</u>			<u>Veh Ext</u>			<u>Max Limit</u>			<u>Max 2</u>			<u>Yellow</u>	<u>Red</u>
	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3		
1 NBL	0	0	0	0	0	0	5	5	5	3	3	3	10	7	7	40	35	35	4	2
2 SBT	7	7	7	14	14	14	7	7	7	1	1	1	35	35	35	0	37	37	4	2.1
3 EBT	4	4	4	17	17	17	7	7	7	2.5	2.5	2.5	24	7	7	55	50	50	4	2.6
4 WBT	5	5	5	13	13	13	7	7	7	3	3	3	24	20	20	75	67	67	4	2.2
5 SBL	0	0	0	0	0	0	5	5	5	3	3	3	13	5	5	60	35	35	4	2
6 NBT	7	7	7	14	14	14	7	7	7	1	1	1	35	35	35	0	37	37	4	2.1
7 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Last In Service Date: unknown

Permitted Phases

	12345678
Default	123456--
External Permit 0	123456--
External Permit 1	123456--
External Permit 2	123456--

TOD Schedule Report

for 2097: US 1&NE 36 St

Print Date:
11/20/2017

Print Time:
1:59 PM

Current TOD Schedule	Plan	Cycle	Green Time								Ring Offset	Offset
			1 NBL	2 SBT	3 EBT	4 WBT	5 SBL	6 NBT	7 -	8 -		
1		115	18	27	25	20	17	28	0	0	0	36
2		130	18	44	25	18	18	44	0	0	0	98
3		190	33	59	30	43	33	59	0	0	0	28
4		170	15	74	29	27	39	50	0	0	0	114
5		160	20	47	45	23	23	44	0	0	0	132
6		140	13	47	25	30	27	33	0	0	0	0
7		140	11	56	30	18	23	44	0	0	0	36
8		160	20	47	45	23	28	39	0	0	0	130
9		160	15	68	27	25	39	44	0	0	0	48
11		140	14	48	29	24	24	38	0	0	0	38
12		180	30	78	24	23	41	67	0	0	0	40
14		140	14	48	29	24	24	38	0	0	0	114
16		120	10	39	26	20	19	30	0	0	0	52
17		120	18	32	25	20	18	32	0	0	0	36
18		110	16	25	24	20	16	25	0	0	0	34
19		140	12	50	30	23	20	42	0	0	0	108
21		140	12	42	25	36	21	33	0	0	0	90
22		120	6	37	24	28	6	37	0	0	0	90
23		110	6	33	30	16	6	33	0	0	0	36
24		180	19	74	29	33	34	59	0	0	0	36
25		180	19	74	29	33	34	59	0	0	0	88
30		140	13	37	34	31	23	27	0	0	0	0

Local TOD Schedule			
Time	Plan	DOW	
0000	Free	M T W Th F	
0000	Free	Su	S
0500	2	M T W Th F	
0600	4	M T W Th F	
0600	19	Su	S
1000	6	M T W Th F	
1300	8	M T W Th F	
1500	12	M T W Th F	
1600	21	Su	S
2000	14	M T W Th F	
2200	16	M T W Th F	
2200	22	Su	S

Current Time of Day Function

Time	Function	Settings *	Day of Week
0000	TOD OUTPUTS	-----	SuM T W ThF S
0700	VEH MAX RECALL	---5---	M T W ThF
1000	VEH MAX RECALL	-----	M T W ThF
1330	VEH MAX RECALL	---5---	M T W ThF
1800	VEH MAX RECALL	-----	M T W ThF

Local Time of Day Function

Time	Function	Settings *	Day of Week
0000	TOD OUTPUTS	-----	SuM T W ThF S
0700	VEH MAX RECALL	---5---	M T W ThF
1000	VEH MAX RECALL	-----	M T W ThF
1330	VEH MAX RECALL	---5---	M T W ThF
1800	VEH MAX RECALL	-----	M T W ThF

* Settings

Blank - FREE - Phase Bank 1, Max 1
 Blank - Plan - Phase Bank 1, Max 2
 1 - Phase Bank 2, Max 1
 2 - Phase Bank 2, Max 2
 3 - Phase Bank 3, Max 1
 4 - Phase Bank 3, Max 2
 5 - EXTERNAL PERMIT 1
 6 - EXTERNAL PERMIT 2
 7 - X-PED OMIT
 8 - TBA

TOD Schedule Report
for 2097: US 1&NE 36 St

Print Date:
11/20/2017

Print Time:
1:59 PM

No Calendar Defined/Enabled

TOD Schedule Report


for 2098: Federal Hwy&NE 36 St&NE 2 Av

Print Date:
11/20/2017

Print Time:
1:59 PM

<u>Asset</u>	<u>Intersection</u>	<u>TOD Schedule</u>	<u>Op Mode</u>	<u>Plan #</u>	<u>Cycle</u>	<u>Offset</u>	<u>TOD Setting</u>	<u>Active PhaseBank</u>	<u>Active Maximum</u>
2098	Federal Hwy&NE 36 St&NE 2 Av	DOW-2		N/A	0	0	N/A	0	Max 0

Splits

<u>PH 1</u>	<u>PH 2</u>	<u>PH 3</u>	<u>PH 4</u>	<u>PH 5</u>	<u>PH 6</u>	<u>PH 7</u>	<u>PH 8</u>
EBL	WBT	SWT	SET	NBT	EBT	-	-
0	0	0	0	0	0	0	0
							

Active Phase Bank: Phase Bank 1

<u>Phase</u>	<u>Walk</u>			<u>Don't Walk</u>			<u>Min Initial</u>			<u>Veh Ext</u>			<u>Max Limit</u>			<u>Max 2</u>			<u>Yellow</u>	<u>Red</u>						
	<u>Phase Bank</u>																									
	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3								
1 EBL	0	-	0	0	0	-	0	0	8	-	8	8	2	-	2	2	6	-	6	6	40	-	40	40	4	4.4
2 WBT	7	-	7	7	19	-	18	18	7	-	7	7	1	-	1	1	30	-	30	30	0	-	0	0	4	4.4
3 SWT	0	-	0	0	0	-	0	0	7	-	7	7	2.5	-	2.5	2.5	10	-	10	10	45	-	38	38	4	2.4
4 SET	7	-	7	7	12	-	12	12	7	-	7	7	2.5	-	2.5	2.5	10	-	10	10	58	-	58	25	4	2.5
5 NBT	7	-	7	7	17	-	17	17	7	-	7	7	2.5	-	2.5	2.5	10	-	10	10	58	-	58	58	4	2.3
6 EBT	7	-	7	7	19	-	18	18	7	-	7	7	1	-	1	1	30	-	30	30	0	-	0	0	4	4.4
7 -	0	-	0	0	0	-	0	0	0	-	0	0	0	-	0	0	0	-	0	0	0	-	0	0	0	0
8 -	0	-	0	0	0	-	0	0	0	-	0	0	0	-	0	0	0	-	0	0	0	-	0	0	0	0

Last In Service Date: unknown

Permitted Phases	
12345678	
Default	123456--
External Permit 0	-23456--
External Permit 1	-23456--
External Permit 2	-23-5---

TOD Schedule Report

for 2098: Federal Hwy&NE 36 St&NE 2 Av

Print Date:
11/20/2017

Print Time:
1:59 PM

Current TOD Schedule	Plan	Cycle	Green Time								Ring Offset	Offset
			1 EBL	2 WBT	3 SWT	4 SET	5 NBT	6 EBT	7 -	8 -		
1		115	11	16	14	16	23	35	0	0	0	68
2		130	9	27	13	21	25	44	0	0	0	86
3		190	6	75	15	15	44	89	0	0	0	142
4		140	13	31	15	21	25	52	0	0	0	50
5		160	9	45	16	25	30	62	0	0	0	36
6		120	10	21	12	18	24	39	0	0	0	36
7		140	11	35	14	21	24	54	0	0	0	98
8		140	0	30	45	0	45	30	0	0	0	50
9		170	12	49	25	24	25	69	0	0	0	32
11		140	9	35	16	21	24	52	0	0	0	92
16		180	9	45	34	19	38	62	0	0	0	130
17		180	9	47	21	23	45	64	0	0	0	26
18		120	10	22	11	17	25	40	0	0	0	68
19		140	13	37	14	16	25	58	0	0	0	86
21		140	15	35	14	16	25	58	0	0	0	88
22		120	10	22	14	14	25	40	0	0	0	74
23		97	13	8	9	16	16	29	0	0	0	22
24		180	15	62	26	14	28	85	0	0	0	130

Local TOD Schedule		
Time	Plan	DOW
0000	Free	Su M T W Th F S
0500	2	M T W Th F
0600	9	M T W Th F
0600	19	Su S
1000	4	M T W Th F
1300	5	M T W Th F
1500	17	M T W Th F
1600	21	Su S
2000	11	M T W Th F
2200	6	M T W Th F
2200	22	Su S

Current Time of Day Function

Time	Function	Settings *	Day of Week
0000	TOD OUTPUTS	-----	SuM T W ThF S

Local Time of Day Function

Time	Function	Settings *	Day of Week
0000	TOD OUTPUTS	-----	SuM T W ThF S

* Settings

- Blank - FREE - Phase Bank 1, Max 1
- Blank - Plan - Phase Bank 1, Max 2
- 1 - Phase Bank 2, Max 1
- 2 - Phase Bank 2, Max 2
- 3 - Phase Bank 3, Max 1
- 4 - Phase Bank 3, Max 2
- 5 - EXTERNAL PERMIT 1
- 6 - EXTERNAL PERMIT 2
- 7 - X-PED OMIT
- 8 - TBA

No Calendar Defined/Enabled

TOD Schedule Report
for 2099: US 1&NE 38 St

Print Date:
12/9/2017

Print Time:
2:00 AM

<u>Asset</u>	<u>Intersection</u>	<u>TOD Schedule</u>	<u>Op Mode</u>	<u>Plan #</u>	<u>Cycle</u>	<u>Offset</u>	<u>TOD Setting</u>	<u>Active PhaseBank</u>	<u>Active Maximum</u>
2099	US 1&NE 38 St	DOW-7		N/A	0	0	N/A	0	Max 0

Splits

<u>PH 1</u>	<u>PH 2</u>	<u>PH 3</u>	<u>PH 4</u>	<u>PH 5</u>	<u>PH 6</u>	<u>PH 7</u>	<u>PH 8</u>
-	SBT	-	WBT	SBL	NBT	-	-
0	0	0	0	0	0	0	0



Active Phase Bank: Phase Bank 1

<u>Phase</u>	<u>Walk</u>			<u>Don't Walk</u>			<u>Min Initial</u>			<u>Veh Ext</u>			<u>Max Limit</u>			<u>Max 2</u>			<u>Yellow</u>	<u>Red</u>
	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3		
1 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2 SBT	7	7	7	12	12	12	7	7	7	1	1	1	30	30	30	0	60	60	4	2
3 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4 WBT	7	7	7	16	16	16	7	7	7	4	3.5	3.5	23	23	23	60	60	60	4	2.6
5 SBL	0	0	0	0	0	0	5	5	5	4	2	2	10	10	10	60	60	60	4	2
6 NBT	7	7	7	12	12	12	7	7	7	1	1	1	30	30	30	0	60	60	4	2
7 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Last In Service Date: unknown

Permitted Phases	
	12345678
Default	-2-456--
External Permit 0	-2-4-6--
External Permit 1	-2-4-6--
External Permit 2	-2-4-6--

TOD Schedule Report

for 2099: US 1&NE 38 St

Print Date:
12/9/2017

Print Time:
2:00 AM

Current TOD Schedule	Plan	Cycle	Green Time								Ring Offset	Offset
			1	2	3	4	5	6	7	8		
			-	SBT	-	WBT	SBL	NBT	-	-		
1		115	0	70	0	32	20	44	0	0	0	56
2		130	0	78	0	39	22	50	0	0	0	32
3		190	0	137	0	40	35	96	0	0	0	58
4		170	0	123	0	34	43	74	0	0	0	147
5		160	0	113	0	34	25	82	0	0	0	152
6		140	0	93	0	34	25	62	0	0	0	16
7		140	0	103	0	24	19	78	0	0	0	70
8		160	0	103	0	44	30	67	0	0	0	145
9		160	0	107	0	40	35	66	0	0	0	58
11		140	0	98	0	29	20	72	0	0	0	58
12		180	0	107	0	60	28	73	0	0	0	59
14		140	0	97	0	30	25	66	0	0	0	54
16		120	0	77	0	30	20	51	0	0	0	0
17		120	0	75	0	32	20	49	0	0	0	40
18		110	0	75	0	22	11	58	0	0	0	34
19		140	0	89	0	38	25	58	0	0	0	40
21		140	0	83	0	44	15	62	0	0	0	30
22		120	0	68	0	39	22	40	0	0	0	36
24		180	0	126	0	41	38	82	0	0	0	76
25		180	0	126	0	41	38	82	0	0	0	126

Local TOD Schedule		
Time	Plan	DOW
0000	Free	M T W Th F
0000	Free	Su S
0500	2	M T W Th F
0600	4	M T W Th F
0600	19	Su S
1000	6	M T W Th F
1300	8	M T W Th F
1500	12	M T W Th F
1600	21	Su S
2000	14	M T W Th F
2200	16	M T W Th F
2200	22	Su S

Current Time of Day Function

Time	Function	Settings *	Day of Week
0000	TOD OUTPUTS	-----	SuM T W ThF S

Local Time of Day Function

Time	Function	Settings *	Day of Week
0000	TOD OUTPUTS	-----	SuM T W ThF S

* Settings

- Blank - FREE - Phase Bank 1, Max 1
- Blank - Plan - Phase Bank 1, Max 2
- 1 - Phase Bank 2, Max 1
- 2 - Phase Bank 2, Max 2
- 3 - Phase Bank 3, Max 1
- 4 - Phase Bank 3, Max 2
- 5 - EXTERNAL PERMIT 1
- 6 - EXTERNAL PERMIT 2
- 7 - X-PED OMIT
- 8 - TBA

TOD Schedule Report
for 2099: US 1&NE 38 St

Print Date:
12/9/2017

Print Time:
2:00 AM

No Calendar Defined/Enabled







TOD Schedule Report
for 2102: NE 2 Av&NE 39 St

Print Date:
11/20/2017

Print Time:
2:00 PM

<u>Asset</u>	<u>Intersection</u>	<u>TOD Schedule</u>	<u>Op Mode</u>	<u>Plan #</u>	<u>Cycle</u>	<u>Offset</u>	<u>TOD Setting</u>	<u>Active PhaseBank</u>	<u>Active Maximum</u>
2102	NE 2 Av&NE 39 St	DOW-2		N/A	0	0	N/A	0	Max 0

Splits

<u>PH 1</u>	<u>PH 2</u>	<u>PH 3</u>	<u>PH 4</u>	<u>PH 5</u>	<u>PH 6</u>	<u>PH 7</u>	<u>PH 8</u>
NBL	SBT	-	WBT	SBL	NBT	-	EBT
0	0	0	0	0	0	0	0
							

Active Phase Bank: Phase Bank 1

<u>Phase</u>	<u>Walk</u>			<u>Don't Walk</u>			<u>Min Initial</u>			<u>Veh Ext</u>			<u>Max Limit</u>			<u>Max 2</u>			<u>Yellow</u>	<u>Red</u>
	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3		
1 NBL	0	0	0	0	0	0	5	5	5	2	2	2	12	12	12	20	20	20	4	2
2 SBT	7	7	7	13	13	13	7	7	7	1	1	1	30	30	30	0	20	20	4	2
3 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4 WBT	7	7	7	18	18	18	7	7	7	3.5	-2.5	-2.5	30	30	30	50	50	50	4	2
5 SBL	0	0	0	0	0	0	5	5	5	2	2	2	12	12	12	20	20	20	4	2
6 NBT	7	7	7	13	13	13	7	7	7	1	1	1	30	30	30	0	20	20	4	2
7 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8 EBT	7	7	7	18	18	18	7	7	7	3.5	-2.5	-2.5	30	30	30	50	50	50	4	2

Last In Service Date: unknown

Permitted Phases	
	<u>12345678</u>
Default	12-456-8
External Permit 0	-----
External Permit 1	-----
External Permit 2	-----

TOD Schedule Report
for 2102: NE 2 Av&NE 39 St

Print Date:
11/20/2017

Print Time:
2:00 PM

Current TOD Schedule	Plan	Cycle	Green Time								Ring Offset	Offset
			1 NBL	2 SBT	3 -	4 WBT	5 SBL	6 NBT	7 -	8 EBT		
2		130	10	67	0	35	10	67	0	35	0	46
3		90	0	84	0	0	0	84	0	0	0	0
4		85	8	34	0	25	8	34	0	25	0	51
5		85	0	79	0	0	0	79	0	0	0	0
6		70	5	17	0	30	5	17	0	30	0	41
7		70	5	10	0	37	5	10	0	37	0	2
8		160	10	87	0	45	10	87	0	45	0	74
9		110	0	104	0	0	0	104	0	0	0	0
11		70	0	64	0	0	0	64	0	0	0	10
12		180	17	100	0	45	17	100	0	45	0	135
14		140	10	74	0	38	10	74	0	38	0	66
16		120	10	57	0	35	10	57	0	35	0	14
17		60	5	18	0	19	5	18	0	19	0	54
18		55	5	13	0	19	5	13	0	19	0	34
19		140	15	62	0	45	15	62	0	45	0	54
21		140	10	72	0	40	10	72	0	40	0	44
22		60	5	12	0	25	5	12	0	25	0	36
23		80	0	74	0	0	0	74	0	0	0	0

Local TOD Schedule		
Time	Plan	DOW
0000	Free	M T W Th F
0000	Free	Su S
0500	2	M T W Th F
0600	4	M T W Th F
0600	19	Su S
1000	6	M T W Th F
1300	8	M T W Th F
1500	12	M T W Th F
1900	21	Su S
2000	14	M T W Th F
2200	16	M T W Th F
2300	Free	Su S

Current Time of Day Function

Time	Function	Settings *	Day of Week
0000	TOD OUTPUTS	-----	SuM T W ThF S

Local Time of Day Function

Time	Function	Settings *	Day of Week
0000	TOD OUTPUTS	-----	SuM T W ThF S

*** Settings**

- Blank - FREE - Phase Bank 1, Max 1
- Blank - Plan - Phase Bank 1, Max 2
- 1 - Phase Bank 2, Max 1
- 2 - Phase Bank 2, Max 2
- 3 - Phase Bank 3, Max 1
- 4 - Phase Bank 3, Max 2
- 5 - EXTERNAL PERMIT 1
- 6 - EXTERNAL PERMIT 2
- 7 - X-PED OMIT
- 8 - TBA

No Calendar Defined/Enabled

TOD Schedule Report

for 3298: Federal Hwy&NE 38 St&NE 39 St

Print Date:
11/20/2017

Print Time:
4:32 PM

<u>Asset</u>	<u>Intersection</u>	<u>TOD Schedule</u>	<u>Op Mode</u>	<u>Plan #</u>	<u>Cycle</u>	<u>Offset</u>	<u>TOD Setting</u>	<u>Active PhaseBank</u>	<u>Active Maximum</u>
3298	Federal Hwy&NE 38 St&NE 39 St	DOW-2		N/A	0	0	N/A	0	Max 0

Splits

<u>PH 1</u>	<u>PH 2</u>	<u>PH 3</u>	<u>PH 4</u>	<u>PH 5</u>	<u>PH 6</u>	<u>PH 7</u>	<u>PH 8</u>
-	SBT	-	-	-	NBT	WBT	EBT
0	0	0	0	0	0	0	0



Active Phase Bank: Phase Bank 1

Phase	Walk			Don't Walk			Min Initial			Veh Ext			Max Limit			Max 2			Yellow	Red
	Phase Bank																			
	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3		
1 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2 SBT	7	7	7	10	10	10	7	7	7	1	1	1	50	50	50	0	50	50	4	2
3 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
6 NBT	7	7	7	10	10	10	7	7	7	1	1	1	50	50	50	0	50	50	4	2
7 WBT	7	7	7	10	10	10	7	7	7	4	-2.5	-2.5	20	20	20	50	30	30	4	2
8 EBT	7	7	7	8	8	8	10	7	7	2.5	-2.5	-2.5	20	20	20	46	30	30	4	2

Last In Service Date: unknown

Permitted Phases	
Default	-2---678
External Permit 0	-----
External Permit 1	-----
External Permit 2	-----

TOD Schedule Report

for 3298: Federal Hwy&NE 38 St&NE 39 St

Print Date:
11/20/2017

Print Time:
4:32 PM

Current TOD Schedule	Plan	Cycle	Green Time								Ring Offset	Offset
			1	2	3	4	5	6	7	8		
			-	SBT	-	-	-	NBT	WBT	EBT		
1		80	0	21	0	0	0	21	26	15	0	22
2		130	0	51	0	0	0	51	36	25	0	40
3		90	0	31	0	0	0	31	30	11	0	78
4		170	0	78	0	0	0	78	39	35	0	36
5		85	0	17	0	0	0	17	32	18	0	6
6		140	0	58	0	0	0	58	37	27	0	32
7		70	0	17	0	0	0	17	18	17	0	8
8		160	0	64	0	0	0	64	44	34	0	68
9		110	0	41	0	0	0	41	32	19	0	40
11		70	0	11	0	0	0	11	26	15	0	2
12		180	0	61	0	0	0	61	56	45	0	118
14		140	0	61	0	0	0	61	36	25	0	58
16		120	0	46	0	0	0	46	28	28	0	8
17		60	0	19	0	0	0	19	12	11	0	54
18		55	0	4	0	0	0	4	19	14	0	34
19		140	0	54	0	0	0	54	39	29	0	44
21		140	0	54	0	0	0	54	39	29	0	38
22		60	0	14	0	0	0	14	19	9	0	36
23		80	0	21	0	0	0	21	26	15	0	22

Local TOD Schedule			
Time	Plan	DOW	
0000	Free	M T W Th F	
0000	Free	Su	S
0500	2	M T W Th F	
0600	4	M T W Th F	
0600	19	Su	S
1000	6	M T W Th F	
1300	8	M T W Th F	
1500	12	M T W Th F	
1900	21	Su	S
2000	14	M T W Th F	
2200	16	M T W Th F	
2300	Free	Su	S

Current Time of Day Function

Time	Function	Settings *	Day of Week
0000	TOD OUTPUTS	-----	SuM T W ThF S

Local Time of Day Function

Time	Function	Settings *	Day of Week
0000	TOD OUTPUTS	-----	SuM T W ThF S

* Settings

- Blank - FREE - Phase Bank 1, Max 1
- Blank - Plan - Phase Bank 1, Max 2
- 1 - Phase Bank 2, Max 1
- 2 - Phase Bank 2, Max 2
- 3 - Phase Bank 3, Max 1
- 4 - Phase Bank 3, Max 2
- 5 - EXTERNAL PERMIT 1
- 6 - EXTERNAL PERMIT 2
- 7 - X-PED OMIT
- 8 - TBA

No Calendar Defined/Enabled

TOD Schedule Report
for 5039: NW 12 Av&NW 39 St

Print Date:
11/20/2017

Print Time:
8:03 PM

Asset	Intersection	TOD Schedule	Op Mode	Plan #	Cycle	Offset	TOD Setting	Active PhaseBank	Active Maximum
5039	NW 12 Av&NW 39 St	DOW-2		N/A	0	0	N/A	0	Max 0

Splits

PH 1	PH 2	PH 3	PH 4	PH 5	PH 6	PH 7	PH 8
-	SBT	-	-	SBL	NBT	-	EBT
0	0	0	0	0	0	0	0



Active Phase Bank: Phase Bank 1

Phase	Walk			Don't Walk			Min Initial			Veh Ext			Max Limit			Max 2			Yellow	Red
	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3		
1 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2 SBT	7	0	7	10	0	10	7	18	7	1	1	1	25	25	25	0	0	30	4	2
3 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5 SBL	0	0	0	0	0	0	5	5	5	2	2	2	6	6	6	10	10	10	4	2
6 NBT	7	0	7	10	0	10	7	18	7	1	1	1	25	25	25	0	0	30	4	2
7 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8 EBT	7	0	7	16	0	16	7	7	7	2.5	-2.5	-2.5	15	15	15	35	30	35	4	2

Last In Service Date: unknown

Permitted Phases	
	12345678
Default	-23-56-8
External Permit 0	-----
External Permit 1	-23--6-8
External Permit 2	-23--6-8

Current TOD Schedule	Plan	Cycle	Green Time								Ring Offset	Offset
			1	2	3	4	5	6	7	8		
	1	80	0	29	0	0	5	18	0	39	0	58
	4	90	0	55	0	0	9	40	0	23	0	70
	10	60	0	30	0	0	5	19	0	18	0	50

Local TOD Schedule		
Time	Plan	DOW
0000	Flash	Su M T W Th F S
0500	Free	M T W Th F
0600	10	Su M T W Th F S
0730	1	M T W Th F
0900	4	M T W Th F
1330	1	M T W Th F
1530	4	M T W Th F
1800	10	M T W Th F

TOD Schedule Report
for 5039: NW 12 Av&NW 39 St

Print Date:
11/20/2017

Print Time:
8:03 PM

Current Time of Day Function			
<u>Time</u>	<u>Function</u>	<u>Settings *</u>	<u>Day of Week</u>
0000	TOD OUTPUTS	-7-----	SuM T W ThF S
0730	TOD OUTPUTS	-----1	M T W ThF
0900	TOD OUTPUTS	-7-----	M T W ThF
1330	TOD OUTPUTS	-----1	M T W ThF
1530	TOD OUTPUTS	-7-----	M T W ThF

Local Time of Day Function			
<u>Time</u>	<u>Function</u>	<u>Settings *</u>	<u>Day of Week</u>
0000	TOD OUTPUTS	-7-----	SuM T W ThF S
0730	TOD OUTPUTS	-----1	M T W ThF
0900	TOD OUTPUTS	-7-----	M T W ThF
1330	TOD OUTPUTS	-----1	M T W ThF
1530	TOD OUTPUTS	-7-----	M T W ThF

* Settings
Blank - FREE - Phase Bank 1, Max 1
Blank - Plan - Phase Bank 1, Max 2
1 - Phase Bank 2, Max 1
2 - Phase Bank 2, Max 2
3 - Phase Bank 3, Max 1
4 - Phase Bank 3, Max 2
5 - EXTERNAL PERMIT 1
6 - EXTERNAL PERMIT 2
7 - X-PED OMIT
8 - TBA

No Calendar Defined/Enabled

TOD Schedule Report
for 5041: NW 12 Av&NW 40 St

Print Date:
11/20/2017

Print Time:
8:04 PM

<u>Asset</u>	<u>Intersection</u>	<u>TOD Schedule</u>	<u>Op Mode</u>	<u>Plan #</u>	<u>Cycle</u>	<u>Offset</u>	<u>TOD Setting</u>	<u>Active PhaseBank</u>	<u>Active Maximum</u>
5041	NW 12 Av&NW 40 St	DOW-2		N/A	0	0	N/A	0	Max 0

Splits

<u>PH 1</u>	<u>PH 2</u>	<u>PH 3</u>	<u>PH 4</u>	<u>PH 5</u>	<u>PH 6</u>	<u>PH 7</u>	<u>PH 8</u>
NBL	SBT	-	WBT	-	NBT	-	-
0	0	0	0	0	0	0	0

Active Phase Bank: Phase Bank 1

<u>Phase</u>	<u>Walk</u>			<u>Don't Walk</u>			<u>Min Initial</u>			<u>Veh Ext</u>			<u>Max Limit</u>			<u>Max 2</u>			<u>Yellow</u>	<u>Red</u>
	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3		
1 NBL	0	0	0	0	0	0	5	5	5	2	2	2	10	10	10	15	15	15	4	2
2 SBT	7	0	7	7	0	7	7	16	7	1	1	1	40	40	40	0	40	40	4	2
3 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4 WBT	7	0	7	14	0	14	7	7	7	2.5	-2.5	-2.5	18	18	18	40	40	40	4	2
5 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
6 NBT	7	0	7	7	0	7	7	16	7	1	1	1	40	40	40	0	40	40	4	2
7 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Last In Service Date: unknown

Permitted Phases

	12345678
Default	1234-6--
External Permit 0	-----
External Permit 1	-234-6--
External Permit 2	-234-6--

<u>Current TOD Schedule</u>	<u>Plan</u>	<u>Cycle</u>	<u>Green Time</u>								<u>Ring Offset</u>	<u>Offset</u>
			1 NBL	2 SBT	3 -	4 WBT	5 -	6 NBT	7 -	8 -		
	1	80	5	20	0	37	0	31	0	0	0	50
	4	90	9	38	0	25	0	53	0	0	0	72
	10	60	5	16	0	21	0	27	0	0	0	50

Local TOD Schedule

<u>Time</u>	<u>Plan</u>	<u>DOW</u>
0000	Flash	Su M T W Th F S
0500	Free	M T W Th F
0600	10	Su M T W Th F S
0730	1	M T W Th F
0900	4	M T W Th F
1330	1	M T W Th F
1530	4	M T W Th F
1800	10	M T W Th F

TOD Schedule Report
for 5041: NW 12 Av&NW 40 St

Print Date:
11/20/2017

Print Time:
8:04 PM

Current Time of Day Function			
<u>Time</u>	<u>Function</u>	<u>Settings *</u>	<u>Day of Week</u>
0000	TOD OUTPUTS	-7-----	SuM T W ThF S
0730	TOD OUTPUTS	-----2-	M T W ThF
0900	TOD OUTPUTS	-7-----	M T W ThF
1330	TOD OUTPUTS	-----2-	M T W ThF
1530	TOD OUTPUTS	-7-----	M T W ThF

Local Time of Day Function			
<u>Time</u>	<u>Function</u>	<u>Settings *</u>	<u>Day of Week</u>
0000	TOD OUTPUTS	-7-----	SuM T W ThF S
0730	TOD OUTPUTS	-----2-	M T W ThF
0900	TOD OUTPUTS	-7-----	M T W ThF
1330	TOD OUTPUTS	-----2-	M T W ThF
1530	TOD OUTPUTS	-7-----	M T W ThF

* Settings
Blank - FREE - Phase Bank 1, Max 1
Blank - Plan - Phase Bank 1, Max 2
1 - Phase Bank 2, Max 1
2 - Phase Bank 2, Max 2
3 - Phase Bank 3, Max 1
4 - Phase Bank 3, Max 2
5 - EXTERNAL PERMIT 1
6 - EXTERNAL PERMIT 2
7 - X-PED OMIT
8 - TBA

No Calendar Defined/Enabled

TOD Schedule Report

for 6336: I- 195 EB Off&N Miami Av

Print Date:
11/20/2017

Print Time:
9:43 PM

<u>Asset</u>	<u>Intersection</u>	<u>TOD Schedule</u>	<u>Op Mode</u>	<u>Plan #</u>	<u>Cycle</u>	<u>Offset</u>	<u>TOD Setting</u>	<u>Active PhaseBank</u>	<u>Active Maximum</u>
6336	I- 195 EB Off&N Miami Av	DOW-2		N/A	0	0	N/A	0	Max 0

Splits

<u>PH 1</u>	<u>PH 2</u>	<u>PH 3</u>	<u>PH 4</u>	<u>PH 5</u>	<u>PH 6</u>	<u>PH 7</u>	<u>PH 8</u>
-	SBT	-	-	-	NBT	-	EBT
0	0	0	0	0	0	0	0



Active Phase Bank: Phase Bank 1

<u>Phase</u>	<u>Phase Bank</u>																			
	<u>Walk</u>			<u>Don't Walk</u>			<u>Min Initial</u>			<u>Veh Ext</u>			<u>Max Limit</u>			<u>Max 2</u>			<u>Yellow</u>	<u>Red</u>
	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3		
1 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2 SBT	7	7	7	12	12	12	12	12	12	1	1	1	25	25	25	0	27	27	4	2
3 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4 -	0	0	0	0	0	0	0	7	0	0	0	0	0	0	0	0	0	0	0	0
5 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
6 NBT	7	7	7	12	12	12	12	12	12	1	1	1	25	25	25	0	27	27	4	2
7 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8 EBT	0	0	0	0	0	0	7	7	7	3.5	-2.5	-2.5	20	20	20	60	40	40	4.4	2

Last In Service Date: unknown

Permitted Phases

	12345678
Default	-2---6-8
External Permit 0	-----
External Permit 1	-----
External Permit 2	-----

Green Time

<u>Current TOD Schedule</u>	<u>Plan</u>	<u>Cycle</u>	1	2	3	4	5	6	7	8	<u>Ring Offset</u>	<u>Offset</u>
			-	SBT	-	-	-	NBT	-	EBT		
	3	180	0	114	0	0	0	114	0	54	0	84
	5	120	0	70	0	0	0	70	0	38	0	64
	9	130	0	75	0	0	0	75	0	43	0	29
	12	150	0	89	0	0	0	89	0	49	0	128
	13	110	0	69	0	0	0	69	0	29	0	6
	14	90	0	64	0	0	0	64	0	14	0	60
	19	90	0	54	0	0	0	54	0	24	0	78
	20	110	0	62	0	0	0	62	0	36	0	56
	21	120	0	68	0	0	0	68	0	40	0	52
	22	100	0	64	0	0	0	64	0	24	0	52

Local TOD Schedule

<u>Time</u>	<u>Plan</u>	<u>DOW</u>
0000	Free	Su M T W Th F S
0545	3	M T W Th F
0600	19	Su
1000	20	Su
1000	5	M T W Th F
1200	9	M T W Th F
1300	21	Su
1500	12	M T W Th F
1800	22	Su
1900	13	M T W Th F S
2200	14	M T W Th F
2200	Free	Su

TOD Schedule Report
for 6336: I- 195 EB Off&N Miami Av

Print Date:
11/20/2017

Print Time:
9:43 PM

Current Time of Day Function				Local Time of Day Function				* Settings
<u>Time</u>	<u>Function</u>	<u>Settings *</u>	<u>Day of Week</u>	<u>Time</u>	<u>Function</u>	<u>Settings *</u>	<u>Day of Week</u>	
0000	TOD OUTPUTS	-----	SuM T W ThF S	0000	TOD OUTPUTS	-----	SuM T W ThF S	Blank - FREE - Phase Bank 1, Max 1
								Blank - Plan - Phase Bank 1, Max 2
								1 - Phase Bank 2, Max 1
								2 - Phase Bank 2, Max 2
								3 - Phase Bank 3, Max 1
								4 - Phase Bank 3, Max 2
								5 - EXTERNAL PERMIT 1
								6 - EXTERNAL PERMIT 2
								7 - X-PED OMIT
								8 - TBA

No Calendar Defined/Enabled

TOD Schedule Report

for 6337: I- 195 WB On&N Miami Av&NE 38 St


Print Date:
11/20/2017

Print Time:
9:43 PM

<u>Asset</u>	<u>Intersection</u>	<u>TOD Schedule</u>	<u>Op Mode</u>	<u>Plan #</u>	<u>Cycle</u>	<u>Offset</u>	<u>TOD Setting</u>	<u>Active PhaseBank</u>	<u>Active Maximum</u>
6337	I- 195 WB On&N Miami Av&NE 38 St	DOW-2		N/A	0	0	N/A	0	Max 0

Splits

<u>PH 1</u>	<u>PH 2</u>	<u>PH 3</u>	<u>PH 4</u>	<u>PH 5</u>	<u>PH 6</u>	<u>PH 7</u>	<u>PH 8</u>
NBL	SBT	-	WBT	-	NBT	-	-
0	0	0	0	0	0	0	0



Active Phase Bank: Phase Bank 1

<u>Phase</u>	<u>Walk</u>	<u>Don't Walk</u>			<u>Min Initial</u>			<u>Veh Ext</u>			<u>Max Limit</u>			<u>Max 2</u>			<u>Yellow</u>	<u>Red</u>
		<u>Phase Bank</u>																
		1	2	3	1	2	3	1	2	3	1	2	3	1	2	3		
1 NBL	0 - 0 - 0	0 - 0 - 0	5 - 5 - 5	3 - 2 - 2	10 - 10 - 10	70 - 17 - 17	4	2										
2 SBT	0 - 0 - 0	0 - 0 - 0	12 - 12 - 12	1 - 1 - 1	25 - 25 - 25	0 - 25 - 25	4	2										
3 -	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0	0										
4 WBT	0 - 0 - 0	0 - 0 - 0	10 - 7 - 7	2.5 - 2.5 - 2.5	10 - 10 - 10	40 - 18 - 18	4	2										
5 -	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0	0										
6 NBT	0 - 0 - 0	0 - 0 - 0	12 - 12 - 12	1 - 1 - 1	25 - 25 - 25	0 - 25 - 25	4	2										
7 -	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0	0										
8 -	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0 - 0 - 0	0	0										

Last In Service Date: unknown

<u>Permitted Phases</u>	
	12345678
Default	12-4-6--
External Permit 0	-----
External Permit 1	-----
External Permit 2	-----

<u>Current TOD Schedule</u>	<u>Plan</u>	<u>Cycle</u>	<u>Green Time</u>								<u>Ring Offset</u>	<u>Offset</u>
			1 NBL	2 SBT	3 -	4 WBT	5 -	6 NBT	7 -	8 -		
	3	180	41	87	0	34	0	134	0	0	0	84
	5	120	26	47	0	29	0	79	0	0	0	54
	9	130	24	52	0	36	0	82	0	0	0	35
	12	150	49	44	0	39	0	99	0	0	0	128
	13	110	24	49	0	19	0	79	0	0	0	6
	14	90	21	39	0	12	0	66	0	0	0	60
	19	90	24	30	0	18	0	60	0	0	0	78
	20	110	26	40	0	26	0	72	0	0	0	56
	21	120	30	42	0	30	0	78	0	0	0	52
	22	100	20	42	0	20	0	68	0	0	0	52

<u>Local TOD Schedule</u>			
<u>Time</u>	<u>Plan</u>	<u>DOW</u>	
0000	Free	Su M T W Th F S	
0600	19	Su	S
0600	3	M T W Th F	
1000	5	M T W Th F	
1000	20	Su	S
1200	9	M T W Th F	
1300	21	Su	S
1500	12	M T W Th F	
1800	22	Su	S
1900	13	M T W Th F S	
2200	14	M T W Th F	
2200	Free	Su	S

TOD Schedule Report

for 6337: I- 195 WB On&N Miami Av&NE 38 St

Print Date:
11/20/2017

Print Time:
9:43 PM

Current Time of Day Function

<u>Time</u>	<u>Function</u>	<u>Settings *</u>	<u>Day of Week</u>
0000	TOD OUTPUTS	-----	SuM T W ThF S

Local Time of Day Function

<u>Time</u>	<u>Function</u>	<u>Settings *</u>	<u>Day of Week</u>
0000	TOD OUTPUTS	-----	SuM T W ThF S

*** Settings**

- Blank - FREE - Phase Bank 1, Max 1
- Blank - Plan - Phase Bank 1, Max 2
- 1 - Phase Bank 2, Max 1
- 2 - Phase Bank 2, Max 2
- 3 - Phase Bank 3, Max 1
- 4 - Phase Bank 3, Max 2
- 5 - EXTERNAL PERMIT 1
- 6 - EXTERNAL PERMIT 2
- 7 - X-PED OMIT
- 8 - TBA

No Calendar Defined/Enabled

TOD Schedule Report

for 2649: Alton Rd&Chase Av&N Bay Rd

Print Date:
11/20/2017

Print Time:
3:16 PM

Asset	Intersection	TOD Schedule	Op Mode	Plan #	Cycle	Offset	TOD Setting	Active PhaseBank	Active Maximum
2649	Alton Rd&Chase Av&N Bay Rd	DOW-2		N/A	0	0	N/A	0	Max 0

Splits

PH 1	PH 2	PH 3	PH 4	PH 5	PH 6	PH 7	PH 8
-	NBT	-	EBT	-	SBT	-	WBT
0	0	0	0	0	0	0	0

Active Phase Bank: Phase Bank 1

Phase	Phase Bank																			
	Walk			Don't Walk			Min Initial			Veh Ext			Max Limit			Max 2			Yellow	Red
	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3		
1 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2 NBT	7	7	7	20	20	20	7	7	7	1	1	1	35	35	35	0	35	35	4	2.4
3 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4 EBT	0	0	0	0	0	0	7	7	7	2.5	-2.5	-2.5	12	12	15	12	12	25	4	2.8
5 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
6 SBT	7	7	7	20	20	20	7	7	7	1	1	1	35	35	35	0	35	35	4	2.4
7 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8 WBT	4	4	4	29	29	29	7	7	7	2.5	-2.5	-2.5	12	12	15	12	12	25	4	2.8

Last In Service Date: unknown

Permitted Phases	
	12345678
Default	-2-4-6-8
External Permit 0	-2-4-6-8
External Permit 1	-2-4-6-8
External Permit 2	-2-4-6-8

Current TOD Schedule	Plan	Cycle	Green Time								Ring Offset	Offset
			1	2	3	4	5	6	7	8		
			-	NBT	-	EBT	-	SBT	-	WBT		
	1	90	0	43	0	34	0	43	0	34	0	0
	2	90	0	43	0	34	0	43	0	34	0	0
	3	100	0	53	0	34	0	53	0	34	0	0
	4	90	0	43	0	34	0	43	0	34	0	0
	5	90	0	43	0	34	0	43	0	34	0	0
	6	90	0	43	0	34	0	43	0	34	0	0
	9	120	0	73	0	34	0	73	0	34	0	0
	10	90	0	43	0	34	0	43	0	34	0	0
	21	90	0	43	0	34	0	43	0	34	0	0
	26	180	0	133	0	34	0	133	0	34	0	0

Local TOD Schedule		
Time	Plan	DOW
0000	Free	Su M T W Th F S
0500	2	M T W Th F
0545	3	M T W Th F
0600	2	Su
0630	9	M T W Th F
0800	9	Su
1900	2	Su M T W Th F S

TOD Schedule Report

for 2649: Alton Rd&Chase Av&N Bay Rd

Print Date:
11/20/2017

Print Time:
3:16 PM

Current Time of Day Function			
Time	Function	Settings *	Day of Week
0000	TOD LOCAL MULTIFU	----4---	SuM T W ThF S
0000	TOD OUTPUTS	-----	SuM T W ThF S
0500	TOD LOCAL MULTIFU	-----	SuM T W ThF S

Local Time of Day Function			
Time	Function	Settings *	Day of Week
0000	TOD LOCAL MULTIFUNCT	----4---	SuM T W ThF S
0000	TOD OUTPUTS	-----	SuM T W ThF S
0000	TOD OUTPUTS	-----	S
0500	TOD LOCAL MULTIFUNCT	-----	SuM T W ThF S
0815	TOD OUTPUTS	----3--	S
0815	PED RECALL	8-----	S
0830	PED RECALL	-----	S
0830	TOD OUTPUTS	-----	S
0915	TOD OUTPUTS	----3--	S
0915	PED RECALL	8-----	S
0930	PED RECALL	-----	S
0930	TOD OUTPUTS	-----	S
1120	TOD OUTPUTS	----3--	S
1120	PED RECALL	8-----	S
1135	PED RECALL	-----	S
1135	TOD OUTPUTS	-----	S

* Settings
Blank - FREE - Phase Bank 1, Max 1
Blank - Plan - Phase Bank 1, Max 2
1 - Phase Bank 2, Max 1
2 - Phase Bank 2, Max 2
3 - Phase Bank 3, Max 1
4 - Phase Bank 3, Max 2
5 - EXTERNAL PERMIT 1
6 - EXTERNAL PERMIT 2
7 - X-PED OMIT
8 - TBA

No Calendar Defined/Enabled









TOD Schedule Report
for 2650: Alton Rd&Art Godfrey Rd

Print Date:
1/21/2018

Print Time:
2:00 AM

<u>Asset</u>	<u>Intersection</u>	<u>TOD Schedule</u>	<u>Op Mode</u>	<u>Plan #</u>	<u>Cycle</u>	<u>Offset</u>	<u>TOD Setting</u>	<u>Active PhaseBank</u>	<u>Active Maximum</u>
2650	Alton Rd&Art Godfrey Rd	DOW-1		N/A	0	0	N/A	0	Max 0

Splits

<u>PH 1</u>	<u>PH 2</u>	<u>PH 3</u>	<u>PH 4</u>	<u>PH 5</u>	<u>PH 6</u>	<u>PH 7</u>	<u>PH 8</u>
EBL	WBT	SBL	NBT	WBL	EBT	NBL	SBT
0	0	0	0	0	0	0	0
							

Active Phase Bank: Phase Bank 1

Phase	<u>Walk</u>			<u>Don't Walk</u>			<u>Min Initial</u>			<u>Veh Ext</u>			<u>Max Limit</u>			<u>Max 2</u>			<u>Yellow</u>	<u>Red</u>
	Phase Bank																			
	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3		
1 EBL	0	0	0	0	0	0	5	5	5	2	2	2	6	6	10	22	15	15	3.7	2
2 WBT	5	5	5	14	14	14	7	7	7	1	1	1	30	32	40	0	26	0	4	2.3
3 SBL	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4 NBT	4	4	4	26	26	26	7	7	7	2.5	2.5	2.5	15	16	20	60	16	16	4	2.3
5 WBL	0	0	0	0	0	0	5	5	5	2	2	2	6	6	10	20	6	10	3.7	2
6 EBT	5	5	5	14	14	14	7	7	7	1	1	1	30	32	40	0	26	0	4	2.3
7 NBL	0	0	0	0	0	0	5	5	5	2	2	2	6	6	10	10	6	5	3.7	2
8 SBT	4	4	4	26	26	26	7	7	7	2.5	2.5	2.5	15	16	20	60	16	16	4	2.3

Last In Service Date: unknown

Permitted Phases	
	12345678
Default	12-45678
External Permit 0	12-45678
External Permit 1	-2-4-6-8
External Permit 2	-2-4-6-8

TOD Schedule Report

for 2650: Alton Rd&Art Godfrey Rd

Print Date:
1/21/2018

Print Time:
2:00 AM

Current TOD Schedule	Plan	Cycle	Green Time								Ring Offset	Offset
			1 EBL	2 WBT	3 SBL	4 NBT	5 WBL	6 EBT	7 NBL	8 SBT		
1		180	21	99	0	41	7	113	6	30	0	109
2		100	7	38	0	37	8	37	6	25	0	43
3		120	6	54	0	40	7	53	6	30	0	110
4		100	6	34	0	41	7	33	6	30	0	62
5		120	11	54	0	36	7	58	6	25	0	86
6		140	9	68	0	45	19	58	9	30	0	103
7		120	6	59	0	36	7	58	6	25	0	40
8		120	7	52	0	43	7	52	7	30	0	65
9		140	9	68	0	45	19	58	9	30	0	103
10		160	21	79	0	41	7	93	6	30	0	129
11		100	6	34	0	41	7	33	6	30	0	62
13		140	9	70	0	43	9	70	7	30	0	83
18		160	14	83	0	45	14	83	9	30	0	90
19		160	6	93	0	43	6	93	7	30	0	118
23		160	9	74	0	59	9	74	8	45	0	118
24		160	7	82	0	53	7	82	8	39	0	118
25		180	6	114	0	41	27	93	6	30	0	129

Local TOD Schedule		
Time	Plan	DOW
0000	Free	M T W Th F
0000	10	Su S
0200	Free	Su S
0600	13	M T W Th F
0700	4	Su S
0715	9	M T W Th F
0800	2	Su S
0900	10	M T W Th F
1000	10	Su S
1145	10	M T W Th F
1230	10	S
1230	10	Su
1345	10	M T W Th F
1430	10	W
1600	19	M T Th F
1800	10	Su S
1930	2	M T W Th F
2100	10	Su S
2300	Free	M T W Th F

Current Time of Day Function			
Time	Function	Settings *	Day of Week
0000	TOD OUTPUTS	-----1	SuM T W ThF S
0000	TOD LOCAL MULTIFU	----4--	SuM T W ThF S
0200	TOD OUTPUTS	-----2-	Su S
0500	TOD LOCAL MULTIFU	-----	SuM T W ThF S
0700	TOD OUTPUTS	-----	Su S
0800	TOD OUTPUTS	----4--	Su S
1800	TOD OUTPUTS	-----	Su S

Local Time of Day Function			
Time	Function	Settings *	Day of Week
0000	TOD OUTPUTS	-----1	SuM T W ThF S
0000	TOD LOCAL MULTIFUNCT	----4--	SuM T W ThF S
0100	TOD OUTPUTS	-----2-	M T W ThF
0200	TOD OUTPUTS	-----2-	Su S
0500	TOD LOCAL MULTIFUNCT	-----	SuM T W ThF S
0630	TOD OUTPUTS	-----	M T W ThF
0700	TOD OUTPUTS	-----	Su S
0800	TOD OUTPUTS	----4--	Su S
1800	TOD OUTPUTS	-----	Su S
2330	TOD OUTPUTS	-----1	M T W Th

* Settings
Blank - FREE - Phase Bank 1, Max 1
Blank - Plan - Phase Bank 1, Max 2
1 - Phase Bank 2, Max 1
2 - Phase Bank 2, Max 2
3 - Phase Bank 3, Max 1
4 - Phase Bank 3, Max 2
5 - EXTERNAL PERMIT 1
6 - EXTERNAL PERMIT 2
7 - X-PED OMIT
8 - TBA

No Calendar Defined/Enabled

TOD Schedule Report

for 2651: Alton Rd&Sullivan Dr&43 St

Print Date:
12/6/2017

Print Time:
2:01 AM

<u>Asset</u>	<u>Intersection</u>	<u>TOD Schedule</u>	<u>Op Mode</u>	<u>Plan #</u>	<u>Cycle</u>	<u>Offset</u>	<u>TOD Setting</u>	<u>Active PhaseBank</u>	<u>Active Maximum</u>
2651	Alton Rd&Sullivan Dr&43 St	DOW-4		N/A	0	0	N/A	0	Max 0

Splits

<u>PH 1</u>	<u>PH 2</u>	<u>PH 3</u>	<u>PH 4</u>	<u>PH 5</u>	<u>PH 6</u>	<u>PH 7</u>	<u>PH 8</u>
SBL	NBT	-	-	NBL	SBT	EBT	WBT
0	0	0	0	0	0	0	0



Active Phase Bank: Phase Bank 1

Phase	Walk			Don't Walk			Min Initial			Veh Ext			Max Limit			Max 2			Yellow	Red
	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3	1	2	3		
1 SBL	0	0	0	0	0	0	5	5	5	2	2	2	5	12	8	13	12	12	3	0
2 NBT	7	7	7	21	21	21	7	7	7	1	1	1	30	80	48	30	80	46	4	3.3
3 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4 -	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5 NBL	0	0	0	0	0	0	5	5	5	5	5	5	10	35	37	37	37	37	3.7	2
6 SBT	7	7	7	21	21	21	7	7	7	1	1	1	30	80	48	30	80	46	4	3.3
7 EBT	0	0	0	0	0	0	7	12	12	2.5	2.5	2.5	7	20	25	25	20	35	4	2.3
8 WBT	7	7	7	21	21	21	7	7	7	3.5	3.5	3.5	7	20	25	25	25	35	4	2.3

Last In Service Date: unknown

Permitted Phases

	12345678
Default	12--5678
External Permit 0	-2--5678
External Permit 1	-2--5678
External Permit 2	-2--5678

Green Time

Current TOD Schedule	Plan	Cycle	Green Time								Ring Offset	Offset
			1 SBL	2 NBT	3 -	4 -	5 NBL	6 SBT	7 EBT	8 WBT		
	5	150	12	98	0	0	24	83	8	10	0	0
	6	115	12	63	0	0	18	54	8	10	0	0
	10	155	11	104	0	0	24	88	8	10	0	0

Local TOD Schedule

Time	Plan	DOW
0000	Free	Su M T W Th F S

TOD Schedule Report

for 2651: Alton Rd&Sullivan Dr&43 St

Print Date:
12/6/2017

Print Time:
2:01 AM

Current Time of Day Function			
Time	Function	Settings *	Day of Week
0000	TOD OUTPUTS	-----	SuM T W ThF S
0000	TOD LOCAL MULTIFU	----4--	SuM T W ThF S
0500	TOD LOCAL MULTIFU	-----	SuM T W ThF S
0545	TOD OUTPUTS	----3--	M T W ThF
0730	TOD OUTPUTS	----4--	M T W ThF
0800	TOD OUTPUTS	-----2-	M T W ThF
1000	TOD OUTPUTS	-----1	M T W ThF
1145	TOD OUTPUTS	----3--	M T W ThF
1600	TOD OUTPUTS	----4--	SuM T W ThF S
1800	TOD OUTPUTS	----4--	SuM T W ThF S
1900	TOD OUTPUTS	-----	SuM T W ThF S

Local Time of Day Function			
Time	Function	Settings *	Day of Week
0000	TOD OUTPUTS	-----	SuM T W ThF S
0000	TOD LOCAL MULTIFUNCT	----4--	SuM T W ThF S
0500	TOD LOCAL MULTIFUNCT	-----	SuM T W ThF S
0545	TOD OUTPUTS	----3--	M T W ThF
0630	TOD OUTPUTS	-----1	Su S
0730	TOD OUTPUTS	----4--	M T W ThF
0800	TOD OUTPUTS	-----2-	M T W ThF
0800	TOD OUTPUTS	----3--	Su S
0930	TOD OUTPUTS	-----	Su S
1000	TOD OUTPUTS	-----1	M T W ThF
1145	TOD OUTPUTS	----3--	M T W ThF
1600	TOD OUTPUTS	----4--	SuM T W ThF S
1800	TOD OUTPUTS	----4--	SuM T W ThF S
1900	TOD OUTPUTS	-----	SuM T W ThF S

* Settings
Blank - FREE - Phase Bank 1, Max 1
Blank - Plan - Phase Bank 1, Max 2
1 - Phase Bank 2, Max 1
2 - Phase Bank 2, Max 2
3 - Phase Bank 3, Max 1
4 - Phase Bank 3, Max 2
5 - EXTERNAL PERMIT 1
6 - EXTERNAL PERMIT 2
7 - X-PED OMIT
8 - TBA

No Calendar Defined/Enabled

APPENDIX E – SAFETY ANALYSIS SUPPORTING INFORMATION

- STUDY LIMITS CRASH DATA (2011-2015)
- SEGMENTS CRASH DATA (2011-2015)
 - RAMP CRASH DATA (2011-2015)
 - CLUSTER CRASH DATA (2011-2015)

STUDY LIMITS CRASH DATA (2011-2015)

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2011 TO 12/ 2011 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	3.871	07/04/11	Mon	0442	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
2	3.905	06/20/11	Mon	1545	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
3	3.934	06/26/11	Sun	2115	Rear-End	0	2	0	Night	Wet	Careless or Negligent Manner
4	3.961	03/27/11	Sun	0503	Rear-End	0	0	1	Night	Dry	#N/A
5	4.017	01/30/11	Sun	0920	Rear-End	0	1	0	Day	Dry	No Contributing Action
6	4.017	09/22/11	Thu	1805	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
7	4.036	08/24/11	Wed	0830	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
8	0.129	08/19/11	Fri	1335	Guardrail Face	0	0	1	Day	Wet	#N/A
9	0.228	08/17/11	Wed	1500	Sideswipe	0	1	0	Day	Dry	#N/A
10	0.378	08/22/11	Mon	1200	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
11	0.427	01/31/11	Mon	1100	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
12	0.427	01/31/11	Mon	1100	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
13	0.440	08/22/11	Mon	1108	Other Non-Fixed Object	0	1	0	Day	Dry	Careless or Negligent Manner
14	0.440	12/03/11	Sat	0547	Other Non-Fixed Object	0	1	0	Night	Dry	Careless or Negligent Manner
15	0.450	05/13/11	Fri	1740	Rear-End	0	0	1	Day	Dry	Failed To Keep In Proper Lane
16	0.459	10/31/11	Mon	0400	Concrete Traffic Barrier	0	0	1	Night	Wet	Swerved Or Avoided
17	0.459	04/13/11	Wed	0815	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
18	0.498	09/11/11	Sun	0250	Impact Attenuator/Crash Cushion	0	0	1	Night	Dry	Careless or Negligent Manner
19	0.517	04/12/11	Tue	2135	Rear-End	0	4	0	Night	Dry	Careless or Negligent Manner
20	0.517	05/11/11	Wed	1610	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
21	0.527	05/11/11	Wed	1547	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
22	0.536	05/20/11	Fri	1127	Sideswipe	0	0	1	Day	Dry	No Contributing Action
23	0.546	03/04/11	Fri	1434	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
24	0.546	07/04/11	Mon	1828	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
25	0.546	07/25/11	Mon	1415	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
26	0.546	07/25/11	Mon	1505	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
27	0.584	05/21/11	Sat	1415	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
28	0.584	05/20/11	Fri	1122	Angle	0	0	1	Day	Dry	No Contributing Action
29	0.589	06/18/11	Sat	0430	Rear-End	0	1	0	Night	Dry	#N/A
30	0.589	09/16/11	Fri	0841	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
31	0.631	08/24/11	Wed	1830	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
32	0.689	06/01/11	Wed	0819	Rear-End	0	0	1	Day	Dry	No Contributing Action
33	0.728	07/06/11	Wed	1740	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
34	0.739	02/23/11	Wed	0845	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
35	0.739	04/17/11	Sun	1810	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
36	0.739	04/21/11	Thu	1820	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
37	0.739	05/19/11	Thu	1600	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
38	0.739	06/02/11	Thu	0850	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
39	0.739	06/18/11	Sat	2026	Rear-End	0	1	0	Night	Wet	#N/A
40	0.739	07/28/11	Thu	1737	Rear-End	0	0	1	Day	Dry	#N/A
41	0.778	09/06/11	Tue	0830	Rear-End	0	1	0	Day	Dry	No Contributing Action
42	0.778	09/06/11	Tue	0928	Rear-End	0	4	0	Day	Dry	No Contributing Action
43	0.778	09/24/11	Sat	1240	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
44	0.778	11/22/11	Tue	1225	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
45	0.778	09/19/11	Mon	1000	Rear-End	0	4	0	Day	Dry	Careless or Negligent Manner
46	0.778	09/20/11	Tue	1250	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
47	0.778	09/20/11	Tue	1343	Rear-End	0	4	0	Day	Dry	Careless or Negligent Manner
48	0.785	01/12/11	Wed	0140	Sideswipe	0	0	1	Night	Dry	No Contributing Action
49	0.787	02/13/11	Sun	1700	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
50	0.787	06/02/11	Thu	0927	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
51	0.787	08/17/11	Wed	0904	Rear-End	0	0	1	Day	Dry	#N/A
52	0.949	07/20/11	Wed	2100	Concrete Traffic Barrier	0	0	1	Night	Dry	#N/A
53	0.978	11/02/11	Wed	0955	Rear-End	0	1	0	Day	Dry	Other Contributing Action
54	0.978	11/02/11	Wed	1040	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
55	0.989	09/18/11	Sun	0136	Concrete Traffic Barrier	0	1	0	Night	Dry	Careless or Negligent Manner
56	0.993	06/16/11	Thu	1545	Rear-End	0	2	0	Day	Dry	#N/A
57	1.028	08/16/11	Tue	1210	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
58	1.028	08/12/11	Fri	1751	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
59	1.028	12/09/11	Fri	1212	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
60	1.028	11/17/11	Thu	1905	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
61	1.028	09/20/11	Tue	0929	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
62	1.028	11/28/11	Mon	1937	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
63	1.028	12/05/11	Mon	1718	Rear-End	0	1	0	Night	Dry	No Contributing Action
64	1.038	08/12/11	Fri	1030	Rear-End	0	0	1	Day	Dry	No Contributing Action
65	1.043	03/17/11	Thu	0700	Rear-End	0	0	1	Day	Dry	#N/A
66	1.043	03/17/11	Thu	0930	Sideswipe	0	0	1	Day	Dry	No Contributing Action
67	1.043	03/05/11	Sat	0357	Curb	0	1	0	Night	Dry	Failed To Keep In Proper Lane
68	1.078	12/17/11	Sat	1145	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
69	1.083	10/20/11	Thu	0955	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
70	1.083	10/20/11	Thu	1041	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
71	1.089	11/28/11	Mon	1218	Sideswipe	0	0	1	Day	Dry	No Contributing Action
72	1.100	08/11/11	Thu	0930	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
73	1.100	08/11/11	Thu	1510	Sideswipe	0	0	1	Day	Dry	#N/A
74	1.110	03/02/11	Wed	0920	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
75	1.110	03/29/11	Tue	0934	Sideswipe	0	0	1	Day	Dry	#N/A
76	1.119	07/02/11	Sat	1200	Rear-End	0	0	1	Day	Dry	No Contributing Action
77	1.119	02/18/11	Fri	1914	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
78	1.119	11/16/11	Wed	0715	Rear-End	0	1	0	Day	Dry	Other Contributing Action
79	1.129	04/29/11	Fri	1750	Rear-End	0	1	0	Day	Dry	Followed too Closely
80	1.136	02/22/11	Tue	0825	Rear-End	0	4	0	Day	Dry	Careless or Negligent Manner
81	1.178	12/13/11	Tue	1205	Rear-End	0	4	0	Day	Dry	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2011 TO 12/ 2011 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
82	1.183	04/21/11	Thu	2215	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
83	1.183	09/12/11	Mon	2323	Sideswipe	0	0	1	Night	Dry	No Contributing Action
84	1.183	07/16/11	Sat	0655	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
85	1.183	10/31/11	Mon	1325	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
86	1.183	11/15/11	Tue	1145	Sideswipe	0	2	0	Day	Dry	Other Contributing Action
87	1.187	09/26/11	Mon	1055	Angle	0	0	1	Day	Wet	Careless or Negligent Manner
88	1.187	08/24/11	Wed	1645	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
89	1.196	06/11/11	Sat	0040	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
90	1.196	05/30/11	Mon	0450	Sideswipe	0	6	0	Night	Dry	Failed To Keep In Proper Lane
91	1.206	03/25/11	Fri	1825	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
92	1.206	05/10/11	Tue	1611	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
93	1.206	09/09/11	Fri	0750	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
94	1.221	03/27/11	Sun	1809	Rear-End	0	2	0	Day	Dry	No Contributing Action
95	1.221	05/11/11	Wed	1355	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
96	1.221	11/15/11	Tue	1845	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
97	1.240	07/24/11	Sun	0535	Sideswipe	0	2	0	Night	Wet	Careless or Negligent Manner
98	1.240	08/13/11	Sat	1412	Not Coded	0	2	0	Day	Dry	Careless or Negligent Manner
99	1.240	10/27/11	Thu	1717	Rear-End	0	0	1	Day	Dry	No Contributing Action
100	1.240	11/19/11	Sat	1955	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
101	1.259	09/30/11	Fri	2005	Not Coded	0	1	0	Night	Wet	No Contributing Action
102	1.259	11/24/11	Thu	1410	Impact Attenuator/Crash Cushion	0	1	0	Day	Dry	Careless or Negligent Manner
103	1.268	08/11/11	Thu	1115	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
104	1.269	10/28/11	Fri	1418	Rear-End	0	0	1	Day	Dry	No Contributing Action
105	1.269	11/03/11	Thu	1950	Rear-End	0	3	0	Night	Dry	Careless or Negligent Manner
106	1.272	11/29/11	Tue	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
107	1.278	10/08/11	Sat	1130	Sideswipe	0	0	1	Day	Wet	Careless or Negligent Manner
108	1.358	08/18/11	Thu	1005	Rear-End	0	0	1	Day	Wet	#N/A
109	1.373	09/23/11	Fri	0745	Concrete Traffic Barrier	0	0	1	Day	Dry	#N/A
110	1.418	08/13/11	Sat	1650	Rear-End	0	0	1	Day	Dry	#N/A
111	1.488	08/22/11	Mon	1115	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
112	1.489	09/28/11	Wed	1705	Concrete Traffic Barrier	0	2	0	Day	Dry	Ran Off Roadway
113	1.555	09/25/11	Sun	1539	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
114	1.565	09/06/11	Tue	1755	Rear-End	0	0	1	Day	Dry	Improper Passing
115	1.583	11/11/11	Fri	0434	Guardrail Face	0	1	0	Night	Dry	#N/A
116	1.592	04/29/11	Fri	1602	Concrete Traffic Barrier	0	1	0	Day	Wet	No Contributing Action
117	1.592	10/27/11	Thu	1827	Sideswipe	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
118	1.602	08/27/11	Sat	1555	Rear-End	0	6	0	Day	Dry	Careless or Negligent Manner
119	1.611	05/03/11	Tue	2325	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
120	1.611	03/21/11	Mon	0435	Angle	0	1	0	Day	Dry	Careless or Negligent Manner
121	1.611	03/09/11	Wed	0045	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
122	1.611	11/27/11	Sun	0420	Other Traffic Barrier	0	0	1	Night	Dry	No Contributing Action
123	1.621	06/22/11	Wed	1610	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
124	1.621	05/02/11	Mon	0030	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
125	1.621	05/27/11	Fri	1533	Not Coded	#N/A	0	1	Day	Dry	#N/A
126	1.621	08/05/11	Fri	0012	Other Non-Fixed Object	0	0	1	Night	Dry	Careless or Negligent Manner
127	1.621	11/20/11	Sun	0559	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
128	1.640	08/31/11	Wed	2320	Concrete Traffic Barrier	0	0	1	Night	Wet	No Contributing Action
129	1.678	04/28/11	Thu	1750	Rear-End	0	1	0	Day	Dry	Other Contributing Action
130	1.678	12/01/11	Thu	1813	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
131	1.683	09/03/11	Sat	0225	Concrete Traffic Barrier	0	0	1	Night	Wet	Careless or Negligent Manner
132	1.753	04/17/11	Sun	0545	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
133	1.774	03/17/11	Thu	0635	Concrete Traffic Barrier	0	0	1	Day	Dry	#N/A
134	1.774	06/14/11	Tue	0730	Other Non-Collision	0	1	0	Day	Dry	Other Contributing Action
135	1.783	09/25/11	Sun	1030	Rear-End	1	2	0	Day	Dry	Careless or Negligent Manner
136	1.833	01/24/11	Mon	0930	Rear-End	0	1	0	Day	Dry	Erratic, Reckless or Aggressive
137	1.833	03/14/11	Mon	1745	Sideswipe	0	2	0	Day	Dry	Failed To Keep In Proper Lane
138	1.833	10/01/11	Sat	1810	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
139	1.983	05/06/11	Fri	1057	Rear-End	0	1	0	Day	Wet	#N/A
140	2.046	05/23/11	Mon	1705	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
141	2.046	05/23/11	Mon	1708	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
142	2.083	02/11/11	Fri	2230	Other Non-Fixed Object	0	2	0	Night	Dry	Careless or Negligent Manner
143	2.083	03/24/11	Thu	1429	Not Coded	0	1	0	Day	Dry	Careless or Negligent Manner
144	2.083	01/08/11	Sat	0801	Sideswipe	0	2	0	Day	Dry	Careless or Negligent Manner
145	2.083	03/02/11	Wed	1940	Guardrail End	0	1	0	Night	Dry	Other Contributing Action
146	2.083	03/12/11	Sat	1950	Angle	0	0	1	Night	Dry	Careless or Negligent Manner
147	2.083	07/17/11	Sun	0300	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
148	2.083	07/17/11	Sun	0304	Angle	0	0	1	Night	Dry	Other Contributing Action
149	2.183	05/01/11	Sun	0559	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
150	2.239	03/27/11	Sun	0530	Guardrail Face	0	0	1	Day	Dry	#N/A
151	2.239	03/29/11	Tue	0000	Sideswipe	0	0	1	Day	Dry	#N/A
152	2.383	02/11/11	Fri	2228	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
153	2.546	06/07/11	Tue	0911	Other Non-Fixed Object	0	2	0	Day	Dry	Careless or Negligent Manner
154	2.583	01/09/11	Sun	0115	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
155	2.583	12/21/11	Wed	1540	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
156	2.583	05/31/11	Tue	0425	Rear-End	0	0	1	Night	Wet	#N/A
157	2.583	04/27/11	Wed	1407	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
158	2.583	05/11/11	Wed	1850	Not Coded	0	0	1	Day	Dry	Failed To Keep In Proper Lane
159	2.583	07/08/11	Fri	0300	Guardrail Face	0	0	1	Night	Wet	Careless or Negligent Manner
160	2.583	08/02/11	Tue	1350	Overturn/Rollover	0	1	0	Day	Wet	Careless or Negligent Manner
161	2.833	06/22/11	Wed	1345	Sideswipe	0	1	0	Day	Dry	#N/A
162	3.230	04/24/11	Sun	1900	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2011 TO 12/ 2011 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
163	3.583	02/22/11	Tue	0842	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
164	3.730	03/11/11	Fri	1515	Rear-End	0	0	1	Day	Dry	#N/A
165	3.730	08/14/11	Sun	0130	Rear-End	0	5	0	Night	Dry	Careless or Negligent Manner
166	3.730	09/10/11	Sat	1635	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
167	3.730	11/18/11	Fri	1002	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
168	3.730	10/09/11	Sun	0222	Other Fixed Object	0	1	0	Night	Wet	Careless or Negligent Manner
169	3.730	06/18/11	Sat	0515	Rear-End	0	2	0	Night	Dry	No Contributing Action
170	3.980	07/02/11	Sat	0150	Guardrail Face	0	1	0	Night	Dry	Ran Off Roadway
171	4.087	07/24/11	Sun	2234	Guardrail Face	0	0	1	Night	Dry	Swerved Or Avoided
172	4.230	03/02/11	Wed	0120	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
173	4.230	01/10/11	Mon	1045	Rear-End	0	0	1	Day	Dry	Other Contributing Action
174	4.230	04/20/11	Wed	1433	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
175	4.230	06/07/11	Tue	1110	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
176	4.230	07/05/11	Tue	1355	Sideswipe	0	0	1	Day	Dry	No Contributing Action
177	4.230	06/16/11	Thu	0839	Angle	0	2	0	Day	Dry	Careless or Negligent Manner
178	4.230	07/30/11	Sat	0642	Angle	0	1	0	Day	Dry	#N/A
179	4.230	08/07/11	Sun	1320	Concrete Traffic Barrier	0	1	0	Day	Wet	Careless or Negligent Manner
180	4.230	09/06/11	Tue	2110	Pedalcycle	0	1	0	Night	Dry	Other Contributing Action
181	4.230	12/29/11	Thu	1506	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
182	4.430	01/31/11	Mon	1925	Angle	0	2	0	Night	Dry	Careless or Negligent Manner
183	4.430	02/26/11	Sat	1525	Pedestrian	0	1	0	Day	Dry	Careless or Negligent Manner
184	4.430	06/27/11	Mon	0440	Parked Motor Vehicle	0	0	1	Night	Dry	#N/A
185	4.480	01/24/11	Mon	1045	Rear-End	0	0	1	Day	Dry	Followed too Closely
186	4.480	03/03/11	Thu	1505	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
187	4.480	03/20/11	Sun	1400	Rear-End	0	5	0	Day	Dry	Careless or Negligent Manner
188	4.480	03/13/11	Sun	1443	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
189	4.480	02/03/11	Thu	1655	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
190	4.480	03/13/11	Sun	1520	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
191	4.480	04/10/11	Sun	1650	Struck by Falling/Shifting Cargo	0	1	0	Day	Dry	No Contributing Action
192	4.480	02/25/11	Fri	1545	Sideswipe	0	0	1	Day	Dry	#N/A
193	4.480	04/29/11	Fri	1853	Rear-End	0	2	0	Day	Wet	Followed too Closely
194	4.480	08/21/11	Sun	1427	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
195	4.480	08/14/11	Sun	1605	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
196	4.480	09/23/11	Fri	1556	Rear-End	0	0	1	Day	Dry	#N/A
197	4.530	03/12/11	Sat	1145	Overturn/Rollover	0	1	0	Day	Dry	No Contributing Action
198	4.530	05/23/11	Mon	0713	Sideswipe	0	0	1	Day	Dry	#N/A
199	4.530	06/09/11	Thu	0914	Rear-End	0	1	0	Day	Dry	No Contributing Action
200	4.530	09/22/11	Thu	2028	Angle	0	1	0	Night	Dry	No Contributing Action
201	4.541	10/05/11	Wed	0805	Concrete Traffic Barrier	0	1	0	Day	Dry	Careless or Negligent Manner
202	4.541	07/15/11	Fri	1550	Angle	0	1	0	Day	Dry	Careless or Negligent Manner
203	4.541	08/22/11	Mon	0852	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
204	4.560	03/27/11	Sun	0535	Rear-End	0	1	0	Night	Dry	No Contributing Action
205	4.635	06/16/11	Thu	0832	Rear-End	0	0	1	Day	Dry	No Contributing Action
206	4.635	06/03/11	Fri	0655	Rear-End	0	2	0	Day	Dry	#N/A
207	4.635	06/16/11	Thu	0850	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
208	4.635	09/09/11	Fri	1730	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
209	4.635	01/29/11	Sat	1444	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
210	4.673	02/26/11	Sat	1437	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
211	4.673	03/13/11	Sun	1620	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
212	4.711	03/06/11	Sun	0910	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
213	4.721	01/14/11	Fri	0825	Not Coded	0	0	1	Day	Dry	No Contributing Action
214	4.724	11/11/11	Fri	0617	Not Coded	0	0	1	Day	Dry	No Contributing Action
215	4.726	06/03/11	Fri	1150	Rear-End	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
216	4.904	02/04/11	Fri	0757	Rear-End	0	2	0	Day	Dry	No Contributing Action
217	0.388	07/03/11	Sun	0518	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
218	0.000	07/18/11	Mon	1557	Other Non-Fixed Object	0	1	0	Day	Wet	No Contributing Action
219	0.038	09/21/11	Wed	0910	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
220	0.000	12/01/11	Thu	0450	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
221	0.000	11/29/11	Tue	0123	Not Coded	0	0	1	Night	Dry	#N/A
222	0.000	01/28/11	Fri	1450	Concrete Traffic Barrier	0	0	1	Day	Dry	No Contributing Action
223	0.000	03/25/11	Fri	0215	Other Non-Collision	0	0	1	Night	Dry	No Contributing Action
224	0.000	06/13/11	Mon	1409	Parked Motor Vehicle	0	1	0	Day	Dry	#N/A
225	0.104	04/20/11	Wed	0031	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
226	0.000	11/08/11	Tue	1206	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
227	0.157	07/13/11	Wed	2045	Angle	0	0	1	Night	Dry	#N/A
228	0.000	04/27/11	Wed	0445	Overturn/Rollover	0	0	1	Day	Dry	Other Contributing Action
229	0.187	03/10/11	Thu	0905	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
230	0.187	04/10/11	Sun	2125	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
231	0.000	09/15/11	Thu	0855	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
232	0.000	12/09/11	Fri	0045	Overturn/Rollover	0	1	0	Night	Dry	No Contributing Action
233	0.224	01/27/11	Thu	1020	Pedestrian	1	1	0	Night	Dry	No Contributing Action
234	0.375	07/31/11	Sun	1555	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
235	0.394	04/22/11	Fri	1240	Rear-End	0	2	0	Day	Dry	Other Contributing Action
236	0.075	07/01/11	Fri	0606	Other Non-Collision	0	1	0	Day	Dry	Drove too Fast for Conditions
237	0.000	10/31/11	Mon	2203	Angle	0	1	0	Night	Dry	Ran Red Light
238	0.000	12/21/11	Wed	1745	Rear-End	0	0	1	Day	Dry	No Contributing Action
239	0.000	10/16/11	Sun	1720	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
240	0.000	10/04/11	Tue	1430	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
241	0.000	09/01/11	Thu	1604	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
242	0.000	10/06/11	Thu	2025	Left-Turn	0	0	1	Night	Wet	Careless or Negligent Manner
243	0.002	02/27/11	Sun	1231	Sideswipe	0	0	1	Day	Dry	#N/A

**State of Florida Department of Transportation
CRASH SUMMARY**

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ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2011 TO 12/ 2011 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
244	0.004	01/12/11	Wed	1313	Rear-End	0	1	0	Day	Dry	Followed too Closely
245	0.004	06/29/11	Wed	1523	Parked Motor Vehicle	0	4	0	Day	Dry	Other Contributing Action
246	0.044	06/23/11	Thu	1527	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
247	0.047	11/22/11	Tue	1215	Parked Motor Vehicle	0	0	1	Day	Dry	Careless or Negligent Manner
248	0.049	03/21/11	Mon	1536	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
249	0.049	10/02/11	Sun	1228	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
250	13.449	04/18/11	Mon	1214	Overturn/Rollover	0	0	1	Day	Dry	Careless or Negligent Manner
251	13.449	05/01/11	Sun	1350	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
252	13.449	05/21/11	Sat	1530	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
253	13.449	06/18/11	Sat	1900	Angle	0	0	1	Day	Wet	Failed to Yield Right-Of-Way
254	13.449	08/31/11	Wed	0824	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
255	13.449	09/02/11	Fri	1540	Left-Turn	0	2	0	Day	Wet	Failed to Yield Right-Of-Way
256	13.449	09/08/11	Thu	1100	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
257	13.449	09/16/11	Fri	0930	Left-Turn	0	2	0	Day	Dry	Failed to Yield Right-Of-Way
258	13.449	09/21/11	Wed	1650	Rear-End	0	0	1	Day	Dry	No Contributing Action
259	13.449	09/30/11	Fri	0800	Not Coded	0	0	1	Day	Dry	No Contributing Action
260	13.449	10/07/11	Fri	1300	Angle	0	1	0	Day	Dry	Careless or Negligent Manner
261	13.449	10/31/11	Mon	1730	Angle	0	0	1	Day	Wet	No Contributing Action
262	13.449	12/02/11	Fri	1645	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
263	13.449	12/03/11	Sat	1905	Angle	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
264	13.449	12/08/11	Thu	1450	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
265	13.449	12/12/11	Mon	0720	Left-Turn	0	2	0	Day	Dry	Failed to Yield Right-Of-Way
266	13.466	12/10/11	Sat	0040	Pedestrian	0	1	0	Night	Wet	No Contributing Action
267	13.480	03/30/11	Wed	0800	Not Coded	0	0	1	Day	Dry	Other Contributing Action
268	13.480	05/14/11	Sat	1300	Angle	0	0	1	Day	Dry	Other Contributing Action
269	13.480	05/20/11	Fri	1340	Not Coded	0	1	0	Day	Dry	Other Contributing Action
270	13.480	06/01/11	Wed	0135	Angle	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
271	13.480	06/04/11	Sat	2130	Not Coded	0	2	0	Night	Dry	Other Contributing Action
272	13.480	06/09/11	Thu	1830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
273	13.480	07/12/11	Tue	1720	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
274	13.480	07/26/11	Tue	1400	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
275	13.480	08/09/11	Tue	1610	Pedestrian	0	1	0	Day	Wet	Ran Red Light
276	13.480	08/10/11	Wed	1700	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
277	13.480	09/20/11	Tue	1007	Not Coded	0	1	0	Day	Wet	Failed to Yield Right-Of-Way
278	13.480	10/19/11	Wed	1145	Sideswipe	0	0	1	Day	Wet	Disregarded other Road Markings
279	13.480	10/25/11	Tue	0700	Left-Turn	0	0	1	Day	Dry	Improper Passing
280	13.480	11/01/11	Tue	1800	Sideswipe	0	0	1	Night	Wet	Failed To Keep In Proper Lane
281	13.480	11/16/11	Wed	0933	Not Coded	0	0	1	Day	Dry	No Contributing Action
282	13.480	11/17/11	Thu	1600	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
283	13.480	11/18/11	Fri	1030	Angle	0	0	1	Day	Wet	Careless or Negligent Manner
284	13.480	11/22/11	Tue	2250	Angle	0	2	0	Night	Dry	No Contributing Action
285	13.480	12/12/11	Mon	2200	Angle	0	0	1	Night	Wet	No Contributing Action
286	13.480	12/28/11	Wed	1158	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
287	13.488	09/02/11	Fri	0840	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
288	13.489	11/18/11	Fri	0755	Angle	0	0	1	Day	Wet	Failed to Yield Right-Of-Way
289	13.508	09/29/11	Thu	1255	Pedestrian	0	0	1	Day	Dry	No Contributing Action
290	13.541	09/24/11	Sat	0119	Rear-End	0	0	1	Night	Dry	Followed too Closely
291	13.541	11/05/11	Sat	1930	Angle	0	0	1	Night	Dry	Other Contributing Action
292	13.541	12/03/11	Sat	1642	Rear-End	0	0	1	Day	Dry	Other Contributing Action
293	13.567	05/26/11	Thu	0845	Ran into Water/Canal	0	0	1	Day	Dry	No Contributing Action
294	13.599	10/28/11	Fri	0200	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
295	13.605	02/28/11	Mon	1620	Rear-End	0	0	1	Day	Dry	Improper Passing
296	13.605	02/16/11	Wed	1530	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
297	13.605	04/27/11	Wed	2300	Not Coded	0	0	1	Night	Dry	No Contributing Action
298	13.605	03/23/11	Wed	1430	Not Coded	0	0	1	Day	Dry	Other Contributing Action
299	13.605	04/16/11	Sat	0148	Sideswipe	0	0	1	Night	Dry	Improper Passing
300	13.605	06/04/11	Sat	2213	Angle	0	0	1	Night	Dry	Other Contributing Action
301	13.605	07/19/11	Tue	1535	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
302	13.605	07/19/11	Tue	2300	Overturn/Rollover	0	0	1	Night	Dry	No Contributing Action
303	13.605	07/20/11	Wed	1825	Angle	0	0	1	Day	Dry	Failed To Keep In Proper Lane
304	13.605	07/27/11	Wed	1645	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
305	13.605	08/18/11	Thu	1238	Not Coded	0	1	0	Day	Dry	Failed To Keep In Proper Lane
306	13.605	08/20/11	Sat	0119	Rear-End	0	0	1	Night	Wet	Other Contributing Action
307	13.605	08/27/11	Sat	0105	Rear-End	0	0	1	Day	Dry	No Contributing Action
308	13.605	09/15/11	Thu	0020	Rear-End	0	0	1	Night	Dry	No Contributing Action
309	13.605	09/21/11	Wed	1149	Angle	0	1	0	Day	Dry	Ran Red Light
310	13.605	10/05/11	Wed	1124	Not Coded	0	0	1	Day	Dry	Failed To Keep In Proper Lane
311	13.605	10/17/11	Mon	0700	Sideswipe	0	0	1	Day	Wet	Careless or Negligent Manner
312	13.605	10/24/11	Mon	1217	Angle	0	0	1	Day	Dry	No Contributing Action
313	13.605	10/30/11	Sun	1325	Sideswipe	0	0	1	Day	Wet	Failed To Keep In Proper Lane
314	13.605	11/08/11	Tue	0800	Rear-End	0	0	1	Day	Dry	Other Contributing Action
315	13.605	12/01/11	Thu	1000	Angle	0	0	1	Day	Dry	Improper Passing
316	13.605	12/05/11	Mon	0916	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
317	13.605	12/19/11	Mon	1910	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
318	13.605	12/30/11	Fri	0520	Not Coded	0	0	1	Night	Dry	Failed To Keep In Proper Lane
319	13.609	12/27/11	Tue	0252	Rear-End	0	0	1	Day	Dry	No Contributing Action
320	13.611	09/20/11	Tue	1225	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
321	13.624	08/29/11	Mon	1240	Rear-End	0	0	1	Day	Dry	Followed too Closely
322	13.627	12/09/11	Fri	0330	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
323	2.553	05/14/11	Sat	1716	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
324	2.563	04/28/11	Thu	0000	Not Coded	0	0	1	Night	Dry	Other Contributing Action

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2011 TO 12/ 2011 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
325	2.563	03/28/11	Mon	2025	Not Coded	0	1	0	Night	Wet	Other Contributing Action
326	2.564	11/08/11	Tue	1653	Sideswipe	0	0	1	Day	Dry	No Contributing Action
327	2.567	06/06/11	Mon	1814	Rear-End	0	0	1	Day	Dry	Followed too Closely
328	2.572	03/25/11	Fri	1459	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
329	2.572	11/24/11	Thu	1147	Not Coded	0	0	1	Day	Dry	No Contributing Action
330	2.612	02/06/11	Sun	1910	Angle	0	0	1	Night	Dry	Careless or Negligent Manner
331	2.749	02/17/11	Thu	0903	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
332	2.768	06/09/11	Thu	1300	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
333	2.862	03/24/11	Thu	1320	Rear-End	0	0	1	Day	Dry	Other Contributing Action
334	0.000	08/07/11	Sun	1825	Guardrail Face	0	0	1	Day	Wet	Careless or Negligent Manner
335	0.000	06/12/11	Sun	1630	Not Coded	0	0	1	Day	Wet	Careless or Negligent Manner
336	0.000	10/24/11	Mon	2200	Guardrail Face	0	1	0	Night	Dry	No Contributing Action
337	4.193	06/02/11	Thu	0910	Pedalcycle	0	1	0	Day	Dry	No Contributing Action
338	4.193	10/09/11	Sun	1711	Angle	0	3	0	Day	Dry	Failed to Yield Right-Of-Way
339	4.209	01/10/11	Mon	1113	Other Non-Fixed Object	0	0	1	Day	Dry	Careless or Negligent Manner
340	4.245	07/06/11	Wed	0950	Angle	0	1	0	Day	Wet	Other Contributing Action
341	19.627	08/11/11	Thu	2327	Not Coded	0	0	1	Night	Dry	Careless or Negligent Manner
342	19.637	08/18/11	Thu	0000	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
343	19.637	08/19/11	Fri	1141	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
344	19.638	09/26/11	Mon	1415	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
345	19.646	01/28/11	Fri	1515	Rear-End	0	0	1	Day	Dry	Followed too Closely
346	19.655	09/19/11	Mon	1420	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
347	19.655	11/18/11	Fri	1601	Angle	0	0	1	Day	Wet	Failed to Yield Right-Of-Way
348	19.655	12/24/11	Sat	1400	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
349	19.674	04/26/11	Tue	1354	Rear-End	0	0	1	Day	Dry	Followed too Closely
350	19.674	05/22/11	Sun	0325	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
351	19.674	09/05/11	Mon	1830	Angle	0	0	1	Day	Dry	Swerved Or Avoided
352	19.674	10/15/11	Sat	2351	Angle	0	0	1	Night	Dry	Improper Turn
353	19.674	10/25/11	Tue	0712	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
354	19.674	10/27/11	Thu	0120	Not Coded	0	0	1	Night	Dry	Followed too Closely
355	19.674	11/08/11	Tue	0000	Left-Turn	0	0	1	Day	Dry	No Contributing Action
356	19.674	12/04/11	Sun	1600	Not Coded	0	0	1	Day	Dry	No Contributing Action
357	19.674	12/09/11	Fri	0810	Rear-End	0	3	0	Day	Wet	Careless or Negligent Manner
358	19.674	12/15/11	Thu	2130	Angle	0	0	1	Night	Dry	Careless or Negligent Manner
359	19.683	06/12/11	Sun	1500	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
360	19.683	07/20/11	Wed	1440	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
361	19.688	07/11/11	Mon	0909	Rear-End	0	0	1	Day	Dry	Failed To Keep In Proper Lane
362	19.688	12/10/11	Sat	2355	Sideswipe	0	1	0	Day	Dry	Other Contributing Action
363	19.735	09/11/11	Sun	1425	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
364	19.794	07/22/11	Fri	0840	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
365	19.794	08/31/11	Wed	1445	Backed Into	0	0	1	Day	Dry	No Contributing Action
366	19.794	09/02/11	Fri	1105	Overturn/Rollover	0	0	1	Day	Dry	Other Contributing Action
367	19.794	09/08/11	Thu	1943	Sideswipe	0	0	1	Night	Wet	Improper Passing
368	19.794	12/21/11	Wed	0920	Not Coded	0	0	1	Day	Dry	Other Contributing Action
369	19.798	11/10/11	Thu	0300	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
370	19.803	10/07/11	Fri	0904	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
371	19.809	10/17/11	Mon	1444	Sideswipe	0	0	1	Day	Wet	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
371	2	154	217	184	0	46	8	0	45	1	7	4	26	1	31
Percent	0.54%	41.51%	58.49%	49.60%	0.00%	12.40%	2.16%	0.00%	12.13%	0.27%	1.89%	1.08%	7.01%	0.27%	8.36%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	274	97	51	320	176	23	1	3	1	6	1	1	2	5	0
Percent	73.85%	26.15%	13.75%	86.25%	47.44%	6.20%	0.27%	0.81%	0.27%	1.62%	0.27%	0.27%	0.54%	1.35%	0.00%

TOTAL ENTERING VEHICLES/ADT: **106,470** SEGMENT CRASH RATE: **1.944 CRASHES PER MILLION VEHICLE MILES**

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	4.094	04/30/12	Mon	1628	Overturn/Rollover	0	1	0	Day	Wet	No Contributing Action
2	4.098	03/26/12	Mon	1250	Rear-End	0	3	0	Day	Dry	#N/A
3	4.107	07/17/12	Tue	1416	Sideswipe	0	0	1	Day	Wet	Failed to Yield Right-Of-Way
4	0.278	04/22/12	Sun	1700	Sideswipe	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
5	0.326	04/15/12	Sun	2240	Other Traffic Barrier	0	1	0	Night	Dry	Failed To Keep In Proper Lane
6	0.330	12/16/12	Sun	0352	Rear-End	0	1	0	Night	Dry	Other Contributing Action
7	0.378	01/05/12	Thu	1235	Other Non-Fixed Object	0	0	1	Day	Dry	No Contributing Action
8	0.378	02/03/12	Fri	0915	Sideswipe	0	0	1	Day	Dry	#N/A
9	0.378	02/16/12	Thu	1215	Rear-End	0	2	0	Day	Dry	Other Contributing Action
10	0.456	05/22/12	Tue	1440	Sideswipe	0	0	1	Day	Wet	No Contributing Action
11	0.459	09/11/12	Tue	1340	Not Coded	0	0	1	Day	Dry	#N/A
12	0.493	01/24/12	Tue	1715	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
13	0.508	02/26/12	Sun	0550	Concrete Traffic Barrier	0	0	1	Night	Dry	Careless or Negligent Manner
14	0.508	10/22/12	Mon	0812	Rear-End	0	0	1	Day	Dry	Other Contributing Action
15	0.527	09/23/12	Sun	0510	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
16	0.527	12/11/12	Tue	1914	Rear-End	0	0	1	Night	Dry	No Contributing Action
17	0.539	06/05/12	Tue	0742	Concrete Traffic Barrier	0	1	0	Day	Dry	No Contributing Action
18	0.546	01/21/12	Sat	0225	Parked Motor Vehicle	0	1	0	Night	Dry	Careless or Negligent Manner
19	0.584	09/21/12	Fri	1706	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
20	0.589	01/25/12	Wed	0800	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
21	0.589	08/22/12	Wed	1658	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
22	0.641	05/06/12	Sun	1416	Sideswipe	0	1	0	Day	Dry	Improper Passing
23	0.659	08/17/12	Fri	1558	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
24	0.678	01/18/12	Wed	1245	Sideswipe	0	0	1	Day	Dry	#N/A
25	0.689	01/23/12	Mon	1817	Angle	0	0	1	Day	Dry	No Contributing Action
26	0.728	04/24/12	Tue	2030	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
27	0.735	05/19/12	Sat	0339	Impact Attenuator/Crash Cushion	0	1	0	Night	Dry	Careless or Negligent Manner
28	0.739	02/22/12	Wed	2234	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
29	0.739	08/31/12	Fri	1607	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
30	0.755	12/08/12	Sat	1505	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
31	0.778	02/04/12	Sat	1735	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
32	0.778	11/01/12	Thu	0030	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
33	0.785	03/03/12	Sat	0235	Impact Attenuator/Crash Cushion	0	1	0	Night	Dry	No Contributing Action
34	0.785	05/20/12	Sun	1712	Sideswipe	0	1	0	Day	Dry	Careless or Negligent Manner
35	0.785	12/17/12	Mon	1415	Sideswipe	0	0	1	Day	Dry	Improper Passing
36	0.888	06/16/12	Sat	2311	Sideswipe	0	0	1	Night	Dry	#N/A
37	0.899	05/10/12	Thu	1200	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
38	0.928	07/10/12	Tue	1302	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
39	0.978	03/05/12	Mon	1742	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
40	0.978	05/05/12	Sat	1620	Angle	0	0	1	Day	Dry	#N/A
41	0.989	03/02/12	Fri	1710	Rear-End	0	0	1	Day	Dry	Followed too Closely
42	0.989	06/06/12	Wed	1800	Rear-End	0	0	1	Day	Dry	Other Contributing Action
43	0.989	06/06/12	Wed	2317	Rear-End	0	0	1	Night	Dry	No Contributing Action
44	0.989	09/28/12	Fri	1710	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
45	1.024	05/20/12	Sun	1140	Other Fixed Object	0	2	0	Day	Dry	#N/A
46	1.028	02/13/12	Mon	1303	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
47	1.038	09/19/12	Wed	1510	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
48	1.043	10/31/12	Wed	2130	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
49	1.078	02/04/12	Sat	1745	Not Coded	0	0	1	Day	Dry	#N/A
50	1.078	01/05/12	Thu	0815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
51	1.078	07/24/12	Tue	1007	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
52	1.078	07/24/12	Tue	1314	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
53	1.081	06/03/12	Sun	0150	Sideswipe	0	1	0	Night	Dry	Careless or Negligent Manner
54	1.108	11/14/12	Wed	1820	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
55	1.119	06/03/12	Sun	1435	Angle	0	0	1	Day	Dry	#N/A
56	1.129	08/21/12	Tue	0815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
57	1.164	08/11/12	Sat	1020	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
58	1.164	08/18/12	Sat	0840	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
59	1.168	07/19/12	Thu	0439	Rear-End	0	3	0	Night	Dry	Careless or Negligent Manner
60	1.178	01/13/12	Fri	0400	Sideswipe	0	1	0	Night	Wet	Failed to Yield Right-Of-Way
61	1.178	01/23/12	Mon	1540	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
62	1.178	07/06/12	Fri	1338	Angle	0	0	1	Day	Dry	No Contributing Action
63	1.178	09/29/12	Sat	0642	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
64	1.178	10/03/12	Wed	1400	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
65	1.178	12/01/12	Sat	0500	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
66	1.183	05/11/12	Fri	1855	Sideswipe	0	0	1	Day	Dry	Erratic, Reckless or Aggressive
67	1.183	06/28/12	Thu	1050	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
68	1.183	06/28/12	Thu	1135	Angle	0	1	0	Day	Dry	Careless or Negligent Manner
69	1.183	08/20/12	Mon	1500	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
70	1.183	10/24/12	Wed	1255	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
71	1.187	07/17/12	Tue	0045	Sideswipe	0	0	1	Night	Dry	#N/A
72	1.196	05/09/12	Wed	0825	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
73	1.196	04/08/12	Sun	1325	Angle	0	3	0	Day	Dry	#N/A
74	1.202	05/22/12	Tue	2340	Rear-End	0	0	1	Night	Wet	No Contributing Action
75	1.221	02/07/12	Tue	0820	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
76	1.221	01/12/12	Thu	1600	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
77	1.250	08/11/12	Sat	0715	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
78	1.250	06/22/12	Fri	2105	Concrete Traffic Barrier	0	0	1	Night	Wet	#N/A
79	1.250	12/04/12	Tue	1900	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
80	1.259	03/06/12	Tue	0735	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
81	1.259	04/29/12	Sun	0200	Concrete Traffic Barrier	0	1	0	Night	Wet	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: **87004000** STATE ROUTE: **112**
 ROADWAY LIMITS: **MP 0.000 to MP 0.500** M.P. **0.000** TO **4.910** ENGINEER: **FDOT D6**
 STUDY PERIOD: FROM **1/ 2012** TO **12/ 2012** COUNTY: **Miami-Dade**

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
82	1.259	05/26/12	Sat	2220	Not Coded	0	1	0	Night	Dry	#N/A
83	1.259	10/03/12	Wed	1440	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
84	1.259	07/27/12	Fri	0948	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
85	1.259	11/19/12	Mon	0848	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
86	1.266	10/13/12	Sat	0624	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
87	1.269	10/13/12	Sat	0540	Concrete Traffic Barrier	0	0	1	Night	Wet	No Contributing Action
88	1.270	08/04/12	Sat	0410	Concrete Traffic Barrier	1	1	0	Night	Dry	No Contributing Action
89	1.275	04/09/12	Mon	0915	Rear-End	0	9	0	Day	Dry	No Contributing Action
90	1.276	10/04/12	Thu	1654	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
91	1.278	07/27/12	Fri	0700	Concrete Traffic Barrier	0	0	1	Day	Dry	No Contributing Action
92	1.316	10/08/12	Mon	2150	Other Non-Fixed Object	0	0	1	Night	Dry	#N/A
93	1.373	07/12/12	Thu	1445	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
94	1.602	03/07/12	Wed	1129	Sideswipe	0	0	1	Day	Dry	Improper Passing
95	1.611	05/05/12	Sat	0750	Rear-End	0	1	0	Day	Dry	No Contributing Action
96	1.611	06/09/12	Sat	0010	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
97	1.611	06/09/12	Sat	0115	Rear-End	0	3	0	Night	Dry	Careless or Negligent Manner
98	1.611	06/04/12	Mon	0200	Rear-End	0	0	1	Night	Dry	No Contributing Action
99	1.621	04/21/12	Sat	1830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
100	1.621	05/30/12	Wed	1545	Not Coded	0	0	1	Day	Wet	Drove too Fast for Conditions
101	1.621	08/01/12	Wed	1855	Other Traffic Barrier	0	1	0	Day	Dry	Drove too Fast for Conditions
102	1.621	08/10/12	Fri	1135	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
103	1.621	09/22/12	Sat	0830	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
104	1.646	08/30/12	Thu	1717	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
105	1.659	07/22/12	Sun	1625	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
106	1.678	07/12/12	Thu	1725	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
107	1.683	03/26/12	Mon	1739	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
108	1.683	04/06/12	Fri	1439	Rear-End	0	0	1	Day	Dry	Improper Passing
109	1.683	05/21/12	Mon	1003	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
110	1.697	02/22/12	Wed	1620	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
111	1.698	12/31/12	Mon	0700	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
112	1.730	08/10/12	Fri	1245	Rear-End	0	3	0	Day	Dry	No Contributing Action
113	1.753	04/07/12	Sat	0115	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
114	1.778	03/01/12	Thu	1548	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
115	1.778	07/13/12	Fri	0830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
116	1.806	11/29/12	Thu	0150	Concrete Traffic Barrier	0	1	0	Night	Wet	Careless or Negligent Manner
117	1.810	04/11/12	Wed	0640	Other Non-Collision	0	1	0	Day	Dry	Failed To Keep In Proper Lane
118	1.833	03/18/12	Sun	0111	Concrete Traffic Barrier	0	0	1	Night	Wet	Over-Correcting/Over-Steering
119	1.833	09/10/12	Mon	1519	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
120	1.883	03/26/12	Mon	1626	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
121	1.917	08/25/12	Sat	0007	Not Coded	0	0	1	Night	Wet	Other Contributing Action
122	1.948	08/24/12	Fri	1710	Angle	0	0	1	Day	Wet	Careless or Negligent Manner
123	2.046	12/21/12	Fri	0950	Sideswipe	0	1	0	Day	Dry	Other Contributing Action
124	2.083	03/07/12	Wed	1600	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
125	2.083	06/30/12	Sat	0950	Angle	0	2	0	Day	Dry	Other Contributing Action
126	2.083	08/13/12	Mon	1535	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
127	2.151	04/27/12	Fri	1803	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
128	2.278	02/18/12	Sat	1620	Rear-End	0	0	1	Day	Dry	Followed too Closely
129	2.278	07/17/12	Tue	1505	Sideswipe	0	1	0	Day	Wet	#N/A
130	2.283	09/22/12	Sat	1427	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
131	2.333	07/29/12	Sun	1933	Angle	0	0	1	Night	Dry	#N/A
132	2.546	12/22/12	Sat	1813	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
133	2.583	02/11/12	Sat	1950	Not Coded	0	0	1	Night	Dry	#N/A
134	2.583	04/13/12	Fri	2350	Curb	0	0	1	Night	Wet	Careless or Negligent Manner
135	2.583	05/24/12	Thu	1730	Sideswipe	0	1	0	Day	Dry	#N/A
136	2.583	04/30/12	Mon	1730	Rear-End	0	0	1	Day	Wet	Other Contributing Action
137	2.583	07/13/12	Fri	0830	Angle	0	0	1	Day	Wet	No Contributing Action
138	2.583	07/30/12	Mon	0640	Guardrail Face	0	2	0	Night	Wet	Swerved Or Avoided
139	2.583	09/23/12	Sun	0925	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
140	2.583	09/23/12	Sun	1029	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
141	2.583	08/18/12	Sat	1825	Not Coded	0	0	1	Day	Wet	Drove too Fast for Conditions
142	2.583	10/16/12	Tue	1725	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
143	2.583	09/23/12	Sun	1712	Angle	0	0	1	Day	Wet	Improper Passing
144	2.730	03/23/12	Fri	1537	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
145	3.083	06/10/12	Sun	2125	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
146	3.083	07/19/12	Thu	1828	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
147	3.083	02/24/12	Fri	2029	Not Coded	0	3	0	Night	Dry	Careless or Negligent Manner
148	3.230	03/15/12	Thu	1540	Rear-End	0	0	1	Day	Wet	Followed too Closely
149	3.230	04/21/12	Sat	0339	Pedestrian	1	0	0	Night	Wet	#N/A
150	3.230	05/17/12	Thu	2300	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
151	3.546	12/27/12	Thu	1920	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
152	3.730	05/23/12	Wed	1015	Guardrail Face	0	1	0	Day	Wet	Careless or Negligent Manner
153	3.730	01/31/12	Tue	2347	Guardrail Face	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
154	3.730	02/04/12	Sat	1728	Not Coded	0	1	0	Night	Dry	No Contributing Action
155	3.730	08/08/12	Wed	1628	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
156	3.730	05/26/12	Sat	0025	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
157	3.730	03/23/12	Fri	0830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
158	3.730	04/21/12	Sat	0409	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
159	3.730	07/14/12	Sat	0242	Guardrail Face	0	0	1	Night	Wet	Careless or Negligent Manner
160	3.730	05/27/12	Sun	0050	Angle	0	0	1	Night	Dry	Improper Passing
161	3.730	11/29/12	Thu	0014	Guardrail Face	0	0	1	Night	Wet	#N/A
162	3.787	02/13/12	Mon	0144	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
163	4.130	12/28/12	Fri	0000	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
164	4.230	03/27/12	Tue	1440	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
165	4.230	04/06/12	Fri	0202	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
166	4.230	06/25/12	Mon	1445	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
167	4.230	10/07/12	Sun	1120	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
168	4.230	12/29/12	Sat	0000	Rear-End	0	3	0	Night	Dry	Careless or Negligent Manner
169	4.230	12/05/12	Wed	1748	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
170	4.330	01/13/12	Fri	1549	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
171	4.404	08/13/12	Mon	1715	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
172	4.410	03/01/12	Thu	0125	Rear-End	0	2	0	Night	Wet	Careless or Negligent Manner
173	4.423	03/11/12	Sun	1445	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
174	4.430	02/25/12	Sat	0610	Pedestrian	1	0	0	Night	Dry	No Contributing Action
175	4.430	03/17/12	Sat	1607	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
176	4.480	03/12/12	Mon	1735	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
177	4.530	11/13/12	Tue	1225	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
178	4.530	08/17/12	Fri	0910	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
179	4.530	12/15/12	Sat	2322	Sideswipe	0	2	0	Night	Dry	Failed to Yield Right-Of-Way
180	4.560	12/07/12	Fri	1430	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
181	4.578	03/01/12	Thu	1655	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
182	4.578	04/01/12	Sun	1510	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
183	4.630	03/30/12	Fri	1135	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
184	4.630	08/08/12	Wed	1649	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
185	4.630	11/07/12	Wed	1549	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
186	4.630	12/03/12	Mon	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
187	4.630	10/24/12	Wed	0900	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
188	4.630	11/13/12	Tue	1947	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
189	4.635	07/24/12	Tue	0050	Rear-End	0	0	1	Night	Dry	Other Contributing Action
190	4.660	02/24/12	Fri	1320	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
191	4.673	02/21/12	Tue	1007	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
192	4.673	06/25/12	Mon	1212	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
193	4.673	12/06/12	Thu	0030	Other Fixed Object	0	0	1	Night	Wet	Careless or Negligent Manner
194	4.692	03/14/12	Wed	1332	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
195	4.692	03/12/12	Mon	1704	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
196	4.692	10/02/12	Tue	0828	Rear-End	0	0	1	Day	Wet	No Contributing Action
197	4.711	09/13/12	Thu	2028	Rear-End	0	1	0	Night	Wet	No Contributing Action
198	4.711	12/28/12	Fri	2115	Rear-End	0	0	1	Night	Dry	No Contributing Action
199	4.721	09/16/12	Sun	2112	Angle	0	0	1	Night	Dry	Careless or Negligent Manner
200	4.721	06/05/12	Tue	1405	Sideswipe	0	0	1	Day	Dry	No Contributing Action
201	4.724	01/21/12	Sat	2300	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
202	4.872	09/05/12	Wed	0854	Bridge Overhead Structure	0	0	1	Day	Dry	Other Contributing Action
203	4.910	02/19/12	Sun	2340	Rear-End	1	1	0	Night	Dry	Careless or Negligent Manner
204	0.392	05/31/12	Thu	1114	Guardrail Face	0	0	1	Day	Wet	Careless or Negligent Manner
205	0.000	05/23/12	Wed	0910	Sideswipe	0	0	1	Day	Wet	No Contributing Action
206	0.000	02/05/12	Sun	1020	Other Non-Fixed Object	0	0	1	Day	Dry	No Contributing Action
207	0.000	05/20/12	Sun	0350	Not Coded	0	0	1	Night	Dry	#N/A
208	0.000	05/07/12	Mon	0900	Angle	0	0	1	Day	Dry	No Contributing Action
209	0.000	12/06/12	Thu	0803	Concrete Traffic Barrier	0	0	1	Day	Dry	Careless or Negligent Manner
210	0.000	04/12/12	Thu	0419	Guardrail Face	0	1	0	Night	Dry	Careless or Negligent Manner
211	0.000	09/05/12	Wed	1801	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
212	0.000	09/10/12	Mon	1050	Parked Motor Vehicle	0	0	1	Day	Dry	No Contributing Action
213	0.000	11/05/12	Mon	0550	Not Coded	0	1	0	Night	Dry	Careless or Negligent Manner
214	0.092	03/21/12	Wed	0832	Sideswipe	0	0	1	Day	Dry	No Contributing Action
215	0.000	10/14/12	Sun	1140	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
216	0.000	10/12/12	Fri	1024	Angle	0	0	1	Day	Dry	No Contributing Action
217	0.000	06/08/12	Fri	1300	Rear-End	0	0	1	Day	Dry	Other Contributing Action
218	0.000	12/07/12	Fri	1135	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
219	0.000	05/11/12	Fri	1438	Rear-End	0	0	1	Day	Dry	Other Contributing Action
220	0.009	09/08/12	Sat	2115	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
221	0.009	09/14/12	Fri	1143	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
222	0.066	06/28/12	Thu	1100	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
223	0.104	04/27/12	Fri	1325	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
224	0.104	08/25/12	Sat	0300	Angle	0	0	1	Night	Dry	No Contributing Action
225	0.104	12/12/12	Wed	1521	Rear-End	0	0	1	Day	Dry	Followed too Closely
226	0.000	03/27/12	Tue	1155	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
227	0.000	05/18/12	Fri	0325	Traffic Sign Support	0	0	1	Night	Dry	#N/A
228	0.000	01/20/12	Fri	1650	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
229	0.000	09/11/12	Tue	1137	Sideswipe	0	1	0	Night	Dry	Careless or Negligent Manner
230	0.187	11/12/12	Mon	0804	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
231	0.000	03/11/12	Sun	1445	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
232	0.000	05/20/12	Sun	0730	Guardrail Face	0	0	1	Day	Dry	Careless or Negligent Manner
233	0.000	07/16/12	Mon	1641	Guardrail Face	0	1	0	Day	Wet	Careless or Negligent Manner
234	0.000	08/25/12	Sat	1329	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
235	0.000	02/19/12	Sun	1300	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
236	0.000	03/02/12	Fri	1248	Not Coded	0	0	1	Day	Dry	#N/A
237	0.000	02/16/12	Thu	1130	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
238	0.000	07/22/12	Sun	0715	Angle	0	0	1	Day	Wet	Careless or Negligent Manner
239	0.000	05/21/12	Mon	1505	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
240	0.000	12/07/12	Fri	1223	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
241	0.000	06/03/12	Sun	1535	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
242	0.129	01/28/12	Sat	1255	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
243	0.163	09/09/12	Sun	1502	Rear-End	0	0	1	Day	Dry	#N/A

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
244	0.013	04/21/12	Sat	0350	Traffic Sign Support	0	0	1	Night	Wet	Over-Correcting/Over-Steering
245	0.075	09/17/12	Mon	0800	Guardrail Face	0	1	0	Day	Wet	Careless or Negligent Manner
246	0.000	01/13/12	Fri	0940	Left-Turn	0	0	1	Day	Dry	No Contributing Action
247	0.000	11/08/12	Thu	1958	Right-Turn	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
248	0.000	12/17/12	Mon	1959	Pedestrian	0	1	0	Night	Dry	No Contributing Action
249	0.005	06/14/12	Thu	1228	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
250	0.019	04/12/12	Thu	1529	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
251	13.449	01/25/12	Wed	0745	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
252	13.449	01/25/12	Wed	1012	Parked Motor Vehicle	0	0	1	Day	Dry	Careless or Negligent Manner
253	13.449	02/10/12	Fri	1715	Angle	0	0	1	Day	Wet	Other Contributing Action
254	13.449	02/13/12	Mon	1645	Angle	0	2	0	Day	Dry	Failed to Yield Right-Of-Way
255	13.449	02/14/12	Tue	1425	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
256	13.449	02/16/12	Thu	0837	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
257	13.449	03/10/12	Sat	1421	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
258	13.449	04/12/12	Thu	1240	Rear-End	0	0	1	Day	Dry	No Contributing Action
259	13.449	04/29/12	Sun	2240	Other Fixed Object	0	1	0	Night	Wet	No Contributing Action
260	13.449	05/03/12	Thu	1120	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
261	13.449	05/15/12	Tue	1000	Not Coded	0	0	1	Night	Wet	Failed to Yield Right-Of-Way
262	13.449	07/18/12	Wed	2118	Angle	0	3	0	Night	Dry	Failed to Yield Right-Of-Way
263	13.449	07/25/12	Wed	0858	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
264	13.449	08/25/12	Sat	1515	Rear-End	0	0	1	Day	Dry	Followed too Closely
265	13.449	09/02/12	Sun	1355	Angle	0	1	0	Day	Dry	Improper Turn
266	13.449	09/10/12	Mon	0915	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
267	13.449	09/28/12	Fri	1215	Left-Turn	0	0	1	Day	Dry	Careless or Negligent Manner
268	13.449	10/04/12	Thu	1330	Angle	0	0	1	Day	Dry	Other Contributing Action
269	13.449	11/11/12	Sun	0050	Not Coded	0	2	0	Night	Dry	Failed to Yield Right-Of-Way
270	13.449	11/27/12	Tue	1405	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
271	13.449	11/30/12	Fri	0821	Angle	0	2	0	Day	Dry	Failed to Yield Right-Of-Way
272	13.449	12/03/12	Mon	1300	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
273	13.449	12/22/12	Sat	1630	Angle	0	0	1	Day	Dry	Ran Stop Sign
274	13.461	09/13/12	Thu	0630	Sideswipe	0	0	1	Night	Dry	Other Contributing Action
275	13.471	10/26/12	Fri	1940	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
276	13.476	12/30/12	Sun	0800	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
277	13.477	11/21/12	Wed	1855	Angle	0	0	1	Night	Dry	Other Contributing Action
278	13.480	02/06/12	Mon	0930	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
279	13.480	02/14/12	Tue	1710	Rear-End	0	1	0	Day	Dry	No Contributing Action
280	13.480	03/16/12	Fri	1625	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
281	13.480	03/21/12	Wed	1700	Not Coded	0	0	1	Day	Dry	Other Contributing Action
282	13.480	04/02/12	Mon	1500	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
283	13.480	04/03/12	Tue	0531	Rear-End	0	0	1	Night	Dry	No Contributing Action
284	13.480	04/21/12	Sat	0615	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
285	13.480	04/22/12	Sun	1700	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
286	13.480	05/03/12	Thu	0700	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
287	13.480	05/24/12	Thu	2300	Sideswipe	0	0	1	Night	Dry	Improper Turn
288	13.480	05/29/12	Tue	0722	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
289	13.480	05/30/12	Wed	1630	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
290	13.480	06/04/12	Mon	1400	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
291	13.480	07/18/12	Wed	0844	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
292	13.480	08/08/12	Wed	0230	Not Coded	0	0	1	Night	Dry	Improper Turn
293	13.480	08/09/12	Thu	1900	Rear-End	0	0	1	Day	Dry	No Contributing Action
294	13.480	08/22/12	Wed	1300	Left-Turn	0	0	1	Day	Dry	No Contributing Action
295	13.480	08/25/12	Sat	1538	Not Coded	0	0	1	Day	Wet	Careless or Negligent Manner
296	13.480	09/08/12	Sat	2230	Angle	0	0	1	Night	Dry	Careless or Negligent Manner
297	13.480	09/17/12	Mon	1926	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
298	13.480	09/23/12	Sun	0918	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
299	13.480	10/19/12	Fri	1730	Angle	0	0	1	Day	Dry	No Contributing Action
300	13.480	10/19/12	Fri	0530	Angle	0	0	1	Day	Dry	No Contributing Action
301	13.480	10/22/12	Mon	1000	Rear-End	0	0	1	Day	Dry	No Contributing Action
302	13.480	10/31/12	Wed	0720	Overturn/Rollover	0	0	1	Day	Dry	No Contributing Action
303	13.480	11/09/12	Fri	1825	Rear-End	0	0	1	Night	Dry	No Contributing Action
304	13.480	11/14/12	Wed	0817	Sideswipe	0	0	1	Day	Wet	Failed To Keep In Proper Lane
305	13.480	11/17/12	Sat	1202	Not Coded	0	2	0	Day	Dry	No Contributing Action
306	13.480	11/30/12	Fri	2100	Angle	0	0	1	Night	Dry	Improper Passing
307	13.480	12/07/12	Fri	0912	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
308	13.480	12/07/12	Fri	1615	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
309	13.490	11/09/12	Fri	1249	Rear-End	0	0	1	Day	Dry	No Contributing Action
310	13.499	04/27/12	Fri	1737	Pedestrian	0	1	0	Day	Dry	No Contributing Action
311	13.499	05/12/12	Sat	1030	Sideswipe	0	0	1	Day	Dry	No Contributing Action
312	13.500	04/28/12	Sat	0330	Not Coded	0	0	1	Day	Wet	Careless or Negligent Manner
313	13.506	03/22/12	Thu	2300	Not Coded	0	0	1	Night	Dry	Followed too Closely
314	13.518	01/29/12	Sun	2000	Angle	0	2	0	Night	Wet	Other Contributing Action
315	13.518	12/17/12	Mon	0959	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
316	13.539	02/02/12	Thu	0755	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
317	13.541	08/18/12	Sat	1700	Rear-End	0	3	0	Day	Wet	Other Contributing Action
318	13.548	05/24/12	Thu	0539	Rear-End	0	0	1	Day	Dry	Other Contributing Action
319	13.563	03/21/12	Wed	1430	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
320	13.591	05/01/12	Tue	1235	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
321	13.601	12/11/12	Tue	1338	Not Coded	0	0	1	Day	Dry	No Contributing Action
322	13.605	01/02/12	Mon	2245	Left-Turn	0	0	1	Night	Dry	Other Contributing Action
323	13.605	01/30/12	Mon	0820	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
324	13.605	02/01/12	Wed	1830	Angle	0	0	1	Night	Dry	Failed to Yield Right-Of-Way

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
325	13.605	02/01/12	Wed	1915	Rear-End	0	0	1	Night	Dry	No Contributing Action
326	13.605	02/03/12	Fri	0750	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
327	13.605	02/08/12	Wed	1640	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
328	13.605	02/14/12	Tue	1516	Sideswipe	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
329	13.605	02/15/12	Wed	0910	Not Coded	0	0	1	Day	Dry	Improper Passing
330	13.605	02/16/12	Thu	0050	Left-Turn	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
331	13.605	02/18/12	Sat	0412	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
332	13.605	02/23/12	Thu	1720	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
333	13.605	03/08/12	Thu	0015	Left-Turn	0	1	0	Night	Wet	No Contributing Action
334	13.605	03/31/12	Sat	1350	Sideswipe	0	0	1	Day	Dry	No Contributing Action
335	13.605	04/22/12	Sun	1150	Rear-End	0	0	1	Day	Dry	Improper Passing
336	13.605	05/04/12	Fri	1445	Rear-End	0	0	1	Day	Dry	Other Contributing Action
337	13.605	05/07/12	Mon	1745	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
338	13.605	05/18/12	Fri	0841	Not Coded	0	0	1	Day	Dry	Other Contributing Action
339	13.605	05/20/12	Sun	1800	Angle	0	4	0	Day	Dry	Failed to Yield Right-Of-Way
340	13.605	05/22/12	Tue	1415	Sideswipe	0	0	1	Day	Wet	Careless or Negligent Manner
341	13.605	06/03/12	Sun	0640	Other Fixed Object	0	0	1	Day	Dry	Careless or Negligent Manner
342	13.605	06/06/12	Wed	1336	Rear-End	0	0	1	Day	Dry	Other Contributing Action
343	13.605	06/18/12	Mon	1030	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
344	13.605	07/02/12	Mon	1735	Angle	0	1	0	Day	Dry	Other Contributing Action
345	13.605	07/12/12	Thu	0840	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
346	13.605	07/22/12	Sun	0050	Not Coded	0	0	1	Night	Wet	No Contributing Action
347	13.605	07/24/12	Tue	1000	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
348	13.605	08/03/12	Fri	1340	Not Coded	0	0	1	Day	Wet	No Contributing Action
349	13.605	08/06/12	Mon	1550	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
350	13.605	08/13/12	Mon	1020	Parked Motor Vehicle	0	1	0	Night	Dry	Failed to Yield Right-Of-Way
351	13.605	08/31/12	Fri	2030	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
352	13.605	09/04/12	Tue	1200	Not Coded	0	0	1	Day	Dry	Improper Passing
353	13.605	10/26/12	Fri	2350	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
354	13.605	11/01/12	Thu	1230	Rear-End	0	0	1	Day	Dry	Followed too Closely
355	13.605	11/05/12	Mon	1325	Rear-End	0	0	1	Day	Dry	Followed too Closely
356	13.605	11/26/12	Mon	1700	Parked Motor Vehicle	0	0	1	Night	Dry	Careless or Negligent Manner
357	13.605	12/23/12	Sun	2034	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
358	13.614	08/13/12	Mon	1232	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
359	13.624	12/28/12	Fri	1920	Sideswipe	0	0	1	Night	Dry	Improper Passing
360	13.628	02/27/12	Mon	2011	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
361	13.633	03/14/12	Wed	0822	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
362	2.534	07/28/12	Sat	2335	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
363	2.572	04/18/12	Wed	1205	Sideswipe	0	0	1	Day	Dry	Improper Passing
364	2.572	09/14/12	Fri	1515	Parked Motor Vehicle	0	0	1	Day	Dry	Failed To Keep In Proper Lane
365	2.612	02/29/12	Wed	1110	Pedestrian	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
366	2.612	07/06/12	Fri	0730	Angle	0	0	1	Day	Dry	No Contributing Action
367	2.747	12/18/12	Tue	0732	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
368	2.781	09/18/12	Tue	0923	Left-Turn	0	1	0	Day	Dry	No Contributing Action
369	2.783	10/18/12	Thu	0845	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
370	4.193	03/01/12	Thu	0730	Rear-End	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
371	4.193	06/14/12	Thu	0425	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
372	4.193	10/15/12	Mon	1500	Angle	0	0	1	Day	Dry	Ran Red Light
373	4.245	03/23/12	Fri	1555	Rear-End	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
374	4.245	07/09/12	Mon	1350	Parked Motor Vehicle	0	0	1	Day	Dry	#N/A
375	19.636	04/12/12	Thu	2100	Angle	0	0	1	Night	Dry	No Contributing Action
376	19.640	01/26/12	Thu	1855	Rear-End	0	0	1	Night	Dry	No Contributing Action
377	19.650	02/08/12	Wed	1421	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
378	19.673	09/29/12	Sat	1000	Not Coded	0	0	1	Day	Dry	No Contributing Action
379	19.674	02/01/12	Wed	1230	Left-Turn	0	0	1	Day	Dry	Improper Turn
380	19.674	02/20/12	Mon	1227	Sideswipe	0	0	1	Day	Dry	Improper Passing
381	19.674	02/27/12	Mon	0510	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
382	19.674	03/14/12	Wed	1450	Angle	0	0	1	Day	Dry	Other Contributing Action
383	19.674	03/26/12	Mon	0710	Rear-End	0	0	1	Day	Dry	Other Contributing Action
384	19.674	06/13/12	Wed	1505	Not Coded	0	0	1	Day	Dry	Other Contributing Action
385	19.674	06/14/12	Thu	1630	Rear-End	0	0	1	Day	Dry	No Contributing Action
386	19.674	06/22/12	Fri	1005	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
387	19.674	07/13/12	Fri	0730	Rear-End	0	3	0	Day	Wet	Other Contributing Action
388	19.674	08/20/12	Mon	2213	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
389	19.674	09/04/12	Tue	1254	Sideswipe	0	0	1	Day	Dry	No Contributing Action
390	19.674	09/05/12	Wed	0745	Rear-End	0	1	0	Day	Wet	Other Contributing Action
391	19.674	09/05/12	Wed	1351	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
392	19.674	10/01/12	Mon	1200	Rear-End	0	0	1	Day	Wet	Other Contributing Action
393	19.674	10/05/12	Fri	1050	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
394	19.674	12/07/12	Fri	1035	Not Coded	0	0	1	Day	Dry	No Contributing Action
395	19.674	12/21/12	Fri	2136	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
396	19.674	10/25/12	Thu	1650	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
397	19.677	03/30/12	Fri	1320	Rear-End	0	0	1	Day	Dry	No Contributing Action
398	19.683	03/06/12	Tue	1045	Not Coded	0	1	0	Day	Dry	Other Contributing Action
399	19.690	03/02/12	Fri	1240	Sideswipe	0	2	0	Day	Dry	Failed To Keep In Proper Lane
400	19.693	11/04/12	Sun	0103	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
401	19.693	11/19/12	Mon	1930	Angle	0	0	1	Night	Dry	Followed too Closely
402	19.703	11/26/12	Mon	1459	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
403	19.765	02/03/12	Fri	1506	Sideswipe	0	0	1	Day	Dry	Improper Passing
404	19.769	04/16/12	Mon	1145	Rear-End	0	0	1	Day	Dry	No Contributing Action
405	19.794	05/30/12	Wed	1815	Other Non-Collision	0	0	1	Day	Dry	#N/A

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000													STATE ROUTE: 112						
ROADWAY LIMITS: MP 0.000 to MP 0.500													M.P. 0.000		TO 4.910		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2012													TO 12/ 2012		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE					FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)				
406	19.794	02/13/12	Mon	1530	Not Coded					0	0	1	Day	Dry	No Contributing Action				
407	19.794	09/16/12	Sun	0845	Tree (Standing)					0	0	1	Night	Dry	Careless or Negligent Manner				
408	19.794	12/07/12	Fri	1649	Not Coded					0	0	1	Day	Dry	Other Contributing Action				
409	19.796	05/03/12	Thu	0107	Other Fixed Object					0	0	1	Night	Dry	Careless or Negligent Manner				
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
409	4	112	295	194	0	53	10	1	58	0	5	7	34	0	40				
Percent	0.98%	27.38%	72.13%	47.43%	0.00%	12.96%	2.44%	0.24%	14.18%	0.00%	1.22%	1.71%	8.31%	0.00%	9.78%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	292	117	67	342	191	38	4	1	3	14	0	1	0	8	0				
Percent	71.39%	28.61%	16.38%	83.62%	46.70%	9.29%	0.98%	0.24%	0.73%	3.42%	0.00%	0.24%	0.00%	1.96%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 105,084										SEGMENT CRASH RATE: 2.172 CRASHES PER MILLION VEHICLE MILES									

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: **87004000** STATE ROUTE: **112**
 ROADWAY LIMITS: **MP 0.000 to MP 0.500** M.P. **0.000** TO **4.910** ENGINEER: **FDOT D6**
 STUDY PERIOD: FROM **1/ 2013** TO **12/ 2013** COUNTY: **Miami-Dade**

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	3.862	01/06/13	Sun	1345	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
2	3.862	03/15/13	Fri	2309	Backed Into	0	0	1	Night	Dry	Improper Backing
3	3.862	07/24/13	Wed	0915	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
4	3.867	10/22/13	Tue	1655	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
5	3.886	08/10/13	Sat	1710	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
6	3.943	08/13/13	Tue	2320	Concrete Traffic Barrier	0	2	0	Night	Dry	Careless or Negligent Manner
7	3.947	02/16/13	Sat	1846	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
8	3.962	10/08/13	Tue	1830	Rear-End	0	1	0	Night	Dry	#N/A
9	3.971	07/07/13	Sun	1800	Rear-End	0	0	1	Day	Dry	Other Contributing Action
10	4.026	05/30/13	Thu	1737	Rear-End	0	0	1	Day	Wet	Failed to Yield Right-Of-Way
11	4.055	07/26/13	Fri	1555	Thrown or Falling Object	0	0	1	Day	Dry	No Contributing Action
12	4.093	01/12/13	Sat	1855	Sideswipe	1	0	0	Night	Dry	#N/A
13	4.107	02/19/13	Tue	1745	Rear-End	0	2	0	Day	Dry	No Contributing Action
14	0.000	05/17/13	Fri	1035	Sideswipe	0	0	1	Day	Dry	#N/A
15	0.106	06/07/13	Fri	1750	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
16	0.175	10/12/13	Sat	0140	Other Non-Fixed Object	0	1	0	Night	Dry	Other Contributing Action
17	0.378	03/07/13	Thu	1115	Sideswipe	0	0	1	Day	Dry	No Contributing Action
18	0.378	05/26/13	Sun	1205	Guardrail Face	0	1	0	Day	Dry	Swerved Or Avoided
19	0.421	04/02/13	Tue	2015	Rear-End	0	1	0	Night	Dry	Failed to Yield Right-Of-Way
20	0.447	07/11/13	Thu	2145	Sideswipe	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
21	0.466	10/04/13	Fri	1120	Sideswipe	0	1	0	Day	Dry	Failed To Keep In Proper Lane
22	0.490	03/08/13	Fri	1853	Rear-End	0	0	1	Night	Dry	No Contributing Action
23	0.490	07/28/13	Sun	0130	Concrete Traffic Barrier	0	0	1	Night	Wet	Careless or Negligent Manner
24	0.505	08/20/13	Tue	1753	Sideswipe	0	0	1	Day	Dry	Improper Passing
25	0.508	01/07/13	Mon	0827	Rear-End	0	0	1	Day	Dry	No Contributing Action
26	0.521	10/14/13	Mon	1030	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
27	0.523	11/21/13	Thu	1913	Rear-End	0	0	1	Night	Wet	#N/A
28	0.527	04/26/13	Fri	0130	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
29	0.546	04/29/13	Mon	1700	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
30	0.546	04/01/13	Mon	1454	Rear-End	0	1	0	Day	Dry	No Contributing Action
31	0.546	06/17/13	Mon	2040	Concrete Traffic Barrier	0	0	1	Night	Dry	No Contributing Action
32	0.562	10/25/13	Fri	1330	Sideswipe	0	0	1	Day	Dry	Improper Passing
33	0.584	01/28/13	Mon	2150	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
34	0.586	09/12/13	Thu	0805	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
35	0.589	06/11/13	Tue	2352	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
36	0.603	01/07/13	Mon	1021	Concrete Traffic Barrier	0	1	0	Day	Wet	Other Contributing Action
37	0.637	10/06/13	Sun	1300	Guardrail End	0	5	0	Day	Dry	No Contributing Action
38	0.656	07/11/13	Thu	1725	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
39	0.736	07/10/13	Wed	1450	Sideswipe	0	1	0	Day	Dry	Careless or Negligent Manner
40	0.736	09/26/13	Thu	1930	Concrete Traffic Barrier	0	0	1	Night	Dry	Careless or Negligent Manner
41	0.755	05/10/13	Fri	0435	Sideswipe	0	0	1	Night	Wet	Disregarded other Road Markings
42	0.759	06/03/13	Mon	0420	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
43	0.789	05/01/13	Wed	1624	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
44	0.838	12/06/13	Fri	2235	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
45	0.854	11/22/13	Fri	0104	Pedestrian	1	0	0	Night	Wet	#N/A
46	0.880	11/25/13	Mon	1052	Sideswipe	0	0	1	Day	Dry	No Contributing Action
47	0.893	08/20/13	Tue	1230	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
48	0.986	12/09/13	Mon	1615	Rear-End	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
49	0.989	01/07/13	Mon	1625	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
50	0.989	01/21/13	Mon	1628	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
51	1.028	05/03/13	Fri	2055	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
52	1.028	09/17/13	Tue	0854	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
53	1.038	09/15/13	Sun	1105	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
54	1.089	05/10/13	Fri	1100	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
55	1.089	06/21/13	Fri	0935	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
56	1.089	10/26/13	Sat	0031	Concrete Traffic Barrier	0	0	1	Night	Dry	Careless or Negligent Manner
57	1.100	05/25/13	Sat	2011	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
58	1.108	02/28/13	Thu	1515	Rear-End	0	0	1	Day	Dry	#N/A
59	1.119	06/30/13	Sun	1750	Sideswipe	0	0	1	Day	Dry	Improper Passing
60	1.126	05/29/13	Wed	1345	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
61	1.138	06/15/13	Sat	0550	Concrete Traffic Barrier	0	0	1	Day	Dry	No Contributing Action
62	1.169	12/03/13	Tue	0930	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
63	1.187	05/15/13	Wed	1831	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
64	1.202	09/29/13	Sun	0916	Concrete Traffic Barrier	0	1	0	Day	Wet	Careless or Negligent Manner
65	1.240	05/16/13	Thu	1603	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
66	1.240	11/13/13	Wed	1335	Not Coded	0	0	1	Day	Wet	Drove too Fast for Conditions
67	1.259	08/08/13	Thu	1853	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
68	1.274	06/08/13	Sat	1531	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
69	1.275	04/20/13	Sat	1145	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
70	1.338	07/03/13	Wed	1925	Concrete Traffic Barrier	0	0	1	Day	Wet	Drove too Fast for Conditions
71	1.505	11/24/13	Sun	1840	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
72	1.547	11/09/13	Sat	0415	Not Coded	0	2	0	Night	Wet	#N/A
73	1.565	02/15/13	Fri	1720	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
74	1.565	05/20/13	Mon	1755	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
75	1.574	11/09/13	Sat	0900	Angle	0	1	0	Day	Dry	No Contributing Action
76	1.603	02/01/13	Fri	0500	Concrete Traffic Barrier	0	1	0	Night	Dry	Careless or Negligent Manner
77	1.603	07/25/13	Thu	1611	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
78	1.603	09/28/13	Sat	1443	Angle	0	0	1	Day	Dry	Over-Correcting/Over-Steering
79	1.641	01/23/13	Wed	1333	Sideswipe	0	1	0	Day	Dry	#N/A
80	1.641	02/14/13	Thu	0932	Pedestrian	0	1	0	Day	Dry	Careless or Negligent Manner
81	1.641	04/27/13	Sat	1035	Rear-End	0	1	0	Day	Dry	Erratic, Reckless or Aggressive

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
82	1.646	05/10/13	Fri	0025	Rear-End	0	0	1	Night	Dry	Other Contributing Action
83	1.646	08/26/13	Mon	2040	Angle	0	3	0	Night	Dry	Other Contributing Action
84	1.646	12/04/13	Wed	0745	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
85	1.698	06/19/13	Wed	0615	Not Coded	0	1	0	Day	Dry	No Contributing Action
86	1.716	12/25/13	Wed	0335	Concrete Traffic Barrier	0	0	1	Night	Dry	#N/A
87	1.735	10/18/13	Fri	2316	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
88	1.746	05/22/13	Wed	0900	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
89	1.746	06/03/13	Mon	0920	Sideswipe	0	0	1	Day	Dry	#N/A
90	1.796	04/09/13	Tue	0120	Ditch	0	0	1	Night	Dry	Swerved Or Avoided
91	1.796	09/21/13	Sat	0456	Pedestrian	0	1	0	Night	Dry	#N/A
92	1.806	11/23/13	Sat	2345	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
93	1.846	04/06/13	Sat	1300	Angle	0	0	1	Day	Dry	#N/A
94	1.846	08/08/13	Thu	0910	Other Non-Fixed Object	0	0	1	Day	Dry	#N/A
95	1.846	09/22/13	Sun	2245	Guardrail Face	0	1	0	Night	Dry	Other Contributing Action
96	1.946	05/19/13	Sun	0743	Tree (Standing)	0	0	1	Day	Dry	#N/A
97	2.046	01/02/13	Wed	0208	Concrete Traffic Barrier	0	1	0	Night	Dry	Failed To Keep In Proper Lane
98	2.046	03/19/13	Tue	0400	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
99	2.046	03/11/13	Mon	1800	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
100	2.046	05/18/13	Sat	0130	Guardrail Face	0	0	1	Night	Wet	#N/A
101	2.046	05/03/13	Fri	2045	Pedestrian	1	0	0	Night	Dry	#N/A
102	2.046	05/11/13	Sat	1335	Sideswipe	0	0	1	Day	Dry	#N/A
103	2.046	08/02/13	Fri	0610	Guardrail Face	0	0	1	Night	Dry	Careless or Negligent Manner
104	2.046	05/20/13	Mon	1635	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
105	2.046	07/13/13	Sat	0410	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
106	2.046	08/08/13	Thu	1720	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
107	2.046	12/14/13	Sat	0422	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
108	2.296	05/30/13	Thu	1440	Concrete Traffic Barrier	0	0	1	Day	Wet	#N/A
109	2.486	07/13/13	Sat	0330	Not Coded	0	2	0	Night	Dry	No Contributing Action
110	2.546	08/29/13	Thu	2130	Rear-End	0	0	1	Night	Dry	No Contributing Action
111	2.546	01/07/13	Mon	0840	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
112	2.546	01/22/13	Tue	1405	Guardrail Face	0	0	1	Day	Dry	Careless or Negligent Manner
113	2.546	04/23/13	Tue	1110	Cargo/Equipment Loss or Shift	0	0	1	Day	Dry	No Contributing Action
114	2.546	07/13/13	Sat	0400	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
115	2.546	06/12/13	Wed	1350	Not Coded	0	0	1	Day	Wet	Careless or Negligent Manner
116	2.546	06/12/13	Wed	1500	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
117	2.546	06/12/13	Wed	1500	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
118	2.546	06/30/13	Sun	1451	Sideswipe	0	3	0	Day	Dry	No Contributing Action
119	2.546	08/16/13	Fri	0620	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
120	2.546	11/15/13	Fri	1924	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
121	2.546	10/19/13	Sat	0735	Guardrail Face	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
122	2.546	11/03/13	Sun	0125	Angle	0	0	1	Night	Dry	Careless or Negligent Manner
123	2.546	10/24/13	Thu	2110	Overturn/Rollover	0	1	0	Night	Dry	Careless or Negligent Manner
124	3.230	10/23/13	Wed	1407	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
125	3.730	02/07/13	Thu	0851	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
126	3.730	01/07/13	Mon	1738	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
127	3.730	02/16/13	Sat	1434	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
128	3.730	02/19/13	Tue	0828	Rear-End	0	2	0	Day	Dry	No Contributing Action
129	3.730	04/07/13	Sun	1720	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
130	3.730	04/06/13	Sat	1915	Guardrail Face	0	3	0	Night	Dry	Other Contributing Action
131	3.730	04/07/13	Sun	1710	Sideswipe	0	4	0	Day	Dry	Careless or Negligent Manner
132	3.730	05/28/13	Tue	0615	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
133	3.730	05/28/13	Tue	0615	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
134	3.730	05/28/13	Tue	0816	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
135	3.730	07/11/13	Thu	1420	Guardrail Face	0	1	0	Day	Dry	No Contributing Action
136	3.730	08/09/13	Fri	1452	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
137	3.730	09/12/13	Thu	0633	Sideswipe	0	0	1	Night	Wet	Other Contributing Action
138	3.730	12/04/13	Wed	1730	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
139	3.730	12/26/13	Thu	2231	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
140	4.177	03/22/13	Fri	1508	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
141	4.230	02/15/13	Fri	1205	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
142	4.230	02/07/13	Thu	1745	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
143	4.230	02/22/13	Fri	1555	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
144	4.230	01/26/13	Sat	1800	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
145	4.230	05/08/13	Wed	0958	Pedestrian	0	2	0	Day	Dry	No Contributing Action
146	4.230	03/07/13	Thu	1814	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
147	4.230	04/22/13	Mon	2340	Concrete Traffic Barrier	0	0	1	Night	Dry	#N/A
148	4.230	04/27/13	Sat	0600	Rear-End	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
149	4.230	10/28/13	Mon	0800	Guardrail Face	0	2	0	Day	Dry	Careless or Negligent Manner
150	4.230	10/19/13	Sat	1607	Sideswipe	0	0	1	Day	Dry	#N/A
151	4.230	09/22/13	Sun	1215	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
152	4.230	12/04/13	Wed	0933	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
153	4.230	09/29/13	Sun	0938	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
154	4.230	12/16/13	Mon	0120	Angle	0	2	0	Night	Dry	Careless or Negligent Manner
155	4.287	02/15/13	Fri	1813	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
156	4.287	02/26/13	Tue	1600	Other Non-Fixed Object	0	0	1	Day	Dry	Careless or Negligent Manner
157	4.423	04/30/13	Tue	0930	Rear-End	0	0	1	Day	Dry	#N/A
158	4.423	12/30/13	Mon	1444	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
159	4.430	05/06/13	Mon	2350	Rear-End	0	4	0	Night	Dry	Other Contributing Action
160	4.430	05/25/13	Sat	2310	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
161	4.430	10/04/13	Fri	1030	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
162	4.446	05/07/13	Tue	1435	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
163	4.446	02/16/13	Sat	1530	Overturn/Rollover	0	0	1	Day	Dry	Careless or Negligent Manner
164	4.480	02/02/13	Sat	1527	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
165	4.480	02/03/13	Sun	1520	Overturn/Rollover	0	1	0	Day	Dry	No Contributing Action
166	4.480	04/06/13	Sat	1500	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
167	4.480	05/11/13	Sat	1730	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
168	4.480	09/23/13	Mon	0820	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
169	4.480	10/30/13	Wed	1242	Sideswipe	0	1	0	Day	Dry	Swerved Or Avoided
170	4.480	10/05/13	Sat	1617	Guardrail Face	0	0	1	Day	Dry	Failed To Keep In Proper Lane
171	4.480	10/13/13	Sun	0143	Angle	0	0	1	Night	Dry	No Contributing Action
172	4.480	10/13/13	Sun	0203	Angle	0	0	1	Night	Dry	#N/A
173	4.480	03/04/13	Mon	1938	Sideswipe	0	0	1	Night	Dry	Improper Passing
174	4.510	07/19/13	Fri	0430	Rear-End	0	0	1	Night	Wet	#N/A
175	4.530	02/06/13	Wed	2015	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
176	4.530	09/02/13	Mon	0740	Concrete Traffic Barrier	0	1	0	Day	Dry	Careless or Negligent Manner
177	4.530	11/04/13	Mon	0915	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
178	4.537	03/21/13	Thu	0910	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
179	4.541	11/17/13	Sun	1555	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
180	4.541	11/17/13	Sun	1508	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
181	4.541	11/23/13	Sat	0342	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
182	4.541	11/22/13	Fri	0846	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
183	4.541	11/22/13	Fri	0949	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
184	4.541	11/17/13	Sun	1555	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
185	4.560	03/22/13	Fri	2005	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
186	4.560	05/07/13	Tue	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
187	4.560	12/27/13	Fri	2350	Rear-End	0	0	1	Night	Wet	#N/A
188	4.578	03/19/13	Tue	0830	Rear-End	0	0	1	Day	Wet	No Contributing Action
189	4.578	03/20/13	Wed	0930	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
190	4.578	04/20/13	Sat	1710	Rear-End	0	0	1	Day	Dry	#N/A
191	4.578	08/22/13	Thu	1900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
192	4.630	03/01/13	Fri	2308	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
193	4.630	03/26/13	Tue	1449	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
194	4.630	03/16/13	Sat	1743	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
195	4.630	08/29/13	Thu	1055	Other Non-Collision	0	0	1	Day	Dry	Other Contributing Action
196	4.630	08/29/13	Thu	1130	Rear-End	0	0	1	Day	Dry	#N/A
197	4.630	09/18/13	Wed	0745	Rear-End	0	1	0	Day	Dry	No Contributing Action
198	4.635	02/16/13	Sat	1618	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
199	4.635	08/01/13	Thu	1000	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
200	4.635	04/23/13	Tue	0915	Other Non-Fixed Object	0	1	0	Day	Dry	Careless or Negligent Manner
201	4.635	06/22/13	Sat	1745	Sideswipe	0	0	1	Day	Dry	#N/A
202	4.635	12/28/13	Sat	1815	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
203	4.635	07/19/13	Fri	1015	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
204	4.654	01/05/13	Sat	1925	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
205	4.654	04/14/13	Sun	1440	Rear-End	0	6	0	Day	Dry	Careless or Negligent Manner
206	4.654	06/20/13	Thu	1553	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
207	4.654	11/10/13	Sun	1310	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
208	4.673	12/23/13	Mon	1217	Guardrail End	0	1	0	Day	Dry	Careless or Negligent Manner
209	4.673	12/31/13	Tue	1150	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
210	4.673	12/10/13	Tue	0943	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
211	4.683	12/02/13	Mon	1720	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
212	4.692	05/06/13	Mon	1540	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
213	4.692	06/24/13	Mon	1537	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
214	4.692	08/17/13	Sat	2255	Sideswipe	0	2	0	Night	Wet	Careless or Negligent Manner
215	4.692	10/08/13	Tue	0830	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
216	4.692	10/25/13	Fri	1850	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
217	4.692	01/13/13	Sun	1454	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
218	4.702	11/08/13	Fri	0905	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
219	4.702	08/01/13	Thu	0805	Not Coded	0	0	1	Day	Dry	#N/A
220	4.702	06/11/13	Tue	1420	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
221	4.711	02/11/13	Mon	1202	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
222	4.711	02/15/13	Fri	1504	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
223	4.711	03/17/13	Sun	1900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
224	4.711	09/05/13	Thu	1321	Sideswipe	0	0	1	Day	Dry	Improper Passing
225	4.718	07/17/13	Wed	1435	Rear-End	0	2	0	Day	Wet	Careless or Negligent Manner
226	4.721	04/26/13	Fri	0550	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
227	4.722	05/27/13	Mon	0020	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
228	4.724	07/06/13	Sat	0055	Not Coded	0	0	1	Night	Dry	Other Contributing Action
229	4.728	04/04/13	Thu	1026	Rear-End	0	0	1	Day	Dry	No Contributing Action
230	4.728	06/13/13	Thu	0842	Rear-End	0	0	1	Day	Dry	No Contributing Action
231	4.787	07/24/13	Wed	1912	Angle	0	3	0	Day	Dry	No Contributing Action
232	4.791	07/16/13	Tue	1235	Rear-End	0	0	1	Day	Dry	No Contributing Action
233	4.815	03/19/13	Tue	0950	Rear-End	0	1	0	Night	Dry	No Contributing Action
234	4.860	03/28/13	Thu	0520	Concrete Traffic Barrier	0	0	1	Night	Dry	Careless or Negligent Manner
235	4.873	12/02/13	Mon	0510	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
236	4.891	03/15/13	Fri	0852	Sideswipe	0	0	1	Day	Dry	No Contributing Action
237	4.905	05/01/13	Wed	1005	Rear-End	0	0	1	Day	Dry	No Contributing Action
238	4.906	12/15/13	Sun	2240	Not Coded	0	0	1	Night	Wet	#N/A
239	4.906	09/26/13	Thu	0556	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
240	4.909	02/20/13	Wed	1442	Parked Motor Vehicle	0	0	1	Day	Dry	No Contributing Action
241	4.910	08/03/13	Sat	0502	Guardrail Face	0	0	1	Night	Dry	No Contributing Action
242	0.392	04/26/13	Fri	1615	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
243	0.392	09/17/13	Tue	1427	Rear-End	0	0	1	Day	Dry	#N/A

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: **87004000** STATE ROUTE: **112**
ROADWAY LIMITS: **MP 0.000 to MP 0.500** M.P. **0.000** TO **4.910** ENGINEER: **FDOT D6**
STUDY PERIOD: FROM **1/ 2013** TO **12/ 2013** COUNTY: **Miami-Dade**

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
244	0.000	05/28/13	Tue	0725	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
245	0.000	07/22/13	Mon	0730	Rear-End	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
246	0.000	07/26/13	Fri	1210	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
247	0.000	03/13/13	Wed	0540	Guardrail Face	0	0	1	Night	Dry	#N/A
248	0.000	03/19/13	Tue	0945	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
249	0.000	03/05/13	Tue	1720	Rear-End	0	0	1	Day	Dry	No Contributing Action
250	0.000	02/15/13	Fri	1345	Sideswipe	0	0	1	Day	Wet	Careless or Negligent Manner
251	0.000	05/13/13	Mon	1630	Guardrail Face	0	0	1	Day	Dry	Improper Passing
252	0.000	10/16/13	Wed	0843	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
253	0.000	12/13/13	Fri	2220	Concrete Traffic Barrier	0	1	0	Night	Dry	Other Contributing Action
254	0.000	07/31/13	Wed	0850	Not Coded	0	0	1	Night	Wet	Failed To Keep In Proper Lane
255	0.000	01/10/13	Thu	2245	Not Coded	0	1	0	Night	Dry	Ran Red Light
256	0.000	09/22/13	Sun	1234	Sideswipe	0	0	1	Day	Dry	#N/A
257	0.076	11/18/13	Mon	0700	Sideswipe	0	0	1	Day	Dry	#N/A
258	0.085	11/07/13	Thu	0815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
259	0.102	08/26/13	Mon	1345	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
260	0.104	07/07/13	Sun	1150	Angle	0	1	0	Day	Dry	Ran Red Light
261	0.104	09/21/13	Sat	1230	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
262	0.104	07/22/13	Mon	0520	Concrete Traffic Barrier	0	0	1	Night	Dry	#N/A
263	0.104	01/06/13	Sun	1805	Angle	0	0	1	Day	Dry	No Contributing Action
264	0.104	02/18/13	Mon	2200	Angle	0	0	1	Night	Dry	Ran Red Light
265	0.104	11/19/13	Tue	0910	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
266	0.104	12/11/13	Wed	2140	Rear-End	0	0	1	Night	Dry	No Contributing Action
267	0.187	03/30/13	Sat	2240	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
268	0.000	07/21/13	Sun	1608	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
269	0.000	07/19/13	Fri	0841	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
270	0.000	05/16/13	Thu	1541	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
271	0.000	08/18/13	Sun	0004	Rear-End	0	0	1	Night	Dry	Followed too Closely
272	0.000	07/26/13	Fri	1352	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
273	0.000	09/21/13	Sat	1025	Sideswipe	0	0	1	Day	Dry	#N/A
274	0.000	08/19/13	Mon	0831	Rear-End	0	1	0	Day	Dry	No Contributing Action
275	0.000	09/20/13	Fri	0835	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
276	0.113	03/09/13	Sat	1546	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
277	0.113	03/09/13	Sat	1547	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
278	0.207	10/04/13	Fri	1000	Angle	0	0	1	Day	Dry	No Contributing Action
279	0.261	09/30/13	Mon	0922	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
280	0.313	10/31/13	Thu	1948	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
281	0.366	07/22/13	Mon	1635	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
282	0.394	10/29/13	Tue	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
283	0.075	01/25/13	Fri	1545	Sideswipe	0	0	1	Day	Dry	Improper Passing
284	0.075	05/02/13	Thu	1736	Not Coded	0	0	1	Day	Wet	Followed too Closely
285	0.075	09/23/13	Mon	1656	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
286	0.000	05/21/13	Tue	2238	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
287	0.000	07/24/13	Wed	1822	Right-Turn	0	0	1	Day	Dry	No Contributing Action
288	0.000	03/20/13	Wed	1755	Left-Turn	0	1	0	Day	Wet	Failed to Yield Right-Of-Way
289	0.000	09/04/13	Wed	2220	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
290	0.001	07/03/13	Wed	1236	Rear-End	0	0	1	Day	Dry	No Contributing Action
291	0.005	04/18/13	Thu	1030	Rear-End	0	2	0	Day	Dry	Other Contributing Action
292	0.005	11/27/13	Wed	0951	Rear-End	0	0	1	Day	Wet	No Contributing Action
293	0.009	04/03/13	Wed	1340	Sideswipe	0	0	1	Day	Dry	No Contributing Action
294	0.009	02/23/13	Sat	2359	Rear-End	0	0	1	Night	Dry	Other Contributing Action
295	0.009	03/22/13	Fri	1519	Rear-End	0	2	0	Day	Dry	Followed too Closely
296	0.011	11/27/13	Wed	1237	Rear-End	0	6	0	Day	Dry	No Contributing Action
297	0.019	02/14/13	Thu	0850	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
298	0.039	03/15/13	Fri	1441	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
299	0.050	05/19/13	Sun	1532	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
300	13.432	02/08/13	Fri	1238	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
301	13.447	03/10/13	Sun	1253	Rear-End	0	0	1	Day	Dry	Followed too Closely
302	13.449	05/31/13	Fri	1137	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
303	13.449	06/18/13	Tue	1805	Left-Turn	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
304	13.449	06/27/13	Thu	0630	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
305	13.449	07/10/13	Wed	0810	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
306	13.449	07/11/13	Thu	0845	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
307	13.449	08/08/13	Thu	0740	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
308	13.449	08/16/13	Fri	1453	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
309	13.449	08/30/13	Fri	1538	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
310	13.449	09/08/13	Sun	2052	Not Coded	0	2	0	Night	Dry	No Contributing Action
311	13.449	01/31/13	Thu	1730	Sideswipe	0	0	1	Unknown	Dry	Improper Backing
312	13.449	02/26/13	Tue	0750	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
313	13.449	03/19/13	Tue	1925	Angle	0	1	0	Night	Dry	Failed to Yield Right-Of-Way
314	13.449	03/27/13	Wed	1640	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
315	13.449	04/23/13	Tue	1830	Angle	0	1	0	Day	Dry	Careless or Negligent Manner
316	13.449	05/06/13	Mon	1220	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
317	13.449	09/26/13	Thu	1815	Angle	0	0	1	Day	Dry	No Contributing Action
318	13.449	10/09/13	Wed	1155	Rear-End	0	0	1	Day	Dry	Followed too Closely
319	13.449	11/14/13	Thu	1630	Angle	0	0	1	Night	Dry	No Contributing Action
320	13.449	11/30/13	Sat	1438	Left-Turn	0	0	1	Day	Dry	Careless or Negligent Manner
321	13.449	12/18/13	Wed	2350	Sideswipe	0	0	1	Day	Dry	Improper Turn
322	13.449	12/30/13	Mon	1420	Backed Into	0	0	1	Day	Dry	No Contributing Action
323	13.449	12/23/13	Mon	1101	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
324	13.449	12/23/13	Mon	1101	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
325	13.449	12/18/13	Wed	1150	Sideswipe	0	0	1	Day	Dry	Improper Turn
326	13.449	12/26/13	Thu	1315	Angle	0	0	1	Day	Wet	Failed to Yield Right-Of-Way
327	13.449	12/27/13	Fri	1512	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
328	13.451	04/20/13	Sat	1353	Not Coded	0	0	1	Day	Dry	Other Contributing Action
329	13.452	07/15/13	Mon	1030	Not Coded	0	0	1	Day	Dry	Other Contributing Action
330	13.480	05/21/13	Tue	1432	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
331	13.480	05/31/13	Fri	1557	Angle	0	0	1	Day	Dry	Disregarded other Road Markings
332	13.480	06/11/13	Tue	0815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
333	13.480	06/18/13	Tue	1934	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
334	13.480	06/29/13	Sat	1725	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
335	13.480	08/15/13	Thu	0010	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
336	13.480	08/28/13	Wed	0940	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
337	13.480	09/13/13	Fri	1711	Rear-End	0	0	1	Day	Dry	No Contributing Action
338	13.480	01/10/13	Thu	1225	Angle	0	0	1	Day	Dry	Other Contributing Action
339	13.480	02/12/13	Tue	0910	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
340	13.480	03/08/13	Fri	1236	Rear-End	0	0	1	Day	Dry	Improper Passing
341	13.480	04/03/13	Wed	0403	Left-Turn	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
342	13.480	04/15/13	Mon	1803	Left-Turn	0	0	1	Day	Wet	Ran Red Light
343	13.480	11/07/13	Thu	0835	Not Coded	0	0	1	Day	Dry	No Contributing Action
344	13.480	10/26/13	Sat	1931	Left-Turn	0	3	0	Night	Dry	Failed to Yield Right-Of-Way
345	13.480	11/15/13	Fri	1421	Pedestrian	0	2	0	Day	Wet	Failed to Yield Right-Of-Way
346	13.480	12/02/13	Mon	1308	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
347	13.480	12/02/13	Mon	1724	Angle	0	0	1	Night	Dry	Careless or Negligent Manner
348	13.480	12/05/13	Thu	2245	Sideswipe	0	0	1	Night	Dry	Improper Passing
349	13.480	12/30/13	Mon	1200	Rear-End	0	1	0	Day	Dry	Followed too Closely
350	13.481	04/11/13	Thu	1305	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
351	13.482	06/17/13	Mon	1700	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
352	13.485	10/30/13	Wed	1200	Sideswipe	0	0	1	Day	Dry	No Contributing Action
353	13.485	12/14/13	Sat	1130	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
354	13.486	11/08/13	Fri	1010	Rear-End	0	0	1	Day	Wet	Improper Passing
355	13.487	12/27/13	Fri	2045	Sideswipe	0	0	1	Night	Wet	Improper Turn
356	13.488	08/28/13	Wed	1020	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
357	13.489	10/05/13	Sat	1140	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
358	13.489	12/28/13	Sat	1953	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
359	13.495	03/15/13	Fri	1900	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
360	13.497	01/25/13	Fri	1630	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
361	13.499	02/12/13	Tue	0915	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
362	13.508	10/10/13	Thu	1135	Not Coded	0	0	1	Day	Dry	Other Contributing Action
363	13.508	11/06/13	Wed	0655	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
364	13.513	06/14/13	Fri	0934	Rear-End	0	1	0	Day	Dry	#N/A
365	13.537	04/26/13	Fri	0825	Rear-End	0	1	0	Day	Dry	Followed too Closely
366	13.541	02/08/13	Fri	1530	Rear-End	0	0	1	Day	Wet	Followed too Closely
367	13.567	06/21/13	Fri	2130	Rear-End	0	1	0	Night	Dry	Other Contributing Action
368	13.577	12/07/13	Sat	1720	Rear-End	0	0	1	Day	Dry	Followed too Closely
369	13.596	10/04/13	Fri	1410	Angle	0	0	1	Day	Dry	Other Contributing Action
370	13.602	06/11/13	Tue	1335	Rear-End	0	0	1	Day	Dry	No Contributing Action
371	13.602	02/01/13	Fri	0020	Rear-End	0	0	1	Day	Dry	Followed too Closely
372	13.605	05/18/13	Sat	2158	Left-Turn	0	2	0	Night	Dry	Careless or Negligent Manner
373	13.605	06/04/13	Tue	1623	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
374	13.605	06/10/13	Mon	0809	Angle	0	3	0	Day	Dry	Failed to Yield Right-Of-Way
375	13.605	06/19/13	Wed	1330	Rear-End	0	0	1	Day	Dry	Followed too Closely
376	13.605	06/26/13	Wed	1125	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
377	13.605	07/16/13	Tue	0830	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
378	13.605	08/13/13	Tue	1541	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
379	13.605	08/20/13	Tue	1530	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
380	13.605	09/10/13	Tue	1315	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
381	13.605	09/16/13	Mon	1125	Not Coded	0	0	1	Day	Dry	Improper Passing
382	13.605	09/17/13	Tue	0930	Angle	0	0	1	Day	Dry	Other Contributing Action
383	13.605	01/03/13	Thu	1315	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
384	13.605	01/06/13	Sun	1349	Not Coded	0	0	1	Night	Wet	No Contributing Action
385	13.605	01/11/13	Fri	1145	Pedalcycle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
386	13.605	02/01/13	Fri	0753	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
387	13.605	02/04/13	Mon	1703	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
388	13.605	02/11/13	Mon	1310	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
389	13.605	02/13/13	Wed	1758	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
390	13.605	02/19/13	Tue	1555	Rear-End	0	0	1	Day	Dry	Other Contributing Action
391	13.605	03/24/13	Sun	1300	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
392	13.605	03/29/13	Fri	1310	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
393	13.605	04/06/13	Sat	2125	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
394	13.605	04/09/13	Tue	1045	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
395	13.605	04/15/13	Mon	0830	Rear-End	0	0	1	Day	Dry	Followed too Closely
396	13.605	04/18/13	Thu	0815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
397	13.605	04/23/13	Tue	1500	Rear-End	0	0	1	Day	Dry	Followed too Closely
398	13.605	05/05/13	Sun	0220	Angle	0	4	0	Night	Dry	Careless or Negligent Manner
399	13.605	05/06/13	Mon	1235	Sideswipe	0	0	1	Day	Dry	No Contributing Action
400	13.605	05/07/13	Tue	1130	Angle	0	0	1	Day	Dry	No Contributing Action
401	13.605	09/27/13	Fri	1340	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
402	13.605	10/16/13	Wed	2050	Not Coded	0	0	1	Night	Dry	Other Contributing Action
403	13.605	11/09/13	Sat	2223	Not Coded	0	1	0	Night	Wet	#N/A
404	13.605	11/13/13	Wed	0800	Angle	0	0	1	Day	Dry	Failed To Keep In Proper Lane
405	13.605	11/15/13	Fri	1125	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: **87004000** STATE ROUTE: **112**
 ROADWAY LIMITS: **MP 0.000 to MP 0.500** M.P. **0.000** TO **4.910** ENGINEER: **FDOT D6**
 STUDY PERIOD: FROM **1/ 2013** TO **12/ 2013** COUNTY: **Miami-Dade**

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
406	13.605	11/16/13	Sat	1755	Angle	0	0	1	Night	Dry	Careless or Negligent Manner
407	13.605	04/12/13	Fri	1915	Sideswipe	0	0	1	Night	Dry	Other Contributing Action
408	13.605	12/05/13	Thu	1639	Overturn/Rollover	0	0	1	Day	Dry	Other Contributing Action
409	13.605	12/14/13	Sat	1800	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
410	13.605	12/20/13	Fri	2300	Jackknife	0	1	0	Night	Dry	Careless or Negligent Manner
411	13.605	12/23/13	Mon	1625	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
412	13.614	09/26/13	Thu	1716	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
413	2.552	10/20/13	Sun	1910	Parked Motor Vehicle	0	0	1	Night	Wet	#N/A
414	2.553	12/28/13	Sat	1930	Sideswipe	0	0	1	Night	Dry	No Contributing Action
415	2.553	03/15/13	Fri	2045	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
416	2.566	04/20/13	Sat	0032	Curb	0	0	1	Night	Wet	Over-Correcting/Over-Steering
417	2.572	04/01/13	Mon	1139	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
418	2.583	11/17/13	Sun	0430	Embankment	0	0	1	Night	Dry	Other Contributing Action
419	2.585	07/24/13	Wed	0730	Rear-End	0	0	1	Day	Dry	Followed too Closely
420	2.612	10/10/13	Thu	0850	Angle	0	0	1	Day	Dry	No Contributing Action
421	2.830	08/08/13	Thu	1859	Sideswipe	0	0	1	Day	Wet	Other Contributing Action
422	2.862	11/16/13	Sat	1338	Tree (Standing)	0	0	1	Day	Dry	Careless or Negligent Manner
423	0.000	10/14/13	Mon	1552	Sideswipe	0	0	1	Day	Dry	No Contributing Action
424	4.193	08/14/13	Wed	1030	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
425	4.193	01/30/13	Wed	1445	Left-Turn	0	0	1	Day	Dry	Careless or Negligent Manner
426	4.193	04/04/13	Thu	0750	Left-Turn	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
427	4.193	10/01/13	Tue	1210	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
428	4.245	05/28/13	Tue	0840	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
429	4.245	06/10/13	Mon	1511	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
430	4.245	06/17/13	Mon	1840	Pedestrian	0	2	0	Night	Dry	Other Contributing Action
431	19.655	08/15/13	Thu	0815	Angle	0	0	1	Day	Wet	Failed to Yield Right-Of-Way
432	19.655	02/02/13	Sat	1440	Left-Turn	0	0	1	Day	Dry	Careless or Negligent Manner
433	19.655	11/21/13	Thu	1300	Rear-End	0	0	1	Day	Dry	Improper Passing
434	19.665	04/10/13	Wed	1518	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
435	19.665	05/01/13	Wed	0935	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
436	19.665	11/20/13	Wed	2009	Pedestrian	0	2	0	Night	Wet	No Contributing Action
437	19.668	01/05/13	Sat	1624	Sideswipe	0	0	1	Day	Dry	No Contributing Action
438	19.668	09/28/13	Sat	2230	Angle	0	0	1	Night	Wet	Failed to Yield Right-Of-Way
439	19.674	08/02/13	Fri	1632	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
440	19.674	08/09/13	Fri	1935	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
441	19.674	02/11/13	Mon	0910	Angle	0	0	1	Day	Dry	Improper Turn
442	19.674	03/13/13	Wed	0850	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
443	19.674	03/11/13	Mon	1800	Right-Turn	0	1	0	Day	Dry	Erratic, Reckless or Aggressive
444	19.674	04/19/13	Fri	0615	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
445	19.674	04/22/13	Mon	0642	Left-Turn	0	2	0	Night	Wet	Failed to Yield Right-Of-Way
446	19.674	10/23/13	Wed	1740	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
447	19.674	11/23/13	Sat	1420	Not Coded	0	1	0	Day	Wet	No Contributing Action
448	19.676	10/20/13	Sun	0429	Rear-End	0	0	1	Night	Dry	Followed too Closely
449	19.683	07/15/13	Mon	1125	Rear-End	0	0	1	Day	Dry	Other Contributing Action
450	19.683	05/10/13	Fri	1508	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
451	19.688	04/21/13	Sun	2140	Angle	0	0	1	Night	Dry	Improper Turn
452	19.693	04/01/13	Mon	1835	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
453	19.704	06/11/13	Tue	1954	Angle	0	1	0	Night	Dry	Careless or Negligent Manner
454	19.731	08/27/13	Tue	0207	Not Coded	0	2	0	Day	Wet	Other Contributing Action
455	19.746	04/14/13	Sun	1730	Other Fixed Object	0	1	1	Day	Dry	Other Contributing Action
456	19.790	02/14/13	Thu	1100	Not Coded	0	0	1	Day	Dry	Improper Passing
457	19.794	04/07/13	Sun	1745	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
458	19.813	03/21/13	Thu	1453	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
459	19.843	08/19/13	Mon	1830	Sideswipe	0	0	1	Day	Dry	Improper Turn

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
459	3	129	327	226	0	50	18	2	68	2	9	2	39	0	31
Percent	0.65%	28.10%	71.24%	49.24%	0.00%	10.89%	3.92%	0.44%	14.81%	0.44%	1.96%	0.44%	8.50%	0.00%	6.75%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	329	129	81	378	217	42	6	4	2	13	2	2	0	11	0
Percent	71.68%	28.10%	17.65%	82.35%	47.28%	9.15%	1.31%	0.87%	0.44%	2.83%	0.44%	0.44%	0.00%	2.40%	0.00%

TOTAL ENTERING VEHICLES/ADT: **108,456**

SEGMENT CRASH RATE: **2.361** CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: **87004000** STATE ROUTE: **112**
 ROADWAY LIMITS: **MP 0.000 to MP 0.500** M.P. **0.000** TO **4.910** ENGINEER: **FDOT D6**
 STUDY PERIOD: FROM **1/ 2014** TO **12/ 2014** COUNTY: **Miami-Dade**

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	3.848	06/02/14	Mon	1430	Concrete Traffic Barrier	0	0	1	Day	Dry	#N/A
2	3.905	12/24/14	Wed	0710	Other Non-Fixed Object	0	0	1	Day	Dry	Other Contributing Action
3	3.934	11/19/14	Wed	1100	Sideswipe	0	0	1	Day	Wet	#N/A
4	3.943	05/21/14	Wed	0000	Sideswipe	0	3	0	Night	Dry	Other Contributing Action
5	3.971	12/06/14	Sat	2037	Concrete Traffic Barrier	0	0	1	Night	Dry	Other Contributing Action
6	4.098	03/31/14	Mon	1245	Sideswipe	0	0	1	Day	Dry	#N/A
7	4.112	06/09/14	Mon	1700	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
8	0.022	02/01/14	Sat	1849	Not Coded	0	0	1	Night	Dry	Improper Passing
9	0.096	08/27/14	Wed	1750	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
10	0.114	05/09/14	Fri	1630	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
11	0.225	03/04/14	Tue	0905	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
12	0.278	04/01/14	Tue	0940	Angle	0	1	0	Day	Dry	Failed To Keep In Proper Lane
13	0.313	02/08/14	Sat	0330	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
14	0.375	01/20/14	Mon	1230	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
15	0.380	12/26/14	Fri	0943	Sideswipe	0	0	1	Day	Wet	No Contributing Action
16	0.383	02/23/14	Sun	0300	Rear-End	0	0	1	Night	Dry	#N/A
17	0.429	06/06/14	Fri	1515	Angle	0	0	1	Day	Dry	Other Contributing Action
18	0.456	01/31/14	Fri	0102	Concrete Traffic Barrier	0	0	1	Night	Wet	Careless or Negligent Manner
19	0.456	10/07/14	Tue	0640	Impact Attenuator/Crash Cushion	0	1	0	Night	Dry	Careless or Negligent Manner
20	0.456	12/08/14	Mon	1900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
21	0.467	08/26/14	Tue	1320	Rear-End	0	2	0	Day	Dry	Improper Passing
22	0.562	09/23/14	Tue	1022	Rear-End	0	2	0	Day	Wet	Careless or Negligent Manner
23	0.580	06/09/14	Mon	2220	Sideswipe	0	0	1	Night	Dry	#N/A
24	0.581	07/23/14	Wed	1743	Rear-End	0	0	1	Day	Dry	#N/A
25	0.581	12/13/14	Sat	0410	Angle	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
26	0.581	12/05/14	Fri	1755	Rear-End	0	4	0	Night	Dry	Careless or Negligent Manner
27	0.585	09/22/14	Mon	0943	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
28	0.586	08/29/14	Fri	0830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
29	0.600	10/21/14	Tue	1425	Rear-End	0	2	0	Day	Wet	Failed To Keep In Proper Lane
30	0.619	08/20/14	Wed	1330	Angle	0	0	1	Day	Dry	Other Contributing Action
31	0.627	10/17/14	Fri	1038	Not Coded	0	1	0	Day	Dry	Failed To Keep In Proper Lane
32	0.637	03/16/14	Sun	2245	Rear-End	0	0	1	Night	Dry	#N/A
33	0.638	03/08/14	Sat	2130	Rear-End	0	0	1	Night	Dry	Other Contributing Action
34	0.638	11/20/14	Thu	2355	Rear-End	0	0	1	Night	Wet	Other Contributing Action
35	0.656	12/18/14	Thu	1715	Rear-End	0	3	0	Night	Dry	Failed To Keep In Proper Lane
36	0.675	10/31/14	Fri	0638	Other Fixed Object	0	1	0	Day	Dry	No Contributing Action
37	0.735	03/05/14	Wed	0835	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
38	0.736	11/13/14	Thu	1835	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
39	0.736	12/04/14	Thu	0018	Angle	0	0	1	Night	Dry	#N/A
40	0.755	07/31/14	Thu	0856	Sideswipe	0	0	1	Day	Dry	Improper Passing
41	0.755	07/18/14	Fri	1715	Not Coded	0	0	1	Day	Dry	#N/A
42	0.759	01/31/14	Fri	1520	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
43	0.778	06/30/14	Mon	2155	Angle	0	0	1	Night	Wet	#N/A
44	0.778	09/28/14	Sun	1030	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
45	0.778	10/25/14	Sat	2220	Concrete Traffic Barrier	0	0	1	Night	Dry	No Contributing Action
46	0.787	01/26/14	Sun	2010	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
47	0.790	01/13/14	Mon	0836	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
48	0.888	12/05/14	Fri	1310	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
49	0.888	12/19/14	Fri	1923	Sideswipe	0	2	0	Night	Dry	#N/A
50	0.938	11/11/14	Tue	2150	Angle	0	1	0	Night	Dry	Failed to Yield Right-Of-Way
51	0.960	12/07/14	Sun	0050	Rear-End	0	4	0	Night	Dry	Careless or Negligent Manner
52	0.968	09/14/14	Sun	1547	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
53	0.986	04/22/14	Tue	1510	Sideswipe	0	0	1	Day	Dry	#N/A
54	1.005	04/12/14	Sat	2046	Rear-End	0	0	1	Night	Dry	Drove too Fast for Conditions
55	1.028	02/12/14	Wed	2108	Sideswipe	0	0	1	Night	Wet	#N/A
56	1.028	10/15/14	Wed	1900	Rear-End	0	0	1	Night	Dry	#N/A
57	1.036	02/22/14	Sat	1218	Impact Attenuator/Crash Cushion	0	1	0	Day	Dry	Other Contributing Action
58	1.038	07/07/14	Mon	0900	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
59	1.038	09/18/14	Thu	1900	Sideswipe	0	2	0	Day	Wet	Other Contributing Action
60	1.038	09/14/14	Sun	1618	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
61	1.040	09/04/14	Thu	1800	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
62	1.046	05/08/14	Thu	0654	Other Non-Collision	0	0	1	Day	Dry	Over-Correcting/Over-Steering
63	1.081	02/08/14	Sat	2047	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
64	1.081	02/16/14	Sun	1419	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
65	1.081	12/03/14	Wed	1944	Sideswipe	0	0	1	Night	Dry	Other Contributing Action
66	1.091	11/21/14	Fri	1237	Concrete Traffic Barrier	0	1	0	Day	Wet	Over-Correcting/Over-Steering
67	1.108	04/24/14	Thu	0812	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
68	1.108	07/18/14	Fri	1955	Not Coded	0	1	0	Day	Wet	Drove too Fast for Conditions
69	1.108	08/27/14	Wed	1625	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
70	1.108	08/27/14	Wed	1620	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
71	1.110	07/29/14	Tue	0000	Parked Motor Vehicle	0	0	1	Day	Dry	#N/A
72	1.110	12/04/14	Thu	1135	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
73	1.138	06/27/14	Fri	0620	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
74	1.164	11/02/14	Sun	1535	Rear-End	0	2	0	Day	Dry	No Contributing Action
75	1.178	10/25/14	Sat	0550	Concrete Traffic Barrier	1	2	0	Night	Dry	No Contributing Action
76	1.183	03/04/14	Tue	0835	Sideswipe	0	1	0	Day	Dry	Failed To Keep In Proper Lane
77	1.183	03/04/14	Tue	0835	Rear-End	0	0	1	Day	Dry	#N/A
78	1.183	10/21/14	Tue	1930	Struck by Falling/Shifting Cargo	0	0	1	Night	Dry	Other Contributing Action
79	1.221	12/08/14	Mon	1845	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
80	1.225	12/15/14	Mon	1420	Sideswipe	0	0	1	Day	Dry	No Contributing Action
81	1.240	09/02/14	Tue	0900	Concrete Traffic Barrier	0	1	0	Day	Wet	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
82	1.240	10/31/14	Fri	1510	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
83	1.250	03/20/14	Thu	0832	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
84	1.259	01/21/14	Tue	2250	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
85	1.259	03/14/14	Fri	1120	Other Non-Fixed Object	0	1	0	Day	Dry	#N/A
86	1.259	08/18/14	Mon	2154	Sideswipe	0	3	0	Night	Dry	#N/A
87	1.259	11/11/14	Tue	1400	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
88	1.263	12/08/14	Mon	0930	Rear-End	0	0	1	Day	Dry	#N/A
89	1.272	09/02/14	Tue	1004	Rear-End	0	0	1	Day	Wet	#N/A
90	1.272	09/02/14	Tue	1000	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
91	1.278	02/16/14	Sun	1915	Rear-End	0	5	0	Night	Dry	Careless or Negligent Manner
92	1.284	05/24/14	Sat	1550	Not Coded	0	0	1	Day	Dry	No Contributing Action
93	1.306	10/23/14	Thu	0645	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
94	1.373	05/05/14	Mon	0127	Rear-End	0	5	0	Night	Dry	Careless or Negligent Manner
95	1.373	05/06/14	Tue	1858	Sideswipe	0	0	1	Day	Dry	#N/A
96	1.378	11/16/14	Sun	0900	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
97	1.418	09/15/14	Mon	1545	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
98	1.478	05/02/14	Fri	1541	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
99	1.478	08/05/14	Tue	0645	Concrete Traffic Barrier	0	1	0	Day	Dry	No Contributing Action
100	1.505	07/20/14	Sun	2330	Guardrail Face	0	0	1	Night	Dry	Careless or Negligent Manner
101	1.546	09/04/14	Thu	1638	Guardrail Face	0	1	0	Day	Dry	Careless or Negligent Manner
102	1.546	12/12/14	Fri	0715	Rear-End	0	0	1	Day	Dry	Other Contributing Action
103	1.565	03/12/14	Wed	0839	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
104	1.565	04/12/14	Sat	0930	Struck by Falling/Shifting Cargo	0	0	1	Day	Dry	#N/A
105	1.565	08/03/14	Sun	1450	Sideswipe	0	0	1	Day	Dry	Improper Passing
106	1.574	06/09/14	Mon	1825	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
107	1.584	03/20/14	Thu	1012	Sideswipe	0	0	1	Day	Dry	#N/A
108	1.584	07/24/14	Thu	1734	Pedalcycle	0	0	1	Day	Dry	No Contributing Action
109	1.603	04/23/14	Wed	0229	Rear-End	0	1	0	Night	Dry	#N/A
110	1.603	08/26/14	Tue	0412	Not Coded	0	1	0	Night	Wet	Erratic, Reckless or Aggressive
111	1.603	11/10/14	Mon	1520	Struck by Falling/Shifting Cargo	0	0	1	Night	Dry	#N/A
112	1.641	04/01/14	Tue	1017	Sideswipe	0	0	1	Day	Dry	Improper Passing
113	1.641	06/11/14	Wed	1340	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
114	1.641	08/01/14	Fri	1721	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
115	1.641	10/24/14	Fri	1151	Embankment	0	0	1	Day	Wet	Careless or Negligent Manner
116	1.646	03/27/14	Thu	1556	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
117	1.646	03/31/14	Mon	0620	Rear-End	0	0	1	Night	Dry	#N/A
118	1.679	09/24/14	Wed	0045	Other Traffic Barrier	0	1	0	Night	Wet	Failed To Keep In Proper Lane
119	1.698	09/13/14	Sat	0830	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
120	1.716	02/07/14	Fri	0455	Concrete Traffic Barrier	0	0	1	Night	Dry	No Contributing Action
121	1.716	06/01/14	Sun	1145	Other Fixed Object	0	2	0	Day	Dry	Careless or Negligent Manner
122	1.716	08/09/14	Sat	1329	Guardrail Face	0	1	0	Day	Dry	Failed To Keep In Proper Lane
123	1.716	11/07/14	Fri	0715	Rear-End	0	0	1	Day	Dry	Other Contributing Action
124	1.730	01/24/14	Fri	1907	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
125	1.735	05/23/14	Fri	1524	Rear-End	0	2	0	Day	Dry	No Contributing Action
126	1.735	05/24/14	Sat	1540	Angle	0	1	0	Day	Dry	Careless or Negligent Manner
127	1.746	04/19/14	Sat	0640	Rear-End	0	2	0	Day	Wet	Careless or Negligent Manner
128	1.746	04/19/14	Sat	0645	Rear-End	0	2	0	Day	Wet	Careless or Negligent Manner
129	1.746	01/13/14	Mon	2155	Pedestrian	0	0	1	Night	Dry	No Contributing Action
130	1.746	11/27/14	Thu	1010	Rear-End	0	0	1	Day	Dry	#N/A
131	1.746	11/06/14	Thu	0457	Concrete Traffic Barrier	0	0	1	Night	Dry	#N/A
132	1.778	09/28/14	Sun	0645	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
133	1.796	12/04/14	Thu	1910	Sideswipe	0	0	1	Night	Wet	#N/A
134	1.846	01/03/14	Fri	0920	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
135	1.846	02/08/14	Sat	2102	Guardrail Face	0	1	0	Night	Dry	Careless or Negligent Manner
136	1.846	04/01/14	Tue	0930	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
137	1.846	06/18/14	Wed	0920	Rear-End	0	0	1	Day	Dry	No Contributing Action
138	1.846	09/29/14	Mon	0215	Other Non-Fixed Object	0	0	1	Night	Dry	No Contributing Action
139	1.925	08/27/14	Wed	1315	Thrown or Falling Object	0	0	1	Day	Dry	Other Contributing Action
140	2.046	04/26/14	Sat	1713	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
141	2.046	07/04/14	Fri	1533	Angle	0	0	1	Day	Dry	Swerved Or Avoided
142	2.046	11/01/14	Sat	0458	Rear-End	0	1	0	Night	Dry	No Contributing Action
143	2.246	12/02/14	Tue	2034	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
144	2.278	11/21/14	Fri	0210	Rear-End	0	0	1	Night	Wet	Other Contributing Action
145	2.546	02/05/14	Wed	0823	Other Fixed Object	0	2	0	Day	Dry	Careless or Negligent Manner
146	2.546	02/14/14	Fri	1159	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
147	2.546	01/22/14	Wed	1635	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
148	2.546	07/02/14	Wed	0300	Rear-End	0	3	0	Night	Dry	Careless or Negligent Manner
149	2.546	04/09/14	Wed	0000	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
150	2.546	08/28/14	Thu	1700	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
151	2.546	12/16/14	Tue	1900	Rear-End	0	1	0	Night	Dry	Other Contributing Action
152	2.621	02/18/14	Tue	1800	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
153	2.730	04/18/14	Fri	1434	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
154	2.730	08/04/14	Mon	2001	Guardrail Face	0	1	0	Night	Wet	Failed To Keep In Proper Lane
155	2.730	12/02/14	Tue	2149	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
156	2.787	04/09/14	Wed	2053	Rear-End	0	1	0	Night	Dry	#N/A
157	3.046	12/01/14	Mon	1805	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
158	3.546	04/24/14	Thu	1950	Not Coded	0	0	1	Night	Dry	Failed To Keep In Proper Lane
159	3.546	08/16/14	Sat	0110	Other Non-Fixed Object	0	0	1	Night	Wet	Careless or Negligent Manner
160	3.730	02/02/14	Sun	1240	Guardrail Face	0	0	1	Day	Dry	Careless or Negligent Manner
161	3.730	01/03/14	Fri	1500	Rear-End	0	0	1	Day	Dry	Other Contributing Action
162	3.730	01/05/14	Sun	1543	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner

State of Florida Department of Transportation
CRASH SUMMARY

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
163	3.730	06/06/14	Fri	0716	Rear-End	0	0	1	Day	Dry	#N/A
164	3.730	06/19/14	Thu	1946	Overturn/Rollover	0	1	0	Night	Wet	Careless or Negligent Manner
165	3.730	07/25/14	Fri	0957	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
166	3.730	08/23/14	Sat	0728	Sideswipe	0	1	0	Day	Dry	Other Contributing Action
167	3.730	11/21/14	Fri	0241	Head-On	0	1	0	Night	Wet	Careless or Negligent Manner
168	3.730	12/02/14	Tue	0000	Guardrail End	0	0	1	Night	Dry	Other Contributing Action
169	3.730	12/04/14	Thu	1758	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
170	3.730	01/05/14	Sun	1456	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
171	3.730	09/29/14	Mon	1743	Struck by Falling/Shifting Cargo	0	0	1	Day	Dry	No Contributing Action
172	3.780	02/22/14	Sat	1640	Other Non-Collision	1	6	0	Day	Dry	Other Contributing Action
173	3.923	01/24/14	Fri	1053	Rear-End	0	0	1	Day	Dry	No Contributing Action
174	4.230	04/15/14	Tue	0830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
175	4.230	02/13/14	Thu	1440	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
176	4.230	01/05/14	Sun	2200	Guardrail Face	0	1	0	Night	Wet	Drove too Fast for Conditions
177	4.230	02/15/14	Sat	0525	Sideswipe	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
178	4.230	03/03/14	Mon	1515	Sideswipe	0	0	1	Day	Dry	#N/A
179	4.230	05/26/14	Mon	2330	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
180	4.230	05/09/14	Fri	0017	Overturn/Rollover	0	1	0	Night	Dry	No Contributing Action
181	4.230	07/08/14	Tue	0355	Guardrail Face	0	0	1	Night	Wet	#N/A
182	4.230	09/19/14	Fri	1625	Overturn/Rollover	0	0	1	Day	Wet	Careless or Negligent Manner
183	4.230	10/21/14	Tue	0500	Concrete Traffic Barrier	0	0	1	Night	Wet	#N/A
184	4.230	10/12/14	Sun	0000	Concrete Traffic Barrier	0	0	1	Night	Dry	Other Contributing Action
185	4.230	11/14/14	Fri	1525	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
186	4.287	02/14/14	Fri	1452	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
187	4.330	02/16/14	Sun	2255	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
188	4.423	05/23/14	Fri	1900	Not Coded	0	0	1	Night	Wet	Followed too Closely
189	4.430	02/03/14	Mon	1834	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
190	4.430	03/01/14	Sat	0700	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
191	4.430	05/21/14	Wed	0841	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
192	4.430	05/19/14	Mon	1440	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
193	4.446	08/18/14	Mon	0810	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
194	4.446	08/22/14	Fri	0951	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
195	4.446	01/28/14	Tue	1710	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
196	4.480	06/11/14	Wed	2320	Concrete Traffic Barrier	0	1	0	Night	Dry	Careless or Negligent Manner
197	4.530	04/30/14	Wed	0520	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
198	4.530	01/14/14	Tue	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
199	4.530	02/14/14	Fri	2032	Ran into Water/Canal	0	1	0	Night	Dry	Careless or Negligent Manner
200	4.530	01/31/14	Fri	1403	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
201	4.530	10/29/14	Wed	0910	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
202	4.530	12/29/14	Mon	1035	Guardrail End	0	1	0	Day	Dry	No Contributing Action
203	4.530	12/12/14	Fri	0030	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
204	4.537	02/24/14	Mon	1454	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
205	4.541	08/05/14	Tue	1515	Guardrail Face	0	1	0	Day	Dry	Careless or Negligent Manner
206	4.541	12/16/14	Tue	1722	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
207	4.541	03/23/14	Sun	0015	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
208	4.541	11/05/14	Wed	1633	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
209	4.546	02/24/14	Mon	1627	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
210	4.546	04/11/14	Fri	1737	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
211	4.546	11/11/14	Tue	1303	Not Coded	0	1	0	Day	Dry	Careless or Negligent Manner
212	4.550	06/18/14	Wed	0807	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
213	4.560	06/23/14	Mon	0100	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
214	4.560	08/28/14	Thu	2015	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
215	4.560	10/31/14	Fri	2250	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
216	4.560	12/09/14	Tue	0840	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
217	4.578	02/13/14	Thu	1140	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
218	4.578	12/31/14	Wed	1755	Rear-End	0	6	0	Night	Wet	Careless or Negligent Manner
219	4.597	06/06/14	Fri	0745	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
220	4.597	04/16/14	Wed	0912	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
221	4.597	04/23/14	Wed	1915	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
222	4.616	12/27/14	Sat	1350	Rear-End	0	6	0	Day	Dry	Careless or Negligent Manner
223	4.616	12/28/14	Sun	1400	Rear-End	0	4	0	Day	Dry	Careless or Negligent Manner
224	4.630	02/14/14	Fri	0946	Rear-End	0	0	1	Day	Dry	#N/A
225	4.630	03/27/14	Thu	0725	Sideswipe	0	0	1	Day	Dry	#N/A
226	4.630	06/20/14	Fri	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
227	4.630	07/05/14	Sat	1145	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
228	4.630	07/09/14	Wed	2151	Not Coded	0	0	1	Night	Wet	#N/A
229	4.635	10/23/14	Thu	0845	Rear-End	0	0	1	Day	Wet	No Contributing Action
230	4.635	05/16/14	Fri	0813	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
231	4.635	02/16/14	Sun	1330	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
232	4.635	08/27/14	Wed	1617	Sideswipe	0	0	1	Day	Dry	Improper Passing
233	4.635	11/10/14	Mon	0855	Rear-End	0	2	0	Day	Wet	Careless or Negligent Manner
234	4.635	11/10/14	Mon	1015	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
235	4.635	12/16/14	Tue	1819	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
236	4.654	07/05/14	Sat	1200	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
237	4.654	12/28/14	Sun	1455	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
238	4.665	07/23/14	Wed	1522	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
239	4.673	01/10/14	Fri	1440	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
240	4.673	01/11/14	Sat	1105	Concrete Traffic Barrier	0	1	0	Day	Dry	Careless or Negligent Manner
241	4.673	04/13/14	Sun	1326	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
242	4.673	04/27/14	Sun	1503	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
243	4.673	10/21/14	Tue	1606	Rear-End	0	1	0	Day	Wet	Followed too Closely

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
244	4.683	11/16/14	Sun	1514	Rear-End	0	7	0	Day	Dry	Careless or Negligent Manner
245	4.692	03/07/14	Fri	1445	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
246	4.692	06/09/14	Mon	0520	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
247	4.702	07/19/14	Sat	1922	Guardrail Face	0	1	0	Day	Dry	Careless or Negligent Manner
248	4.711	03/21/14	Fri	1942	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
249	4.711	03/21/14	Fri	1909	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
250	4.711	08/18/14	Mon	0917	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
251	4.711	11/15/14	Sat	1340	Guardrail Face	0	1	0	Day	Dry	Careless or Negligent Manner
252	4.711	12/26/14	Fri	1410	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
253	4.721	12/05/14	Fri	0826	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
254	4.722	01/22/14	Wed	0942	Angle	0	0	1	Day	Dry	No Contributing Action
255	4.727	02/16/14	Sun	1433	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
256	4.728	02/27/14	Thu	0710	Rear-End	0	2	0	Day	Wet	Careless or Negligent Manner
257	4.730	09/17/14	Wed	1718	Rear-End	0	0	1	Day	Dry	No Contributing Action
258	4.844	03/05/14	Wed	0845	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
259	4.882	02/07/14	Fri	1030	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
260	4.882	06/10/14	Tue	0845	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
261	4.882	09/22/14	Mon	1042	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
262	4.887	04/13/14	Sun	1220	Rear-End	0	4	0	Day	Dry	Careless or Negligent Manner
263	4.901	04/02/14	Wed	0030	Rear-End	0	0	1	Day	Dry	No Contributing Action
264	4.910	02/06/14	Thu	1544	Left-Turn	0	2	0	Day	Dry	Failed to Yield Right-Of-Way
265	4.910	06/28/14	Sat	0615	Angle	0	2	0	Day	Dry	Ran Red Light
266	0.392	09/05/14	Fri	1133	Sideswipe	0	1	0	Day	Dry	Careless or Negligent Manner
267	0.000	07/15/14	Tue	1530	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
268	0.000	11/19/14	Wed	0845	Sideswipe	0	0	1	Day	Dry	#N/A
269	0.000	05/06/14	Tue	1440	Pedestrian	0	0	1	Day	Dry	Careless or Negligent Manner
270	0.000	09/30/14	Tue	1144	Sideswipe	0	0	1	Day	Dry	No Contributing Action
271	0.000	02/02/14	Sun	1045	Rear-End	0	0	1	Day	Wet	#N/A
272	0.000	07/19/14	Sat	2225	Angle	0	0	1	Night	Dry	No Contributing Action
273	0.000	12/05/14	Fri	0027	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
274	0.000	11/14/14	Fri	1045	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
275	0.000	10/26/14	Sun	0249	Angle	0	0	1	Night	Dry	No Contributing Action
276	0.009	12/05/14	Fri	1105	Sideswipe	0	0	1	Day	Dry	No Contributing Action
277	0.047	04/04/14	Fri	0859	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
278	0.066	01/18/14	Sat	2000	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
279	0.085	02/16/14	Sun	1620	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
280	0.099	10/03/14	Fri	2238	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
281	0.102	08/01/14	Fri	1535	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
282	0.104	02/03/14	Mon	1500	Rear-End	0	0	1	Day	Dry	Other Contributing Action
283	0.104	02/12/14	Wed	0745	Rear-End	0	0	1	Day	Dry	Followed too Closely
284	0.104	08/04/14	Mon	1107	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
285	0.104	03/23/14	Sun	0652	Not Coded	0	0	1	Night	Dry	No Contributing Action
286	0.104	03/26/14	Wed	1330	Sideswipe	0	0	1	Day	Dry	Not Coded
287	0.104	11/25/14	Tue	1411	Rear-End	0	0	1	Day	Dry	Other Contributing Action
288	0.002	11/04/14	Tue	2330	Rear-End	0	0	1	Day	Dry	No Contributing Action
289	0.000	11/21/14	Fri	0230	Tree (Standing)	0	0	1	Night	Wet	#N/A
290	0.184	03/13/14	Thu	0200	Not Coded	0	1	0	Night	Wet	Careless or Negligent Manner
291	0.055	01/04/14	Sat	1300	Rear-End	0	1	0	Day	Wet	#N/A
292	0.187	06/12/14	Thu	0745	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
293	0.000	02/15/14	Sat	1028	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
294	0.000	02/21/14	Fri	1350	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
295	0.000	02/17/14	Mon	1220	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
296	0.000	08/28/14	Thu	1805	Other Non-Collision	1	0	0	Day	Dry	Improper Passing
297	0.000	11/26/14	Wed	1030	Guardrail Face	0	0	1	Day	Dry	Careless or Negligent Manner
298	0.000	03/19/14	Wed	0145	Guardrail End	0	0	1	Night	Dry	Careless or Negligent Manner
299	0.034	02/17/14	Mon	1337	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
300	0.313	11/16/14	Sun	1247	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
301	0.318	01/15/14	Wed	0749	Guardrail Face	0	0	1	Day	Wet	Careless or Negligent Manner
302	0.318	12/03/14	Wed	1553	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
303	0.385	03/22/14	Sat	1515	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
304	0.394	10/26/14	Sun	1454	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
305	0.413	10/21/14	Tue	0910	Rear-End	0	0	1	Day	Dry	#N/A
306	0.413	09/23/14	Tue	1015	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
307	0.019	01/16/14	Thu	1210	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
308	0.038	12/22/14	Mon	1915	Other Non-Collision	0	0	1	Night	Dry	No Contributing Action
309	0.075	01/13/14	Mon	1130	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
310	0.075	08/08/14	Fri	1827	Sideswipe	0	0	1	Night	Dry	No Contributing Action
311	0.000	03/06/14	Thu	1707	Rear-End	0	0	1	Day	Wet	No Contributing Action
312	0.000	09/28/14	Sun	1122	Not Coded	0	0	1	Day	Dry	#N/A
313	0.000	09/23/14	Tue	1555	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
314	0.000	12/17/14	Wed	1148	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
315	0.004	11/16/14	Sun	1136	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
316	0.028	04/02/14	Wed	0920	Rear-End	0	0	1	Day	Dry	#N/A
317	13.445	01/28/14	Tue	2145	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
318	13.445	03/04/14	Tue	1510	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
319	13.447	04/16/14	Wed	0635	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
320	13.449	01/10/14	Fri	1530	Angle	0	1	0	Day	Dry	Other Contributing Action
321	13.449	01/29/14	Wed	0909	Angle	0	0	1	Day	Wet	No Contributing Action
322	13.449	02/14/14	Fri	0820	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
323	13.449	03/20/14	Thu	1757	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
324	13.449	05/16/14	Fri	1345	Left-Turn	0	1	0	Day	Dry	No Contributing Action

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
325	13.449	05/06/14	Tue	1400	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
326	13.449	07/01/14	Tue	0920	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
327	13.449	07/12/14	Sat	1400	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
328	13.449	08/07/14	Thu	0745	Left-Turn	0	0	1	Day	Dry	No Contributing Action
329	13.449	09/01/14	Mon	1230	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
330	13.449	09/09/14	Tue	1646	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
331	13.449	09/22/14	Mon	1227	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
332	13.449	11/26/14	Wed	1630	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
333	13.449	01/23/14	Thu	1330	Angle	0	1	0	Day	Dry	Careless or Negligent Manner
334	13.449	02/06/14	Thu	1200	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
335	13.449	03/01/14	Sat	1911	Angle	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
336	13.449	03/21/14	Fri	1230	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
337	13.449	03/22/14	Sat	2357	Angle	0	1	0	Night	Dry	Failed to Yield Right-Of-Way
338	13.449	04/02/14	Wed	1815	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
339	13.449	04/04/14	Fri	1835	Left-Turn	0	0	1	Day	Dry	Improper Turn
340	13.449	04/04/14	Fri	0430	Pedestrian	0	1	0	Night	Dry	Not Coded
341	13.449	05/15/14	Thu	1835	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
342	13.449	07/30/14	Wed	1350	Rear-End	0	0	1	Day	Dry	No Contributing Action
343	13.449	08/28/14	Thu	1307	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
344	13.449	09/03/14	Wed	0835	Left-Turn	0	0	1	Day	Wet	Failed to Yield Right-Of-Way
345	13.449	09/11/14	Thu	1615	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
346	13.449	10/12/14	Sun	0058	Angle	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
347	13.449	10/14/14	Tue	1915	Angle	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
348	13.449	10/25/14	Sat	1653	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
349	13.449	11/23/14	Sun	0025	Rear-End	0	0	1	Night	Dry	#N/A
350	13.449	11/26/14	Wed	0837	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
351	13.449	12/17/14	Wed	0955	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
352	13.449	12/20/14	Sat	2000	Angle	0	1	0	Night	Dry	Failed to Yield Right-Of-Way
353	13.450	06/05/14	Thu	1412	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
354	13.458	07/21/14	Mon	1848	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
355	13.461	10/20/14	Mon	1715	Angle	0	1	0	Day	Wet	Other Contributing Action
356	13.461	09/03/14	Wed	0940	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
357	13.471	12/02/14	Tue	1345	Rear-End	0	0	1	Day	Dry	Followed too Closely
358	13.476	12/07/14	Sun	1540	Angle	0	0	1	Day	Dry	Other Contributing Action
359	13.479	05/14/14	Wed	1125	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
360	13.479	09/26/14	Fri	1030	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
361	13.480	12/04/14	Thu	2015	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
362	13.480	01/25/14	Sat	0030	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
363	13.480	02/08/14	Sat	0842	Angle	0	0	1	Day	Dry	No Contributing Action
364	13.480	03/27/14	Thu	1545	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
365	13.480	04/13/14	Sun	1948	Rear-End	0	0	1	Night	Wet	Followed too Closely
366	13.480	05/09/14	Fri	1335	Rear-End	0	1	0	Night	Dry	No Contributing Action
367	13.480	06/05/14	Thu	1245	Rear-End	0	0	1	Day	Dry	No Contributing Action
368	13.480	06/06/14	Fri	0020	Right-Turn	0	0	1	Night	Dry	Improper Turn
369	13.480	08/05/14	Tue	1640	Sideswipe	0	0	1	Day	Dry	Improper Passing
370	13.480	09/07/14	Sun	1450	Angle	0	0	1	Day	Dry	No Contributing Action
371	13.480	01/03/14	Fri	1622	Rear-End	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
372	13.480	03/01/14	Sat	1147	Sideswipe	0	0	1	Day	Dry	No Contributing Action
373	13.480	03/17/14	Mon	1920	Right-Turn	0	0	1	Night	Dry	Careless or Negligent Manner
374	13.480	03/30/14	Sun	0937	Left-Turn	0	1	0	Day	Dry	Careless or Negligent Manner
375	13.480	03/30/14	Sun	1940	Sideswipe	0	0	1	Night	Dry	No Contributing Action
376	13.480	04/24/14	Thu	1208	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
377	13.480	04/26/14	Sat	1815	Not Coded	0	0	1	Day	Dry	Improper Turn
378	13.480	05/01/14	Thu	1905	Rear-End	0	1	0	Day	Dry	Followed too Closely
379	13.480	06/08/14	Sun	0504	Angle	0	0	1	Night	Dry	Ran Red Light
380	13.480	07/05/14	Sat	2016	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
381	13.480	10/22/14	Wed	1441	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
382	13.480	10/26/14	Sun	2300	Rear-End	0	0	1	Night	Dry	Followed too Closely
383	13.480	10/28/14	Tue	1540	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
384	13.480	11/21/14	Fri	1830	Rear-End	0	0	1	Night	Dry	Followed too Closely
385	13.480	11/14/14	Fri	2109	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
386	13.480	12/09/14	Tue	0728	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
387	13.480	12/14/14	Sun	1500	Rear-End	0	0	1	Day	Dry	Followed too Closely
388	13.482	10/03/14	Fri	0910	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
389	13.482	10/22/14	Wed	1615	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
390	13.484	01/05/14	Sun	0158	Sideswipe	0	0	1	Night	Dry	Improper Turn
391	13.484	06/10/14	Tue	2130	Angle	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
392	13.485	09/03/14	Wed	0750	Not Coded	0	0	1	Day	Dry	No Contributing Action
393	13.486	11/17/14	Mon	0830	Angle	0	0	1	Night	Dry	Other Contributing Action
394	13.487	12/09/14	Tue	1928	Angle	0	0	1	Night	Dry	No Contributing Action
395	13.489	07/29/14	Tue	1715	Not Coded	0	2	0	Day	Dry	No Contributing Action
396	13.489	11/19/14	Wed	0550	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
397	13.489	12/04/14	Thu	1820	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
398	13.491	12/03/14	Wed	2130	Rear-End	0	2	0	Night	Dry	Other Contributing Action
399	13.500	11/04/14	Tue	0840	Rear-End	0	0	1	Day	Dry	Followed too Closely
400	13.508	03/02/14	Sun	0745	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
401	13.508	10/19/14	Sun	0330	Not Coded	0	0	1	Night	Dry	Other Contributing Action
402	13.508	04/02/14	Wed	1045	Rear-End	0	0	1	Day	Dry	Not Coded
403	13.518	11/05/14	Wed	1155	Sideswipe	0	1	0	Day	Wet	No Contributing Action
404	13.522	10/25/14	Sat	1445	Rear-End	0	0	1	Day	Dry	No Contributing Action
405	13.538	08/10/14	Sun	1805	Rear-End	0	0	1	Day	Dry	Followed too Closely

**State of Florida Department of Transportation
CRASH SUMMARY**

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STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
406	13.541	08/12/14	Tue	1620	Sideswipe	0	0	1	Day	Dry	No Contributing Action
407	13.541	03/01/14	Sat	1620	Left-Turn	0	0	1	Day	Dry	Careless or Negligent Manner
408	13.544	02/17/14	Mon	1151	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
409	13.550	11/04/14	Tue	1650	Rear-End	0	0	1	Day	Dry	Failed To Keep In Proper Lane
410	13.560	04/28/14	Mon	1454	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
411	13.574	05/19/14	Mon	0835	Angle	0	0	1	Day	Dry	Other Contributing Action
412	13.577	01/10/14	Fri	1500	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
413	13.586	12/11/14	Thu	1829	Sideswipe	0	0	1	Night	Dry	No Contributing Action
414	13.596	02/05/14	Wed	1905	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
415	13.603	03/22/14	Sat	1740	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
416	13.605	01/12/14	Sun	0405	Rear-End	0	0	1	Night	Dry	Wrong Side or Wrong Way
417	13.605	02/01/14	Sat	0010	Not Coded	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
418	13.605	06/02/14	Mon	1315	Rear-End	0	0	1	Day	Wet	Other Contributing Action
419	13.605	06/16/14	Mon	1535	Angle	0	0	1	Day	Dry	Ran Red Light
420	13.605	06/30/14	Mon	1000	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
421	13.605	07/11/14	Fri	0400	Angle	0	0	1	Night	Dry	Other Contributing Action
422	13.605	08/31/14	Sun	2045	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
423	13.605	09/03/14	Wed	1515	Angle	0	0	1	Day	Dry	Other Contributing Action
424	13.605	11/08/14	Sat	1758	Rear-End	0	0	1	Night	Dry	No Contributing Action
425	13.605	12/08/14	Mon	1515	Not Coded	0	0	1	Day	Dry	Improper Passing
426	13.605	01/17/14	Fri	0930	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
427	13.605	01/27/14	Mon	1505	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
428	13.605	02/16/14	Sun	1438	Sideswipe	0	0	1	Day	Dry	No Contributing Action
429	13.605	02/26/14	Wed	1520	Not Coded	0	0	1	Day	Dry	Failed To Keep In Proper Lane
430	13.605	03/11/14	Tue	1430	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
431	13.605	03/17/14	Mon	1050	Sideswipe	0	0	1	Day	Dry	Improper Passing
432	13.605	03/21/14	Fri	1030	Pedalcycle	0	1	0	Day	Dry	No Contributing Action
433	13.605	04/10/14	Thu	1745	Rear-End	0	0	1	Day	Dry	No Contributing Action
434	13.605	04/11/14	Fri	1700	Angle	0	0	1	Day	Dry	No Contributing Action
435	13.605	05/12/14	Mon	1625	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
436	13.605	05/21/14	Wed	1110	Angle	0	2	0	Day	Dry	Failed to Yield Right-Of-Way
437	13.605	06/02/14	Mon	1500	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
438	13.605	06/09/14	Mon	1405	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
439	13.605	07/16/14	Wed	1800	Sideswipe	0	0	1	Day	Dry	No Contributing Action
440	13.605	08/03/14	Sun	0132	Angle	0	1	0	Night	Wet	#N/A
441	13.605	08/12/14	Tue	1815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
442	13.605	09/07/14	Sun	1140	Left-Turn	0	0	1	Day	Dry	No Contributing Action
443	13.605	10/20/14	Mon	1350	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
444	13.605	10/29/14	Wed	1215	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
445	13.605	11/06/14	Thu	1435	Work Zone/Maintenance Equip.	0	0	1	Day	Dry	Followed too Closely
446	13.605	11/18/14	Tue	1126	Sideswipe	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
447	13.605	11/18/14	Tue	1236	Angle	0	0	1	Day	Dry	Other Contributing Action
448	13.605	12/15/14	Mon	1445	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
449	13.609	02/05/14	Wed	1550	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
450	13.633	03/05/14	Wed	1800	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
451	13.633	09/26/14	Fri	1850	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
452	2.533	01/26/14	Sun	1700	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
453	2.572	06/12/14	Thu	1608	Angle	0	0	1	Day	Wet	Careless or Negligent Manner
454	2.735	12/14/14	Sun	0535	Other Post, Pole Or Support	0	0	1	Night	Dry	Careless or Negligent Manner
455	2.768	03/19/14	Wed	1500	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
456	2.783	11/03/14	Mon	1715	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
457	2.805	02/12/14	Wed	1720	Sideswipe	0	0	1	Day	Wet	Other Contributing Action
458	2.805	06/04/14	Wed	1525	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
459	2.848	10/17/14	Fri	2210	Head-On	0	0	1	Night	Dry	Careless or Negligent Manner
460	2.862	02/23/14	Sun	1910	Rear-End	0	0	1	Night	Dry	Followed too Closely
461	4.193	01/26/14	Sun	0930	Sideswipe	0	1	0	Day	Dry	Careless or Negligent Manner
462	4.193	08/16/14	Sat	1005	Angle	0	0	1	Day	Dry	Other Contributing Action
463	4.193	09/30/14	Tue	1805	Not Coded	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
464	4.193	02/11/14	Tue	1635	Angle	0	3	0	Day	Dry	Other Contributing Action
465	4.193	03/16/14	Sun	1530	Parked Motor Vehicle	0	0	1	Day	Dry	Improper Backing
466	4.193	08/29/14	Fri	0719	Angle	0	0	1	Day	Dry	Not Coded
467	4.193	09/10/14	Wed	0740	Angle	0	0	1	Day	Dry	Ran Red Light
468	4.221	08/25/14	Mon	1630	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
469	4.245	07/31/14	Thu	0850	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
470	4.245	05/29/14	Thu	1427	Sideswipe	0	0	1	Day	Dry	No Contributing Action
471	19.599	02/18/14	Tue	1930	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
472	19.637	06/13/14	Fri	1923	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
473	19.640	04/11/14	Fri	0815	Rear-End	0	0	1	Day	Dry	Improper Turn
474	19.655	02/22/14	Sat	0610	Not Coded	0	0	1	Day	Dry	Not Coded
475	19.655	12/29/14	Mon	1755	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
476	19.659	05/18/14	Sun	1745	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
477	19.665	03/21/14	Fri	1210	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
478	19.672	07/01/14	Tue	1650	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
479	19.672	05/08/14	Thu	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
480	19.674	04/13/14	Sun	2345	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
481	19.674	06/09/14	Mon	1346	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
482	19.674	06/27/14	Fri	1830	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
483	19.674	07/07/14	Mon	0939	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
484	19.674	07/17/14	Thu	1335	Rear-End	0	0	1	Day	Wet	No Contributing Action
485	19.674	10/11/14	Sat	0839	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
486	19.674	12/22/14	Mon	0918	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner

**State of Florida Department of Transportation
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 STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
487	19.674	12/02/14	Tue	1820	Sideswipe			0	0	1	Night	Dry	Careless or Negligent Manner		
488	19.683	03/27/14	Thu	1545	Rear-End			0	0	1	Day	Dry	Careless or Negligent Manner		
489	19.683	07/14/14	Mon	0810	Rear-End			0	0	1	Day	Dry	Other Contributing Action		
490	19.683	11/03/14	Mon	0745	Angle			0	0	1	Day	Dry	Careless or Negligent Manner		
491	19.702	03/27/14	Thu	1132	Sideswipe			0	0	1	Day	Dry	Careless or Negligent Manner		
492	19.702	05/04/14	Sun	1222	Rear-End			0	0	1	Day	Dry	Improper Backing		
493	19.752	07/13/14	Sun	1220	Not Coded			0	1	0	Night	Dry	Other Contributing Action		
494	19.771	08/12/14	Tue	1338	Sideswipe			0	0	1	Day	Dry	Improper Turn		
495	19.780	04/26/14	Sat	0540	Not Coded			0	0	1	Night	Dry	Careless or Negligent Manner		
496	19.780	08/20/14	Wed	2348	Not Coded			0	0	1	Night	Dry	Improper Passing		
497	19.780	11/21/14	Fri	0450	Rear-End			0	0	1	Night	Wet	Improper Turn		
498	19.789	08/07/14	Thu	1800	Rear-End			0	0	1	Day	Dry	Careless or Negligent Manner		
499	19.799	07/25/14	Fri	0313	Other Fixed Object			0	0	1	Night	Dry	Careless or Negligent Manner		
500	19.808	11/07/14	Fri	1552	Rear-End			0	0	1	Day	Dry	Careless or Negligent Manner		
501	19.818	01/31/14	Fri	1150	Angle			0	1	0	Day	Dry	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
501	3	137	363	242	2	72	12	2	73	0	5	2	39	1	31
Percent	0.60%	27.35%	72.46%	48.30%	0.40%	14.37%	2.40%	0.40%	14.57%	0.00%	1.00%	0.40%	7.78%	0.20%	6.19%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	357	144	74	427	217	42	6	4	2	13	2	2	0	12	0
Percent	71.26%	28.74%	14.77%	85.23%	43.31%	8.38%	1.20%	0.80%	0.40%	2.59%	0.40%	0.40%	0.00%	2.40%	0.00%
TOTAL ENTERING VEHICLES/ADT: 108,456								SEGMENT CRASH RATE: 2.361 CRASHES PER MILLION VEHICLE MILES							

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	3.810	04/24/15	Fri	1100	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
2	3.862	04/15/15	Wed	0810	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
3	3.905	10/26/15	Mon	0650	Sideswipe	0	0	1	Day	Dry	#N/A
4	3.905	11/10/15	Tue	0816	Rear-End	0	1	0	Day	Dry	No Contributing Action
5	3.924	05/06/15	Wed	1325	Rear-End	0	3	0	Day	Dry	#N/A
6	3.924	04/21/15	Tue	0830	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
7	3.943	03/13/15	Fri	1909	Sideswipe	0	0	1	Night	Dry	#N/A
8	3.965	06/13/15	Sat	0102	Pedestrian	1	0	0	Night	Dry	No Contributing Action
9	4.026	09/15/15	Tue	0750	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
10	4.057	02/08/15	Sun	2105	Pedestrian	0	1	0	Night	Dry	No Contributing Action
11	4.074	04/11/15	Sat	0820	Sideswipe	0	0	1	Day	Dry	#N/A
12	4.093	11/07/15	Sat	1709	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
13	0.100	08/01/15	Sat	0030	Sideswipe	0	0	1	Night	Dry	#N/A
14	0.116	04/16/15	Thu	1336	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
15	0.225	03/17/15	Tue	1854	Sideswipe	0	0	1	Day	Dry	No Contributing Action
16	0.225	08/26/15	Wed	1100	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
17	0.255	02/13/15	Fri	1620	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
18	0.375	04/18/15	Sat	0205	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
19	0.375	05/26/15	Tue	0940	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
20	0.375	08/27/15	Thu	1759	Not Coded	0	0	1	Day	Dry	No Contributing Action
21	0.375	10/08/15	Thu	0900	Angle	0	0	1	Day	Dry	No Contributing Action
22	0.380	09/16/15	Wed	1710	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
23	0.399	11/28/15	Sat	0525	Rear-End	0	1	0	Night	Dry	#N/A
24	0.437	07/19/15	Sun	1320	Rear-End	0	0	1	Day	Dry	Drove too Fast for Conditions
25	0.505	05/28/15	Thu	1330	Sideswipe	0	0	1	Day	Dry	#N/A
26	0.505	06/02/15	Tue	0945	Rear-End	0	0	1	Day	Dry	Other Contributing Action
27	0.505	04/15/15	Wed	1448	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
28	0.505	12/02/15	Wed	1510	Not Coded	0	0	1	Day	Dry	#N/A
29	0.514	06/15/15	Mon	0845	Concrete Traffic Barrier	0	0	1	Day	Wet	Careless or Negligent Manner
30	0.524	10/11/15	Sun	0530	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
31	0.524	11/12/15	Thu	1035	Rear-End	0	1	0	Day	Dry	Other Contributing Action
32	0.524	12/06/15	Sun	1600	Sideswipe	0	0	1	Day	Dry	No Contributing Action
33	0.524	12/17/15	Thu	0800	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
34	0.543	05/18/15	Mon	1354	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
35	0.543	12/08/15	Tue	1747	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
36	0.546	03/08/15	Sun	0615	Guardrail Face	0	0	1	Night	Dry	Careless or Negligent Manner
37	0.562	11/12/15	Thu	1955	Sideswipe	0	0	1	Night	Dry	#N/A
38	0.562	11/16/15	Mon	1915	Not Coded	#N/A	0	1	Day	Dry	#N/A
39	0.580	07/23/15	Thu	0900	Not Coded	0	0	1	Day	Dry	No Contributing Action
40	0.581	12/17/15	Thu	0800	Sideswipe	0	2	0	Day	Dry	Failed to Yield Right-Of-Way
41	0.585	01/17/15	Sat	1435	Sideswipe	0	1	0	Day	Dry	#N/A
42	0.585	02/17/15	Tue	1045	Rear-End	0	0	1	Day	Dry	Improper Passing
43	0.585	03/18/15	Wed	1828	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
44	0.655	01/16/15	Fri	0930	Sideswipe	0	0	1	Day	Dry	No Contributing Action
45	0.655	07/22/15	Wed	1655	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
46	0.660	01/14/15	Wed	1815	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
47	0.660	10/16/15	Fri	1525	Sideswipe	0	0	1	Day	Dry	#N/A
48	0.675	04/10/15	Fri	1705	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
49	0.675	10/02/15	Fri	1015	Other Non-Fixed Object	0	0	1	Day	Dry	No Contributing Action
50	0.675	10/02/15	Fri	1058	Other Non-Fixed Object	0	0	1	Day	Dry	No Contributing Action
51	0.717	02/02/15	Mon	0830	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
52	0.736	11/02/15	Mon	1905	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
53	0.736	08/20/15	Thu	1648	Rear-End	0	0	1	Day	Dry	No Contributing Action
54	0.736	08/20/15	Thu	1846	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
55	0.736	11/24/15	Tue	1758	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
56	0.736	12/13/15	Sun	1820	Rear-End	0	4	0	Night	Dry	Careless or Negligent Manner
57	0.755	01/14/15	Wed	1320	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
58	0.756	01/01/15	Thu	1838	Pedestrian	0	0	1	Night	Dry	Improper Passing
59	0.770	05/29/15	Fri	1300	Rear-End	0	1	0	Day	Dry	#N/A
60	0.774	02/20/15	Fri	1655	Not Coded	0	0	1	Day	Dry	#N/A
61	0.778	06/18/15	Thu	0835	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
62	0.793	09/04/15	Fri	1635	Not Coded	0	0	1	Day	Dry	Improper Passing
63	0.813	05/18/15	Mon	1346	Sideswipe	0	0	1	Day	Dry	Improper Passing
64	0.850	01/22/15	Thu	1453	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
65	0.850	02/10/15	Tue	1909	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
66	0.850	02/16/15	Mon	1217	Concrete Traffic Barrier	0	0	1	Day	Dry	#N/A
67	0.878	11/20/15	Fri	0509	Concrete Traffic Barrier	0	1	0	Night	Dry	Careless or Negligent Manner
68	0.888	08/25/15	Tue	2245	Concrete Traffic Barrier	0	2	0	Night	Dry	Careless or Negligent Manner
69	0.899	07/24/15	Fri	1825	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
70	0.949	11/25/15	Wed	1149	Rear-End	0	0	1	Day	Wet	Other Contributing Action
71	0.955	01/22/15	Thu	0831	Rear-End	0	0	1	Day	Dry	No Contributing Action
72	0.968	01/05/15	Mon	1450	Rear-End	0	2	0	Day	Dry	Failed To Keep In Proper Lane
73	0.986	03/12/15	Thu	1310	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
74	0.986	03/26/15	Thu	1526	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
75	0.986	05/01/15	Fri	0930	Rear-End	0	0	1	Day	Dry	No Contributing Action
76	1.005	02/15/15	Sun	0119	Sideswipe	0	2	0	Night	Dry	Failed to Yield Right-Of-Way
77	1.005	10/02/15	Fri	1505	Sideswipe	0	3	0	Day	Dry	Failed To Keep In Proper Lane
78	1.005	12/27/15	Sun	1116	Sideswipe	0	0	1	Day	Dry	#N/A
79	1.012	10/21/15	Wed	1735	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
80	1.024	12/03/15	Thu	1055	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
81	1.028	03/13/15	Fri	0020	Angle	0	0	1	Night	Dry	#N/A

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
82	1.028	03/30/15	Mon	1628	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
83	1.028	11/19/15	Thu	1921	Rear-End	0	3	0	Night	Dry	Careless or Negligent Manner
84	1.036	07/30/15	Thu	0900	Impact Attenuator/Crash Cushion	0	0	1	Day	Dry	Other Contributing Action
85	1.043	01/23/15	Fri	1350	Rear-End	0	0	1	Day	Dry	#N/A
86	1.081	12/15/15	Tue	0758	Rear-End	0	0	1	Night	Dry	#N/A
87	1.100	03/14/15	Sat	1150	Concrete Traffic Barrier	0	0	1	Day	Dry	Careless or Negligent Manner
88	1.110	10/05/15	Mon	0700	Not Coded	0	1	0	Day	Wet	Other Contributing Action
89	1.119	02/14/15	Sat	1355	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
90	1.145	07/08/15	Wed	1230	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
91	1.164	02/11/15	Wed	1925	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
92	1.178	01/15/15	Thu	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
93	1.178	10/21/15	Wed	0649	Rear-End	0	0	1	Day	Dry	Other Contributing Action
94	1.183	01/01/15	Thu	1520	Overturn/Rollover	0	1	0	Day	Dry	Other Contributing Action
95	1.195	02/04/15	Wed	0835	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
96	1.202	12/04/15	Fri	1655	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
97	1.221	07/08/15	Wed	0931	Rear-End	0	0	1	Day	Dry	Other Contributing Action
98	1.221	07/11/15	Sat	0650	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
99	1.240	09/05/15	Sat	1955	Concrete Traffic Barrier	0	1	0	Night	Wet	Failed To Keep In Proper Lane
100	1.259	06/22/15	Mon	1443	Struck by Falling/Shifting Cargo	0	0	1	Day	Dry	#N/A
101	1.268	11/13/15	Fri	0645	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
102	1.269	08/21/15	Fri	0800	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
103	1.298	10/20/15	Tue	1600	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
104	1.306	01/26/15	Mon	0605	Traffic Sign Support	0	0	1	Night	Dry	Careless or Negligent Manner
105	1.316	01/07/15	Wed	0900	Angle	0	2	0	Day	Dry	Careless or Negligent Manner
106	1.378	09/05/15	Sat	1010	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
107	1.378	10/08/15	Thu	0116	Embankment	0	0	1	Night	Wet	Careless or Negligent Manner
108	1.411	01/23/15	Fri	1800	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
109	1.411	02/27/15	Fri	1615	Rear-End	0	1	0	Day	Dry	Failed To Keep In Proper Lane
110	1.448	01/09/15	Fri	1655	Rear-End	0	0	1	Day	Dry	#N/A
111	1.467	07/15/15	Wed	1608	Rear-End	0	0	1	Day	Wet	No Contributing Action
112	1.478	02/19/15	Thu	0710	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
113	1.478	02/26/15	Thu	1120	Rear-End	0	0	1	Day	Dry	No Contributing Action
114	1.528	02/23/15	Mon	0040	Concrete Traffic Barrier	0	2	0	Night	Dry	Careless or Negligent Manner
115	1.565	04/02/15	Thu	1907	Other Non-Collision	0	1	0	Night	Dry	Other Contributing Action
116	1.565	12/03/15	Thu	1142	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
117	1.578	02/18/15	Wed	0945	Sideswipe	0	0	1	Day	Wet	Careless or Negligent Manner
118	1.603	03/21/15	Sat	0850	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
119	1.619	10/08/15	Thu	0116	Concrete Traffic Barrier	0	1	0	Night	Wet	Careless or Negligent Manner
120	1.622	09/04/15	Fri	1820	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
121	1.641	03/18/15	Wed	0907	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
122	1.641	04/04/15	Sat	1445	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
123	1.641	04/09/15	Thu	1553	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
124	1.641	08/28/15	Fri	1731	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
125	1.646	05/13/15	Wed	0116	Other Post, Pole Or Support	0	1	0	Night	Dry	Careless or Negligent Manner
126	1.646	07/24/15	Fri	1130	Guardrail Face	0	0	1	Day	Dry	Careless or Negligent Manner
127	1.646	07/30/15	Thu	1640	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
128	1.646	12/19/15	Sat	2330	Sideswipe	0	0	1	Night	Dry	Other Contributing Action
129	1.646	12/10/15	Thu	0030	Not Coded	0	0	1	Night	Dry	#N/A
130	1.646	12/10/15	Thu	0040	Rear-End	0	0	1	Night	Dry	Other Contributing Action
131	1.679	05/07/15	Thu	1230	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
132	1.688	11/14/15	Sat	0152	Concrete Traffic Barrier	0	0	1	Night	Wet	Over-Correcting/Over-Steering
133	1.698	07/01/15	Wed	1150	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
134	1.716	08/31/15	Mon	0830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
135	1.735	05/16/15	Sat	1946	Sideswipe	0	0	1	Night	Dry	#N/A
136	1.735	08/09/15	Sun	2102	Rear-End	0	4	0	Night	Dry	Careless or Negligent Manner
137	1.746	07/09/15	Thu	1655	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
138	1.746	07/28/15	Tue	1737	Rear-End	0	5	0	Day	Dry	Careless or Negligent Manner
139	1.746	11/03/15	Tue	1625	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
140	1.755	02/16/15	Mon	1700	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
141	1.778	01/08/15	Thu	0850	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
142	1.796	11/25/15	Wed	1545	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
143	1.796	05/24/15	Sun	1939	Sideswipe	0	1	0	Night	Dry	#N/A
144	1.796	05/28/15	Thu	0920	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
145	1.796	06/30/15	Tue	1635	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
146	1.796	07/30/15	Thu	1922	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
147	1.796	11/09/15	Mon	1910	Rear-End	0	3	0	Night	Dry	Careless or Negligent Manner
148	1.796	12/21/15	Mon	1519	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
149	1.846	05/03/15	Sun	2349	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
150	1.846	05/12/15	Tue	0130	Guardrail Face	0	1	0	Night	Dry	No Contributing Action
151	1.846	11/27/15	Fri	2232	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
152	1.846	11/27/15	Fri	2245	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
153	2.046	04/20/15	Mon	2240	Concrete Traffic Barrier	1	2	0	Night	Dry	Over-Correcting/Over-Steering
154	2.046	06/09/15	Tue	1545	Guardrail Face	0	0	1	Day	Wet	Careless or Negligent Manner
155	2.046	06/09/15	Tue	1720	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
156	2.046	06/03/15	Wed	1700	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
157	2.046	11/22/15	Sun	0405	Sideswipe	0	1	0	Night	Wet	No Contributing Action
158	2.046	12/10/15	Thu	0935	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
159	2.278	06/09/15	Tue	1715	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
160	2.278	09/07/15	Mon	2152	Guardrail Face	0	0	1	Night	Dry	Careless or Negligent Manner
161	2.296	06/24/15	Wed	2340	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
162	2.296	04/21/15	Tue	1818	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

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 ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
163	2.346	03/30/15	Mon	1541	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
164	2.446	06/11/15	Thu	0028	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
165	2.546	03/27/15	Fri	1530	Rear-End	0	0	1	Day	Dry	No Contributing Action
166	2.546	03/16/15	Mon	1630	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
167	2.546	05/13/15	Wed	1645	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
168	2.546	05/13/15	Wed	1250	Sideswipe	0	0	1	Day	Dry	No Contributing Action
169	2.546	04/18/15	Sat	1521	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
170	2.546	03/27/15	Fri	2022	Guardrail Face	0	0	1	Night	Wet	Careless or Negligent Manner
171	2.546	06/17/15	Wed	2059	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
172	2.546	06/30/15	Tue	0830	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
173	2.546	11/30/15	Mon	0945	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
174	2.546	05/29/15	Fri	0846	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
175	2.546	08/08/15	Sat	2050	Rear-End	0	0	1	Night	Dry	#N/A
176	2.546	12/30/15	Wed	1953	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
177	2.546	08/21/15	Fri	1715	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
178	2.546	08/12/15	Wed	0929	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
179	2.546	11/30/15	Mon	1310	Sideswipe	0	1	0	Day	Dry	Other Contributing Action
180	2.546	12/31/15	Thu	0237	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
181	2.730	07/29/15	Wed	1740	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
182	3.046	11/18/15	Wed	1019	Not Coded	0	0	1	Day	Dry	Failed to Keep In Proper Lane
183	3.230	08/16/15	Sun	0730	Guardrail Face	0	0	1	Day	Wet	Careless or Negligent Manner
184	3.278	01/12/15	Mon	1635	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
185	3.278	01/12/15	Mon	1630	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
186	3.278	01/12/15	Mon	1815	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
187	3.530	12/29/15	Tue	1528	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
188	3.546	03/26/15	Thu	1730	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
189	3.546	07/31/15	Fri	1808	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
190	3.730	01/15/15	Thu	0949	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
191	3.730	02/02/15	Mon	2045	Not Coded	0	1	0	Night	Dry	#N/A
192	3.730	02/11/15	Wed	2137	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
193	3.730	03/11/15	Wed	2230	Overturn/Rollover	0	4	0	Night	Dry	Careless or Negligent Manner
194	3.730	10/23/15	Fri	2027	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
195	3.730	05/15/15	Fri	1648	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
196	3.730	04/03/15	Fri	1600	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
197	3.730	05/01/15	Fri	0825	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
198	3.730	07/08/15	Wed	2033	Pedestrian	1	0	0	Night	Wet	No Contributing Action
199	3.730	05/22/15	Fri	2330	Rear-End	0	0	1	Night	Dry	#N/A
200	3.730	06/30/15	Tue	0505	Rear-End	0	2	0	Night	Dry	#N/A
201	3.730	06/19/15	Fri	1615	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
202	3.730	08/21/15	Fri	0116	Not Coded	0	0	1	Night	Dry	#N/A
203	3.730	11/10/15	Tue	1841	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
204	3.730	11/11/15	Wed	1120	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
205	3.730	12/11/15	Fri	1159	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
206	3.730	12/04/15	Fri	1455	Parked Motor Vehicle	0	4	0	Day	Wet	Careless or Negligent Manner
207	3.730	12/29/15	Tue	1735	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
208	3.730	12/30/15	Wed	1912	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
209	3.730	12/30/15	Wed	1917	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
210	3.730	11/11/15	Wed	1950	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
211	3.730	06/15/15	Mon	1544	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
212	3.980	05/04/15	Mon	0140	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
213	3.980	12/11/15	Fri	1126	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
214	4.043	10/18/15	Sun	2300	Guardrail Face	0	0	1	Night	Wet	Careless or Negligent Manner
215	4.130	10/09/15	Fri	1636	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
216	4.230	02/28/15	Sat	0006	Rear-End	0	0	1	Night	Wet	#N/A
217	4.230	01/07/15	Wed	1835	Rear-End	0	0	1	Night	Dry	Other Contributing Action
218	4.230	01/21/15	Wed	2040	Sideswipe	0	0	1	Night	Dry	No Contributing Action
219	4.230	03/14/15	Sat	1415	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
220	4.230	02/06/15	Fri	1516	Sideswipe	0	0	1	Day	Dry	Improper Passing
221	4.230	03/18/15	Wed	0930	Rear-End	0	0	1	Day	Dry	Other Contributing Action
222	4.230	04/08/15	Wed	0829	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
223	4.230	05/16/15	Sat	1425	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
224	4.230	04/27/15	Mon	0930	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
225	4.230	03/15/15	Sun	1506	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
226	4.230	04/16/15	Thu	1755	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
227	4.230	03/29/15	Sun	2256	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
228	4.230	05/24/15	Sun	0230	Tree (Standing)	0	1	0	Night	Dry	Careless or Negligent Manner
229	4.230	06/16/15	Tue	1745	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
230	4.230	08/11/15	Tue	1020	Rear-End	0	0	1	Day	Dry	#N/A
231	4.230	07/25/15	Sat	1445	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
232	4.230	08/07/15	Fri	1435	Rear-End	0	0	1	Day	Dry	Other Contributing Action
233	4.230	08/08/15	Sat	2000	Rear-End	0	0	1	Night	Dry	Other Contributing Action
234	4.230	07/29/15	Wed	1615	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
235	4.230	08/07/15	Fri	1543	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
236	4.230	09/29/15	Tue	1520	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
237	4.230	07/31/15	Fri	1958	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
238	4.230	08/12/15	Wed	0740	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
239	4.230	12/09/15	Wed	0815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
240	4.230	12/04/15	Fri	1025	Rear-End	0	2	0	Day	Wet	No Contributing Action
241	4.230	12/02/15	Wed	0718	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
242	4.230	12/14/15	Mon	0827	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
243	4.330	12/03/15	Thu	2220	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
244	4.351	06/09/15	Tue	0023	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
245	4.430	02/13/15	Fri	0715	Rear-End	0	0	1	Day	Dry	Other Contributing Action
246	4.430	08/10/15	Mon	0951	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
247	4.480	04/12/15	Sun	1309	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
248	4.480	04/12/15	Sun	1402	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
249	4.480	05/04/15	Mon	0047	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
250	4.480	05/15/15	Fri	1022	Rear-End	0	0	1	Day	Dry	#N/A
251	4.480	03/13/15	Fri	1445	Rear-End	0	0	1	Day	Dry	#N/A
252	4.480	05/16/15	Sat	1553	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
253	4.480	06/22/15	Mon	1444	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
254	4.480	07/11/15	Sat	1640	Rear-End	0	8	0	Day	Dry	Careless or Negligent Manner
255	4.480	10/10/15	Sat	1829	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
256	4.480	08/08/15	Sat	2126	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
257	4.480	08/22/15	Sat	1545	Rear-End	0	0	1	Day	Dry	No Contributing Action
258	4.480	10/16/15	Fri	1935	Rear-End	0	2	0	Night	Wet	Drove too Fast for Conditions
259	4.480	10/23/15	Fri	1918	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
260	4.480	10/23/15	Fri	1934	Rear-End	0	4	0	Night	Dry	Careless or Negligent Manner
261	4.480	10/25/15	Sun	1640	Other Non-Collision	0	1	0	Day	Dry	Drove too Fast for Conditions
262	4.480	11/30/15	Mon	1613	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
263	4.480	12/14/15	Mon	1735	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
264	4.480	12/31/15	Thu	2231	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
265	4.480	12/28/15	Mon	1818	Rear-End	0	0	1	Night	Dry	#N/A
266	4.480	12/10/15	Thu	1817	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
267	4.503	07/22/15	Wed	1545	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
268	4.530	01/26/15	Mon	0910	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
269	4.530	01/31/15	Sat	1310	Rear-End	0	5	0	Day	Dry	Careless or Negligent Manner
270	4.530	02/18/15	Wed	0915	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
271	4.530	03/12/15	Thu	1540	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
272	4.530	06/22/15	Mon	0700	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
273	4.530	05/04/15	Mon	0120	Rear-End	0	0	1	Night	Dry	#N/A
274	4.530	05/04/15	Mon	0125	Rear-End	0	0	1	Night	Dry	#N/A
275	4.530	07/03/15	Fri	1300	Rear-End	0	0	1	Day	Dry	No Contributing Action
276	4.530	08/07/15	Fri	1605	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
277	4.530	12/13/15	Sun	2000	Angle	0	1	0	Night	Dry	Careless or Negligent Manner
278	4.541	04/03/15	Fri	1440	Rear-End	0	0	1	Day	Dry	#N/A
279	4.541	05/28/15	Thu	0755	Sideswipe	0	1	0	Day	Dry	No Contributing Action
280	4.541	10/04/15	Sun	2040	Sideswipe	0	1	0	Night	Dry	Careless or Negligent Manner
281	4.541	10/09/15	Fri	1230	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
282	4.541	12/13/15	Sun	2145	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
283	4.560	03/28/15	Sat	1420	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
284	4.560	02/01/15	Sun	0330	Not Coded	0	0	1	Night	Dry	#N/A
285	4.560	05/15/15	Fri	1545	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
286	4.578	06/02/15	Tue	1915	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
287	4.578	07/16/15	Thu	1400	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
288	4.578	12/05/15	Sat	1749	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
289	4.597	01/01/15	Thu	1355	Rear-End	0	4	0	Day	Wet	Careless or Negligent Manner
290	4.597	01/03/15	Sat	1335	Rear-End	0	7	0	Day	Dry	Careless or Negligent Manner
291	4.597	11/12/15	Thu	1000	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
292	4.616	03/13/15	Fri	1350	Sideswipe	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
293	4.616	09/02/15	Wed	0900	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
294	4.616	12/07/15	Mon	1802	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
295	4.616	12/15/15	Tue	1530	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
296	4.630	03/01/15	Sun	0555	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
297	4.630	02/27/15	Fri	1425	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
298	4.630	04/07/15	Tue	1510	Rear-End	0	7	0	Day	Dry	Careless or Negligent Manner
299	4.630	12/29/15	Tue	1245	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
300	4.630	10/28/15	Wed	1000	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
301	4.630	10/29/15	Thu	1000	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
302	4.630	11/20/15	Fri	1115	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
303	4.630	11/21/15	Sat	1030	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
304	4.635	10/07/15	Wed	1910	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
305	4.635	05/13/15	Wed	1723	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
306	4.635	05/30/15	Sat	1135	Sideswipe	0	0	1	Day	Dry	#N/A
307	4.635	08/02/15	Sun	1515	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
308	4.635	06/22/15	Mon	1602	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
309	4.635	06/29/15	Mon	1546	Rear-End	0	0	1	Day	Dry	Other Contributing Action
310	4.635	10/23/15	Fri	1956	Angle	0	2	0	Night	Dry	Careless or Negligent Manner
311	4.635	10/02/15	Fri	1547	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
312	4.635	10/16/15	Fri	1511	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
313	4.635	06/09/15	Tue	1615	Rear-End	0	0	1	Day	Wet	No Contributing Action
314	4.654	11/01/15	Sun	0008	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
315	4.664	11/30/15	Mon	1827	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
316	4.673	03/20/15	Fri	1100	Rear-End	0	3	0	Day	Dry	No Contributing Action
317	4.673	06/27/15	Sat	1140	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
318	4.692	07/02/15	Thu	0855	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
319	4.692	10/23/15	Fri	1648	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
320	4.702	04/12/15	Sun	1307	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
321	4.702	06/13/15	Sat	1824	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
322	4.711	01/23/15	Fri	1528	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
323	4.711	02/28/15	Sat	1539	Rear-End	0	0	1	Day	Wet	#N/A
324	4.711	11/04/15	Wed	1552	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
325	4.711	12/11/15	Fri	2036	Rear-End	0	4	0	Night	Dry	Careless or Negligent Manner
326	4.711	05/07/15	Thu	1442	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
327	4.719	07/03/15	Fri	1243	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
328	4.721	07/11/15	Sat	2216	Fell/Jumped from Motor Vehicle	0	1	0	Night	Dry	No Contributing Action
329	4.721	09/14/15	Mon	0035	Rear-End	0	1	0	Night	Dry	Exceed Posted Speed
330	4.725	04/29/15	Wed	2026	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
331	4.726	02/10/15	Tue	1305	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
332	4.726	03/06/15	Fri	1321	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
333	4.727	04/15/15	Wed	1554	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
334	4.730	01/07/15	Wed	1119	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
335	4.730	02/10/15	Tue	1718	Sideswipe	0	0	1	Day	Dry	No Contributing Action
336	4.730	02/21/15	Sat	1555	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
337	4.768	07/10/15	Fri	1018	Rear-End	0	0	1	Day	Dry	Followed too Closely
338	4.810	10/06/15	Tue	0815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
339	4.844	10/14/15	Wed	1010	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
340	4.872	05/27/15	Wed	0914	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
341	4.901	12/08/15	Tue	1035	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
342	4.908	09/24/15	Thu	0711	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
343	0.392	09/11/15	Fri	1716	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
344	0.392	09/13/15	Sun	0437	Not Coded	0	1	0	Night	Dry	Erratic, Reckless or Aggressive
345	0.000	03/27/15	Fri	2351	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
346	0.000	03/20/15	Fri	1910	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
347	0.000	05/31/15	Sun	1007	Sideswipe	0	0	1	Day	Wet	No Contributing Action
348	0.000	09/29/15	Tue	0630	Not Coded	0	0	1	Day	Dry	No Contributing Action
349	0.000	10/19/15	Mon	0533	Ditch	0	0	1	Night	Dry	Careless or Negligent Manner
350	0.009	10/23/15	Fri	0414	Guardrail Face	0	0	1	Night	Wet	Careless or Negligent Manner
351	0.000	04/25/15	Sat	2154	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
352	0.000	07/16/15	Thu	1318	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
353	0.028	01/16/15	Fri	1229	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
354	0.047	06/02/15	Tue	1205	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
355	0.057	12/02/15	Wed	1110	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
356	0.066	12/17/15	Thu	1945	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
357	0.085	01/10/15	Sat	1800	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
358	0.085	08/11/15	Tue	0935	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
359	0.095	04/16/15	Thu	1422	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
360	0.095	07/28/15	Tue	0910	Sideswipe	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
361	0.104	07/08/15	Wed	1300	Sideswipe	0	0	1	Day	Dry	No Contributing Action
362	0.104	02/17/15	Tue	1845	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
363	0.104	04/04/15	Sat	1554	Rear-End	0	0	1	Day	Dry	Wrong Side or Wrong Way
364	0.104	05/06/15	Wed	1022	Rear-End	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
365	0.104	11/23/15	Mon	0920	Angle	0	0	1	Day	Dry	Other Contributing Action
366	0.104	12/24/15	Thu	1421	Sideswipe	0	0	1	Day	Dry	No Contributing Action
367	0.000	03/07/15	Sat	0115	Concrete Traffic Barrier	0	0	1	Night	Wet	Failed To Keep In Proper Lane
368	0.009	10/06/15	Tue	0805	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
369	0.000	05/17/15	Sun	1030	Other Post, Pole Or Support	0	0	1	Day	Dry	Careless or Negligent Manner
370	0.089	09/25/15	Fri	1627	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
371	0.000	12/28/15	Mon	0642	Rear-End	0	0	1	Night	Dry	#N/A
372	0.187	08/16/15	Sun	1200	Sideswipe	0	0	1	Day	Dry	#N/A
373	0.000	09/26/15	Sat	0930	Guardrail Face	0	1	0	Day	Wet	Swerved Or Avoided
374	0.163	12/24/15	Thu	1535	Rear-End	0	1	0	Day	Dry	No Contributing Action
375	0.163	12/26/15	Sat	1601	Rear-End	0	0	1	Day	Dry	No Contributing Action
376	0.213	01/10/15	Sat	1430	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
377	0.261	03/14/15	Sat	1200	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
378	0.261	07/08/15	Wed	1000	Rear-End	0	0	1	Day	Dry	No Contributing Action
379	0.261	07/10/15	Fri	1220	Rear-End	0	0	1	Day	Dry	No Contributing Action
380	0.280	07/08/15	Wed	0900	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
381	0.299	11/11/15	Wed	1550	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
382	0.318	06/12/15	Fri	0857	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
383	0.318	10/31/15	Sat	2035	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
384	0.318	12/31/15	Thu	1733	Not Coded	0	8	0	Night	Dry	Careless or Negligent Manner
385	0.356	03/22/15	Sun	1427	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
386	0.356	11/07/15	Sat	2054	Not Coded	0	1	0	Night	Dry	#N/A
387	0.375	05/04/15	Mon	1648	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
388	0.413	02/22/15	Sun	1250	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
389	0.413	08/05/15	Wed	1000	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
390	0.413	01/01/15	Thu	0548	Embankment	0	0	1	Night	Dry	Careless or Negligent Manner
391	0.413	04/28/15	Tue	1252	Sideswipe	0	0	1	Day	Wet	Improper Passing
392	0.413	04/11/15	Sat	0430	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
393	0.047	02/24/15	Tue	1625	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
394	0.075	10/01/15	Thu	1655	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
395	0.075	03/24/15	Tue	1655	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
396	0.000	12/31/15	Thu	0054	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
397	0.000	01/28/15	Wed	0834	Rear-End	0	0	1	Day	Dry	Followed too Closely
398	0.000	04/02/15	Thu	0956	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
399	0.000	07/29/15	Wed	1748	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
400	0.000	12/05/15	Sat	0856	Angle	0	0	1	Day	Dry	No Contributing Action
401	0.000	09/20/15	Sun	2013	Sideswipe	0	0	1	Night	Wet	Careless or Negligent Manner
402	0.002	01/13/15	Tue	0740	Sideswipe	0	0	1	Day	Wet	Careless or Negligent Manner
403	0.004	08/22/15	Sat	1224	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
404	0.009	11/20/15	Fri	0010	Rear-End	0	0	1	Night	Wet	Followed too Closely
405	0.019	06/14/15	Sun	0226	Rear-End	0	1	0	Night	Dry	Followed too Closely

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
406	0.019	06/01/15	Mon	1250	Not Coded	0	0	1	Day	Dry	#N/A
407	0.028	09/22/15	Tue	1419	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
408	0.038	08/15/15	Sat	0319	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
409	0.049	06/26/15	Fri	1430	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
410	13.430	01/16/15	Fri	2010	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
411	13.430	05/15/15	Fri	1440	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
412	13.440	04/01/15	Wed	0945	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
413	13.442	01/12/15	Mon	0929	Sideswipe	0	0	1	Day	Dry	No Contributing Action
414	13.449	01/02/15	Fri	2116	Angle	0	0	1	Night	Dry	Ran Stop Sign
415	13.449	02/16/15	Mon	1250	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
416	13.449	02/23/15	Mon	1450	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
417	13.449	04/07/15	Tue	1250	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
418	13.449	01/13/15	Tue	1937	Angle	0	0	1	Night	Dry	No Contributing Action
419	13.449	01/16/15	Fri	1905	Left-Turn	0	1	0	Night	Dry	Failed to Yield Right-Of-Way
420	13.449	01/25/15	Sun	1245	Angle	0	0	1	Day	Dry	Ran Stop Sign
421	13.449	02/02/15	Mon	0800	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
422	13.449	02/28/15	Sat	1700	Left-Turn	0	0	1	Day	Wet	Failed to Yield Right-Of-Way
423	13.449	03/05/15	Thu	1246	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
424	13.449	03/10/15	Tue	1515	Angle	0	0	1	Day	Dry	No Contributing Action
425	13.449	04/04/15	Sat	1220	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
426	13.449	04/26/15	Sun	1430	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
427	13.449	05/04/15	Mon	1100	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
428	13.449	05/05/15	Tue	2134	Angle	0	5	0	Night	Dry	Failed to Yield Right-Of-Way
429	13.449	05/11/15	Mon	1700	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
430	13.449	05/14/15	Thu	1521	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
431	13.449	05/16/15	Sat	1645	Angle	0	0	1	Day	Dry	Not Coded
432	13.449	05/19/15	Tue	1250	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
433	13.449	05/30/15	Sat	1330	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
434	13.449	06/24/15	Wed	1230	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
435	13.449	07/08/15	Wed	1715	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
436	13.449	07/13/15	Mon	0927	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
437	13.449	07/30/15	Thu	1755	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
438	13.449	08/04/15	Tue	1310	Angle	0	0	1	Day	Dry	Not Coded
439	13.449	08/14/15	Fri	1218	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
440	13.449	08/24/15	Mon	1745	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
441	13.449	09/12/15	Sat	1955	Angle	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
442	13.449	10/05/15	Mon	0933	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
443	13.449	06/04/15	Thu	1630	Angle	0	2	0	Day	Dry	Failed to Yield Right-Of-Way
444	13.449	06/15/15	Mon	1000	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
445	13.449	07/05/15	Sun	1050	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
446	13.449	07/11/15	Sat	1601	Angle	0	6	0	Day	Dry	Failed to Yield Right-Of-Way
447	13.449	10/16/15	Fri	1440	Angle	0	0	1	Day	Wet	Failed to Yield Right-Of-Way
448	13.449	10/17/15	Sat	1325	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
449	13.449	10/23/15	Fri	1608	Angle	0	0	1	Day	Dry	Not Coded
450	13.449	10/24/15	Sat	1600	Angle	0	0	1	Day	Dry	Improper Turn
451	13.449	11/16/15	Mon	0820	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
452	13.449	11/15/15	Sun	1245	Angle	0	0	1	Day	Wet	Careless or Negligent Manner
453	13.449	11/20/15	Fri	2228	Angle	0	0	1	Night	Dry	Careless or Negligent Manner
454	13.449	11/22/15	Sun	1435	Angle	0	0	1	Day	Wet	Disregarded Other Traffic Sign
455	13.449	11/25/15	Wed	1325	Right-Turn	0	0	1	Day	Wet	Ran Stop Sign
456	13.449	12/10/15	Thu	1935	Angle	0	2	0	Night	Dry	Failed to Yield Right-Of-Way
457	13.449	12/10/15	Thu	2010	Angle	0	0	1	Night	Dry	Disregarded Other Traffic Sign
458	13.449	12/12/15	Sat	1535	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
459	13.449	12/15/15	Tue	1935	Left-Turn	0	4	0	Night	Dry	Improper Turn
460	13.449	12/23/15	Wed	1300	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
461	13.449	12/23/15	Wed	2005	Angle	0	1	0	Night	Dry	Failed to Yield Right-Of-Way
462	13.449	12/26/15	Sat	1930	Angle	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
463	13.452	11/19/15	Thu	1722	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
464	13.452	12/26/15	Sat	1813	Rear-End	0	0	1	Day	Dry	Followed too Closely
465	13.458	02/03/15	Tue	0703	Parked Motor Vehicle	0	0	1	Day	Dry	Careless or Negligent Manner
466	13.458	08/25/15	Tue	1400	Rear-End	0	0	1	Day	Dry	No Contributing Action
467	13.461	06/17/15	Wed	0930	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
468	13.461	10/26/15	Mon	1334	Angle	0	0	1	Day	Dry	No Contributing Action
469	13.469	09/02/15	Wed	1617	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
470	13.471	12/11/15	Fri	1241	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
471	13.476	03/28/15	Sat	1848	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
472	13.477	12/20/15	Sun	1815	Rear-End	0	0	1	Night	Dry	Followed too Closely
473	13.478	04/24/15	Fri	1146	Rear-End	0	0	1	Day	Dry	No Contributing Action
474	13.480	02/22/15	Sun	1739	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
475	13.480	03/31/15	Tue	1627	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
476	13.480	04/01/15	Wed	1440	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
477	13.480	04/02/15	Thu	1640	Sideswipe	0	0	1	Day	Dry	Erratic, Reckless or Aggressive
478	13.480	04/04/15	Sat	0120	Angle	0	0	1	Night	Dry	Not Coded
479	13.480	04/08/15	Wed	1920	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
480	13.480	04/25/15	Sat	1735	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
481	13.480	04/26/15	Sun	1930	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
482	13.480	05/14/15	Thu	2310	Angle	0	2	0	Night	Dry	No Contributing Action
483	13.480	05/24/15	Sun	2240	Angle	0	0	1	Night	Dry	Ran Red Light
484	13.480	06/12/15	Fri	1100	Rear-End	0	1	0	Day	Dry	Other Contributing Action
485	13.480	06/18/15	Thu	1055	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
486	13.480	06/29/15	Mon	1024	Rear-End	0	0	1	Day	Dry	Followed too Closely

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
487	13.480	07/22/15	Wed	1010	Right-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
488	13.480	07/26/15	Sun	2145	Left-Turn	0	0	1	Night	Dry	Not Coded
489	13.480	07/29/15	Wed	2034	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
490	13.480	08/18/15	Tue	0745	Sideswipe	0	0	1	Day	Dry	Improper Passing
491	13.480	08/24/15	Mon	1935	Left-Turn	0	3	0	Day	Dry	Failed to Yield Right-Of-Way
492	13.480	10/12/15	Mon	0750	Angle	0	3	0	Day	Dry	Careless or Negligent Manner
493	13.480	06/20/15	Sat	1400	Sideswipe	0	2	0	Day	Dry	Careless or Negligent Manner
494	13.480	08/03/15	Mon	1210	Sideswipe	0	0	1	Day	Dry	Improper Passing
495	13.480	10/30/15	Fri	0930	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
496	13.480	11/10/15	Tue	1024	Angle	0	0	1	Day	Dry	Not Coded
497	13.480	11/26/15	Thu	0730	Angle	0	0	1	Day	Wet	Not Coded
498	13.480	12/14/15	Mon	1140	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
499	13.482	08/27/15	Thu	0901	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
500	13.484	02/02/15	Mon	1145	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
501	13.484	12/28/15	Mon	0100	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
502	13.484	12/31/15	Thu	1027	Angle	0	0	1	Day	Dry	No Contributing Action
503	13.485	03/29/15	Sun	0051	Pedestrian	0	1	0	Night	Dry	No Contributing Action
504	13.486	11/04/15	Wed	1140	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
505	13.488	11/19/15	Thu	1225	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
506	13.489	03/06/15	Fri	1635	Rear-End	0	0	1	Day	Dry	#N/A
507	13.489	04/19/15	Sun	0625	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
508	13.489	10/01/15	Thu	1935	Rear-End	0	0	1	Night	Dry	Followed too Closely
509	13.489	11/19/15	Thu	0948	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
510	13.489	12/18/15	Fri	0930	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
511	13.494	04/19/15	Sun	1923	Not Coded	0	1	0	Night	Dry	Careless or Negligent Manner
512	13.494	05/25/15	Mon	1600	Rear-End	0	0	1	Day	Dry	No Contributing Action
513	13.499	02/10/15	Tue	1334	Sideswipe	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
514	13.499	06/20/15	Sat	2305	Rear-End	0	0	1	Night	Dry	Followed too Closely
515	13.499	10/02/15	Fri	1940	Angle	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
516	13.499	06/27/15	Sat	0900	Angle	0	0	1	Day	Dry	No Contributing Action
517	13.499	10/20/15	Tue	1054	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
518	13.499	12/11/15	Fri	2055	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
519	13.508	02/01/15	Sun	1140	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
520	13.508	03/09/15	Mon	1635	Rear-End	0	0	1	Day	Dry	Other Contributing Action
521	13.508	05/20/15	Wed	0759	Rear-End	0	0	1	Day	Dry	Other Contributing Action
522	13.508	12/11/15	Fri	2323	Rear-End	0	0	1	Day	Dry	Followed too Closely
523	13.513	07/24/15	Fri	1110	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
524	13.518	05/06/15	Wed	2050	Pedalcycle	0	1	0	Night	Dry	No Contributing Action
525	13.518	06/27/15	Sat	1715	Pedestrian	0	1	0	Day	Dry	No Contributing Action
526	13.518	08/09/15	Sun	0055	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
527	13.538	12/03/15	Thu	0830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
528	13.541	02/03/15	Tue	1205	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
529	13.541	02/22/15	Sun	1455	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
530	13.558	07/16/15	Thu	1235	Rear-End	0	0	1	Day	Wet	Followed too Closely
531	13.575	07/07/15	Tue	1730	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
532	13.586	03/18/15	Wed	1235	Rear-End	0	1	0	Day	Dry	No Contributing Action
533	13.586	07/26/15	Sun	2113	Rear-End	0	5	0	Night	Dry	Careless or Negligent Manner
534	13.596	01/11/15	Sun	2310	Rear-End	0	0	1	Night	Wet	Followed too Closely
535	13.599	04/01/15	Wed	1404	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
536	13.601	09/10/15	Thu	1230	Angle	0	0	1	Day	Dry	Failed To Keep In Proper Lane
537	13.602	08/21/15	Fri	2020	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
538	13.605	01/23/15	Fri	1220	Rear-End	0	0	1	Day	Dry	Followed too Closely
539	13.605	02/25/15	Wed	1200	Not Coded	0	0	1	Day	Dry	No Contributing Action
540	13.605	02/28/15	Sat	1430	Not Coded	0	1	0	Day	Other	No Contributing Action
541	13.605	01/09/15	Fri	1540	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
542	13.605	01/21/15	Wed	1434	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
543	13.605	01/27/15	Tue	0751	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
544	13.605	02/06/15	Fri	1415	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
545	13.605	02/11/15	Wed	1000	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
546	13.605	03/10/15	Tue	2000	Not Coded	0	0	1	Night	Dry	Other Contributing Action
547	13.605	03/27/15	Fri	1010	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
548	13.605	03/27/15	Fri	1755	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
549	13.605	04/01/15	Wed	1735	Sideswipe	0	0	1	Day	Dry	No Contributing Action
550	13.605	04/13/15	Mon	1935	Not Coded	0	0	1	Night	Dry	Improper Turn
551	13.605	04/17/15	Fri	1750	Left-Turn	0	0	1	Day	Dry	Improper Turn
552	13.605	04/17/15	Fri	1940	Not Coded	0	0	1	Night	Dry	Other Contributing Action
553	13.605	04/21/15	Tue	0655	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
554	13.605	05/17/15	Sun	0608	Angle	0	0	1	Day	Dry	Not Coded
555	13.605	05/12/15	Tue	1525	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
556	13.605	05/29/15	Fri	0900	Not Coded	0	0	1	Day	Dry	Improper Turn
557	13.605	05/29/15	Fri	1815	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
558	13.605	06/12/15	Fri	1515	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
559	13.605	06/14/15	Sun	0703	Not Coded	0	0	1	Day	Wet	No Contributing Action
560	13.605	06/15/15	Mon	1100	Angle	0	0	1	Day	Dry	Failed To Keep In Proper Lane
561	13.605	07/15/15	Wed	1207	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
562	13.605	07/17/15	Fri	1715	Backed Into	0	0	1	Day	Dry	Improper Backing
563	13.605	07/22/15	Wed	0613	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
564	13.605	07/26/15	Sun	0714	Rear-End	0	1	0	Night	Dry	No Contributing Action
565	13.605	07/30/15	Thu	1940	Left-Turn	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
566	13.605	08/02/15	Sun	1150	Angle	0	1	0	Night	Dry	Ran Red Light
567	13.605	08/20/15	Thu	2345	Left-Turn	0	0	1	Night	Dry	No Contributing Action

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.000 to MP 0.500 M.P. 0.000 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
568	13.605	08/21/15	Fri	1256	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
569	13.605	08/31/15	Mon	1544	Not Coded	0	2	0	Day	Dry	Improper Passing
570	13.605	09/04/15	Fri	1135	Sideswipe	0	0	1	Day	Dry	Not Coded
571	13.605	09/12/15	Sat	1800	Rear-End	0	0	1	Day	Dry	Other Contributing Action
572	13.605	09/14/15	Mon	0950	Angle	0	0	1	Day	Dry	Other Contributing Action
573	13.605	10/05/15	Mon	2000	Angle	0	0	1	Night	Dry	Not Coded
574	13.605	10/06/15	Tue	1725	Angle	0	0	1	Day	Dry	Improper Passing
575	13.605	10/09/15	Fri	1100	Rear-End	0	0	1	Day	Dry	Followed too Closely
576	13.605	10/14/15	Wed	0847	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
577	13.605	10/22/15	Thu	1720	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
578	13.605	10/28/15	Wed	1226	Backed Into	0	0	1	Day	Dry	Improper Backing
579	13.605	11/06/15	Fri	1150	Rear-End	0	0	1	Day	Dry	Not Coded
580	13.605	11/18/15	Wed	1540	Sideswipe	0	0	1	Day	Dry	No Contributing Action
581	13.605	12/12/15	Sat	1050	Left-Turn	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
582	13.613	04/09/15	Thu	1240	Other Non-Collision	0	0	1	Day	Dry	Other Contributing Action
583	13.613	04/09/15	Thu	1240	Other Non-Collision	0	0	1	Day	Dry	Other Contributing Action
584	13.614	03/21/15	Sat	1735	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
585	13.614	04/01/15	Wed	1413	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
586	13.614	04/07/15	Tue	0630	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
587	13.614	07/30/15	Thu	1950	Rear-End	0	0	1	Night	Dry	Followed too Closely
588	13.614	08/14/15	Fri	0855	Sideswipe	0	0	1	Day	Dry	No Contributing Action
589	13.618	04/08/15	Wed	1800	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
590	13.624	03/19/15	Thu	1300	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
591	13.624	04/07/15	Tue	0929	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
592	13.624	09/12/15	Sat	1255	Not Coded	0	1	0	Day	Dry	Other Contributing Action
593	13.633	05/19/15	Tue	0909	Angle	0	0	1	Day	Dry	Other Contributing Action
594	13.633	08/25/15	Tue	1345	Rear-End	0	0	1	Day	Dry	Followed too Closely
595	2.563	03/31/15	Tue	1845	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
596	2.563	08/25/15	Tue	1713	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
597	2.568	08/06/15	Thu	0215	Angle	0	0	1	Night	Dry	Failed To Keep In Proper Lane
598	2.571	04/14/15	Tue	1626	Angle	0	0	1	Day	Dry	No Contributing Action
599	2.572	08/04/15	Tue	1845	Sideswipe	0	0	1	Day	Dry	No Contributing Action
600	2.574	08/04/15	Tue	1511	Angle	0	0	1	Day	Dry	No Contributing Action
601	2.578	03/15/15	Sun	2111	Rear-End	0	0	1	Night	Dry	Followed too Closely
602	0.177	09/23/15	Wed	1401	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
603	0.181	03/07/15	Sat	1755	Rear-End	0	0	1	Day	Wet	Followed too Closely
604	0.189	02/10/15	Tue	1157	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
605	0.076	08/29/15	Sat	0315	Other Fixed Object	0	0	1	Night	Dry	#N/A
606	0.111	09/24/15	Thu	2206	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
607	0.000	05/07/15	Thu	1600	Rear-End	0	0	1	Day	Dry	Followed too Closely
608	4.184	02/02/15	Mon	0920	Rear-End	0	0	1	Day	Wet	Followed too Closely
609	4.193	03/18/15	Wed	1235	Sideswipe	0	0	1	Day	Dry	Improper Turn
610	4.193	05/14/15	Thu	2339	Angle	0	0	1	Night	Dry	Ran Red Light
611	4.193	05/16/15	Sat	1852	Angle	0	1	0	Day	Dry	Careless or Negligent Manner
612	4.193	06/09/15	Tue	2130	Rear-End	0	0	1	Night	Dry	Followed too Closely
613	4.193	10/30/15	Fri	1200	Pedestrian	0	0	1	Day	Dry	Other Contributing Action
614	4.245	07/03/15	Fri	1135	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
615	19.599	11/22/15	Sun	2305	Angle	0	0	1	Night	Wet	Careless or Negligent Manner
616	19.627	10/14/15	Wed	0745	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
617	19.646	03/13/15	Fri	0940	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
618	19.646	06/15/15	Mon	1700	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
619	19.646	11/21/15	Sat	1912	Rear-End	0	0	1	Day	Wet	Improper Passing
620	19.655	06/19/15	Fri	2245	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
621	19.655	07/21/15	Tue	0855	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
622	19.657	07/16/15	Thu	1950	Not Coded	0	0	1	Night	Dry	Not Coded
623	19.660	07/08/15	Wed	0900	Parked Motor Vehicle	0	0	1	Day	Dry	No Contributing Action
624	19.665	06/13/15	Sat	2315	Sideswipe	0	1	0	Night	Dry	Other Contributing Action
625	19.665	11/08/15	Sun	0057	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
626	19.665	12/03/15	Thu	1950	Angle	0	2	0	Night	Wet	Improper Passing
627	19.666	12/18/15	Fri	1609	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
628	19.668	02/04/15	Wed	1700	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
629	19.668	07/06/15	Mon	0530	Angle	0	0	1	Night	Dry	Not Coded
630	19.668	11/24/15	Tue	1910	Rear-End	0	0	1	Night	Dry	No Contributing Action
631	19.670	05/25/15	Mon	1230	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
632	19.670	11/12/15	Thu	1800	Sideswipe	0	0	1	Night	Dry	No Contributing Action
633	19.671	09/26/15	Sat	2339	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
634	19.672	08/02/15	Sun	1945	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
635	19.673	01/21/15	Wed	1747	Not Coded	0	0	1	Night	Dry	Improper Passing
636	19.674	01/04/15	Sun	0240	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
637	19.674	01/23/15	Fri	1028	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
638	19.674	10/03/15	Sat	1930	Angle	0	0	1	Night	Dry	Failed To Keep In Proper Lane
639	19.674	12/08/15	Tue	1900	Left-Turn	0	0	1	Night	Wet	Failed to Yield Right-Of-Way
640	19.677	02/06/15	Fri	0815	Rear-End	0	0	1	Day	Dry	Other Contributing Action
641	19.678	11/27/15	Fri	1900	Rear-End	0	0	1	Night	Dry	Not Coded
642	19.679	05/22/15	Fri	1600	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
643	19.683	03/04/15	Wed	1300	Pedestrian	0	1	0	Day	Dry	No Contributing Action
644	19.683	05/14/15	Thu	1400	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
645	19.683	05/16/15	Sat	1740	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
646	19.683	12/02/15	Wed	1404	Angle	0	0	1	Day	Wet	Improper Passing
647	19.687	11/19/15	Thu	0814	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
648	19.702	10/26/15	Mon	1151	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000													STATE ROUTE: 112						
ROADWAY LIMITS: MP 0.000 to MP 0.500													M.P. 0.000		TO 4.910		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2015													TO 12/ 2015		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)					
649	19.702	12/15/15	Tue	0723	Angle				0	0	1	Day	Dry	No Contributing Action					
650	19.761	12/08/15	Tue	1100	Angle				0	0	1	Day	Wet	Careless or Negligent Manner					
651	19.774	12/22/15	Tue	1248	Rear-End				0	0	1	Day	Dry	Followed too Closely					
652	19.780	01/16/15	Fri	2323	Rear-End				0	0	1	Night	Dry	Followed too Closely					
653	19.780	01/17/15	Sat	1300	Rear-End				0	0	1	Day	Dry	Other Contributing Action					
654	19.780	11/05/15	Thu	2035	Rear-End				0	0	1	Night	Dry	Followed too Closely					
655	19.799	05/06/15	Wed	1817	Not Coded				0	0	1	Day	Dry	Not Coded					
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
655	3	171	482	360	0	96	16	2	92	2	9	3	39	0	31				
Percent	0.46%	26.11%	73.59%	54.96%	0.00%	14.66%	2.44%	0.31%	14.05%	0.31%	1.37%	0.46%	5.95%	0.00%	4.73%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	463	192	77	577	217	42	6	4	2	13	2	2	0	8	0				
Percent	70.69%	29.31%	11.76%	88.09%	33.13%	6.41%	0.92%	0.61%	0.31%	1.98%	0.31%	0.31%	0.00%	1.22%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 108,456									SEGMENT CRASH RATE: 2.361 CRASHES PER MILLION VEHICLE MILES										

SEGMENTS CRASH DATA (2011-2015)

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87003000 STATE ROUTE: 112
ROADWAY LIMITS: MP 3.810 TO MP 4.132 M.P. 3.810 TO 4.132 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2011 TO 12/ 2011 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	3.871	07/04/11	Mon	0442	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
2	3.905	06/20/11	Mon	1545	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
3	3.934	06/26/11	Sun	2115	Rear-End	0	2	0	Night	Wet	Careless or Negligent Manner
4	3.961	03/27/11	Sun	0503	Rear-End	0	0	1	Night	Dry	#N/A
5	4.017	01/30/11	Sun	0920	Rear-End	0	1	0	Day	Dry	No Contributing Action
6	4.017	09/22/11	Thu	1805	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
7	4.036	08/24/11	Wed	0830	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
7	0	4	3	6	0	0	0	0	1	0	0	0	0	0	0
Percent	0.00%	57.14%	42.86%	85.71%	0.00%	0.00%	0.00%	0.00%	14.29%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	4	3	2	5	4	0	0	0	0	0	0	0	0	0	0
Percent	57.14%	42.86%	28.57%	71.43%	57.14%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **110,000** SEGMENT CRASH RATE: **0.541** CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87003000				STATE ROUTE:		112			
ROADWAY LIMITS:		MP 3.810 TO MP 4.132				M.P. 3.810 TO 4.132		ENGINEER: FDOT D6			
STUDY PERIOD:		FROM 1/ 2012		TO 12/ 2012		COUNTY: Miami-Dade					
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	4.094	04/30/12	Mon	1628	Overturn/Rollover	0	1	0	Day	Wet	No Contributing Action
2	4.098	03/26/12	Mon	1250	Rear-End	0	3	0	Day	Dry	#N/A
3	4.107	07/17/12	Tue	1416	Sideswipe	0	0	1	Day	Wet	Failed to Yield Right-Of-Way

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87003000										STATE ROUTE:		112			
ROADWAY LIMITS:		MP 3.810 TO MP 4.132							M.P. 3.810		TO 4.132		ENGINEER: FDOT D6				
STUDY PERIOD:		FROM 1/ 2012			TO 12/ 2012			COUNTY: Miami-Dade									
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)				
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other		
3	0	2	1	1	0	0	0	0	1	0	0	0	0	0	0		
Percent	0.00%	66.67%	33.33%	33.33%	0.00%	0.00%	0.00%	0.00%	33.33%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%		
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way		
Total	3	0	2	1	0	1	0	0	0	0	0	0	0	0	0		
Percent	100.00%	0.00%	66.67%	33.33%	0.00%	33.33%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%		
TOTAL ENTERING VEHICLES/ADT: 100,500								SEGMENT CRASH RATE: 0.254 CRASHES PER MILLION VEHICLE MILES									

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87003000 STATE ROUTE: 112
ROADWAY LIMITS: MP 3.810 TO MP 4.132 M.P. 3.810 TO 4.132 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)								
1	3.862	01/06/13	Sun	1345	Rear-End			0	0	1	Day	Dry	Careless or Negligent Manner								
2	3.862	03/15/13	Fri	2309	Backed Into			0	0	1	Night	Dry	Improper Backing								
3	3.862	07/24/13	Wed	0915	Angle			0	0	1	Day	Dry	Careless or Negligent Manner								
4	3.867	10/22/13	Tue	1655	Rear-End			0	2	0	Day	Dry	Careless or Negligent Manner								
5	3.886	08/10/13	Sat	1710	Rear-End			0	2	0	Day	Dry	Careless or Negligent Manner								
6	3.943	08/13/13	Tue	2320	Concrete Traffic Barrier			0	2	0	Night	Dry	Careless or Negligent Manner								
7	3.947	02/16/13	Sat	1846	Rear-End			0	0	1	Night	Dry	Careless or Negligent Manner								
8	3.962	10/08/13	Tue	1830	Rear-End			0	1	0	Night	Dry	#N/A								
9	3.971	07/07/13	Sun	1800	Rear-End			0	0	1	Day	Dry	Other Contributing Action								
10	4.026	05/30/13	Thu	1737	Rear-End			0	0	1	Day	Wet	Failed to Yield Right-Of-Way								
11	4.055	07/26/13	Fri	1555	Thrown or Falling Object			0	0	1	Day	Dry	No Contributing Action								
12	4.093	01/12/13	Sat	1855	Sideswipe			1	0	0	Night	Dry	#N/A								
13	4.107	02/19/13	Tue	1745	Rear-End			0	2	0	Day	Dry	No Contributing Action								
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other						
13	1	5	7	8	0	1	0	0	1	1	0	0	1	0	0						
Percent	7.69%	38.46%	53.85%	61.54%	0.00%	7.69%	0.00%	0.00%	7.69%	7.69%	0.00%	0.00%	7.69%	0.00%	0.00%						
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way						
Total	8	5	1	12	6	1	0	0	0	0	0	0	0	1	0						
Percent	61.54%	38.46%	7.69%	92.31%	46.15%	7.69%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	7.69%	0.00%						
TOTAL ENTERING VEHICLES/ADT:								104,000	SEGMENT CRASH RATE:								1.064	CRASHES PER MILLION VEHICLE MILES			

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87003000													STATE ROUTE: 112				
ROADWAY LIMITS: MP 3.810 TO MP 4.132													M.P. 3.810 TO 4.132		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014													COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)			
1	3.848	06/02/14	Mon	1430	Concrete Traffic Barrier				0	0	1	Day	Dry	#N/A			
2	3.905	12/24/14	Wed	0710	Other Non-Fixed Object				0	0	1	Day	Dry	Other Contributing Action			
3	3.934	11/19/14	Wed	1100	Sideswipe				0	0	1	Day	Wet	#N/A			
4	3.943	05/21/14	Wed	0000	Sideswipe				0	3	0	Night	Dry	Other Contributing Action			
5	3.971	12/06/14	Sat	2037	Concrete Traffic Barrier				0	0	1	Night	Dry	Other Contributing Action			
6	4.098	03/31/14	Mon	1245	Sideswipe				0	0	1	Day	Dry	#N/A			
7	4.112	06/09/14	Mon	1700	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner			
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other		
7	0	1	6	1	0	0	0	0	3	0	0	0	1	0	0		
Percent	0.00%	14.29%	85.71%	14.29%	0.00%	0.00%	0.00%	0.00%	42.86%	0.00%	0.00%	0.00%	14.29%	0.00%	0.00%		
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way		
Total	5	2	1	6	6	1	0	0	0	0	0	0	0	0	0		
Percent	71.43%	28.57%	14.29%	85.71%	85.71%	14.29%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%		
TOTAL ENTERING VEHICLES/ADT: 104,000									SEGMENT CRASH RATE: 1.064 CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87003000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 3.810 TO MP 4.132 M.P. 3.810 TO 4.132 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	3.810	04/24/15	Fri	1100	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner		
2	3.862	04/15/15	Wed	0810	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner		
3	3.905	10/26/15	Mon	0650	Sideswipe				0	0	1	Day	Dry	#N/A		
4	3.905	11/10/15	Tue	0816	Rear-End				0	1	0	Day	Dry	No Contributing Action		
5	3.924	05/06/15	Wed	1325	Rear-End				0	3	0	Day	Dry	#N/A		
6	3.924	04/21/15	Tue	0830	Rear-End				0	1	0	Day	Wet	Careless or Negligent Manner		
7	3.943	03/13/15	Fri	1909	Sideswipe				0	0	1	Night	Dry	#N/A		
8	3.965	06/13/15	Sat	0102	Pedestrian				1	0	0	Night	Dry	No Contributing Action		
9	4.026	09/15/15	Tue	0750	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner		
10	4.057	02/08/15	Sun	2105	Pedestrian				0	1	0	Night	Dry	No Contributing Action		
11	4.074	04/11/15	Sat	0820	Sideswipe				0	0	1	Day	Dry	#N/A		
12	4.093	11/07/15	Sat	1709	Rear-End				0	0	1	Night	Dry	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
12	1	4	7	7	0	0	0	0	3	0	2	0	1	0	0	
Percent	8.33%	33.33%	58.33%	58.33%	0.00%	0.00%	0.00%	0.00%	25.00%	0.00%	16.67%	0.00%	8.33%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	8	4	1	11	6	1	0	0	0	0	0	0	0	0	0	
Percent	66.67%	33.33%	8.33%	91.67%	50.00%	8.33%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 104,000									SEGMENT CRASH RATE: 1.064 CRASHES PER MILLION VEHICLE MILES							

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000													STATE ROUTE: 112						
ROADWAY LIMITS: MP 0.000 to MP 0.486													M.P. 0.000		TO 0.486		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2011													TO 12/ 2011		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)					
1	0.000	07/18/11	Mon	1557	Other Non-Fixed Object				0	1	0	Day	Wet	No Contributing Action					
2	0.038	09/21/11	Wed	0910	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner					
3	0.129	08/19/11	Fri	1335	Guardrail Face				0	0	1	Day	Wet	#N/A					
4	0.228	08/17/11	Wed	1500	Sideswipe				0	1	0	Day	Dry	#N/A					
5	0.378	08/22/11	Mon	1200	Rear-End				0	2	0	Day	Dry	Careless or Negligent Manner					
6	0.388	07/03/11	Sun	0518	Rear-End				0	1	0	Night	Dry	Careless or Negligent Manner					
7	0.427	01/31/11	Mon	1100	Rear-End				0	2	0	Day	Dry	Careless or Negligent Manner					
8	0.427	01/31/11	Mon	1100	Rear-End				0	2	0	Day	Dry	Careless or Negligent Manner					
9	0.440	08/22/11	Mon	1108	Other Non-Fixed Object				0	1	0	Day	Dry	Careless or Negligent Manner					
10	0.440	12/03/11	Sat	0547	Other Non-Fixed Object				0	1	0	Night	Dry	Careless or Negligent Manner					
11	0.450	05/13/11	Fri	1740	Rear-End				0	0	1	Day	Dry	Failed To Keep In Proper Lane					
12	0.459	10/31/11	Mon	0400	Concrete Traffic Barrier				0	0	1	Night	Wet	Swerved Or Avoided					
13	0.459	04/13/11	Wed	0815	Sideswipe				0	0	1	Day	Dry	Other Contributing Action					
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
13	0	8	5	6	0	0	0	0	2	0	0	0	2	0	0				
Percent	0.00%	61.54%	38.46%	46.15%	0.00%	0.00%	0.00%	0.00%	15.38%	0.00%	0.00%	0.00%	15.38%	0.00%	0.00%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	10	3	3	10	7	0	0	0	0	0	0	0	0	0	0				
Percent	76.92%	23.08%	23.08%	76.92%	53.85%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 100,004									SEGMENT CRASH RATE: 0.733 CRASHES PER MILLION VEHICLE MILES										

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.000 to MP 0.486 M.P. 0.000 TO 0.486 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.000	05/23/12	Wed	0910	Sideswipe	0	0	1	Day	Wet	No Contributing Action
2	0.000	02/05/12	Sun	1020	Other Non-Fixed Object	0	0	1	Day	Dry	No Contributing Action
3	0.000	05/20/12	Sun	0350	Not Coded	0	0	1	Night	Dry	#N/A
4	0.000	05/07/12	Mon	0900	Angle	0	0	1	Day	Dry	No Contributing Action
5	0.000	12/06/12	Thu	0803	Concrete Traffic Barrier	0	0	1	Day	Dry	Careless or Negligent Manner
6	0.278	04/22/12	Sun	1700	Sideswipe	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
7	0.326	04/15/12	Sun	2240	Other Traffic Barrier	0	1	0	Night	Dry	Failed To Keep In Proper Lane
8	0.330	12/16/12	Sun	0352	Rear-End	0	1	0	Night	Dry	Other Contributing Action
9	0.378	01/05/12	Thu	1235	Other Non-Fixed Object	0	0	1	Day	Dry	No Contributing Action
10	0.378	02/03/12	Fri	0915	Sideswipe	0	0	1	Day	Dry	#N/A
11	0.378	02/16/12	Thu	1215	Rear-End	0	2	0	Day	Dry	Other Contributing Action
12	0.392	05/31/12	Thu	1114	Guardrail Face	0	0	1	Day	Wet	Careless or Negligent Manner
13	0.456	05/22/12	Tue	1440	Sideswipe	0	0	1	Day	Wet	No Contributing Action
14	0.459	09/11/12	Tue	1340	Not Coded	0	0	1	Day	Dry	#N/A

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
14	0	3	11	2	0	1	0	0	4	0	0	0	3	0	2
Percent	0.00%	21.43%	78.57%	14.29%	0.00%	7.14%	0.00%	0.00%	28.57%	0.00%	0.00%	0.00%	21.43%	0.00%	14.29%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	11	3	3	11	2	1	0	0	0	0	0	0	0	1	0
Percent	78.57%	21.43%	21.43%	78.57%	14.29%	7.14%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	7.14%	0.00%

TOTAL ENTERING VEHICLES/ADT: **98,075** SEGMENT CRASH RATE: **0.805** CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.000 to MP 0.486 M.P. 0.000 TO 0.486 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.000	05/28/13	Tue	0725	Rear-End				0	1	0	Day	Dry	Careless or Negligent Manner		
2	0.000	07/22/13	Mon	0730	Rear-End				0	0	1	Day	Dry	Failed to Yield Right-Of-Way		
3	0.000	07/26/13	Fri	1210	Sideswipe				0	0	1	Day	Dry	Other Contributing Action		
4	0.000	05/17/13	Fri	1035	Sideswipe				0	0	1	Day	Dry	#N/A		
5	0.106	06/07/13	Fri	1750	Rear-End				0	0	1	Day	Wet	Careless or Negligent Manner		
6	0.175	10/12/13	Sat	0140	Other Non-Fixed Object				0	1	0	Night	Dry	Other Contributing Action		
7	0.378	03/07/13	Thu	1115	Sideswipe				0	0	1	Day	Dry	No Contributing Action		
8	0.378	05/26/13	Sun	1205	Guardrail Face				0	1	0	Day	Dry	Swerved Or Avoided		
9	0.392	04/26/13	Fri	1615	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner		
10	0.392	09/17/13	Tue	1427	Rear-End				0	0	1	Day	Dry	#N/A		
11	0.421	04/02/13	Tue	2015	Rear-End				0	1	0	Night	Dry	Failed to Yield Right-Of-Way		
12	0.447	07/11/13	Thu	2145	Sideswipe				0	0	1	Night	Dry	Failed to Yield Right-Of-Way		
13	0.466	10/04/13	Fri	1120	Sideswipe				0	1	0	Day	Dry	Failed To Keep In Proper Lane		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
13	0	5	8	6	0	0	0	0	5	0	0	0	1	0	0	
Percent	0.00%	38.46%	61.54%	46.15%	0.00%	0.00%	0.00%	0.00%	38.46%	0.00%	0.00%	0.00%	7.69%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	10	3	1	12	3	3	0	0	0	0	0	0	0	0	0	
Percent	76.92%	23.08%	7.69%	92.31%	23.08%	23.08%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 101,521									SEGMENT CRASH RATE: 0.722 CRASHES PER MILLION VEHICLE MILES							

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.000 to MP 0.486 M.P. 0.000 TO 0.486 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.000	07/15/14	Tue	1530	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
2	0.000	11/19/14	Wed	0845	Sideswipe	0	0	1	Day	Dry	#N/A
3	0.022	02/01/14	Sat	1849	Not Coded	0	0	1	Night	Dry	Improper Passing
4	0.096	08/27/14	Wed	1750	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
5	0.114	05/09/14	Fri	1630	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
6	0.225	03/04/14	Tue	0905	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
7	0.278	04/01/14	Tue	0940	Not Coded	0	1	0	Day	Dry	Failed To Keep In Proper Lane
8	0.313	02/08/14	Sat	0330	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
9	0.375	01/20/14	Mon	1230	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
10	0.380	12/26/14	Fri	0943	Sideswipe	0	0	1	Day	Wet	No Contributing Action
11	0.383	02/23/14	Sun	0300	Rear-End	0	0	1	Night	Dry	#N/A
12	0.392	09/05/14	Fri	1133	Sideswipe	0	1	0	Day	Dry	Careless or Negligent Manner
13	0.429	06/06/14	Fri	1515	Angle	0	0	1	Day	Dry	Other Contributing Action
14	0.456	01/31/14	Fri	0102	Concrete Traffic Barrier	0	0	1	Night	Wet	Careless or Negligent Manner
15	0.456	10/07/14	Tue	0640	Impact Attenuator/Crash Cushion	0	1	0	Night	Dry	Careless or Negligent Manner
16	0.456	12/08/14	Mon	1900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
17	0.467	08/26/14	Tue	1320	Rear-End	0	2	0	Day	Dry	Improper Passing

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
17	0	7	10	8	0	1	0	0	4	0	0	0	1	0	0
Percent	0.00%	41.18%	58.82%	47.06%	0.00%	5.88%	0.00%	0.00%	23.53%	0.00%	0.00%	0.00%	5.88%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	12	5	2	15	3	3	0	0	0	0	0	0	0	1	0
Percent	70.59%	29.41%	11.76%	88.24%	17.65%	17.65%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	5.88%	0.00%

TOTAL ENTERING VEHICLES/ADT: **101,521** SEGMENT CRASH RATE: **0.722 CRASHES PER MILLION VEHICLE MILES**

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.000 to MP 0.486 M.P. 0.000 TO 0.486 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)				
1	0.000	03/27/15	Fri	2351	Rear-End			0	2	0	Night	Dry	Careless or Negligent Manner				
2	0.100	08/01/15	Sat	0030	Sideswipe			0	0	1	Night	Dry	#N/A				
3	0.116	04/16/15	Thu	1336	Rear-End			0	1	0	Day	Dry	Careless or Negligent Manner				
4	0.225	03/17/15	Tue	1854	Sideswipe			0	0	1	Day	Dry	No Contributing Action				
5	0.225	08/26/15	Wed	1100	Sideswipe			0	0	1	Day	Dry	Careless or Negligent Manner				
6	0.255	02/13/15	Fri	1620	Rear-End			0	0	1	Day	Dry	Careless or Negligent Manner				
7	0.375	04/18/15	Sat	0205	Rear-End			0	0	1	Night	Dry	Careless or Negligent Manner				
8	0.375	05/26/15	Tue	0940	Sideswipe			0	0	1	Day	Dry	Other Contributing Action				
9	0.375	08/27/15	Thu	1759	Not Coded			0	0	1	Day	Dry	No Contributing Action				
10	0.375	10/08/15	Thu	0900	Angle			0	0	1	Day	Dry	No Contributing Action				
11	0.380	09/16/15	Wed	1710	Rear-End			0	0	1	Day	Wet	Careless or Negligent Manner				
12	0.392	09/11/15	Fri	1716	Rear-End			0	1	0	Day	Dry	Careless or Negligent Manner				
13	0.392	09/13/15	Sun	0437	Not Coded			0	1	0	Night	Dry	Erratic, Reckless or Aggressive				
14	0.399	11/28/15	Sat	0525	Rear-End			0	1	0	Night	Dry	#N/A				
15	0.437	07/19/15	Sun	1320	Rear-End			0	0	1	Day	Dry	Drove too Fast for Conditions				
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other		
15	0	5	10	8	0	1	0	0	4	0	0	0	1	0	0		
Percent	0.00%	33.33%	66.67%	53.33%	0.00%	6.67%	0.00%	0.00%	26.67%	0.00%	0.00%	0.00%	6.67%	0.00%	0.00%		
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl	Erratic/Aggress	Ran off Road	DUI	Wrong Way		
Total	10	5	1	14	3	3	0	0	0	0	0	0	0	0	0		
Percent	66.67%	33.33%	6.67%	93.33%	20.00%	20.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%		
TOTAL ENTERING VEHICLES/ADT:								101,521	SEGMENT CRASH RATE:								0.722 CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.838 To MP 2.046 M.P. 0.838 TO 2.046 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2011 TO 12/ 2011 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.949	07/20/11	Wed	2100	Concrete Traffic Barrier	0	0	1	Night	Dry	#N/A
2	0.978	11/02/11	Wed	0955	Rear-End	0	1	0	Day	Dry	Other Contributing Action
3	0.978	11/02/11	Wed	1040	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
4	0.989	09/18/11	Sun	0136	Concrete Traffic Barrier	0	1	0	Night	Dry	Careless or Negligent Manner
5	0.993	06/16/11	Thu	1545	Rear-End	0	2	0	Day	Dry	#N/A
6	1.028	08/16/11	Tue	1210	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
7	1.028	08/12/11	Fri	1751	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
8	1.028	12/09/11	Fri	1212	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
9	1.028	11/17/11	Thu	1905	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
10	1.028	09/20/11	Tue	0929	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
11	1.028	11/28/11	Mon	1937	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
12	1.028	12/05/11	Mon	1718	Rear-End	0	1	0	Night	Dry	No Contributing Action
13	1.038	08/12/11	Fri	1030	Rear-End	0	0	1	Day	Dry	No Contributing Action
14	1.043	03/17/11	Thu	0700	Rear-End	0	0	1	Day	Dry	#N/A
15	1.043	03/17/11	Thu	0930	Sideswipe	0	0	1	Day	Dry	No Contributing Action
16	1.043	03/05/11	Sat	0357	Curb	0	1	0	Night	Dry	Failed To Keep In Proper Lane
17	1.078	12/17/11	Sat	1145	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
18	1.083	10/20/11	Thu	0955	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
19	1.083	10/20/11	Thu	1041	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
20	1.089	11/28/11	Mon	1218	Sideswipe	0	0	1	Day	Dry	No Contributing Action
21	1.100	08/11/11	Thu	0930	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
22	1.100	08/11/11	Thu	1510	Sideswipe	0	0	1	Day	Dry	#N/A
23	1.110	03/02/11	Wed	0920	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
24	1.110	03/29/11	Tue	0934	Sideswipe	0	0	1	Day	Dry	#N/A
25	1.119	07/02/11	Sat	1200	Rear-End	0	0	1	Day	Dry	No Contributing Action
26	1.119	02/18/11	Fri	1914	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
27	1.119	11/16/11	Wed	0715	Rear-End	0	1	0	Day	Dry	Other Contributing Action
28	1.129	04/29/11	Fri	1750	Rear-End	0	1	0	Day	Dry	Followed too Closely
29	1.136	02/22/11	Tue	0825	Rear-End	0	4	0	Day	Dry	Careless or Negligent Manner
30	1.178	12/13/11	Tue	1205	Rear-End	0	4	0	Day	Dry	Careless or Negligent Manner
31	1.183	04/21/11	Thu	2215	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
32	1.183	09/12/11	Mon	2323	Sideswipe	0	0	1	Night	Dry	No Contributing Action
33	1.183	07/16/11	Sat	0655	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
34	1.183	10/31/11	Mon	1325	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
35	1.183	11/15/11	Tue	1145	Sideswipe	0	2	0	Day	Dry	Other Contributing Action
36	1.187	09/26/11	Mon	1055	Angle	0	0	1	Day	Wet	Careless or Negligent Manner
37	1.187	08/24/11	Wed	1645	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
38	1.196	06/11/11	Sat	0040	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
39	1.196	05/30/11	Mon	0450	Sideswipe	0	6	0	Night	Dry	Failed To Keep In Proper Lane
40	1.206	03/25/11	Fri	1825	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
41	1.206	05/10/11	Tue	1611	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
42	1.206	09/09/11	Fri	0750	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
43	1.221	03/27/11	Sun	1809	Rear-End	0	2	0	Day	Dry	No Contributing Action
44	1.221	05/11/11	Wed	1355	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
45	1.221	11/15/11	Tue	1845	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
46	1.240	07/24/11	Sun	0535	Sideswipe	0	2	0	Night	Wet	Careless or Negligent Manner
47	1.240	08/13/11	Sat	1412	Not Coded	0	2	0	Day	Dry	Careless or Negligent Manner
48	1.240	10/27/11	Thu	1717	Rear-End	0	0	1	Day	Dry	No Contributing Action
49	1.240	11/19/11	Sat	1955	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
50	1.259	09/30/11	Fri	2005	Not Coded	0	1	0	Night	Wet	No Contributing Action
51	1.259	11/24/11	Thu	1410	Impact Attenuator/Crash Cushion	0	1	0	Day	Dry	Careless or Negligent Manner
52	1.268	08/11/11	Thu	1115	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
53	1.269	10/28/11	Fri	1418	Rear-End	0	0	1	Day	Dry	No Contributing Action
54	1.269	11/03/11	Thu	1950	Rear-End	0	3	0	Night	Dry	Careless or Negligent Manner
55	1.272	11/29/11	Tue	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
56	1.278	10/08/11	Sat	1130	Sideswipe	0	0	1	Day	Wet	Careless or Negligent Manner
57	1.358	08/18/11	Thu	1005	Rear-End	0	0	1	Day	Wet	#N/A
58	1.373	09/23/11	Fri	0745	Concrete Traffic Barrier	0	0	1	Day	Dry	#N/A
59	1.418	08/13/11	Sat	1650	Rear-End	0	0	1	Day	Dry	#N/A
60	1.488	08/22/11	Mon	1115	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
61	1.489	09/28/11	Wed	1705	Concrete Traffic Barrier	0	2	0	Day	Dry	Ran Off Roadway
62	1.555	09/25/11	Sun	1539	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
63	1.565	09/06/11	Tue	1755	Rear-End	0	0	1	Day	Dry	Improper Passing
64	1.583	11/11/11	Fri	0434	Guardrail Face	0	1	0	Night	Dry	#N/A
65	1.592	04/29/11	Fri	1602	Concrete Traffic Barrier	0	1	0	Day	Wet	No Contributing Action
66	1.592	10/27/11	Thu	1827	Sideswipe	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
67	1.602	08/27/11	Sat	1555	Rear-End	0	6	0	Day	Dry	Careless or Negligent Manner
68	1.611	05/03/11	Tue	2325	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
69	1.611	03/21/11	Mon	0435	Angle	0	1	0	Day	Dry	Careless or Negligent Manner
70	1.611	03/09/11	Wed	0045	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
71	1.611	11/27/11	Sun	0420	Other Traffic Barrier	0	0	1	Night	Dry	No Contributing Action
72	1.621	06/22/11	Wed	1610	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
73	1.621	05/02/11	Mon	0030	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
74	1.621	05/27/11	Fri	1533	Not Coded	0	0	1	Day	Dry	#N/A
75	1.621	08/05/11	Fri	0012	Other Non-Fixed Object	0	0	1	Night	Dry	Careless or Negligent Manner
76	1.621	11/20/11	Sun	0559	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
77	1.640	08/31/11	Wed	2320	Concrete Traffic Barrier	0	0	1	Night	Wet	No Contributing Action
78	1.678	04/28/11	Thu	1750	Rear-End	0	1	0	Day	Dry	Other Contributing Action
79	1.678	12/01/11	Thu	1813	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
80	1.683	09/03/11	Sat	0225	Concrete Traffic Barrier	0	0	1	Night	Wet	Careless or Negligent Manner
81	1.753	04/17/11	Sun	0545	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000													STATE ROUTE: 112				
ROADWAY LIMITS: MP 0.838 To MP 2.046													M.P. 0.838 TO 2.046		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2011 TO 12/ 2011													COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)			
82	1.774	03/17/11	Thu	0635	Concrete Traffic Barrier				0	0	1	Day	Dry	#N/A			
83	1.774	06/14/11	Tue	0730	Other Non-Collision				0	1	0	Day	Dry	Other Contributing Action			
84	1.783	09/25/11	Sun	1030	Rear-End				1	2	0	Day	Dry	Careless or Negligent Manner			
85	1.833	01/24/11	Mon	0930	Rear-End				0	1	0	Day	Dry	Erratic, Reckless or Aggressive			
86	1.833	03/14/11	Mon	1745	Sideswipe				0	2	0	Day	Dry	Failed To Keep In Proper Lane			
87	1.833	10/01/11	Sat	1810	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner			
88	1.983	05/06/11	Fri	1057	Rear-End				0	1	0	Day	Wet	#N/A			
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other		
88	1	42	46	57	0	2	0	0	12	0	0	0	12	0	3		
Percent	1.14%	47.73%	52.27%	64.77%	0.00%	2.27%	0.00%	0.00%	13.64%	0.00%	0.00%	0.00%	13.64%	0.00%	3.41%		
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way		
Total	62	26	9	79	49	1	0	0	0	1	0	1	1	1	0		
Percent	70.45%	29.55%	10.23%	89.77%	55.68%	1.14%	0.00%	0.00%	0.00%	1.14%	0.00%	1.14%	1.14%	1.14%	0.00%		
TOTAL ENTERING VEHICLES/ADT: 113,170									SEGMENT CRASH RATE: 1.764 CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.838 To MP 2.046 M.P. 0.838 TO 2.046 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.888	06/16/12	Sat	2311	Sideswipe	0	0	1	Night	Dry	#N/A
2	0.899	05/10/12	Thu	1200	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
3	0.928	07/10/12	Tue	1302	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
4	0.978	03/05/12	Mon	1742	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
5	0.978	05/05/12	Sat	1620	Angle	0	0	1	Day	Dry	#N/A
6	0.989	03/02/12	Fri	1710	Rear-End	0	0	1	Day	Dry	Followed too Closely
7	0.989	06/06/12	Wed	1800	Rear-End	0	0	1	Day	Dry	Other Contributing Action
8	0.989	06/06/12	Wed	2317	Rear-End	0	0	1	Night	Dry	No Contributing Action
9	0.989	09/28/12	Fri	1710	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
10	1.024	05/20/12	Sun	1140	Other Fixed Object	0	2	0	Day	Dry	#N/A
11	1.028	02/13/12	Mon	1303	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
12	1.038	09/19/12	Wed	1510	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
13	1.043	10/31/12	Wed	2130	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
14	1.078	02/04/12	Sat	1745	Not Coded	0	0	1	Day	Dry	#N/A
15	1.078	01/05/12	Thu	0815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
16	1.078	07/24/12	Tue	1007	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
17	1.078	07/24/12	Tue	1314	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
18	1.081	06/03/12	Sun	0150	Sideswipe	0	1	0	Night	Dry	Careless or Negligent Manner
19	1.108	11/14/12	Wed	1820	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
20	1.119	06/03/12	Sun	1435	Angle	0	0	1	Day	Dry	#N/A
21	1.129	08/21/12	Tue	0815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
22	1.164	08/11/12	Sat	1020	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
23	1.164	08/18/12	Sat	0840	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
24	1.168	07/19/12	Thu	0439	Rear-End	0	3	0	Night	Dry	Careless or Negligent Manner
25	1.178	01/13/12	Fri	0400	Sideswipe	0	1	0	Night	Wet	Failed to Yield Right-Of-Way
26	1.178	01/23/12	Mon	1540	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
27	1.178	07/06/12	Fri	1338	Angle	0	0	1	Day	Dry	No Contributing Action
28	1.178	09/29/12	Sat	0642	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
29	1.178	10/03/12	Wed	1400	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
30	1.178	12/01/12	Sat	0500	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
31	1.183	05/11/12	Fri	1855	Sideswipe	0	0	1	Day	Dry	Erratic, Reckless or Aggressive
32	1.183	06/28/12	Thu	1050	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
33	1.183	06/28/12	Thu	1135	Angle	0	1	0	Day	Dry	Careless or Negligent Manner
34	1.183	08/20/12	Mon	1500	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
35	1.183	10/24/12	Wed	1255	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
36	1.187	07/17/12	Tue	0045	Sideswipe	0	0	1	Night	Dry	#N/A
37	1.196	05/09/12	Wed	0825	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
38	1.196	04/08/12	Sun	1325	Angle	0	3	0	Day	Dry	#N/A
39	1.202	05/22/12	Tue	2340	Rear-End	0	0	1	Night	Wet	No Contributing Action
40	1.221	02/07/12	Tue	0820	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
41	1.221	01/12/12	Thu	1600	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
42	1.250	08/11/12	Sat	0715	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
43	1.250	06/22/12	Fri	2105	Concrete Traffic Barrier	0	0	1	Night	Wet	#N/A
44	1.250	12/04/12	Tue	1900	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
45	1.259	03/06/12	Tue	0735	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
46	1.259	04/29/12	Sun	0200	Concrete Traffic Barrier	0	1	0	Night	Wet	Careless or Negligent Manner
47	1.259	05/26/12	Sat	2220	Not Coded	0	1	0	Night	Dry	#N/A
48	1.259	10/03/12	Wed	1440	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
49	1.259	07/27/12	Fri	0948	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
50	1.259	11/19/12	Mon	0848	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
51	1.266	10/13/12	Sat	0624	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
52	1.269	10/13/12	Sat	0540	Concrete Traffic Barrier	0	0	1	Night	Wet	No Contributing Action
53	1.270	08/04/12	Sat	0410	Concrete Traffic Barrier	1	1	0	Night	Dry	No Contributing Action
54	1.275	04/09/12	Mon	0915	Rear-End	0	9	0	Day	Dry	No Contributing Action
55	1.276	10/04/12	Thu	1654	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
56	1.278	07/27/12	Fri	0700	Concrete Traffic Barrier	0	0	1	Day	Dry	No Contributing Action
57	1.316	10/08/12	Mon	2150	Other Non-Fixed Object	0	0	1	Night	Dry	#N/A
58	1.373	07/12/12	Thu	1445	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
59	1.602	03/07/12	Wed	1129	Sideswipe	0	0	1	Day	Dry	Improper Passing
60	1.611	05/05/12	Sat	0750	Rear-End	0	1	0	Day	Dry	No Contributing Action
61	1.611	06/09/12	Sat	0010	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
62	1.611	06/09/12	Sat	0115	Rear-End	0	3	0	Night	Dry	Careless or Negligent Manner
63	1.611	06/04/12	Mon	0200	Rear-End	0	0	1	Night	Dry	No Contributing Action
64	1.621	04/21/12	Sat	1830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
65	1.621	05/30/12	Wed	1545	Not Coded	0	0	1	Day	Wet	Drove too Fast for Conditions
66	1.621	08/01/12	Wed	1855	Other Traffic Barrier	0	1	0	Day	Dry	Drove too Fast for Conditions
67	1.621	08/10/12	Fri	1135	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
68	1.621	09/22/12	Sat	0830	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
69	1.646	08/30/12	Thu	1717	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
70	1.659	07/22/12	Sun	1625	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
71	1.678	07/12/12	Thu	1725	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
72	1.683	03/26/12	Mon	1739	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
73	1.683	04/06/12	Fri	1439	Rear-End	0	0	1	Day	Dry	Improper Passing
74	1.683	05/21/12	Mon	1003	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
75	1.697	02/22/12	Wed	1620	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
76	1.698	12/31/12	Mon	0700	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
77	1.730	08/10/12	Fri	1245	Rear-End	0	3	0	Day	Dry	No Contributing Action
78	1.753	04/07/12	Sat	0115	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
79	1.778	03/01/12	Thu	1548	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
80	1.778	07/13/12	Fri	0830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
81	1.806	11/29/12	Thu	0150	Concrete Traffic Barrier	0	1	0	Night	Wet	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000													STATE ROUTE: 112				
ROADWAY LIMITS: MP 0.838 To MP 2.046													M.P. 0.838 TO 2.046		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012 COUNTY: Miami-Dade																	
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE					FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
82	1.810	04/11/12	Wed	0640	Other Non-Collision					0	1	0	Day	Dry	Failed To Keep In Proper Lane		
83	1.833	03/18/12	Sun	0111	Concrete Traffic Barrier					0	0	1	Night	Wet	Over-Correcting/Over-Steering		
84	1.833	09/10/12	Mon	1519	Rear-End					0	0	1	Day	Wet	Careless or Negligent Manner		
85	1.883	03/26/12	Mon	1626	Rear-End					0	0	1	Day	Dry	Careless or Negligent Manner		
86	1.917	08/25/12	Sat	0007	Not Coded					0	0	1	Night	Wet	Other Contributing Action		
87	1.948	08/24/12	Fri	1710	Angle					0	0	1	Day	Wet	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other		
87	1	29	58	54	0	6	0	0	10	0	0	0	9	0	6		
Percent	1.15%	33.33%	66.67%	62.07%	0.00%	6.90%	0.00%	0.00%	11.49%	0.00%	0.00%	0.00%	10.34%	0.00%	6.90%		
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way		
Total	62	25	18	69	53	2	0	0	2	2	0	1	0	2	0		
Percent	71.26%	28.74%	20.69%	79.31%	60.92%	2.30%	0.00%	0.00%	2.30%	2.30%	0.00%	1.15%	0.00%	2.30%	0.00%		
TOTAL ENTERING VEHICLES/ADT: 113,533										SEGMENT CRASH RATE: 1.738 CRASHES PER MILLION VEHICLE MILES							

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.838 To MP 2.046 M.P. 0.838 TO 2.046 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.854	11/22/13	Fri	0104	Pedestrian	1	0	0	Night	Wet	#N/A
2	0.880	11/25/13	Mon	1052	Sideswipe	0	0	1	Day	Dry	No Contributing Action
3	0.893	08/20/13	Tue	1230	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
4	0.986	12/09/13	Mon	1615	Rear-End	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
5	0.989	01/07/13	Mon	1625	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
6	0.989	01/21/13	Mon	1628	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
7	1.028	05/03/13	Fri	2055	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
8	1.028	09/17/13	Tue	0854	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
9	1.038	09/15/13	Sun	1105	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
10	1.089	05/10/13	Fri	1100	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
11	1.089	06/21/13	Fri	0935	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
12	1.089	10/26/13	Sat	0031	Concrete Traffic Barrier	0	0	1	Night	Dry	Careless or Negligent Manner
13	1.100	05/25/13	Sat	2011	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
14	1.108	02/28/13	Thu	1515	Rear-End	0	0	1	Day	Dry	#N/A
15	1.119	06/30/13	Sun	1750	Sideswipe	0	0	1	Day	Dry	Improper Passing
16	1.126	05/29/13	Wed	1345	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
17	1.138	06/15/13	Sat	0550	Concrete Traffic Barrier	0	0	1	Day	Dry	No Contributing Action
18	1.169	12/03/13	Tue	0930	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
19	1.187	05/15/13	Wed	1831	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
20	1.202	09/29/13	Sun	0916	Concrete Traffic Barrier	0	1	0	Day	Wet	Careless or Negligent Manner
21	1.240	05/16/13	Thu	1603	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
22	1.240	11/13/13	Wed	1335	Not Coded	0	0	1	Day	Wet	Drove too Fast for Conditions
23	1.259	08/08/13	Thu	1853	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
24	1.274	06/08/13	Sat	1531	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
25	1.275	04/20/13	Sat	1145	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
26	1.338	07/03/13	Wed	1925	Concrete Traffic Barrier	0	0	1	Day	Wet	Drove too Fast for Conditions
27	1.505	11/24/13	Sun	1840	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
28	1.547	11/09/13	Sat	0415	Not Coded	0	2	0	Night	Wet	#N/A
29	1.565	02/15/13	Fri	1720	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
30	1.565	05/20/13	Mon	1755	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
31	1.574	11/09/13	Sat	0900	Angle	0	1	0	Day	Dry	No Contributing Action
32	1.603	02/01/13	Fri	0500	Concrete Traffic Barrier	0	1	0	Night	Dry	Careless or Negligent Manner
33	1.603	07/25/13	Thu	1611	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
34	1.603	09/28/13	Sat	1443	Angle	0	0	1	Day	Dry	Over-Correcting/Over-Steering
35	1.641	01/23/13	Wed	1333	Sideswipe	0	1	0	Day	Dry	#N/A
36	1.641	02/14/13	Thu	0932	Pedestrian	0	1	0	Day	Dry	Careless or Negligent Manner
37	1.641	04/27/13	Sat	1035	Rear-End	0	1	0	Day	Dry	Erratic, Reckless or Aggressive
38	1.646	05/10/13	Fri	0025	Rear-End	0	0	1	Night	Dry	Other Contributing Action
39	1.646	08/26/13	Mon	2040	Angle	0	3	0	Night	Dry	Other Contributing Action
40	1.646	12/04/13	Wed	0745	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
41	1.698	06/19/13	Wed	0615	Not Coded	0	1	0	Day	Dry	No Contributing Action
42	1.716	12/25/13	Wed	0335	Concrete Traffic Barrier	0	0	1	Night	Dry	#N/A
43	1.735	10/18/13	Fri	2316	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
44	1.746	05/22/13	Wed	0900	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
45	1.746	06/03/13	Mon	0920	Sideswipe	0	0	1	Day	Dry	#N/A
46	1.796	04/09/13	Tue	0120	Ditch	0	0	1	Night	Dry	Swerved Or Avoided
47	1.796	09/21/13	Sat	0456	Pedestrian	0	1	0	Night	Dry	#N/A
48	1.806	11/23/13	Sat	2345	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
49	1.846	04/06/13	Sat	1300	Angle	0	0	1	Day	Dry	#N/A
50	1.846	08/08/13	Thu	0910	Other Non-Fixed Object	0	0	1	Day	Dry	#N/A
51	1.846	09/22/13	Sun	2245	Guardrail Face	0	1	0	Night	Dry	Other Contributing Action
52	1.946	05/19/13	Sun	0743	Tree (Standing)	0	0	1	Day	Dry	#N/A

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
52	1	17	34	25	0	4	0	0	7	0	3	0	9	0	3
Percent	1.92%	32.69%	65.38%	48.08%	0.00%	7.69%	0.00%	0.00%	13.46%	0.00%	5.77%	0.00%	17.31%	0.00%	5.77%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	37	15	13	39	27	1	0	0	2	1	0	1	0	2	0
Percent	71.15%	28.85%	25.00%	75.00%	51.92%	1.92%	0.00%	0.00%	3.85%	1.92%	0.00%	1.92%	0.00%	3.85%	0.00%

TOTAL ENTERING VEHICLES/ADT: **118,441** SEGMENT CRASH RATE: **0.996** CRASHES PER MILLION VEHICLE MILES

State of Florida Department of Transportation
CRASH SUMMARY

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.838 To MP 2.046 M.P. 0.838 TO 2.046 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.888	12/05/14	Fri	1310	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
2	0.888	12/19/14	Fri	1923	Sideswipe	0	2	0	Night	Dry	#N/A
3	0.938	11/11/14	Tue	2150	Angle	0	1	0	Night	Dry	Failed to Yield Right-Of-Way
4	0.960	12/07/14	Sun	0050	Rear-End	0	4	0	Night	Dry	Careless or Negligent Manner
5	0.968	09/14/14	Sun	1547	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
6	0.986	04/22/14	Tue	1510	Sideswipe	0	0	1	Day	Dry	#N/A
7	1.005	04/12/14	Sat	2046	Rear-End	0	0	1	Night	Dry	Drove too Fast for Conditions
8	1.028	02/12/14	Wed	2108	Sideswipe	0	0	1	Night	Wet	#N/A
9	1.028	10/15/14	Wed	1900	Rear-End	0	0	1	Night	Dry	#N/A
10	1.036	02/22/14	Sat	1218	Impact Attenuator/Crash Cushion	0	1	0	Day	Dry	Other Contributing Action
11	1.038	07/07/14	Mon	0900	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
12	1.038	09/18/14	Thu	1900	Sideswipe	0	2	0	Day	Wet	Other Contributing Action
13	1.038	09/14/14	Sun	1618	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
14	1.040	09/04/14	Thu	1800	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
15	1.046	05/08/14	Thu	0654	Other Non-Collision	0	0	1	Day	Dry	Over-Correcting/Over-Steering
16	1.081	02/08/14	Sat	2047	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
17	1.081	02/16/14	Sun	1419	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
18	1.081	12/03/14	Wed	1944	Sideswipe	0	0	1	Night	Dry	Other Contributing Action
19	1.091	11/21/14	Fri	1237	Concrete Traffic Barrier	0	1	0	Day	Wet	Over-Correcting/Over-Steering
20	1.108	04/24/14	Thu	0812	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
21	1.108	07/18/14	Fri	1955	Not Coded	0	1	0	Day	Wet	Drove too Fast for Conditions
22	1.108	08/27/14	Wed	1625	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
23	1.108	08/27/14	Wed	1620	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
24	1.110	07/29/14	Tue	0000	Parked Motor Vehicle	0	0	1	Day	Dry	#N/A
25	1.110	12/04/14	Thu	1135	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
26	1.138	06/27/14	Fri	0620	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
27	1.164	11/02/14	Sun	1535	Rear-End	0	2	0	Day	Dry	No Contributing Action
28	1.178	10/25/14	Sat	0550	Concrete Traffic Barrier	1	2	0	Night	Dry	No Contributing Action
29	1.183	03/04/14	Tue	0835	Sideswipe	0	1	0	Day	Dry	Failed To Keep In Proper Lane
30	1.183	03/04/14	Tue	0835	Rear-End	0	0	1	Day	Dry	#N/A
31	1.183	10/21/14	Tue	1930	Struck by Falling/Shifting Cargo	0	0	1	Night	Dry	Other Contributing Action
32	1.221	12/08/14	Mon	1845	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
33	1.225	12/15/14	Mon	1420	Sideswipe	0	0	1	Day	Dry	No Contributing Action
34	1.240	09/02/14	Tue	0900	Concrete Traffic Barrier	0	1	0	Day	Wet	Careless or Negligent Manner
35	1.240	10/31/14	Fri	1510	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
36	1.250	03/20/14	Thu	0832	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
37	1.259	01/21/14	Tue	2250	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
38	1.259	03/14/14	Fri	1120	Other Non-Fixed Object	0	1	0	Day	Dry	#N/A
39	1.259	08/18/14	Mon	2154	Sideswipe	0	3	0	Night	Dry	#N/A
40	1.259	11/11/14	Tue	1400	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
41	1.263	12/08/14	Mon	0930	Rear-End	0	0	1	Day	Dry	#N/A
42	1.272	09/02/14	Tue	1004	Rear-End	0	0	1	Day	Wet	#N/A
43	1.272	09/02/14	Tue	1000	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
44	1.278	02/16/14	Sun	1915	Rear-End	0	5	0	Night	Dry	Careless or Negligent Manner
45	1.284	05/24/14	Sat	1550	Not Coded	0	0	1	Day	Dry	No Contributing Action
46	1.306	10/23/14	Thu	0645	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
47	1.373	05/05/14	Mon	0127	Rear-End	0	5	0	Night	Dry	Careless or Negligent Manner
48	1.373	05/06/14	Tue	1858	Sideswipe	0	0	1	Day	Dry	#N/A
49	1.378	11/16/14	Sun	0900	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
50	1.418	09/15/14	Mon	1545	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
51	1.478	05/02/14	Fri	1541	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
52	1.478	08/05/14	Tue	0645	Concrete Traffic Barrier	0	1	0	Day	Dry	No Contributing Action
53	1.505	07/20/14	Sun	2330	Guardrail Face	0	0	1	Night	Dry	Careless or Negligent Manner
54	1.546	09/04/14	Thu	1638	Guardrail Face	0	1	0	Day	Dry	Careless or Negligent Manner
55	1.546	12/12/14	Fri	0715	Rear-End	0	0	1	Day	Dry	Other Contributing Action
56	1.565	03/12/14	Wed	0839	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
57	1.565	04/12/14	Sat	0930	Struck by Falling/Shifting Cargo	0	0	1	Day	Dry	#N/A
58	1.565	08/03/14	Sun	1450	Sideswipe	0	0	1	Day	Dry	Improper Passing
59	1.574	06/09/14	Mon	1825	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
60	1.584	03/20/14	Thu	1012	Sideswipe	0	0	1	Day	Dry	#N/A
61	1.584	07/24/14	Thu	1734	Pedalcycle	0	0	1	Day	Dry	No Contributing Action
62	1.603	04/23/14	Wed	0229	Rear-End	0	1	0	Night	Dry	#N/A
63	1.603	08/26/14	Tue	0412	Not Coded	0	1	0	Night	Wet	Erratic, Reckless or Aggressive
64	1.603	11/10/14	Mon	1520	Struck by Falling/Shifting Cargo	0	0	1	Night	Dry	#N/A
65	1.641	04/01/14	Tue	1017	Sideswipe	0	0	1	Day	Dry	Improper Passing
66	1.641	06/11/14	Wed	1340	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
67	1.641	08/01/14	Fri	1721	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
68	1.641	10/24/14	Fri	1151	Embankment	0	0	1	Day	Wet	Careless or Negligent Manner
69	1.646	03/27/14	Thu	1556	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
70	1.646	03/31/14	Mon	0620	Rear-End	0	0	1	Night	Dry	#N/A
71	1.679	09/24/14	Wed	0045	Other Traffic Barrier	0	1	0	Night	Wet	Failed To Keep In Proper Lane
72	1.698	09/13/14	Sat	0830	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
73	1.716	02/07/14	Fri	0455	Concrete Traffic Barrier	0	0	1	Night	Dry	No Contributing Action
74	1.716	06/01/14	Sun	1145	Other Fixed Object	0	2	0	Day	Dry	Careless or Negligent Manner
75	1.716	08/09/14	Sat	1329	Guardrail Face	0	1	0	Day	Dry	Failed To Keep In Proper Lane
76	1.716	11/07/14	Fri	0715	Rear-End	0	0	1	Day	Dry	Other Contributing Action
77	1.730	01/24/14	Fri	1907	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
78	1.735	05/23/14	Fri	1524	Rear-End	0	2	0	Day	Dry	No Contributing Action
79	1.735	05/24/14	Sat	1540	Angle	0	1	0	Day	Dry	Careless or Negligent Manner
80	1.746	04/19/14	Sat	0640	Rear-End	0	2	0	Day	Wet	Careless or Negligent Manner
81	1.746	04/19/14	Sat	0645	Rear-End	0	2	0	Day	Wet	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.838 To MP 2.046 M.P. 0.838 TO 2.046 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
82	1.746	01/13/14	Mon	2155	Pedestrian	0	0	1	Night	Dry	No Contributing Action
83	1.746	11/27/14	Thu	1010	Rear-End	0	0	1	Day	Dry	#N/A
84	1.746	11/06/14	Thu	0457	Concrete Traffic Barrier	0	0	1	Night	Dry	#N/A
85	1.778	09/28/14	Sun	0645	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
86	1.796	12/04/14	Thu	1910	Sideswipe	0	0	1	Night	Wet	#N/A
87	1.846	01/03/14	Fri	0920	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
88	1.846	02/08/14	Sat	2102	Guardrail Face	0	1	0	Night	Dry	Careless or Negligent Manner
89	1.846	04/01/14	Tue	0930	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
90	1.846	06/18/14	Wed	0920	Rear-End	0	0	1	Day	Dry	No Contributing Action
91	1.846	09/29/14	Mon	0215	Other Non-Fixed Object	0	0	1	Night	Dry	No Contributing Action
92	1.925	08/27/14	Wed	1315	Thrown or Falling Object	0	0	1	Day	Dry	Other Contributing Action

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
92	1	36	56	47	0	4	0	0	13	0	2	1	9	0	3
Percent	1.09%	39.13%	60.87%	51.09%	0.00%	4.35%	0.00%	0.00%	14.13%	0.00%	2.17%	1.09%	9.78%	0.00%	3.26%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	63	29	15	77	27	1	0	0	2	1	0	1	0	3	0
Percent	68.48%	31.52%	16.30%	83.70%	29.35%	1.09%	0.00%	0.00%	2.17%	1.09%	0.00%	1.09%	0.00%	3.26%	0.00%

TOTAL ENTERING VEHICLES/ADT: **118,441** SEGMENT CRASH RATE: **0.996** CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.838 To MP 2.046 M.P. 0.838 TO 2.046 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.850	01/22/15	Thu	1453	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
2	0.850	02/10/15	Tue	1909	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
3	0.850	02/16/15	Mon	1217	Concrete Traffic Barrier	0	0	1	Day	Dry	#N/A
4	0.878	11/20/15	Fri	0509	Concrete Traffic Barrier	0	1	0	Night	Dry	Careless or Negligent Manner
5	0.888	08/25/15	Tue	2245	Concrete Traffic Barrier	0	2	0	Night	Dry	Careless or Negligent Manner
6	0.899	07/24/15	Fri	1825	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
7	0.949	11/25/15	Wed	1149	Rear-End	0	0	1	Day	Wet	Other Contributing Action
8	0.955	01/22/15	Thu	0831	Rear-End	0	0	1	Day	Dry	No Contributing Action
9	0.968	01/05/15	Mon	1450	Rear-End	0	2	0	Day	Dry	Failed To Keep In Proper Lane
10	0.986	03/12/15	Thu	1310	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
11	0.986	03/26/15	Thu	1526	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
12	0.986	05/01/15	Fri	0930	Rear-End	0	0	1	Day	Dry	No Contributing Action
13	1.005	02/15/15	Sun	0119	Sideswipe	0	2	0	Night	Dry	Failed To Yield Right-Of-Way
14	1.005	10/02/15	Fri	1505	Sideswipe	0	3	0	Day	Dry	Failed To Keep In Proper Lane
15	1.005	12/27/15	Sun	1116	Sideswipe	0	0	1	Day	Dry	#N/A
16	1.012	10/21/15	Wed	1735	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
17	1.024	12/03/15	Thu	1055	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
18	1.028	03/13/15	Fri	0020	Angle	0	0	1	Night	Dry	#N/A
19	1.028	03/30/15	Mon	1628	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
20	1.028	11/19/15	Thu	1921	Rear-End	0	3	0	Night	Dry	Careless or Negligent Manner
21	1.036	07/30/15	Thu	0900	Impact Attenuator/Crash Cushion	0	0	1	Day	Dry	Other Contributing Action
22	1.043	01/23/15	Fri	1350	Rear-End	0	0	1	Day	Dry	#N/A
23	1.081	12/15/15	Tue	0758	Rear-End	0	0	1	Night	Dry	#N/A
24	1.100	03/14/15	Sat	1150	Concrete Traffic Barrier	0	0	1	Day	Dry	Careless or Negligent Manner
25	1.110	10/05/15	Mon	0700	Not Coded	0	1	0	Day	Wet	Other Contributing Action
26	1.119	02/14/15	Sat	1355	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
27	1.145	07/08/15	Wed	1230	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
28	1.164	02/11/15	Wed	1925	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
29	1.178	01/15/15	Thu	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
30	1.178	10/21/15	Wed	0649	Rear-End	0	0	1	Day	Dry	Other Contributing Action
31	1.183	01/01/15	Thu	1520	Overturn/Rollover	0	1	0	Day	Dry	Other Contributing Action
32	1.195	02/04/15	Wed	0835	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
33	1.202	12/04/15	Fri	1655	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
34	1.221	07/08/15	Wed	0931	Rear-End	0	0	1	Day	Dry	Other Contributing Action
35	1.221	07/11/15	Sat	0650	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
36	1.240	09/05/15	Sat	1955	Concrete Traffic Barrier	0	1	0	Night	Wet	Failed To Keep In Proper Lane
37	1.259	06/22/15	Mon	1443	Struck by Falling/Shifting Cargo	0	0	1	Day	Dry	#N/A
38	1.268	11/13/15	Fri	0645	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
39	1.269	08/21/15	Fri	0800	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
40	1.298	10/20/15	Tue	1600	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
41	1.306	01/26/15	Mon	0605	Traffic Sign Support	0	0	1	Night	Dry	Careless or Negligent Manner
42	1.316	01/07/15	Wed	0900	Angle	0	2	0	Day	Dry	Careless or Negligent Manner
43	1.378	09/05/15	Sat	1010	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
44	1.378	10/08/15	Thu	0116	Embankment	0	0	1	Night	Wet	Careless or Negligent Manner
45	1.411	01/23/15	Fri	1800	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
46	1.411	02/27/15	Fri	1615	Rear-End	0	1	0	Day	Dry	Failed To Keep In Proper Lane
47	1.448	01/09/15	Fri	1655	Rear-End	0	0	1	Day	Dry	#N/A
48	1.467	07/15/15	Wed	1608	Rear-End	0	0	1	Day	Wet	No Contributing Action
49	1.478	02/19/15	Thu	0710	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
50	1.478	02/26/15	Thu	1120	Rear-End	0	0	1	Day	Dry	No Contributing Action
51	1.528	02/23/15	Mon	0040	Concrete Traffic Barrier	0	2	0	Night	Dry	Careless or Negligent Manner
52	1.565	04/02/15	Thu	1907	Other Non-Collision	0	1	0	Night	Dry	Other Contributing Action
53	1.565	12/03/15	Thu	1142	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
54	1.578	02/18/15	Wed	0945	Sideswipe	0	0	1	Day	Wet	Careless or Negligent Manner
55	1.603	03/21/15	Sat	0850	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
56	1.619	10/08/15	Thu	0116	Concrete Traffic Barrier	0	1	0	Night	Wet	Careless or Negligent Manner
57	1.622	09/04/15	Fri	1820	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
58	1.641	03/18/15	Wed	0907	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
59	1.641	04/04/15	Sat	1445	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
60	1.641	04/09/15	Thu	1553	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
61	1.641	08/28/15	Fri	1731	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
62	1.646	05/13/15	Wed	0116	Other Post, Pole Or Support	0	1	0	Night	Dry	Careless or Negligent Manner
63	1.646	07/24/15	Fri	1130	Guardrail Face	0	0	1	Day	Dry	Careless or Negligent Manner
64	1.646	07/30/15	Thu	1640	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
65	1.646	12/19/15	Sat	2330	Sideswipe	0	0	1	Night	Dry	Other Contributing Action
66	1.646	12/10/15	Thu	0030	Not Coded	0	0	1	Night	Dry	#N/A
67	1.646	12/10/15	Thu	0040	Rear-End	0	0	1	Night	Dry	Other Contributing Action
68	1.679	05/07/15	Thu	1230	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
69	1.688	11/14/15	Sat	0152	Concrete Traffic Barrier	0	0	1	Night	Wet	Over-Correcting/Over-Steering
70	1.698	07/01/15	Wed	1150	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
71	1.716	08/31/15	Mon	0830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
72	1.735	05/16/15	Sat	1946	Sideswipe	0	0	1	Night	Dry	#N/A
73	1.735	08/09/15	Sun	2102	Rear-End	0	4	0	Night	Dry	Careless or Negligent Manner
74	1.746	07/09/15	Thu	1655	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
75	1.746	07/28/15	Tue	1737	Rear-End	0	5	0	Day	Dry	Careless or Negligent Manner
76	1.746	11/03/15	Tue	1625	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
77	1.755	02/16/15	Mon	1700	Not Coded	0	0	1	Day	Dry	Failed To Yield Right-Of-Way
78	1.778	01/08/15	Thu	0850	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
79	1.796	11/25/15	Wed	1545	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
80	1.796	05/24/15	Sun	1939	Sideswipe	0	1	0	Night	Dry	#N/A
81	1.796	05/28/15	Thu	0920	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000													STATE ROUTE: 112						
ROADWAY LIMITS: MP 0.838 To MP 2.046													M.P. 0.838		TO 2.046		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2015													TO 12/ 2015		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE					FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)				
82	1.796	06/30/15	Tue	1635	Rear-End					0	2	0	Day	Dry	Careless or Negligent Manner				
83	1.796	07/30/15	Thu	1922	Rear-End					0	1	0	Night	Dry	Careless or Negligent Manner				
84	1.796	11/09/15	Mon	1910	Rear-End					0	3	0	Night	Dry	Careless or Negligent Manner				
85	1.796	12/21/15	Mon	1519	Rear-End					0	0	1	Day	Dry	Careless or Negligent Manner				
86	1.846	05/03/15	Sun	2349	Rear-End					0	2	0	Night	Dry	Careless or Negligent Manner				
87	1.846	05/12/15	Tue	0130	Guardrail Face					0	1	0	Night	Dry	No Contributing Action				
88	1.846	11/27/15	Fri	2232	Rear-End					0	0	1	Night	Dry	Careless or Negligent Manner				
89	1.846	11/27/15	Fri	2245	Rear-End					0	0	1	Night	Dry	Careless or Negligent Manner				
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
89	0	29	60	54	0	2	0	0	13	0	0	0	9	0	3				
Percent	0.00%	32.58%	67.42%	60.67%	0.00%	2.25%	0.00%	0.00%	14.61%	0.00%	0.00%	0.00%	10.11%	0.00%	3.37%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	61	28	10	79	27	1	0	0	2	1	0	1	0	0	0				
Percent	68.54%	31.46%	11.24%	88.76%	30.34%	1.12%	0.00%	0.00%	2.25%	1.12%	0.00%	1.12%	0.00%	0.00%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 118,441									SEGMENT CRASH RATE: 0.996 CRASHES PER MILLION VEHICLE MILES										

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: **87004000** STATE ROUTE: **112**
ROADWAY LIMITS: **MP 0.486 To MP 0.838** M.P. **0.486** TO **0.838** ENGINEER: **FDOT D6**
STUDY PERIOD: FROM **1/ 2011** TO **12/ 2011** COUNTY: **Miami-Dade**

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)					
1	0.498	09/11/11	Sun	0250	Impact Attenuator/Crash Cushion	0	0	1	Night	Dry	Careless or Negligent Manner					
2	0.517	04/12/11	Tue	2135	Rear-End	0	4	0	Night	Dry	Careless or Negligent Manner					
3	0.517	05/11/11	Wed	1610	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner					
4	0.527	05/11/11	Wed	1547	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner					
5	0.536	05/20/11	Fri	1127	Sideswipe	0	0	1	Day	Dry	No Contributing Action					
6	0.546	03/04/11	Fri	1434	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
7	0.546	07/04/11	Mon	1828	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
8	0.546	07/25/11	Mon	1415	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
9	0.546	07/25/11	Mon	1505	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
10	0.584	05/21/11	Sat	1415	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane					
11	0.584	05/20/11	Fri	1122	Angle	0	0	1	Day	Dry	No Contributing Action					
12	0.589	06/18/11	Sat	0430	Rear-End	0	1	0	Night	Dry	#N/A					
13	0.589	09/16/11	Fri	0841	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
14	0.631	08/24/11	Wed	1830	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner					
15	0.689	06/01/11	Wed	0819	Rear-End	0	0	1	Day	Dry	No Contributing Action					
16	0.728	07/06/11	Wed	1740	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner					
17	0.739	02/23/11	Wed	0845	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner					
18	0.739	04/17/11	Sun	1810	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner					
19	0.739	04/21/11	Thu	1820	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner					
20	0.739	05/19/11	Thu	1600	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner					
21	0.739	06/02/11	Thu	0850	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner					
22	0.739	06/18/11	Sat	2026	Rear-End	0	1	0	Night	Wet	#N/A					
23	0.739	07/28/11	Thu	1737	Rear-End	0	0	1	Day	Dry	#N/A					
24	0.778	09/06/11	Tue	0830	Rear-End	0	1	0	Day	Dry	No Contributing Action					
25	0.778	09/06/11	Tue	0928	Rear-End	0	4	0	Day	Dry	No Contributing Action					
26	0.778	09/24/11	Sat	1240	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
27	0.778	11/22/11	Tue	1225	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
28	0.778	09/19/11	Mon	1000	Rear-End	0	4	0	Day	Dry	Careless or Negligent Manner					
29	0.778	09/20/11	Tue	1250	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner					
30	0.778	09/20/11	Tue	1343	Rear-End	0	4	0	Day	Dry	Careless or Negligent Manner					
31	0.785	01/12/11	Wed	0140	Sideswipe	0	0	1	Night	Dry	No Contributing Action					
32	0.787	02/13/11	Sun	1700	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner					
33	0.787	06/02/11	Thu	0927	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
34	0.787	08/17/11	Wed	0904	Rear-End	0	0	1	Day	Dry	#N/A					
35	0.000	12/01/11	Thu	0450	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner					
36	0.000	11/29/11	Tue	0123	Not Coded	0	0	1	Night	Dry	#N/A					
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
36	0	18	18	30	0	1	0	0	3	0	0	0	1	0	1	
Percent	0.00%	50.00%	50.00%	83.33%	0.00%	2.78%	0.00%	0.00%	8.33%	0.00%	0.00%	0.00%	2.78%	0.00%	2.78%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	29	7	3	33	24	0	0	0	0	0	0	0	0	0	0	
Percent	80.56%	19.44%	8.33%	91.67%	66.67%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT:						110,000	SEGMENT CRASH RATE:						2.547 CRASHES PER MILLION VEHICLE MILES			

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.486 To MP 0.838 M.P. 0.486 TO 0.838 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.493	01/24/12	Tue	1715	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
2	0.508	02/26/12	Sun	0550	Concrete Traffic Barrier	0	0	1	Night	Dry	Careless or Negligent Manner
3	0.508	10/22/12	Mon	0812	Rear-End	0	0	1	Day	Dry	Other Contributing Action
4	0.527	09/23/12	Sun	0510	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
5	0.527	12/11/12	Tue	1914	Rear-End	0	0	1	Night	Dry	No Contributing Action
6	0.539	06/05/12	Tue	0742	Concrete Traffic Barrier	0	1	0	Day	Dry	No Contributing Action
7	0.546	01/21/12	Sat	0225	Parked Motor Vehicle	0	1	0	Night	Dry	Careless or Negligent Manner
8	0.584	09/21/12	Fri	1706	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
9	0.589	01/25/12	Wed	0800	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
10	0.589	08/22/12	Wed	1658	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
11	0.641	05/06/12	Sun	1416	Sideswipe	0	1	0	Day	Dry	Improper Passing
12	0.659	08/17/12	Fri	1558	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
13	0.678	01/18/12	Wed	1245	Sideswipe	0	0	1	Day	Dry	#N/A
14	0.689	01/23/12	Mon	1817	Angle	0	0	1	Day	Dry	No Contributing Action
15	0.728	04/24/12	Tue	2030	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
16	0.735	05/19/12	Sat	0339	Impact Attenuator/Crash Cushion	0	1	0	Night	Dry	Careless or Negligent Manner
17	0.739	02/22/12	Wed	2234	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
18	0.739	08/31/12	Fri	1607	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
19	0.755	12/08/12	Sat	1505	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
20	0.778	02/04/12	Sat	1735	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
21	0.778	11/01/12	Thu	0030	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
22	0.785	03/03/12	Sat	0235	Impact Attenuator/Crash Cushion	0	1	0	Night	Dry	No Contributing Action
23	0.785	05/20/12	Sun	1712	Sideswipe	0	1	0	Day	Dry	Careless or Negligent Manner
24	0.785	12/17/12	Mon	1415	Sideswipe	0	0	1	Day	Dry	Improper Passing
25	0.000	04/12/12	Thu	0419	Guardrail Face	0	1	0	Night	Dry	Careless or Negligent Manner
26	0.000	09/05/12	Wed	1801	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
27	0.000	09/10/12	Mon	1050	Parked Motor Vehicle	0	0	1	Day	Dry	No Contributing Action
28	0.000	11/05/12	Mon	0550	Not Coded	0	1	0	Night	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
28	0	10	18	14	0	1	0	0	4	0	0	2	5	0	2
Percent	0.00%	35.71%	64.29%	50.00%	0.00%	3.57%	0.00%	0.00%	14.29%	0.00%	0.00%	7.14%	17.86%	0.00%	7.14%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	17	11	1	27	19	0	0	0	0	2	0	0	0	2	0
Percent	60.71%	39.29%	3.57%	96.43%	67.86%	0.00%	0.00%	0.00%	0.00%	7.14%	0.00%	0.00%	0.00%	7.14%	0.00%

TOTAL ENTERING VEHICLES/ADT: **100,500** SEGMENT CRASH RATE: **2.168** CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.486 To MP 0.838 M.P. 0.486 TO 0.838 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)				
1	0.490	03/08/13	Fri	1853	Rear-End	0	0	1	Night	Dry	No Contributing Action				
2	0.490	07/28/13	Sun	0130	Concrete Traffic Barrier	0	0	1	Night	Wet	Careless or Negligent Manner				
3	0.505	08/20/13	Tue	1753	Sideswipe	0	0	1	Day	Dry	Improper Passing				
4	0.508	01/07/13	Mon	0827	Rear-End	0	0	1	Day	Dry	No Contributing Action				
5	0.521	10/14/13	Mon	1030	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner				
6	0.523	11/21/13	Thu	1913	Rear-End	0	0	1	Night	Wet	#N/A				
7	0.527	04/26/13	Fri	0130	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane				
8	0.546	04/29/13	Mon	1700	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner				
9	0.546	04/01/13	Mon	1454	Rear-End	0	1	0	Day	Dry	No Contributing Action				
10	0.546	06/17/13	Mon	2040	Concrete Traffic Barrier	0	0	1	Night	Dry	No Contributing Action				
11	0.562	10/25/13	Fri	1330	Sideswipe	0	0	1	Day	Dry	Improper Passing				
12	0.584	01/28/13	Mon	2150	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner				
13	0.586	09/12/13	Thu	0805	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner				
14	0.589	06/11/13	Tue	2352	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner				
15	0.603	01/07/13	Mon	1021	Concrete Traffic Barrier	0	1	0	Day	Wet	Other Contributing Action				
16	0.637	10/06/13	Sun	1300	Guardrail End	0	5	0	Day	Dry	No Contributing Action				
17	0.656	07/11/13	Thu	1725	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
18	0.736	07/10/13	Wed	1450	Sideswipe	0	1	0	Day	Dry	Careless or Negligent Manner				
19	0.736	09/26/13	Thu	1930	Concrete Traffic Barrier	0	0	1	Night	Dry	Careless or Negligent Manner				
20	0.755	05/10/13	Fri	0435	Sideswipe	0	0	1	Night	Wet	Disregarded other Road Markings				
21	0.759	06/03/13	Mon	0420	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner				
22	0.789	05/01/13	Wed	1624	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner				
23	0.838	12/06/13	Fri	2235	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner				
24	0.000	03/13/13	Wed	0540	Guardrail Face	0	0	1	Night	Dry	#N/A				
25	0.000	03/19/13	Tue	0945	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner				
26	0.000	03/05/13	Tue	1720	Rear-End	0	0	1	Day	Dry	No Contributing Action				
27	0.000	02/15/13	Fri	1345	Sideswipe	0	0	1	Day	Wet	Careless or Negligent Manner				
28	0.000	05/13/13	Mon	1630	Guardrail Face	0	0	1	Day	Dry	Improper Passing				
29	0.000	10/16/13	Wed	0843	Sideswipe	0	0	1	Day	Dry	Other Contributing Action				
30	0.000	12/13/13	Fri	2220	Concrete Traffic Barrier	0	1	0	Night	Dry	Other Contributing Action				
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
30	0	9	21	14	0	0	0	0	7	0	0	0	8	0	1
Percent	0.00%	30.00%	70.00%	46.67%	0.00%	0.00%	0.00%	0.00%	23.33%	0.00%	0.00%	0.00%	26.67%	0.00%	3.33%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	17	13	9	21	14	0	0	0	0	3	1	0	0	1	0
Percent	56.67%	43.33%	30.00%	70.00%	46.67%	0.00%	0.00%	0.00%	0.00%	10.00%	3.33%	0.00%	0.00%	3.33%	0.00%
TOTAL ENTERING VEHICLES/ADT: 106,746						SEGMENT CRASH RATE: 2.187 CRASHES PER MILLION VEHICLE MILES									

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 0.486 To MP 0.838 M.P. 0.486 TO 0.838 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)				
1	0.562	09/23/14	Tue	1022	Rear-End	0	2	0	Day	Wet	Careless or Negligent Manner				
2	0.580	06/09/14	Mon	2220	Sideswipe	0	0	1	Night	Dry	#N/A				
3	0.581	07/23/14	Wed	1743	Rear-End	0	0	1	Day	Dry	#N/A				
4	0.581	12/13/14	Sat	0410	Angle	0	0	1	Night	Dry	Failed to Yield Right-Of-Way				
5	0.581	12/05/14	Fri	1755	Rear-End	0	4	0	Night	Dry	Careless or Negligent Manner				
6	0.585	09/22/14	Mon	0943	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner				
7	0.586	08/29/14	Fri	0830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
8	0.600	10/21/14	Tue	1425	Rear-End	0	2	0	Day	Wet	Failed To Keep In Proper Lane				
9	0.619	08/20/14	Wed	1330	Angle	0	0	1	Day	Dry	Other Contributing Action				
10	0.627	10/17/14	Fri	1038	Not Coded	0	1	0	Day	Dry	Failed To Keep In Proper Lane				
11	0.637	03/16/14	Sun	2245	Rear-End	0	0	1	Night	Dry	#N/A				
12	0.638	03/08/14	Sat	2130	Rear-End	0	0	1	Night	Dry	Other Contributing Action				
13	0.638	11/20/14	Thu	2355	Rear-End	0	0	1	Night	Wet	Other Contributing Action				
14	0.656	12/18/14	Thu	1715	Rear-End	0	3	0	Night	Dry	Failed To Keep In Proper Lane				
15	0.675	10/31/14	Fri	0638	Other Fixed Object	0	1	0	Day	Dry	No Contributing Action				
16	0.735	03/05/14	Wed	0835	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
17	0.736	11/13/14	Thu	1835	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner				
18	0.736	12/04/14	Thu	0018	Not Coded	0	0	1	Night	Dry	#N/A				
19	0.755	07/31/14	Thu	0856	Sideswipe	0	0	1	Day	Dry	Improper Passing				
20	0.755	07/18/14	Fri	1715	Not Coded	0	0	1	Day	Dry	#N/A				
21	0.759	01/31/14	Fri	1520	Sideswipe	0	0	1	Day	Dry	Other Contributing Action				
22	0.778	06/30/14	Mon	2155	Angle	0	0	1	Night	Wet	#N/A				
23	0.778	09/28/14	Sun	1030	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner				
24	0.778	10/25/14	Sat	2220	Concrete Traffic Barrier	0	0	1	Night	Dry	No Contributing Action				
25	0.787	01/26/14	Sun	2010	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner				
26	0.790	01/13/14	Mon	0836	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
27	0.000	05/06/14	Tue	1440	Pedestrian	0	0	1	Day	Dry	Careless or Negligent Manner				
28	0.000	09/30/14	Tue	1144	Sideswipe	0	0	1	Day	Dry	No Contributing Action				
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
28	0	7	21	15	0	3	0	0	4	0	1	0	8	0	1
Percent	0.00%	25.00%	75.00%	53.57%	0.00%	10.71%	0.00%	0.00%	14.29%	0.00%	3.57%	0.00%	28.57%	0.00%	3.57%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	16	12	5	23	14	0	0	0	0	3	1	0	0	0	0
Percent	57.14%	42.86%	17.86%	82.14%	50.00%	0.00%	0.00%	0.00%	0.00%	10.71%	3.57%	0.00%	0.00%	0.00%	0.00%
TOTAL ENTERING VEHICLES/ADT:						106,746									
SEGMENT CRASH RATE:						2.187 CRASHES PER MILLION VEHICLE MILES									

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 0.486 To MP 0.838 M.P. 0.486 TO 0.838 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.505	05/28/15	Thu	1330	Sideswipe	0	0	1	Day	Dry	#N/A
2	0.505	06/02/15	Tue	0945	Rear-End	0	0	1	Day	Dry	Other Contributing Action
3	0.505	04/15/15	Wed	1448	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
4	0.505	12/02/15	Wed	1510	Not Coded	0	0	1	Day	Dry	#N/A
5	0.514	06/15/15	Mon	0845	Concrete Traffic Barrier	0	0	1	Day	Wet	Careless or Negligent Manner
6	0.524	10/11/15	Sun	0530	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
7	0.524	11/12/15	Thu	1035	Rear-End	0	1	0	Day	Dry	Other Contributing Action
8	0.524	12/06/15	Sun	1600	Sideswipe	0	0	1	Day	Dry	No Contributing Action
9	0.524	12/17/15	Thu	0800	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
10	0.543	05/18/15	Mon	1354	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
11	0.543	12/08/15	Tue	1747	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
12	0.546	03/08/15	Sun	0615	Guardrail Face	0	0	1	Night	Dry	Careless or Negligent Manner
13	0.562	11/12/15	Thu	1955	Sideswipe	0	0	1	Night	Dry	#N/A
14	0.562	11/16/15	Mon	1915	Not Coded	0	0	1	Day	Dry	#N/A
15	0.580	07/23/15	Thu	0900	Not Coded	0	0	1	Day	Dry	No Contributing Action
16	0.581	12/17/15	Thu	0800	Sideswipe	0	2	0	Day	Dry	Failed to Yield Right-Of-Way
17	0.585	01/17/15	Sat	1435	Sideswipe	0	1	0	Day	Dry	#N/A
18	0.585	02/17/15	Tue	1045	Rear-End	0	0	1	Day	Dry	Improper Passing
19	0.585	03/18/15	Wed	1828	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
20	0.655	01/16/15	Fri	0930	Sideswipe	0	0	1	Day	Dry	No Contributing Action
21	0.655	07/22/15	Wed	1655	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
22	0.660	01/14/15	Wed	1815	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
23	0.660	10/16/15	Fri	1525	Sideswipe	0	0	1	Day	Dry	#N/A
24	0.675	04/10/15	Fri	1705	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
25	0.675	10/02/15	Fri	1015	Other Non-Fixed Object	0	0	1	Day	Dry	No Contributing Action
26	0.675	10/02/15	Fri	1058	Other Non-Fixed Object	0	0	1	Day	Dry	No Contributing Action
27	0.717	02/02/15	Mon	0830	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
28	0.736	11/02/15	Mon	1905	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
29	0.736	08/20/15	Thu	1648	Rear-End	0	0	1	Day	Dry	No Contributing Action
30	0.736	08/20/15	Thu	1846	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
31	0.736	11/24/15	Tue	1758	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
32	0.736	12/13/15	Sun	1820	Rear-End	0	4	0	Night	Dry	Careless or Negligent Manner
33	0.755	01/14/15	Wed	1320	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
34	0.756	01/01/15	Thu	1838	Pedestrian	0	0	1	Night	Dry	Improper Passing
35	0.770	05/29/15	Fri	1300	Rear-End	0	1	0	Day	Dry	#N/A
36	0.774	02/20/15	Fri	1655	Not Coded	0	0	1	Day	Dry	#N/A
37	0.778	06/18/15	Thu	0835	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
38	0.793	09/04/15	Fri	1635	Angle	0	0	1	Day	Dry	Improper Passing
39	0.813	05/18/15	Mon	1346	Sideswipe	0	0	1	Day	Dry	Improper Passing
40	0.000	03/20/15	Fri	1910	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
41	0.000	05/31/15	Sun	1007	Sideswipe	0	0	1	Day	Wet	No Contributing Action
42	0.000	09/29/15	Tue	0630	Not Coded	0	0	1	Day	Dry	No Contributing Action
43	0.000	10/19/15	Mon	0533	Ditch	0	0	1	Night	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
43	0	12	31	18	0	1	0	0	13	0	1	0	8	0	1
Percent	0.00%	27.91%	72.09%	41.86%	0.00%	2.33%	0.00%	0.00%	30.23%	0.00%	2.33%	0.00%	18.60%	0.00%	2.33%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	33	10	2	41	14	0	0	0	0	3	1	0	0	1	0
Percent	76.74%	23.26%	4.65%	95.35%	32.56%	0.00%	0.00%	0.00%	0.00%	6.98%	2.33%	0.00%	0.00%	2.33%	0.00%

TOTAL ENTERING VEHICLES/ADT: **106,746** SEGMENT CRASH RATE: **2.187** CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: **87004000** STATE ROUTE: **112**
ROADWAY LIMITS: **MP 2.046 To MP 4.351** M.P. **2.046** TO **4.351** ENGINEER: **FDOT D6**
STUDY PERIOD: FROM **1/ 2011** TO **12/ 2011** COUNTY: **Miami-Dade**

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)				
1	2.046	05/23/11	Mon	1705	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner				
2	2.046	05/23/11	Mon	1708	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner				
3	2.083	02/11/11	Fri	2230	Other Non-Fixed Object	0	2	0	Night	Dry	Careless or Negligent Manner				
4	2.083	03/24/11	Thu	1429	Not Coded	0	1	0	Day	Dry	Careless or Negligent Manner				
5	2.083	01/08/11	Sat	0801	Sideswipe	0	2	0	Day	Dry	Careless or Negligent Manner				
6	2.083	03/02/11	Wed	1940	Guardrail End	0	1	0	Night	Dry	Other Contributing Action				
7	2.083	03/12/11	Sat	1950	Angle	0	0	1	Night	Dry	Careless or Negligent Manner				
8	2.083	07/17/11	Sun	0300	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner				
9	2.083	07/17/11	Sun	0304	Angle	0	0	1	Night	Dry	Other Contributing Action				
10	2.183	05/01/11	Sun	0559	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner				
11	2.239	03/27/11	Sun	0530	Guardrail Face	0	0	1	Day	Dry	#N/A				
12	2.239	03/29/11	Tue	0000	Sideswipe	0	0	1	Day	Dry	#N/A				
13	2.383	02/11/11	Fri	2228	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner				
14	2.546	06/07/11	Tue	0911	Other Non-Fixed Object	0	2	0	Day	Dry	Careless or Negligent Manner				
15	2.583	01/09/11	Sun	0115	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner				
16	2.583	12/21/11	Wed	1540	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
17	2.583	05/31/11	Tue	0425	Rear-End	0	0	1	Night	Wet	#N/A				
18	2.583	04/27/11	Wed	1407	Angle	0	0	1	Day	Dry	Careless or Negligent Manner				
19	2.583	05/11/11	Wed	1850	Not Coded	0	0	1	Day	Dry	Failed To Keep In Proper Lane				
20	2.583	07/08/11	Fri	0300	Guardrail Face	0	0	1	Night	Wet	Careless or Negligent Manner				
21	2.583	08/02/11	Tue	1350	Overturn/Rollover	0	1	0	Day	Wet	Careless or Negligent Manner				
22	2.833	06/22/11	Wed	1345	Sideswipe	0	1	0	Day	Dry	#N/A				
23	3.230	04/24/11	Sun	1900	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner				
24	3.583	02/22/11	Tue	0842	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner				
25	3.730	03/11/11	Fri	1515	Rear-End	0	0	1	Day	Dry	#N/A				
26	3.730	08/14/11	Sun	0130	Rear-End	0	5	0	Night	Dry	Careless or Negligent Manner				
27	3.730	09/10/11	Sat	1635	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner				
28	3.730	11/18/11	Fri	1002	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner				
29	3.730	10/09/11	Sun	0222	Other Fixed Object	0	1	0	Night	Wet	Careless or Negligent Manner				
30	3.730	06/18/11	Sat	0515	Rear-End	0	2	0	Night	Dry	No Contributing Action				
31	3.980	07/02/11	Sat	0150	Guardrail Face	0	1	0	Night	Dry	Ran Off Roadway				
32	4.087	07/24/11	Sun	2234	Guardrail Face	0	0	1	Night	Dry	Swerved Or Avoided				
33	4.230	03/02/11	Wed	0120	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner				
34	4.230	01/10/11	Mon	1045	Rear-End	0	0	1	Day	Dry	Other Contributing Action				
35	4.230	04/20/11	Wed	1433	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
36	4.230	06/07/11	Tue	1110	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
37	4.230	07/05/11	Tue	1355	Sideswipe	0	0	1	Day	Dry	No Contributing Action				
38	4.230	06/16/11	Thu	0839	Angle	0	2	0	Day	Dry	Careless or Negligent Manner				
39	4.230	07/30/11	Sat	0642	Angle	0	1	0	Day	Dry	#N/A				
40	4.230	08/07/11	Sun	1320	Concrete Traffic Barrier	0	1	0	Day	Wet	Careless or Negligent Manner				
41	4.230	09/06/11	Tue	2110	Pedalcycle	0	1	0	Night	Dry	Other Contributing Action				
42	4.230	12/29/11	Thu	1506	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner				
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
42	0	23	19	19	0	5	0	0	5	0	1	0	7	0	2
Percent	0.00%	54.76%	45.24%	45.24%	0.00%	11.90%	0.00%	0.00%	11.90%	0.00%	2.38%	0.00%	16.67%	0.00%	4.76%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	24	18	7	35	27	0	0	0	0	0	0	0	1	2	0
Percent	57.14%	42.86%	16.67%	83.33%	64.29%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	2.38%	4.76%	0.00%
TOTAL ENTERING VEHICLES/ADT: 103,091						SEGMENT CRASH RATE: 0.484 CRASHES PER MILLION VEHICLE MILES									

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 2.046 To MP 4.351 M.P. 2.046 TO 4.351 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	2.046	12/21/12	Fri	0950	Sideswipe	0	1	0	Day	Dry	Other Contributing Action
2	2.083	03/07/12	Wed	1600	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
3	2.083	06/30/12	Sat	0950	Angle	0	2	0	Day	Dry	Other Contributing Action
4	2.083	08/13/12	Mon	1535	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
5	2.151	04/27/12	Fri	1803	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
6	2.278	02/18/12	Sat	1620	Rear-End	0	0	1	Day	Dry	Followed too Closely
7	2.278	07/17/12	Tue	1505	Sideswipe	0	1	0	Day	Wet	#N/A
8	2.283	09/22/12	Sat	1427	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
9	2.333	07/29/12	Sun	1933	Angle	0	0	1	Night	Dry	#N/A
10	2.546	12/22/12	Sat	1813	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
11	2.583	02/11/12	Sat	1950	Not Coded	0	0	1	Night	Dry	#N/A
12	2.583	04/13/12	Fri	2350	Curb	0	0	1	Night	Wet	Careless or Negligent Manner
13	2.583	05/24/12	Thu	1730	Sideswipe	0	1	0	Day	Dry	#N/A
14	2.583	04/30/12	Mon	1730	Rear-End	0	0	1	Day	Wet	Other Contributing Action
15	2.583	07/13/12	Fri	0830	Angle	0	0	1	Day	Wet	No Contributing Action
16	2.583	07/30/12	Mon	0640	Guardrail Face	0	2	0	Night	Wet	Swerved Or Avoided
17	2.583	09/23/12	Sun	0925	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
18	2.583	09/23/12	Sun	1029	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
19	2.583	08/18/12	Sat	1825	Not Coded	0	0	1	Day	Wet	Drove too Fast for Conditions
20	2.583	10/16/12	Tue	1725	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
21	2.583	09/23/12	Sun	1712	Angle	0	0	1	Day	Wet	Improper Passing
22	2.730	03/23/12	Fri	1537	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
23	3.083	06/10/12	Sun	2125	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
24	3.083	07/19/12	Thu	1828	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
25	3.083	02/24/12	Fri	2029	Not Coded	0	3	0	Night	Dry	Careless or Negligent Manner
26	3.230	03/15/12	Thu	1540	Rear-End	0	0	1	Day	Wet	Followed too Closely
27	3.230	04/21/12	Sat	0339	Pedestrian	1	0	0	Night	Wet	#N/A
28	3.230	05/17/12	Thu	2300	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
29	3.546	12/27/12	Thu	1920	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
30	3.730	05/23/12	Wed	1015	Guardrail Face	0	1	0	Day	Wet	Careless or Negligent Manner
31	3.730	01/31/12	Tue	2347	Guardrail Face	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
32	3.730	02/04/12	Sat	1728	Not Coded	0	1	0	Night	Dry	No Contributing Action
33	3.730	08/08/12	Wed	1628	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
34	3.730	05/26/12	Sat	0025	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
35	3.730	03/23/12	Fri	0830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
36	3.730	04/21/12	Sat	0409	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
37	3.730	07/14/12	Sat	0242	Guardrail Face	0	0	1	Night	Wet	Careless or Negligent Manner
38	3.730	05/27/12	Sun	0050	Angle	0	0	1	Night	Dry	Improper Passing
39	3.730	11/29/12	Thu	0014	Guardrail Face	0	0	1	Night	Wet	#N/A
40	3.787	02/13/12	Mon	0144	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
41	4.130	12/28/12	Fri	0000	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
42	4.230	03/27/12	Tue	1440	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
43	4.230	04/06/12	Fri	0202	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
44	4.230	06/25/12	Mon	1445	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
45	4.230	10/07/12	Sun	1120	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
46	4.230	12/29/12	Sat	0000	Rear-End	0	3	0	Night	Dry	Careless or Negligent Manner
47	4.230	12/05/12	Wed	1748	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
48	4.330	01/13/12	Fri	1549	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
48	1	18	29	29	0	5	0	0	3	0	1	0	6	0	4
Percent	2.08%	37.50%	60.42%	60.42%	0.00%	10.42%	0.00%	0.00%	6.25%	0.00%	2.08%	0.00%	12.50%	0.00%	8.33%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	26	22	15	33	30	1	0	0	1	2	0	0	0	2	0
Percent	54.17%	45.83%	31.25%	68.75%	62.50%	2.08%	0.00%	0.00%	2.08%	4.17%	0.00%	0.00%	0.00%	4.17%	0.00%

TOTAL ENTERING VEHICLES/ADT: **102,506** SEGMENT CRASH RATE: **0.557** CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 2.046 To MP 4.351 M.P. 2.046 TO 4.351 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	2.046	01/02/13	Wed	0208	Concrete Traffic Barrier	0	1	0	Night	Dry	Failed To Keep In Proper Lane
2	2.046	03/19/13	Tue	0400	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
3	2.046	03/11/13	Mon	1800	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
4	2.046	05/18/13	Sat	0130	Guardrail Face	0	0	1	Night	Wet	#N/A
5	2.046	05/03/13	Fri	2045	Pedestrian	1	0	0	Night	Dry	#N/A
6	2.046	05/11/13	Sat	1335	Sideswipe	0	0	1	Day	Dry	#N/A
7	2.046	08/02/13	Fri	0610	Guardrail Face	0	0	1	Night	Dry	Careless or Negligent Manner
8	2.046	05/20/13	Mon	1635	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
9	2.046	07/13/13	Sat	0410	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
10	2.046	08/08/13	Thu	1720	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
11	2.046	12/14/13	Sat	0422	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
12	2.296	05/30/13	Thu	1440	Concrete Traffic Barrier	0	0	1	Day	Wet	#N/A
13	2.486	07/13/13	Sat	0330	Not Coded	0	2	0	Night	Dry	No Contributing Action
14	2.546	08/29/13	Thu	2130	Rear-End	0	0	1	Night	Dry	No Contributing Action
15	2.546	01/07/13	Mon	0840	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
16	2.546	01/22/13	Tue	1405	Guardrail Face	0	0	1	Day	Dry	Careless or Negligent Manner
17	2.546	04/23/13	Tue	1110	Cargo/Equipment Loss or Shift	0	0	1	Day	Dry	No Contributing Action
18	2.546	07/13/13	Sat	0400	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
19	2.546	06/12/13	Wed	1350	Not Coded	0	0	1	Day	Wet	Careless or Negligent Manner
20	2.546	06/12/13	Wed	1500	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
21	2.546	06/12/13	Wed	1500	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
22	2.546	06/30/13	Sun	1451	Sideswipe	0	3	0	Day	Dry	No Contributing Action
23	2.546	08/16/13	Fri	0620	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
24	2.546	11/15/13	Fri	1924	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
25	2.546	10/19/13	Sat	0735	Guardrail Face	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
26	2.546	11/03/13	Sun	0125	Angle	0	0	1	Night	Dry	Careless or Negligent Manner
27	2.546	10/24/13	Thu	2110	Overturn/Rollover	0	1	0	Night	Dry	Careless or Negligent Manner
28	3.230	10/23/13	Wed	1407	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
29	3.730	02/07/13	Thu	0851	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
30	3.730	01/07/13	Mon	1738	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
31	3.730	02/16/13	Sat	1434	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
32	3.730	02/19/13	Tue	0828	Rear-End	0	2	0	Day	Dry	No Contributing Action
33	3.730	04/07/13	Sun	1720	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
34	3.730	04/06/13	Sat	1915	Guardrail Face	0	3	0	Night	Dry	Other Contributing Action
35	3.730	04/07/13	Sun	1710	Sideswipe	0	4	0	Day	Dry	Careless or Negligent Manner
36	3.730	05/28/13	Tue	0615	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
37	3.730	05/28/13	Tue	0615	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
38	3.730	05/28/13	Tue	0816	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
39	3.730	07/11/13	Thu	1420	Guardrail Face	0	1	0	Day	Dry	No Contributing Action
40	3.730	08/09/13	Fri	1452	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
41	3.730	09/12/13	Thu	0633	Sideswipe	0	0	1	Night	Wet	Other Contributing Action
42	3.730	12/04/13	Wed	1730	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
43	3.730	12/26/13	Thu	2231	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
44	4.177	03/22/13	Fri	1508	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
45	4.230	02/15/13	Fri	1205	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
46	4.230	02/07/13	Thu	1745	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
47	4.230	02/22/13	Fri	1555	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
48	4.230	01/26/13	Sat	1800	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
49	4.230	05/08/13	Wed	0958	Pedestrian	0	2	0	Day	Dry	No Contributing Action
50	4.230	03/07/13	Thu	1814	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
51	4.230	04/22/13	Mon	2340	Concrete Traffic Barrier	0	0	1	Night	Dry	#N/A
52	4.230	04/27/13	Sat	0600	Rear-End	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
53	4.230	10/28/13	Mon	0800	Guardrail Face	0	2	0	Day	Dry	Careless or Negligent Manner
54	4.230	10/19/13	Sat	1607	Sideswipe	0	0	1	Day	Dry	#N/A
55	4.230	09/22/13	Sun	1215	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
56	4.230	12/04/13	Wed	0933	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
57	4.230	09/29/13	Sun	0938	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
58	4.230	12/16/13	Mon	0120	Angle	0	2	0	Night	Dry	Careless or Negligent Manner
59	4.287	02/15/13	Fri	1813	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
60	4.287	02/26/13	Tue	1600	Other Non-Fixed Object	0	0	1	Day	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
60	1	22	37	33	0	3	0	0	7	0	2	0	10	0	2
Percent	1.67%	36.67%	61.67%	55.00%	0.00%	5.00%	0.00%	0.00%	11.67%	0.00%	3.33%	0.00%	16.67%	0.00%	3.33%
Contrib.					Careless	Improper	Ran Red	Exceeded	Improper	Disreg Cntl	Erratic/	Ran off			Wrong
Cause	Day	Night	Wet	Dry	Driving	FTYRW	Turn	Light	Speed	Passing	Dev	Aggress	Road	DUI	Way
Total	37	23	16	44	40	2	0	0	0	0	0	0	0	5	0
Percent	61.67%	38.33%	26.67%	73.33%	66.67%	3.33%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	8.33%	0.00%

TOTAL ENTERING VEHICLES/ADT: **105,721** SEGMENT CRASH RATE: **0.675** CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 2.046 To MP 4.351 M.P. 2.046 TO 4.351 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	2.046	04/26/14	Sat	1713	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
2	2.046	07/04/14	Fri	1533	Angle	0	0	1	Day	Dry	Swerved Or Avoided
3	2.046	11/01/14	Sat	0458	Rear-End	0	1	0	Night	Dry	No Contributing Action
4	2.246	12/02/14	Tue	2034	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
5	2.278	11/21/14	Fri	0210	Rear-End	0	0	1	Night	Wet	Other Contributing Action
6	2.546	02/05/14	Wed	0823	Other Fixed Object	0	2	0	Day	Dry	Careless or Negligent Manner
7	2.546	02/14/14	Fri	1159	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
8	2.546	01/22/14	Wed	1635	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
9	2.546	07/02/14	Wed	0300	Rear-End	0	3	0	Night	Dry	Careless or Negligent Manner
10	2.546	04/09/14	Wed	0000	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
11	2.546	08/28/14	Thu	1700	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
12	2.546	12/16/14	Tue	1900	Rear-End	0	1	0	Night	Dry	Other Contributing Action
13	2.621	02/18/14	Tue	1800	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
14	2.730	04/18/14	Fri	1434	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
15	2.730	08/04/14	Mon	2001	Guardrail Face	0	1	0	Night	Wet	Failed To Keep In Proper Lane
16	2.730	12/02/14	Tue	2149	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
17	2.787	04/09/14	Wed	2053	Rear-End	0	1	0	Night	Dry	#N/A
18	3.046	12/01/14	Mon	1805	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
19	3.546	04/24/14	Thu	1950	Not Coded	0	0	1	Night	Dry	Failed To Keep In Proper Lane
20	3.546	08/16/14	Sat	0110	Other Non-Fixed Object	0	0	1	Night	Wet	Careless or Negligent Manner
21	3.730	02/02/14	Sun	1240	Guardrail Face	0	0	1	Day	Dry	Careless or Negligent Manner
22	3.730	01/03/14	Fri	1500	Rear-End	0	0	1	Day	Dry	Other Contributing Action
23	3.730	01/05/14	Sun	1543	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
24	3.730	06/06/14	Fri	0716	Rear-End	0	0	1	Day	Dry	#N/A
25	3.730	06/19/14	Thu	1946	Overturn/Rollover	0	1	0	Night	Wet	Careless or Negligent Manner
26	3.730	07/25/14	Fri	0957	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
27	3.730	08/23/14	Sat	0728	Sideswipe	0	1	0	Day	Dry	Other Contributing Action
28	3.730	11/21/14	Fri	0241	Head-On	0	1	0	Night	Wet	Careless or Negligent Manner
29	3.730	12/02/14	Tue	0000	Guardrail End	0	0	1	Night	Dry	Other Contributing Action
30	3.730	12/04/14	Thu	1758	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
31	3.730	01/05/14	Sun	1456	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
32	3.730	09/29/14	Mon	1743	Struck by Falling/Shifting Cargo	0	0	1	Day	Dry	No Contributing Action
33	3.780	02/22/14	Sat	1640	Other Non-Collision	1	6	0	Day	Dry	Other Contributing Action
34	3.923	01/24/14	Fri	1053	Rear-End	0	0	1	Day	Dry	No Contributing Action
35	4.230	04/15/14	Tue	0830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
36	4.230	02/13/14	Thu	1440	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
37	4.230	01/05/14	Sun	2200	Guardrail Face	0	1	0	Night	Wet	Drove too Fast for Conditions
38	4.230	02/15/14	Sat	0525	Sideswipe	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
39	4.230	03/03/14	Mon	1515	Sideswipe	0	0	1	Day	Dry	#N/A
40	4.230	05/26/14	Mon	2330	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
41	4.230	05/09/14	Fri	0017	Overturn/Rollover	0	1	0	Night	Dry	No Contributing Action
42	4.230	07/08/14	Tue	0355	Guardrail Face	0	0	1	Night	Wet	#N/A
43	4.230	09/19/14	Fri	1625	Overturn/Rollover	0	0	1	Day	Wet	Careless or Negligent Manner
44	4.230	10/21/14	Tue	0500	Concrete Traffic Barrier	0	0	1	Night	Wet	#N/A
45	4.230	10/12/14	Sun	0000	Concrete Traffic Barrier	0	0	1	Night	Dry	Other Contributing Action
46	4.230	11/14/14	Fri	1525	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
47	4.287	02/14/14	Fri	1452	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
48	4.330	02/16/14	Sun	2255	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
48	1	18	30	26	1	1	0	0	5	0	0	0	10	0	2
Percent	2.08%	37.50%	62.50%	54.17%	2.08%	2.08%	0.00%	0.00%	10.42%	0.00%	0.00%	0.00%	20.83%	0.00%	4.17%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	23	25	14	34	40	2	0	0	0	0	0	0	0	1	0
Percent	47.92%	52.08%	29.17%	70.83%	83.33%	4.17%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	2.08%	0.00%

TOTAL ENTERING VEHICLES/ADT: **105,721** SEGMENT CRASH RATE: **0.675** CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 2.046 To MP 4.351 M.P. 2.046 TO 4.351 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	2.046	04/20/15	Mon	2240	Concrete Traffic Barrier	1	2	0	Night	Dry	Over-Correcting/Over-Steering
2	2.046	06/09/15	Tue	1545	Guardrail Face	0	0	1	Day	Wet	Careless or Negligent Manner
3	2.046	06/09/15	Tue	1720	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
4	2.046	06/03/15	Wed	1700	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
5	2.046	11/22/15	Sun	0405	Sideswipe	0	1	0	Night	Wet	No Contributing Action
6	2.046	12/10/15	Thu	0935	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
7	2.278	06/09/15	Tue	1715	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
8	2.278	09/07/15	Mon	2152	Guardrail Face	0	0	1	Night	Dry	Careless or Negligent Manner
9	2.296	06/24/15	Wed	2340	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
10	2.296	04/21/15	Tue	1818	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
11	2.346	03/30/15	Mon	1541	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
12	2.446	06/11/15	Thu	0028	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
13	2.546	03/27/15	Fri	1530	Rear-End	0	0	1	Day	Dry	No Contributing Action
14	2.546	03/16/15	Mon	1630	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
15	2.546	05/13/15	Wed	1645	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
16	2.546	05/13/15	Wed	1250	Sideswipe	0	0	1	Day	Dry	No Contributing Action
17	2.546	04/18/15	Sat	1521	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
18	2.546	03/27/15	Fri	2022	Guardrail Face	0	0	1	Night	Wet	Careless or Negligent Manner
19	2.546	06/17/15	Wed	2059	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
20	2.546	06/30/15	Tue	0830	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
21	2.546	11/30/15	Mon	0945	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
22	2.546	05/29/15	Fri	0846	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
23	2.546	08/08/15	Sat	2050	Rear-End	0	0	1	Night	Dry	#N/A
24	2.546	12/30/15	Wed	1953	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
25	2.546	08/21/15	Fri	1715	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
26	2.546	08/12/15	Wed	0929	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
27	2.546	11/30/15	Mon	1310	Sideswipe	0	1	0	Day	Dry	Other Contributing Action
28	2.546	12/31/15	Thu	0237	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
29	2.730	07/29/15	Wed	1740	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
30	3.046	11/18/15	Wed	1019	Not Coded	0	0	1	Day	Dry	Failed To Keep In Proper Lane
31	3.230	08/16/15	Sun	0730	Guardrail Face	0	0	1	Day	Wet	Careless or Negligent Manner
32	3.278	01/12/15	Mon	1635	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
33	3.278	01/12/15	Mon	1630	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
34	3.278	01/12/15	Mon	1815	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
35	3.530	12/29/15	Tue	1528	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
36	3.546	03/26/15	Thu	1730	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
37	3.546	07/31/15	Fri	1808	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
38	3.730	01/15/15	Thu	0949	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
39	3.730	02/02/15	Mon	2045	Not Coded	0	1	0	Night	Dry	#N/A
40	3.730	02/11/15	Wed	2137	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
41	3.730	03/11/15	Wed	2230	Overturn/Rollover	0	4	0	Night	Dry	Careless or Negligent Manner
42	3.730	10/23/15	Fri	2027	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
43	3.730	05/15/15	Fri	1648	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
44	3.730	04/03/15	Fri	1600	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
45	3.730	05/01/15	Fri	0825	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
46	3.730	07/08/15	Wed	2033	Pedestrian	1	0	0	Night	Wet	No Contributing Action
47	3.730	05/22/15	Fri	2330	Rear-End	0	0	1	Night	Dry	#N/A
48	3.730	06/30/15	Tue	0505	Rear-End	0	2	0	Night	Dry	#N/A
49	3.730	06/19/15	Fri	1615	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
50	3.730	08/21/15	Fri	0116	Not Coded	0	0	1	Night	Dry	#N/A
51	3.730	11/10/15	Tue	1841	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
52	3.730	11/11/15	Wed	1120	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
53	3.730	12/11/15	Fri	1159	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
54	3.730	12/04/15	Fri	1455	Parked Motor Vehicle	0	4	0	Day	Wet	Careless or Negligent Manner
55	3.730	12/29/15	Tue	1735	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
56	3.730	12/30/15	Wed	1912	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
57	3.730	12/30/15	Wed	1917	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
58	3.730	11/11/15	Wed	1950	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
59	3.730	06/15/15	Mon	1544	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
60	3.980	05/04/15	Mon	0140	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
61	3.980	12/11/15	Fri	1126	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
62	4.043	10/18/15	Sun	2300	Guardrail Face	0	0	1	Night	Wet	Careless or Negligent Manner
63	4.130	10/09/15	Fri	1636	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
64	4.230	02/28/15	Sat	0006	Rear-End	0	0	1	Night	Wet	#N/A
65	4.230	01/07/15	Wed	1835	Rear-End	0	0	1	Night	Dry	Other Contributing Action
66	4.230	01/21/15	Wed	2040	Sideswipe	0	0	1	Night	Dry	No Contributing Action
67	4.230	03/14/15	Sat	1415	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
68	4.230	02/06/15	Fri	1516	Sideswipe	0	0	1	Day	Dry	Improper Passing
69	4.230	03/18/15	Wed	0930	Rear-End	0	0	1	Day	Dry	Other Contributing Action
70	4.230	04/08/15	Wed	0829	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
71	4.230	05/16/15	Sat	1425	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
72	4.230	04/27/15	Mon	0930	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
73	4.230	03/15/15	Sun	1506	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
74	4.230	04/16/15	Thu	1755	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
75	4.230	03/29/15	Sun	2256	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
76	4.230	05/24/15	Sun	0230	Tree (Standing)	0	1	0	Night	Dry	Careless or Negligent Manner
77	4.230	06/16/15	Tue	1745	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
78	4.230	08/11/15	Tue	1020	Rear-End	0	0	1	Day	Dry	#N/A
79	4.230	07/25/15	Sat	1445	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
80	4.230	08/07/15	Fri	1435	Rear-End	0	0	1	Day	Dry	Other Contributing Action
81	4.230	08/08/15	Sat	2000	Rear-End	0	0	1	Night	Dry	Other Contributing Action

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 2.046 To MP 4.351 M.P. 2.046 TO 4.351 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
82	4.230	07/29/15	Wed	1615	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
83	4.230	08/07/15	Fri	1543	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
84	4.230	09/29/15	Tue	1520	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
85	4.230	07/31/15	Fri	1958	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
86	4.230	08/12/15	Wed	0740	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
87	4.230	12/09/15	Wed	0815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
88	4.230	12/04/15	Fri	1025	Rear-End	0	2	0	Day	Wet	No Contributing Action
89	4.230	12/02/15	Wed	0718	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
90	4.230	12/14/15	Mon	0827	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
91	4.330	12/03/15	Thu	2220	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
92	4.351	06/09/15	Tue	0023	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
92	2	31	60	70	0	2	0	0	6	0	1	1	10	0	2
Percent	2.17%	33.70%	65.22%	76.09%	0.00%	2.17%	0.00%	0.00%	6.52%	0.00%	1.09%	1.09%	10.87%	0.00%	2.17%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	56	36	21	71	40	2	0	0	0	0	0	0	0	2	0
Percent	60.87%	39.13%	22.83%	77.17%	43.48%	2.17%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	2.17%	0.00%

TOTAL ENTERING VEHICLES/ADT: **105,721** SEGMENT CRASH RATE: **0.675** CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 4.351 To MP 4.91 M.P. 4.351 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2011 TO 12/ 2011 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)				
1	4.430	01/31/11	Mon	1925	Angle	0	2	0	Night	Dry	Careless or Negligent Manner				
2	4.430	02/26/11	Sat	1525	Pedestrian	0	1	0	Day	Dry	Careless or Negligent Manner				
3	4.430	06/27/11	Mon	0440	Parked Motor Vehicle	0	0	1	Night	Dry	#N/A				
4	4.480	01/24/11	Mon	1045	Rear-End	0	0	1	Day	Dry	Followed too Closely				
5	4.480	03/03/11	Thu	1505	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner				
6	4.480	03/20/11	Sun	1400	Rear-End	0	5	0	Day	Dry	Careless or Negligent Manner				
7	4.480	03/13/11	Sun	1443	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner				
8	4.480	02/03/11	Thu	1655	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner				
9	4.480	03/13/11	Sun	1520	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner				
10	4.480	04/10/11	Sun	1650	Struck by Falling/Shifting Cargo	0	1	0	Day	Dry	No Contributing Action				
11	4.480	02/25/11	Fri	1545	Sideswipe	0	0	1	Day	Dry	#N/A				
12	4.480	04/29/11	Fri	1853	Rear-End	0	2	0	Day	Wet	Followed too Closely				
13	4.480	08/21/11	Sun	1427	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner				
14	4.480	08/14/11	Sun	1605	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner				
15	4.480	09/23/11	Fri	1556	Rear-End	0	0	1	Day	Dry	#N/A				
16	4.530	03/12/11	Sat	1145	Overturn/Rollover	0	1	0	Day	Dry	No Contributing Action				
17	4.530	05/23/11	Mon	0713	Sideswipe	0	0	1	Day	Dry	#N/A				
18	4.530	06/09/11	Thu	0914	Rear-End	0	1	0	Day	Dry	No Contributing Action				
19	4.530	09/22/11	Thu	2028	Angle	0	1	0	Night	Dry	No Contributing Action				
20	4.541	10/05/11	Wed	0805	Concrete Traffic Barrier	0	1	0	Day	Dry	Careless or Negligent Manner				
21	4.541	07/15/11	Fri	1550	Angle	0	1	0	Day	Dry	Careless or Negligent Manner				
22	4.541	08/22/11	Mon	0852	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner				
23	4.560	03/27/11	Sun	0535	Rear-End	0	1	0	Night	Dry	No Contributing Action				
24	4.635	06/16/11	Thu	0832	Rear-End	0	0	1	Day	Dry	No Contributing Action				
25	4.635	06/03/11	Fri	0655	Rear-End	0	2	0	Day	Dry	#N/A				
26	4.635	06/16/11	Thu	0850	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner				
27	4.635	09/09/11	Fri	1730	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner				
28	4.635	01/29/11	Sat	1444	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner				
29	4.673	02/26/11	Sat	1437	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
30	4.673	03/13/11	Sun	1620	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner				
31	4.711	03/06/11	Sun	0910	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner				
32	4.721	01/14/11	Fri	0825	Not Coded	0	0	1	Day	Dry	No Contributing Action				
33	4.724	11/11/11	Fri	0617	Not Coded	0	0	1	Day	Dry	No Contributing Action				
34	4.726	06/03/11	Fri	1150	Rear-End	0	0	1	Day	Dry	Failed to Yield Right-Of-Way				
35	4.904	02/04/11	Fri	0757	Rear-End	0	2	0	Day	Dry	No Contributing Action				
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
35	0	24	11	23	0	3	0	0	2	0	1	1	1	0	2
Percent	0.00%	68.57%	31.43%	65.71%	0.00%	8.57%	0.00%	0.00%	5.71%	0.00%	2.86%	2.86%	2.86%	0.00%	5.71%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	30	5	3	32	18	1	0	0	0	0	0	0	0	0	0
Percent	85.71%	14.29%	8.57%	91.43%	51.43%	2.86%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
TOTAL ENTERING VEHICLES/ADT: 101,165						SEGMENT CRASH RATE: 1.696 CRASHES PER MILLION VEHICLE MILES									

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: **87004000** STATE ROUTE: **112**
 ROADWAY LIMITS: **MP 4.351 To MP 4.91** M.P. **4.351** TO **4.910** ENGINEER: **FDOT D6**
 STUDY PERIOD: FROM **1/ 2012** TO **12/ 2012** COUNTY: **Miami-Dade**

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)					
1	4.404	08/13/12	Mon	1715	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner					
2	4.410	03/01/12	Thu	0125	Rear-End	0	2	0	Night	Wet	Careless or Negligent Manner					
3	4.423	03/11/12	Sun	1445	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
4	4.430	02/25/12	Sat	0610	Pedestrian	1	0	0	Night	Dry	No Contributing Action					
5	4.430	03/17/12	Sat	1607	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
6	4.480	03/12/12	Mon	1735	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner					
7	4.530	11/13/12	Tue	1225	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner					
8	4.530	08/17/12	Fri	0910	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner					
9	4.530	12/15/12	Sat	2322	Sideswipe	0	2	0	Night	Dry	Failed to Yield Right-Of-Way					
10	4.560	12/07/12	Fri	1430	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
11	4.578	03/01/12	Thu	1655	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
12	4.578	04/01/12	Sun	1510	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
13	4.630	03/30/12	Fri	1135	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner					
14	4.630	08/08/12	Wed	1649	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
15	4.630	11/07/12	Wed	1549	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
16	4.630	12/03/12	Mon	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
17	4.630	10/24/12	Wed	0900	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner					
18	4.630	11/13/12	Tue	1947	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner					
19	4.635	07/24/12	Tue	0050	Rear-End	0	0	1	Night	Dry	Other Contributing Action					
20	4.660	02/24/12	Fri	1320	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
21	4.673	02/21/12	Tue	1007	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner					
22	4.673	06/25/12	Mon	1212	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner					
23	4.673	12/06/12	Thu	0030	Other Fixed Object	0	0	1	Night	Wet	Careless or Negligent Manner					
24	4.692	03/14/12	Wed	1332	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner					
25	4.692	03/12/12	Mon	1704	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner					
26	4.692	10/02/12	Tue	0828	Rear-End	0	0	1	Day	Wet	No Contributing Action					
27	4.711	09/13/12	Thu	2028	Rear-End	0	1	0	Night	Wet	No Contributing Action					
28	4.711	12/28/12	Fri	2115	Rear-End	0	0	1	Night	Dry	No Contributing Action					
29	4.721	09/16/12	Sun	2112	Angle	0	0	1	Night	Dry	Careless or Negligent Manner					
30	4.721	06/05/12	Tue	1405	Sideswipe	0	0	1	Day	Dry	No Contributing Action					
31	4.724	01/21/12	Sat	2300	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner					
32	4.872	09/05/12	Wed	0854	Bridge Overhead Structure	0	0	1	Day	Dry	Other Contributing Action					
33	4.910	02/19/12	Sun	2340	Rear-End	1	1	0	Night	Dry	Careless or Negligent Manner					
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
33	2	12	20	25	0	1	0	0	4	0	1	0	2	0	0	
Percent	6.06%	36.36%	60.61%	75.76%	0.00%	3.03%	0.00%	0.00%	12.12%	0.00%	3.03%	0.00%	6.06%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	22	11	5	28	25	1	0	0	0	0	0	0	0	0	0	
Percent	66.67%	33.33%	15.15%	84.85%	75.76%	3.03%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT:						100,664	SEGMENT CRASH RATE:									1.607 CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: **87004000** STATE ROUTE: **112**
 ROADWAY LIMITS: **MP 4.351 To MP 4.91** M.P. **4.351** TO **4.910** ENGINEER: **FDOT D6**
 STUDY PERIOD: FROM **1/ 2013** TO **12/ 2013** COUNTY: **Miami-Dade**

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	4.423	04/30/13	Tue	0930	Rear-End	0	0	1	Day	Dry	#N/A
2	4.423	12/30/13	Mon	1444	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
3	4.430	05/06/13	Mon	2350	Rear-End	0	4	0	Night	Dry	Other Contributing Action
4	4.430	05/25/13	Sat	2310	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
5	4.430	10/04/13	Fri	1030	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
6	4.446	05/07/13	Tue	1435	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
7	4.446	02/16/13	Sat	1530	Overturn/Rollover	0	0	1	Day	Dry	Careless or Negligent Manner
8	4.480	02/02/13	Sat	1527	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
9	4.480	02/03/13	Sun	1520	Overturn/Rollover	0	1	0	Day	Dry	No Contributing Action
10	4.480	04/06/13	Sat	1500	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
11	4.480	05/11/13	Sat	1730	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
12	4.480	09/23/13	Mon	0820	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
13	4.480	10/30/13	Wed	1242	Sideswipe	0	1	0	Day	Dry	Swerved Or Avoided
14	4.480	10/05/13	Sat	1617	Guardrail Face	0	0	1	Day	Dry	Failed To Keep In Proper Lane
15	4.480	10/13/13	Sun	0143	Angle	0	0	1	Night	Dry	No Contributing Action
16	4.480	10/13/13	Sun	0203	Angle	0	0	1	Night	Dry	#N/A
17	4.480	03/04/13	Mon	1938	Sideswipe	0	0	1	Night	Dry	Improper Passing
18	4.510	07/19/13	Fri	0430	Rear-End	0	0	1	Night	Wet	#N/A
19	4.530	02/06/13	Wed	2015	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
20	4.530	09/02/13	Mon	0740	Concrete Traffic Barrier	0	1	0	Day	Dry	Careless or Negligent Manner
21	4.530	11/04/13	Mon	0915	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
22	4.537	03/21/13	Thu	0910	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
23	4.541	11/17/13	Sun	1555	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
24	4.541	11/17/13	Sun	1508	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
25	4.541	11/23/13	Sat	0342	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
26	4.541	11/22/13	Fri	0846	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
27	4.541	11/22/13	Fri	0949	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
28	4.541	11/17/13	Sun	1555	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
29	4.560	03/22/13	Fri	2005	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
30	4.560	05/07/13	Tue	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
31	4.560	12/27/13	Fri	2350	Rear-End	0	0	1	Night	Wet	#N/A
32	4.578	03/19/13	Tue	0830	Rear-End	0	0	1	Day	Wet	No Contributing Action
33	4.578	03/20/13	Wed	0930	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
34	4.578	04/20/13	Sat	1710	Rear-End	0	0	1	Day	Dry	#N/A
35	4.578	08/22/13	Thu	1900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
36	4.630	03/01/13	Fri	2308	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
37	4.630	03/26/13	Tue	1449	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
38	4.630	03/16/13	Sat	1743	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
39	4.630	08/29/13	Thu	1055	Other Non-Collision	0	0	1	Day	Dry	Other Contributing Action
40	4.630	08/29/13	Thu	1130	Rear-End	0	0	1	Day	Dry	#N/A
41	4.630	09/18/13	Wed	0745	Rear-End	0	1	0	Day	Dry	No Contributing Action
42	4.635	02/16/13	Sat	1618	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
43	4.635	08/01/13	Thu	1000	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
44	4.635	04/23/13	Tue	0915	Other Non-Fixed Object	0	1	0	Day	Dry	Careless or Negligent Manner
45	4.635	06/22/13	Sat	1745	Sideswipe	0	0	1	Day	Dry	#N/A
46	4.635	12/28/13	Sat	1815	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
47	4.635	07/19/13	Fri	1015	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
48	4.654	01/05/13	Sat	1925	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
49	4.654	04/14/13	Sun	1440	Rear-End	0	6	0	Day	Dry	Careless or Negligent Manner
50	4.654	06/20/13	Thu	1553	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
51	4.654	11/10/13	Sun	1310	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
52	4.673	12/23/13	Mon	1217	Guardrail End	0	1	0	Day	Dry	Careless or Negligent Manner
53	4.673	12/31/13	Tue	1150	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
54	4.673	12/10/13	Tue	0943	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
55	4.683	12/02/13	Mon	1720	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
56	4.692	05/06/13	Mon	1540	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
57	4.692	06/24/13	Mon	1537	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
58	4.692	08/17/13	Sat	2255	Sideswipe	0	2	0	Night	Wet	Careless or Negligent Manner
59	4.692	10/08/13	Tue	0830	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
60	4.692	10/25/13	Fri	1850	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
61	4.692	01/13/13	Sun	1454	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
62	4.702	11/08/13	Fri	0905	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
63	4.702	08/01/13	Thu	0805	Not Coded	0	0	1	Day	Dry	#N/A
64	4.702	06/11/13	Tue	1420	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
65	4.711	02/11/13	Mon	1202	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
66	4.711	02/15/13	Fri	1504	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
67	4.711	03/17/13	Sun	1900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
68	4.711	09/05/13	Thu	1321	Sideswipe	0	0	1	Day	Dry	Improper Passing
69	4.718	07/17/13	Wed	1435	Rear-End	0	2	0	Day	Wet	Careless or Negligent Manner
70	4.721	04/26/13	Fri	0550	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
71	4.722	05/27/13	Mon	0020	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
72	4.724	07/06/13	Sat	0055	Not Coded	0	0	1	Night	Dry	Other Contributing Action
73	4.728	04/04/13	Thu	1026	Rear-End	0	0	1	Day	Dry	No Contributing Action
74	4.728	06/13/13	Thu	0842	Rear-End	0	0	1	Day	Dry	No Contributing Action
75	4.787	07/24/13	Wed	1912	Angle	0	3	0	Day	Dry	No Contributing Action
76	4.791	07/16/13	Tue	1235	Rear-End	0	0	1	Day	Dry	No Contributing Action
77	4.815	03/19/13	Tue	0950	Rear-End	0	1	0	Night	Dry	No Contributing Action
78	4.860	03/28/13	Thu	0520	Concrete Traffic Barrier	0	0	1	Night	Dry	Careless or Negligent Manner
79	4.873	12/02/13	Mon	0510	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
80	4.891	03/15/13	Fri	0852	Sideswipe	0	0	1	Day	Dry	No Contributing Action
81	4.905	05/01/13	Wed	1005	Rear-End	0	0	1	Day	Dry	No Contributing Action

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000												STATE ROUTE: 112							
ROADWAY LIMITS: MP 4.351 To MP 4.91												M.P. 4.351		TO 4.910		ENGINEER: FDOT D6			
STUDY PERIOD: FROM 1/ 2013												TO 12/ 2013		COUNTY: Miami-Dade					
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)					
82	4.906	12/15/13	Sun	2240	Not Coded				0	0	1	Night	Wet	#N/A					
83	4.906	09/26/13	Thu	0556	Rear-End				0	0	1	Night	Dry	Careless or Negligent Manner					
84	4.909	02/20/13	Wed	1442	Parked Motor Vehicle				0	0	1	Day	Dry	No Contributing Action					
85	4.910	08/03/13	Sat	0502	Guardrail Face				0	0	1	Night	Dry	No Contributing Action					
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
85	0	29	56	61	0	3	0	0	8	0	0	1	5	0	3				
Percent	0.00%	34.12%	65.88%	71.76%	0.00%	3.53%	0.00%	0.00%	9.41%	0.00%	0.00%	1.18%	5.88%	0.00%	3.53%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	60	25	12	73	54	0	0	0	0	2	0	0	0	1	0				
Percent	70.59%	29.41%	14.12%	85.88%	63.53%	0.00%	0.00%	0.00%	0.00%	2.35%	0.00%	0.00%	0.00%	1.18%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 104,779									SEGMENT CRASH RATE: 3.976 CRASHES PER MILLION VEHICLE MILES										

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 4.351 To MP 4.91 M.P. 4.351 TO 4.910 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	4.423	05/23/14	Fri	1900	Not Coded	0	0	1	Night	Wet	Followed too Closely
2	4.430	02/03/14	Mon	1834	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
3	4.430	03/01/14	Sat	0700	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
4	4.430	05/21/14	Wed	0841	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
5	4.430	05/19/14	Mon	1440	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
6	4.446	08/18/14	Mon	0810	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
7	4.446	08/22/14	Fri	0951	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
8	4.446	01/28/14	Tue	1710	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
9	4.480	06/11/14	Wed	2320	Concrete Traffic Barrier	0	1	0	Night	Dry	Careless or Negligent Manner
10	4.530	04/30/14	Wed	0520	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
11	4.530	01/14/14	Tue	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
12	4.530	02/14/14	Fri	2032	Ran into Water/Canal	0	1	0	Night	Dry	Careless or Negligent Manner
13	4.530	01/31/14	Fri	1403	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
14	4.530	10/29/14	Wed	0910	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
15	4.530	12/29/14	Mon	1035	Guardrail End	0	1	0	Day	Dry	No Contributing Action
16	4.530	12/12/14	Fri	0030	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
17	4.537	02/24/14	Mon	1454	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
18	4.541	08/05/14	Tue	1515	Guardrail Face	0	1	0	Day	Dry	Careless or Negligent Manner
19	4.541	12/16/14	Tue	1722	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
20	4.541	03/23/14	Sun	0015	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
21	4.541	11/05/14	Wed	1633	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
22	4.546	02/24/14	Mon	1627	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
23	4.546	04/11/14	Fri	1737	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
24	4.546	11/11/14	Tue	1303	Not Coded	0	1	0	Day	Dry	Careless or Negligent Manner
25	4.550	06/18/14	Wed	0807	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
26	4.560	06/23/14	Mon	0100	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
27	4.560	08/28/14	Thu	2015	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
28	4.560	10/31/14	Fri	2250	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
29	4.560	12/09/14	Tue	0840	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
30	4.578	02/13/14	Thu	1140	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
31	4.578	12/31/14	Wed	1755	Rear-End	0	6	0	Night	Wet	Careless or Negligent Manner
32	4.597	06/06/14	Fri	0745	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
33	4.597	04/16/14	Wed	0912	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
34	4.597	04/23/14	Wed	1915	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
35	4.616	12/27/14	Sat	1350	Rear-End	0	6	0	Day	Dry	Careless or Negligent Manner
36	4.616	12/28/14	Sun	1400	Rear-End	0	4	0	Day	Dry	Careless or Negligent Manner
37	4.630	02/14/14	Fri	0946	Rear-End	0	0	1	Day	Dry	#N/A
38	4.630	03/27/14	Thu	0725	Sideswipe	#N/A	0	1	Day	Dry	#N/A
39	4.630	06/20/14	Fri	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
40	4.630	07/05/14	Sat	1145	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
41	4.630	07/09/14	Wed	2151	Not Coded	0	0	1	Night	Wet	#N/A
42	4.635	10/23/14	Thu	0845	Rear-End	0	0	1	Day	Wet	No Contributing Action
43	4.635	05/16/14	Fri	0813	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
44	4.635	02/16/14	Sun	1330	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
45	4.635	08/27/14	Wed	1617	Sideswipe	0	0	1	Day	Dry	Improper Passing
46	4.635	11/10/14	Mon	0855	Rear-End	0	2	0	Day	Wet	Careless or Negligent Manner
47	4.635	11/10/14	Mon	1015	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
48	4.635	12/16/14	Tue	1819	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
49	4.654	07/05/14	Sat	1200	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
50	4.654	12/28/14	Sun	1455	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
51	4.665	07/23/14	Wed	1522	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
52	4.673	01/10/14	Fri	1440	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
53	4.673	01/11/14	Sat	1105	Concrete Traffic Barrier	0	1	0	Day	Dry	Careless or Negligent Manner
54	4.673	04/13/14	Sun	1326	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
55	4.673	04/27/14	Sun	1503	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
56	4.673	10/21/14	Tue	1606	Rear-End	0	1	0	Day	Wet	Followed too Closely
57	4.683	11/16/14	Sun	1514	Rear-End	0	7	0	Day	Dry	Careless or Negligent Manner
58	4.692	03/07/14	Fri	1445	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
59	4.692	06/09/14	Mon	0520	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
60	4.702	07/19/14	Sat	1922	Guardrail Face	0	1	0	Day	Dry	Careless or Negligent Manner
61	4.711	03/21/14	Fri	1942	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
62	4.711	03/21/14	Fri	1909	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
63	4.711	08/18/14	Mon	0917	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
64	4.711	11/15/14	Sat	1340	Guardrail Face	0	1	0	Day	Dry	Careless or Negligent Manner
65	4.711	12/26/14	Fri	1410	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
66	4.721	12/05/14	Fri	0826	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
67	4.722	01/22/14	Wed	0942	Angle	0	0	1	Day	Dry	No Contributing Action
68	4.727	02/16/14	Sun	1433	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
69	4.728	02/27/14	Thu	0710	Rear-End	0	2	0	Day	Wet	Careless or Negligent Manner
70	4.730	09/17/14	Wed	1718	Rear-End	0	0	1	Day	Dry	No Contributing Action
71	4.844	03/05/14	Wed	0845	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
72	4.882	02/07/14	Fri	1030	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
73	4.882	06/10/14	Tue	0845	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
74	4.882	09/22/14	Mon	1042	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
75	4.887	04/13/14	Sun	1220	Rear-End	0	4	0	Day	Dry	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:	87004000										STATE ROUTE:		112				
ROADWAY LIMITS:	MP 4.351 To MP 4.91										M.P.:	4.351	TO	4.910	ENGINEER: FDOT D6		
STUDY PERIOD:	FROM	1/ 2014										TO	12/ 2014	COUNTY: Miami-Dade			
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)			
76	4.901	04/02/14	Wed	0030	Rear-End				0	0	1	Day	Dry	No Contributing Action			
77	4.910	02/06/14	Thu	1544	Left-Turn				0	2	0	Day	Dry	Failed to Yield Right-Of-Way			
78	4.910	06/28/14	Sat	0615	Angle				0	2	0	Day	Dry	Ran Red Light			
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other		
78	0	29	49	54	0	3	1	0	9	0	0	0	5	1	3		
Percent	0.00%	37.18%	62.82%	69.23%	0.00%	3.85%	1.28%	0.00%	11.54%	0.00%	0.00%	0.00%	6.41%	1.28%	3.85%		
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way		
Total	62	16	15	63	54	0	0	0	0	2	0	0	0	1	0		
Percent	79.49%	20.51%	19.23%	80.77%	69.23%	0.00%	0.00%	0.00%	0.00%	2.56%	0.00%	0.00%	0.00%	1.28%	0.00%		
TOTAL ENTERING VEHICLES/ADT: 104,779								SEGMENT CRASH RATE: 3.976 CRASHES PER MILLION VEHICLE MILES									

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 ROADWAY LIMITS: MP 4.351 To MP 4.91 M.P. 4.351 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	4.430	02/13/15	Fri	0715	Rear-End	0	0	1	Day	Dry	Other Contributing Action
2	4.430	08/10/15	Mon	0951	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
3	4.480	04/12/15	Sun	1309	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
4	4.480	04/12/15	Sun	1402	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
5	4.480	05/04/15	Mon	0047	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
6	4.480	05/15/15	Fri	1022	Rear-End	0	0	1	Day	Dry	#N/A
7	4.480	03/13/15	Fri	1445	Rear-End	0	0	1	Day	Dry	#N/A
8	4.480	05/16/15	Sat	1553	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
9	4.480	06/22/15	Mon	1444	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
10	4.480	07/11/15	Sat	1640	Rear-End	0	8	0	Day	Dry	Careless or Negligent Manner
11	4.480	10/10/15	Sat	1829	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
12	4.480	08/08/15	Sat	2126	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
13	4.480	08/22/15	Sat	1545	Rear-End	0	0	1	Day	Dry	No Contributing Action
14	4.480	10/16/15	Fri	1935	Rear-End	0	2	0	Night	Wet	Drove too Fast for Conditions
15	4.480	10/23/15	Fri	1918	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner
16	4.480	10/23/15	Fri	1934	Rear-End	0	4	0	Night	Dry	Careless or Negligent Manner
17	4.480	10/25/15	Sun	1640	Other Non-Collision	0	1	0	Day	Dry	Drove too Fast for Conditions
18	4.480	11/30/15	Mon	1613	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
19	4.480	12/14/15	Mon	1735	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
20	4.480	12/31/15	Thu	2231	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
21	4.480	12/28/15	Mon	1818	Rear-End	0	0	1	Night	Dry	#N/A
22	4.480	12/10/15	Thu	1817	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
23	4.503	07/22/15	Wed	1545	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
24	4.530	01/26/15	Mon	0910	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
25	4.530	01/31/15	Sat	1310	Rear-End	0	5	0	Day	Dry	Careless or Negligent Manner
26	4.530	02/18/15	Wed	0915	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
27	4.530	03/12/15	Thu	1540	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
28	4.530	06/22/15	Mon	0700	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
29	4.530	05/04/15	Mon	0120	Rear-End	0	0	1	Night	Dry	#N/A
30	4.530	05/04/15	Mon	0125	Rear-End	0	0	1	Night	Dry	#N/A
31	4.530	07/03/15	Fri	1300	Rear-End	0	0	1	Day	Dry	No Contributing Action
32	4.530	08/07/15	Fri	1605	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
33	4.530	12/13/15	Sun	2000	Angle	0	1	0	Night	Dry	Careless or Negligent Manner
34	4.541	04/03/15	Fri	1440	Rear-End	0	0	1	Day	Dry	#N/A
35	4.541	05/28/15	Thu	0755	Sideswipe	0	1	0	Day	Dry	No Contributing Action
36	4.541	10/04/15	Sun	2040	Sideswipe	0	1	0	Night	Dry	Careless or Negligent Manner
37	4.541	10/09/15	Fri	1230	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
38	4.541	12/13/15	Sun	2145	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
39	4.560	03/28/15	Sat	1420	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
40	4.560	02/01/15	Sun	0330	Not Coded	0	0	1	Night	Dry	#N/A
41	4.560	05/15/15	Fri	1545	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
42	4.578	06/02/15	Tue	1915	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
43	4.578	07/16/15	Thu	1400	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
44	4.578	12/05/15	Sat	1749	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
45	4.597	01/01/15	Thu	1355	Rear-End	0	4	0	Day	Wet	Careless or Negligent Manner
46	4.597	01/03/15	Sat	1335	Rear-End	0	7	0	Day	Dry	Careless or Negligent Manner
47	4.597	11/12/15	Thu	1000	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
48	4.616	03/13/15	Fri	1350	Sideswipe	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
49	4.616	09/02/15	Wed	0900	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
50	4.616	12/07/15	Mon	1802	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
51	4.616	12/15/15	Tue	1530	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
52	4.630	03/01/15	Sun	0555	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
53	4.630	02/27/15	Fri	1425	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
54	4.630	04/07/15	Tue	1510	Rear-End	0	7	0	Day	Dry	Careless or Negligent Manner
55	4.630	12/29/15	Tue	1245	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
56	4.630	10/28/15	Wed	1000	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
57	4.630	10/29/15	Thu	1000	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
58	4.630	11/20/15	Fri	1115	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
59	4.630	11/21/15	Sat	1030	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
60	4.635	10/07/15	Wed	1910	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
61	4.635	05/13/15	Wed	1723	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
62	4.635	05/30/15	Sat	1135	Sideswipe	0	0	1	Day	Dry	#N/A
63	4.635	08/02/15	Sun	1515	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
64	4.635	06/22/15	Mon	1602	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
65	4.635	06/29/15	Mon	1546	Rear-End	0	0	1	Day	Dry	Other Contributing Action
66	4.635	10/23/15	Fri	1956	Angle	0	2	0	Night	Dry	Careless or Negligent Manner
67	4.635	10/02/15	Fri	1547	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
68	4.635	10/16/15	Fri	1511	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
69	4.635	06/09/15	Tue	1615	Rear-End	0	0	1	Day	Wet	No Contributing Action
70	4.654	11/01/15	Sun	0008	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
71	4.664	11/30/15	Mon	1827	Rear-End	0	1	0	Night	Wet	Careless or Negligent Manner
72	4.673	03/20/15	Fri	1100	Rear-End	0	3	0	Day	Dry	No Contributing Action
73	4.673	06/27/15	Sat	1140	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
74	4.692	07/02/15	Thu	0855	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
75	4.692	10/23/15	Fri	1648	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
76	4.702	04/12/15	Sun	1307	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
77	4.702	06/13/15	Sat	1824	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
78	4.711	01/23/15	Fri	1528	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
79	4.711	02/28/15	Sat	1539	Rear-End	0	0	1	Day	Wet	#N/A
80	4.711	11/04/15	Wed	1552	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
81	4.711	12/11/15	Fri	2036	Rear-End	0	4	0	Night	Dry	Careless or Negligent Manner

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
ROADWAY LIMITS: MP 4.351 To MP 4.91 M.P. 4.351 TO 4.910 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
82	4.711	05/07/15	Thu	1442	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
83	4.719	07/03/15	Fri	1243	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
84	4.721	07/11/15	Sat	2216	Fell/Jumped from Motor Vehicle	0	1	0	Night	Dry	No Contributing Action
85	4.721	09/14/15	Mon	0035	Rear-End	0	1	0	Night	Dry	Exceed Posted Speed
86	4.725	04/29/15	Wed	2026	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
87	4.726	02/10/15	Tue	1305	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
88	4.726	03/06/15	Fri	1321	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
89	4.727	04/15/15	Wed	1554	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
90	4.730	01/07/15	Wed	1119	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
91	4.730	02/10/15	Tue	1718	Sideswipe	0	0	1	Day	Dry	No Contributing Action
92	4.730	02/21/15	Sat	1555	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
93	4.768	07/10/15	Fri	1018	Rear-End	0	0	1	Day	Dry	Followed too Closely
94	4.810	10/06/15	Tue	0815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
95	4.844	10/14/15	Wed	1010	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
96	4.872	05/27/15	Wed	0914	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
97	4.901	12/08/15	Tue	1035	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
98	4.908	09/24/15	Thu	0711	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
99	0.177	09/23/15	Wed	1401	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
100	0.181	03/07/15	Sat	1755	Rear-End	0	0	1	Day	Wet	Followed too Closely
101	0.189	02/10/15	Tue	1157	Angle	0	0	1	Day	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
101	0	34	67	85	0	4	1	0	8	0	0	0	5	0	3
Percent	0.00%	33.66%	66.34%	84.16%	0.00%	3.96%	0.99%	0.00%	7.92%	0.00%	0.00%	0.00%	4.95%	0.00%	2.97%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	74	27	12	89	54	0	0	0	0	2	0	0	0	2	0
Percent	73.27%	26.73%	11.88%	88.12%	53.47%	0.00%	0.00%	0.00%	0.00%	1.98%	0.00%	0.00%	0.00%	1.98%	0.00%

TOTAL ENTERING VEHICLES/ADT: **104,779**

SEGMENT CRASH RATE: **3.976 CRASHES PER MILLION VEHICLE MILES**

RAMP CRASH DATA (2011-2015)

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:	87004019										STATE ROUTE:		#N/A						
ROADWAY LIMITS:	MP 0 TO MP 0.104					M.P. 0.000		TO 0.104		ENGINEER: FDOT D6									
STUDY PERIOD:	FROM		1/ 2011			TO		12/ 2011			COUNTY: Miami-Dade								
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)					
1	0.000	01/28/11	Fri	1450	Concrete Traffic Barrier				0	0	1	Day	Dry	No Contributing Action					
2	0.000	03/25/11	Fri	0215	Other Non-Collision				0	0	1	Night	Dry	No Contributing Action					
3	0.000	06/13/11	Mon	1409	Parked Motor Vehicle				0	1	0	Day	Dry	#N/A					
4	0.104	04/20/11	Wed	0031	Rear-End				0	1	0	Day	Dry	Careless or Negligent Manner					
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
4	0	2	2	1	0	0	0	0	0	0	0	1	1	0	0				
Percent	0.00%	50.00%	50.00%	25.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	25.00%	25.00%	0.00%	0.00%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	3	1	0	4	1	0	0	0	0	0	0	0	0	0	0				
Percent	75.00%	25.00%	0.00%	100.00%	25.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%				
TOTAL ENTERING VEHICLES/ADT:								12,500		SEGMENT CRASH RATE:								8.430 CRASHES PER MILLION VEHICLE MILES	

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: **87004019** STATE ROUTE: **#N/A**
 ROADWAY LIMITS: **MP 0 TO MP 0.104** M.P. **0.000** TO **0.104** ENGINEER: **FDOT D6**
 STUDY PERIOD: FROM **1/ 2012** TO **12/ 2012** COUNTY: **Miami-Dade**

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.000	10/14/12	Sun	1140	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
2	0.000	10/12/12	Fri	1024	Angle	0	0	1	Day	Dry	No Contributing Action
3	0.000	06/08/12	Fri	1300	Rear-End	0	0	1	Day	Dry	Other Contributing Action
4	0.000	12/07/12	Fri	1135	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
5	0.000	05/11/12	Fri	1438	Rear-End	0	0	1	Day	Dry	Other Contributing Action
6	0.009	09/08/12	Sat	2115	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
7	0.009	09/14/12	Fri	1143	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
8	0.066	06/28/12	Thu	1100	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
9	0.104	04/27/12	Fri	1325	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
10	0.104	08/25/12	Sat	0300	Angle	0	0	1	Night	Dry	No Contributing Action
11	0.104	12/12/12	Wed	1521	Rear-End	0	0	1	Day	Dry	Followed too Closely

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
11	0	1	10	8	0	2	0	0	0	0	0	0	0	0	1
Percent	0.00%	9.09%	90.91%	72.73%	0.00%	18.18%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	9.09%

Contrib. Cause: Day Night Wet Dry Careless Driving FTYRW Improper Turn Ran Red Light Exceeded Speed Improper Passing Disreg Cntl Dev Aggress Road DUI Wrong Way
Total 9 2 0 11 5 1 0 0 0 0 0 0 0 0 0
Percent 81.82% 18.18% 0.00% 100.00% 45.45% 9.09% 0.00% 0.00% 0.00% 0.00% 0.00% 0.00% 0.00% 0.00% 0.00%

TOTAL ENTERING VEHICLES/ADT: **12,500** SEGMENT CRASH RATE: **23.182** CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004019 STATE ROUTE: #N/A
 ROADWAY LIMITS: MP 0 TO MP 0.104 M.P. 0.000 TO 0.104 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)							
1	0.000	07/31/13	Wed	0850	Not Coded			0	0	1	Night	Wet	Failed To Keep In Proper Lane							
2	0.000	01/10/13	Thu	2245	Angle			0	1	0	Night	Dry	Ran Red Light							
3	0.000	09/22/13	Sun	1234	Sideswipe			0	0	1	Day	Dry	#N/A							
4	0.076	11/18/13	Mon	0700	Sideswipe			0	0	1	Day	Dry	#N/A							
5	0.085	11/07/13	Thu	0815	Rear-End			0	0	1	Day	Dry	Careless or Negligent Manner							
6	0.102	08/26/13	Mon	1345	Rear-End			0	0	1	Day	Wet	Careless or Negligent Manner							
7	0.104	07/07/13	Sun	1150	Angle			0	1	0	Day	Dry	Ran Red Light							
8	0.104	09/21/13	Sat	1230	Sideswipe			0	0	1	Day	Dry	Careless or Negligent Manner							
9	0.104	07/22/13	Mon	0520	Concrete Traffic Barrier			0	0	1	Night	Dry	#N/A							
10	0.104	01/06/13	Sun	1805	Angle			0	0	1	Day	Dry	No Contributing Action							
11	0.104	02/18/13	Mon	2200	Angle			0	0	1	Night	Dry	Ran Red Light							
12	0.104	11/19/13	Tue	0910	Angle			0	0	1	Day	Dry	Failed to Yield Right-Of-Way							
13	0.104	12/11/13	Wed	2140	Rear-End			0	0	1	Night	Dry	No Contributing Action							
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other					
13	0	2	11	3	0	5	0	0	3	0	0	0	1	0	1					
Percent	0.00%	15.38%	84.62%	23.08%	0.00%	38.46%	0.00%	0.00%	23.08%	0.00%	0.00%	0.00%	7.69%	0.00%	7.69%					
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way					
Total	8	5	2	11	3	1	0	3	0	0	0	0	0	1	0					
Percent	61.54%	38.46%	15.38%	84.62%	23.08%	7.69%	0.00%	23.08%	0.00%	0.00%	0.00%	0.00%	0.00%	7.69%	0.00%					
TOTAL ENTERING VEHICLES/ADT:								12,500	SEGMENT CRASH RATE:								27.397	CRASHES PER MILLION VEHICLE MILES		

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004019 STATE ROUTE: #N/A
ROADWAY LIMITS: MP 0 TO MP 0.104 M.P. 0.000 TO 0.104 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.000	02/02/14	Sun	1045	Rear-End	0	0	1	Day	Wet	#N/A
2	0.000	07/19/14	Sat	2225	Angle	0	0	1	Night	Dry	No Contributing Action
3	0.000	12/05/14	Fri	0027	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
4	0.000	11/14/14	Fri	1045	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
5	0.000	10/26/14	Sun	0249	Angle	0	0	1	Night	Dry	No Contributing Action
6	0.009	12/05/14	Fri	1105	Sideswipe	0	0	1	Day	Dry	No Contributing Action
7	0.047	04/04/14	Fri	0859	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
8	0.066	01/18/14	Sat	2000	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
9	0.085	02/16/14	Sun	1620	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
10	0.099	10/03/14	Fri	2238	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
11	0.102	08/01/14	Fri	1535	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
12	0.104	02/03/14	Mon	1500	Rear-End	0	0	1	Day	Dry	Other Contributing Action
13	0.104	02/12/14	Wed	0745	Rear-End	0	0	1	Day	Dry	Followed too Closely
14	0.104	08/04/14	Mon	1107	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
15	0.104	03/23/14	Sun	0652	Not Coded	0	0	1	Night	Dry	No Contributing Action
16	0.104	03/26/14	Wed	1330	Sideswipe	0	0	1	Day	Dry	Not Coded
17	0.104	11/25/14	Tue	1411	Rear-End	0	0	1	Day	Dry	Other Contributing Action

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
17	0	1	16	11	0	2	0	0	3	0	0	0	1	0	1
Percent	0.00%	5.88%	94.12%	64.71%	0.00%	11.76%	0.00%	0.00%	17.65%	0.00%	0.00%	0.00%	5.88%	0.00%	5.88%

Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	12	5	1	16	3	1	0	3	0	0	0	0	0	1	0
Percent	70.59%	29.41%	5.88%	94.12%	17.65%	5.88%	0.00%	17.65%	0.00%	0.00%	0.00%	0.00%	0.00%	5.88%	0.00%

TOTAL ENTERING VEHICLES/ADT: **12,500** SEGMENT CRASH RATE: **27.397** CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004019 STATE ROUTE: #N/A
 ROADWAY LIMITS: MP 0 TO MP 0.104 M.P. 0.000 TO 0.104 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.000	04/25/15	Sat	2154	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
2	0.000	07/16/15	Thu	1318	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
3	0.028	01/16/15	Fri	1229	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
4	0.047	06/02/15	Tue	1205	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
5	0.057	12/02/15	Wed	1110	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
6	0.066	12/17/15	Thu	1945	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
7	0.085	01/10/15	Sat	1800	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
8	0.085	08/11/15	Tue	0935	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
9	0.095	04/16/15	Thu	1422	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
10	0.095	07/28/15	Tue	0910	Sideswipe	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
11	0.104	07/08/15	Wed	1300	Sideswipe	0	0	1	Day	Dry	No Contributing Action
12	0.104	02/17/15	Tue	1845	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
13	0.104	04/04/15	Sat	1554	Rear-End	0	0	1	Day	Dry	Wrong Side or Wrong Way
14	0.104	05/06/15	Wed	1022	Rear-End	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
15	0.104	11/23/15	Mon	0920	Angle	0	0	1	Day	Dry	Other Contributing Action
16	0.104	12/24/15	Thu	1421	Sideswipe	0	0	1	Day	Dry	No Contributing Action

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
16	0	4	12	11	0	1	0	0	4	0	0	0	1	0	1
Percent	0.00%	25.00%	75.00%	68.75%	0.00%	6.25%	0.00%	0.00%	25.00%	0.00%	0.00%	0.00%	6.25%	0.00%	6.25%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	12	4	2	14	3	1	0	3	0	0	0	0	0	0	0
Percent	75.00%	25.00%	12.50%	87.50%	18.75%	6.25%	0.00%	18.75%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **12,500** SEGMENT CRASH RATE: **27.397** CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000													STATE ROUTE: 112				
ROADWAY LIMITS: MP 0.000 To MP 0.092													M.P. 0.000 TO 0.092		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2012													TO 12/ 2012		COUNTY: Miami-Dade		
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)			
1	0.092	03/21/12	Wed	0832	Sideswipe				0	0	1	Day	Dry	No Contributing Action			
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other		
1	0	0	1	0	0	0	0	0	1	0	0	0	0	0	0		
Percent	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%		
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way		
Total	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0		
Percent	100.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%		
TOTAL ENTERING VEHICLES/ADT: 14,500									SEGMENT CRASH RATE: 2.054 CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004000					STATE ROUTE:					112				
ROADWAY LIMITS:		MP 0.000 To MP 0.092					M.P. 0.000		TO 0.092		ENGINEER: FDOT D6					
STUDY PERIOD:		FROM 1/ 2015			TO 12/ 2015			COUNTY: Miami-Dade								
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)			
1	0.009	10/23/15	Fri	0414	Guardrail Face			0	0	1	Night	Wet	Careless or Negligent Manner			
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	
Percent	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	
Percent	0.00%	100.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: #DIV/0!								SEGMENT CRASH RATE: #DIV/0! CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004021					STATE ROUTE:					#N/A				
ROADWAY LIMITS:		MP 0.000 to MP 0.047					M.P. 0.000		TO 0.047		ENGINEER: FDOT D6					
STUDY PERIOD:		FROM 1/ 2011			TO 12/ 2011			COUNTY: Miami-Dade								
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)			
1	0.000	11/08/11	Tue	1206	Rear-End			0	0	1	Day	Dry	Careless or Negligent Manner			
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	
Percent	0.00%	0.00%	100.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	
Percent	100.00%	0.00%	0.00%	100.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 7,700								SEGMENT CRASH RATE: 7.570 CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004021				STATE ROUTE:				#N/A						
ROADWAY LIMITS:		MP 0.000 to MP 0.047				M.P. 0.000		TO 0.047		ENGINEER: FDOT D6						
STUDY PERIOD:		FROM 1/ 2015		TO 12/ 2015		COUNTY: Miami-Dade										
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.000	03/07/15	Sat	0115	Concrete Traffic Barrier				0	0	1	Night	Wet	Failed To Keep In Proper Lane		
2	0.009	10/06/15	Tue	0805	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
2	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	
Percent	0.00%	0.00%	100.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	
Percent	50.00%	50.00%	50.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: #DIV/0!								SEGMENT CRASH RATE: #DIV/0! CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004020					STATE ROUTE:					#N/A				
ROADWAY LIMITS:		MP 0.000 to MP 0.061					M.P. 0.000		TO 0.061		ENGINEER: FDOT D6					
STUDY PERIOD:		FROM 1/ 2012			TO 12/ 2012			COUNTY: Miami-Dade								
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.000	03/27/12	Tue	1155	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	
Percent	0.00%	0.00%	100.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	
Percent	100.00%	0.00%	0.00%	100.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 11,000								SEGMENT CRASH RATE: 4.083 CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004020					STATE ROUTE:					#N/A				
ROADWAY LIMITS:		MP 0.000 to MP 0.061					M.P. 0.000		TO 0.061		ENGINEER: FDOT D6					
STUDY PERIOD:		FROM 1/ 2014			TO 12/ 2014			COUNTY: Miami-Dade								
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)			
1	0.002	11/04/14	Tue	2330	Rear-End			0	0	1	Day	Dry	No Contributing Action			
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	
Percent	0.00%	0.00%	100.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	
Percent	100.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: #DIV/0!								SEGMENT CRASH RATE: #DIV/0! CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004023										STATE ROUTE:		#N/A			
ROADWAY LIMITS:		MP 0.000 to MP 0.187										M.P. 0.000 TO 0.104		ENGINEER: FDOT D6			
STUDY PERIOD:		FROM 1/ 2011				TO 12/ 2011				COUNTY: Miami-Dade							
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)			
1	0.000	04/27/11	Wed	0445	Overturn/Rollover				0	0	1	Day	Dry	Other Contributing Action			
2	0.187	03/10/11	Thu	0905	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner			
3	0.187	04/10/11	Sun	2125	Sideswipe				0	0	1	Night	Dry	Careless or Negligent Manner			
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other		
3	0	0	3	1	0	0	0	0	1	0	0	0	0	0	0		
Percent	0.00%	0.00%	100.00%	33.33%	0.00%	0.00%	0.00%	0.00%	33.33%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%		
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way		
Total	2	1	0	3	2	0	0	0	0	0	0	0	0	0	0		
Percent	66.67%	33.33%	0.00%	100.00%	66.67%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%		
TOTAL ENTERING VEHICLES/ADT: 13,000									SEGMENT CRASH RATE: 6.079 CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004023					STATE ROUTE:					#N/A				
ROADWAY LIMITS:		MP 0.000 to MP 0.187					M.P. 0.000		TO 0.104		ENGINEER: FDOT D6					
STUDY PERIOD:		FROM 1/ 2012			TO 12/ 2012			COUNTY: Miami-Dade								
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.187	11/12/12	Mon	0804	Rear-End				0	0	1	Day	Wet	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	
Percent	0.00%	0.00%	100.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	1	0	1	0	1	0	0	0	0	0	0	0	0	0	0	
Percent	100.00%	0.00%	100.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 13,500								SEGMENT CRASH RATE: 1.951 CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004023				STATE ROUTE:				#N/A					
ROADWAY LIMITS:		MP 0.000 to MP 0.187				M.P. 0.000		TO 0.104		ENGINEER: FDOT D6					
STUDY PERIOD:		FROM 1/ 2013		TO 12/ 2013		COUNTY: Miami-Dade									
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.187	03/30/13	Sat	2240	Rear-End			0	1	0	Night	Dry	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
1	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0
Percent	0.00%	100.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0
Percent	0.00%	100.00%	0.00%	100.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
TOTAL ENTERING VEHICLES/ADT: 13,500								SEGMENT CRASH RATE: 1.951 CRASHES PER MILLION VEHICLE MILES							

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004023						STATE ROUTE:		#N/A						
ROADWAY LIMITS:		MP 0.000 to MP 0.187						M.P. 0.000 TO 0.104		ENGINEER: FDOT D6						
STUDY PERIOD:		FROM 1/ 2014			TO 12/ 2014			COUNTY: Miami-Dade								
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.055	01/04/14	Sat	1300	Rear-End				0	1	0	Day	Wet	#N/A		
2	0.187	06/12/14	Thu	0745	Rear-End				0	1	0	Day	Wet	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
2	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	
Percent	0.00%	100.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	2	0	2	0	1	0	0	0	0	0	0	0	0	0	0	
Percent	100.00%	0.00%	100.00%	0.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 13,500								SEGMENT CRASH RATE: 1.951 CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004023						STATE ROUTE:		#N/A						
ROADWAY LIMITS:		MP 0.000 to MP 0.187						M.P. 0.000 TO 0.104		ENGINEER: FDOT D6						
STUDY PERIOD:		FROM 1/ 2015			TO 12/ 2015			COUNTY: Miami-Dade								
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.000	12/28/15	Mon	0642	Rear-End				0	0	1	Night	Dry	#N/A		
2	0.187	08/16/15	Sun	1200	Sideswipe				0	0	1	Day	Dry	#N/A		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
2	0	0	2	1	0	0	0	0	1	0	0	0	0	0	0	
Percent	0.00%	0.00%	100.00%	50.00%	0.00%	0.00%	0.00%	0.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	1	1	0	2	1	0	0	0	0	0	0	0	0	0	0	
Percent	50.00%	50.00%	0.00%	100.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 13,500								SEGMENT CRASH RATE: 1.951 CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:	87004022										STATE ROUTE:		#N/A			
ROADWAY LIMITS:	MP 0.000 to MP 0.184										M.P. 0.000 TO 0.184		ENGINEER: FDOT D6			
STUDY PERIOD:	FROM 1/ 2011					TO 12/ 2011					COUNTY: Miami-Dade					
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.157	07/13/11	Wed	2045	Angle				0	0	1	Night	Dry	#N/A		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
1	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	
Percent	0.00%	0.00%	100.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	
Percent	0.00%	100.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 13,000									SEGMENT CRASH RATE: 1.145 CRASHES PER MILLION VEHICLE MILES							

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004022										STATE ROUTE:		#N/A		
ROADWAY LIMITS:		MP 0.000 to MP 0.184										M.P. 0.000 TO 0.184		ENGINEER: FDOT D6		
STUDY PERIOD:		FROM 1/ 2012					TO 12/ 2012					COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.000	05/18/12	Fri	0325	Traffic Sign Support				0	0	1	Night	Dry	#N/A		
2	0.000	01/20/12	Fri	1650	Rear-End				0	1	0	Day	Dry	Careless or Negligent Manner		
3	0.000	09/11/12	Tue	1137	Sideswipe				0	1	0	Night	Dry	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
3	0	2	1	1	0	0	0	0	1	0	0	0	1	0	0	
Percent	0.00%	66.67%	33.33%	33.33%	0.00%	0.00%	0.00%	0.00%	33.33%	0.00%	0.00%	0.00%	33.33%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	1	2	0	3	2	0	0	0	0	0	0	0	0	0	0	
Percent	33.33%	66.67%	0.00%	100.00%	66.67%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 15,500									SEGMENT CRASH RATE: 2.882 CRASHES PER MILLION VEHICLE MILES							

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004022						STATE ROUTE:				#N/A			
ROADWAY LIMITS:		MP 0.000 to MP 0.184						M.P. 0.000		TO 0.184		ENGINEER: FDOT D6			
STUDY PERIOD:		FROM 1/ 2014			TO 12/ 2014			COUNTY: Miami-Dade							
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.000	11/21/14	Fri	0230	Tree (Standing)			0	0	1	Night	Wet	#N/A		
2	0.184	03/13/14	Thu	0200	Not Coded			0	1	0	Night	Wet	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
2	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0
Percent	0.00%	50.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0
Percent	0.00%	100.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
TOTAL ENTERING VEHICLES/ADT: #DIV/0!								SEGMENT CRASH RATE: #DIV/0! CRASHES PER MILLION VEHICLE MILES							

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004022					STATE ROUTE:					#N/A				
ROADWAY LIMITS:		MP 0.000 to MP 0.184					M.P. 0.000		TO 0.184		ENGINEER: FDOT D6					
STUDY PERIOD:		FROM 1/ 2015			TO 12/ 2015			COUNTY: Miami-Dade								
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)			
1	0.000	05/17/15	Sun	1030	Other Post, Pole Or Support			0	0	1	Day	Dry	Careless or Negligent Manner			
2	0.089	09/25/15	Fri	1627	Rear-End			0	1	0	Day	Dry	Careless or Negligent Manner			
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
2	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	
Percent	0.00%	50.00%	50.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	2	0	0	2	0	0	0	0	0	0	0	0	0	0	0	
Percent	100.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: #DIV/0!								SEGMENT CRASH RATE: #DIV/0! CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004024													STATE ROUTE: #N/A						
ROADWAY LIMITS: MP 0.000 to MP 0.413													M.P. 0.000		TO 0.413		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2011													TO 12/ 2011		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)					
1	0.000	09/15/11	Thu	0855	Rear-End				0	2	0	Day	Dry	Careless or Negligent Manner					
2	0.000	12/09/11	Fri	0045	Overturn/Rollover				0	1	0	Night	Dry	No Contributing Action					
3	0.224	01/27/11	Thu	1020	Pedestrian				1	1	0	Night	Dry	No Contributing Action					
4	0.375	07/31/11	Sun	1555	Rear-End				0	2	0	Day	Dry	Careless or Negligent Manner					
5	0.394	04/22/11	Fri	1240	Rear-End				0	2	0	Day	Dry	Other Contributing Action					
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
5	1	5	0	3	0	0	0	0	0	0	1	0	0	0	0				
Percent	20.00%	100.00%	0.00%	60.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	20.00%	0.00%	0.00%	0.00%	0.00%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	3	2	0	5	2	0	0	0	0	0	0	0	0	1	0				
Percent	60.00%	40.00%	0.00%	100.00%	40.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	20.00%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 28,500									SEGMENT CRASH RATE: 1.164 CRASHES PER MILLION VEHICLE MILES										

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004024 STATE ROUTE: #N/A
ROADWAY LIMITS: MP 0.000 to MP 0.413 M.P. 0.000 TO 0.413 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)			
1	0.000	03/11/12	Sun	1445	Rear-End				0	1	0	Day	Dry	Careless or Negligent Manner			
2	0.000	05/20/12	Sun	0730	Guardrail Face				0	0	1	Day	Dry	Careless or Negligent Manner			
3	0.000	07/16/12	Mon	1641	Guardrail Face				0	1	0	Day	Wet	Careless or Negligent Manner			
4	0.000	08/25/12	Sat	1329	Rear-End				0	0	1	Day	Wet	Careless or Negligent Manner			
5	0.000	02/19/12	Sun	1300	Rear-End				0	1	0	Day	Dry	Careless or Negligent Manner			
6	0.000	03/02/12	Fri	1248	Not Coded				0	0	1	Day	Dry	#N/A			
7	0.000	02/16/12	Thu	1130	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner			
8	0.000	07/22/12	Sun	0715	Angle				0	0	1	Day	Wet	Careless or Negligent Manner			
9	0.000	05/21/12	Mon	1505	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner			
10	0.000	12/07/12	Fri	1223	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner			
11	0.000	06/03/12	Sun	1535	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner			
12	0.129	01/28/12	Sat	1255	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner			
13	0.163	09/09/12	Sun	1502	Rear-End				0	0	1	Day	Dry	#N/A			
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other		
13	0	3	10	9	0	1	0	0	0	0	0	0	2	0	1		
Percent	0.00%	23.08%	76.92%	69.23%	0.00%	7.69%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	15.38%	0.00%	7.69%		
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way		
Total	13	0	3	10	11	0	0	0	0	0	0	0	0	0	0		
Percent	100.00%	0.00%	23.08%	76.92%	84.62%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%		
TOTAL ENTERING VEHICLES/ADT:								35,000	SEGMENT CRASH RATE:								2.464 CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004024 STATE ROUTE: #N/A
ROADWAY LIMITS: MP 0.000 to MP 0.413 M.P. 0.000 TO 0.413 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)				
1	0.000	07/21/13	Sun	1608	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner				
2	0.000	07/19/13	Fri	0841	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner				
3	0.000	05/16/13	Thu	1541	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
4	0.000	08/18/13	Sun	0004	Rear-End	0	0	1	Night	Dry	Followed too Closely				
5	0.000	07/26/13	Fri	1352	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner				
6	0.000	09/21/13	Sat	1025	Sideswipe	0	0	1	Day	Dry	#N/A				
7	0.000	08/19/13	Mon	0831	Rear-End	0	1	0	Day	Dry	No Contributing Action				
8	0.000	09/20/13	Fri	0835	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner				
9	0.113	03/09/13	Sat	1546	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
10	0.113	03/09/13	Sat	1547	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
11	0.207	10/04/13	Fri	1000	Angle	0	0	1	Day	Dry	No Contributing Action				
12	0.261	09/30/13	Mon	0922	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
13	0.313	10/31/13	Thu	1948	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner				
14	0.366	07/22/13	Mon	1635	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
15	0.394	10/29/13	Tue	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
15	0	5	10	12	0	1	0	0	2	0	0	0	0	0	0
Percent	0.00%	33.33%	66.67%	80.00%	0.00%	6.67%	0.00%	0.00%	13.33%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	13	2	1	14	11	0	0	0	0	0	0	0	0	0	0
Percent	86.67%	13.33%	6.67%	93.33%	73.33%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
TOTAL ENTERING VEHICLES/ADT:						35,500	SEGMENT CRASH RATE:						2.803 CRASHES PER MILLION VEHICLE MILES		

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004024 STATE ROUTE: #N/A
ROADWAY LIMITS: MP 0.000 to MP 0.413 M.P. 0.000 TO 0.413 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.000	02/15/14	Sat	1028	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
2	0.000	02/21/14	Fri	1350	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
3	0.000	02/17/14	Mon	1220	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
4	0.000	08/28/14	Thu	1805	Other Non-Collision	1	0	0	Day	Dry	Improper Passing
5	0.000	11/26/14	Wed	1030	Guardrail Face	0	0	1	Day	Dry	Careless or Negligent Manner
6	0.000	03/19/14	Wed	0145	Guardrail End	0	0	1	Night	Dry	Careless or Negligent Manner
7	0.034	02/17/14	Mon	1337	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
8	0.313	11/16/14	Sun	1247	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
9	0.318	01/15/14	Wed	0749	Guardrail Face	0	0	1	Day	Wet	Careless or Negligent Manner
10	0.318	12/03/14	Wed	1553	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
11	0.385	03/22/14	Sat	1515	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
12	0.394	10/26/14	Sun	1454	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
13	0.413	10/21/14	Tue	0910	Rear-End	0	0	1	Day	Dry	#N/A
14	0.413	09/23/14	Tue	1015	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
14	1	4	9	10	0	0	0	0	0	0	0	0	0	0	0
Percent	7.14%	28.57%	64.29%	71.43%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%

Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	13	1	2	12	11	0	0	0	0	0	0	0	0	2	0
Percent	92.86%	7.14%	14.29%	85.71%	78.57%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	14.29%	0.00%

TOTAL ENTERING VEHICLES/ADT: **35,500** SEGMENT CRASH RATE: **2.803** CRASHES PER MILLION VEHICLE MILES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004024 STATE ROUTE: #N/A
ROADWAY LIMITS: MP 0.000 to MP 0.413 M.P. 0.000 TO 0.413 ENGINEER: FDOT D6
STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.000	09/26/15	Sat	0930	Guardrail Face	0	1	0	Day	Wet	Swerved Or Avoided
2	0.163	12/24/15	Thu	1535	Rear-End	0	1	0	Day	Dry	No Contributing Action
3	0.163	12/26/15	Sat	1601	Rear-End	0	0	1	Day	Dry	No Contributing Action
4	0.213	01/10/15	Sat	1430	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
5	0.261	03/14/15	Sat	1200	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
6	0.261	07/08/15	Wed	1000	Rear-End	0	0	1	Day	Dry	No Contributing Action
7	0.261	07/10/15	Fri	1220	Rear-End	0	0	1	Day	Dry	No Contributing Action
8	0.280	07/08/15	Wed	0900	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
9	0.299	11/11/15	Wed	1550	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
10	0.318	06/12/15	Fri	0857	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
11	0.318	10/31/15	Sat	2035	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
12	0.318	12/31/15	Thu	1733	Angle	0	8	0	Night	Dry	Careless or Negligent Manner
13	0.356	03/22/15	Sun	1427	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
14	0.356	11/07/15	Sat	2054	Not Coded	0	1	0	Night	Dry	#N/A
15	0.375	05/04/15	Mon	1648	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
16	0.413	02/22/15	Sun	1250	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
17	0.413	08/05/15	Wed	1000	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
18	0.413	01/01/15	Thu	0548	Embankment	0	0	1	Night	Dry	Careless or Negligent Manner
19	0.413	04/28/15	Tue	1252	Sideswipe	0	0	1	Day	Wet	Improper Passing
20	0.413	04/11/15	Sat	0430	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
20	0	6	14	15	0	1	0	0	1	0	0	0	0	0	0
Percent	0.00%	30.00%	70.00%	75.00%	0.00%	5.00%	0.00%	0.00%	5.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%

Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	15	5	3	17	11	0	0	0	0	0	0	0	0	1	0
Percent	75.00%	25.00%	15.00%	85.00%	55.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	5.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **35,500**

SEGMENT CRASH RATE: **2.803 CRASHES PER MILLION VEHICLE MILES**

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87037201					STATE ROUTE:					#N/A				
ROADWAY LIMITS:		MP 0.000 to MP 0.264					M.P. 0.000		TO 0.264		ENGINEER: FDOT D6					
STUDY PERIOD:		FROM 1/ 2011			TO 12/ 2011			COUNTY: Miami-Dade								
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.000	08/07/11	Sun	1825	Guardrail Face				0	0	1	Day	Wet	Careless or Negligent Manner		
2	0.000	06/12/11	Sun	1630	Not Coded				0	0	1	Day	Wet	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
2	0	0	2	0	0	0	0	0	0	0	0	0	1	0	1	
Percent	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	50.00%	0.00%	50.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	2	0	2	0	2	0	0	0	0	0	0	0	0	0	0	
Percent	100.00%	0.00%	100.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 12,000								SEGMENT CRASH RATE: 1.730 CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87037201						STATE ROUTE:		#N/A						
ROADWAY LIMITS:		MP 0.000 to MP 0.264						M.P. 0.000 TO 0.264		ENGINEER: FDOT D6						
STUDY PERIOD:		FROM 1/ 2015			TO 12/ 2015			COUNTY: Miami-Dade								
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.076	08/29/15	Sat	0315	Other Fixed Object				0	0	1	Night	Dry	#N/A		
2	0.111	09/24/15	Thu	2206	Rear-End				0	0	1	Night	Dry	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
2	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	
Percent	0.00%	0.00%	100.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	
Percent	0.00%	100.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: #DIV/0!								SEGMENT CRASH RATE: #DIV/0! CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:	87037202										STATE ROUTE:		#N/A				
ROADWAY LIMITS:	MP 0.000 to MP 0.264										M.P. 0.000 TO 0.264		ENGINEER: FDOT D6				
STUDY PERIOD:	FROM 1/ 2011 TO 12/ 2011										COUNTY: Miami-Dade						
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)			
1	0.000	10/24/11	Mon	2200	Guardrail Face				0	1	0	Night	Dry	No Contributing Action			
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other		
1	0	1	0	0	0	0	0	0	0	0	0	0	1	0	0		
Percent	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%		
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way		
Total	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0		
Percent	0.00%	100.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%		
TOTAL ENTERING VEHICLES/ADT:									15,000		SEGMENT CRASH RATE:					0.692 CRASHES PER MILLION VEHICLE MILES	

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87037202					STATE ROUTE:					#N/A				
ROADWAY LIMITS:		MP 0.000 to MP 0.264					M.P. 0.000		TO 0.264		ENGINEER: FDOT D6					
STUDY PERIOD:		FROM 1/ 2013			TO 12/ 2013			COUNTY: Miami-Dade								
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.000	10/14/13	Mon	1552	Sideswipe				0	0	1	Day	Dry	No Contributing Action		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
1	0	0	1	0	0	0	0	0	1	0	0	0	0	0	0	
Percent	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	
Percent	100.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 14,500									SEGMENT CRASH RATE: 0.716 CRASHES PER MILLION VEHICLE MILES							

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87037202				STATE ROUTE:				#N/A					
ROADWAY LIMITS:		MP 0.000 to MP 0.264				M.P. 0.000		TO 0.264		ENGINEER: FDOT D6					
STUDY PERIOD:		FROM 1/ 2015		TO 12/ 2015		COUNTY: Miami-Dade									
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.000	05/07/15	Thu	1600	Rear-End			0	0	1	Day	Dry	Followed too Closely		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0
Percent	0.00%	0.00%	100.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0
Percent	100.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
TOTAL ENTERING VEHICLES/ADT: 14,500								SEGMENT CRASH RATE: 0.716 CRASHES PER MILLION VEHICLE MILES							

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004025					STATE ROUTE:					#N/A				
ROADWAY LIMITS:		MP 0.000 to MP 0.264					M.P. 0.000		TO 0.008		ENGINEER: FDOT D6					
STUDY PERIOD:		FROM 1/ 2011			TO 12/ 2011			COUNTY: Miami-Dade								
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)			
1	0.075	07/01/11	Fri	0606	Other Non-Collision			0	1	0	Day	Dry	Drove too Fast for Conditions			
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	
Percent	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	1	0	0	1	0	0	0	0	1	0	0	0	0	1	0	
Percent	100.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	100.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 15,000								SEGMENT CRASH RATE: 24.353 CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:		87004025				STATE ROUTE:				#N/A						
ROADWAY LIMITS:		MP 0.000 to MP 0.264				M.P. 0.000		TO 0.008		ENGINEER: FDOT D6						
STUDY PERIOD:		FROM 1/ 2012		TO 12/ 2012		COUNTY: Miami-Dade										
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.013	04/21/12	Sat	0350	Traffic Sign Support				0	0	1	Night	Wet	Over-Correcting/Over-Steering		
2	0.075	09/17/12	Mon	0800	Guardrail Face				0	1	0	Day	Wet	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
2	0	1	1	0	0	0	0	0	0	0	0	0	2	0	0	
Percent	0.00%	50.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	1	1	2	0	1	0	0	0	0	0	0	0	0	0	0	
Percent	50.00%	50.00%	100.00%	0.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 16,000								SEGMENT CRASH RATE: 45.662 CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004025												STATE ROUTE: #N/A						
ROADWAY LIMITS: MP 0.000 to MP 0.264												M.P. 0.000		TO 0.008		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2013												TO 12/ 2013		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE					FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)			
1	0.075	01/25/13	Fri	1545	Sideswipe					0	0	1	Day	Dry	Improper Passing			
2	0.075	05/02/13	Thu	1736	Not Coded					0	0	1	Day	Wet	Followed too Closely			
3	0.075	09/23/13	Mon	1656	Rear-End					0	0	1	Night	Dry	Careless or Negligent Manner			
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other			
3	0	0	3	1	0	0	0	0	1	0	0	0	0	0	1			
Percent	0.00%	0.00%	100.00%	33.33%	0.00%	0.00%	0.00%	0.00%	33.33%	0.00%	0.00%	0.00%	0.00%	0.00%	33.33%			
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way			
Total	2	1	1	2	1	0	0	0	0	1	0	0	0	0	0			
Percent	66.67%	33.33%	33.33%	66.67%	33.33%	0.00%	0.00%	0.00%	0.00%	33.33%	0.00%	0.00%	0.00%	0.00%	0.00%			
TOTAL ENTERING VEHICLES/ADT: 16,500										SEGMENT CRASH RATE: 66.418 CRASHES PER MILLION VEHICLE MILES								

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004025													STATE ROUTE: #N/A						
ROADWAY LIMITS: MP 0.000 to MP 0.264													M.P. 0.000		TO 0.008		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2014													TO 12/ 2014		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE					FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)				
1	0.019	01/16/14	Thu	1210	Rear-End					0	0	1	Day	Dry	Careless or Negligent Manner				
2	0.038	12/22/14	Mon	1915	Other Non-Collision					0	0	1	Night	Dry	No Contributing Action				
3	0.075	01/13/14	Mon	1130	Not Coded					0	0	1	Day	Dry	Careless or Negligent Manner				
4	0.075	08/08/14	Fri	1827	Sideswipe					0	0	1	Night	Dry	No Contributing Action				
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
4	0	0	4	1	0	0	0	0	1	0	0	0	0	0	1				
Percent	0.00%	0.00%	100.00%	25.00%	0.00%	0.00%	0.00%	0.00%	25.00%	0.00%	0.00%	0.00%	0.00%	0.00%	25.00%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	2	2	0	4	1	0	0	0	0	1	0	0	0	0	0				
Percent	50.00%	50.00%	0.00%	100.00%	25.00%	0.00%	0.00%	0.00%	0.00%	25.00%	0.00%	0.00%	0.00%	0.00%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 16,500									SEGMENT CRASH RATE: 66.418 CRASHES PER MILLION VEHICLE MILES										

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004025													STATE ROUTE: #N/A						
ROADWAY LIMITS: MP 0.000 to MP 0.264													M.P. 0.000		TO 0.008		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2015													TO 12/ 2015		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE					FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)				
1	0.047	02/24/15	Tue	1625	Rear-End					0	0	1	Day	Dry	Careless or Negligent Manner				
2	0.075	10/01/15	Thu	1655	Rear-End					0	0	1	Day	Dry	Careless or Negligent Manner				
3	0.075	03/24/15	Tue	1655	Rear-End					0	1	0	Day	Dry	Careless or Negligent Manner				
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
3	0	1	2	3	0	0	0	0	0	0	0	0	0	0	1				
Percent	0.00%	33.33%	66.67%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	33.33%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	3	0	0	3	1	0	0	0	0	1	0	0	0	0	0				
Percent	100.00%	0.00%	0.00%	100.00%	33.33%	0.00%	0.00%	0.00%	0.00%	33.33%	0.00%	0.00%	0.00%	0.00%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 16,500										SEGMENT CRASH RATE: 66.418 CRASHES PER MILLION VEHICLE MILES									

CLUSTER CRASH DATA (2011-2015)

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87003000													STATE ROUTE: 112						
INTERSECTING ROADWAY: Exit Ramp To NW 12 Avenue													M.P. 3.905		TO 4.193		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2011													TO 12/ 2011		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)					
1	3.905	06/20/11	Mon	1545	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner					
2	3.934	06/26/11	Sun	2115	Rear-End				0	2	0	Night	Wet	Careless or Negligent Manner					
3	3.961	03/27/11	Sun	0503	Rear-End				0	0	1	Night	Dry	#N/A					
4	4.017	09/22/11	Thu	1805	Sideswipe				0	0	1	Day	Dry	Failed To Keep In Proper Lane					
5	4.017	01/30/11	Sun	0920	Rear-End				0	1	0	Day	Dry	No Contributing Action					
6	4.036	08/24/11	Wed	0830	Rear-End				0	1	0	Day	Dry	Careless or Negligent Manner					
7	4.193	10/09/11	Sun	1711	Angle				0	3	0	Day	Dry	Failed to Yield Right-Of-Way					
8	4.193	06/02/11	Thu	0910	Pedalcycle				0	1	0	Day	Dry	No Contributing Action					
9	4.209	01/10/11	Mon	1113	Other Non-Fixed Object				0	0	1	Day	Dry	Careless or Negligent Manner					
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
9	0	5	4	5	0	1	0	0	1	0	1	0	0	0	0				
Percent	0.00%	55.56%	44.44%	55.56%	0.00%	11.11%	0.00%	0.00%	11.11%	0.00%	11.11%	0.00%	0.00%	0.00%	0.00%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	7	2	1	8	4	1	0	0	0	0	0	0	0	0	0				
Percent	77.78%	22.22%	11.11%	88.89%	44.44%	11.11%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 110,000									SPOT CRASH RATE: 0.224 CRASHES PER MILLION ENTERING VEHICLES										

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87003000													STATE ROUTE: 112						
INTERSECTING ROADWAY: Exit Ramp To NW 12 Avenue													M.P. 3.905		TO 4.193		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2012													TO 12/ 2012		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)					
1	4.193	06/14/12	Thu	0425	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner					
2	4.193	10/15/12	Mon	1500	Angle				0	0	1	Day	Dry	Ran Red Light					
3	4.193	03/01/12	Thu	0730	Rear-End				0	0	1	Day	Dry	Failed to Yield Right-Of-Way					
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
3	0	0	3	2	0	1	0	0	0	0	0	0	0	0	0				
Percent	0.00%	0.00%	100.00%	66.67%	0.00%	33.33%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	3	0	0	3	1	1	0	1	0	0	0	0	0	0	0				
Percent	100.00%	0.00%	0.00%	100.00%	33.33%	33.33%	0.00%	33.33%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%				
TOTAL ENTERING VEHICLES/ADT: #DIV/0!										SPOT CRASH RATE: #DIV/0! CRASHES PER MILLION ENTERING VEHICLES									

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87003000 STATE ROUTE: 112
 INTERSECTING ROADWAY: Exit Ramp To NW 12 Avenue M.P. 3.905 TO 4.193 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE					FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	3.943	08/13/13	Tue	2320	Concrete Traffic Barrier					0	2	0	Night	Dry	Careless or Negligent Manner
2	3.947	02/16/13	Sat	1846	Rear-End					0	0	1	Night	Dry	Careless or Negligent Manner
3	3.962	10/08/13	Tue	1830	Rear-End					0	1	0	Night	Dry	#N/A
4	3.971	07/07/13	Sun	1800	Rear-End					0	0	1	Day	Dry	Other Contributing Action
5	4.026	05/30/13	Thu	1737	Rear-End					0	0	1	Day	Wet	Failed to Yield Right-Of-Way
6	4.193	04/04/13	Thu	0750	Left-Turn					0	1	0	Day	Dry	Failed to Yield Right-Of-Way
7	4.193	01/30/13	Wed	1445	Left-Turn					0	0	1	Day	Dry	Careless or Negligent Manner
8	4.193	08/14/13	Wed	1030	Rear-End					0	0	1	Day	Dry	Careless or Negligent Manner
9	4.193	10/01/13	Tue	1210	Sideswipe					0	0	1	Day	Dry	Other Contributing Action
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
9	0	3	6	5	0	0	2	0	1	0	0	0	1	0	0
Percent	0.00%	33.33%	66.67%	55.56%	0.00%	0.00%	22.22%	0.00%	11.11%	0.00%	0.00%	0.00%	11.11%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	6	3	1	8	4	2	0	0	0	0	0	0	0	0	0
Percent	66.67%	33.33%	11.11%	88.89%	44.44%	22.22%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
TOTAL ENTERING VEHICLES/ADT: 104,000										SPOT CRASH RATE: 0.237 CRASHES PER MILLION ENTERING VEHICLES					

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87003000 STATE ROUTE: 112
 INTERSECTING ROADWAY: Exit Ramp To NW 12 Avenue M.P. 3.905 TO 4.193 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)				
1	3.905	12/24/14	Wed	0710	Other Non-Fixed Object	0	0	1	Day	Dry	Other Contributing Action				
2	3.934	11/19/14	Wed	1100	Sideswipe	0	0	1	Day	Wet	#N/A				
3	3.943	05/21/14	Wed	0000	Sideswipe	0	3	0	Night	Dry	Other Contributing Action				
4	3.971	12/06/14	Sat	2037	Concrete Traffic Barrier	0	0	1	Night	Dry	Other Contributing Action				
5	4.193	09/30/14	Tue	1805	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way				
6	4.193	08/29/14	Fri	0719	Angle	0	0	1	Day	Dry	Not Coded				
7	4.193	01/26/14	Sun	0930	Sideswipe	0	1	0	Day	Dry	Careless or Negligent Manner				
8	4.193	03/16/14	Sun	1530	Parked Motor Vehicle	0	0	1	Day	Dry	Improper Backing				
9	4.193	02/11/14	Tue	1635	Angle	0	3	0	Day	Dry	Other Contributing Action				
10	4.193	09/10/14	Wed	0740	Angle	0	0	1	Day	Dry	Ran Red Light				
11	4.193	08/16/14	Sat	1005	Angle	0	0	1	Day	Dry	Other Contributing Action				
12	4.221	08/25/14	Mon	1630	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner				
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
12	0	5	7	1	0	5	0	0	3	0	0	1	1	0	0
Percent	0.00%	41.67%	58.33%	8.33%	0.00%	41.67%	0.00%	0.00%	25.00%	0.00%	0.00%	8.33%	8.33%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	10	2	1	11	4	2	0	0	0	0	0	0	0	1	0
Percent	83.33%	16.67%	8.33%	91.67%	33.33%	16.67%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	8.33%	0.00%
TOTAL ENTERING VEHICLES/ADT: 104,000						SPOT CRASH RATE: 0.237 CRASHES PER MILLION ENTERING VEHICLES									

**State of Florida Department of Transportation
CRASH SUMMARY**

State of Florida Department of Transportation CRASH SUMMARY															
SECTION:		87003000										STATE ROUTE: 112			
INTERSECTING ROADWAY:		Exit Ramp To NW 12 Avenue						M.P. 3.905		TO 4.193		ENGINEER: FDOT D6			
STUDY PERIOD:		FROM 1/ 2015				TO 12/ 2015				COUNTY: Miami-Dade					
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)	
1	3.905	11/10/15	Tue	0816	Rear-End				0	1	0	Day	Dry	No Contributing Action	
2	3.905	10/26/15	Mon	0650	Sideswipe				0	0	1	Day	Dry	#N/A	
3	3.924	04/21/15	Tue	0830	Rear-End				0	1	0	Day	Wet	Careless or Negligent Manner	
4	3.924	05/06/15	Wed	1325	Rear-End				0	3	0	Day	Dry	#N/A	
5	3.943	03/13/15	Fri	1909	Sideswipe				0	0	1	Night	Dry	#N/A	
6	3.965	06/13/15	Sat	0102	Pedestrian				1	0	0	Night	Dry	No Contributing Action	
7	4.026	09/15/15	Tue	0750	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner	
8	4.184	02/02/15	Mon	0920	Rear-End				0	0	1	Day	Wet	Followed too Closely	
9	4.193	05/14/15	Thu	2339	Angle				0	0	1	Night	Dry	Ran Red Light	
10	4.193	06/09/15	Tue	2130	Rear-End				0	0	1	Night	Dry	Followed too Closely	
11	4.193	05/16/15	Sat	1852	Angle				0	1	0	Day	Dry	Careless or Negligent Manner	
12	4.193	10/30/15	Fri	1200	Angle				0	0	1	Day	Dry	Other Contributing Action	
13	4.193	03/18/15	Wed	1235	Sideswipe				0	0	1	Day	Dry	Improper Turn	
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
13	1	4	8	6	0	3	0	0	3	0	1	0	1	0	0
Percent	7.69%	30.77%	61.54%	46.15%	0.00%	23.08%	0.00%	0.00%	23.08%	0.00%	7.69%	0.00%	7.69%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	9	4	2	11	4	2	0	0	0	0	0	0	0	0	
Percent	69.23%	30.77%	15.38%	84.62%	30.77%	15.38%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 104,000								SPOT CRASH RATE: 0.237 CRASHES PER MILLION ENTERING VEHICLES							

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000													STATE ROUTE: 112						
INTERSECTING ROADWAY: I-95													M.P. 0.490		TO 0.543		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2011													TO 12/ 2011		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)					
1	0.498	09/11/11	Sun	0250	Impact Attenuator/Crash Cushion				0	0	1	Night	Dry	Careless or Negligent Manner					
2	0.517	05/11/11	Wed	1610	Rear-End				0	1	0	Day	Dry	Careless or Negligent Manner					
3	0.517	04/12/11	Tue	2135	Rear-End				0	4	0	Night	Dry	Careless or Negligent Manner					
4	0.527	05/11/11	Wed	1547	Rear-End				0	1	0	Day	Dry	Careless or Negligent Manner					
5	0.536	05/20/11	Fri	1127	Sideswipe				0	0	1	Day	Dry	No Contributing Action					
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
5	0	3	2	3	0	0	0	0	1	0	0	0	1	0	0				
Percent	0.00%	60.00%	40.00%	60.00%	0.00%	0.00%	0.00%	0.00%	20.00%	0.00%	0.00%	0.00%	20.00%	0.00%	0.00%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	3	2	0	5	4	0	0	0	0	0	0	0	0	0	0				
Percent	60.00%	40.00%	0.00%	100.00%	80.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 110,000										SPOT CRASH RATE: 0.125 CRASHES PER MILLION ENTERING VEHICLES									

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000													STATE ROUTE: 112						
INTERSECTING ROADWAY: I-95													M.P. 0.490		TO 0.543		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2012													TO 12/ 2012		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)						
1	0.493	01/24/12	Tue	1715	Rear-End			0	0	1	Day	Dry	Careless or Negligent Manner						
2	0.508	02/26/12	Sun	0550	Concrete Traffic Barrier			0	0	1	Night	Dry	Careless or Negligent Manner						
3	0.508	10/22/12	Mon	0812	Rear-End			0	0	1	Day	Dry	Other Contributing Action						
4	0.527	09/23/12	Sun	0510	Rear-End			0	0	1	Night	Dry	Careless or Negligent Manner						
5	0.527	12/11/12	Tue	1914	Rear-End			0	0	1	Night	Dry	No Contributing Action						
6	0.539	06/05/12	Tue	0742	Concrete Traffic Barrier			0	1	0	Day	Dry	No Contributing Action						
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
6	0	1	5	4	0	0	0	0	0	0	0	0	2	0	0				
Percent	0.00%	16.67%	83.33%	66.67%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	33.33%	0.00%	0.00%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	3	3	0	6	3	0	0	0	0	0	0	0	0	0	0				
Percent	50.00%	50.00%	0.00%	100.00%	50.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 100,500								SPOT CRASH RATE: 0.164 CRASHES PER MILLION ENTERING VEHICLES											

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: I-95 M.P. 0.490 TO 0.543 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.490	07/28/13	Sun	0130	Concrete Traffic Barrier	0	0	1	Night	Wet	Careless or Negligent Manner
2	0.490	03/08/13	Fri	1853	Rear-End	0	0	1	Night	Dry	No Contributing Action
3	0.505	08/20/13	Tue	1753	Sideswipe	0	0	1	Day	Dry	Improper Passing
4	0.508	01/07/13	Mon	0827	Rear-End	0	0	1	Day	Dry	No Contributing Action
5	0.521	10/14/13	Mon	1030	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
6	0.523	11/21/13	Thu	1913	Rear-End	0	0	1	Night	Wet	#N/A
7	0.527	04/26/13	Fri	0130	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
7	0	0	7	3	0	0	0	0	2	0	0	0	1	0	1
Percent	0.00%	0.00%	100.00%	42.86%	0.00%	0.00%	0.00%	0.00%	28.57%	0.00%	0.00%	0.00%	14.29%	0.00%	14.29%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	3	4	2	5	2	0	0	0	0	1	0	0	0	0	0
Percent	42.86%	57.14%	28.57%	71.43%	28.57%	0.00%	0.00%	0.00%	0.00%	14.29%	0.00%	0.00%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **104,000** SPOT CRASH RATE: **0.184** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: I-95 M.P. 0.490 TO 0.543 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.505	04/15/15	Wed	1448	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
2	0.505	06/02/15	Tue	0945	Rear-End	0	0	1	Day	Dry	Other Contributing Action
3	0.505	12/02/15	Wed	1510	Not Coded	0	0	1	Day	Dry	#N/A
4	0.505	05/28/15	Thu	1330	Sideswipe	0	0	1	Day	Dry	#N/A
5	0.514	06/15/15	Mon	0845	Concrete Traffic Barrier	0	0	1	Day	Wet	Careless or Negligent Manner
6	0.524	10/11/15	Sun	0530	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
7	0.524	12/06/15	Sun	1600	Sideswipe	0	0	1	Day	Dry	No Contributing Action
8	0.524	11/12/15	Thu	1035	Rear-End	0	1	0	Day	Dry	Other Contributing Action
9	0.524	12/17/15	Thu	0800	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
10	0.543	05/18/15	Mon	1354	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
11	0.543	12/08/15	Tue	1747	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
11	0	4	7	6	0	0	0	0	3	0	0	0	1	0	1
Percent	0.00%	36.36%	63.64%	54.55%	0.00%	0.00%	0.00%	0.00%	27.27%	0.00%	0.00%	0.00%	9.09%	0.00%	9.09%

Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	9	2	1	10	2	0	0	0	0	1	0	0	0	0	0
Percent	81.82%	18.18%	9.09%	90.91%	18.18%	0.00%	0.00%	0.00%	0.00%	9.09%	0.00%	0.00%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **104,000**

SPOT CRASH RATE: **0.184** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: N Miami Avenue M.P. 1.028 TO 1.081 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2011 TO 12/ 2011 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	1.028	12/05/11	Mon	1718	Rear-End	0	1	0	Night	Dry	No Contributing Action
2	1.028	12/09/11	Fri	1212	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
3	1.028	08/12/11	Fri	1751	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
4	1.028	09/20/11	Tue	0929	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
5	1.028	11/17/11	Thu	1905	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
6	1.028	11/28/11	Mon	1937	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
7	1.028	08/16/11	Tue	1210	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
8	1.038	08/12/11	Fri	1030	Rear-End	0	0	1	Day	Dry	No Contributing Action
9	1.043	03/05/11	Sat	0357	Curb	0	1	0	Night	Dry	Failed To Keep In Proper Lane
10	1.043	03/17/11	Thu	0700	Rear-End	0	0	1	Day	Dry	#N/A
11	1.043	03/17/11	Thu	0930	Sideswipe	0	0	1	Day	Dry	No Contributing Action
12	1.078	12/17/11	Sat	1145	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
13	1.083	10/20/11	Thu	1041	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
14	1.083	10/20/11	Thu	0955	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
15	1.089	11/28/11	Mon	1218	Sideswipe	0	0	1	Day	Dry	No Contributing Action
16	0.000	01/28/11	Fri	1450	Concrete Traffic Barrier	0	0	1	Day	Dry	No Contributing Action
17	0.000	03/25/11	Fri	0215	Other Non-Collision	0	0	1	Night	Dry	No Contributing Action
18	0.000	06/13/11	Mon	1409	Parked Motor Vehicle	0	1	0	Day	Dry	#N/A
19	0.104	04/20/11	Wed	0031	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
19	0	7	12	13	0	0	0	0	2	0	0	1	2	0	0
Percent	0.00%	36.84%	63.16%	68.42%	0.00%	0.00%	0.00%	0.00%	10.53%	0.00%	0.00%	5.26%	10.53%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	14	5	0	19	10	0	0	0	0	0	0	0	0	0	0
Percent	73.68%	26.32%	0.00%	100.00%	52.63%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **116,000** SPOT CRASH RATE: **0.449** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: N Miami Avenue M.P. 1.028 TO 1.081 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	1.024	05/20/12	Sun	1140	Other Fixed Object	0	2	0	Day	Dry	#N/A
2	1.028	02/13/12	Mon	1303	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
3	1.038	09/19/12	Wed	1510	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
4	1.043	10/31/12	Wed	2130	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
5	1.078	02/04/12	Sat	1745	Not Coded	0	0	1	Day	Dry	#N/A
6	1.078	07/24/12	Tue	1007	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
7	1.078	07/24/12	Tue	1314	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
8	1.078	01/05/12	Thu	0815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
9	1.081	06/03/12	Sun	0150	Sideswipe	0	1	0	Night	Dry	Careless or Negligent Manner
10	0.000	10/14/12	Sun	1140	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
11	0.000	06/08/12	Fri	1300	Rear-End	0	0	1	Day	Dry	Other Contributing Action
12	0.000	12/07/12	Fri	1135	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
13	0.000	05/11/12	Fri	1438	Rear-End	0	0	1	Day	Dry	Other Contributing Action
14	0.000	10/12/12	Fri	1024	Angle	0	0	1	Day	Dry	No Contributing Action
15	0.009	09/08/12	Sat	2115	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
16	0.009	09/14/12	Fri	1143	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
17	0.066	06/28/12	Thu	1100	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
18	0.104	12/12/12	Wed	1521	Rear-End	0	0	1	Day	Dry	Followed too Closely
19	0.104	04/27/12	Fri	1325	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
20	0.104	08/25/12	Sat	0300	Angle	0	0	1	Night	Dry	No Contributing Action

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
20	0	5	15	13	0	2	0	0	2	0	0	0	1	0	2
Percent	0.00%	25.00%	75.00%	65.00%	0.00%	10.00%	0.00%	0.00%	10.00%	0.00%	0.00%	0.00%	5.00%	0.00%	10.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	16	4	0	20	12	1	0	0	0	0	0	0	0	0	0
Percent	80.00%	20.00%	0.00%	100.00%	60.00%	5.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **119,500**

SPOT CRASH RATE: **0.459** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: N Miami Avenue M.P. 1.028 TO 1.081 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	1.028	05/03/13	Fri	2055	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
2	1.028	09/17/13	Tue	0854	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
3	1.038	09/15/13	Sun	1105	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
4	1.089	06/21/13	Fri	0935	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
5	1.089	10/26/13	Sat	0031	Concrete Traffic Barrier	0	0	1	Night	Dry	Careless or Negligent Manner
6	1.089	05/10/13	Fri	1100	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
7	0.000	01/10/13	Thu	2245	Angle	0	1	0	Night	Dry	Ran Red Light
8	0.000	07/31/13	Wed	0850	Not Coded	0	0	1	Night	Wet	Failed To Keep In Proper Lane
9	0.000	09/22/13	Sun	1234	Sideswipe	0	0	1	Day	Dry	#N/A
10	0.076	11/18/13	Mon	0700	Sideswipe	0	0	1	Day	Dry	#N/A
11	0.085	11/07/13	Thu	0815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
12	0.102	08/26/13	Mon	1345	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
13	0.104	01/06/13	Sun	1805	Angle	0	0	1	Day	Dry	No Contributing Action
14	0.104	02/18/13	Mon	2200	Angle	0	0	1	Night	Dry	Ran Red Light
15	0.104	09/21/13	Sat	1230	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
16	0.104	07/07/13	Sun	1150	Angle	0	1	0	Day	Dry	Ran Red Light
17	0.104	11/19/13	Tue	0910	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
18	0.104	07/22/13	Mon	0520	Concrete Traffic Barrier	0	0	1	Night	Dry	#N/A
19	0.104	12/11/13	Wed	2140	Rear-End	0	0	1	Night	Dry	No Contributing Action

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
19	0	2	17	8	0	5	0	0	3	0	0	0	2	0	1
Percent	0.00%	10.53%	89.47%	42.11%	0.00%	26.32%	0.00%	0.00%	15.79%	0.00%	0.00%	0.00%	10.53%	0.00%	5.26%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	12	7	4	15	9	1	0	3	0	0	0	0	0	1	0
Percent	63.16%	36.84%	21.05%	78.95%	47.37%	5.26%	0.00%	15.79%	0.00%	0.00%	0.00%	0.00%	0.00%	5.26%	0.00%

TOTAL ENTERING VEHICLES/ADT: **123,500** SPOT CRASH RATE: **0.421** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: N Miami Avenue M.P. 1.028 TO 1.081 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	1.028	02/12/14	Wed	2108	Sideswipe	0	0	1	Night	Wet	#N/A
2	1.028	10/15/14	Wed	1900	Rear-End	0	0	1	Night	Dry	#N/A
3	1.036	02/22/14	Sat	1218	Impact Attenuator/Crash Cushion	0	1	0	Day	Dry	Other Contributing Action
4	1.038	07/07/14	Mon	0900	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
5	1.038	09/18/14	Thu	1900	Sideswipe	0	2	0	Day	Wet	Other Contributing Action
6	1.038	09/14/14	Sun	1618	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
7	1.040	09/04/14	Thu	1800	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
8	1.046	05/08/14	Thu	0654	Other Non-Collision	0	0	1	Day	Dry	Over-Correcting/Over-Steering
9	1.081	02/08/14	Sat	2047	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
10	1.081	02/16/14	Sun	1419	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
11	1.081	12/03/14	Wed	1944	Sideswipe	0	0	1	Night	Dry	Other Contributing Action
12	1.091	11/21/14	Fri	1237	Concrete Traffic Barrier	0	1	0	Day	Wet	Over-Correcting/Over-Steering
13	0.000	07/19/14	Sat	2225	Angle	0	0	1	Night	Dry	No Contributing Action
14	0.000	11/14/14	Fri	1045	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
15	0.000	10/26/14	Sun	0249	Angle	0	0	1	Night	Dry	No Contributing Action
16	0.000	02/02/14	Sun	1045	Rear-End	0	0	1	Day	Wet	#N/A
17	0.000	12/05/14	Fri	0027	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
18	0.009	12/05/14	Fri	1105	Sideswipe	0	0	1	Day	Dry	No Contributing Action
19	0.047	04/04/14	Fri	0859	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
20	0.066	01/18/14	Sat	2000	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
21	0.085	02/16/14	Sun	1620	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
22	0.099	10/03/14	Fri	2238	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
23	0.102	08/01/14	Fri	1535	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
24	0.104	02/03/14	Mon	1500	Rear-End	0	0	1	Day	Dry	Other Contributing Action
25	0.104	08/04/14	Mon	1107	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
26	0.104	02/12/14	Wed	0745	Rear-End	0	0	1	Day	Dry	Followed too Closely
27	0.104	03/26/14	Wed	1330	Sideswipe	0	0	1	Day	Dry	Not Coded
28	0.104	03/23/14	Sun	0652	Not Coded	0	0	1	Night	Dry	No Contributing Action
29	0.104	11/25/14	Tue	1411	Rear-End	0	0	1	Day	Dry	Other Contributing Action

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
29	0	7	22	17	0	2	0	0	6	0	0	0	2	0	1
Percent	0.00%	24.14%	75.86%	58.62%	0.00%	6.90%	0.00%	0.00%	20.69%	0.00%	0.00%	0.00%	6.90%	0.00%	3.45%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	20	9	4	25	9	1	0	3	0	0	0	0	0	2	0
Percent	68.97%	31.03%	13.79%	86.21%	31.03%	3.45%	0.00%	10.34%	0.00%	0.00%	0.00%	0.00%	0.00%	6.90%	0.00%

TOTAL ENTERING VEHICLES/ADT: **123,500** SPOT CRASH RATE: **0.421** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: **87030000** STATE ROUTE: **5**
 INTERSECTING ROADWAY: **NE 36 Street** M.P. **13.440** TO **13.550** ENGINEER: **FDOT D6**
 STUDY PERIOD: FROM **1/ 2011** TO **12/ 2011** COUNTY: **Miami-Dade**

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
7	13.449	04/18/11	Mon	1214	Overturn/Rollover	0	0	1	Day	Dry	Careless or Negligent Manner
8	13.449	05/01/11	Sun	1350	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
9	13.449	05/21/11	Sat	1530	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
10	13.449	06/18/11	Sat	1900	Angle	0	0	1	Day	Wet	Failed to Yield Right-Of-Way
11	13.449	08/31/11	Wed	0824	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
12	13.449	09/02/11	Fri	1540	Left-Turn	0	2	0	Day	Wet	Failed to Yield Right-Of-Way
13	13.449	09/08/11	Thu	1100	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
14	13.449	09/16/11	Fri	0930	Left-Turn	0	2	0	Day	Dry	Failed to Yield Right-Of-Way
15	13.449	09/21/11	Wed	1650	Rear-End	0	0	1	Day	Dry	No Contributing Action
16	13.449	09/30/11	Fri	0800	Not Coded	0	0	1	Day	Dry	No Contributing Action
17	13.449	10/07/11	Fri	1300	Angle	0	1	0	Day	Dry	Careless or Negligent Manner
18	13.449	10/31/11	Mon	1730	Angle	0	0	1	Day	Wet	No Contributing Action
19	13.449	12/02/11	Fri	1645	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
20	13.449	12/03/11	Sat	1905	Angle	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
21	13.449	12/08/11	Thu	1450	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
22	13.449	12/12/11	Mon	0720	Left-Turn	0	2	0	Day	Dry	Failed to Yield Right-Of-Way
167	13.466	12/10/11	Sat	0040	Pedestrian	0	1	0	Night	Wet	No Contributing Action
180	13.480	03/30/11	Wed	0800	Not Coded	0	0	1	Day	Dry	Other Contributing Action
181	13.480	05/14/11	Sat	1300	Angle	0	0	1	Day	Dry	Other Contributing Action
182	13.480	05/20/11	Fri	1340	Not Coded	0	1	0	Day	Dry	Other Contributing Action
183	13.480	06/01/11	Wed	0135	Angle	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
184	13.480	06/04/11	Sat	2130	Not Coded	0	2	0	Night	Dry	Other Contributing Action
185	13.480	06/09/11	Thu	1830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
186	13.480	07/12/11	Tue	1720	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
187	13.480	07/26/11	Tue	1400	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
188	13.480	08/09/11	Tue	1610	Pedestrian	0	1	0	Day	Wet	Ran Red Light
189	13.480	08/10/11	Wed	1700	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
190	13.480	09/20/11	Tue	1007	Not Coded	0	1	0	Day	Wet	Failed to Yield Right-Of-Way
191	13.480	10/19/11	Wed	1145	Sideswipe	0	0	1	Day	Wet	Disregarded other Road Markings
192	13.480	10/25/11	Tue	0700	Left-Turn	0	0	1	Day	Dry	Improper Passing
193	13.480	11/01/11	Tue	1800	Sideswipe	0	0	1	Night	Wet	Failed To Keep In Proper Lane
194	13.480	11/16/11	Wed	0933	Angle	0	0	1	Day	Dry	No Contributing Action
195	13.480	11/17/11	Thu	1600	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
196	13.480	11/18/11	Fri	1030	Angle	0	0	1	Day	Wet	Careless or Negligent Manner
197	13.480	11/22/11	Tue	2250	Angle	0	2	0	Night	Dry	No Contributing Action
198	13.480	12/12/11	Mon	2200	Angle	0	0	1	Night	Wet	No Contributing Action
199	13.480	12/28/11	Wed	1158	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
322	13.488	09/02/11	Fri	0840	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
325	13.489	11/18/11	Fri	0755	Not Coded	0	0	1	Day	Wet	Failed to Yield Right-Of-Way
354	13.508	09/29/11	Thu	1255	Pedestrian	0	0	1	Day	Dry	No Contributing Action
377	13.541	09/24/11	Sat	0119	Rear-End	0	0	1	Night	Dry	Followed too Closely
378	13.541	11/05/11	Sat	1930	Angle	0	0	1	Night	Dry	Other Contributing Action
379	13.541	12/03/11	Sat	1642	Rear-End	0	0	1	Day	Dry	Other Contributing Action
391	19.627	08/11/11	Thu	2327	Not Coded	0	0	1	Night	Dry	Careless or Negligent Manner
394	19.637	08/18/11	Thu	0000	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
395	19.637	08/19/11	Fri	1141	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
397	19.638	09/26/11	Mon	1415	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
400	19.646	01/28/11	Fri	1515	Rear-End	0	0	1	Day	Dry	Followed too Closely
405	19.655	09/19/11	Mon	1420	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
406	19.655	11/18/11	Fri	1601	Angle	0	0	1	Day	Wet	Failed to Yield Right-Of-Way
407	19.655	12/24/11	Sat	1400	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
439	19.674	04/26/11	Tue	1354	Rear-End	0	0	1	Day	Dry	Followed too Closely
440	19.674	05/22/11	Sun	0325	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
441	19.674	09/05/11	Mon	1830	Not Coded	0	0	1	Day	Dry	Swerved Or Avoided
442	19.674	10/15/11	Sat	2351	Angle	0	0	1	Night	Dry	Improper Turn
443	19.674	10/25/11	Tue	0712	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
444	19.674	10/27/11	Thu	0120	Not Coded	0	0	1	Night	Dry	Followed too Closely
445	19.674	11/08/11	Tue	0000	Left-Turn	0	0	1	Day	Dry	No Contributing Action
446	19.674	12/04/11	Sun	1600	Angle	0	0	1	Day	Dry	No Contributing Action
447	19.674	12/09/11	Fri	0810	Rear-End	0	3	0	Day	Wet	Careless or Negligent Manner
448	19.674	12/15/11	Thu	2130	Angle	0	0	1	Night	Dry	Careless or Negligent Manner
493	19.683	06/12/11	Sun	1500	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
494	19.683	07/20/11	Wed	1440	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
506	19.688	07/11/11	Mon	0909	Rear-End	0	0	1	Day	Dry	Failed To Keep In Proper Lane
507	19.688	12/10/11	Sat	2355	Sideswipe	0	1	0	Day	Dry	Other Contributing Action

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
65	0	13	52	16	0	21	6	0	5	0	3	0	0	0	13
Percent	0.00%	20.00%	80.00%	24.62%	0.00%	32.31%	9.23%	0.00%	7.69%	0.00%	4.62%	0.00%	0.00%	0.00%	20.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	51	14	13	52	18	18	1	1	0	1	1	0	0	0	0
Percent	78.46%	21.54%	20.00%	80.00%	27.69%	27.69%	1.54%	1.54%	0.00%	1.54%	1.54%	0.00%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **55,519** SPOT CRASH RATE: **3.208** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87030000 STATE ROUTE: 5											
INTERSECTING ROADWAY: NE 36 Street				M.P. 13.440 TO 13.550				ENGINEER: FDOT D6			
STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012				COUNTY: Miami-Dade							
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
97	13.461	09/13/12	Thu	0630	Sideswipe	0	0	1	Night	Dry	Other Contributing Action
104	13.471	10/26/12	Fri	1940	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
107	13.476	12/30/12	Sun	0800	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
110	13.477	11/21/12	Wed	1855	Not Coded	0	0	1	Night	Dry	Other Contributing Action
135	13.480	02/06/12	Mon	0930	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
136	13.480	02/14/12	Tue	1710	Rear-End	0	1	0	Day	Dry	No Contributing Action
137	13.480	03/16/12	Fri	1625	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
138	13.480	03/21/12	Wed	1700	Not Coded	0	0	1	Day	Dry	Other Contributing Action
139	13.480	04/02/12	Mon	1500	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
140	13.480	04/03/12	Tue	0531	Rear-End	0	0	1	Night	Dry	No Contributing Action
141	13.480	04/21/12	Sat	0615	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
142	13.480	04/22/12	Sun	1700	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
143	13.480	05/03/12	Thu	0700	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
144	13.480	05/24/12	Thu	2300	Sideswipe	0	0	1	Night	Dry	Improper Turn
145	13.480	05/29/12	Tue	0722	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
146	13.480	05/30/12	Wed	1630	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
147	13.480	06/04/12	Mon	1400	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
148	13.480	07/18/12	Wed	0844	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
149	13.480	08/08/12	Wed	0230	Not Coded	0	0	1	Night	Dry	Improper Turn
150	13.480	08/09/12	Thu	1900	Rear-End	0	0	1	Day	Dry	No Contributing Action
151	13.480	08/22/12	Wed	1300	Left-Turn	0	0	1	Day	Dry	No Contributing Action
152	13.480	08/25/12	Sat	1538	Not Coded	0	0	1	Day	Wet	Careless or Negligent Manner
153	13.480	09/08/12	Sat	2230	Angle	0	0	1	Night	Dry	Careless or Negligent Manner
154	13.480	09/17/12	Mon	1926	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
155	13.480	09/23/12	Sun	0918	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
156	13.480	10/19/12	Fri	1730	Angle	0	0	1	Day	Dry	No Contributing Action
157	13.480	10/19/12	Fri	0530	Angle	0	0	1	Day	Dry	No Contributing Action
158	13.480	10/22/12	Mon	1000	Rear-End	0	0	1	Day	Dry	No Contributing Action
159	13.480	10/31/12	Wed	0720	Overturn/Rollover	0	0	1	Day	Dry	No Contributing Action
160	13.480	11/09/12	Fri	1825	Rear-End	0	0	1	Night	Dry	No Contributing Action
161	13.480	11/14/12	Wed	0817	Sideswipe	0	0	1	Day	Wet	Failed To Keep In Proper Lane
162	13.480	11/17/12	Sat	1202	Angle	0	2	0	Day	Dry	No Contributing Action
163	13.480	11/30/12	Fri	2100	Angle	0	0	1	Night	Dry	Improper Passing
164	13.480	12/07/12	Fri	0912	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
165	13.480	12/07/12	Fri	1615	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
271	13.490	11/09/12	Fri	1249	Rear-End	0	0	1	Day	Dry	No Contributing Action
277	13.499	04/27/12	Fri	1737	Pedestrian	0	1	0	Day	Dry	No Contributing Action
278	13.499	05/12/12	Sat	1030	Sideswipe	0	0	1	Day	Dry	No Contributing Action
286	13.500	04/28/12	Sat	0330	Not Coded	0	0	1	Day	Wet	Careless or Negligent Manner
288	13.506	03/22/12	Thu	2300	Not Coded	0	0	1	Night	Dry	Followed too Closely
301	13.518	01/29/12	Sun	2000	Angle	0	2	0	Night	Wet	Other Contributing Action
302	13.518	12/17/12	Mon	0959	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
311	13.539	02/02/12	Thu	0755	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
315	13.541	08/18/12	Sat	1700	Rear-End	0	3	0	Day	Wet	Other Contributing Action
322	13.548	05/24/12	Thu	0539	Rear-End	0	0	1	Day	Dry	Other Contributing Action
328	19.636	04/12/12	Thu	2100	Angle	0	0	1	Night	Dry	No Contributing Action
333	19.640	01/26/12	Thu	1855	Rear-End	0	0	1	Night	Dry	No Contributing Action
339	19.650	02/08/12	Wed	1421	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
372	19.673	09/29/12	Sat	1000	Not Coded	0	0	1	Day	Dry	No Contributing Action
384	19.674	02/01/12	Wed	1230	Left-Turn	0	0	1	Day	Dry	Improper Turn
385	19.674	02/20/12	Mon	1227	Sideswipe	0	0	1	Day	Dry	Improper Passing
386	19.674	02/27/12	Mon	0510	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
387	19.674	03/14/12	Wed	1450	Angle	0	0	1	Day	Dry	Other Contributing Action
388	19.674	03/26/12	Mon	0710	Rear-End	0	0	1	Day	Dry	Other Contributing Action
389	19.674	06/13/12	Wed	1505	Not Coded	0	0	1	Day	Dry	Other Contributing Action
390	19.674	06/14/12	Thu	1630	Rear-End	0	0	1	Day	Dry	No Contributing Action
391	19.674	06/22/12	Fri	1005	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
392	19.674	07/13/12	Fri	0730	Rear-End	0	3	0	Day	Wet	Other Contributing Action
393	19.674	08/20/12	Mon	2213	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane
394	19.674	09/04/12	Tue	1254	Sideswipe	0	0	1	Day	Dry	No Contributing Action
395	19.674	09/05/12	Wed	0745	Rear-End	0	1	0	Day	Wet	Other Contributing Action
396	19.674	09/05/12	Wed	1351	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
397	19.674	10/01/12	Mon	1200	Rear-End	0	0	1	Day	Wet	Other Contributing Action
398	19.674	10/05/12	Fri	1050	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
399	19.674	12/07/12	Fri	1035	Not Coded	0	0	1	Day	Dry	No Contributing Action
400	19.674	12/21/12	Fri	2136	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
401	19.674	10/25/12	Thu	1650	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
424	19.677	03/30/12	Fri	1320	Rear-End	0	0	1	Day	Dry	No Contributing Action
430	19.683	03/06/12	Tue	1045	Not Coded	0	1	0	Day	Dry	Other Contributing Action
444	19.690	03/02/12	Fri	1240	Sideswipe	0	2	0	Day	Dry	Failed To Keep In Proper Lane
445	19.693	11/04/12	Sun	0103	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
446	19.693	11/19/12	Mon	1930	Angle	0	0	1	Night	Dry	Followed too Closely
452	19.703	11/26/12	Mon	1459	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
73	0	12	61	27	0	15	2	0	15	0	1	0	0	0	0
Percent	0.00%	16.44%	83.56%	36.99%	0.00%	20.55%	2.74%	0.00%	20.55%	0.00%	1.37%	0.00%	1.37%	0.00%	17.81%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	53	20	10	63	20	19	4	0	0	0	0	0	0	0	0
Percent	72.60%	27.40%	13.70%	86.30%	27.40%	26.03%	5.48%	0.00%	0.00%	2.74%	0.00%	0.00%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: 60,398				SPOT CRASH RATE: 4.355 CRASHES PER MILLION ENTERING VEHICLES			
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**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: **87030000** STATE ROUTE: **5**
 INTERSECTING ROADWAY: **NE 36 Street** M.P. **13.440** TO **13.550** ENGINEER: **FDOT D6**
 STUDY PERIOD: FROM **1/ 2013** TO **12/ 2013** COUNTY: **Miami-Dade**

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
70	13.480	05/21/13	Tue	1432	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
71	13.480	05/31/13	Fri	1557	Angle	0	0	1	Day	Dry	Disregarded other Road Markings
72	13.480	06/11/13	Tue	0815	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
73	13.480	06/18/13	Tue	1934	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
74	13.480	06/29/13	Sat	1725	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
75	13.480	08/15/13	Thu	0010	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
76	13.480	08/28/13	Wed	0940	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
77	13.480	09/13/13	Fri	1711	Rear-End	0	0	1	Day	Dry	No Contributing Action
78	13.480	01/10/13	Thu	1225	Angle	0	0	1	Day	Dry	Other Contributing Action
79	13.480	02/12/13	Tue	0910	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
80	13.480	03/08/13	Fri	1236	Rear-End	0	0	1	Day	Dry	Improper Passing
81	13.480	04/03/13	Wed	0403	Left-Turn	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
82	13.480	04/15/13	Mon	1803	Left-Turn	0	0	1	Day	Wet	Ran Red Light
83	13.480	11/07/13	Thu	0835	Not Coded	0	0	1	Day	Dry	No Contributing Action
84	13.480	10/26/13	Sat	1931	Left-Turn	0	3	0	Night	Dry	Failed to Yield Right-Of-Way
85	13.480	11/15/13	Fri	1421	Pedestrian	0	2	0	Day	Wet	Failed to Yield Right-Of-Way
86	13.480	12/02/13	Mon	1308	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
87	13.480	12/02/13	Mon	1724	Angle	0	0	1	Night	Dry	Careless or Negligent Manner
88	13.480	12/05/13	Thu	2245	Sideswipe	0	0	1	Night	Dry	Improper Passing
89	13.480	12/30/13	Mon	1200	Rear-End	0	1	0	Day	Dry	Followed too Closely
142	13.481	04/11/13	Thu	1305	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
143	13.482	06/17/13	Mon	1700	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
152	13.485	10/30/13	Wed	1200	Sideswipe	0	0	1	Day	Dry	No Contributing Action
153	13.485	12/14/13	Sat	1130	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
156	13.486	11/08/13	Fri	1010	Rear-End	0	0	1	Day	Wet	Improper Passing
159	13.487	12/27/13	Fri	2045	Sideswipe	0	0	1	Night	Wet	Improper Turn
162	13.488	08/28/13	Wed	1020	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
165	13.489	10/05/13	Sat	1140	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
166	13.489	12/28/13	Sat	1953	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
179	13.495	03/15/13	Fri	1900	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
180	13.497	01/25/13	Fri	1630	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
183	13.499	02/12/13	Tue	0915	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
194	13.508	10/10/13	Thu	1135	Angle	0	0	1	Day	Dry	Other Contributing Action
195	13.508	11/06/13	Wed	0655	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
203	13.513	06/14/13	Fri	0934	Rear-End	0	1	0	Day	Dry	#N/A
212	13.537	04/26/13	Fri	0825	Rear-End	0	1	0	Day	Dry	Followed too Closely
220	13.541	02/08/13	Fri	1530	Rear-End	0	0	1	Day	Wet	Followed too Closely
247	19.655	08/15/13	Thu	0815	Angle	0	0	1	Day	Wet	Failed to Yield Right-Of-Way
248	19.655	02/02/13	Sat	1440	Left-Turn	0	0	1	Day	Dry	Careless or Negligent Manner
249	19.655	11/21/13	Thu	1300	Rear-End	0	0	1	Day	Dry	Improper Passing
257	19.665	04/10/13	Wed	1518	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
258	19.665	05/01/13	Wed	0935	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
259	19.665	11/20/13	Wed	2009	Pedestrian	0	2	0	Night	Wet	No Contributing Action
265	19.668	01/05/13	Sat	1624	Sideswipe	0	0	1	Day	Dry	No Contributing Action
266	19.668	09/28/13	Sat	2230	Angle	0	0	1	Night	Wet	Failed to Yield Right-Of-Way
306	19.674	08/02/13	Fri	1632	Angle	0	1	0	Day	Dry	Failed to Yield Right-Of-Way
307	19.674	08/09/13	Fri	1935	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
308	19.674	02/11/13	Mon	0910	Angle	0	0	1	Day	Dry	Improper Turn
309	19.674	03/13/13	Wed	0850	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
310	19.674	03/11/13	Mon	1800	Right-Turn	0	1	0	Day	Dry	Erratic, Reckless or Aggressive
311	19.674	04/19/13	Fri	0615	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
312	19.674	04/22/13	Mon	0642	Left-Turn	0	2	0	Night	Wet	Failed to Yield Right-Of-Way
313	19.674	10/23/13	Wed	1740	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
314	19.674	11/23/13	Sat	1420	Not Coded	0	1	0	Day	Wet	No Contributing Action
327	19.676	10/20/13	Sun	0429	Rear-End	0	0	1	Night	Dry	Followed too Closely
335	19.683	07/15/13	Mon	1125	Rear-End	0	0	1	Day	Dry	Other Contributing Action
336	19.683	05/10/13	Fri	1508	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
347	19.688	04/21/13	Sun	2140	Angle	0	0	1	Night	Dry	Improper Turn
351	19.693	04/01/13	Mon	1835	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
357	19.704	06/11/13	Tue	1954	Angle	0	1	0	Night	Dry	Careless or Negligent Manner
358	19.731	08/27/13	Tue	0207	Not Coded	0	2	0	Day	Wet	Other Contributing Action

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
61	0	15	46	26	0	13	5	1	10	0	2	0	0	0	7
Percent	0.00%	24.59%	75.41%	42.62%	0.00%	21.31%	8.20%	1.64%	16.39%	0.00%	3.28%	0.00%	0.00%	0.00%	11.48%
Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	46	15	15	46	25	24	5	1	0	4	1	1	0	0	0
Percent	75.41%	24.59%	24.59%	75.41%	40.98%	39.34%	8.20%	1.64%	0.00%	6.56%	1.64%	1.64%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **57,093** SPOT CRASH RATE: **4.319** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: **87030000** STATE ROUTE: **5**
 INTERSECTING ROADWAY: **NE 36 Street** M.P. **13.440** TO **13.550** ENGINEER: **FDOT D6**
 STUDY PERIOD: FROM **1/ 2014** TO **12/ 2014** COUNTY: **Miami-Dade**

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)									
1	13.480	01/25/14	Sat	0030	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner									
2	13.480	02/08/14	Sat	0842	Angle	0	0	1	Day	Dry	No Contributing Action									
3	13.480	03/27/14	Thu	1545	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner									
4	13.480	04/13/14	Sun	1948	Rear-End	0	0	1	Night	Wet	Followed too Closely									
5	13.480	05/09/14	Fri	1335	Rear-End	0	1	0	Night	Dry	No Contributing Action									
6	13.480	06/05/14	Thu	1245	Rear-End	0	0	1	Day	Dry	No Contributing Action									
7	13.480	06/06/14	Fri	0020	Right-Turn	0	0	1	Night	Dry	Improper Turn									
8	13.480	08/05/14	Tue	1640	Sideswipe	0	0	1	Day	Dry	Improper Passing									
9	13.480	09/07/14	Sun	1450	Angle	0	0	1	Day	Dry	No Contributing Action									
10	13.480	01/03/14	Fri	1622	Rear-End	0	0	1	Day	Dry	Failed to Yield Right-Of-Way									
11	13.480	03/01/14	Sat	1147	Sideswipe	0	0	1	Day	Dry	No Contributing Action									
12	13.480	03/17/14	Mon	1920	Right-Turn	0	0	1	Night	Dry	Careless or Negligent Manner									
13	13.480	03/30/14	Sun	0937	Left-Turn	0	1	0	Day	Dry	Careless or Negligent Manner									
14	13.480	03/30/14	Sun	1940	Sideswipe	0	0	1	Night	Dry	No Contributing Action									
15	13.480	04/24/14	Thu	1208	Angle	0	0	1	Day	Dry	Careless or Negligent Manner									
16	13.480	04/26/14	Sat	1815	Not Coded	0	0	1	Day	Dry	Improper Turn									
17	13.480	05/01/14	Thu	1905	Rear-End	0	1	0	Day	Dry	Followed too Closely									
18	13.480	06/08/14	Sun	0504	Angle	0	0	1	Night	Dry	Ran Red Light									
19	13.480	07/05/14	Sat	2016	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner									
20	13.480	10/22/14	Wed	1441	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner									
21	13.480	10/26/14	Sun	2300	Rear-End	0	0	1	Night	Dry	Followed too Closely									
22	13.480	10/28/14	Tue	1540	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner									
23	13.480	11/21/14	Fri	1830	Rear-End	0	0	1	Night	Dry	Followed too Closely									
24	13.480	11/14/14	Fri	2109	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner									
25	13.480	12/09/14	Tue	0728	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner									
26	13.480	12/14/14	Sun	1500	Rear-End	0	0	1	Day	Dry	Followed too Closely									
54	13.482	10/03/14	Fri	0910	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner									
55	13.482	10/22/14	Wed	1615	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way									
57	13.484	01/05/14	Sun	0158	Sideswipe	0	0	1	Night	Dry	Improper Turn									
58	13.484	06/10/14	Tue	2130	Not Coded	0	0	1	Night	Dry	Failed to Yield Right-Of-Way									
64	13.485	09/03/14	Wed	0750	Not Coded	0	0	1	Day	Dry	No Contributing Action									
67	13.486	11/17/14	Mon	0830	Angle	0	0	1	Night	Dry	Other Contributing Action									
70	13.487	12/09/14	Tue	1928	Angle	0	0	1	Night	Dry	No Contributing Action									
77	13.489	07/29/14	Tue	1715	Not Coded	0	2	0	Day	Dry	No Contributing Action									
78	13.489	11/19/14	Wed	0550	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner									
79	13.489	12/04/14	Thu	1820	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner									
86	13.491	12/03/14	Wed	2130	Rear-End	0	2	0	Night	Dry	Other Contributing Action									
101	13.500	11/04/14	Tue	0840	Rear-End	0	0	1	Day	Dry	Followed too Closely									
106	13.508	03/02/14	Sun	0745	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner									
107	13.508	10/19/14	Sun	0330	Not Coded	0	0	1	Night	Dry	Other Contributing Action									
108	13.508	04/02/14	Wed	1045	Rear-End	0	0	1	Day	Dry	Not Coded									
117	13.518	11/05/14	Wed	1155	Sideswipe	0	1	0	Day	Wet	No Contributing Action									
121	13.522	10/25/14	Sat	1445	Rear-End	0	0	1	Day	Dry	No Contributing Action									
123	13.538	08/10/14	Sun	1805	Rear-End	0	0	1	Day	Dry	Followed too Closely									
131	13.541	08/12/14	Tue	1620	Sideswipe	0	0	1	Day	Dry	No Contributing Action									
132	13.541	03/01/14	Sat	1620	Left-Turn	0	0	1	Day	Dry	Careless or Negligent Manner									
135	13.544	02/17/14	Mon	1151	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner									
137	13.550	11/04/14	Tue	1650	Rear-End	0	0	1	Day	Dry	Failed To Keep In Proper Lane									
138	19.599	02/18/14	Tue	1930	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner									
145	19.637	06/13/14	Fri	1923	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way									
148	19.640	04/11/14	Fri	0815	Rear-End	0	0	1	Day	Dry	Improper Turn									
160	19.655	02/22/14	Sat	0610	Not Coded	0	0	1	Day	Dry	Not Coded									
161	19.655	12/29/14	Mon	1755	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane									
165	19.659	05/18/14	Sun	1745	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner									
170	19.665	03/21/14	Fri	1210	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner									
183	19.672	07/01/14	Tue	1650	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner									
184	19.672	05/08/14	Thu	0900	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner									
225	19.674	04/13/14	Sun	2345	Left-Turn	0	0	1	Day	Dry	Failed to Yield Right-Of-Way									
226	19.674	06/09/14	Mon	1346	Sideswipe	0	0	1	Day	Dry	Other Contributing Action									
227	19.674	06/27/14	Fri	1830	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner									
228	19.674	07/07/14	Mon	0939	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner									
229	19.674	07/17/14	Thu	1335	Rear-End	0	0	1	Day	Wet	No Contributing Action									
230	19.674	10/11/14	Sat	0839	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner									
231	19.674	12/22/14	Mon	0918	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner									
232	19.674	12/02/14	Tue	1820	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner									
247	19.683	03/27/14	Thu	1545	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner									
248	19.683	07/14/14	Mon	0810	Rear-End	0	0	1	Day	Dry	Other Contributing Action									
249	19.683	11/03/14	Mon	0745	Angle	0	0	1	Day	Dry	Careless or Negligent Manner									
262	19.702	03/27/14	Thu	1132	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner									
263	19.702	05/04/14	Sun	1222	Rear-End	0	0	1	Day	Dry	Improper Backing									
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other					
70	0	11	59	35	0	9	3	2	12	0	0	0	0	0	7					
Percent	0.00%	15.71%	84.29%	50.00%	0.00%	12.86%	4.29%	2.86%	17.14%	0.00%	0.00%	0.00%	0.00%	0.00%	10.00%					
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way					
Total	50	20	4	66	25	24	5	1	0	4	1	1	0	1	0					
Percent	71.43%	28.57%	5.71%	94.29%	35.71%	34.29%	7.14%	1.43%	0.00%	5.71%	1.43%	1.43%	0.00%	1.43%	0.00%					
TOTAL ENTERING VEHICLES/ADT:						57,093	SPOT CRASH RATE:									4.319	CRASHES PER MILLION ENTERING VEHICLES			

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87030000 STATE ROUTE: 5
 INTERSECTING ROADWAY: NE 36 Street M.P. 13.440 TO 13.550 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
3	13.518	05/06/15	Wed	2050	Angle	0	1	0	Night	Dry	No Contributing Action
4	13.518	06/27/15	Sat	1715	Pedestrian	0	1	0	Day	Dry	No Contributing Action
5	13.518	08/09/15	Sun	0055	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
9	13.538	12/03/15	Thu	0830	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
18	13.541	02/03/15	Tue	1205	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
19	13.541	02/22/15	Sun	1455	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
24	19.599	11/22/15	Sun	2305	Angle	0	0	1	Night	Wet	Careless or Negligent Manner
26	19.627	10/14/15	Wed	0745	Not Coded	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
35	19.646	03/13/15	Fri	0940	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
36	19.646	06/15/15	Mon	1700	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
37	19.646	11/21/15	Sat	1912	Rear-End	0	0	1	Day	Wet	Improper Passing
47	19.655	06/19/15	Fri	2245	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
48	19.655	07/21/15	Tue	0855	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
49	19.657	07/16/15	Thu	1950	Not Coded	0	0	1	Night	Dry	Not Coded
51	19.660	07/08/15	Wed	0900	Parked Motor Vehicle	0	0	1	Day	Dry	No Contributing Action
56	19.665	06/13/15	Sat	2315	Sideswipe	0	1	0	Night	Dry	Other Contributing Action
57	19.665	11/08/15	Sun	0057	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
58	19.665	12/03/15	Thu	1950	Angle	0	2	0	Night	Wet	Improper Passing
59	19.666	12/18/15	Fri	1609	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
62	19.668	02/04/15	Wed	1700	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
63	19.668	07/06/15	Mon	0530	Angle	0	0	1	Night	Dry	Not Coded
64	19.668	11/24/15	Tue	1910	Rear-End	0	0	1	Night	Dry	No Contributing Action
65	19.670	05/25/15	Mon	1230	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
66	19.670	11/12/15	Thu	1800	Sideswipe	0	0	1	Night	Dry	No Contributing Action
67	19.671	09/26/15	Sat	2339	Rear-End	0	0	1	Night	Wet	Careless or Negligent Manner
70	19.672	08/02/15	Sun	1945	Sideswipe	0	0	1	Night	Dry	Careless or Negligent Manner
72	19.673	01/21/15	Wed	1747	Not Coded	0	0	1	Night	Dry	Improper Passing
118	19.674	01/04/15	Sun	0240	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
119	19.674	01/23/15	Fri	1028	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
120	19.674	10/03/15	Sat	1930	Angle	0	0	1	Night	Dry	Failed To Keep In Proper Lane
121	19.674	12/08/15	Tue	1900	Left-Turn	0	0	1	Night	Wet	Failed to Yield Right-Of-Way
124	19.677	02/06/15	Fri	0815	Rear-End	0	0	1	Day	Dry	Other Contributing Action
125	19.678	11/27/15	Fri	1900	Rear-End	0	0	1	Night	Dry	Not Coded
126	19.679	05/22/15	Fri	1600	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
135	19.683	03/04/15	Wed	1300	Pedestrian	0	1	0	Day	Dry	No Contributing Action
136	19.683	05/14/15	Thu	1400	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
137	19.683	05/16/15	Sat	1740	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
138	19.683	12/02/15	Wed	1404	Angle	0	0	1	Day	Wet	Improper Passing
139	19.687	11/19/15	Thu	0814	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
149	19.702	10/26/15	Mon	1151	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
150	19.702	12/15/15	Tue	0723	Angle	0	0	1	Day	Dry	No Contributing Action

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
41	0	5	36	13	0	10	1	0	10	0	2	1	0	0	7
Percent	0.00%	12.20%	87.80%	31.71%	0.00%	24.39%	2.44%	0.00%	24.39%	0.00%	4.88%	2.44%	0.00%	0.00%	17.07%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	23	18	6	35	25	24	5	1	0	4	1	1	0	2	0
Percent	56.10%	43.90%	14.63%	85.37%	60.98%	58.54%	12.20%	2.44%	0.00%	9.76%	2.44%	2.44%	0.00%	4.88%	0.00%

TOTAL ENTERING VEHICLES/ADT: **57,093** SPOT CRASH RATE: **4.319 CRASHES PER MILLION ENTERING VEHICLES**

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87090000												STATE ROUTE: 25							
INTERSECTING ROADWAY: NE 5 Avenue												M.P. 19.731		TO 19.843		ENGINEER: FDOT D6			
STUDY PERIOD: FROM 1/ 2011												TO 12/ 2011		COUNTY: Miami-Dade					
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)						
1	19.735	09/11/11	Sun	1425	Sideswipe			0	0	1	Day	Dry	Other Contributing Action						
2	19.794	07/22/11	Fri	0840	Angle			0	0	1	Day	Dry	Careless or Negligent Manner						
3	19.794	08/31/11	Wed	1445	Backed Into			0	0	1	Day	Dry	No Contributing Action						
4	19.794	09/02/11	Fri	1105	Overturn/Rollover			0	0	1	Day	Dry	Other Contributing Action						
5	19.794	09/08/11	Thu	1943	Sideswipe			0	0	1	Night	Wet	Improper Passing						
6	19.794	12/21/11	Wed	0920	Not Coded			0	0	1	Day	Dry	Other Contributing Action						
7	19.798	11/10/11	Thu	0300	Rear-End			0	0	1	Night	Dry	Careless or Negligent Manner						
8	19.803	10/07/11	Fri	0904	Sideswipe			0	0	1	Day	Dry	Other Contributing Action						
9	19.809	10/17/11	Mon	1444	Sideswipe			0	0	1	Day	Wet	Careless or Negligent Manner						
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
9	0	0	9	1	0	1	0	0	4	1	0	0	0	0	1				
Percent	0.00%	0.00%	100.00%	11.11%	0.00%	11.11%	0.00%	0.00%	44.44%	11.11%	0.00%	0.00%	0.00%	0.00%	11.11%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	7	2	2	7	3	0	0	0	0	1	0	0	0	0	0				
Percent	77.78%	22.22%	22.22%	77.78%	33.33%	0.00%	0.00%	0.00%	0.00%	11.11%	0.00%	0.00%	0.00%	0.00%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 21,500								SPOT CRASH RATE: 1.147 CRASHES PER MILLION ENTERING VEHICLES											

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87090000 STATE ROUTE: 25
 INTERSECTING ROADWAY: NE 5 Avenue M.P. 19.731 TO 19.843 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	19.765	02/03/12	Fri	1506	Sideswipe	0	0	1	Day	Dry	Improper Passing
2	19.769	04/16/12	Mon	1145	Rear-End	0	0	1	Day	Dry	No Contributing Action
3	19.794	05/30/12	Wed	1815	Other Non-Collision	0	0	1	Day	Dry	#N/A
4	19.794	02/13/12	Mon	1530	Not Coded	0	0	1	Day	Dry	No Contributing Action
5	19.794	09/16/12	Sun	0845	Tree (Standing)	0	0	1	Night	Dry	Careless or Negligent Manner
6	19.794	12/07/12	Fri	1649	Angle	0	0	1	Day	Dry	Other Contributing Action
7	19.796	05/03/12	Thu	0107	Other Fixed Object	0	0	1	Night	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
7	0	0	7	1	0	1	0	0	1	0	0	0	2	0	1
Percent	0.00%	0.00%	100.00%	14.29%	0.00%	14.29%	0.00%	0.00%	14.29%	0.00%	0.00%	0.00%	28.57%	0.00%	14.29%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	5	2	0	7	2	0	0	0	0	1	0	0	0	1	0
Percent	71.43%	28.57%	0.00%	100.00%	28.57%	0.00%	0.00%	0.00%	0.00%	14.29%	0.00%	0.00%	0.00%	14.29%	0.00%

TOTAL ENTERING VEHICLES/ADT: **22,000** SPOT CRASH RATE: **0.872** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87090000													STATE ROUTE: 25						
INTERSECTING ROADWAY: NE 5 Avenue													M.P. 19.731		TO 19.843		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2013													TO 12/ 2013		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)					
1	19.746	04/14/13	Sun	1730	Other Fixed Object				0	0	1	Day	Dry	Other Contributing Action					
2	19.790	02/14/13	Thu	1100	Angle				0	0	1	Day	Dry	Improper Passing					
3	19.794	04/07/13	Sun	1745	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner					
4	19.813	03/21/13	Thu	1453	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner					
5	19.843	08/19/13	Mon	1830	Sideswipe				0	0	1	Day	Dry	Improper Turn					
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
5	0	0	5	2	0	1	0	0	1	0	0	0	1	0	0				
Percent	0.00%	0.00%	100.00%	40.00%	0.00%	20.00%	0.00%	0.00%	20.00%	0.00%	0.00%	0.00%	20.00%	0.00%	0.00%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	5	0	0	5	2	0	1	0	0	1	0	0	0	0	0				
Percent	100.00%	0.00%	0.00%	100.00%	40.00%	0.00%	20.00%	0.00%	0.00%	20.00%	0.00%	0.00%	0.00%	0.00%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 16,500									SPOT CRASH RATE: 0.830 CRASHES PER MILLION ENTERING VEHICLES										

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87090000												STATE ROUTE: 25						
INTERSECTING ROADWAY: NE 5 Avenue												M.P. 19.731		TO 19.843		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2014												TO 12/ 2014		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)				
1	19.752	07/13/14	Sun	1220	Not Coded				0	1	0	Night	Dry	Other Contributing Action				
2	19.771	08/12/14	Tue	1338	Sideswipe				0	0	1	Day	Dry	Improper Turn				
3	19.780	04/26/14	Sat	0540	Not Coded				0	0	1	Night	Dry	Careless or Negligent Manner				
4	19.780	08/20/14	Wed	2348	Not Coded				0	0	1	Night	Dry	Improper Passing				
5	19.780	11/21/14	Fri	0450	Rear-End				0	0	1	Night	Wet	Improper Turn				
6	19.789	08/07/14	Thu	1800	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner				
7	19.799	07/25/14	Fri	0313	Other Fixed Object				0	0	1	Night	Dry	Careless or Negligent Manner				
8	19.808	11/07/14	Fri	1552	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner				
9	19.818	01/31/14	Fri	1150	Angle				0	1	0	Day	Dry	Careless or Negligent Manner				
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other			
9	0	2	7	3	0	1	0	0	1	0	0	0	1	0	0			
Percent	0.00%	22.22%	77.78%	33.33%	0.00%	11.11%	0.00%	0.00%	11.11%	0.00%	0.00%	0.00%	11.11%	0.00%	0.00%			
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way			
Total	4	5	1	8	2	0	1	0	0	1	0	0	0	0	0			
Percent	44.44%	55.56%	11.11%	88.89%	22.22%	0.00%	11.11%	0.00%	0.00%	11.11%	0.00%	0.00%	0.00%	0.00%	0.00%			
TOTAL ENTERING VEHICLES/ADT: 16,500									SPOT CRASH RATE: 0.830 CRASHES PER MILLION ENTERING VEHICLES									

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87090000													STATE ROUTE: 25						
INTERSECTING ROADWAY: NE 5 Avenue													M.P. 19.731		TO 19.843		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2015													TO 12/ 2015		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)					
1	19.761	12/08/15	Tue	1100	Angle				0	0	1	Day	Wet	Careless or Negligent Manner					
2	19.774	12/22/15	Tue	1248	Rear-End				0	0	1	Day	Dry	Followed too Closely					
3	19.780	01/16/15	Fri	2323	Rear-End				0	0	1	Night	Dry	Followed too Closely					
4	19.780	01/17/15	Sat	1300	Rear-End				0	0	1	Day	Dry	Other Contributing Action					
5	19.780	11/05/15	Thu	2035	Rear-End				0	0	1	Night	Dry	Followed too Closely					
6	19.799	05/06/15	Wed	1817	Not Coded				0	0	1	Day	Dry	Not Coded					
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
6	0	0	6	4	0	1	0	0	0	0	0	0	1	0	0				
Percent	0.00%	0.00%	100.00%	66.67%	0.00%	16.67%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	16.67%	0.00%	0.00%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	4	2	1	5	2	0	1	0	0	1	0	0	0	0	0				
Percent	66.67%	33.33%	16.67%	83.33%	33.33%	0.00%	16.67%	0.00%	0.00%	16.67%	0.00%	0.00%	0.00%	0.00%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 16,500									SPOT CRASH RATE: 0.830 CRASHES PER MILLION ENTERING VEHICLES										

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: Exit Ramp To Alton Road M.P. 4.430 TO 4.480 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2011 TO 12/ 2011 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	4.430	01/31/11	Mon	1925	Angle	0	2	0	Night	Dry	Careless or Negligent Manner
2	4.430	02/26/11	Sat	1525	Pedestrian	0	1	0	Day	Dry	Careless or Negligent Manner
3	4.430	06/27/11	Mon	0440	Parked Motor Vehicle	0	0	1	Night	Dry	#N/A
4	4.480	02/03/11	Thu	1655	Rear-End	0	1	0	Night	Dry	Careless or Negligent Manner
5	4.480	08/21/11	Sun	1427	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
6	4.480	04/29/11	Fri	1853	Rear-End	0	2	0	Day	Wet	Followed too Closely
7	4.480	02/25/11	Fri	1545	Sideswipe	0	0	1	Day	Dry	#N/A
8	4.480	04/10/11	Sun	1650	Struck by Falling/Shifting Cargo	0	1	0	Day	Dry	No Contributing Action
9	4.480	08/14/11	Sun	1605	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
10	4.480	01/24/11	Mon	1045	Rear-End	0	0	1	Day	Dry	Followed too Closely
11	4.480	03/20/11	Sun	1400	Rear-End	0	5	0	Day	Dry	Careless or Negligent Manner
12	4.480	09/23/11	Fri	1556	Rear-End	0	0	1	Day	Dry	#N/A
13	4.480	03/03/11	Thu	1505	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
14	4.480	03/13/11	Sun	1443	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
15	4.480	03/13/11	Sun	1520	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
16	0.000	12/09/11	Fri	0045	Overturn/Rollover	0	1	0	Night	Dry	No Contributing Action
17	0.000	09/15/11	Thu	0855	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
17	0	13	4	11	0	1	0	0	1	0	1	1	0	0	0
Percent	0.00%	76.47%	23.53%	64.71%	0.00%	5.88%	0.00%	0.00%	5.88%	0.00%	5.88%	5.88%	0.00%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	13	4	2	15	10	0	0	0	0	0	0	0	0	1	0
Percent	76.47%	23.53%	11.76%	88.24%	58.82%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	5.88%	0.00%

TOTAL ENTERING VEHICLES/ADT: **101,165** SPOT CRASH RATE: **0.460** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: Exit Ramp To Alton Road M.P. 4.430 TO 4.480 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	4.404	08/13/12	Mon	1715	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
2	4.410	03/01/12	Thu	0125	Rear-End	0	2	0	Night	Wet	Careless or Negligent Manner
3	4.423	03/11/12	Sun	1445	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
4	4.430	03/17/12	Sat	1607	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
5	4.430	02/25/12	Sat	0610	Pedestrian	1	0	0	Night	Dry	No Contributing Action
6	4.480	03/12/12	Mon	1735	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
7	0.000	07/22/12	Sun	0715	Angle	0	0	1	Day	Wet	Careless or Negligent Manner
8	0.000	12/07/12	Fri	1223	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
9	0.000	02/16/12	Thu	1130	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
10	0.000	06/03/12	Sun	1535	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
11	0.000	02/19/12	Sun	1300	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
12	0.000	05/20/12	Sun	0730	Guardrail Face	0	0	1	Day	Dry	Careless or Negligent Manner
13	0.000	07/16/12	Mon	1641	Guardrail Face	0	1	0	Day	Wet	Careless or Negligent Manner
14	0.000	03/02/12	Fri	1248	Not Coded	0	0	1	Day	Dry	#N/A
15	0.000	08/25/12	Sat	1329	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
16	0.000	05/21/12	Mon	1505	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
17	0.000	03/11/12	Sun	1445	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
17	1	6	10	12	0	1	0	0	0	0	1	0	2	0	1
Percent	5.88%	35.29%	58.82%	70.59%	0.00%	5.88%	0.00%	0.00%	0.00%	0.00%	5.88%	0.00%	11.76%	0.00%	5.88%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	15	2	4	13	15	0	0	0	0	0	0	0	0	0	0
Percent	88.24%	11.76%	23.53%	76.47%	88.24%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **100,664** SPOT CRASH RATE: **0.463** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: Exit Ramp To Alton Road M.P. 4.430 TO 4.480 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	4.423	04/30/13	Tue	0930	Rear-End	0	0	1	Day	Dry	#N/A
2	4.423	12/30/13	Mon	1444	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
3	4.430	05/25/13	Sat	2310	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
4	4.430	10/04/13	Fri	1030	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
5	4.430	05/06/13	Mon	2350	Rear-End	0	4	0	Night	Dry	Other Contributing Action
6	4.446	02/16/13	Sat	1530	Overturn/Rollover	0	0	1	Day	Dry	Careless or Negligent Manner
7	4.446	05/07/13	Tue	1435	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
8	4.480	02/02/13	Sat	1527	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
9	4.480	05/11/13	Sat	1730	Sideswipe	0	0	1	Day	Dry	Failed To Keep In Proper Lane
10	4.480	02/03/13	Sun	1520	Overturn/Rollover	0	1	0	Day	Dry	No Contributing Action
11	4.480	03/04/13	Mon	1938	Sideswipe	0	0	1	Night	Dry	Improper Passing
12	4.480	10/05/13	Sat	1617	Guardrail Face	0	0	1	Day	Dry	Failed To Keep In Proper Lane
13	4.480	10/30/13	Wed	1242	Sideswipe	0	1	0	Day	Dry	Swerved Or Avoided
14	4.480	09/23/13	Mon	0820	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
15	4.480	10/13/13	Sun	0143	Angle	0	0	1	Night	Dry	No Contributing Action
16	4.480	10/13/13	Sun	0203	Angle	0	0	1	Night	Dry	#N/A
17	4.480	04/06/13	Sat	1500	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
18	0.000	08/19/13	Mon	0831	Rear-End	0	1	0	Day	Dry	No Contributing Action
19	0.000	09/20/13	Fri	0835	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
20	0.000	09/21/13	Sat	1025	Sideswipe	0	0	1	Day	Dry	#N/A
21	0.000	05/16/13	Thu	1541	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
22	0.000	07/21/13	Sun	1608	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
23	0.000	07/26/13	Fri	1352	Sideswipe	0	0	1	Day	Dry	Careless or Negligent Manner
24	0.000	07/19/13	Fri	0841	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
25	0.000	08/18/13	Sun	0004	Rear-End	0	0	1	Night	Dry	Followed too Closely

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
25	0	9	16	15	0	2	0	0	5	0	0	0	1	0	0
Percent	0.00%	36.00%	64.00%	60.00%	0.00%	8.00%	0.00%	0.00%	20.00%	0.00%	0.00%	0.00%	4.00%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Road off Road	DUI	Wrong Way
Total	19	6	2	23	13	0	0	0	0	1	0	0	0	0	0
Percent	76.00%	24.00%	8.00%	92.00%	52.00%	0.00%	0.00%	0.00%	0.00%	4.00%	0.00%	0.00%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **104,779** SPOT CRASH RATE: **0.654** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: Exit Ramp To Alton Road M.P. 4.430 TO 4.480 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	4.423	05/23/14	Fri	1900	Not Coded	0	0	1	Night	Wet	Followed too Closely
2	4.430	05/19/14	Mon	1440	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
3	4.430	03/01/14	Sat	0700	Sideswipe	0	0	1	Day	Dry	Other Contributing Action
4	4.430	05/21/14	Wed	0841	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
5	4.430	02/03/14	Mon	1834	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
6	4.446	08/18/14	Mon	0810	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
7	4.446	08/22/14	Fri	0951	Not Coded	0	0	1	Day	Dry	Careless or Negligent Manner
8	4.446	01/28/14	Tue	1710	Angle	0	0	1	Day	Dry	Careless or Negligent Manner
9	4.480	06/11/14	Wed	2320	Concrete Traffic Barrier	0	1	0	Night	Dry	Careless or Negligent Manner
10	0.000	02/21/14	Fri	1350	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
11	0.000	02/15/14	Sat	1028	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
12	0.000	11/26/14	Wed	1030	Guardrail Face	0	0	1	Day	Dry	Careless or Negligent Manner
13	0.000	02/17/14	Mon	1220	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
14	0.000	08/28/14	Thu	1805	Other Non-Collision	1	0	0	Day	Dry	Improper Passing
15	0.000	03/19/14	Wed	0145	Guardrail End	0	0	1	Night	Dry	Careless or Negligent Manner
16	0.034	02/17/14	Mon	1337	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
16	1	3	12	8	0	1	0	0	1	0	0	0	1	0	0
Percent	6.25%	18.75%	75.00%	50.00%	0.00%	6.25%	0.00%	0.00%	6.25%	0.00%	0.00%	0.00%	6.25%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	12	4	1	15	13	0	0	0	0	1	0	0	0	2	0
Percent	75.00%	25.00%	6.25%	93.75%	81.25%	0.00%	0.00%	0.00%	0.00%	6.25%	0.00%	0.00%	0.00%	12.50%	0.00%

TOTAL ENTERING VEHICLES/ADT: **104,779** SPOT CRASH RATE: **0.654** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004024													STATE ROUTE: #N/A						
INTERSECTING ROADWAY: N/S Ramp To Alton Road													M.P. 0.220		TO 0.410		ENGINEER: FDOT D6		
STUDY PERIOD: FROM 1/ 2011													TO 12/ 2011		COUNTY: Miami-Dade				
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)						
1	0.224	01/27/11	Thu	1020	Pedestrian			1	1	0	Night	Dry	No Contributing Action						
2	0.375	07/31/11	Sun	1555	Rear-End			0	2	0	Day	Dry	Careless or Negligent Manner						
3	0.394	04/22/11	Fri	1240	Rear-End			0	2	0	Day	Dry	Other Contributing Action						
4	0.000	06/12/11	Sun	1630	Not Coded			0	0	1	Day	Wet	Careless or Negligent Manner						
5	0.000	08/07/11	Sun	1825	Guardrail Face			0	0	1	Day	Wet	Careless or Negligent Manner						
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other				
5	1	3	2	2	0	0	0	0	0	0	1	0	1	0	1				
Percent	20.00%	60.00%	40.00%	40.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	20.00%	0.00%	20.00%	0.00%	20.00%				
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way				
Total	4	1	2	3	3	0	0	0	0	0	0	0	0	0	0				
Percent	80.00%	20.00%	40.00%	60.00%	60.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%				
TOTAL ENTERING VEHICLES/ADT: 28,500								SPOT CRASH RATE: 0.481 CRASHES PER MILLION ENTERING VEHICLES											

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION:	87004024										STATE ROUTE:		#N/A			
INTERSECTING ROADWAY:	N/S Ramp To Alton Road										M.P. 0.220 TO 0.410		ENGINEER: FDOT D6			
STUDY PERIOD:	FROM 1/ 2013				TO 12/ 2013				COUNTY: Miami-Dade							
No.	MILE POST	DATE	DAY	TIME	CRASH TYPE				FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	0.261	09/30/13	Mon	0922	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner		
2	0.313	10/31/13	Thu	1948	Rear-End				0	1	0	Night	Dry	Careless or Negligent Manner		
3	0.366	07/22/13	Mon	1635	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner		
4	0.394	10/29/13	Tue	0900	Rear-End				0	0	1	Day	Dry	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other	
4	0	1	3	4	0	0	0	0	0	0	0	0	0	0	0	
Percent	0.00%	25.00%	75.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way	
Total	3	1	0	4	4	0	0	0	0	0	0	0	0	0	0	
Percent	75.00%	25.00%	0.00%	100.00%	100.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
TOTAL ENTERING VEHICLES/ADT: 35,500									SPOT CRASH RATE: 0.309 CRASHES PER MILLION ENTERING VEHICLES							

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004024 STATE ROUTE: #N/A
 INTERSECTING ROADWAY: N/S Ramp To Alton Road M.P. 0.220 TO 0.410 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.313	11/16/14	Sun	1247	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
2	0.318	01/15/14	Wed	0749	Guardrail Face	0	0	1	Day	Wet	Careless or Negligent Manner
3	0.318	12/03/14	Wed	1553	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
4	0.385	03/22/14	Sat	1515	Rear-End	0	3	0	Day	Dry	Careless or Negligent Manner
5	0.394	10/26/14	Sun	1454	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
6	0.413	09/23/14	Tue	1015	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
7	0.413	10/21/14	Tue	0910	Rear-End	0	0	1	Day	Dry	#N/A

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
7	0	2	5	6	0	0	0	0	0	0	0	0	0	0	0
Percent	0.00%	28.57%	71.43%	85.71%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	7	0	2	5	4	0	0	0	0	0	0	0	0	0	0
Percent	100.00%	0.00%	28.57%	71.43%	57.14%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **35,500** SPOT CRASH RATE: **0.309** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004024 STATE ROUTE: #N/A
 INTERSECTING ROADWAY: N/S Ramp To Alton Road M.P. 0.220 TO 0.410 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	0.213	01/10/15	Sat	1430	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
2	0.261	07/08/15	Wed	1000	Rear-End	0	0	1	Day	Dry	No Contributing Action
3	0.261	07/10/15	Fri	1220	Rear-End	0	0	1	Day	Dry	No Contributing Action
4	0.261	03/14/15	Sat	1200	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
5	0.280	07/08/15	Wed	0900	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
6	0.299	11/11/15	Wed	1550	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
7	0.318	06/12/15	Fri	0857	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
8	0.318	10/31/15	Sat	2035	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
9	0.318	12/31/15	Thu	1733	Angle	0	8	0	Night	Dry	Careless or Negligent Manner
10	0.356	11/07/15	Sat	2054	Not Coded	0	1	0	Night	Dry	#N/A
11	0.356	03/22/15	Sun	1427	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
12	0.375	05/04/15	Mon	1648	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
13	0.413	04/28/15	Tue	1252	Sideswipe	0	0	1	Day	Wet	Improper Passing
14	0.413	08/05/15	Wed	1000	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
15	0.413	04/11/15	Sat	0430	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner
16	0.413	02/22/15	Sun	1250	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
17	0.413	01/01/15	Thu	0548	Embankment	0	0	1	Night	Dry	Careless or Negligent Manner
18	0.076	08/29/15	Sat	0315	Other Fixed Object	0	0	1	Night	Dry	#N/A

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
18	0	4	14	13	0	1	0	0	1	0	0	0	0	0	0
Percent	0.00%	22.22%	77.78%	72.22%	0.00%	5.56%	0.00%	0.00%	5.56%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	12	6	2	16	4	0	0	0	0	0	0	0	0	1	0
Percent	66.67%	33.33%	11.11%	88.89%	22.22%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	5.56%	0.00%

TOTAL ENTERING VEHICLES/ADT: **35,500** SPOT CRASH RATE: **0.309** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: Alton Road M.P. 4.844 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2011 TO 12/ 2011 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	4.904	02/04/11	Fri	0757	Rear-End	0	2	0	Day	Dry	No Contributing Action
2	0.000	10/31/11	Mon	2203	Angle	0	1	0	Night	Dry	Ran Red Light
3	0.000	12/21/11	Wed	1745	Rear-End	0	0	1	Day	Dry	No Contributing Action
4	0.000	10/16/11	Sun	1720	Rear-End	0	1	0	Day	Wet	Careless or Negligent Manner
5	0.000	10/04/11	Tue	1430	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
6	0.000	09/01/11	Thu	1604	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
7	0.000	10/06/11	Thu	2025	Left-Turn	0	0	1	Night	Wet	Careless or Negligent Manner
8	0.002	02/27/11	Sun	1231	Sideswipe	0	0	1	Day	Dry	#N/A
9	0.004	01/12/11	Wed	1313	Rear-End	0	1	0	Day	Dry	Followed too Closely
10	0.004	06/29/11	Wed	1523	Parked Motor Vehicle	0	4	0	Day	Dry	Other Contributing Action
11	0.044	06/23/11	Thu	1527	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
12	0.047	11/22/11	Tue	1215	Parked Motor Vehicle	0	0	1	Day	Dry	Careless or Negligent Manner
13	0.049	03/21/11	Mon	1536	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
14	0.049	10/02/11	Sun	1228	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
14	0	8	6	9	0	1	1	0	1	0	0	2	0	0	0
Percent	0.00%	57.14%	42.86%	64.29%	0.00%	7.14%	7.14%	0.00%	7.14%	0.00%	0.00%	14.29%	0.00%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	12	2	2	12	8	0	0	1	0	0	0	0	0	0	0
Percent	85.71%	14.29%	14.29%	85.71%	57.14%	0.00%	0.00%	7.14%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **145,165** SPOT CRASH RATE: **0.264** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: Alton Road M.P. 4.844 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2012 TO 12/ 2012 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	4.872	09/05/12	Wed	0854	Bridge Overhead Structure	0	0	1	Day	Dry	Other Contributing Action
2	4.910	02/19/12	Sun	2340	Rear-End	1	1	0	Night	Dry	Careless or Negligent Manner
3	0.000	01/13/12	Fri	0940	Left-Turn	0	0	1	Day	Dry	No Contributing Action
4	0.000	11/08/12	Thu	1958	Right-Turn	0	0	1	Night	Dry	Failed to Yield Right-Of-Way
5	0.000	12/17/12	Mon	1959	Pedestrian	0	1	0	Night	Dry	No Contributing Action
6	0.005	06/14/12	Thu	1228	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
7	0.019	04/12/12	Thu	1529	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
7	1	4	3	3	0	0	1	1	0	0	1	0	1	0	0
Percent	14.29%	57.14%	42.86%	42.86%	0.00%	0.00%	14.29%	14.29%	0.00%	0.00%	14.29%	0.00%	14.29%	0.00%	0.00%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	4	3	0	7	3	1	0	0	0	0	0	0	0	0	0
Percent	57.14%	42.86%	0.00%	100.00%	42.86%	14.29%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **143,164** SPOT CRASH RATE: **0.134** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: Alton Road M.P. 4.844 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2013 TO 12/ 2013 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)				
1	4.860	03/28/13	Thu	0520	Concrete Traffic Barrier	0	0	1	Night	Dry	Careless or Negligent Manner				
2	4.873	12/02/13	Mon	0510	Sideswipe	0	0	1	Night	Dry	Failed To Keep In Proper Lane				
3	4.891	03/15/13	Fri	0852	Sideswipe	0	0	1	Day	Dry	No Contributing Action				
4	4.905	05/01/13	Wed	1005	Rear-End	0	0	1	Day	Dry	No Contributing Action				
5	4.906	12/15/13	Sun	2240	Not Coded	0	0	1	Night	Wet	#N/A				
6	4.906	09/26/13	Thu	0556	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner				
7	4.909	02/20/13	Wed	1442	Parked Motor Vehicle	0	0	1	Day	Dry	No Contributing Action				
8	4.910	08/03/13	Sat	0502	Guardrail Face	0	0	1	Night	Dry	No Contributing Action				
9	0.000	05/21/13	Tue	2238	Rear-End	0	2	0	Night	Dry	Careless or Negligent Manner				
10	0.000	07/24/13	Wed	1822	Right-Turn	0	0	1	Day	Dry	No Contributing Action				
11	0.000	03/20/13	Wed	1755	Left-Turn	0	1	0	Day	Wet	Failed to Yield Right-Of-Way				
12	0.000	09/04/13	Wed	2220	Rear-End	0	0	1	Night	Dry	Careless or Negligent Manner				
13	0.001	07/03/13	Wed	1236	Rear-End	0	0	1	Day	Dry	No Contributing Action				
14	0.005	04/18/13	Thu	1030	Rear-End	0	2	0	Day	Dry	Other Contributing Action				
15	0.005	11/27/13	Wed	0951	Rear-End	0	0	1	Day	Wet	No Contributing Action				
16	0.009	04/03/13	Wed	1340	Sideswipe	0	0	1	Day	Dry	No Contributing Action				
17	0.009	02/23/13	Sat	2359	Rear-End	0	0	1	Night	Dry	Other Contributing Action				
18	0.009	03/22/13	Fri	1519	Rear-End	0	2	0	Day	Dry	Followed too Closely				
19	0.011	11/27/13	Wed	1237	Rear-End	0	6	0	Day	Dry	No Contributing Action				
20	0.019	02/14/13	Thu	0850	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
21	0.039	03/15/13	Fri	1441	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
22	0.050	05/19/13	Sun	1532	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner				
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
22	0	5	17	13	0	0	1	1	3	0	0	1	2	0	1
Percent	0.00%	22.73%	77.27%	59.09%	0.00%	0.00%	4.55%	4.55%	13.64%	0.00%	0.00%	4.55%	9.09%	0.00%	4.55%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	14	8	3	19	7	1	0	0	0	0	0	0	0	0	0
Percent	63.64%	36.36%	13.64%	86.36%	31.82%	4.55%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
TOTAL ENTERING VEHICLES/ADT: 145,779								SPOT CRASH RATE: 0.413 CRASHES PER MILLION ENTERING VEHICLES							

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: Alton Road M.P. 4.844 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2014 TO 12/ 2014 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE	FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)
1	4.844	03/05/14	Wed	0845	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
2	4.882	02/07/14	Fri	1030	Rear-End	0	0	1	Day	Dry	Careless or Negligent Manner
3	4.882	06/10/14	Tue	0845	Rear-End	0	2	0	Day	Dry	Careless or Negligent Manner
4	4.882	09/22/14	Mon	1042	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
5	4.887	04/13/14	Sun	1220	Rear-End	0	4	0	Day	Dry	Careless or Negligent Manner
6	4.901	04/02/14	Wed	0030	Rear-End	0	0	1	Day	Dry	No Contributing Action
7	4.910	02/06/14	Thu	1544	Left-Turn	0	2	0	Day	Dry	Failed to Yield Right-Of-Way
8	4.910	06/28/14	Sat	0615	Angle	0	2	0	Day	Dry	Ran Red Light
9	0.000	03/06/14	Thu	1707	Rear-End	0	0	1	Day	Wet	No Contributing Action
10	0.000	09/28/14	Sun	1122	Not Coded	0	0	1	Day	Dry	#N/A
11	0.000	09/23/14	Tue	1555	Rear-End	0	0	1	Day	Wet	Careless or Negligent Manner
12	0.000	12/17/14	Wed	1148	Angle	0	0	1	Day	Dry	Failed to Yield Right-Of-Way
13	0.004	11/16/14	Sun	1136	Rear-End	0	1	0	Day	Dry	Careless or Negligent Manner
14	0.028	04/02/14	Wed	0920	Rear-End	0	0	1	Day	Dry	#N/A

Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
14	0	5	9	10	0	2	1	0	0	0	0	0	2	0	1
Percent	0.00%	35.71%	64.29%	71.43%	0.00%	14.29%	7.14%	0.00%	0.00%	0.00%	0.00%	0.00%	14.29%	0.00%	7.14%

Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	14	0	3	11	7	1	0	0	0	0	0	0	0	0	0
Percent	100.00%	0.00%	21.43%	78.57%	50.00%	7.14%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%

TOTAL ENTERING VEHICLES/ADT: **145,779** SPOT CRASH RATE: **0.413** CRASHES PER MILLION ENTERING VEHICLES

**State of Florida Department of Transportation
CRASH SUMMARY**

SECTION: 87004000 STATE ROUTE: 112
 INTERSECTING ROADWAY: Alton Road M.P. 4.844 TO 4.910 ENGINEER: FDOT D6
 STUDY PERIOD: FROM 1/ 2015 TO 12/ 2015 COUNTY: Miami-Dade

No.	MILE POST	DATE	DAY	TIME	CRASH TYPE			FATAL	INJURIES	PROP DAM	DAY / NIGHT	WET / DRY	CONTRIBUTING CAUSE (VEHICLE ONLY)		
1	4.844	10/14/15	Wed	1010	Rear-End			0	0	1	Day	Dry	Careless or Negligent Manner		
2	4.872	05/27/15	Wed	0914	Rear-End			0	0	1	Day	Dry	Careless or Negligent Manner		
3	4.901	12/08/15	Tue	1035	Rear-End			0	1	0	Day	Wet	Careless or Negligent Manner		
4	4.908	09/24/15	Thu	0711	Rear-End			0	0	1	Day	Wet	Careless or Negligent Manner		
5	0.000	12/31/15	Thu	0054	Rear-End			0	1	0	Night	Dry	Careless or Negligent Manner		
6	0.000	01/28/15	Wed	0834	Rear-End			0	0	1	Day	Dry	Followed too Closely		
7	0.000	04/02/15	Thu	0956	Rear-End			0	2	0	Day	Dry	Careless or Negligent Manner		
8	0.000	07/29/15	Wed	1748	Rear-End			0	1	0	Day	Wet	Careless or Negligent Manner		
9	0.000	12/05/15	Sat	0856	Angle			0	0	1	Day	Dry	No Contributing Action		
10	0.000	09/20/15	Sun	2013	Sideswipe			0	0	1	Night	Wet	Careless or Negligent Manner		
11	0.002	01/13/15	Tue	0740	Sideswipe			0	0	1	Day	Wet	Careless or Negligent Manner		
12	0.004	08/22/15	Sat	1224	Rear-End			0	0	1	Day	Dry	Careless or Negligent Manner		
13	0.009	11/20/15	Fri	0010	Rear-End			0	0	1	Night	Wet	Followed too Closely		
14	0.019	06/14/15	Sun	0226	Rear-End			0	1	0	Night	Dry	Followed too Closely		
15	0.019	06/01/15	Mon	1250	Not Coded			0	0	1	Day	Dry	#N/A		
16	0.028	09/22/15	Tue	1419	Rear-End			0	0	1	Day	Dry	Careless or Negligent Manner		
17	0.038	08/15/15	Sat	0319	Rear-End			0	1	0	Night	Dry	Careless or Negligent Manner		
18	0.049	06/26/15	Fri	1430	Rear-End			0	0	1	Day	Dry	Careless or Negligent Manner		
19	0.177	09/23/15	Wed	1401	Angle			0	0	1	Day	Dry	Careless or Negligent Manner		
20	0.181	03/07/15	Sat	1755	Rear-End			0	0	1	Day	Wet	Followed too Closely		
21	0.189	02/10/15	Tue	1157	Angle			0	0	1	Day	Dry	Careless or Negligent Manner		
Total No.	Fatal	Injury	PDO	Rear-End	Head-On	Angle	Left-Turn	Right-Turn	Sideswipe	Backed Into	Ped/Bike	Parked Car	Fixed Object	Ran into Water	Other
21	0	6	15	15	0	3	0	0	2	0	0	0	2	0	1
Percent	0.00%	28.57%	71.43%	71.43%	0.00%	14.29%	0.00%	0.00%	9.52%	0.00%	0.00%	0.00%	9.52%	0.00%	4.76%
Contrib. Cause	Day	Night	Wet	Dry	Careless Driving	FTYRW	Improper Turn	Ran Red Light	Exceeded Speed	Improper Passing	Disreg Cntl Dev	Erratic/Aggress	Ran off Road	DUI	Wrong Way
Total	16	5	7	14	7	1	0	0	0	0	0	0	0	0	0
Percent	76.19%	23.81%	33.33%	66.67%	33.33%	4.76%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%
TOTAL ENTERING VEHICLES/ADT: 145,779								SPOT CRASH RATE: 0.413 CRASHES PER MILLION ENTERING VEHICLES							



APPENDIX F – ENVIRONMENTAL REVIEW SUPPORTING INFORMATION

- COMMUNITY FEATURES
- CONTAMINATED SITES

COMMUNITY FEATURES

Civic Center		
<i>Name</i>	<i>Location</i>	<i>Site No.</i>
One-Mind Entertainment Center	5660 NW 7 Avenue	32

Libraries		
<i>Name</i>	<i>Location</i>	<i>Site No.</i>
Edison Center Branch	531 NW 62 Street	80

Fire Stations		
<i>Name</i>	<i>Location</i>	<i>Site No.</i>
Miami Municipal Fire Station 6	701 NW 36 Street	40

Health Care Facilities		
<i>Name</i>	<i>Location</i>	<i>Site No.</i>
Borinquen Heal Care Center (BMC)	3601 Federal Highway	18
Better Way of Miami	800 NW 28 Street	110
St. John Bosco Clinic (SJBC)	730 NW 34 Street	30
BMC - Behavioral Health Resource Center	100 NE 38 Street Suite 5	104
JHS - Jackson Multispecialty Center - Biscayne	3801 Biscayne Boulevard	114
Mount Sinai Medical Center	4300 Alton Road	125

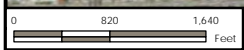
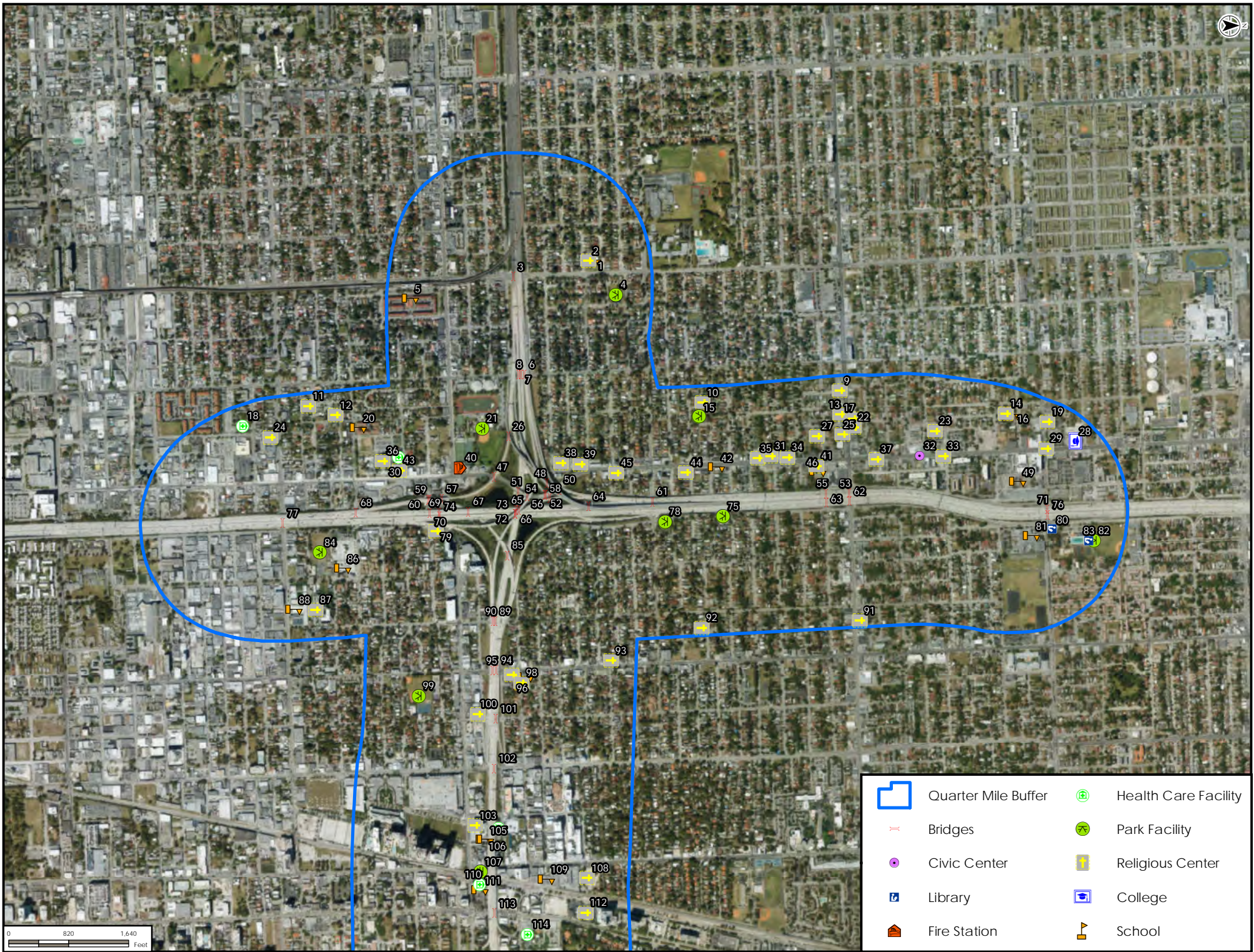
Park Facility		
<i>Name</i>	<i>Location</i>	<i>Site No.</i>
Albert Pallot Park	NE 39 Terrace	116
Woodson Mini Park	3620 NE 2 Avenue	107
Roberto Clemente Park	101 NW 34 Street	99
Moore Park	765 NW 36 Street	21
North Bay Vista Park	NW 49 Street	75
South Bay Vista Park	NW 47 Street	78
West Buena Vista Mini Park	4498 NW 11th Plaza	4
Miller J Dawkins Mini Park	3625 NE 2 Avenue	15
Robert E Lee Park	3000 NW 6 Avenue	84
Athalie Range Park	525 NW 62 Street	82
Martell Park	NE 36 Street / NE 7 Avenue	117
Stearns Park	NE 7 Avenue / NE 38 Street	115
Polo Park	4301 N Michigan Avenue	137

Religious Facilities		
Name	Location	Site No.
Iglesia Presbiteriana Pan Amer	796 NW 29 Street	24
Bible Baptist Temple Inc	865 NW 29 Terrace	11
Church Of God Of Holiness	333 NW 30 Street	87
Bethel Christian Church	3030 NW 8 Avenue	12
Corpus Christi Catholic Church	3220 NW 7 Avenue	36
Salon Del Reino De Los Testigos De Jehovah	3350 NW 7 Avenue	45
Eglise De Dieu L'Arche De L'Eternel	591 NW 35 Street	79
Holy Cross Episcopal Church	121 NE 36 Street	103
New Life Family Center Catholic Charities	3620 NW 1 Avenue	100
St Patrick Church	3716 Garden Avenue	136
Iglesia Cristiana Refugio De Amor	3814 NW 2 Avenue	96
Full Gospel Academy	173 NW 39 Street	98
Beth Israel Congregation	770 W 40 Street	139
Ohaare Ezra Sepharadic Congregation	945 Arthur Godfrey Road	134
Tree Of Life Deliverance Ministry	4150 NW 7 Avenue	38
Truth Missionary Baptist Church	4220 NW 7 Avenue	39
Temple Beth Sholom	4144 Chase Avenue	138
Greater Miami Jewish Fed	4200 Biscayne Boulevard	112
Jerusalem Seventh Day Adventist Church	220 NE 43 Street	108
New Covenant Presbyterian Church	4300 NW 12 Avenue	2
Church Of Christ Redeemer	4314 NW 2 Avenue	93
Solid Rock Deliverance Center	4401 NW 7 Avenue	45
Mount Sinai Baptist Church	698 NW 47 Terrace	44
New Bethany Baptist Church	268 NW 48 Street	92
St Mary'S Wesleyan Methodist	4798 NW 8 Avenue	10
Christian Missionary Church	5022 NW 7 Avenue	35
Faith Anointed Ministries	5126 NW 7 Avenue	31
New Start Outreach Christian Center	5126 NW 7 Avenue	34
Masjid Al-Ansar	5245 NW 7 Avenue	41
New Providence Missionary Baptist Church	760 NW 53 Street	27
First Redeem Baptist Church	880 NW 54 Street	9
Iglesia Jovenes De Miami En Accion	790 NW 54 Street	13
New St Mark Missionary Baptist	744 NW 54 Street	25
Bethany Baptist Ms Church	787 NW 54 Street	17
United Christian World Outreach Ministries	769 NW 54 Street	22
Eglise De Dieu Independante	275 NW 54 Street	91
Solid Rock Faith Ministries	5525 NW 7 Avenue	37

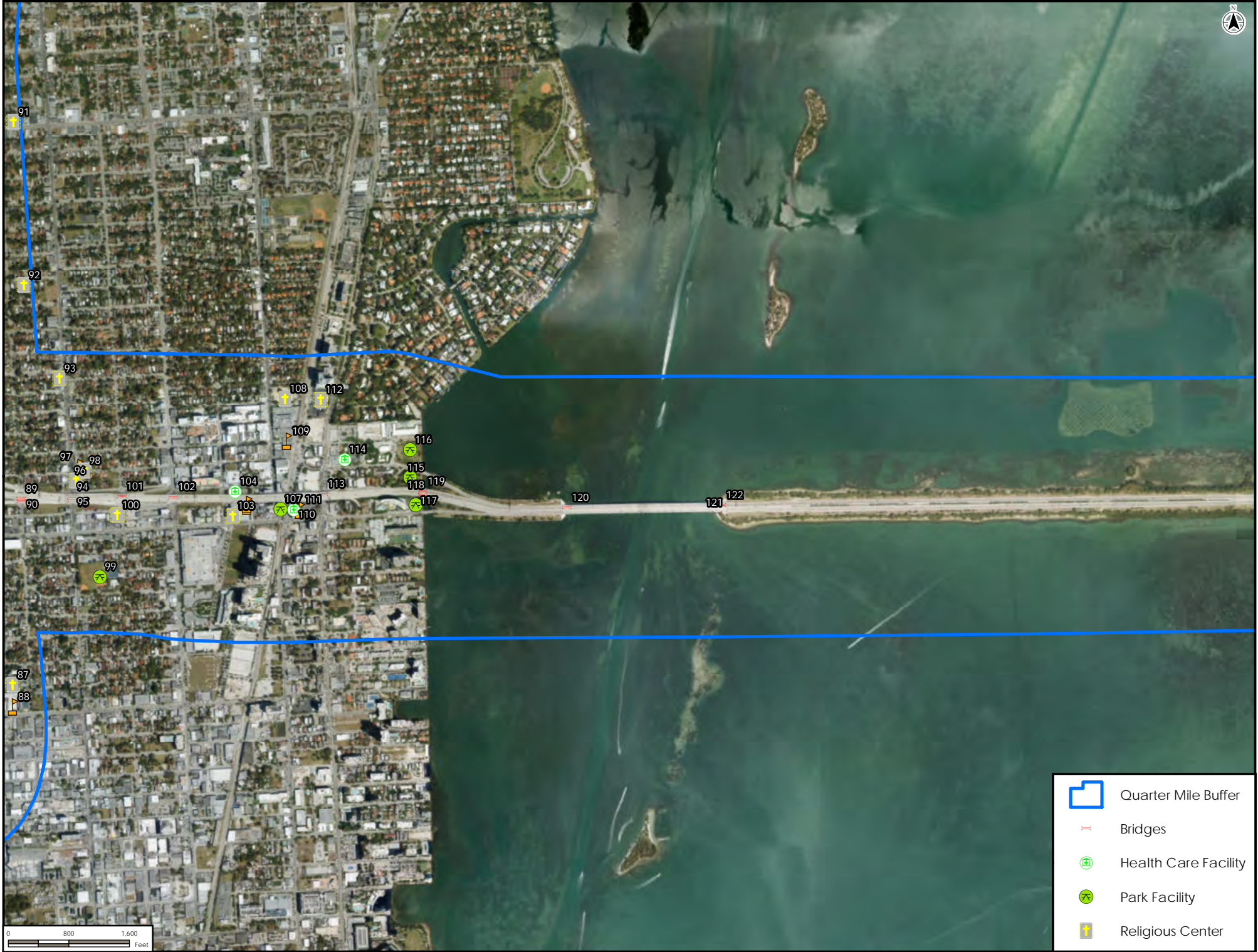
Religious Facilities		
<i>Name</i>	<i>Location</i>	<i>Site No.</i>
Friendship Missionary Baptist	740 NW 58 Street	23
Bible Way Prayer Mission	5789 NW 7 Avenue	33
Church Of The Open Door	6001 NW 8 Avenue	14
St Luke Freewill Baptist Church	749 NW 62 Street	29
Grace & Truth Outreach Ministries	749 NW 62 Street	19






Colleges		
<i>Name</i>	<i>Location</i>	<i>Site No.</i>
Talmudic College of Florida	4000 Alton Road	126
Miami Dade College Entrepreneurial Education Center	6300 NW 7 Avenue	28

Schools		
<i>Name</i>	<i>Location</i>	<i>Site No.</i>
Jose De Diego Middle	3100 NW 5 Avenue	86
Miami Edison Senior High	6161 NW 5 Court	81
Eneida Massas Hartner Elementary	401 NW 28 Street	88
Design & Architecture Senior High	4001 NE 2 Avenue	109
Nautilus Middle	4301 N Michigan Avenue	135

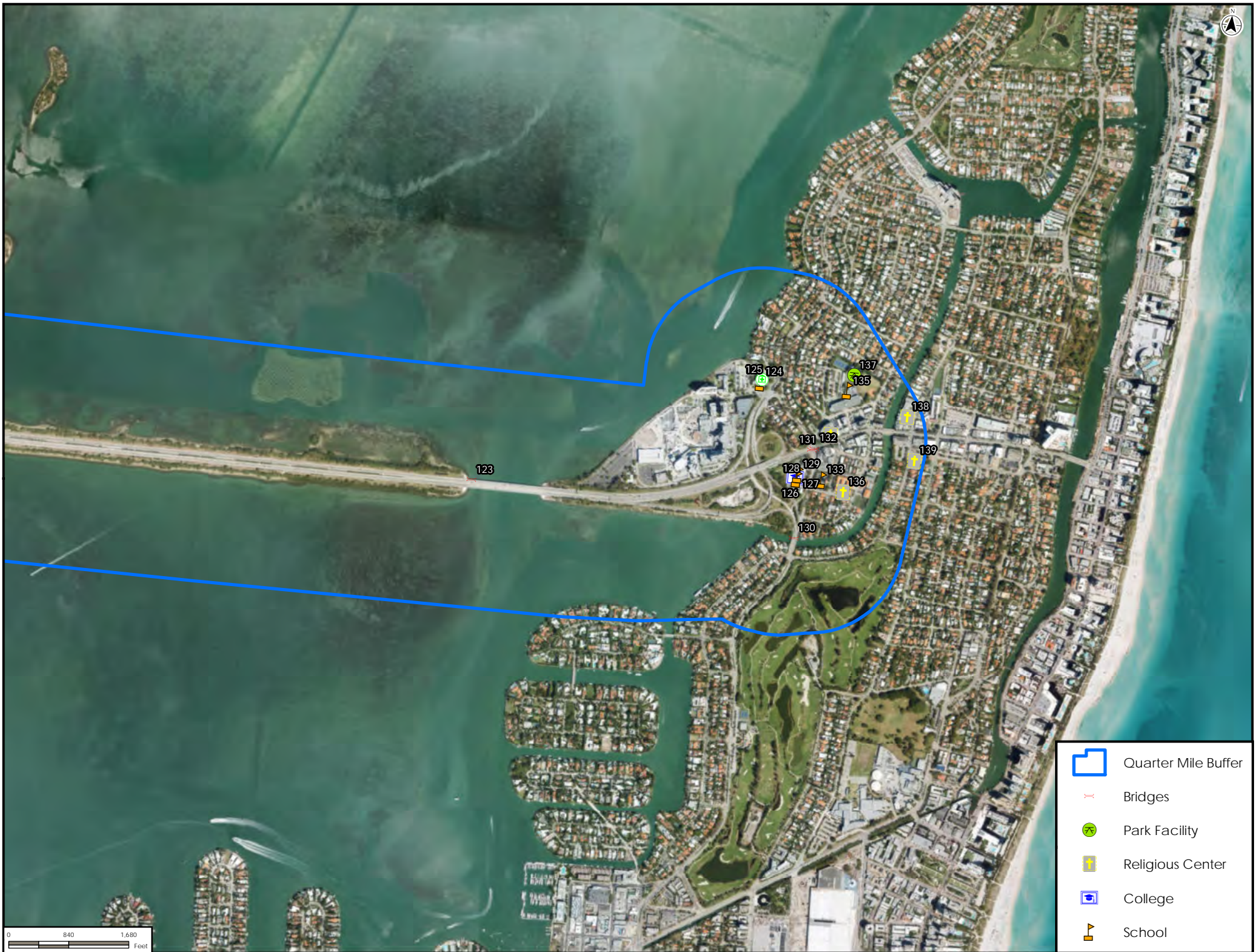


	Quarter Mile Buffer		Health Care Facility
	Bridges		Park Facility
	Civic Center		Religious Center
	Library		College
	Fire Station		School



-  Quarter Mile Buffer
-  Bridges
-  Health Care Facility
-  Park Facility
-  Religious Center

0 800 1,600
Feet



- Quarter Mile Buffer
- Bridges
- Park Facility
- Religious Center
- College
- School

0 840 1,680 Feet

CONTAMINATED SITES

Hazardous Waste

<i>Name</i>	<i>Location</i>	<i>Site No.</i>
Noble Auto Paint	1141 NW 36 Street	2
Organizational Maintenance	700 NW 28 Street	7
Miami Auto Mall Inc.	400 NW 54 Street	23
Target Store #T2188	3401 North Miami Ave	27
Robert A Ballard Armory	700 NW 28 Street	8
Miami Dade County ISD Fleet MGNT Shop #1	703 NW 25 Street	12

Dry Cleaner

<i>Name</i>	<i>Location</i>	<i>Site No.</i>
Parisian Cleaners	555 NW 29 Street	18
La France Cleaners	244 NW 35 Street	24

Superfund Site

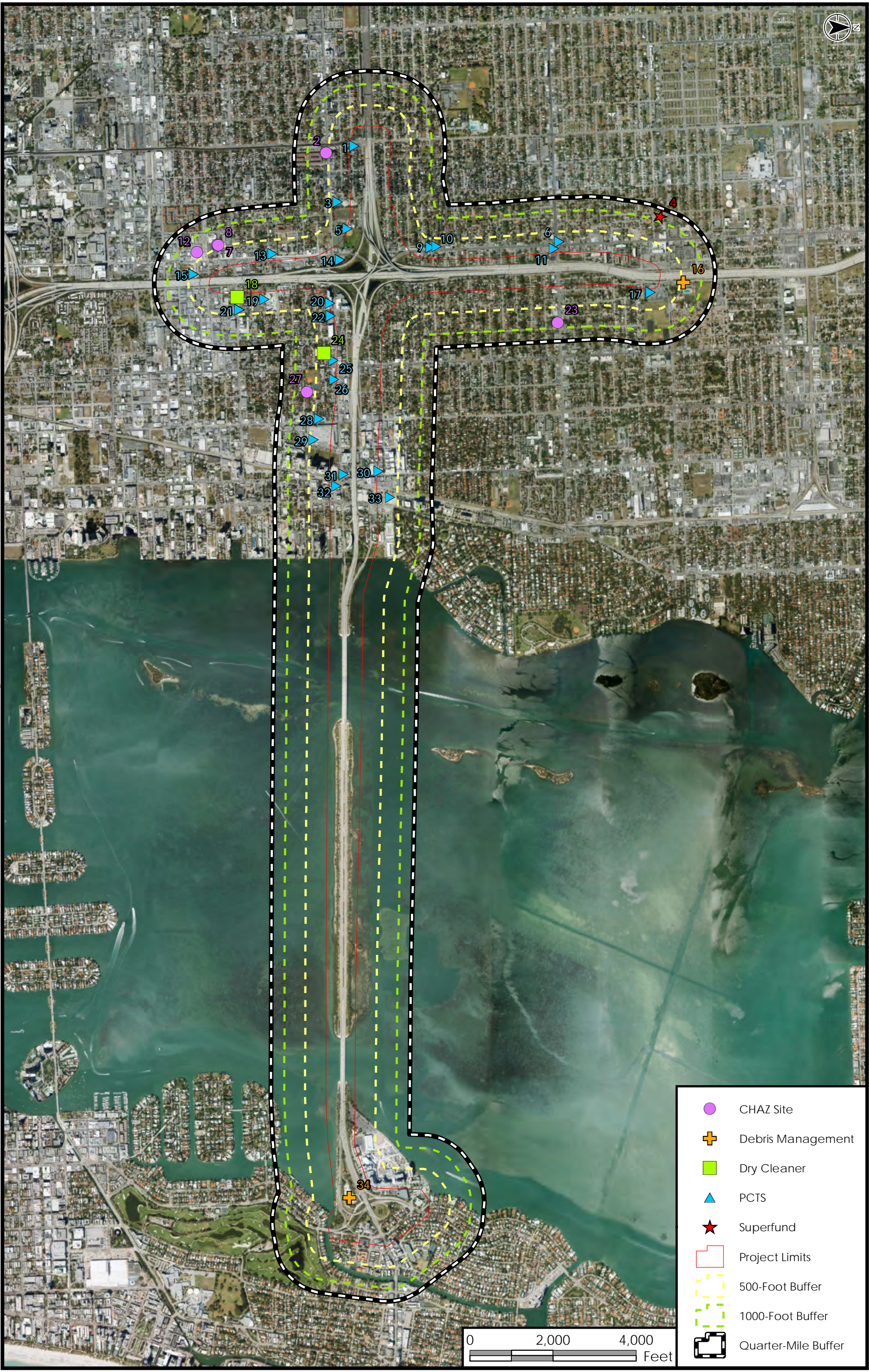
<i>Name</i>	<i>Location</i>	<i>Site No.</i>
Continental Cleaners	798 NW 62 Street	4










Petroleum Contamination Monitoring Discharges (PCTS)

<i>Name</i>	<i>Type</i>	<i>Location</i>	<i>Site No.</i>
McArthur Dairy Inc.	Fuel user/Non-retail	2451 NW 7 Avenue	15
Miami-Dade Water & Sewer 36th Street Maintenance	Local Government	3625 NW 10 Avenue	3
Twin Station Sunoco	Retail Station	200 NW 36 Street	25
Sunshine #210	Retail Station	5343 NW 7 Avenue	11
Sunshine #248	Retail Station	3601 NW 7 Avenue	14
Capital Roofing Co Inc.	Fuel user/Non-retail	3100 NW 7 Avenue	13
BP #00368530	Retail Station	501 NW 29 Street	21
Shell Service Station - Design District	Retail Station	3557 NE 2 Avenue	32
International Gas Corp	Retail Station	5402 NW 7 Avenue	6
FL East Coast Railway Seaboard Marine Ltd.	Other Regulated Facility	100 NE 36 Street	29
Former Gas Station-Sub Shop	Fuel user/Non-retail	3600 NE 2 Avenue	31
Acme Quality Sales Inc.	Fuel user/Non-retail	144 NW 36 Street	26
Los Suenos Property	Residential	500 NW 36 Street	20
Miami Decorating & Design Center	Fuel user/Non-retail	3930 NE 2 Avenue	30
Bryants Towing Service	Fuel user/Non-retail	4420 NW 7 Avenue	9
Jones Station	Retail Station	4500 NW 7 Avenue	10
Amelia Corp MacMillian	Retail Station	3801 NW 12 Avenue	1
M-DCPS Miami Edison Senior High School	Local Government	6161 NW 5 Court	17
Robert E Lee Junior High School	Local Government	3100 NW 5 Avenue	19
Miami City Fire Station #6	Local Government	701 NW 36 Street	5
Diaz Transmission	Fuel user/Non-retail	480-490 NW 36 Street	22
Miami Arts Charter School	Fuel user/Non-retail	3900 Biscayne Boulevard	33
Miami Crane Service Inc.	Fuel user/Non-retail	3452 N Miami Avenue	28

Debris

<i>Name</i>	<i>Type</i>	<i>Location</i>	<i>Site No.</i>
FDOT Miami Beach	Solid Waste	Off Alton Rd, South of I-195	34
Athalie Range Park	Solid Waste	525 NW 62 Street	16



-  CHAZ Site
-  Debris Management
-  Dry Cleaner
-  PCTS
-  Superfund
-  Project Limits
-  500-Foot Buffer
-  1000-Foot Buffer
-  Quarter-Mile Buffer

